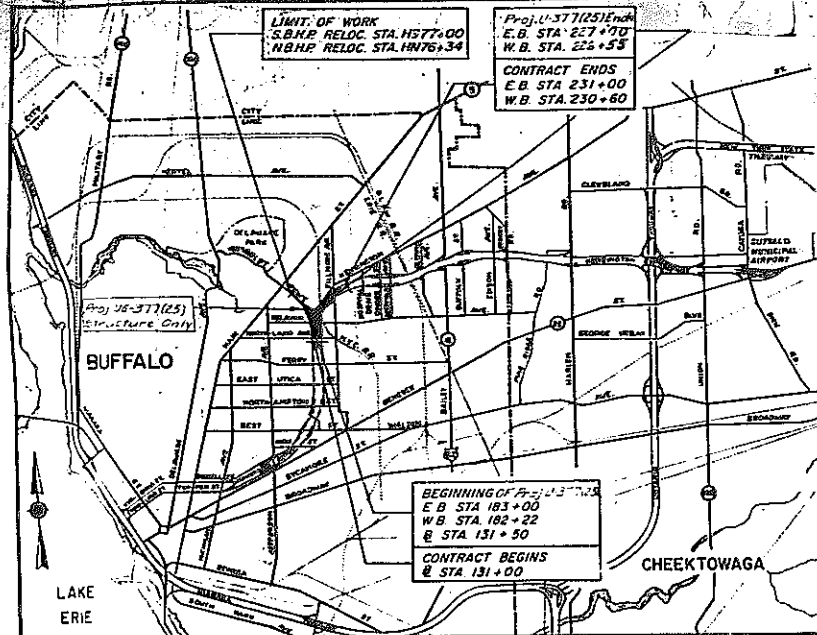


FAC 63-2



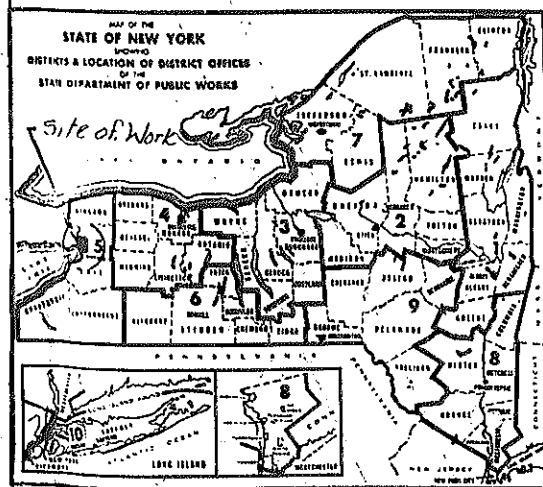
LOCATION MAP

1 MILE

Approx Location: Kensington Expressway Arterial Highway in the City of Buffalo between Buffalo and Fillmore Ave.

DESIGN TRAFFIC DATA
 Design Class U
 Estimated 1985 Traffic D.H. 2300
 A.A.T. 33000

- MALL WIDTH - 10.5' to 33.0'
- DESIGN SPEED INTERCHANGE AREA - 50 MPH
- DESIGN SPEED EXPRESSWAY - 60 MPH
- MAXIMUM EXPRESSWAY GRADE - 2.26 %
- MINIMUM EXPRESSWAY GRADE - 0.70 %
- MAXIMUM EXPRESSWAY CURVATURE - 8°-00'
- MINIMUM EXPRESSWAY S.S.D. - 475'



Note:
 Item 2 replaces Items 3 & 4 where they appear on these plans.
 Item 5 replaces Items 5E & 5R where they appear on these plans.

CONCRETE PAVEMENT CORE RECORD							
CORE No.	STATION	FIELD HEIGHT INCHES	LAB HEIGHT INCHES	CORE No.	STATION	FIELD HEIGHT INCHES	LAB HEIGHT INCHES
NORTHBOUND				WESTBOUND			
195	204+30	8 3/4	8 3/4	97	226+00	8 1/2	9
196	212+00	8 1/2	8 3/4	98	220+00	8	8 1/4
197	214+50	8	8 1/4	99	218+00	8 3/4	9
198	217+00	8 1/4	8 1/2	100	210+00	8	8
199	221+50	8 3/4	8 3/8	101	208+00	8	8 3/8
Eg 221+00 = HN 68+85				102	200+00	8 3/4	8 3/4
200	HN 71+00	8 3/8	8 3/8	103	196+00	8 3/8	8 3/8
SOUTHBOUND				104	190+00	8	8
201	NS 72+50	7 3/8	7 3/8	EASTBOUND			
202	NS 70+00	8 1/4	8 3/8	105	225+00	8 1/4	8 1/4
Eg NS 68+85 = 61+29				106	220+00	8	8
203	7+60	8	8 3/8	107	215+00	8 1/4	8 1/4
204	12+00	8 3/8	8 1/2	RAMP FA			
205	15+00	8 1/4	8 1/2	108	16+50	8	8 1/8
206	17+50	8	8 1/4	109	21+00	8	8
No. Bound Service Rd.				RAMP HA			
90	20+00	8 1/2	8 1/2	110	18+00	8	8
91	17+00	8 1/4	8 1/2	111	205+00	8	8
92	11+50	8 1/4	8 1/4	112	200+00	8 1/2	8 1/2
93	9+50	8 1/2	8 1/4	RAMP HB			
94	L.O. 15+00	8 1/2	8 1/2	113	20+00	8 1/8	8 1/8
95	7+00	8 1/2	8 1/2	RAMP FB			
96	18+50	8 1/4	8 1/4				

RECORD PLAN

STATE OF NEW YORK
 DEPARTMENT OF PUBLIC WORKS
 DIVISION OF CONSTRUCTION

PLANS FOR CONSTRUCTING WITH FEDERAL AID, THE
CITY OF BUFFALO: KENSINGTON EXPRESSWAY ARTERIAL HIGHWAY
 SECTIONS 2 AND 3
 (Northland Avenue to Fillmore Avenue)
CONTRACT No. 3

From Station 131+00 B.L. to Station W.B. 230+60, a length of 0.92 mile (Plus 3.58 miles of Access) in the City of Buffalo

173 SHEETS
 F.A. PROJECT No. U-UG-377 (25) CONTRACT No. F.A.C. 63-2
ERIE COUNTY

CONTRACT	CONTRACTOR	ENGINEER	COST
DATED - June 26, 1965	S. J. GROVES & SONS	N.W. KRAFF - DIST. ENG.	EST. \$716,386.47
BEGAN - June 26, 1965	201 SOUTH MAIN ST.	G.S. PUCIO - ENG. & ARCHT.	\$3,240,482.28
COMP. - Nov. 4, 1965	SYRACUSE, N.Y.	1963-1966	
ARCHD - NOV 18, 1965			

PAVEMENT						
TYPE	WIDTH	DEPTH	THICK	SO. YDS.	MATERIALS	B.T. Nos.
CONC. FOUNDATION FOR PAVEMENT	1-3-6	12	8" UNIFORM	66,002	CEMENT - LENIGH CEMENT CO., LACKAWANNA NY SAND - BUFFALO GRAVEL PRODUCTS, BUFFALO NY SLAG - BUFFALO SLAG CORP., WOODLAWN NY	VARIES 6118+62469 62927
TOP MACADAM FINISH		1 1/2"	BRAND 1/2" TOP		ASPHALT - FRONTIER OIL, BUFFALO NY STONE - BUFFALO CRUSHED STONE CO. B.F.L.O. NY FILLER - FRONTIER OIL, BUFFALO NY	VARIES VARIES
BRIDGES	ITEM OR PART	MATERIALS			B.T. Nos.	
	SUB-STRUCTURE & SUPER-STRUCTURE	CEMENT - LENIGH CEMENT CO., WOODLAWN, NY SAND - BUFFALO GRAVEL PRODUCTS, BUFFALO NY SLAG - BUFFALO SLAG CORP., WOODLAWN, NY			VARIES 6118+62469 62927	
	BAR REINFORCING STEEL	REPUBLIC STEEL CORP., BUFFALO NY			VARIES	

Index Pg. 9

TYPE OF CONSTRUCTION

- Asphalt Concrete, Type 2A - 0.83 Mile
- Asphalt Concrete, Type 2A (Access) - 3.58 Miles
- Miscellaneous Work - 0.09 Mile

Including

- H.G.S., Delavan Avenue over Expressway, 3 Span, Comp. W. F. Beam, 1 @ 69' 6", 1 @ 63' 0" and 1 @ 53' 6" c. to c. bearings
- H.G.S., E.B. Ramp HA over Expressway, 3 Span, Comp. W. F. Beam, 1 @ 107' 12", 1 @ 83' 53" & 1 @ 58' 20" c. to c. bearings
- H.G.S., N.B. Humboldt Parkway over Expressway, 6 Span, Comp. W. F. Beam, 1 @ 93' 35", 1 @ 37' 12", 1 @ 63' 02", 1 @ 110' 85", 1 @ 94' 48" & 1 @ 52' 01" c. to c. bearings
- H.G.S., Fillmore Avenue over Expressway, 2 Span, Comp. W. F. Beam, 2 @ 89' 6" Each c. to c. brgs.
- R.R.G.S., New York Central Railroad over Expressway, 2 Span through Girder, 1 @ 117' 6" & 1 @ 127' 0" c. to c. bearings

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	1 R	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

Pedestrian Overpass E.B. Station 191+09, 6 Span, W. F. Beam and Slab Construction, 2 @ 30' 0", 2 @ 31' 3", 1 @ 101' 0" and 1 @ 102' 0" c. to c. bearings

STANDARD STRUCTURE SHEETS
 50-34, 51-21, 52-2C, 52-2W, 54-9, 54-18P, 55-51, 56-1, 56-52, 58-1, 58-60, 59-11WR1, 59-48A, 59-48B, 59-48D1, 59-48D2, 60-19A, 60-41, 60-48E, 60-48FR1, 60-53, 61-9HB, 61-11, 61-11XR1, 61-19, 61-45, 61-45A, 61-48C, 61-48G, 61-48H, 61-61R1, 61-62C, 61-106, 61-107, 61-109A, 61-109B, 62-4, 62-6, 62-7, 62-11C, 62-17, 62-43A, 62-43B, 62-45B, 62-48Ca, 62-48J, 62-48X, 62-49

All work contemplated under this contract to be covered by and in conformity with the specifications adopted January 2, 1957, except as modified on these plans and in the Itemized Proposal.

PREPARED AND RECOMMENDED BY:
 DE LEUW, CATHER AND BRILL ENGINEERS-ARCHITECTS

C.A. B. O. O. NEW YORK STATE PROFESSIONAL ENGINEER LICENSE NO. 31638

NEW YORK STATE
 DEPARTMENT OF PUBLIC WORKS

APPROVED:

Norman K. Kraft DISTRICT ENGINEER, DATE 9/2/65

John J. Raymond ASST. DEPUTY CHIEF ENGINEER, DATE 9/2/65

V. J. Burns DEPUTY CHIEF ENGINEER, DATE 12/17/65

R. W. Sweet CHIEF ENGINEER, DATE 12/17/65

CITY OF BUFFALO

APPROVED:

Chester A. Koval CHESTER A. KOVAL, MAYOR, CITY OF BUFFALO, DATE 9/2/65

DEPARTMENT OF COMMERCE
 BUREAU OF PUBLIC ROADS

APPROVED:

DIVISION ENGINEER DATE

TITLE SHEET

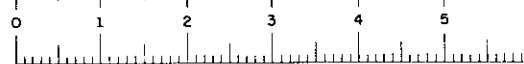
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS
 DRAWN BY: A. J. WEZKA
 CHECKED BY: R. K. CONNERS
 NEW YORK

Index On Page 9

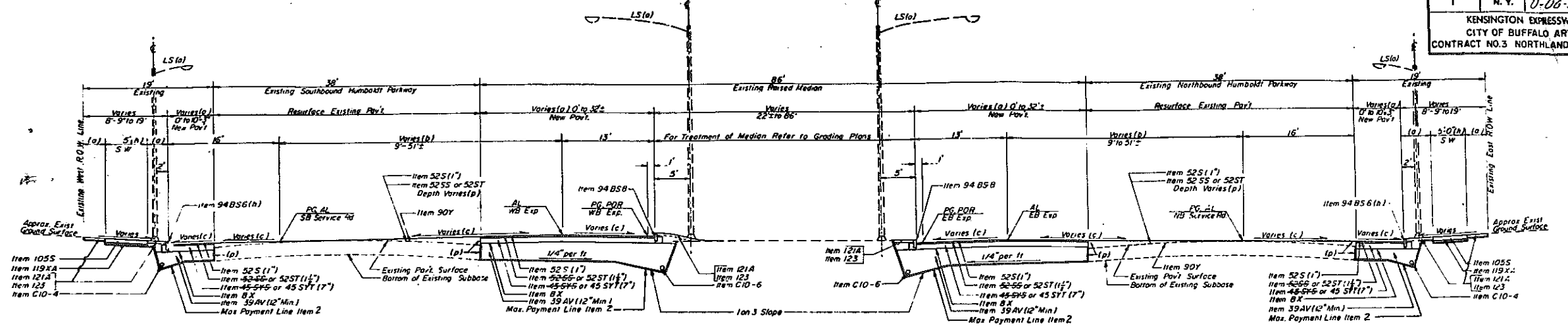
RECORD PLANS
 MADE BY - A. J. WEZKA
 CHECKED BY - R. K. CONNERS

FAC 63-2



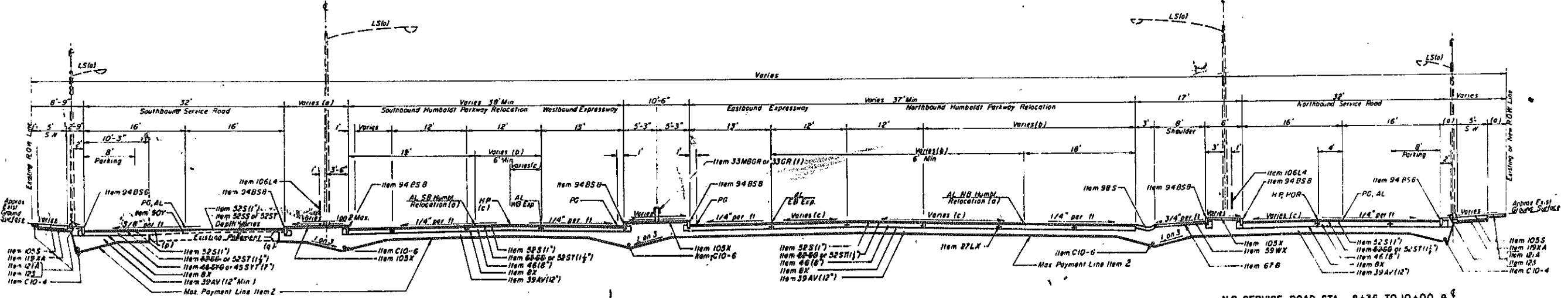
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377 (25)	2 R 1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



W.B. EXPRESSWAY STA. 182+22 TO 187+22

E.B. EXPRESSWAY STA. 183+00 TO 188+00



S.B. SERVICE ROAD STA. 136+97 TO 145+40

W.B. EXPRESSWAY STA. 187+75 TO 196+12

E.B. EXPRESSWAY STA. 188+54 TO 196+73

N.B. SERVICE ROAD STA. 8+36 TO 10+00 & N.B. SERVICE ROAD STA. 13+90 TO 17+22

N.B. SERVICE ROAD STA. 10+00 TO 13+00

DESCRIPTION OF ITEMS

- Item 4 Unclassified Excavation
- Item 7A Trimming Road Section
- Item 8X Preparing Fine Grade
- Item C10-4 Porous Concrete Pipe Underdrain - 4"
- Item C10-6 Porous Concrete Pipe Underdrain - 6"
- Item 27LX Longitudinal Joint Supports - Optional
- Item 32HS Resetting Guide Railing
- Item 33OR Galvanized Beam Type Guide Railing (SL Posts)
- Item 33MR Galvanized Beam Type Wall Barrier (SH Posts)
- Item 37AX Metal Railing
- Item 39AT Foundation Course Granular Material Type A
- Item 45S1E Bottom Course Asphalt-Concrete Type 1A (Slag Aggregate)
- Item 45S1T Bottom Course Asphalt Concrete Type 1A (Stone Aggregate)
- Item 46 Concrete Foundation for Pavement (8")
- Item 52S Asphalt Concrete - Type 2A Top Course
- Item 52SE Asphalt-Concrete - Type 2A Binder Course - Slag Aggregate
- Item 52ST Asphalt Concrete - Type 2A Binder Course - Stone Aggregate
- Item 59X Bituminous Stabilized Course (Including Shoulders)
- Item 59C Stabilized Gravel Surface Course - Slag Aggregate Treatment (incl. Shoulders)
- Item 67B Hotmix Curing Liquid Asphalt - 1 1/2 Gal. per Sq. Yd.
- Item 70B Bituminous Material - Grade-C
- Item 90Y Clearing Existing Pavement

- Item 94AS Stone Curb (4") Neustable
- Item 94AS Stone Curb (6") Sloped
- Item 94BS Stone Curb (6") Vertical
- Item 94BSB Stone Curb (8") Vertical
- Item 94BSB Stone Curb (10") Vertical
- Item 96S Resetting Old Curb
- Item 105S Concrete Sidewalk (6")
- Item 105X Concrete Sidewalk (4")
- Item 106L Chain Link Fence (4")
- Item 106L Chain Link Fence (6")
- Item 119KA Gravel, Slag or Stone Fill (3")
- Item 121A Topsoil Furnished and Placed (4")
- Item 123 Sodding
- Item 124 Sodding (1 1/2")
- Item 155 Broken Rock Trench, Drilling and Blasting
- Item 160 Center-to-Center Pavement
- Item 600 Temporary Wire Fence 5' High
- Item 16A Furnishing and Applying Calcium Chloride

REFERENCES

- (a) For details refer to Alignment and Paving Plans.
- (b) For details refer to Alignment Data, Sheet No. 20.
- (c) For details refer to Profiles and Grading Plans.
- (d) For details refer to Grading Plans.
- (e) For details and limits of underdrain refer to Table, Sheet No. 18.
- (f) For limits of Guide Railing and Wall Barrier refer to Alignment and Paving Plans, Detail on Sheet No. 15 and Table on Sheet No. 15.
- (g) For limits of Median Paving refer to Alignment and Paving Plans.
- (h) For limits of New Sidewalk and Curb refer to Alignment and Paving Plans.
- (i) Height of curb varies as shown on Alignment and Paving Plans.
- (j) For location of Fencing, refer to Grading and Paving Plans.
- (k) For limits of Curb Concrete to Pavement refer to Alignment and Paving Plans (Item 361).
- (l) For locations refer to Detail Sheets.
- (m) For location and details of Electrical Distribution refer to Sheets 53 through 59.
- (n) For details refer to Sheet No. 16.
- (o) For details refer to Sheet No. 11.

GENERAL NOTES

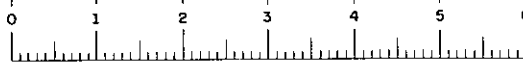
1. Limiting Stations designating Typical Sections are only approximate. Alignment and Paving Plans, Bridge and Utility Plans, and Grading and Paving Plans shall be the final guide for all Roadway Sections. All Typical Sections are drawn looking North or East unless otherwise noted.
2. For Curb, Outlet and Pavement Details refer to Sheet Nos. 11 and 15.
3. Reusable stone curbs affected by the new construction shall be reset under Item 96S as ordered by the Engineer.
4. For variations in the grading of side slopes and the location of Final Bank Lines refer to Grading and Paving Plans.
5. For details of roadside development refer to Sheet No. 17.
6. Gravel surfaced areas not otherwise treated shall be fertilized and seeded (in accordance with Item 125).
7. Any roadway excavation below the Maximum Payment Line for Item 39AV shall be backfilled with Item 39AV at the Contractor's expense.
8. The Maximum Payment Line for Item 39AV, in rock, will be the line shown 3" above the Maximum Payment Line for Item 2.
9. Concrete foundation for Pavement (Item 46) may be placed to a maximum width of 25' at one time at locations where this would not interfere with maintenance of traffic. Refer to standard Sheet 62-48X and Special Notes in Proposal.
10. Locate and outlet pipe underdrain and rock trench underdrain to drainage structures as directed in Table of underdrain on Sheet No. 18.
11. Where Item 39AV notes as backfill around pipe, including pipe underdrain, all material to be placed within 6" of pipe shall pass a 2 1/2" sieve.

LEGEND

- AL - Alignment Line
- PG - Profile Grade Line
- POR - Point of Rotation
- HP - High Point
- SG - Sidewalk
- VC - Vertical Curve
- LS - Light Standard
- EB - Eastbound
- WB - Westbound
- NB - Northbound
- SB - Southbound

REVISION TO ITEMS 45SY5, 52S5, 59C, 67B, 70B

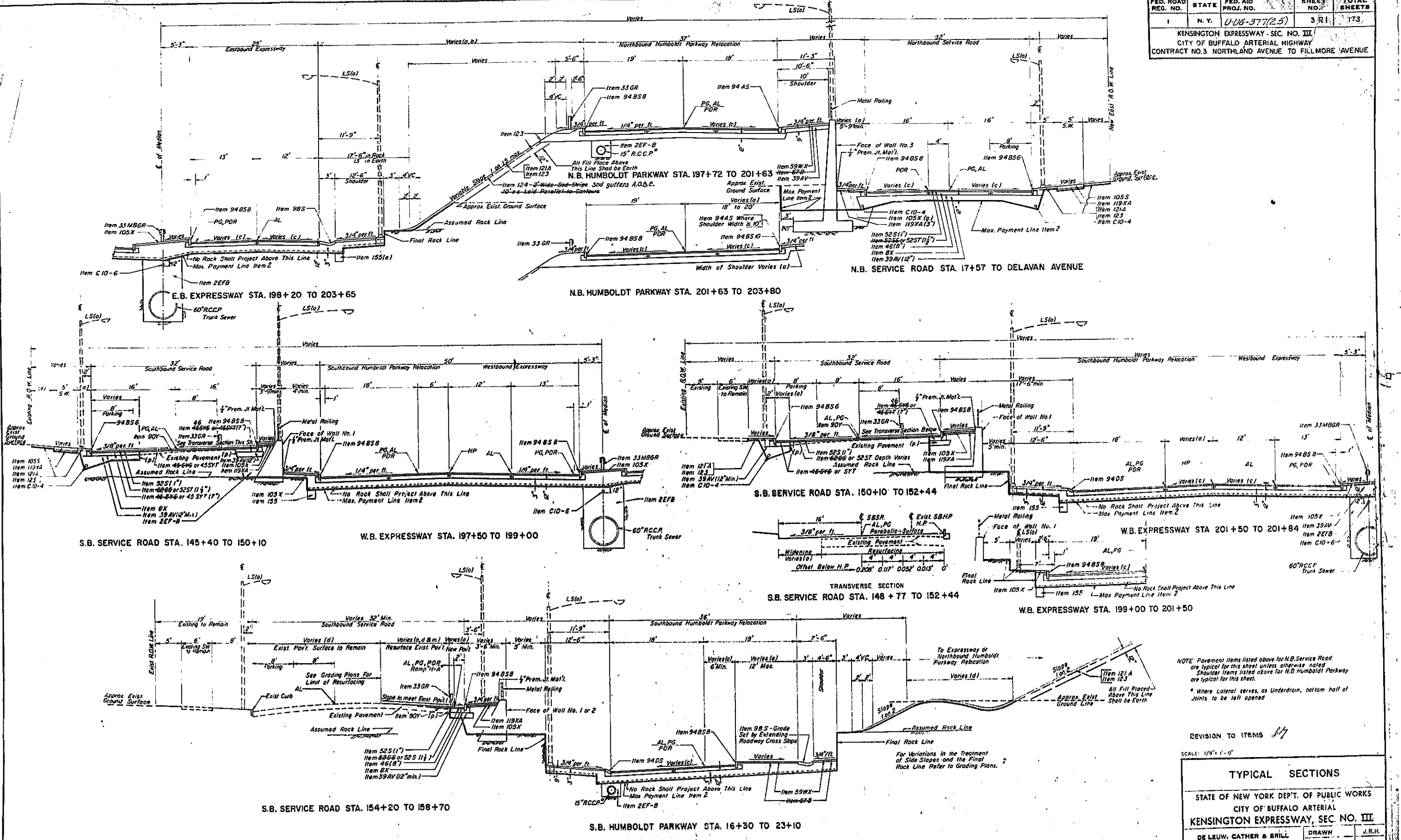
TYPICAL SECTIONS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.L. J.R.H. G.J.B.



FAC 68-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	3 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

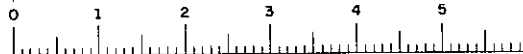


NOTE: Pavement items listed above for N.B. Service Road are typical for this sheet unless otherwise noted. Shoulder items listed above for N.B. Humboldt Parkway are typical for this sheet.
* Where Lateral serves as Underdrain, bottom half of joints to be left opened.

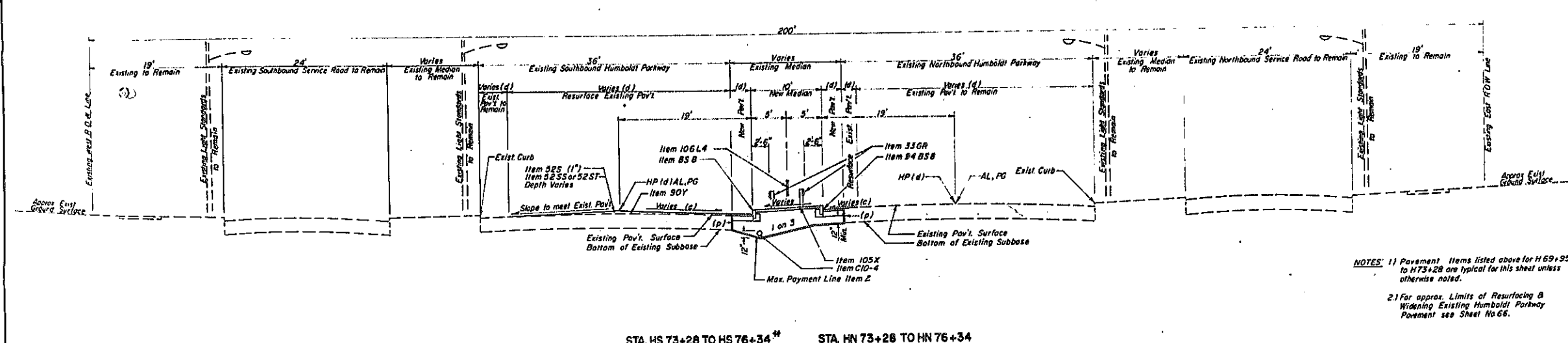
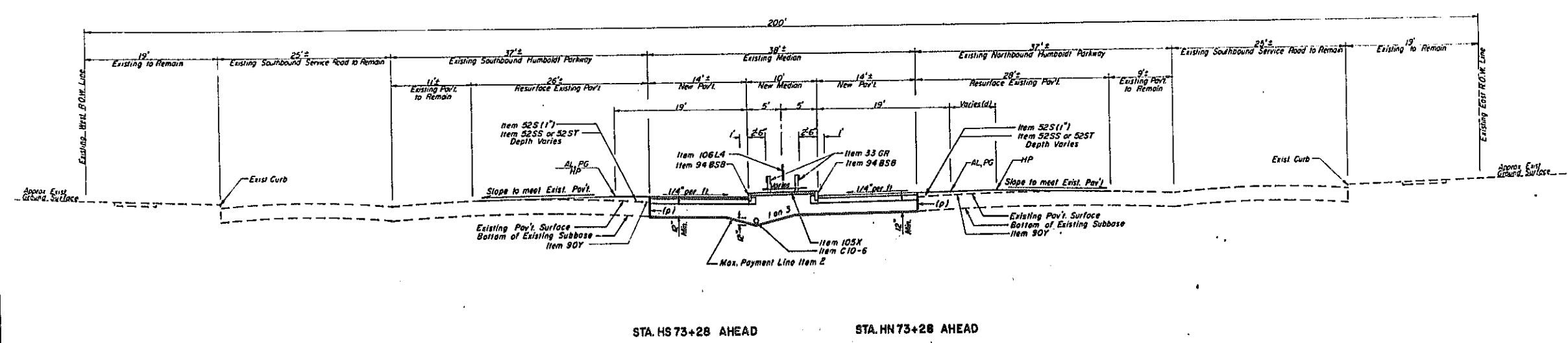
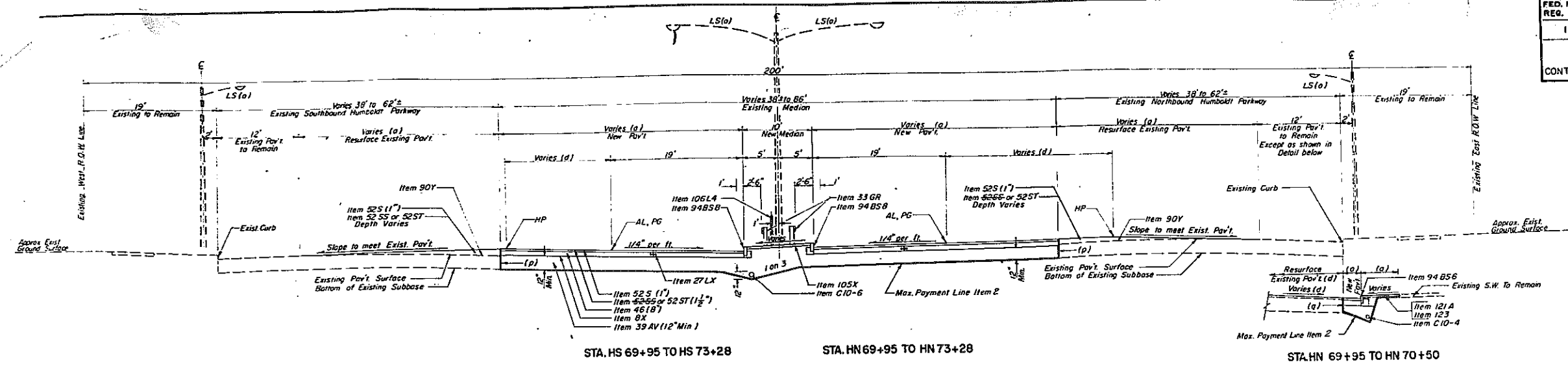
REVISION TO ITEMS 17

SCALE: 1/8" = 1'-0"

TYPICAL SECTIONS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL	DRAWN	J.R.H.
ENGINEERS-ARCHITECTS	CHECKED	T.E.K.
NEW YORK	TRACED	G.J.B.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	5 R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



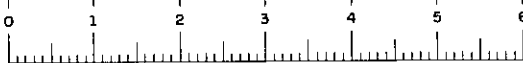
* Clean & Resurface Exist. Pav't between Sta. HS 76+34 & HS 77+00 with item 52S within the Limits shown on Sheet No. 86, Existing Curb to Remain.

NOTES: 1) Pavement Items listed above for H69+95 to H73+28 are typical for this sheet unless otherwise noted.
 2) For approx. Limits of Resurfacing & Widening Existing Humboldt Parkway Pavement see Sheet No. 66.

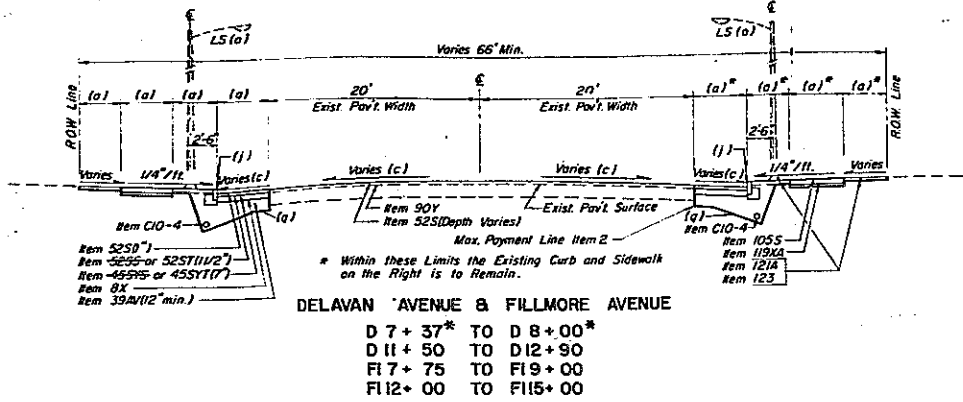
REVISION TO ITEM 52 SS 177

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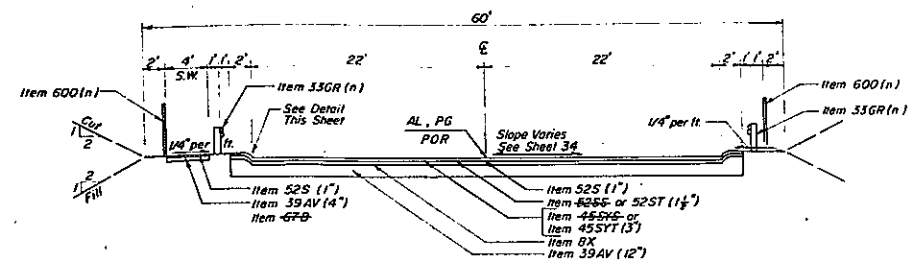
TYPICAL SECTIONS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.R.H. T.E.K. J.R.H.	



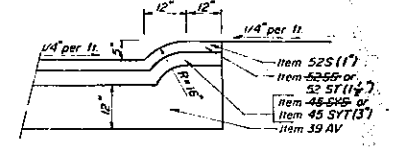
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	8 R.1	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



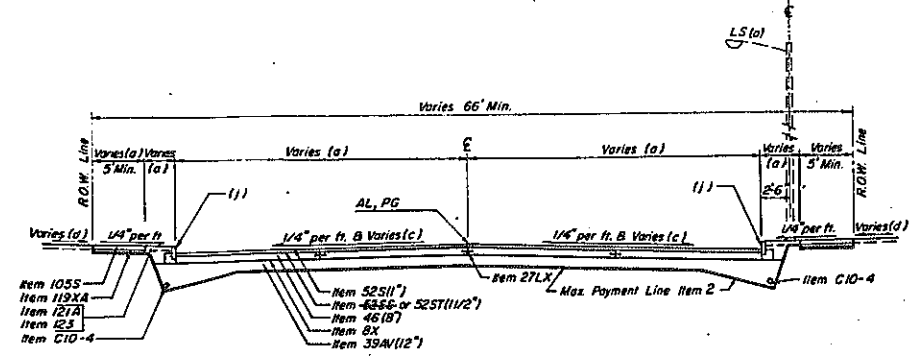
DELAVAN AVENUE & FILLMORE AVENUE
 D 7 + 37* TO D 8 + 00*
 D 11 + 50 TO D 12 + 90
 F 17 + 75 TO F 19 + 00
 F 112 + 00 TO F 115 + 00



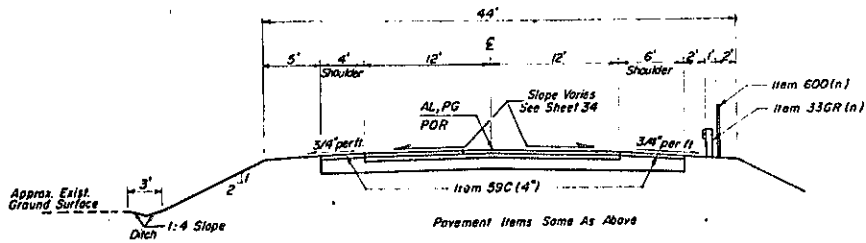
DELAVAN AVENUE DETOUR
(Looking East)



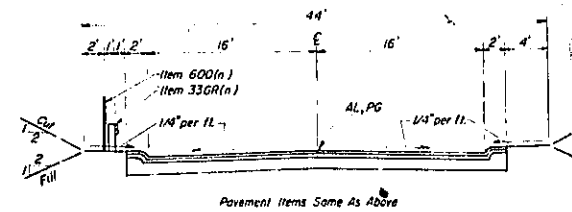
ASPH. CONC. CURB FOR DETOURS
Scale: 1/2" = 1'-0"



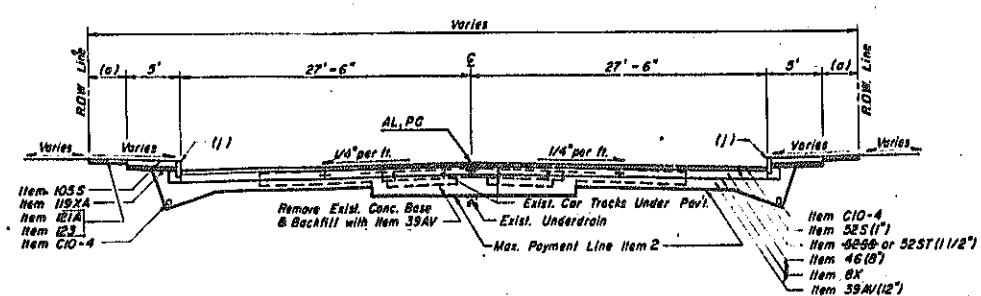
DELAVAN AVENUE
 D 8 + 00 TO S.B. Humboldt Pkwy. Service Road
 D 10 + 65 TO D 11 + 50



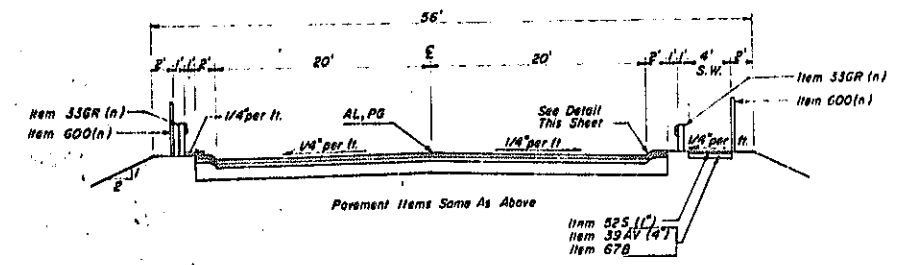
NORTHBOUND HUMBOLDT DETOUR
 Delavan Ave. Detour to Loring-Oakgrove Service Rd.
(Looking North)



NORTHBOUND HUMBOLDT DETOUR
 Delavan Ave. to Delavan Ave. Detour
(Looking North)



FILLMORE AVENUE
 F 9 + 00 TO F 9 + 44
 F 11 + 35 TO F 12 + 00



FILLMORE AVENUE DETOUR
(Looking North)

NOTE: Guide Rails (Item 33GR) will be recovered from the detours and reset as directed by the Engineer.

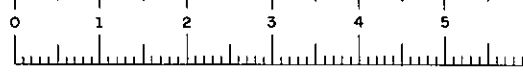
REVISIONS TO ITEMS 525, 46(B), 455T

SCALE 1/2" = 1'-0"

TYPICAL SECTIONS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRADED	J.R.H. T.E.K. J.P.H.
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FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	9	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

INDEX OF DRAWINGS

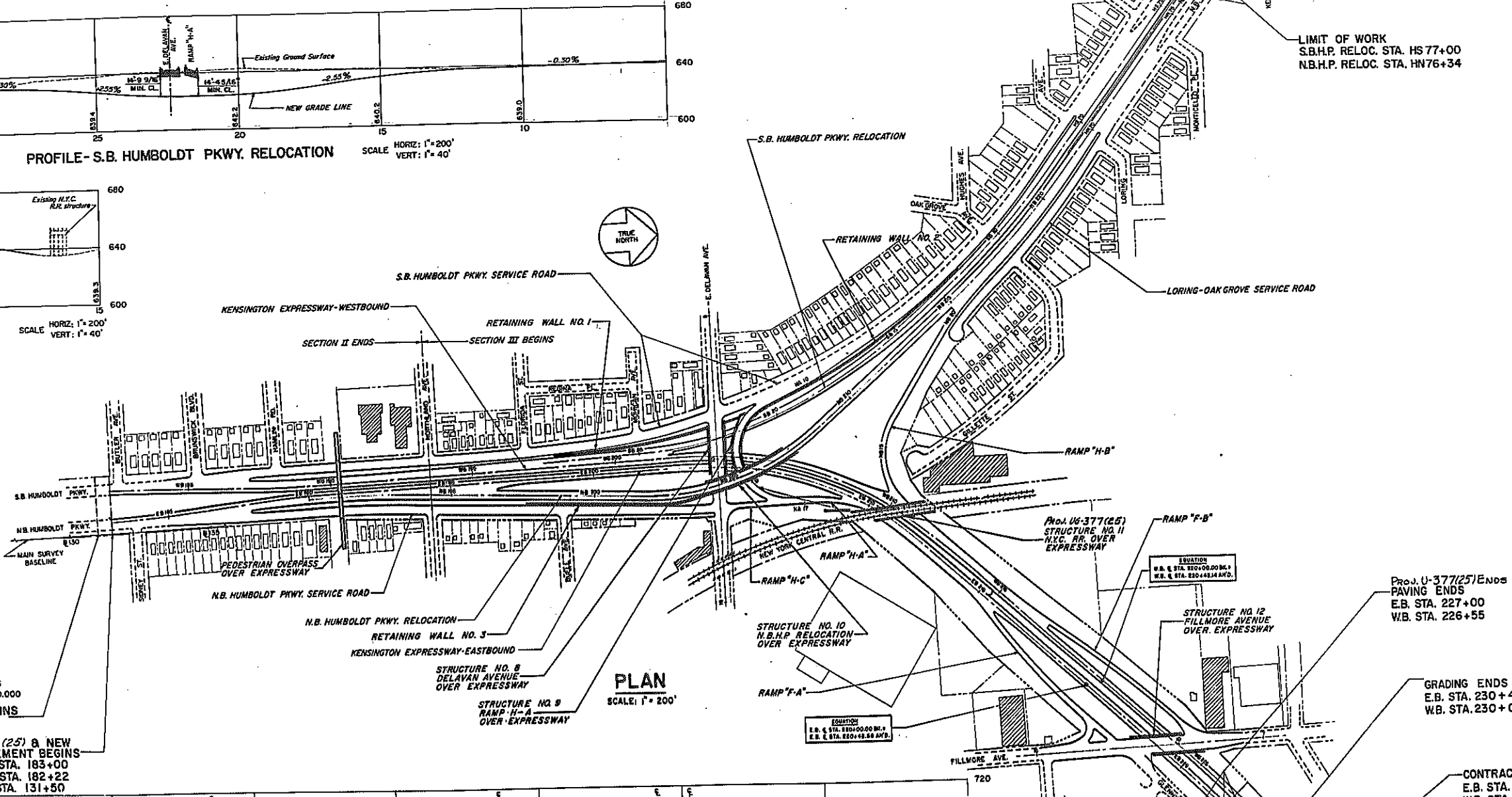
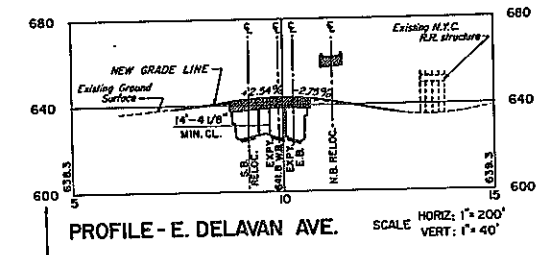
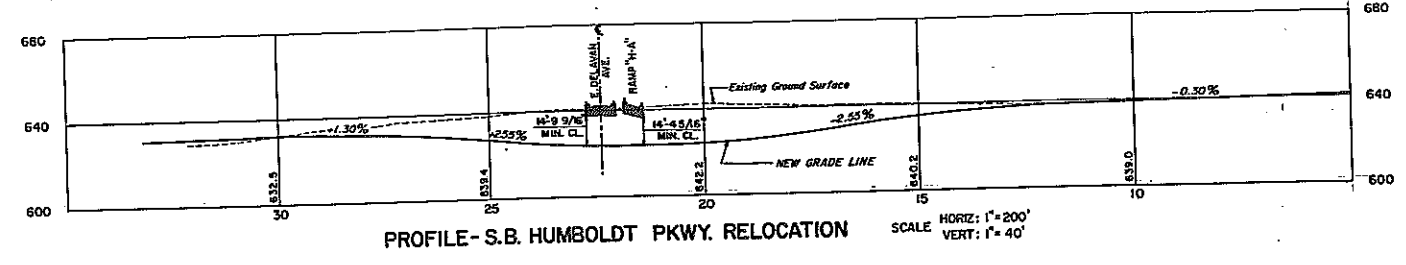
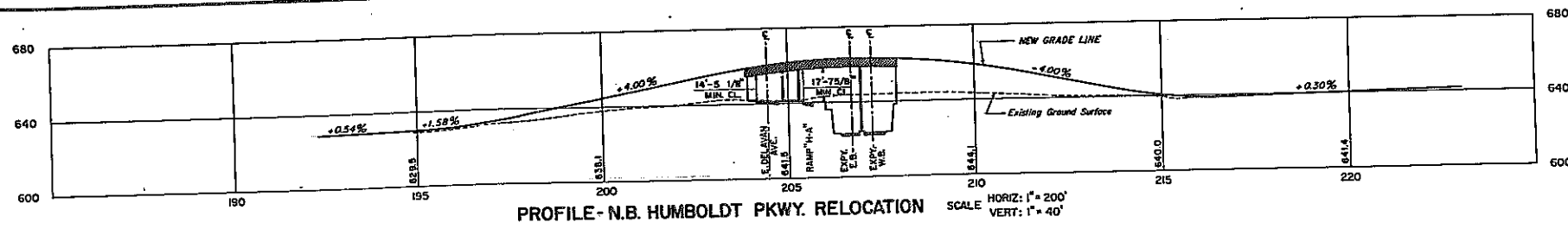
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1.	TITLE SHEET			132.	GENERAL PLAN
	TYPICAL ROADWAY SECTIONS			133.	NOTES, BEARINGS AND ESTIMATE
2.	EXPRESSWAY SECTIONS	43.	DETAILS OF DRAINAGE AND UTILITIES PLANS	134.	PIER
3.	EXPRESSWAY SECTIONS	44.	CATCH BASINS AND DROP INLETS	135.	SOUTH ABUTMENT
4.	EXPRESSWAY SECTIONS	45.	MANHOLES TYPE S-1 AND SPECIAL MANHOLES	136.	SOUTH ABUTMENT DETAILS
5.	EXPRESSWAY SECTIONS	46.	MANHOLES TYPE D-2, D-3, S-2 AND S-3	137.	NORTH ABUTMENT
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7.	EXPRESSWAY SECTIONS	48.	SPECIAL MANHOLES	139.	WINDOW WALL
8.	EXPRESSWAY SECTIONS	49.	SPECIAL MANHOLES	140.	STEEL LAYOUT PLAN
9.	CITY STREETS AND DETOUR SECTIONS	50.	MANHOLE SLABS, FRAMES, COVERS AND STEPS	141.	STRESS SHEET
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11.	LOCATION PLAN AND PROFILE	52.	WATER MAIN DETAILS	143.	GIRDER G-2
12.	MAINTENANCE PLAN		ELECTRICAL DISTRIBUTION PLANS	144.	GIRDER G-3
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14.	ESTIMATE OF QUANTITIES (CONT'D.) AND MISCELLANEOUS TABLES	54.	WB STA. 197+54 TO ER STA. 198+23	146.	SUPERSTRUCTURE DETAILS
	ROADWAY DETAILS	55.	WB STA. 197+54 TO ER STA. 211+46	147.	BAR LIST
14.	CURB AND GUTTER DETAILS	56.	DELAVAN INTERCHANGE CONNECTION TO HUMBOLDT PKWY.		FILLMORE AVENUE OVER EXPRESSWAY
15.	CURB AND MEDIAN DETAILS	57.	WB STA. 211+46 TO ER STA. 226+23	148.	GENERAL PLAN
16.	DRIVEWAY DETAILS & TABLE AND ROADWAY DETAILS	58.	WB STA. 226+23 TO ER STA. 240+56	149.	NOTES, BEARINGS AND ESTIMATE
17.	LANDSCAPING - ROADSIDE DEVELOPMENT SHEET	59.	ELECTRICAL DETAILS	150.	PIER
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19.	LEGENDS		TRAFFIC SIGNALS	152.	SOUTH ABUTMENT
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20.	ALIGNMENT DATA DELAVAN INTERCHANGE	61.	TRAFFIC SIGNAL DETAILS	154.	FRAMING PLAN
21.	WB STA. 185+00 TO ER STA. 182+21		GRADING AND FENCING PLANS	155.	LONGITUDINAL SECTION AND DETAILS
22.	WB STA. 197+54 TO ER STA. 198+23	62.	INTERSECTION GRADING PLAN	156.	DECK PLAN AND SECTION
23.	DELAVAN INTERCHANGE CONNECTION TO HUMBOLDT PARKWAY	63.	INTERSECTION GRADING PLAN	157.	RAILING LAYOUT
24.	WB STA. 211+46 TO ER STA. 211+86	64.	WB STA. 185+00 TO ER STA. 198+23	158.	BAR LIST
25.	WB STA. 226+23 TO ER STA. 240+56	65.	WB STA. 197+54 TO ER STA. 211+46		NORTHBOUND HUMBOLDT PARKWAY OVER EXPRESSWAY
	PROFILES	66.	WB STA. 198+23 TO ER STA. 211+86	159.	GENERAL PLAN
26.	WB STA. 181+00 TO ER STA. 182+00	67.	WB STA. 211+46 TO ER STA. 226+23	160.	PLAN AND ELEVATION RETAINING WALL NO. 1
27.	WB STA. 211+00 TO ER STA. 211+00	68.	WB STA. 226+23 TO ER STA. 240+56	161.	PLAN AND ELEVATION RETAINING WALL NO. 2
28.	WB STA. 211+00 TO ER STA. 226+50		PEDESTRIAN OVERPASS E.B. STA. 191+09.37	162.	PLAN AND ELEVATION RETAINING WALL NO. 3
29.	WB STA. 226+50 TO ER STA. 242+00	69.	GENERAL PLAN	163.	BAR REINFORCEMENT RETAINING WALL NO. 1
30.	NORTHBOUND HUMBOLDT PARKWAY RELOCATION	70.	NOTES, BEARINGS AND ESTIMATE	164.	BAR REINFORCEMENT RETAINING WALL NO. 1
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32.	RAMPS "P-A", "P-B", "H-B" and "H-C"	72.	ABUTMENT REINFORCEMENT	166.	BAR REINFORCEMENT RETAINING WALL NO. 3
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	DETOUR PLANS AND PROFILES	75.	PIER 3	169.	DETAILS
34.	DELAVAN AVENUE AND NORTHBOUND HUMBOLDT PARKWAY DETOURS	76.	RAMPS	170.	RAILING DETAILS
35.	NEW YORK CENTRAL RAILROAD DETOUR STA. 3+00 TO STA. 9+00	77.	RAMP REINFORCEMENT	171.	BAR LIST
36.	NEW YORK CENTRAL RAILROAD DETOUR TOTAL 9+00 TO STA. 15+00	78.	FRAMING AND DECK PLAN	172.	BAR LIST
37.	FILLMORE AVENUE DETOUR	79.	CHAIN LINK FENCE DETAILS	173.	BAR LIST
	DRAINAGE AND UTILITIES PLANS	80.	RAILING DETAILS		
38.	WB STA. 185+00 TO ER STA. 182+21	81.	SHEET PILING DETAILS		
39.	WB STA. 197+54 TO ER STA. 198+23	82.	BAR LIST		
40.	DELAVAN INTERCHANGE CONNECTION TO HUMBOLDT PARKWAY				
41.	WB STA. 211+46 TO ER STA. 211+86				
42.	WB STA. 226+23 TO ER STA. 240+56				

INDEX TO DRAWINGS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHEN & BRILL	DRAWN	C.P.B.
ENGINEERS-ARCHITECTS	CHECKED	N.Y.
NEW YORK	TRACED	

12

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	10	173

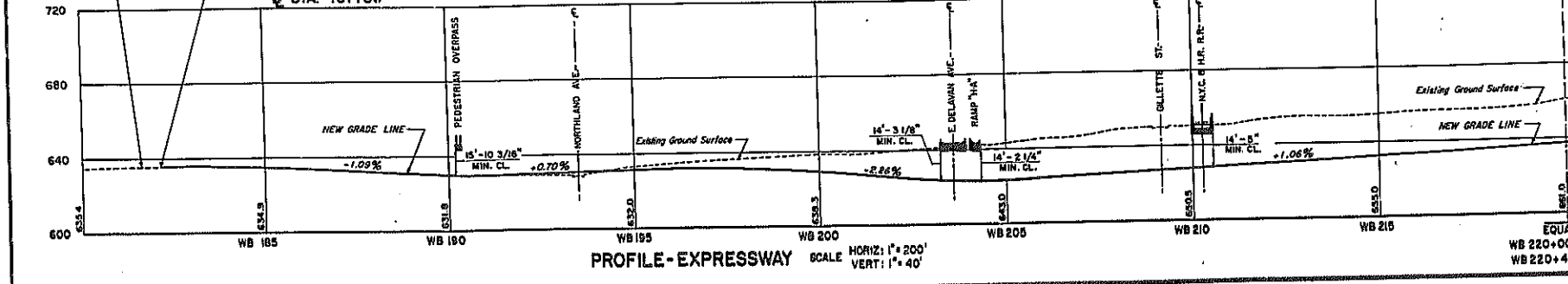
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE:
DATUM IS U.S.C. & G.S. DATUM WHICH IS
575.454 FEET ABOVE CITY DATUM OF 0.000

CONTRACT BEGINS
@ STA. 131+00

Prod. U-377(25) & NEW
PAVEMENT BEGINS
E.B. STA. 183+00
W.B. STA. 182+22
@ STA. 131+50



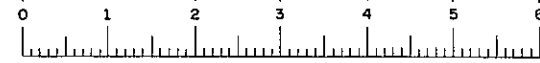
LIMIT OF WORK
S.B.H.P. RELOC. STA. HS 77+00
N.B.H.P. RELOC. STA. HN 76+34

Prod. U-377(25) ENDS
PAVING ENDS
E.B. STA. 227+00
W.B. STA. 226+55

GRADING ENDS
E.B. STA. 230+40
W.B. STA. 230+00

CONTRACT ENDS
E.B. STA. 231+00
W.B. STA. 230+60

LOCATION PLAN & PROFILE			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHER & BRILL	DRAWN	T.E.R.	
ENGINEERS-ARCHITECTS	CHECKED	M.M.	
NEW YORK	TRACED	J.M.B.	



FAC 63-2

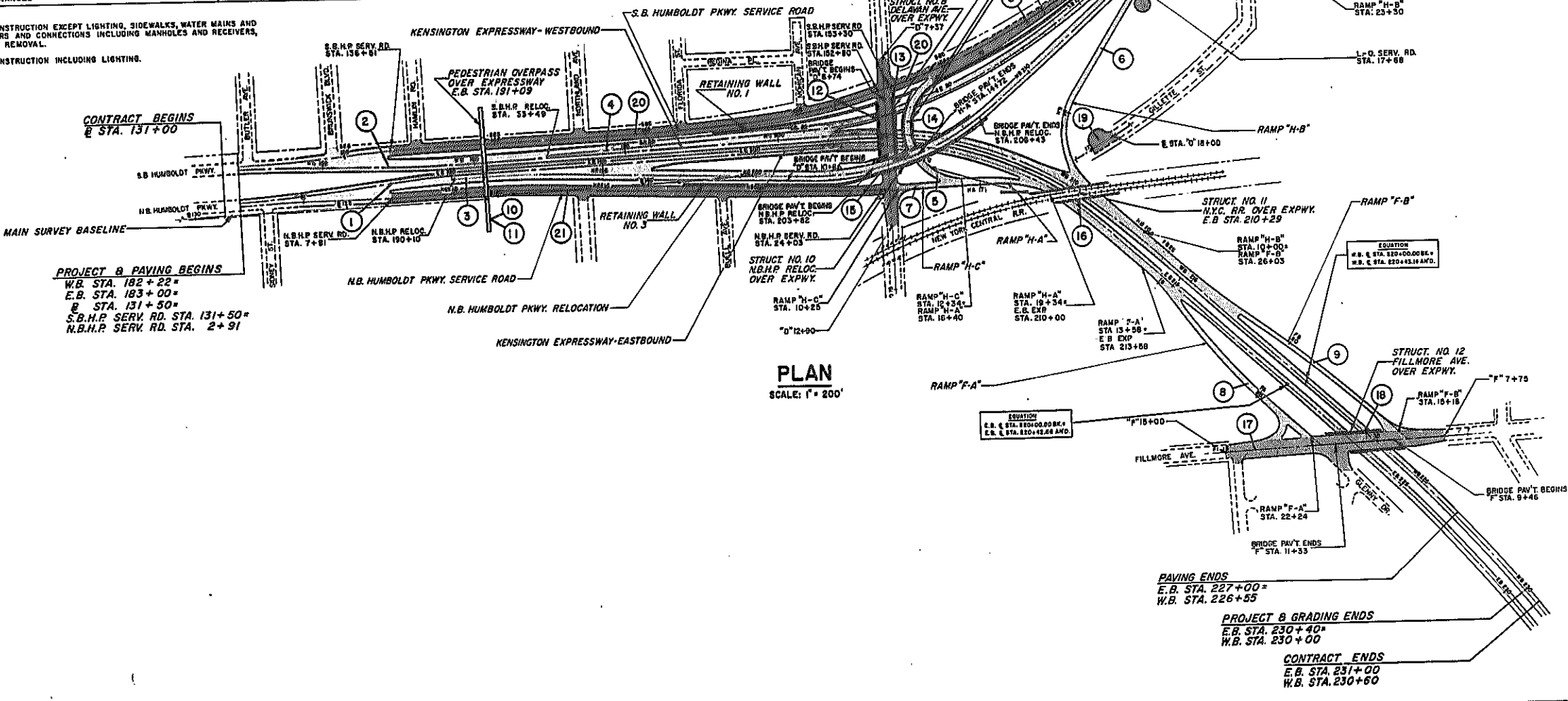
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	11	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

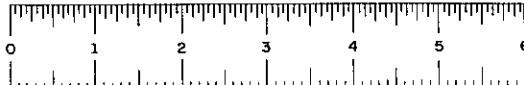
PART	HIGHWAY	LIMITS AND DESCRIPTION	MILES	AGENCY	JURISDICTION	
1	KENSINGTON EXPRESSWAY EASTBOUND	E.B. STA. 183+00 TO E.B. STA. 227+00	.83	STATE	HIGHWAY LAW SECTION 3-9-C SUBDIVISION-18	
2	KENSINGTON EXPRESSWAY WESTBOUND	W.B. STA. 182+22 TO W.B. STA. 226+05	.86	STATE		
3	NORTHBOUND HUMBOLDT PARKWAY RELOCATION	N.B.H.P. RELOC. STA. 190+10 TO N.B.H.P. RELOC. STA. 203+82 & N.B.H.P. RELOC. STA. 208+43 TO N.B.H.P. RELOC. STA. 221+80 "HN" STA. 68+85 TO "HN" STA. 78+34	.67	STATE		
4	SOUTHBOUND HUMBOLDT PARKWAY RELOCATION	S.B.H.P. RELOC. STA. 33+49 TO S.B.H.P. RELOC. STA. 6+29 "HS" STA. 68+85 TO "HS" STA. 77+00	.17	STATE		
5	RAMP "H-A"	"H-A" STA. 7+52 TO "H-A" STA. 11+91 & "H-A" STA. 14+72 TO "H-A" STA. 19+34	.25	STATE		
6	RAMP "H-B"	"H-B" STA. 10+00 TO "H-B" STA. 23+50	.04	STATE		
7	RAMP "H-C"	"H-C" STA. 10+25 TO "H-C" STA. 12+34	.16	STATE		
8	RAMP "F-A"	"F-A" STA. 13+58 TO "F-A" STA. 22+24	.20	STATE		
9	RAMP "F-B"	"F-B" STA. 15+38 TO "F-B" STA. 26+03	.20	STATE		
10	PEDESTRIAN OVERPASS OVER EXPRESSWAY	E.B. STA. 191+09 - SUBSTRUCTURE AND SUPERSTRUCTURE, EXCEPT SURFACE OF WALKWAYS AND APPROACHES THERETO AND EXCLUDING SNOW REMOVAL.	—	CITY	HIGHWAY LAW SECTION 10 SUBDIVISION-18	
11	PEDESTRIAN OVERPASS OVER EXPRESSWAY	E.B. STA. 191+09 - SURFACE OF WALKWAYS AND APPROACHES THERETO, INCLUDING SNOW REMOVAL.	—	CITY		
12	DELAVAN AVENUE OVER EXPRESSWAY - STRUCTURE NO. 8	E.B. STA. 7+37 TO E.B. STA. 12+90 - ALL FEATURES OF CONSTRUCTION INCLUDING, LIGHTING, AND WATER MAIN CHANGES, BUT ONLY PAVEMENT AND SIDEWALKS ON BRIDGE STRUCTURE.	.10	STATE		
13	DELAVAN AVENUE OVER EXPRESSWAY - STRUCTURE NO. 8	ENTIRE STRUCTURE OVER EXPRESSWAY, EXCEPT PAVEMENT AND SIDEWALKS.	—	STATE		
14	RAMP "H-A" EASTBOUND OVER EXPRESSWAY - STRUCTURE NO. 9	"H-A" STA. 11+91 TO "H-A" STA. 14+72 - ENTIRE STRUCTURE OVER EXPRESSWAY.	.05	STATE		
15	NORTHBOUND HUMBOLDT RELOC. OVER EXPRESSWAY - STRUCTURE NO. 10	N.B.H.P. RELOC. STA. 203+82 TO N.B.H.P. RELOC. STA. 208+43 - ENTIRE STRUCTURE OVER EXPRESSWAY.	.09	STATE		
16	NEW YORK CENTRAL RAILROAD OVER EXPRESSWAY - STRUCTURE NO. 11	E.B. STA. 210+29 - ENTIRE STRUCTURE OVER EXPRESSWAY.	—	N.Y.C.R.R.		
17	FILLMORE AVENUE OVER EXPRESSWAY - STRUCTURE NO. 12	E.B. STA. 7+75 TO E.B. STA. 15+00 - ALL FEATURES OF CONSTRUCTION INCLUDING, LIGHTING, AND WATER MAIN CHANGES, BUT ONLY PAVEMENT AND SIDEWALKS ON BRIDGE STRUCTURE.	.14	CITY		
18	FILLMORE AVENUE OVER EXPRESSWAY - STRUCTURE NO. 12	ENTIRE STRUCTURE OVER EXPRESSWAY, EXCEPT PAVEMENT AND SIDEWALKS.	—	STATE		
19	TURN-AROUND GILLETTE STREET	GILLETTE STREET @ STA. "G" 18+00 - ALL FEATURES OF CONSTRUCTION	—	CITY		HIGHWAY LAW SECTION 10 SUBDIVISION-23
20	SOUTHBOUND HUMBOLDT SERVICE ROAD	S.B.H.P. SERV. RD. STA. 136+51 TO S.B.H.P. SERV. RD. STA. 152+80 & S.B.H.P. SERV. RD. STA. 153+30 TO S.B.H.P. SERV. RD. STA. 168+47 "HS" STA. 68+85 TO "HS" STA. 70+50	.55	STATE		
21	NORTHBOUND HUMBOLDT SERVICE ROAD	N.B.H.P. SERV. RD. STA. 7+91 TO N.B.H.P. SERV. RD. STA. 24+05	.18	STATE		
22	LORING-OAK GROVE SERVICE ROAD AND TURN-AROUND	"H" STA. 70+50 TO "H" STA. 68+85 "L-O" SERV. RD. STA. 10+00 TO "L-O" SERV. RD. STA. 17+68	—	STATE		HIGHWAY LAW SECTION 10 SUBDIVISION-26
23	ALTERED AND REPAVED CITY STREETS	ALL FEATURES OF CONSTRUCTION	—	CITY		
	INTERSECTIONS WITHIN THE CONTRACT LIMITS	(NOT SHOWN ON MAINTENANCE PLANS)	—	CITY		
	CONSTRUCTION OF WATER MAINS AND SIDEWALKS	ALL WATER MAINS AND SIDEWALKS WITHIN THE CONTRACT LIMITS	—	STATE		
	CONSTRUCTION OF FIRE ALARM AND POLICE TELEPHONE	ALL FIRE ALARM AND POLICE TELEPHONE SYSTEMS AND APPURTENANCES	—	CITY		
	ERECTED SIGNALS	ALL SIGNALS	—	CITY		
	SEWERS & APPURTENANCES	ALL COMBINED SEWERS AND CONNECTIONS, INCLUDING MANHOLES AND RECEIVERS	—	CITY		
			—	CITY		
			—	CITY		
			—	CITY		

PART 1 THRU 9:
ALL FEATURES OF CONSTRUCTION EXCEPT LIGHTING, SIDEWALKS, WATER MAINS AND ALL COMBINED SEWERS AND CONNECTIONS INCLUDING MANHOLES AND RECEIVERS, BUT INCLUDING SNOW REMOVAL.

PART 20 THRU 21:
ALL FEATURES OF CONSTRUCTION INCLUDING LIGHTING.

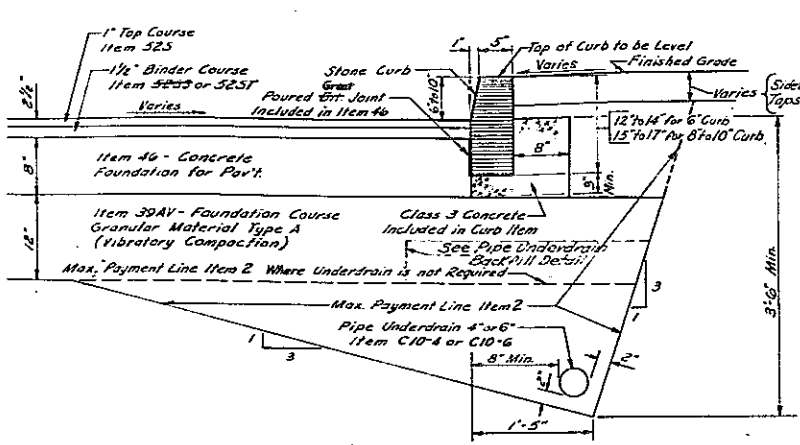


MAINTENANCE PLAN		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. R.G.B. G.C.P.

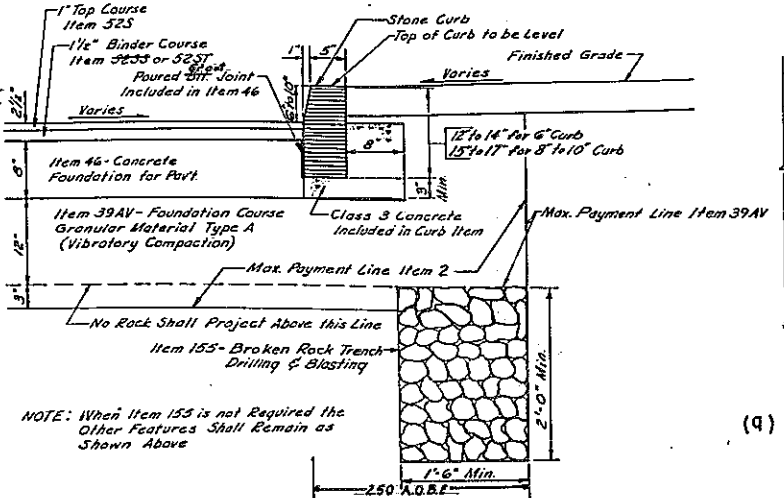


FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	14 RI	173

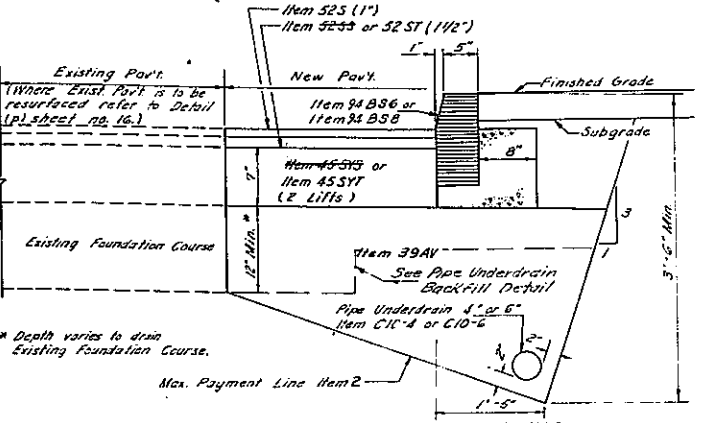
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



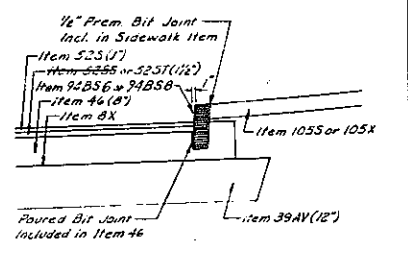
VERTICAL STONE CURB IN EARTH
ITEM 94 BS6, 94 BS8 or 94 BS10



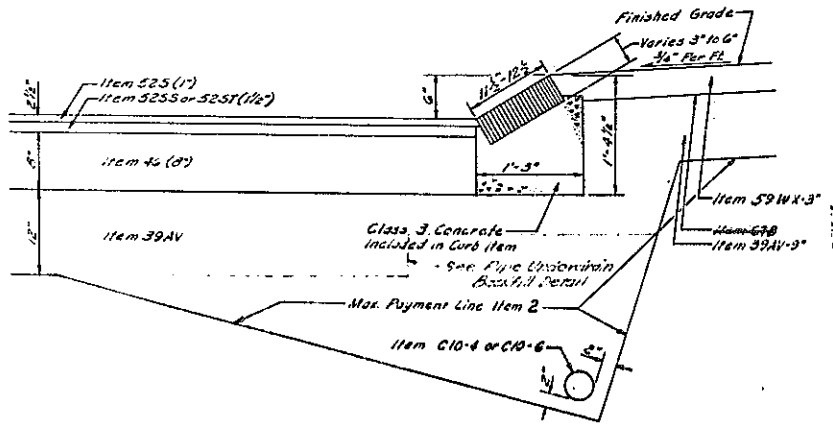
VERTICAL STONE CURB IN ROCK
ITEM 94 BS6, 94 BS8 or 94 BS10



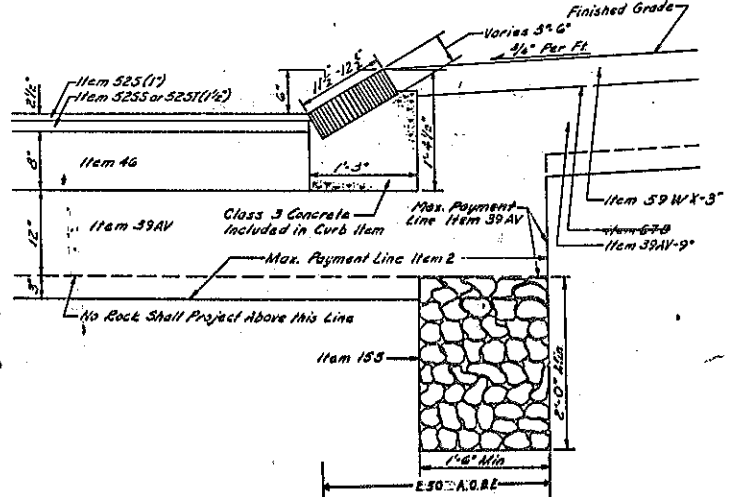
(4) CURB & UNDERDRAIN DETAIL FOR ROAD WIDENING



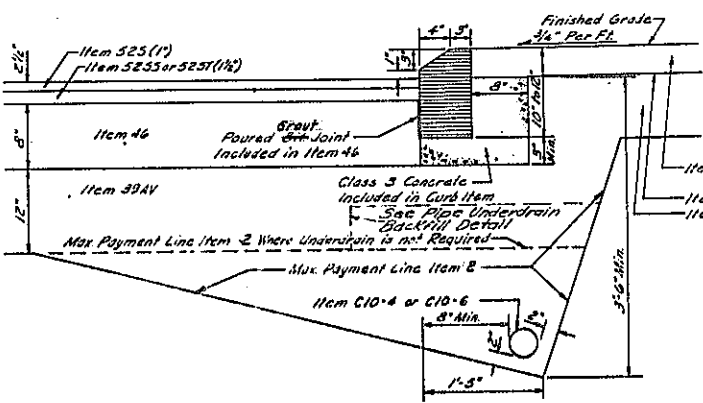
JOINT DETAIL WHERE CONCRETE SIDEWALK PAVEMENT ABUTS CURB
Scale 1/2" = 1'-0"



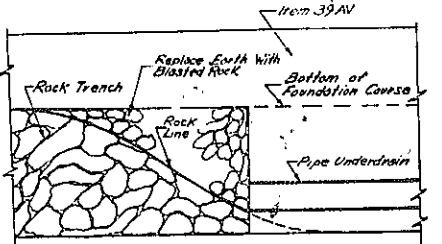
6" SLOPED STONE CURB IN EARTH
ITEM 94 DS



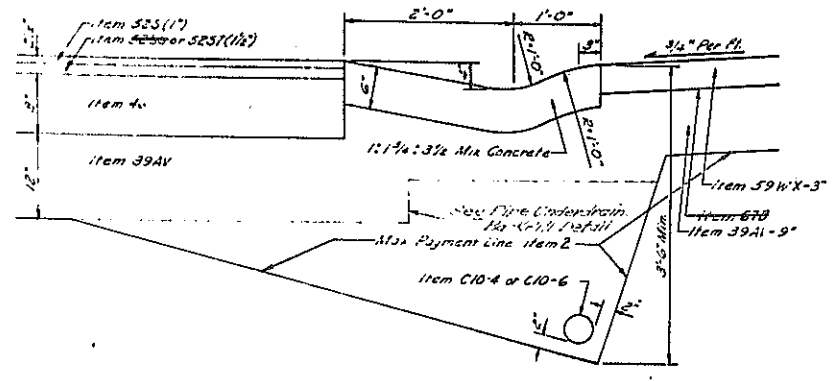
6" SLOPED STONE CURB IN ROCK
ITEM 94 DS



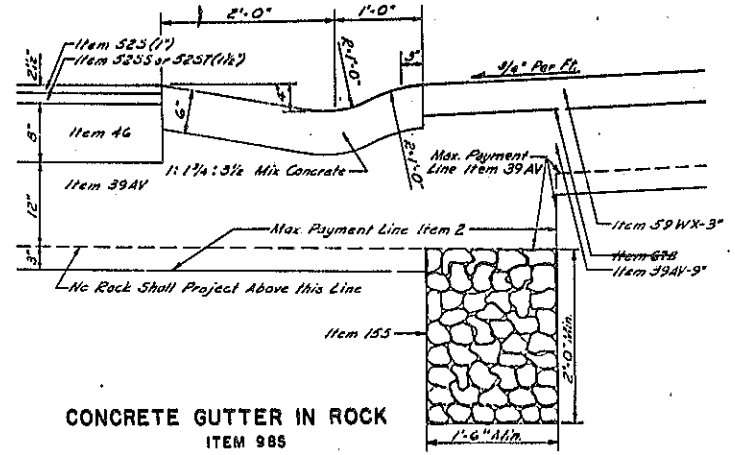
4" MOUNTABLE STONE CURB IN EARTH
ITEM 94 AS



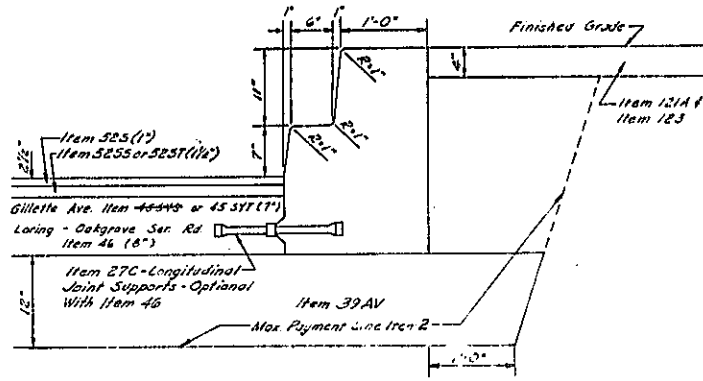
UNDERDRAIN TRANSITION ROCK TRENCH TO PIPE UNDERDRAIN



CONCRETE GUTTER IN EARTH
ITEM 985



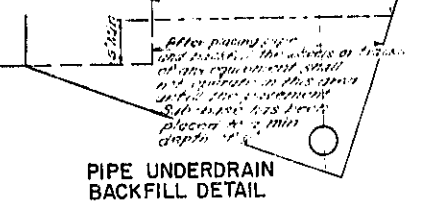
CONCRETE GUTTER IN ROCK
ITEM 985



18" CONCRETE CURB
ITEM 97
(TYPICAL SECTION AT OILLETT AVE CUL-DE-SAC & LORING-DAKROVE SERVICE RD TURN-AROUND)

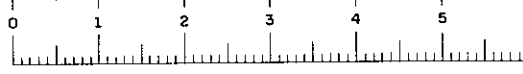
REVISION TO ITEMS 17, 18, 19, 20, 21, 22, 23, 24, 25

NOTES:
Detail for Item 965, Resetting Old Curb, similar to above curb details.
Where Item 39AV, Foundation Course - Granular Material Type A, also acts as backfill around pipe, including pipe underdrains, all material to be placed within 6 inches of pipe shall pass a 2 inch sieve.
For the locations of features detailed above refer to Typical Sections and Alignment and Paving Plans.
All pipe underdrains and broken rock trench shall outlet to new Drop Inlets, Catch Basins or Manholes as outlined in Summary of Underdrain, See Sheet No. 18.



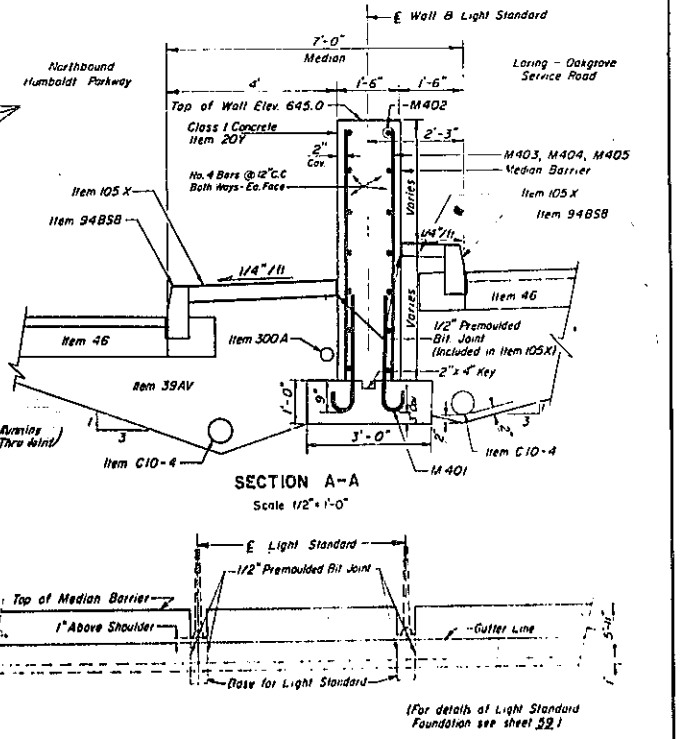
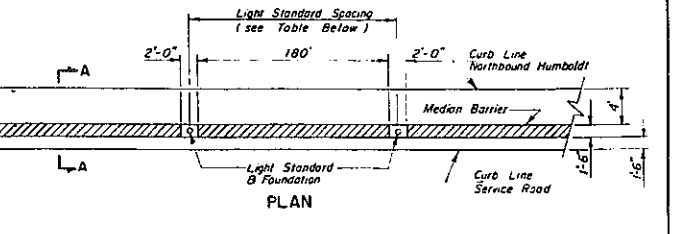
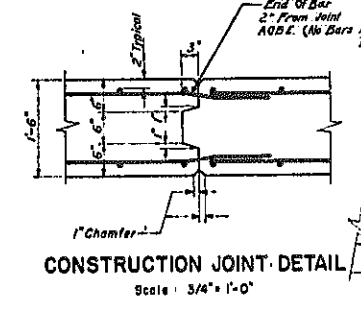
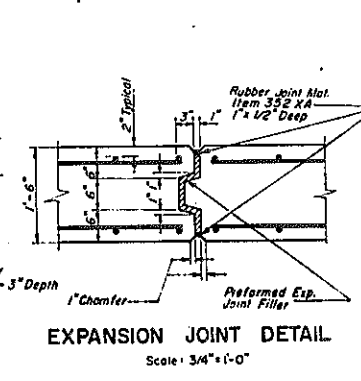
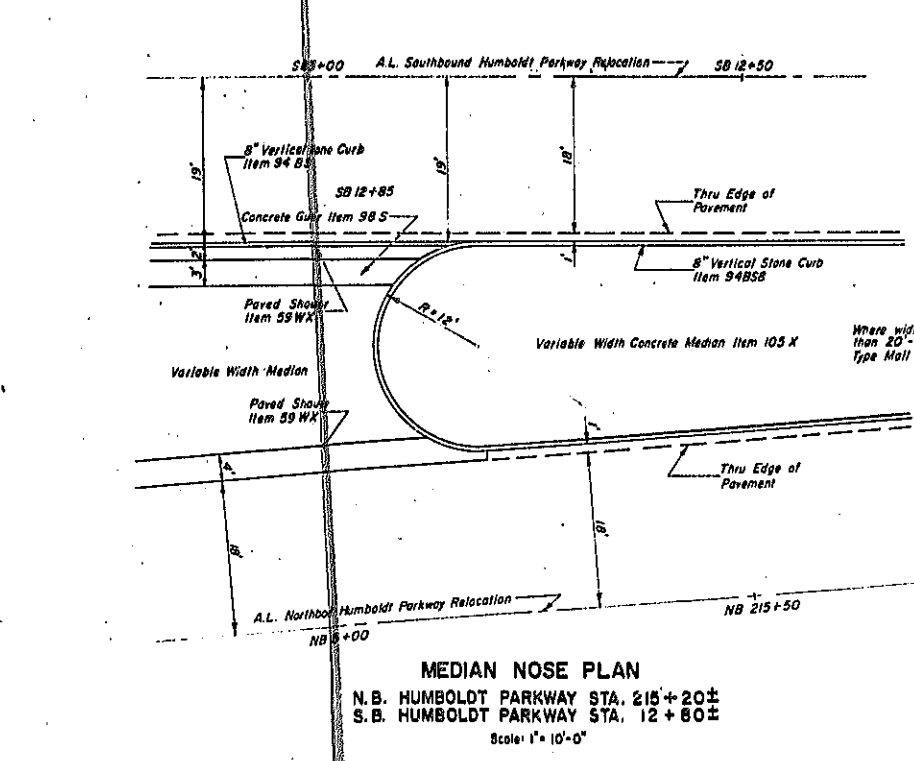
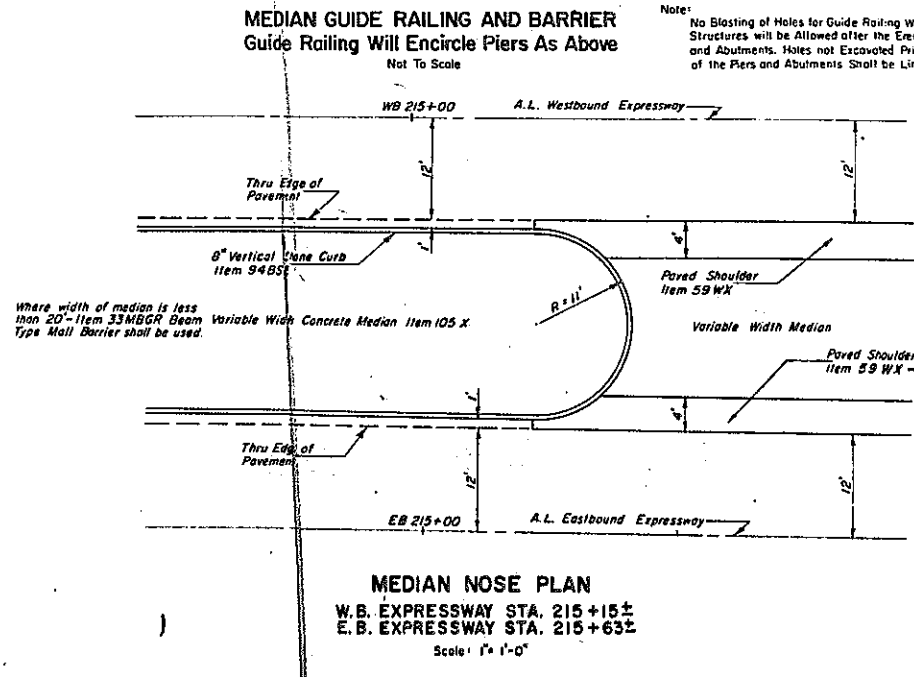
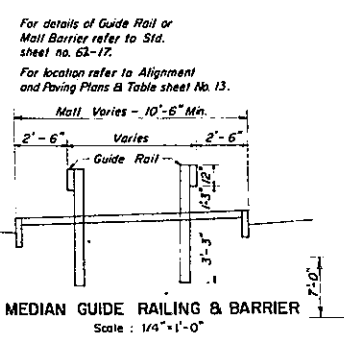
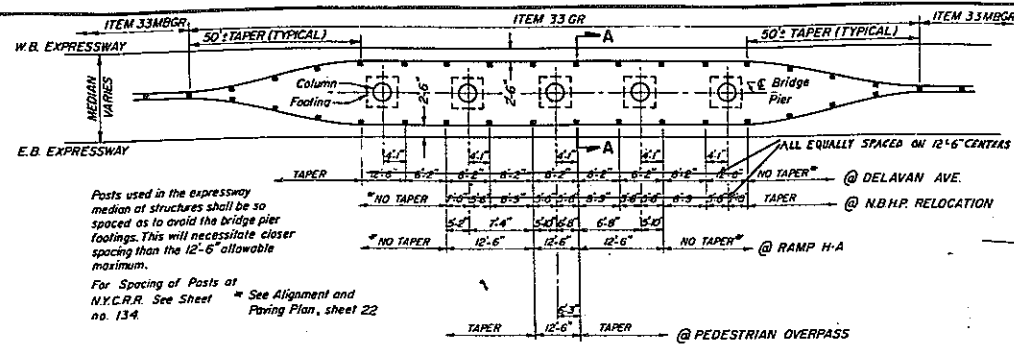
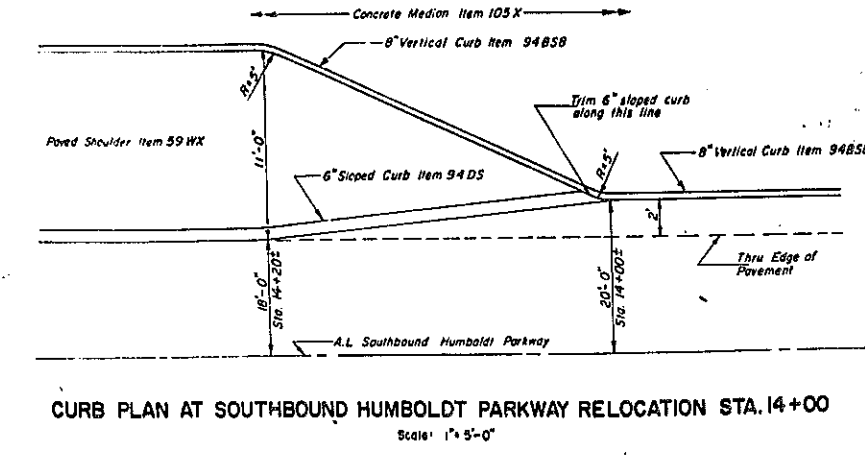
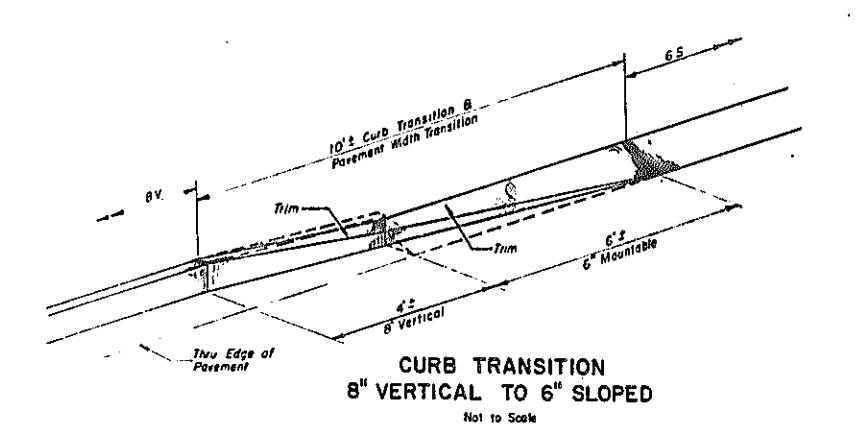
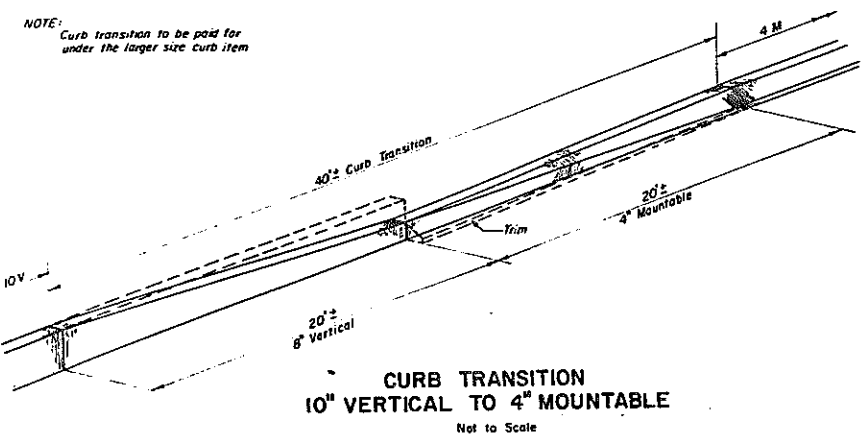
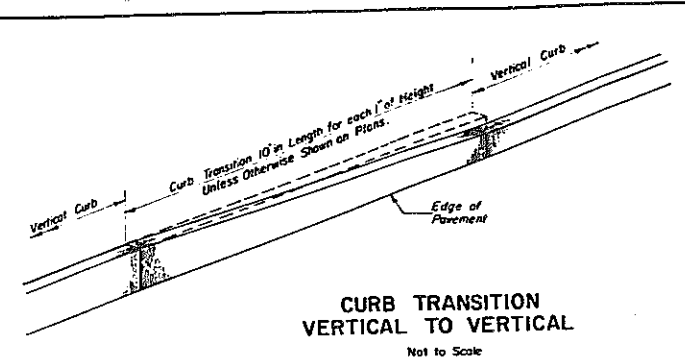
PIPE UNDERDRAIN BACKFILL DETAIL
SCALE: 1" = 1'-0" (Unless otherwise noted)

CURB & GUTTER DETAILS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHAR & BRILL	DRAWN	J.M.B.	
ENGINEERS-ARCHITECTS	CHECKED	F.V.P.	
NEW YORK	TRACED	E.E.K.	



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	15 R 1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



ELEVATION LORING-OAKGROVE CONCRETE DIVIDER
Scale: 1" = 10'-0"
L-0 STA. 8 + 92 TO L-0 STA. 17 + 40

STATION	L-0' FROM TO	LENGTH	NO. PANELS	TYPE	FOOTING ELEV.	HEIGHT STEM
8 + 92	9 + 22	30'	1	A	638.0	6'
9 + 22	10 + 12	90'	3	A	638.0	6'
10 + 12	11 + 02	90'	3	A	638.0	6'
11 + 04	11 + 94	90'	3	A	638.0	6'
12 + 04	12 + 84	90'	3	A	638.0	6'
12 + 88	13 + 78	90'	3	B	637.0	7'
13 + 76	14 + 66	90'	3	C	636.0	6'
14 + 68	15 + 58	90'	3	C	636.0	6'
15 + 58	16 + 48	90'	3	C	636.0	6'
16 + 50	17 + 40	90'	3	B	637.0	7'

PANEL No. OF MARK	PANELS	MARK	SIZE	LENGTH	No BARS	A	B	DESCRIPTION
A	13	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	12			HORIZ BARS
		M403	No. 4	5'-0"	60			VERT BARS
B	6	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	14			HORIZ BARS
		M404	No. 4	6'-9"	60			VERT BARS
C	9	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	16			HORIZ BARS
		M405	No. 4	7'-9"	60			VERT BARS

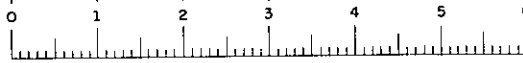
- NOTES:
- Expansion Joints to be constructed at the following Stations: 9 + 22, 10 + 12, 11 + 94, 13 + 76 B 15 + 58.
 - Construction Joints at 30' Intervals.
 - Location of Light Standards in Median Barrier:
 - L-0" 11 + 03
 - L-0" 12 + 85
 - L-0" 14 + 67
 - L-0" 16 + 49

CURB & MEDIAN DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN: T.E.K.
CHECKED: J.R.H.
TRACED: G.C.P.

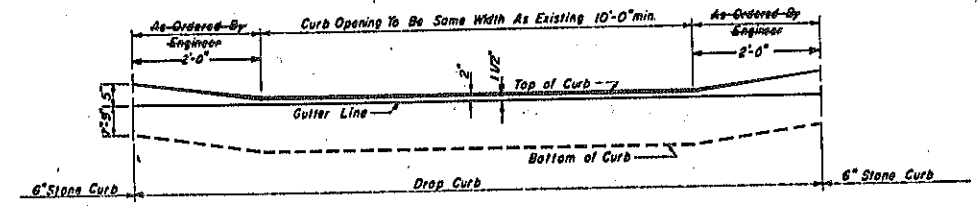
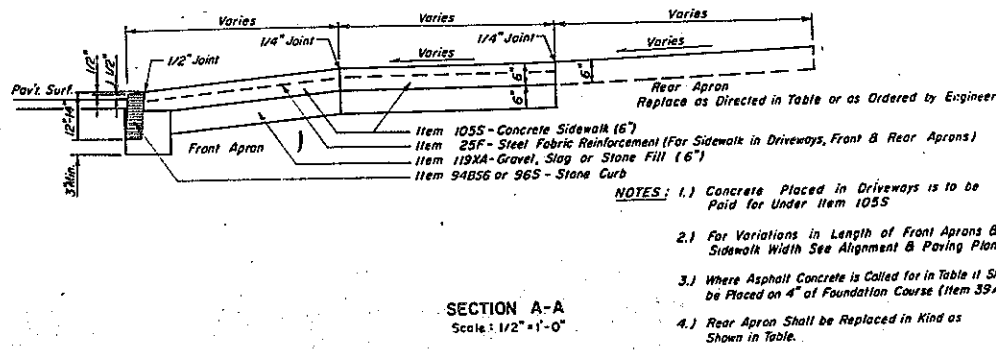
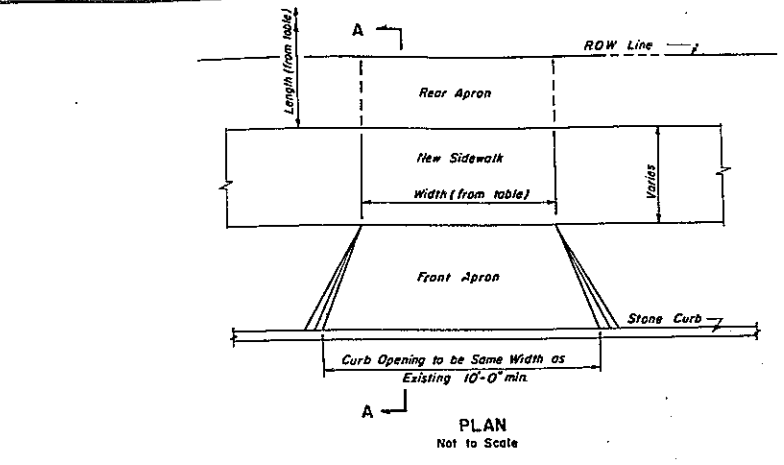


FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-03-377 (25)	16 R 1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

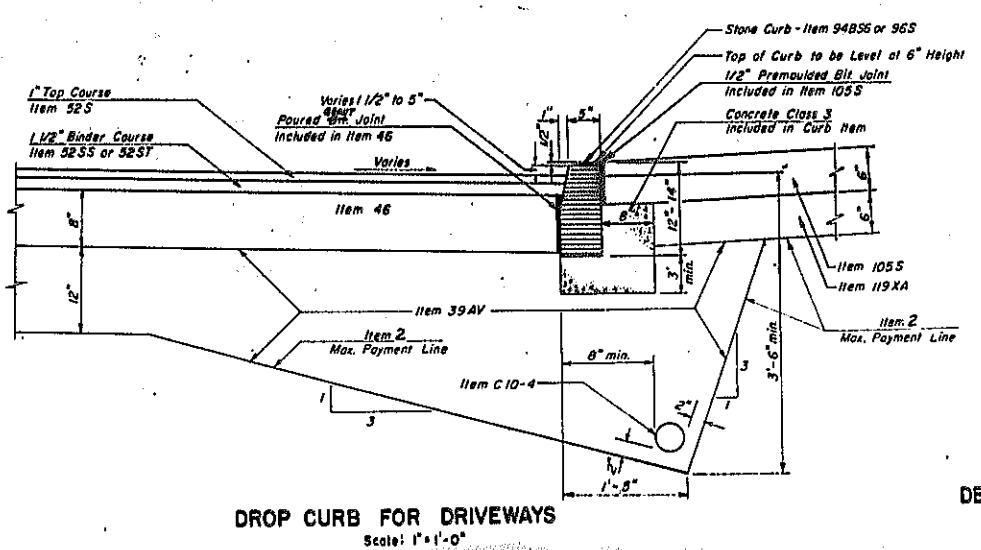
TABLE OF DRIVEWAYS TO BE RE-ESTABLISHED

FOR DRIVEWAY REAR APRONS					
LOCATION	SIDE	LENGTH	WIDTH	DEPTH	TYPE
S.B.H.P. SERVICE RD.					
Sta. 135 + 52	L.T.	4'	9'	2 1/2"	ASPHALT CONCRETE
135 + 94	L.T.	3'	8'	6"	CONCRETE
136 + 39	L.T.	1'	8'	6"	CONCRETE
138 + 24	L.T.	1'	10'	6"	CONCRETE
138 + 66	L.T.	1'	10'	6"	CONCRETE
141 + 15	L.T.	1'	8'	6"	CONCRETE
143 + 85	L.T.	1'	7'	6"	CONCRETE
144 + 16	L.T.	1'	3.5' @ 2.0'	6"	CONCRETE
144 + 51	L.T.	1'	7'	6"	CONCRETE
144 + 84	L.T.	1'	7.5'	6"	CONCRETE
145 + 18	L.T.	1'	7.5'	6"	CONCRETE
145 + 57	L.T.	1'	10'	6"	CONCRETE
N.B.H.P. SERVICE RD.					
Sta. 7 + 18	RT.	2'	7'	6"	CONCRETE
7 + 79	RT.	1'	8'	2 1/2"	ASPHALT CONCRETE
8 + 14	RT.	1'	8'	6"	CONCRETE
8 + 46	RT.	1'	11.5' @ 2.5'	6"	CONCRETE
8 + 86	RT.	1'	2.0' @ 2.5'	6"	CONCRETE
9 + 30' 20	RT.	1'	2.0' @ 2.5'	6"	CONCRETE
9 + 71	RT.	1'	8'	6"	CONCRETE
11 + 43	RT.	4'	8'	6"	CONCRETE
11 + 75	RT.	4'	8'	6"	CONCRETE
12 + 34	RT.	4'	6'	2 1/2"	ASPHALT CONCRETE
12 + 78	RT.	4'	29'	6"	CONCRETE
14 + 84	RT.	5'	8'	6"	CONCRETE
15 + 54	RT.	5'	8'	6"	CONCRETE
15 + 87	RT.	5'	7.5'	6"	CONCRETE
16 + 59	RT.	5'	8'	2 1/2"	ASPHALT CONCRETE
16 + 99	RT.	5'	9'	6"	CONCRETE
17 + 36	RT.	5'	7'	6"	CONCRETE
19 + 59	RT.	5'	8.5'	6"	CONCRETE
19 + 90	RT.	5'	7.5'	6"	CONCRETE
20 + 30	RT.	5'	2.0' @ 2.0'	6"	CONCRETE
21 + 36	RT.	5'	8'	6"	GRAVEL
5 + 80	RT.				
6 + 30	RT.				
7 + 55	RT.				
LORING-DANEGROVE SERV. RD.					
Sta. 10 + 42	L.T.	1'	2.0' @ 3.0'	6"	CONCRETE
10 + 84	L.T.	1'	3.0' @ 2.5'	6"	CONCRETE
11 + 28	L.T.	1'	8'	6"	CONCRETE
11 + 64	L.T.	1'	8'	6"	CONCRETE
12 + 44	L.T.	1'	16'	6"	CONCRETE
12 + 86	L.T.	1'	7'	6"	CONCRETE
13 + 25	L.T.	1'	7'	6"	CONCRETE
15 + 21	L.T.	7'	8'	6"	CONCRETE
15 + 54	L.T.	5'	2.5' @ 2.5'	6"	CONCRETE
16 + 28	L.T.	2'	9'	6"	CONCRETE
16 + 64	L.T.	2'	9'	6"	CONCRETE
17 + 05	L.T.	2'	8'	6"	CONCRETE
17 + 80	L.T.	2'	8'	6"	CONCRETE
FILLMORE AVENUE					
Sta. 7 + 93	RT.	1'	21'	6"	CONCRETE
8 + 31	RT.	1'	31'	6"	CONCRETE
15 + 10	RZ.	4'	20'	6"	GRAVEL

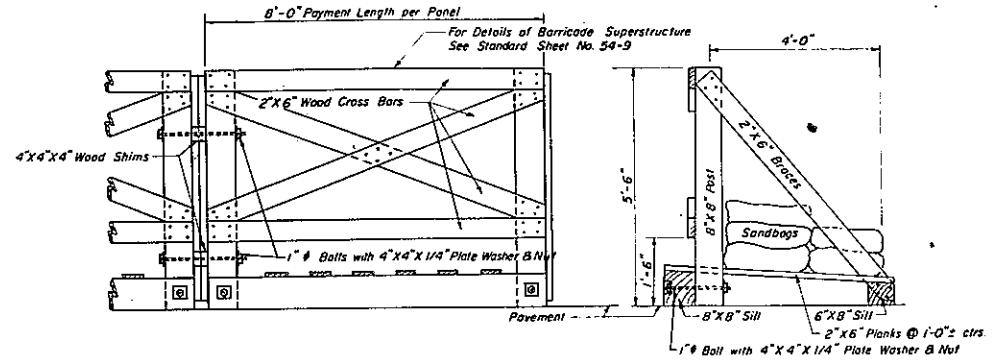
ESTIMATED NEAT QUANTITIES			
	REAR APRON	FRONT APRON	TOTAL
ITEM 25F	95 S.Y.	170 S.Y.	265 S.Y.
ITEM 39AV	4 C.Y.	—	4 C.Y.
ITEM 52S	5 TONS	—	5 TONS
ITEM 105S	95 S.Y.	170 S.Y.	265 S.Y.
ITEM 119XA	17 C.Y.	28 C.Y.	45 C.Y.



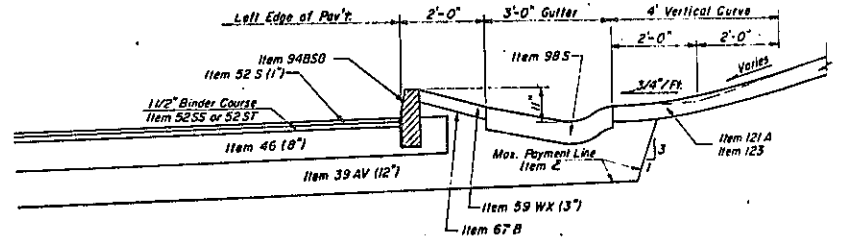
RE-ESTABLISHMENT OF DRIVEWAYS



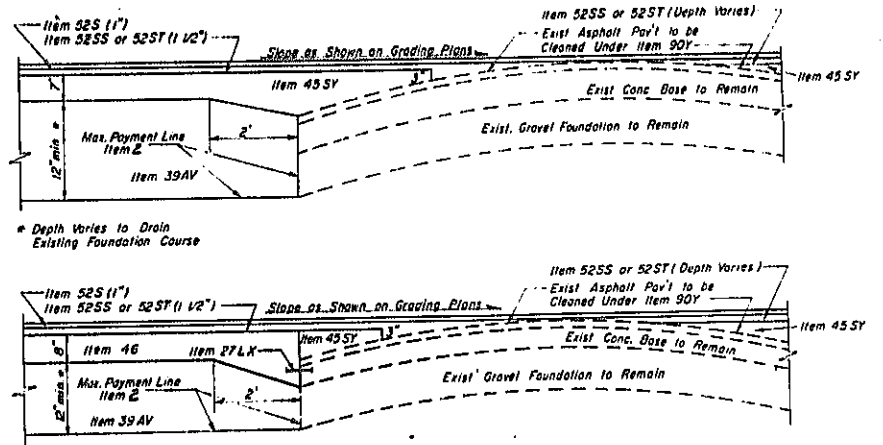
DROP CURB FOR DRIVEWAYS
Scale: 1" = 1'-0"



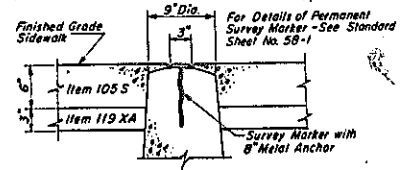
TEMPORARY HIGHWAY RAILROAD BARRICADE
Scale: 1/2" = 1'-0"
ITEM 113 X B ITEM 76 S



SOUTHBOUND HUMBOLDT PARKWAY RELOCATION STA. 12+95 TO 15+45 LT.
(SECTION THRU CURB & GUTTER)
Scale: 1/2" = 1'-0"

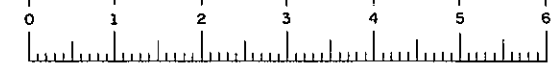


(p) DETAILS FOR ROAD WIDENING
Scale: 1/2" = 1'-0"



DETAIL OF PERMANENT SURVEY MARKER SET IN SIDEWALK
(Not to Scale)

REVISION TO TABLE		
DRIVEWAY DETAILS & TABLE		
ROADWAY DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO.		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACKED	J.R.H. R.J.D. J.R.H.



SCHEDULE A

SCHEDULE B

SCHEDULE C

FED. ROAD REG. NO.	STATE	FEDERAL PROJECT NO.	AID NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)		17	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE

SCHEDULE D

DETAIL SPECIFICATIONS FOR SEEDS

A - MIN. % GERMINATION C - POUNDS PURE LIVE SEED PER ACRE
B - MIN. % GERMINATION AND HARD SEED

NAME	VARIETY	A	B	C
Red Fescue (Festuca Rubra)	Commercial	80		40
Kentucky Bluegrass (Poa Pratensis)	Commercial	75		15
Perennial Rye Grass (Lolium Perenne)	Commercial	85		15
White Clover (Trifolium Repens)	Commercial Max. 25% Hard Seed	60	85	5
TOTAL PURE LIVE SEED PER ACRE				75

STATION TO STATION	SIDE	AREA SQ. FT.	QUANTITY	UNIT	REMARKS
			ITEM 121 A CU. YDS.	ITEM 123 ACRES	
WB 184+00 to WB 187+22	R/L	11729	144.8	0.27	Det. E of S.W. S.W. 1/4 Curb & Median
S.B.H.P. Sere. Rd. to Sta. 15+00	L	6695	82.0	0.15	Det. E of S.W. S.W. 1/4 Curb
N.B.H.P. Sere. Rd. to Sta. 24+00	R	14141	174.6	0.32	Det. E of S.W. S.W. 1/4 Curb & Median
N.B.H.P. Relocation Sta. 196+80 to 204+00	L	20745	256.1	0.48	Det. N.B.H.P. Reloc. & EB Exp'y.
S.B.H.P. Relocation Sta. 21+00 to 13+00	R	65390	807.3	1.50	Det. S.B.H.P. Reloc. & N.B.H.P. Reloc.
Loring-DeGrave Serv. Rd.	R	3674	45.4	0.08	Det. E of S.W. S.W. 1/4 Curb
Ramp HC 10+40 to 11+60	L	7088	87.5	0.16	Det. Ramps HC & HA
EB Expressway Sta. 205+30 to Sta. 206+70	R	2500	30.9	0.06	Det. EB Exp'y & Ramp HA
Ramp HC 10+40 to HA 11+50	R	8760	108.1	0.20	Det. Ramp HC & R.O.W. or R.R.
N.B.H.P. Relocation Sta. 208+40 to 214+00	R	47731	589.8	1.10	Det. N.B.H.P. Reloc. & N.B.H.P. Reloc. or Ramp HA
Ramp HA 12+80 to 13+00	R	27643	341.3	0.64	Det. Ramp HA & R.O.W.
WB 209+50 to WB 215+26	R/L	60984	752.9	1.40	Det. Pavement's & R.O.W.
WB 215+26 to 227+00	-	23186	286.2	0.53	Median
WB 215+26 to Fillmore Ave.	R	22471	277.4	0.52	Det. EB Exp'y. & Ramp FA
" " " " " "	R	27405	338.3	0.63	Det. Ramp FA & R.O.W.
WB 217+50 to Fillmore Ave.	L	12760	157.5	0.29	Det. WB Exp'y. & Ramp FB
WB 215+26 to Fillmore Ave.	L	49374	595.5	1.00	Det. Ramp FB & R.O.W.
Fillmore Ave. to EB 227+00	R/L	18452	227.8	0.42	Pavement's to R.O.W.
Fillmore Ave. 7+70 to 14+50	R/L	4600	56.8	0.11	Det. Curb & S.W. S.W. 1/4 R.O.W. & Island
ROADWAY TOTALS		429,328	5300.3 C.Y.	9.86 ACRES	
N.Y.C.R.R. Structure			12.0 C.Y.	0.02 ACRES	
N.B.H.P. Structure			102.0 C.Y.	0.21 ACRES	
NEET TOTAL			5412.3 C.Y.	10.09 ACRES	

DETAIL SPECIFICATIONS TO ACCOMPANY 1957 PUBLIC WORKS SPECIFICATIONS

ITEM NO.	PAR. NO.	DESCRIPTION
1		CLEARING & GRUBBING
a.		Because of the scarcity of wooded or overgrown areas, work shall be done as ordered by the Engineer.
b.		Protection should be provided for existing trees, which are within construction limits, that will not be removed. Particular care should be taken of trees in Humboldt Parkway and Fillmore Avenue area. Areas outside the limits of construction shall be protected from damage by construction equipment. Restoration of such areas, including grading, seeding, mulching, etc. shall be as specified for Item 123, and shall be at the contractor's expense.
1 WA		APPLYING WATER
		Sodded areas shall be watered as ordered by the Engineer.
121 A		TOPSOIL FURNISHED & PLACED
a.		Areas - See Schedule "A"
b.		As specified for M-47 (Topsoil from borrow)
c.		Thickness - 4"
123		SEEDING
a.		Areas - See Schedule "A"
b.		Seeds - See Schedule "D"
		Fertilizer - M-54 Type 2 @ 10-10
		Mulch - M-57 Straw or mulch material as approved by the Engineer
		Mulch Anchorage - M-5 - Item 70M
c.		Seeding - No seeding shall be done between June 1 and August 15, nor between October 15 and until the ground is workable in the spring, unless otherwise approved by the Engineer.
d.		Rate of Seed - 75 lbs. pure live seed per acre.
e.		Rate of Fertilizer - 800 lbs. per acre or equivalent of 80 lbs. of nitrogen.
f.		Rate of Mulch - 2 to 3 tons per acre or enough to completely cover the ground to the satisfaction of the Engineer.
g.		Rate of Mulch Anchorage - 150 to 200 gallons per acre.
h.		Method - shall apply as specified on 1 on 2 Slopes and steeper.
i.		Method - on flat areas
j.		Alternate "i" as specified
124		SODDING
a.		Areas - See Schedule "A" and as shown on Grading Plans
b.		Topsoil - M-47
		Fertilizer - M-54 Type 2 @ 10-10
		Strikes - M-62
		Water - M-46
		Sod shall be specified under M-59 - TYPE No. 1 - 1 1/2" thick.
		Sod shall be thoroughly watered immediately after placing and before tamping is done.
		Edges of sod shall be placed flush to adjacent surfaces, refer to Standard Sheet No. 50-54.
132		TREE REMOVAL
a.		Areas - See Schedule "A" for the size and number of trees to be removed. See Grading Plans for the location of trees to be removed.
b.		All stumps shall be removed unless otherwise approved.

ITEM 124

LOCATION	SIDE	UNIT	QUANTITY	REMARKS
E.B. Expressway Sta. 198+20 to DeLavan Avenue	R	S.Y.	187	Between EB Exp'y & N.B.H.P.
N.B.H.P. Structure			98	
Neet Total			285	

TREE REMOVAL ITEMS

LOCATION	132-1	132-2	132-3	132-4
	1" DBH	2" DBH	3" DBH	4" DBH
WB Sta. 183+00 to WB Sta. 197+22	31	23	27	15
WB Sta. 197+22 to WB Sta. 204+00	70	72	35	21
DeLavan Av. to Humb. Pkwy	21	36	32	17
WB Sta. 211+44 to WB Sta. 225+26	1	2	1	-
WB Sta. 225+26 to EB Sta. 227+00	-	-	2	3
NEET TOTALS	123	133	97	56

SUMMARY

ITEM	TOTAL QUANTITY ROUNDED	NAME OF ITEM
121A	5,660 CY	Topsoil Furnished and Placed
123	12.00 ACRES	Seeding
124	350 S.Y.	Sodding
132-1	130	Tree Removal
-2	140	"
-3	100	"
-4	60	"

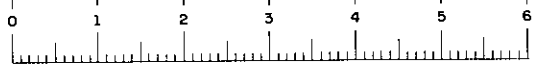
KEY TO ABBREVIATIONS

BBB - BALLED AND BURLAPPED	F.S. - FIELD GROWN
BBP - BALLED AND PLATFORMED	M.C.D. - MINIMUM CROWN DIAMETER
B.E. - BARE ROOT	M.G. - MINOR GROWN
C. - COLLECTED	

ROADSIDE DEVELOPMENT SHEET.

STATE OF NEW YORK - DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY SEC. III.

DE LEUN, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G.L.B. J.K. G.L.B.
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SUMMARY OF DRAINAGE STRUCTURES

Main table with columns: LOCATION, DESCRIPTION, LOCATION, DESCRIPTION, LOCATION, DESCRIPTION, LOCATION, DESCRIPTION. Contains detailed project information for drainage structures.

Project header table with columns: FED. ROAD REG. NO., STATE, FEDERAL AID PROJECT NO., SHEET NO., TOTAL SHEETS. Includes project name: KENSINGTON EXPRESSWAY - SEC. NO. III.

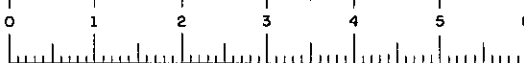
SUMMARY OF UNDERDRAIN (CONT'D.)

Summary of Underdrain table with columns: BEGIN AT STA., OUTLET AT STA., DRAIN NO., SIDE, ITEM CIO-4, ITEM CIO-6, ITEM 155. Includes notes about rock trench and existing manholes.

SUMMARY OF UNDERDRAIN

Summary of Underdrain table with columns: BEGIN AT STA., OUTLET AT STA., DRAIN NO., SIDE, ITEM CIO-4, ITEM CIO-6, ITEM 155. Includes notes about rock trench and existing manholes.

Summary of Drainage Structures and Underdrain header and footer information, including State of New York Dept. of Public Works, City of Buffalo Arterial, Kensington Expressway, Sec. No. III, and drawing details.

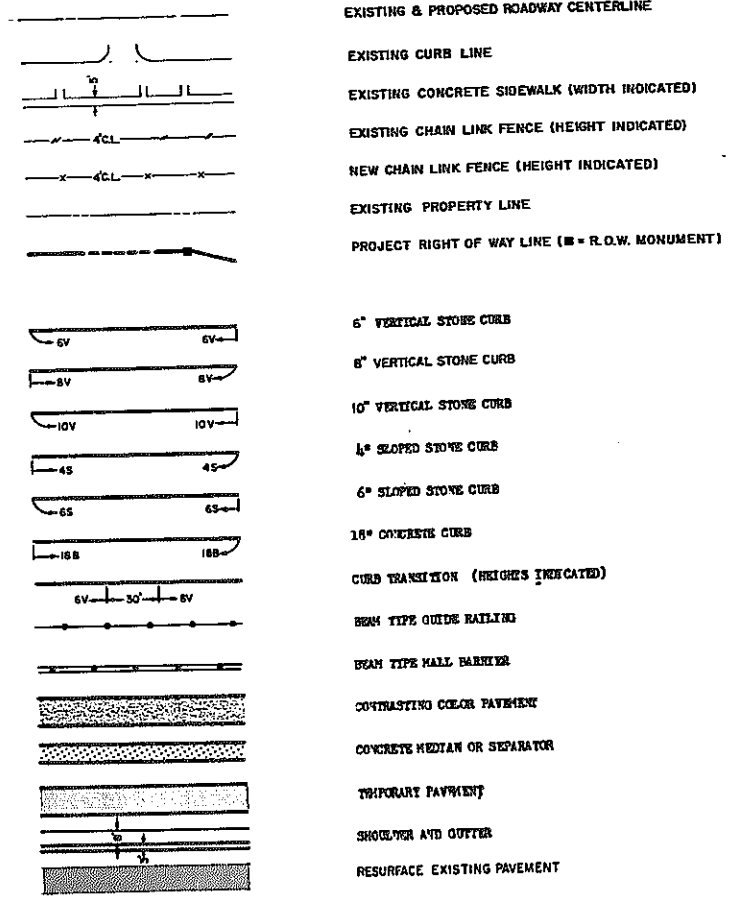


FAC 63-2

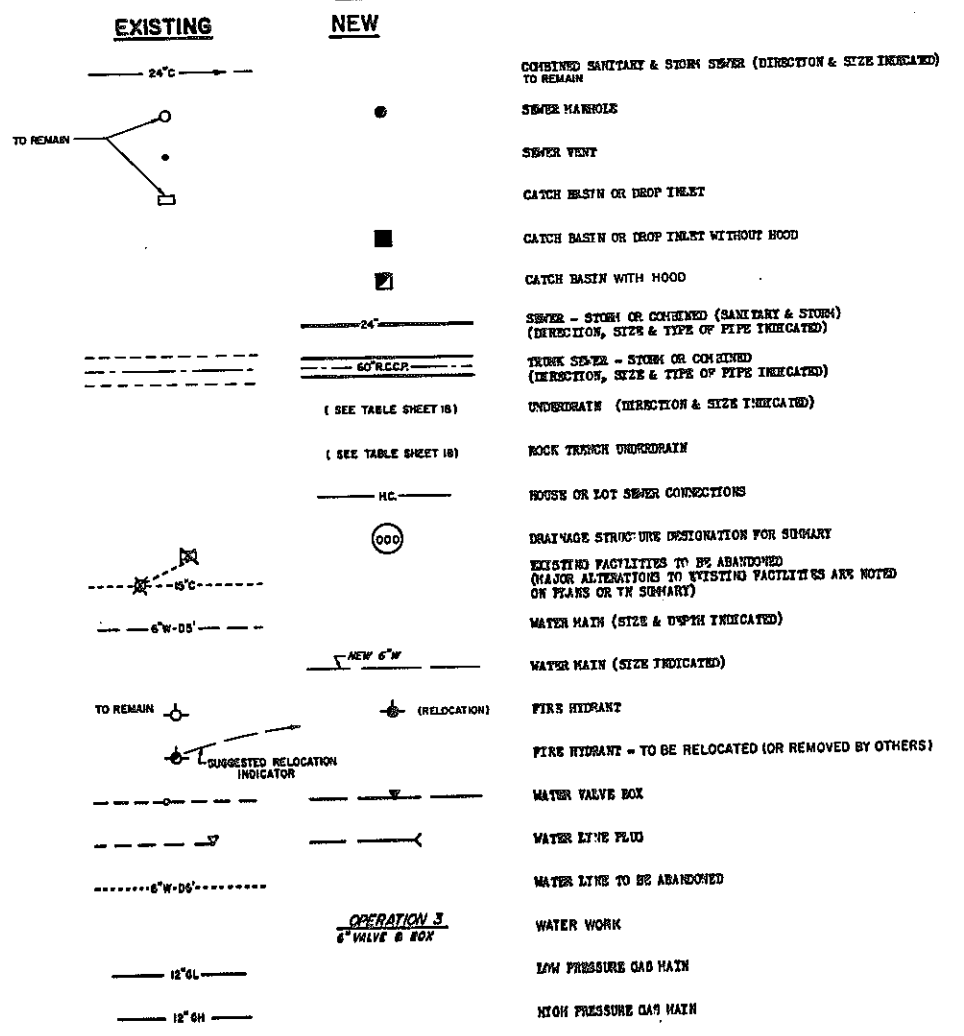
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	19	1173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

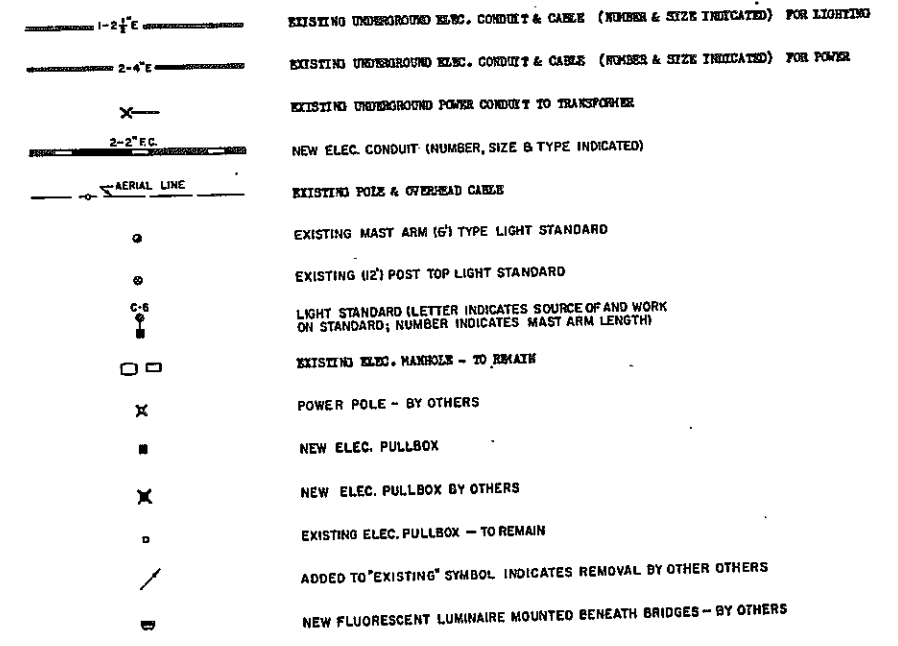
ALIGNMENT & PAVING



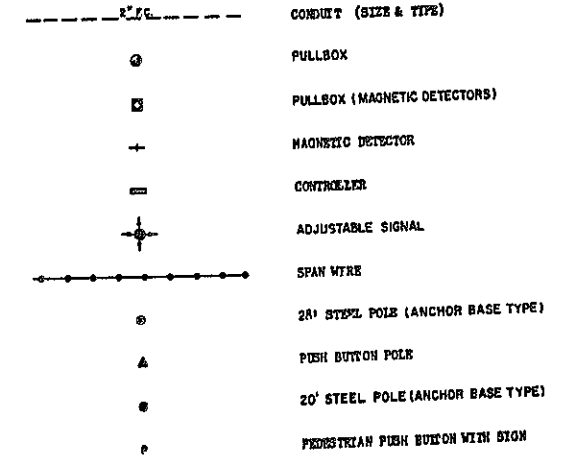
DRAINAGE & UTILITIES



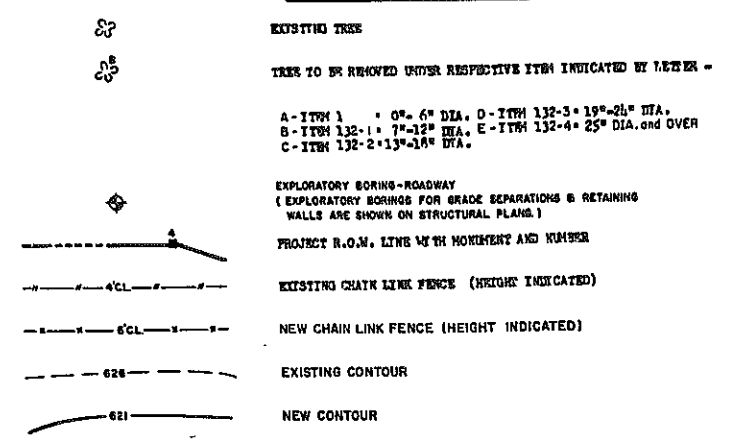
ELECTRICAL DISTRIBUTION



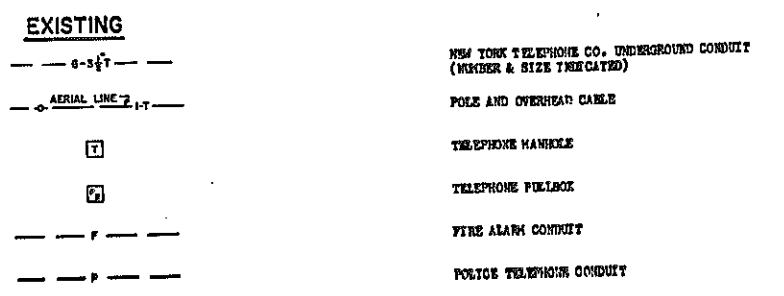
TRAFFIC SIGNALS



GRADING & FENCING



TELEPHONE, POLICE & FIRE ALARM



LEGENDS

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.R. F.J.R. S.J.E.	

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-19-377 (25)	20	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

<p>1 EXPRESSWAY E.B. CURVE DATA</p> <p>$\Delta = 6^{\circ}-10'-41''$ R = 3250' D = 1^{\circ}-45'-46.62" L = 350.44' T = 175.39' E = 473'</p> <p>PC: EB 183+00.01 39.17' LT. E 21+50.00 N 1,062,537.022 E 430,318.465</p> <p>PL: EB 184+75.40 39.17' LT. E 21+50.00 N 1,062,732.412 E 430,318.603</p> <p>PCC: EB 186+50.43 57.88' LT. E 15+99.76 N 1,062,306.798 E 430,299.865</p> <p>PI: EB 183+25.84 75.75' LT. E 136+74.13 N 1,063,300.183 E 430,281.126</p> <p>PT: EB 190+00.67 75.75' LT. E 136+49.52 N 1,063,256.574 E 430,281.264</p> <p>CC: N 1,062,559.574 E 427,068.466</p> <p>CC: N 1,063,254.020 E 433,531.265</p>	<p>2 EXPRESSWAY E.B. CURVE & SPIRAL DATA</p> <p>$\Delta = 4^{\circ}-25'-59''$ R = 1143.92' D = 5^{\circ}-00'-00" L = 5^{\circ}-00'-00" T = 200' E = 145'</p> <p>P = 99.94' K = 99.94' Ts = 568.56' Es = 93.46' Xc = 199.82' Yc = 5.01' Lc = 888.66'</p> <p>PC: EB 202+69.71 12.53' LT. E 151+27.53 N 1,064,523.386 E 430,282.261</p> <p>PL: EB 203+89.23 12.53' LT. E 140+11.53 N 1,064,725.201 E 430,288.228</p> <p>PCC: EB 204+69.71 23.52' LT. E 201+85.50 N 1,063,352.982 E 430,549.212</p> <p>PI: EB 208+38.27 23.52' LT. E 201+85.50 N 1,063,352.982 E 430,549.212</p> <p>PT: EB 210+00.67 75.75' LT. E 136+49.52 N 1,063,256.574 E 430,281.264</p> <p>CC: N 1,062,559.574 E 427,068.466</p> <p>CC: N 1,063,254.020 E 433,531.265</p>	<p>3 EXPRESSWAY W.B. CURVE DATA</p> <p>$\Delta = 6^{\circ}-10'-41''$ R = 3250' D = 1^{\circ}-45'-46.62" L = 350.44' T = 175.39' E = 473'</p> <p>PC: WB 182+21.43 39.17' LT. E 21+50.00 N 1,062,537.110 E 430,206.465</p> <p>PL: WB 183+96.82 39.17' LT. E 21+50.00 N 1,062,732.500 E 430,206.603</p> <p>PCC: WB 185+71.87 57.88' LT. E 15+99.76 N 1,062,306.798 E 430,299.865</p> <p>PI: WB 184+25.84 75.75' LT. E 136+74.13 N 1,063,300.183 E 430,281.126</p> <p>PT: WB 190+00.67 75.75' LT. E 136+49.52 N 1,063,256.574 E 430,281.264</p> <p>CC: N 1,062,559.574 E 427,068.466</p> <p>CC: N 1,063,254.020 E 433,531.265</p>	<p>4 EXPRESSWAY W.B. CURVE & SPIRAL DATA</p> <p>$\Delta = 4^{\circ}-25'-59''$ R = 1143.92' D = 5^{\circ}-00'-00" L = 5^{\circ}-00'-00" T = 200' E = 145'</p> <p>P = 99.94' K = 99.94' Ts = 568.56' Es = 93.46' Xc = 199.82' Yc = 5.01' Lc = 888.66'</p> <p>PC: WB 202+69.71 12.53' LT. E 151+27.53 N 1,064,523.386 E 430,282.261</p> <p>PL: WB 203+89.23 12.53' LT. E 140+11.53 N 1,064,725.201 E 430,288.228</p> <p>PCC: WB 204+69.71 23.52' LT. E 201+85.50 N 1,063,352.982 E 430,549.212</p> <p>PI: WB 208+38.27 23.52' LT. E 201+85.50 N 1,063,352.982 E 430,549.212</p> <p>PT: WB 210+00.67 75.75' LT. E 136+49.52 N 1,063,256.574 E 430,281.264</p> <p>CC: N 1,062,559.574 E 427,068.466</p> <p>CC: N 1,063,254.020 E 433,531.265</p>	<p>5 N.B.H.P. RELOCATION CURVE DATA</p> <p>$\Delta = 4^{\circ}-34'-30''$ R = 3250' D = 1^{\circ}-30'-00" L = 305' T = 152.58' E = 3.05'</p> <p>PC: NB 190+101.2 70.75' LT. E 126+58.75 N 1,065,263.749 E 430,287.272</p> <p>PL: NB 191+62.70 70.75' LT. E 140+11.53 N 1,065,468.375 E 430,287.392</p> <p>PCC: NB 193+15.12 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>PI: NB 193+15.12 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>PT: NB 193+15.12 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>CC: N 1,063,262.794 E 434,106.990</p>	<p>6 N.B.H.P. RELOCATION CURVE & SPIRAL DATA</p> <p>$\Delta = 4^{\circ}-45'-00''$ R = 904.67' D = 3^{\circ}-22'-00" L = 585.08' T = 293.18' E = 11.86'</p> <p>P = 99.91' K = 99.91' Ts = 501.45' Es = 86.67' Xc = 199.71' Yc = 5.53' Lc = 553.95'</p> <p>PC: NB 201+63.05 42.20' LT. E 150+04.22 N 1,064,415.634 E 430,367.980</p> <p>PL: NB 203+83.05 42.20' LT. E 151+92.35 N 1,064,615.285 E 430,376.731</p> <p>PCC: NB 204+63.05 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>PI: NB 206+64.19 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>PT: NB 206+64.19 55.55' LT. E 141+63.42 N 1,064,570.460 E 430,299.682</p> <p>CC: N 1,064,622.503 E 431,466.262</p>	<p>7 S.B.H.P. RELOCATION CURVE DATA</p> <p>$\Delta = 9^{\circ}-16'-04.4''$ R = 3617.07' D = 1^{\circ}-35'-02.54" L = 585.08' T = 293.18' E = 11.86'</p> <p>PCC: SB 190+00.67 98.40' LT. E 166+31.03 N 1,065,249.584 E 429,361.729</p> <p>PL: SB 192+93.85 104.61' LT. E 162+98.02 N 1,065,232.488 E 429,359.928</p> <p>PCC: SB 194+00.67 98.40' LT. E 166+31.03 N 1,065,249.584 E 429,361.729</p> <p>PI: SB 194+00.67 98.40' LT. E 166+31.03 N 1,065,249.584 E 429,361.729</p> <p>PT: SB 194+00.67 98.40' LT. E 166+31.03 N 1,065,249.584 E 429,361.729</p> <p>CC: N 1,065,303.034 E 426,656.519</p>	<p>8 S.B.H.P. RELOCATION CURVE DATA</p> <p>$\Delta = 20^{\circ}-58'-11''$ R = 1900' D = 3^{\circ}-00'-56.05" L = 693.36 T = 183.62' E = 32.26'</p> <p>PCC: SB 201+63.05 82.90' LT. E 159+89.23 N 1,065,249.584 E 429,361.729</p> <p>PL: SB 203+83.05 82.90' LT. E 159+89.23 N 1,065,249.584 E 429,361.729</p> <p>PCC: SB 204+63.05 95.55' LT. E 150+04.22 N 1,064,570.460 E 430,299.682</p> <p>PI: SB 206+64.19 95.55' LT. E 150+04.22 N 1,064,570.460 E 430,299.682</p> <p>PT: SB 206+64.19 95.55' LT. E 150+04.22 N 1,064,570.460 E 430,299.682</p> <p>CC: N 1,064,245.071 E 428,332.164</p>	<p>9 S.B.H.P. RELOCATION CURVE DATA</p> <p>$\Delta = 12^{\circ}-20'-40''$ R = 1700' D = 3^{\circ}-22'-13.22" L = 366.27' T = 183.62' E = 9.91'</p> <p>PCC: SB 22+81.14 84.32' LT. E 152+70.69 N 1,064,415.634 E 430,188.554</p> <p>PL: SB 24+84.98 84.32' LT. E 150+04.22 N 1,064,415.634 E 430,188.554</p> <p>PCC: SB 26+47.41 114.67' LT. E 149+96.36 N 1,064,245.071 E 430,227.375</p> <p>PI: SB 26+47.41 114.67' LT. E 149+96.36 N 1,064,245.071 E 430,227.375</p> <p>PT: SB 26+47.41 114.67' LT. E 149+96.36 N 1,064,245.071 E 430,227.375</p> <p>CC: N 1,064,287.675 E 428,527.574</p>	<p>10 S.B.H.P. SERVICE RD. CURVE DATA</p> <p>$\Delta = 1^{\circ}-52'-24''$ R = 3000' D = 1^{\circ}-54'-35.50" L = 98.09' T = 49.05' E = .40'</p> <p>PC: SB 145+41.34 170.25' LT. E 145+40.00 N 1,065,947.126 E 430,188.307</p> <p>PL: SB 145+90.39 170.25' LT. E 145+40.00 N 1,065,947.126 E 430,188.307</p> <p>PCC: SB 146+39.43 69.47' LT. E 146+47.16 N 1,064,045.197 E 430,186.781</p> <p>PI: SB 146+39.43 69.47' LT. E 146+47.16 N 1,064,045.197 E 430,186.781</p> <p>PT: SB 146+39.43 69.47' LT. E 146+47.16 N 1,064,045.197 E 430,186.781</p> <p>CC: N 1,063,949.492 E 427,188.508</p>
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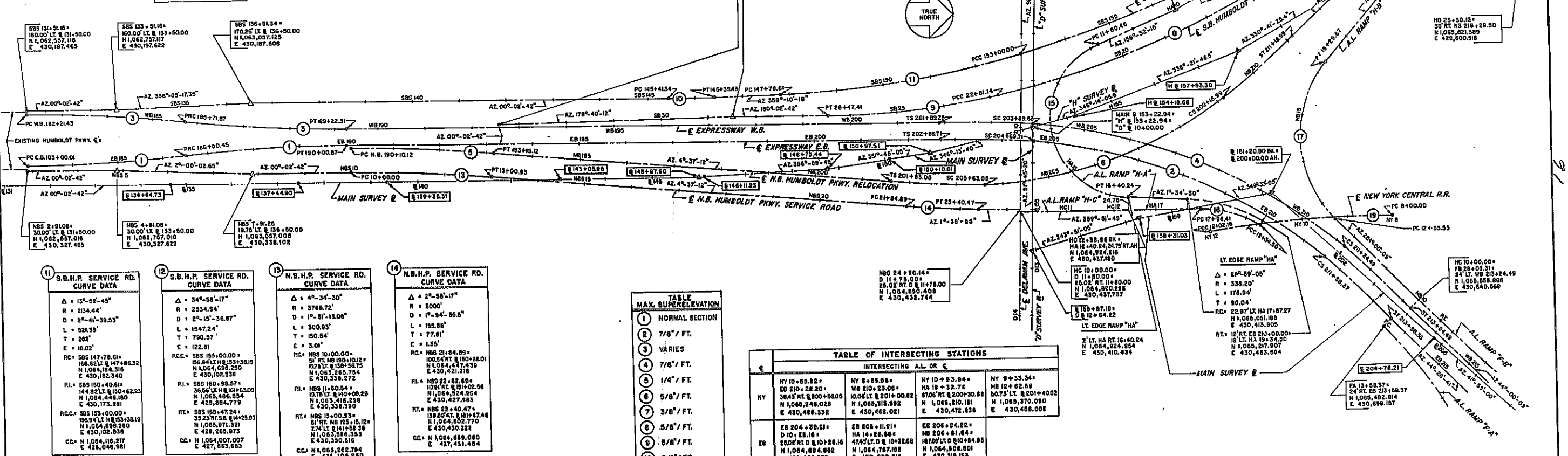


TABLE MAX. SUPERELEVATION

1	NORMAL SECTION
2	7/8" / FT.
3	VARIES
4	7/8" / FT.
5	1/4" / FT.
6	5/8" / FT.
7	3/8" / FT.
8	5/8" / FT.
9	3/8" / FT.
10	3/8" / FT.
11	3/8" / FT.
12	VARIES
13	1/4" / FT.
14	NORMAL SECTION
15	5/8" / FT.
16	7/8" / FT.
17	7/8" / FT.
18	1/2" / FT.
19	VARIES

TABLE OF INTERSECTING STATIONS

STATION	INTERSECTING A.L. OR C.		
	E	NY	NY
NY 10+55.82	EB 210+24.20	NY 8+89.89	NY 19+33.34
NY 210+24.20	EB 210+24.20	NY 210+24.20	NY 12+62.58
NY 210+24.20	EB 210+24.20	NY 210+24.20	NY 50.73' LT. E 201+40.02
NY 210+24.20	EB 210+24.20	NY 210+24.20	NY 1,065,370.088
NY 210+24.20	EB 210+24.20	NY 210+24.20	NY 430,458.098
EB 204+59.81	EB 204+59.81	EB 204+59.81	EB 206+84.22
EB 204+59.81	EB 204+59.81	EB 204+59.81	EB 206+84.22
EB 204+59.81	EB 204+59.81	EB 204+59.81	EB 206+84.22
EB 204+59.81	EB 204+59.81	EB 204+59.81	EB 206+84.22
WB 203+61.81	WB 203+61.81	WB 203+61.81	WB 207+33.55
WB 203+61.81	WB 203+61.81	WB 203+61.81	WB 207+33.55
WB 203+61.81	WB 203+61.81	WB 203+61.81	WB 207+33.55
WB 203+61.81	WB 203+61.81	WB 203+61.81	WB 207+33.55
NB 204+40.26	NB 204+40.26	NB 204+40.26	NB 206+49.54
NB 204+40.26	NB 204+40.26	NB 204+40.26	NB 206+49.54
NB 204+40.26	NB 204+40.26	NB 204+40.26	NB 206+49.54
NB 204+40.26	NB 204+40.26	NB 204+40.26	NB 206+49.54
SB 21+42.13	SB 21+42.13	SB 21+42.13	SB 22+31.53
SB 21+42.13	SB 21+42.13	SB 21+42.13	SB 22+31.53
SB 21+42.13	SB 21+42.13	SB 21+42.13	SB 22+31.53
SB 21+42.13	SB 21+42.13	SB 21+42.13	SB 22+31.53
NY 12+62.58	NY 12+62.58	NY 12+62.58	NY 12+62.58
NY 12+62.58	NY 12+62.58	NY 12+62.58	NY 12+62.58
NY 12+62.58	NY 12+62.58	NY 12+62.58	NY 12+62.58
NY 12+62.58	NY 12+62.58	NY 12+62.58	NY 12+62.58

NOTES:
1. Refer to Expressway and Ramp Profiles for banking on curves.
2. For Base Line Coordinates refer to Alignment and Paving Plans.

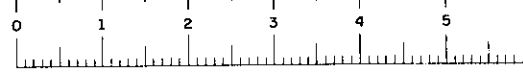
SCALE: 1" = 100'

ALIGNMENT DATA
DELANVAN INTERCHANGE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

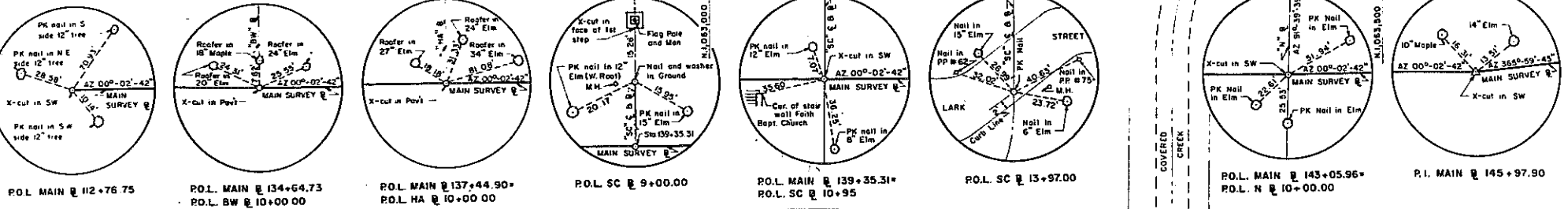
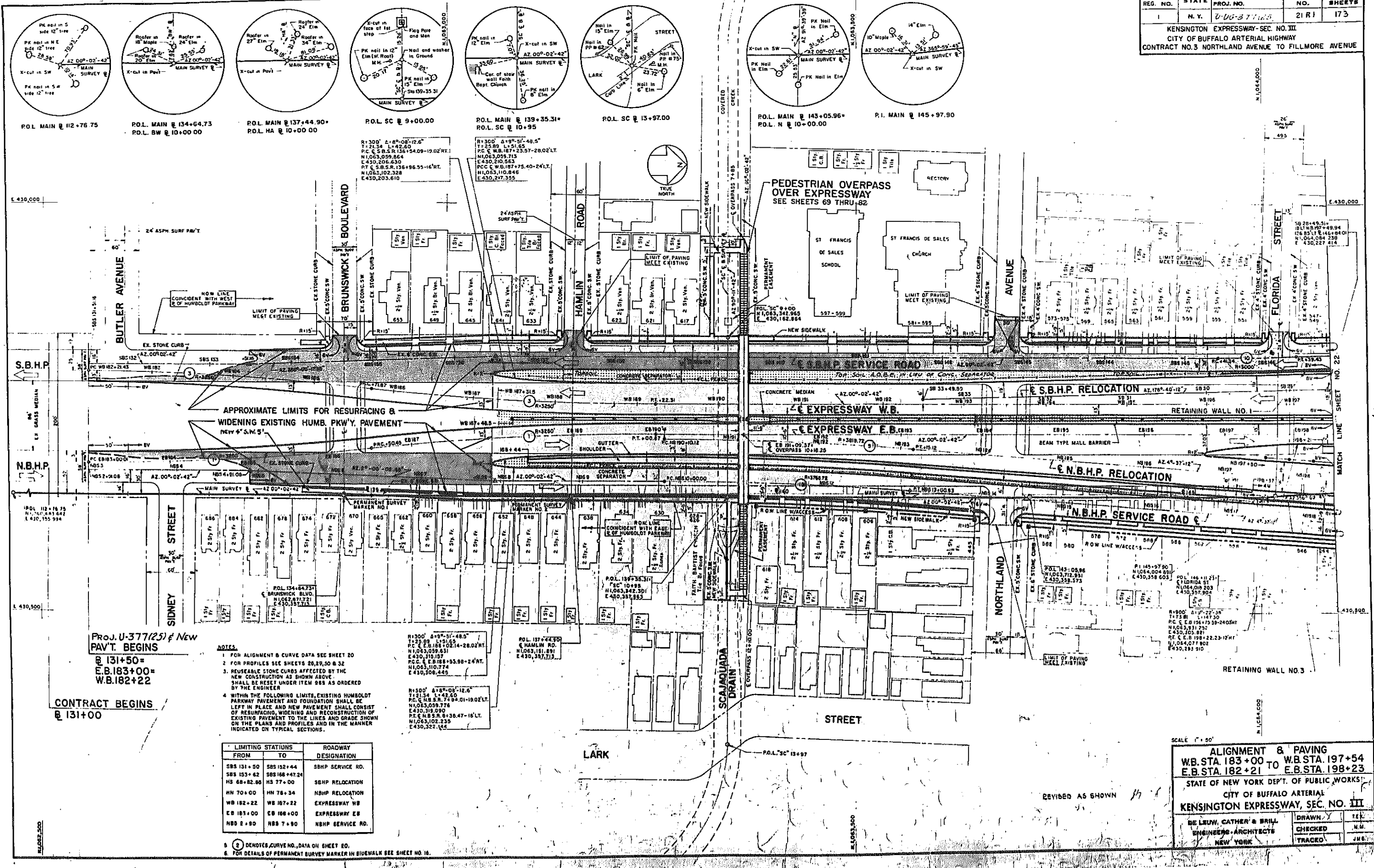
DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN: T.E.K.
CHECKED: J.M.H.
TRACKED: J.M.H.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-06-371(2)5	21 R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



APPROXIMATE LIMITS FOR RESURFACING & WIDENING EXISTING HUMB. PKWY. PAVEMENT
NEW 4' S.W. 5'

Proj. U-377(2)5 New PAV'T. BEGINS
@ 131+50 =
E.B. 183+00 =
W.B. 182+22

CONTRACT BEGINS
@ 131+00

NOTES

- FOR ALIGNMENT & CURVE DATA SEE SHEET 20
- FOR PROFILES SEE SHEETS 20, 29, 30 & 32
- REUSABLE STONE CURBS AFFECTED BY THE NEW CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 088 AS ORDERED BY THE ENGINEER
- WITHIN THE FOLLOWING LIMITS, EXISTING HUMBOLDT PARKWAY PAVEMENT AND FOUNDATION SHALL BE LEFT IN PLACE AND NEW PAVEMENT SHALL CONSIST OF RESURFACING, WIDENING AND RECONSTRUCTION OF EXISTING PAVEMENT TO THE LINES AND GRADE SHOWN ON THE PLANS AND PROFILES AND IN THE MANNER INDICATED ON TYPICAL SECTIONS.

LIMITING STATIONS	ROADWAY DESIGNATION
SBS 131+50 TO SBS 132+44	SBHP SERVICE RD.
SBS 133+42 TO SBS 168+47.24	SBHP RELOCATION
NS 68+82.80 TO NS 77+00	SBHP RELOCATION
NN 70+00 TO NN 78+34	NSHP RELOCATION
WB 182+22 TO WB 187+22	EXPRESSWAY WB
EB 183+00 TO EB 186+00	EXPRESSWAY EB
NBS 7+80 TO NBS 7+80	NBHP SERVICE RD.

5 DENOTES CURVE NO. DATA ON SHEET 20.
6 FOR DETAILS OF PERMANENT SURVEY MARKER IN SIDEWALK SEE SHEET NO. 16.

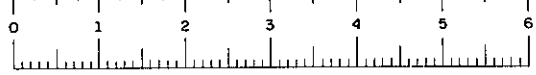
SCALE 1" = 50'

ALIGNMENT & PAVING
W.B. STA. 183+00 TO W.B. STA. 197+54
E.B. STA. 182+21 TO E.B. STA. 198+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

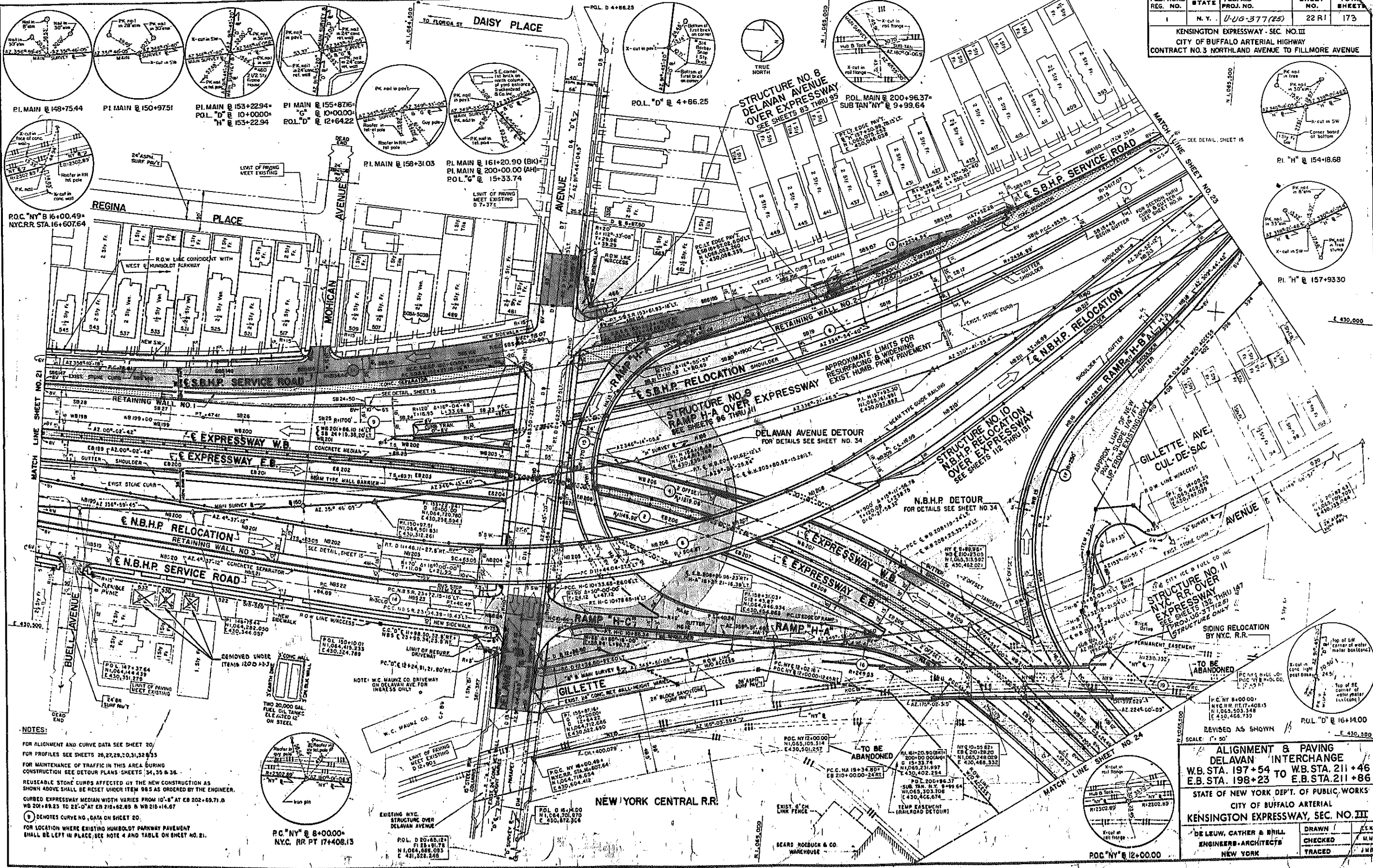
BE LEW, CATHER & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN	TEK
CHECKED	N.M.
TRACED	J.M.E.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	22 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



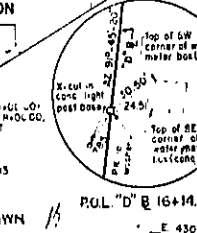
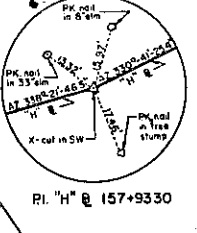
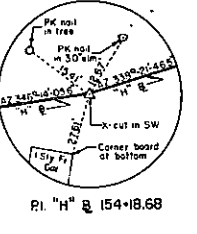
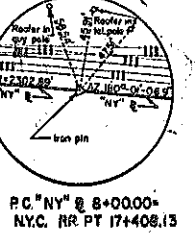
NOTES:

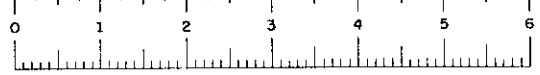
FOR ALIGNMENT AND CURVE DATA SEE SHEET 20/
FOR PROFILES SEE SHEETS 26, 27, 28, 29, 30, 31, 32, 33
FOR MAINTENANCE OF TRAFFIC IN THIS AREA DURING CONSTRUCTION SEE DETOUR PLANS SHEETS 34, 35 & 36
REUSABLE STONE CURBS AFFECTED BY THE NEW CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 865 AS ORDERED BY THE ENGINEER.
CURBED EXPRESSWAY MEDIAN WIDTH VARIES FROM 10'-6" AT EB 202+69.71 & WB 201+89.25 TO 22'-0" AT EB 215+62.85 & WB 216+14.67
⊙ DENOTES CURVE NO. DATA ON SHEET 20.
FOR LOCATION WHERE EXISTING HUMSBOLT PARKWAY PAVEMENT SHALL BE LEFT IN PLACE, SEE NOTE 4 AND TABLE ON SHEET NO. 21.

ALIGNMENT & PAVING
DELAVAN INTERCHANGE
W.B. STA. 197+54 TO W.B. STA. 211+46
E.B. STA. 198+23 TO E.B. STA. 211+86

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

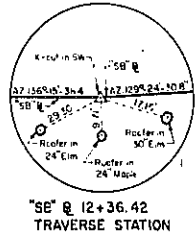
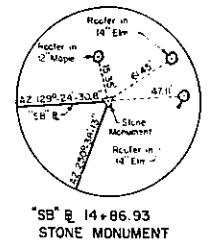
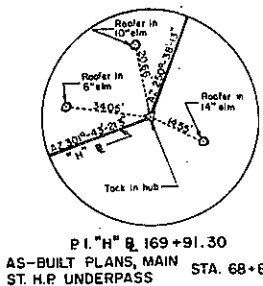
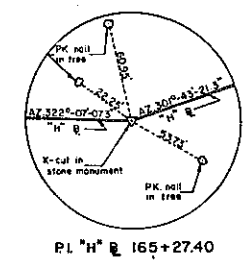
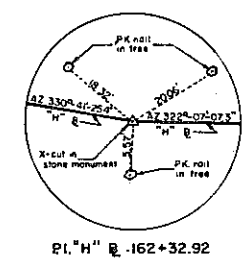
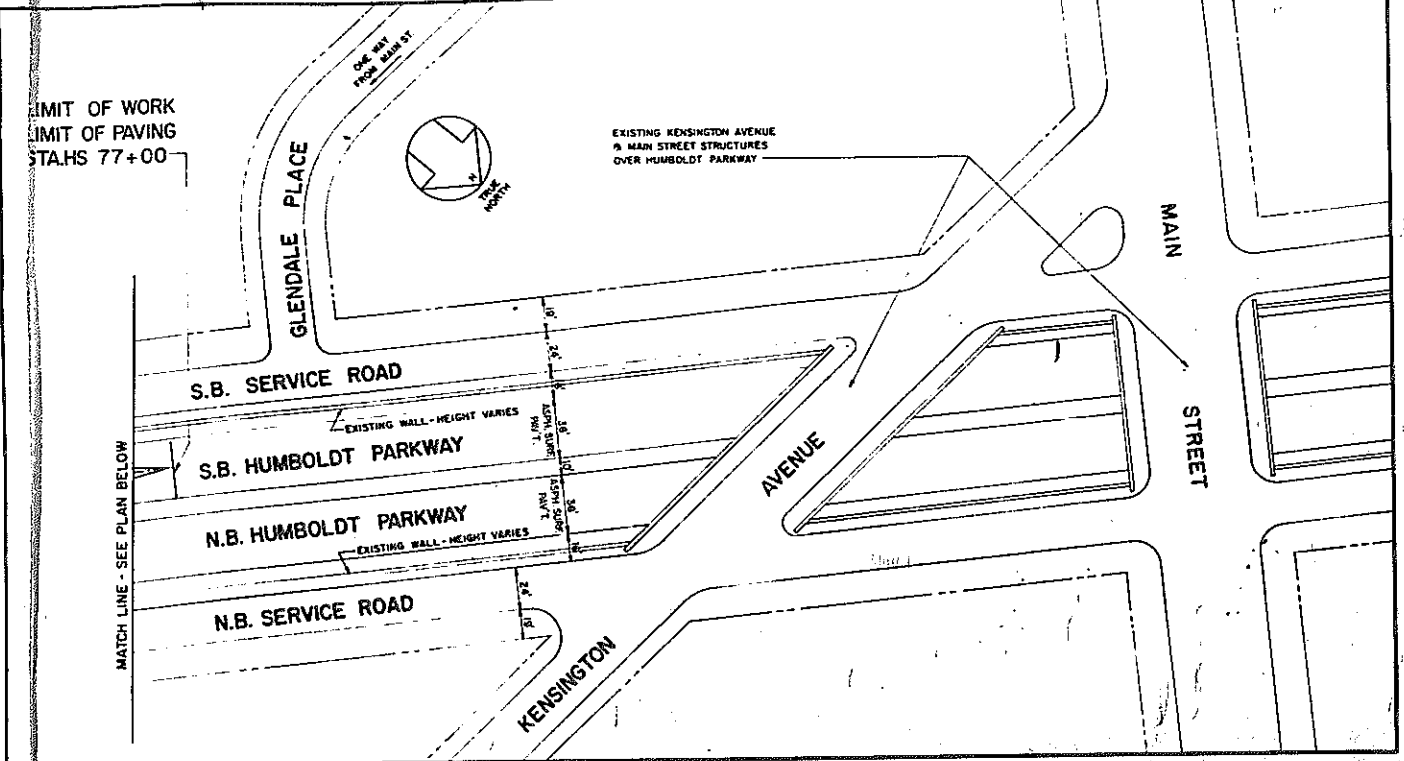
DE LEUW, CATHY & BRILL	DRAWN	J.C.K.
ENGINEERS-ARCHITECTS	CHECKED	M.M.
NEW YORK	TRACED	J.M.B.





FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	0-05-377(85)	23 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



20 N.B.H.P. RELOCATION CURVE DATA

Δ	7°-39'-56"
R	2623.94'
D	2°-11'-00.96"
L	331.05'
T	175.79'
E	5.88'
CC	N1,064,007.007 E 427,663.663
PC	NB 220+05.29
PT	NB 221+80.55
MAX. SUPEREL.	1/2" FT.

21 S.B.H.P. RELOCATION CURVE DATA

Δ	8°-15'-30.6"
R	2575.94'
D	2°-13'-27.37"
L	371.29'
T	193.18'
E	6.70'
CC	N1,064,007.007 E 427,663.663
PC	NB 221+80.55
PT	NB 223+00.27
MAX. SUPEREL.	1/2" FT.

7 S.B.H.P. RELOCATION CURVE DATA

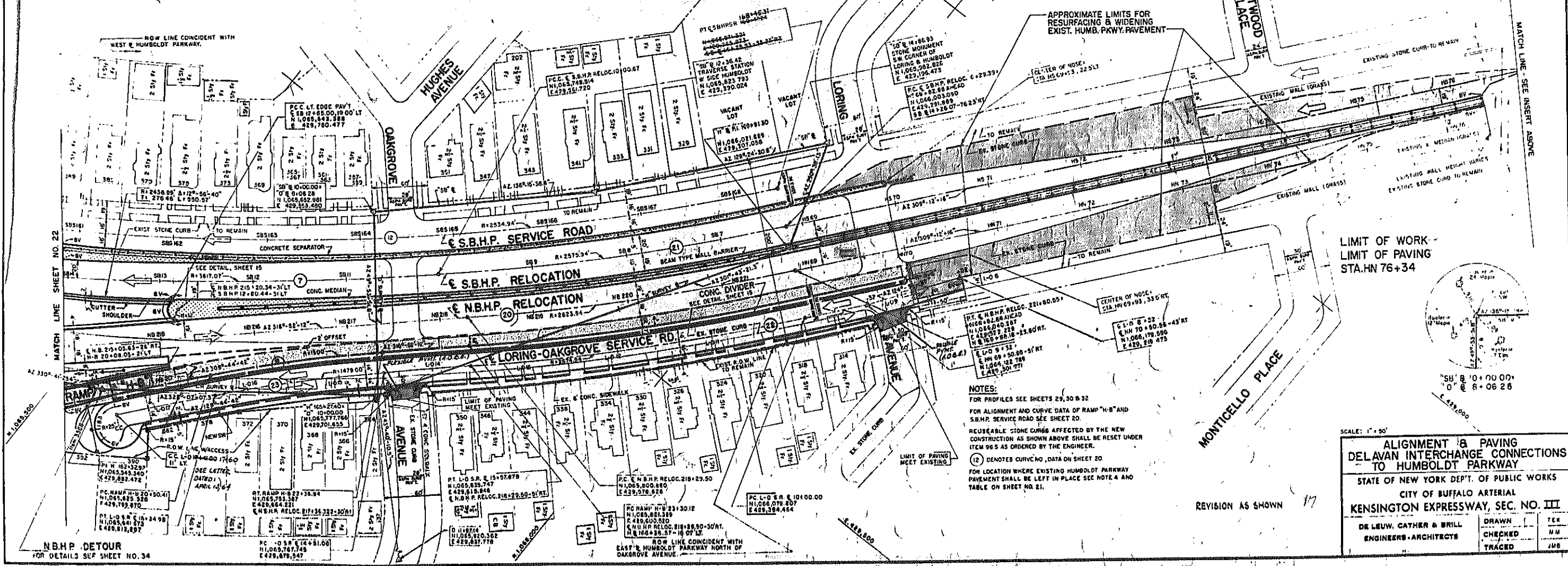
Δ	9°-16'-04.4"
R	3617.07'
D	1°-35'-02.54"
L	585.05'
T	293.18'
E	11.86'
CC	N1,064,007.007 E 427,663.663
PC	NB 223+00.27
PT	NB 225+00.27
MAX. SUPEREL.	1/2" FT.

22 L-O SERVICE RD. CURVE DATA

Δ	7°-39'-56"
R	2674.94'
D	2°-06'-31.02"
L	357.89'
T	179.21'
E	6.00'
CC	N1,064,007.007 E 427,663.663
PC	NB 225+00.27
PT	NB 227+00.27
MAX. SUPEREL.	1/2" FT.

23 L-O SERVICE RD. CURVE DATA

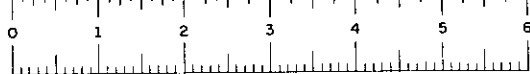
Δ	7°-07'-30"
R	1479.00'
D	3°-52'-26.22"
L	183.92'
T	92.04'
E	2.86'
CC	N1,066,778.975 E 430,299.923
PC	NB 227+00.27
PT	NB 229+00.27
MAX. SUPEREL.	1/2" FT.



NOTES:
 FOR PROFILES SEE SHEETS 29, 30 & 32
 FOR ALIGNMENT AND CURVE DATA OF RAMP "H-B" AND S.B.H.P. SERVICE ROAD SEE SHEET 20
 REUSABLE STONE CURBS AFFECTED BY THE NEW CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 96.5 AS ORDERED BY THE ENGINEER.
 (12) DENOTES CURVING DATA ON SHEET 20
 FOR LOCATION WHERE EXISTING HUMBOLDT PARKWAY PAVEMENT SHALL BE LEFT IN PLACE SEE NOTE 4 AND TABLE ON SHEET NO. 21.

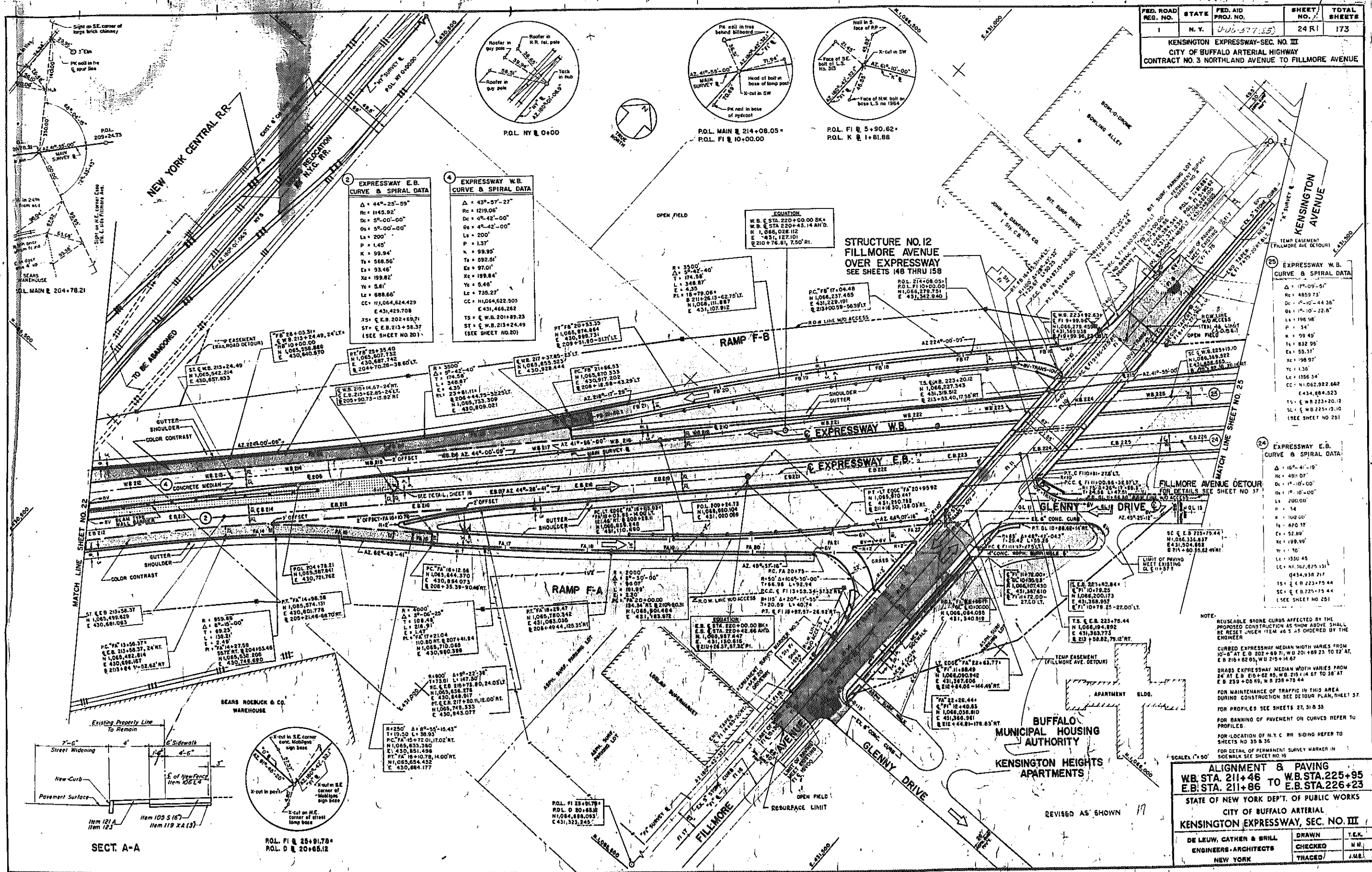
ALIGNMENT & PAVING
 DELAVAN INTERCHANGE CONNECTIONS
 TO HUMBOLDT PARKWAY
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III
 DE LEUW, CATHY & BRILL
 ENGINEERS-ARCHITECTS
 DRAWN: T.C.K.
 CHECKED: M.W.
 TRACED: J.W.B.

REVISION AS SHOWN 17



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	306-377-55	24 R1	173

KENSINGTON EXPRESSWAY-SEC. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



EXPRESSWAY E.B. CURVE & SPIRAL DATA

$\Delta = 44^{\circ}-25'-09''$
$Rc = 1145.92'$
$Dc = 5^{\circ}-00'-00''$
$Os = 5^{\circ}-00'-00''$
$Ls = 200'$
$P = 1.45'$
$K = 99.94'$
$Ts = 568.56'$
$Es = 93.46'$
$Xe = 199.84'$
$Ye = 5.81'$
$Lc = 688.60'$
$Cc = 11,064,524.429$
$E = 431,429.708$
$Ts = E.B. 215+24.49$
$St = E.B. 213+58.37$

EXPRESSWAY W.B. CURVE & SPIRAL DATA

$\Delta = 43^{\circ}-57'-27''$
$Rc = 1219.06'$
$Dc = 4^{\circ}-42'-00''$
$Os = 4^{\circ}-42'-00''$
$Ls = 200'$
$P = 1.37'$
$K = 99.95'$
$Ts = 592.51'$
$Es = 97.01'$
$Xe = 199.84'$
$Ye = 5.40'$
$Lc = 735.27'$
$Cc = 11,064,622.503$
$E = 431,466.262$
$Ts = E.W.B. 201+89.23$
$St = E.W.B. 213+24.49$

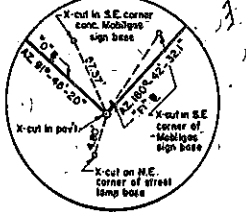
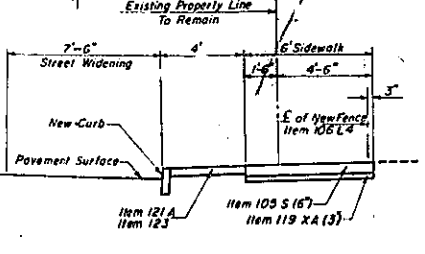
STRUCTURE NO. 12
FILLMORE AVENUE
OVER EXPRESSWAY
SEE SHEETS 148 THRU 158

EXPRESSWAY W.B. CURVE & SPIRAL DATA

$\Delta = 17^{\circ}-09'-51''$
$Rc = 4859.75'$
$Dc = 1^{\circ}-10'-44.36''$
$Os = 1^{\circ}-10'-22.8''$
$Ls = 198.98'$
$P = .34'$
$K = 99.45'$
$Ts = 182.90'$
$Es = 55.37'$
$Xe = 198.97'$
$Ye = 1.30'$
$Lc = 1356.34'$
$Cc = 11,062,922.662$
$E = 434,084.523$
$Ts = E.W.B. 225+10.10$
$St = E.W.B. 225+10.10$

EXPRESSWAY E.B. CURVE & SPIRAL DATA

$\Delta = 16^{\circ}-41'-10''$
$Rc = 4911.07'$
$Dc = 1^{\circ}-10'-00''$
$Os = 1^{\circ}-10'-00''$
$Ls = 200.00'$
$P = .34'$
$K = 100.00'$
$Ts = 870.37'$
$Es = 52.89'$
$Xe = 199.95'$
$Ye = .30'$
$Lc = 1330.45'$
$Cc = 11,062,875.131$
$E = 434,336.217$
$Ts = E.B. 223+75.44$
$St = E.B. 223+75.44$



NOTE:
REUSABLE STONE CURBS AFFECTED BY THE PROPOSED CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 46.5 AS ORDERED BY THE ENGINEER.
CURBED EXPRESSWAY MEDIAN WIDTH VARIES FROM 10'-6" AT E.B. 202+89.71, W.B. 201+89.23 TO 22' AT E.B. 210+62.00, W.B. 210+14.67.
GRASS EXPRESSWAY MEDIAN WIDTH VARIES FROM 24' AT E.B. 210+62.00, W.B. 210+14.67 TO 36' AT E.B. 239+05.59, W.B. 238+75.44.
FOR MAINTENANCE OF TRAFFIC IN THIS AREA DURING CONSTRUCTION SEE DETOUR PLAN, SHEET 37.
FOR PROFILES SEE SHEETS 27, 31 & 35.
FOR BANKING OF PAVEMENT ON CURVES REFER TO PROFILES.
FOR LOCATION OF N.Y.C. RR SIDING REFER TO SHEETS NO. 35 & 36.
FOR DETAIL OF PERMANENT SURVEY MARKER IN SIDEWALK SEE SHEET NO. 10.

ALIGNMENT & PAVING
WB. STA. 211+46 TO WB. STA. 225+95
E.B. STA. 211+86 TO E.B. STA. 226+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

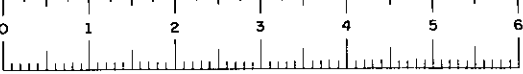
DE LEUW, CATHAR & BRILL	DRAWN	T.C.H.
ENGINEERS-ARCHITECTS	CHECKED	M.M.
NEW YORK	TRACED	J.M.E.

SECT. A-A

ROLL. FI & 25+91.70
ROLL. D & 20+65.12

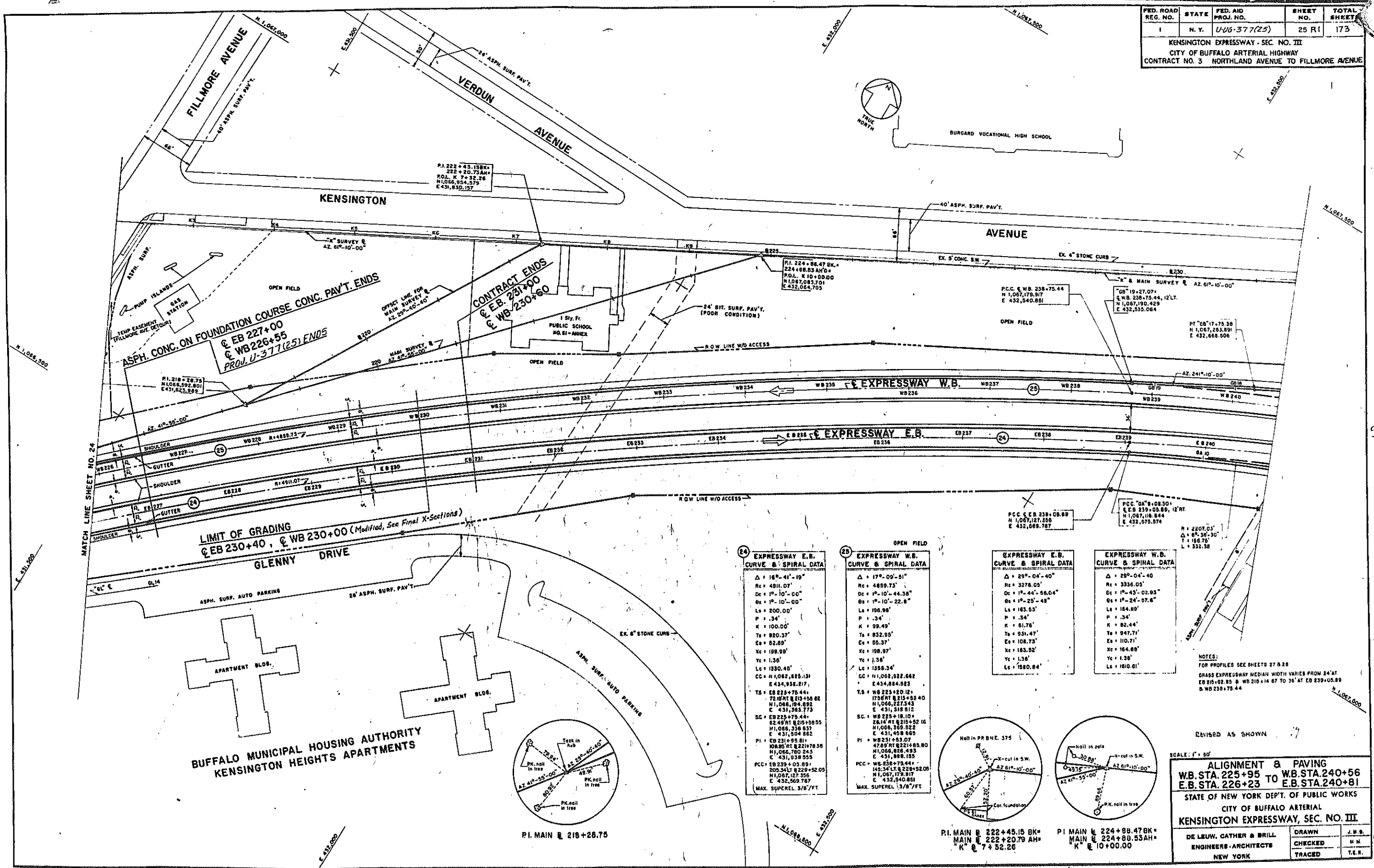
BUFFALO MUNICIPAL HOUSING AUTHORITY
KENSINGTON HEIGHTS APARTMENTS

REVISED AS SHOWN



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	25 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



24 EXPRESSWAY E.B. CURVE & SPIRAL DATA

Δ = 16°-41'-19"
Rc = 4911.07'
Dc = 1°-10'-00"
Ea = 1°-10'-00"
Ls = 200.00'
P = .34'
K = 100.00'
Tc = 820.37'
Ea = 82.89'
Xc = 199.99'
Yc = 1.36'
Lc = 1330.40'
CC = N 1,082,826.13
E 434,938.217'
TS = EB 225+75.44+
72.16 RT @ 215+53.40
N 1,086,194.852
E 431,585.775
SC = EB 225+75.44+
82.49 RT @ 215+53.40
N 1,086,359.522
E 431,534.862
PI = EB 231+95.81+
108.85 RT @ 221+76.36
N 1,086,780.245
E 431,938.555
PCC = EB 239+05.89+
205.34 LT @ 229+52.05
N 1,087,173.917
E 432,540.851
MAX. SUPEREL 3/8"/FT.

25 EXPRESSWAY W.B. CURVE & SPIRAL DATA

Δ = 17°-09'-51"
Rc = 4859.73'
Dc = 1°-10'-44.38"
Ea = 1°-10'-22.8"
Ls = 196.98'
P = .34'
K = 99.49'
Tc = 832.95'
Ea = 85.37'
Xc = 198.97'
Yc = 1.36'
Lc = 1358.34'
CC = N 1,082,822.662
E 434,884.523
TS = WB 225+20.12+
72.16 RT @ 215+53.40
N 1,086,227.543
E 431,518.812
SC = WB 225+19.10+
26.14 RT @ 215+52.16
N 1,086,359.522
E 431,458.665
PI = WB 231+53.07+
47.89 RT @ 221+76.36
N 1,086,826.493
E 431,898.153
PCC = WB 238+75.44+
142.34 LT @ 228+52.05
N 1,087,173.917
E 432,540.851
MAX. SUPEREL 3/8"/FT.

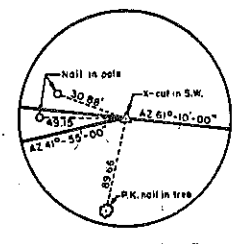
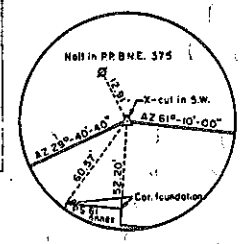
EXPRESSWAY E.B. CURVE & SPIRAL DATA

Δ = 29°-04'-40"
Rc = 3278.05'
Dc = 1°-44'-56.04"
Ea = 1°-25'-48"
Ls = 163.53'
P = .34'
K = 81.76'
Tc = 931.47'
Ea = 108.73'
Xc = 163.52'
Yc = 1.36'
Lc = 1580.84'

EXPRESSWAY W.B. CURVE & SPIRAL DATA

Δ = 29°-04'-40"
Rc = 3336.05'
Dc = 1°-45'-02.93"
Ea = 1°-24'-57.6"
Ls = 164.89'
P = .34'
K = 82.44'
Tc = 947.71'
Ea = 110.71'
Xc = 164.88'
Yc = 1.36'
Lc = 1610.01'

NOTES:
FOR PROFILES SEE SHEETS 27 & 28
GRADE EXPRESSWAY MEDIAN WIDTH VARIES FROM 24' AT
EB 215+02.85 & WB 210+14.67 TO 36' AT EB 239+05.89
& WB 238+75.44



PI MAIN @ 222+45.15 BK+
MAIN @ 222+20.79 AH+
K @ 7+52.26

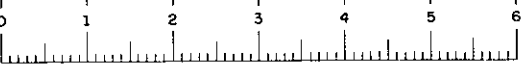
PI MAIN @ 224+88.47 BK+
MAIN @ 224+80.55 AH+
K @ 10+00.00

BUFFALO MUNICIPAL HOUSING AUTHORITY
KENSINGTON HEIGHTS APARTMENTS

PI MAIN @ 218+26.75

SCALE: 1" = 50'
ALIGNMENT & PAVING
WB STA. 225+95 TO WB STA. 240+56
E.B. STA. 226+23 TO E.B. STA. 240+81
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

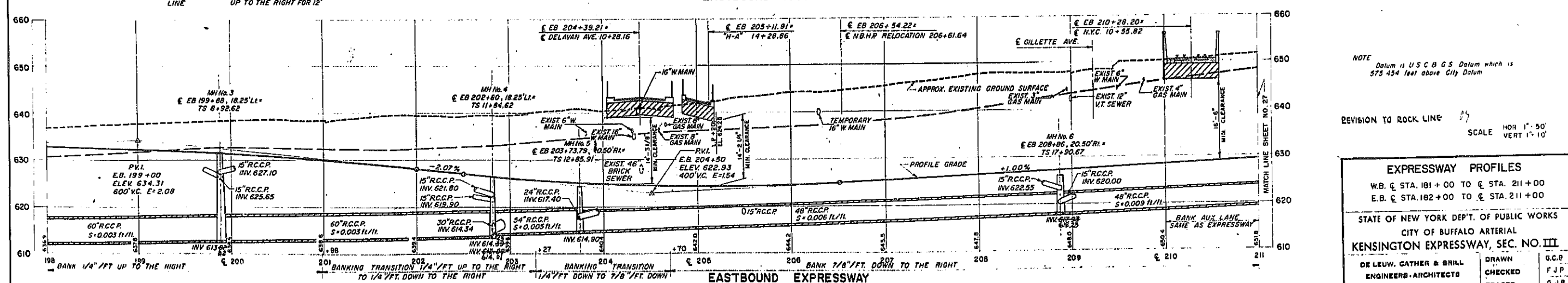
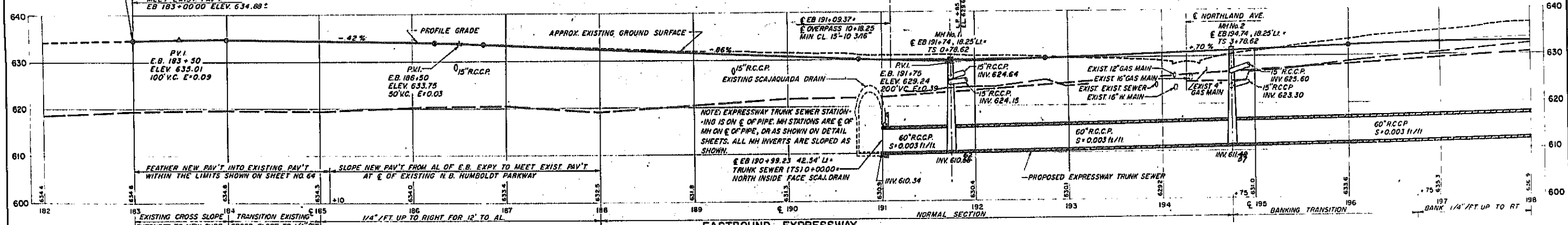
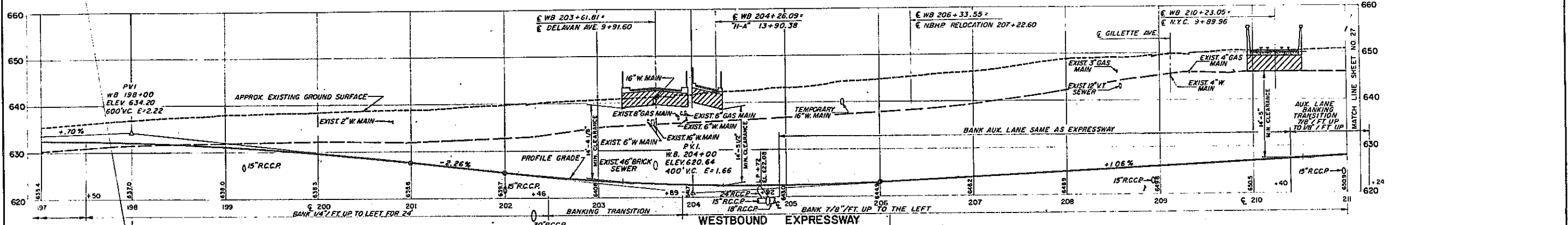
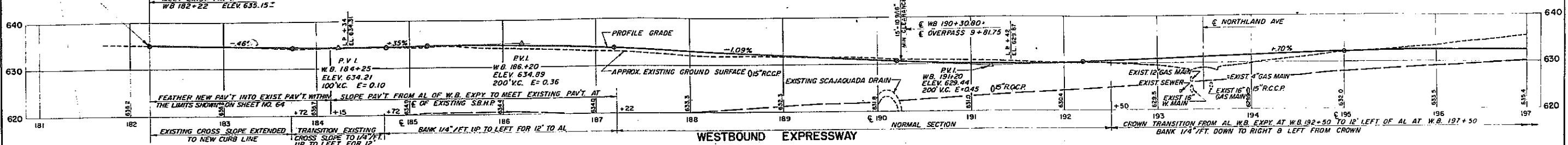
DE LEUW, CATHAR & BRILL	DRAWN	J. M. B.
ENGINEERS-ARCHITECTS	CHECKED	M. M.
NEW YORK	TRACED	T. E. R.



PROJECT AND
NEW PAV'T BEGINS
E.B. 183+00.00
W.B. 182+22

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	26 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE
Datum is U.S.C.G.S. Datum which is
575.454 feet above City Datum

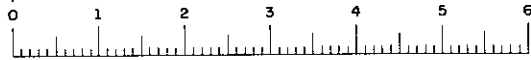
REVISION TO ROCK LINE
SCALE HOR 1" = 50'
VERT 1" = 10'

EXPRESSWAY PROFILES
W.B. § STA. 181+00 TO § STA. 211+00
E.B. § STA. 182+00 TO § STA. 211+00

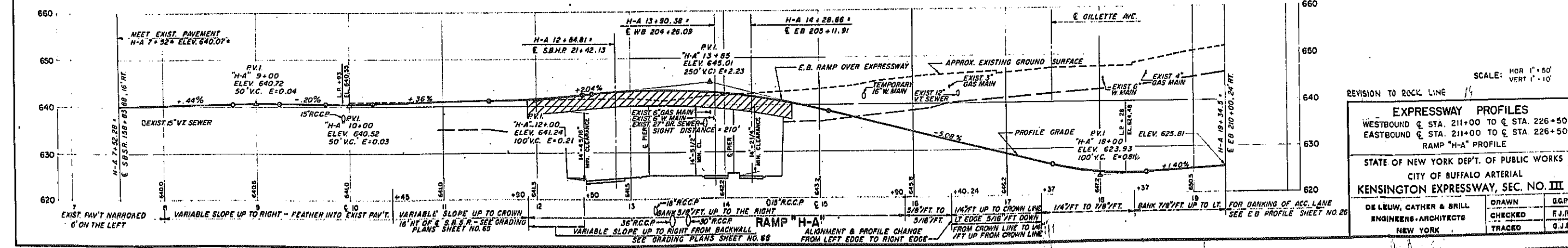
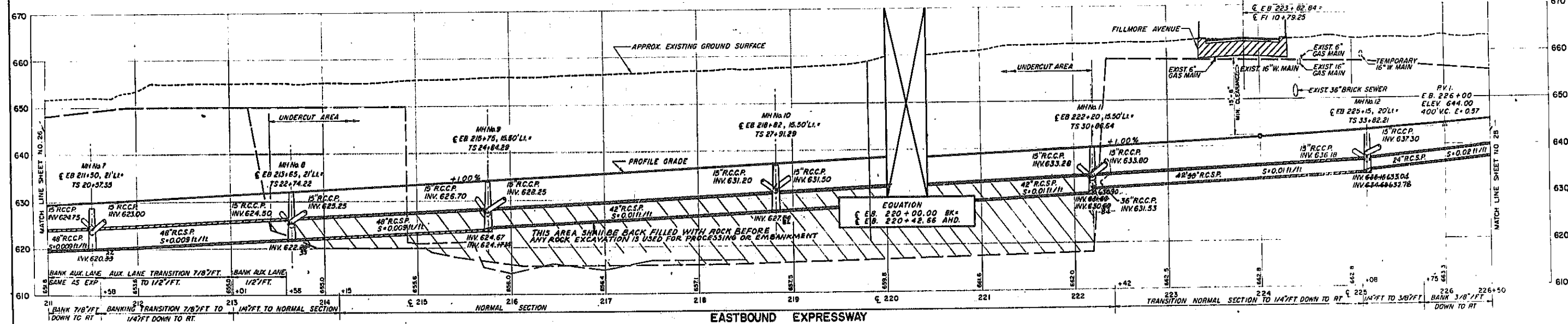
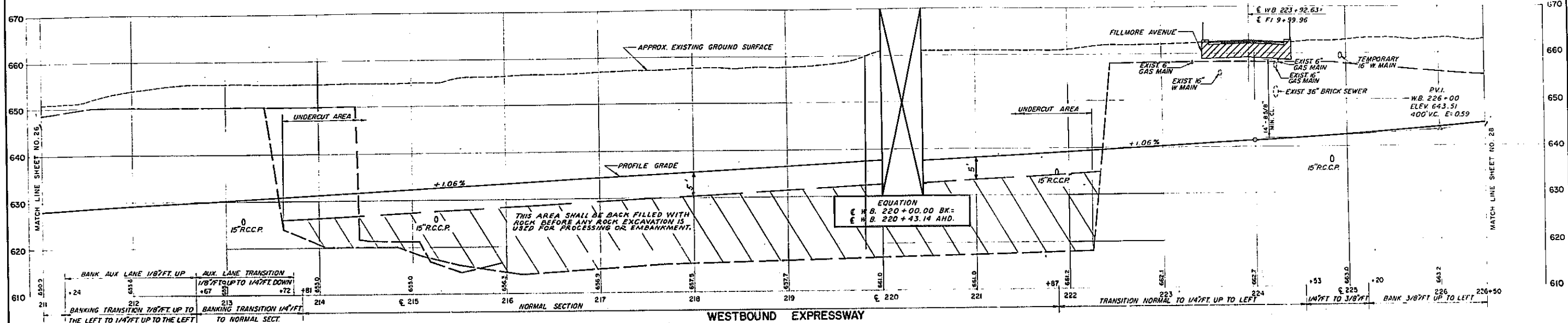
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN G.C.P.
CHECKED F.J.P.
TRACED G.V.P.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	27R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



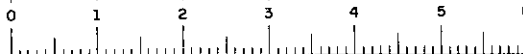
SCALE: HOR 1" = 50'
VERT 1" = 10'

REVISION TO ROCK LINE 1/4

EXPRESSWAY PROFILES
 WESTBOUND & STA. 211+00 TO & STA. 226+50
 EASTBOUND & STA. 211+00 TO & STA. 226+50
 RAMP "H-A" PROFILE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

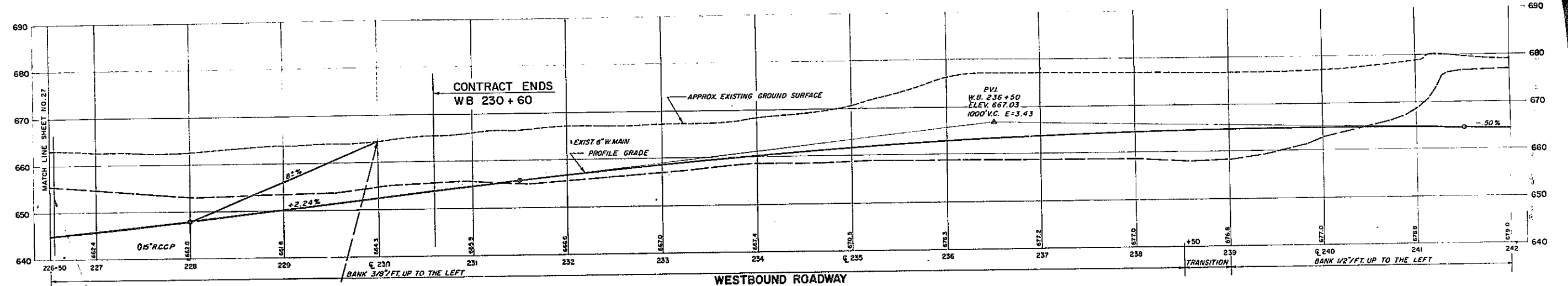
DELEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G.G.P. R.J.P. O.J.B.
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FAE 63-2

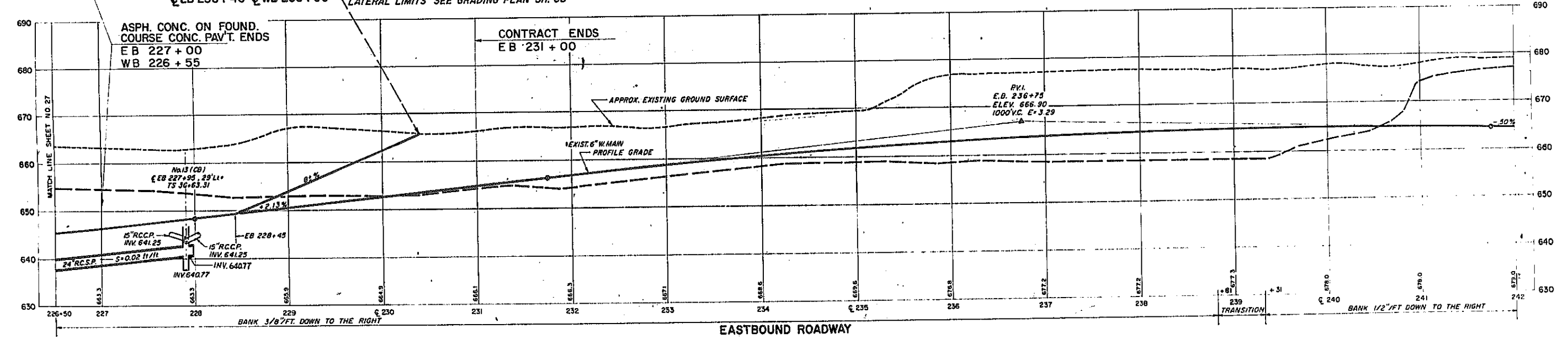
FED. ROAD REB. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	28 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III & IV A
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



GRADING AND PROJECT ENDS
EB 230+40 WB 230+00

LONGITUDINAL TEMPORARY GRADING LIMIT FOR ACCESS. FOR
LATERAL LIMITS SEE GRADING PLAN SH. 68



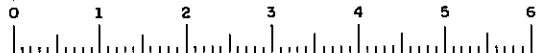
REVISED AS SHOWN

SCALE: HOR. 1" = 50'
VERT. 1" = 10'

EXPRESSWAY PROFILES
WESTBOUND C. STA. 226+50' TO C. STA. 242+00
EASTBOUND C. STA. 226+50 TO C. STA. 242+00

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

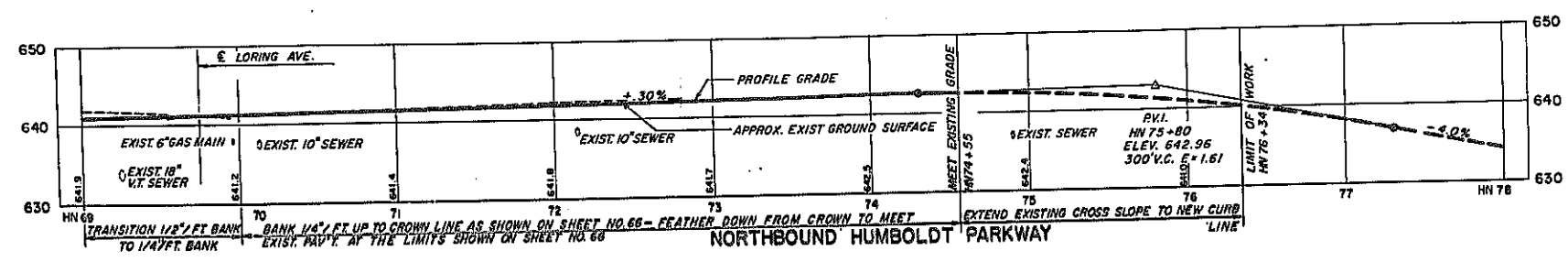
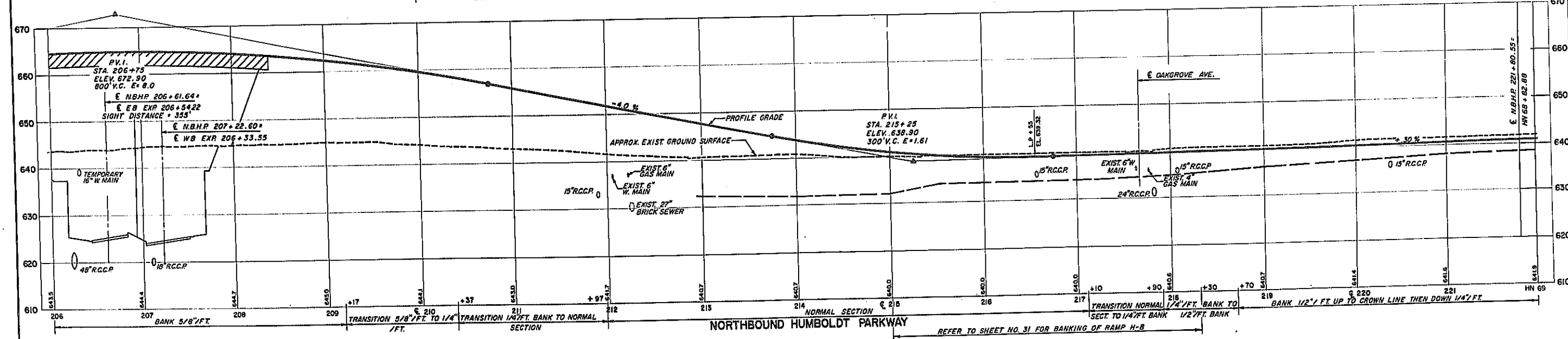
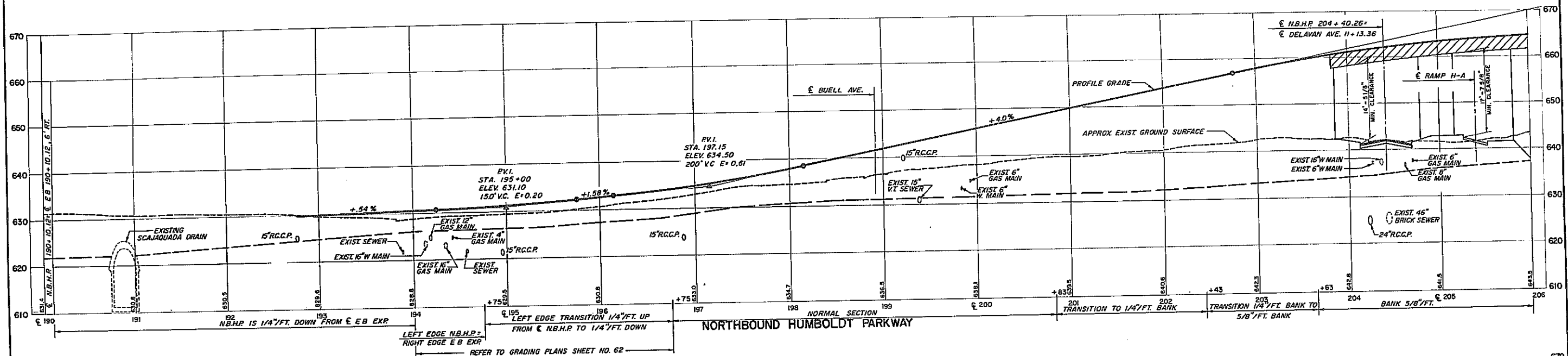
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	O.C.R. F.J.P. G.J.B.
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FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	29	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCALE: HOR. 1" = 50'
VERT. 1" = 10'

PROFILE
NORTHBOUND HUMBOLDT
PARKWAY RELOCATION

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

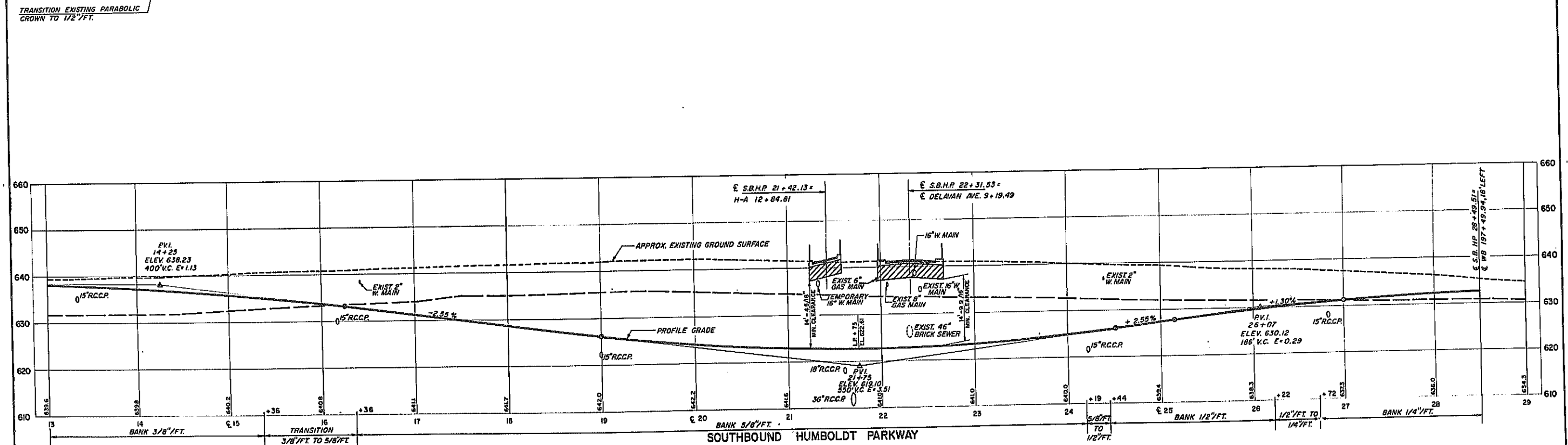
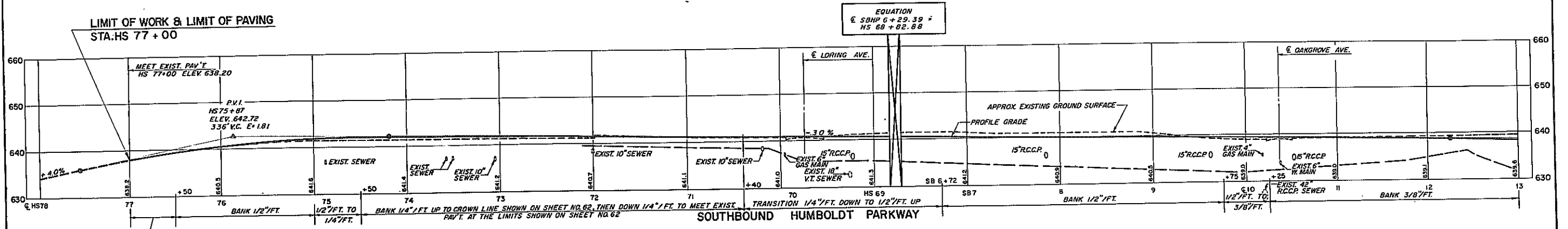
DE LEUW, CATHY & BRILL	DRAWN	G.C.P.
ENGINEERS-ARCHITECTS	CHECKED	M.N.
NEW YORK	TRACED	O.J.B.



FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	30	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

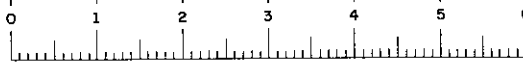


SCALE: HOR. 1" = 80'
VERT. 1" = 10'

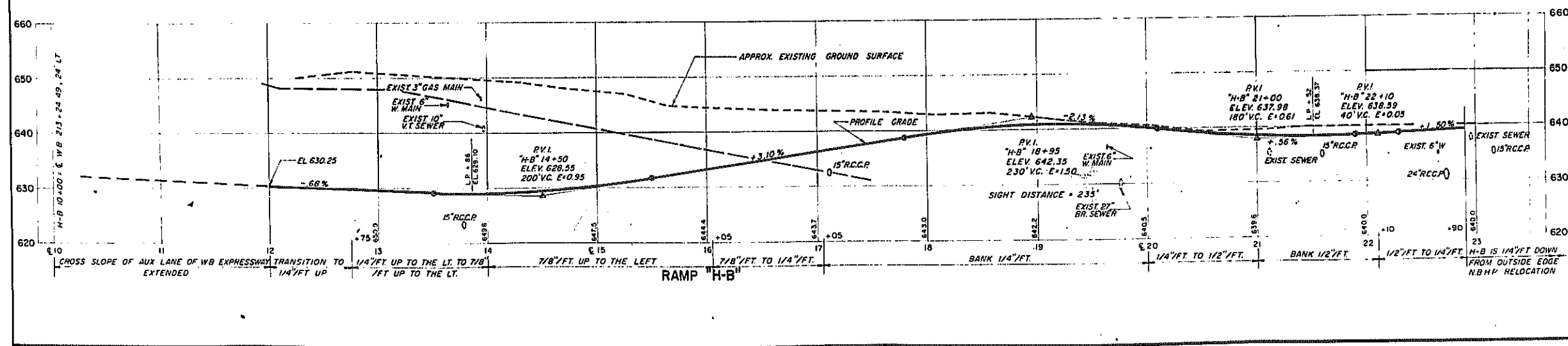
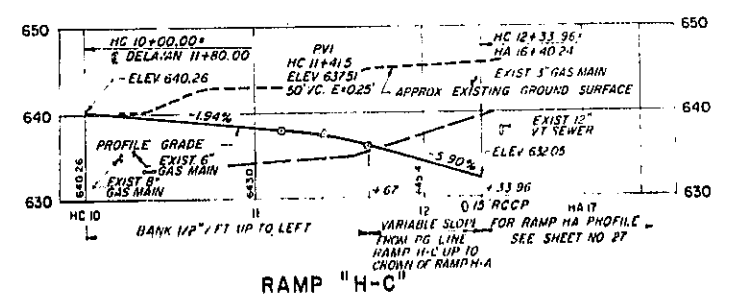
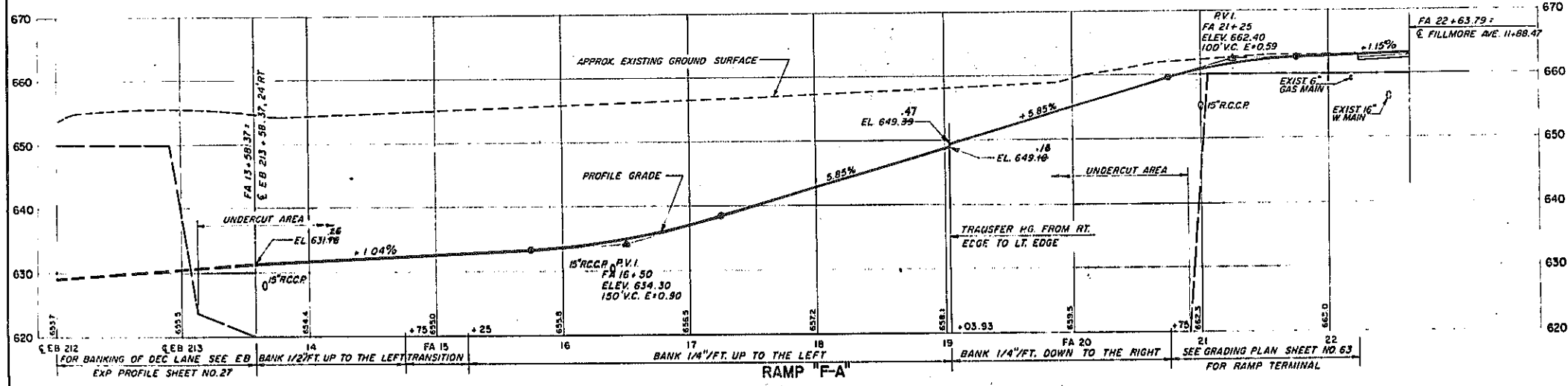
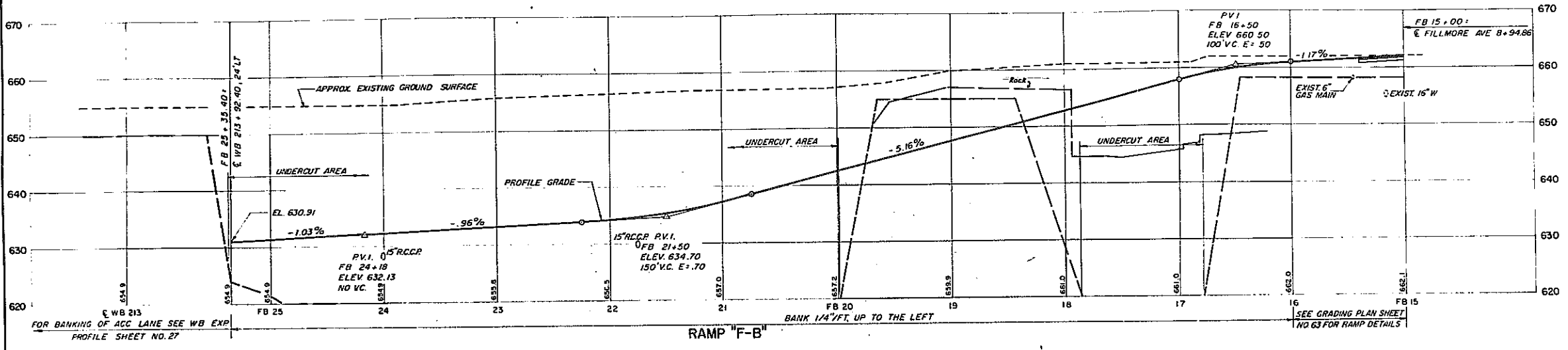
PROFILE
SOUTHBOUND HUMBOLDT
PARKWAY RELOCATION

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G. C. R. M. M. G. J. H.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-37725	31 R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



REVISION TO ROCK LINE
SCALE: HOR 1" = 50'
VERT 1" = 10'

RAMP PROFILES
RAMP "F-A" & "F-B" PROFILE
RAMP "H-B" & "H-C" PROFILE

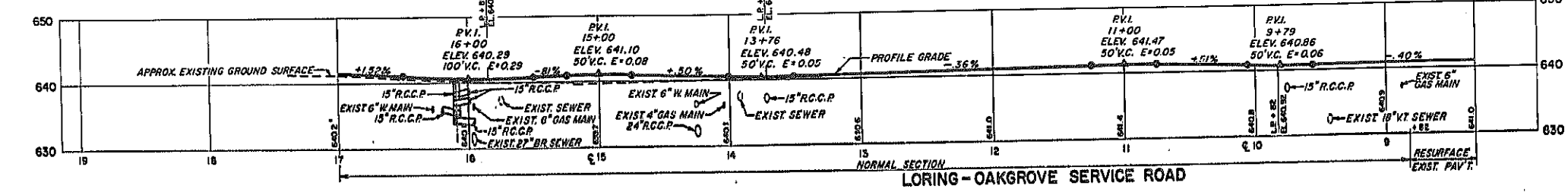
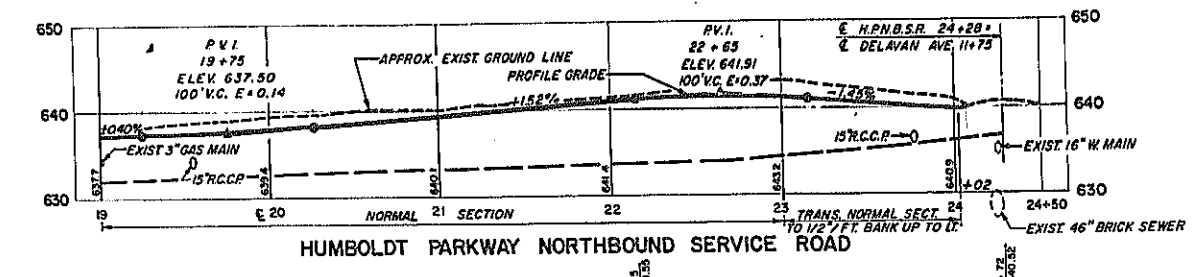
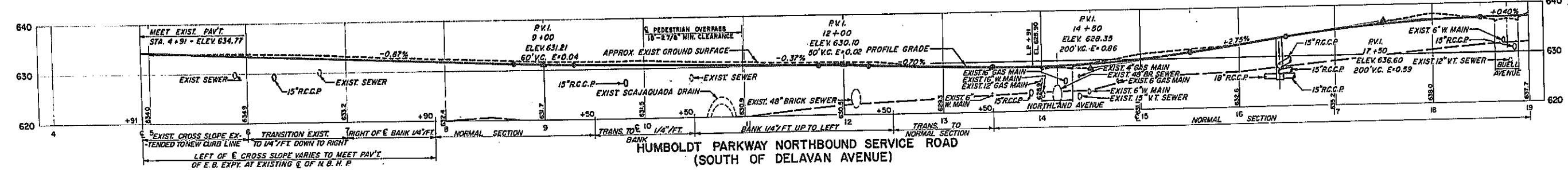
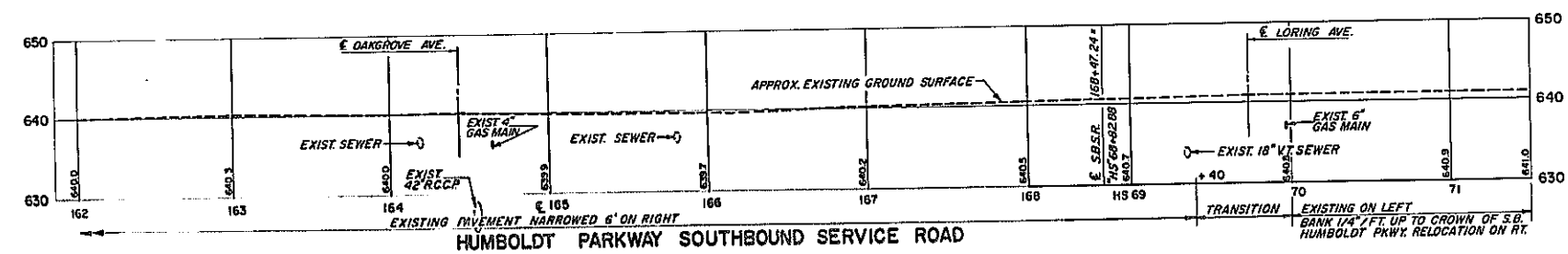
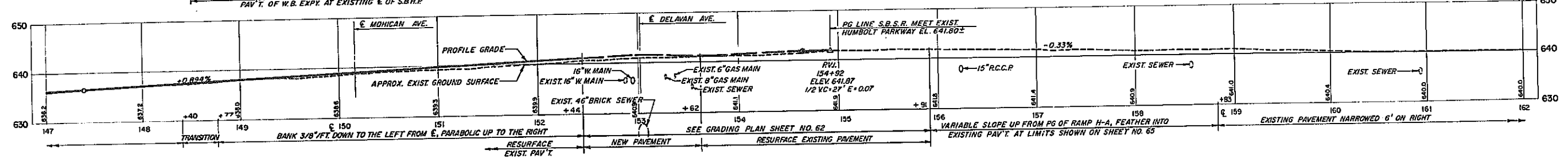
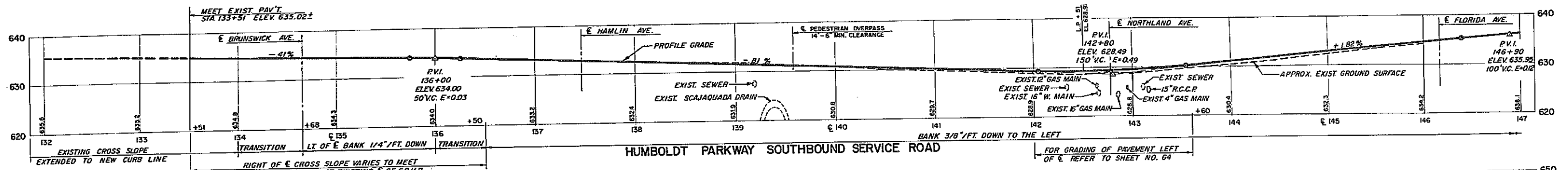
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	O.C.P.
ENGINEERS-ARCHITECTS	CHECKED	M.W.
NEW YORK	TRACED	G.F.B.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	32	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

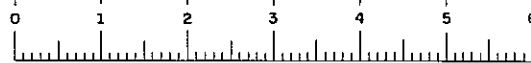


SCALE: HOR. 1"=50'
VERT. 1"=10'

HUMBOLDT PARKWAY SERVICE ROAD PROFILES
SOUTHBOUND S.R. @ STA. 132+00 TO @ STA. 170+00
NORTHBOUND S.R. @ STA. 4+00 TO @ STA. 24+50
LORING-OAKGROVE S.R. @ STA. 19+00 TO @ STA. 170+50

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

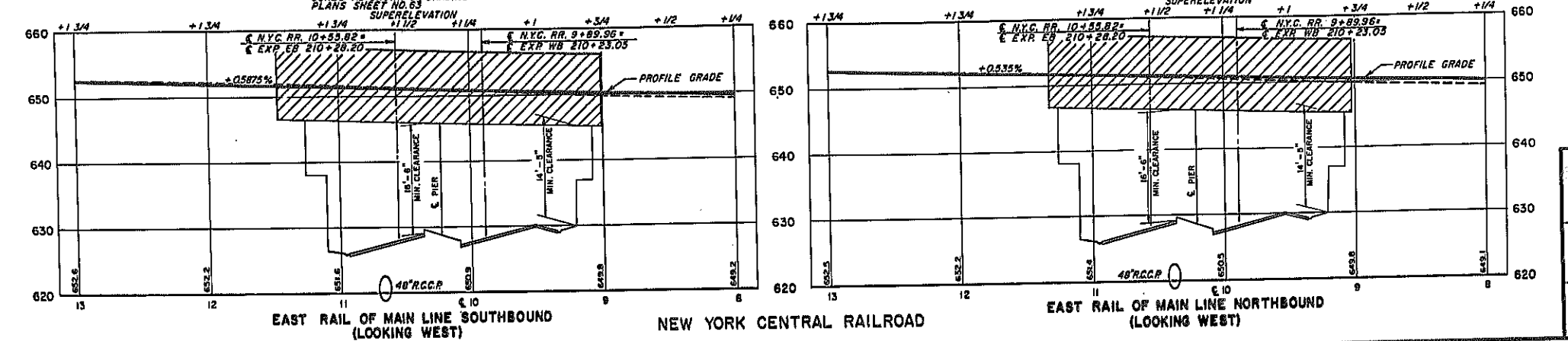
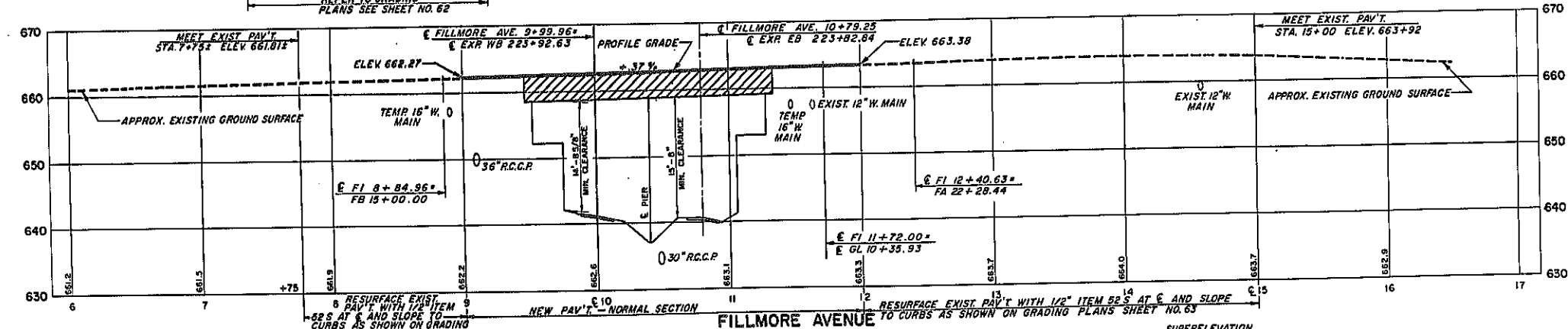
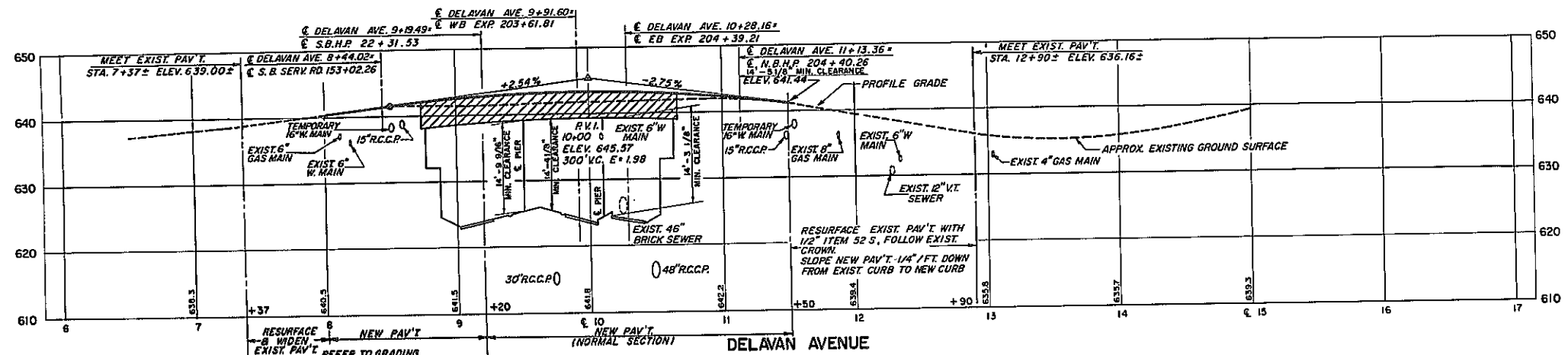
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G.C.R. M.M. G.U.B.
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FAC 63-2

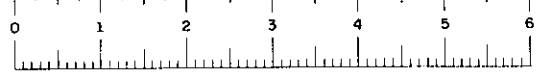
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	33	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



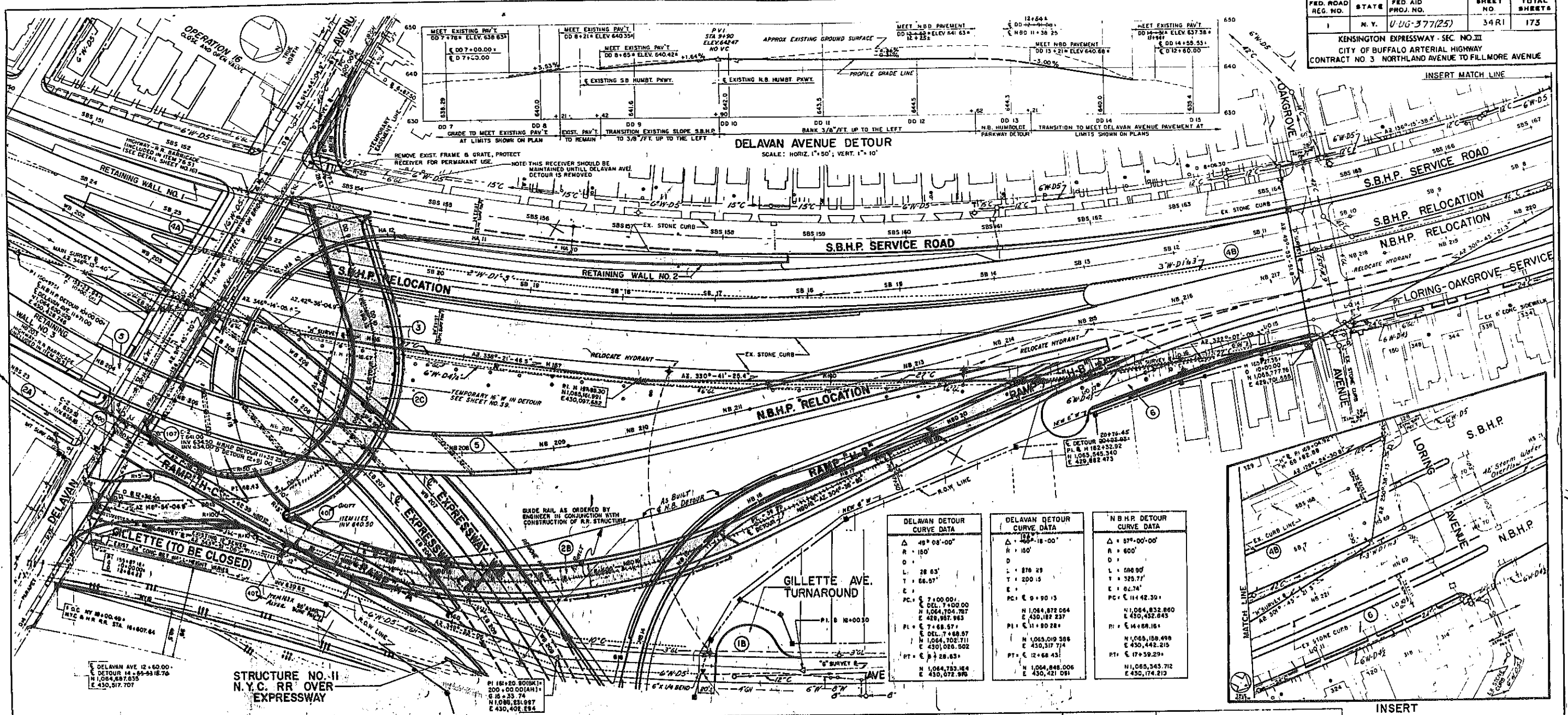
SCALE: HOR. 1" = 50'
 VERT. 1" = 10'

PROFILES							
NORTHLAND AVENUE DELAVAN AVENUE FILLMORE AVENUE NEW YORK CENTRAL R.R.							
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III							
DE LUKW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	<table border="1"> <tr> <td>DRAWN</td> <td>O. C. R.</td> </tr> <tr> <td>CHECKED</td> <td>M. M.</td> </tr> <tr> <td>TRACED</td> <td>O. J. B.</td> </tr> </table>	DRAWN	O. C. R.	CHECKED	M. M.	TRACED	O. J. B.
DRAWN	O. C. R.						
CHECKED	M. M.						
TRACED	O. J. B.						

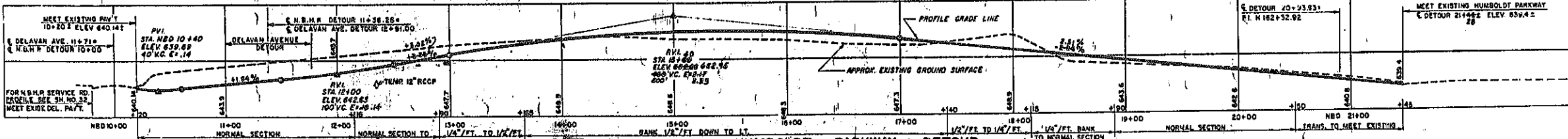


FED. ROAD REG. NO.	STATE	FED AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	UUG-377(25)	34 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



DELAVAN DETOUR CURVE DATA	DELAVAN DETOUR CURVE DATA	N.B.H.P. DETOUR CURVE DATA
Δ = 46° 08' 00"	Δ = 188° 18' 00"	Δ = 87° 00' 00"
R = 150'	R = 150'	R = 600'
D = 120'	D = 120'	D = 120'
L = 28.63'	L = 270.29'	L = 598.00'
T = 66.91'	T = 200.15'	T = 325.77'
E = 7.4'	E = 7.4'	E = 62.74'
PC = 7+00.00'	PC = 0+90.15'	PC = 11+42.30'
DEL. 7+00.00'	N 1,064,872.054	N 1,064,832.800
E 430,867.983	E 430,182.232	E 430,452.043
PI = 7+68.57'	PI = 11+80.28'	PI = 14+68.18'
DEL. 7+68.57'	N 1,065,019.586	N 1,065,168.498
E 430,026.502	E 430,317.714	E 430,542.215
PT = 8+28.63'	PT = 12+66.43'	PT = 17+59.29'
N 1,064,733.184	N 1,064,848.006	N 1,065,343.712
E 430,072.978	E 430,421.091	E 430,174.213



- SUGGESTED PROCEDURE**
- CONSTRUCT E.B. EXPRESSWAY PAVEMENT AND REQUIRED DRAINAGE AND PEDESTRIAN OVERPASS SUBSTRUCTURE WEST OF EXISTING N.B.H.P. IN EAST HALF OF EXISTING MEDIAN OF HUMBOLDT PARKWAY FROM SIDNEY ST. TO STA. E.B. 198+50 AS SHOWN ON SHEET NO. 21. CLOSE BRUNSWICK AND NORTHLAND AVENUES ACROSS HUMBOLDT PARKWAY.
 - CONSTRUCT GILLETTE AVENUE TURNAROUND.
 - DIVERT N.B.H.P. TRAFFIC TO 2A, THENCE BACK TO EXISTING N.B.H.P. AT STA. E.B. 198+50.
 - CONSTRUCT N.B.H.P. SERVICE ROAD FROM SIDNEY ST. TO DELAVAN AVENUE INCLUDING NECESSARY PARTS OF RETAINING WALL NO. 3 AND PEDESTRIAN OVERPASS STRUCTURE.
 - CONSTRUCT TEMPORARY PAVEMENT FOR N.B.H.P. DETOUR FROM DELAVAN AVENUE NORTH TO EXISTING N.B.H.P. AS SHOWN ON PLAN.

- CONSTRUCT DELAVAN AVENUE DETOUR AS SHOWN ON PLAN, EXCEPT FOR THE PORTION CROSSING EXISTING N.B. HUMBOLDT PARKWAY.
- DIVERT N.B.H.P. TRAFFIC TO 2A AND 2B TO EXISTING N.B.H.P. AT BASELINE, STA. H163+30 AND COMPLETE DELAVAN AVENUE DETOUR. DIVERT TRAFFIC TO DELAVAN AVENUE DETOUR.
- CONSTRUCT DELAVAN AVENUE STRUCTURE AND PARTS OF EXPRESSWAY AND RAMP N-A AND N.B.H.P. STRUCTURES.
- CONSTRUCT HUMBOLDT PARKWAY RELOCATIONS NORTH OF DELAVAN AVENUE DETOUR (NORTH OF BASELINE, STA. H163+30, ONLY THAT PORTION WEST OF PRESENT N.B.H.P. MAY BE BUILT).
- DIVERT DELAVAN AVENUE TRAFFIC FROM DETOUR TO COMPLETED DELAVAN AVENUE STRUCTURE.

- COMPLETE CONSTRUCTION OF N.B.H.P. STRUCTURE AND APPROACHES. DIVERT N.B.H.P. TRAFFIC FROM DETOUR TO COMPLETED N.B.H.P. RELOCATION.
- CONSTRUCT LORING-OAKGROVE SERVICE ROAD AND (EAST) 4TH LANE OF N.B.H.P. RELOCATION NORTH OF BASELINE, STA. H163+30.

MAINTENANCE OF TRAFFIC SCHEDULE APPROVED

Edward D. Baxter
EDWARD D. BAXTER
COMM. OF PUBLIC WORKS
CITY OF BUFFALO

- NOTES**
- S.B.H.P. TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION. N.B.H.P., OAKGROVE AND LORING AVENUES SHALL BE MAINTAINED UNTIL N.B.H.P. RELOCATION AND LORING-OAKGROVE SERVICE ROAD (TWO WAY SERVICE) ARE OPERATIVE.
 - DETAILS OF DETOUR ROADWAY SECTIONS ARE SHOWN ON SHEET NO. 3.
 - CONTRACTOR SHALL PROVIDE LIGHTING TO THE AMOUNT OF 0.3 LAMPS PER SQUARE FOOT FOR SIDEWALKS AND ROADWAYS OF DETOURS UNDER ITEM 769.
 - CONTRACTOR SHALL MAINTAIN THE EXISTING DELAVAN AVE. SEWER UNTIL PERMANENT CONNECTION TO THE EXPRESSWAY TRUNK SEWER HAS BEEN MADE.
 - FOR ALIGNMENT DATA FOR COMPLETE IMPROVEMENT OF DELAVAN, N.B.H.P., S.B.H.P. AND EXPRESSWAY, SEE SHEET NO. 20.
 - FOR DETAILS OF TRAFFIC CONTROL SEE SHEET NO. 60. *REV. 10/11/50 TO DETOUR ALIGNMENTS PROFILES*
 - FOR WATER OPERATIONS IN CONNECTION WITH THE DELAVAN AVENUE DETOUR SEE SHEET NO. 30.

INSERT
SOUTHBOUND HUMBOLDT PARKWAY

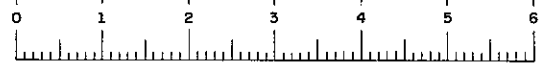
- S.B.H.P. TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
- THE WIDENING OF S.B.H.P. SERVICE RD BETWEEN STA. S.B.S. 143+00 TO S.B.S. 150+00 SHALL BE PERFORMED PRIOR TO EXCAVATING THE BACK SLOPE FOR RETAINING WALL NO. 1, IN ORDER TO MAINTAIN 2-12' LANES DURING CONSTRUCTION.
- PAVING AT S.B.H.P. SERVICE RD - DELAVAN AVE INTERSECTION SHALL BE CONSTRUCTED IN TWO SECTIONS TO MAINTAIN TRAFFIC ON S.B.H.P. SERVICE RD.
- TEMPORARY GUIDE RAILINGS WILL BE ERECTED ON S.B.H.P. SERVICE RD DURING CONSTRUCTION OF RETAINING WALLS NO. 1 & 2 (SEE SHEET NO. 13 FOR STATIONS).

SCALE: 1" = 50'

DELAVAN AVENUE AND N.B.H.P. DETOURS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

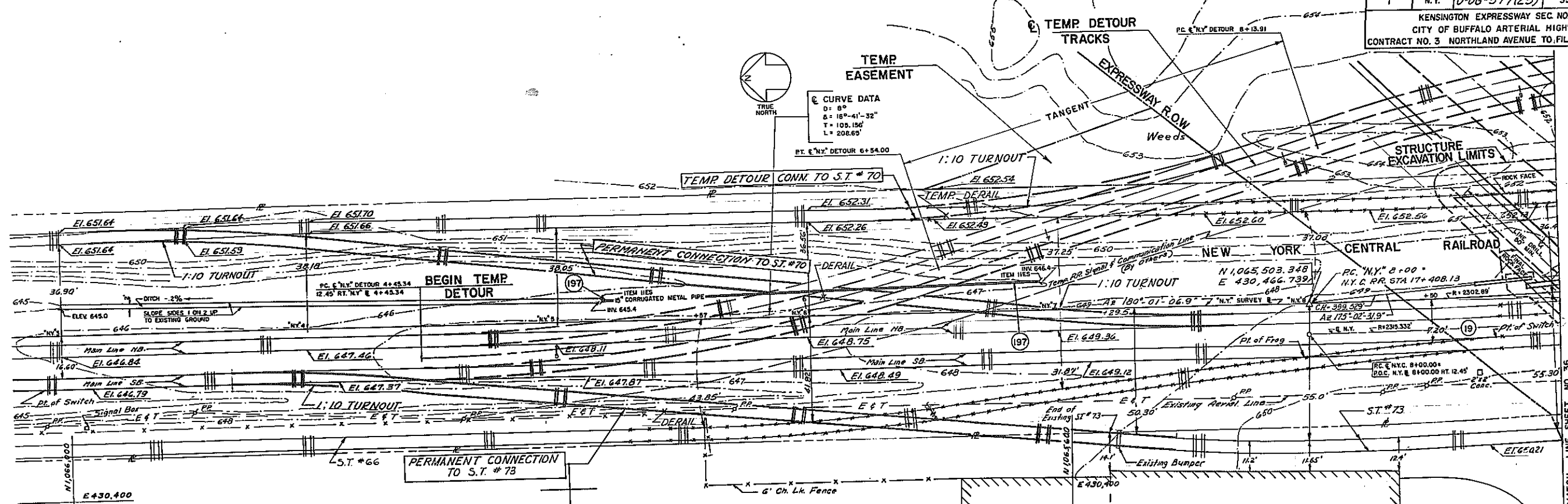
DE LEUW, CATHY & BRILL	DRAWN	T.C.H.
ENGINEERS-ARCHITECTS	CHECKED	J.M.W.
NEW YORK	TRACED	J.M.W.



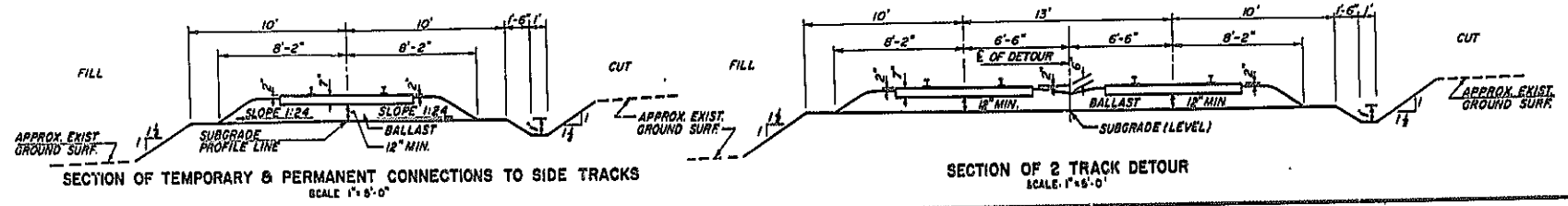
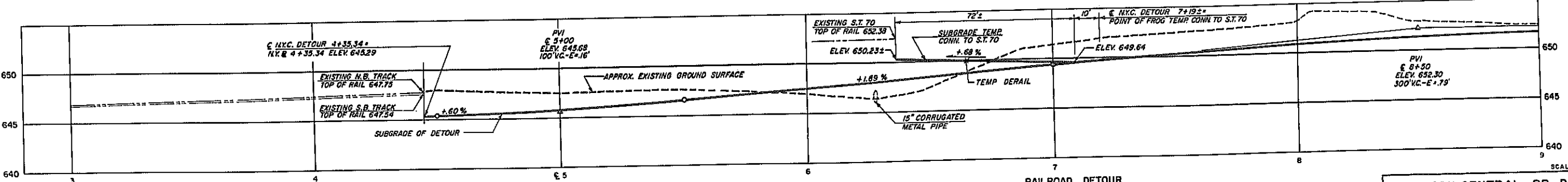
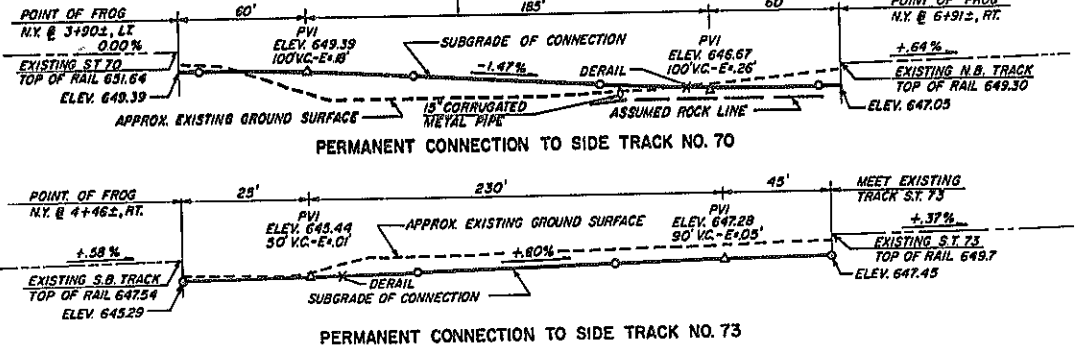
FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	35	173

KENSINGTON EXPRESSWAY SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



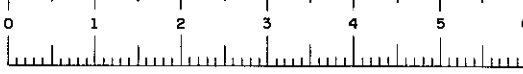
NOTE:
ELEVATIONS SHOWN ON PLAN ARE AT TOP OF EXISTING RAIL.
FOR TRACK LEGEND SEE SHEET 38.
FOR PERMANENT NB AND SB TRACK PROFILES SEE SHEET 33.B.133.
FOR DETAILS OF STRUCTURE SEE SHEETS 132-147.
FOR TIEB SEC SHEET 22.0.24.
FOR PROJECT BENCHMARKS SEE SHEET 12.
FOR TURNOUT DETAILS REFER TO "N.Y.C. SYSTEM ENGINEERING AND MAINTENANCE OF WAY PLANS AND DATA FOR FIELD USE".
FOR ALIGNMENT AND CURVE DATA SEE SHEET 20.



**NEW YORK CENTRAL RR DETOUR
STA. 3+00 TO STA. 9+00**

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY SEC. NO. III

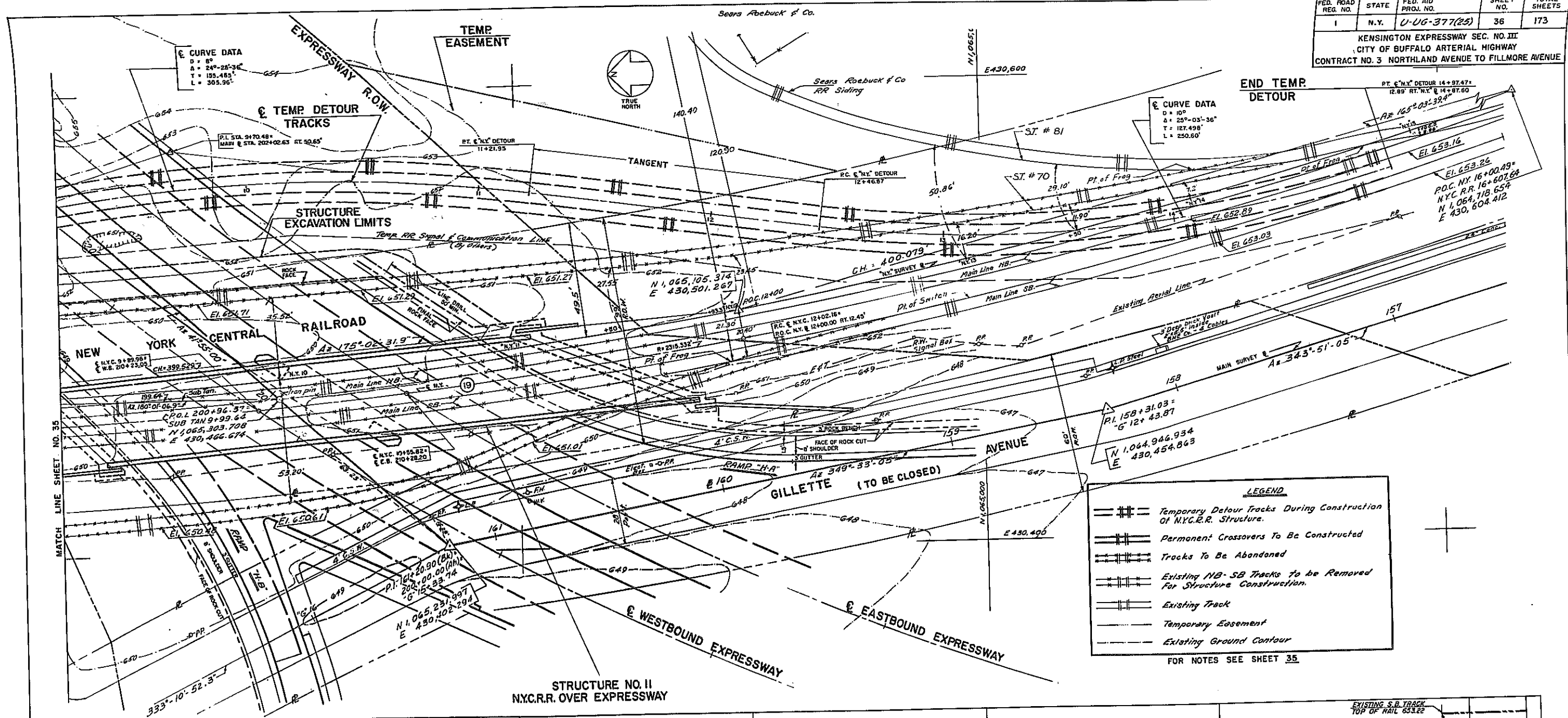
DE LEUW, CATHAR & BRILL	DRAWN
ENGINEERS - ARCHITECTS	CHECKED
NEW YORK	TRACED



FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	36	173

KENSINGTON EXPRESSWAY SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



CURVE DATA
D = 8°
A = 24°-28'-34"
T = 155.481'
L = 303.96'

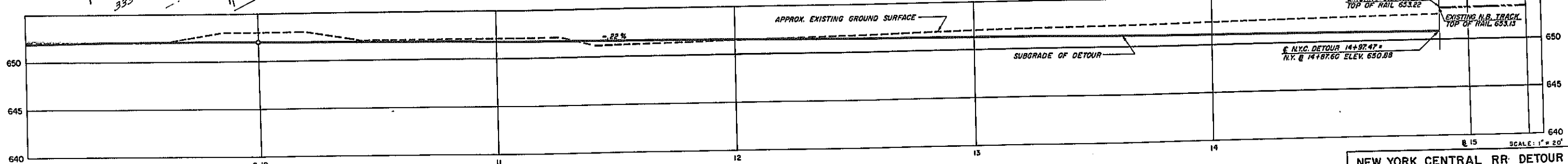
CURVE DATA
D = 10°
A = 25°-03'-36"
T = 127.498'
L = 250.60'

LEGEND

- #---#--- Temporary Detour Tracks During Construction of NYC.R.R. Structure.
- ==== Permanent Crossovers To Be Constructed
- #---#--- Tracks To Be Abandoned
- #---#--- Existing NB-SB Tracks to be Removed for Structure Construction.
- Existing Track
- - - - - Temporary Easement
- - - - - Existing Ground Contour

FOR NOTES SEE SHEET 35

**STRUCTURE NO. II
NYC.R.R. OVER EXPRESSWAY**

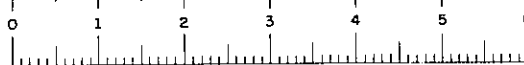


RAILROAD DETOUR
SCALE HORIZ: 1" = 20'
VERT: 1" = 8'

**NEW YORK CENTRAL RR DETOUR
STA. 9+00 TO STA. 15+00**

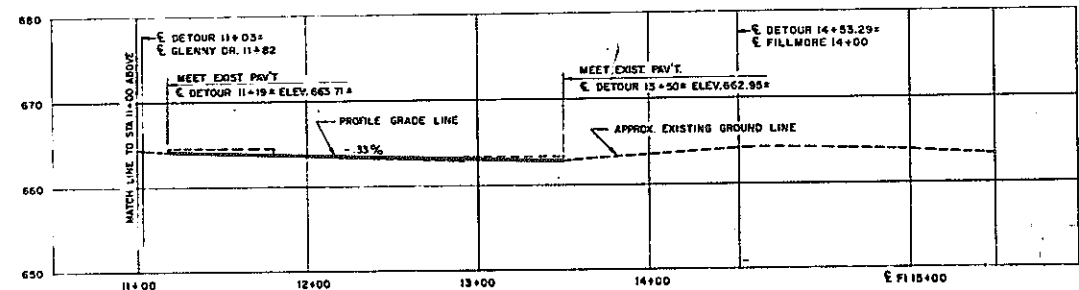
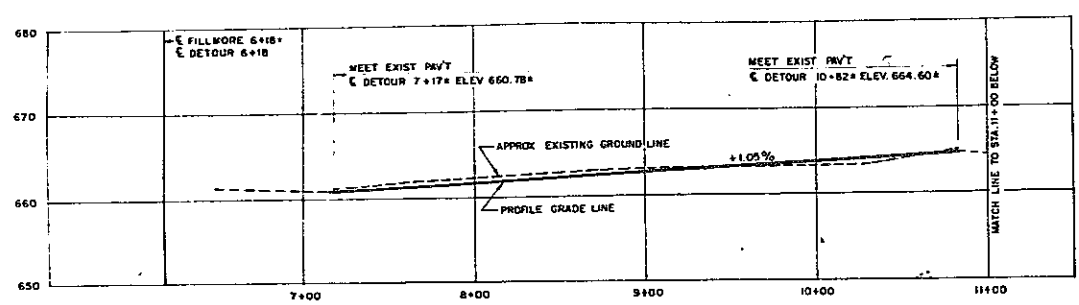
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY SEC. NO. III

DE LEW, GATHER & BRILL ENGINEERS - ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.F.L. M.M. G.B.
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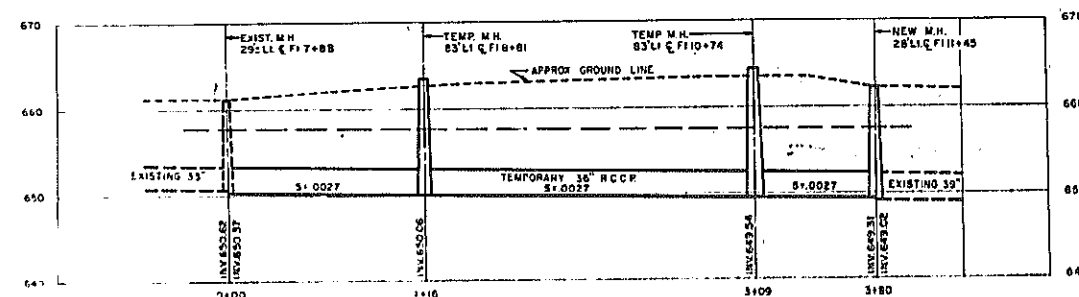


FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	37R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



FILLMORE AVE. DETOUR PROFILE
HOR. 1" = 50'
VERT. 1" = 10'



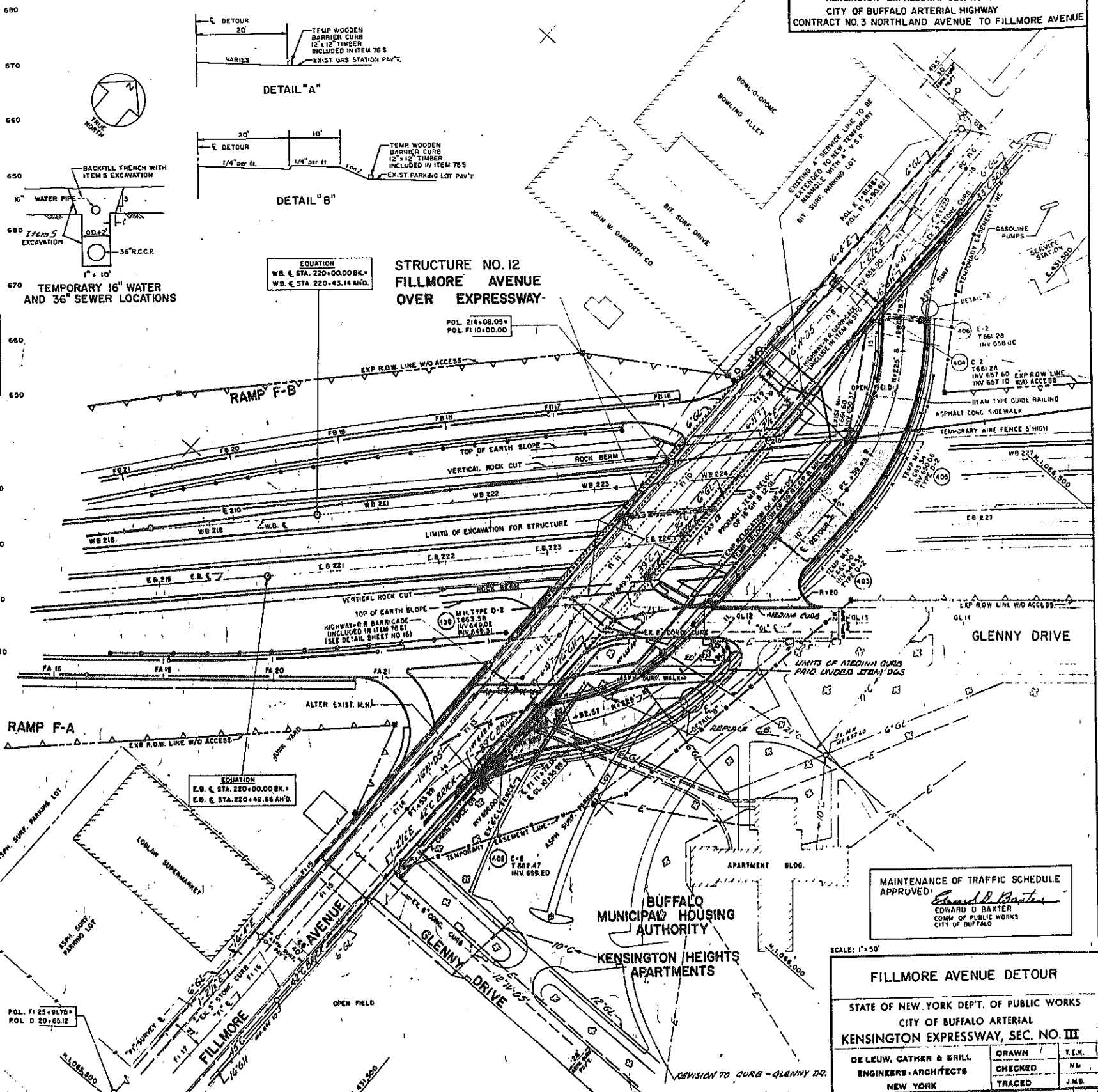
TEMPORARY 36" SEWER
HOR. 1" = 50'
VERT. 1" = 10'

NOTES

- Details of typical section for detour is shown on sheet B.
- Contractor shall provide lighting to the amount of 0.5 lumens per square foot for sidewalks and roadway of the detour under item 765.
- Contractor shall maintain existing Fillmore Ave sewer until temporary sewer and permanent connection to trunk sewer has been made.
- For permanent location of Fillmore Ave sewer see sheet A1.
- All existing curb, sidewalk, fence, paved areas disturbed by detour construction are to be replaced under appropriate items.
- Provide temporary sewer service for John W. Danforth Co. by extending the existing sewer service to temp. M.H. 83' LI. at PI 8+81.
- Upon completion of temporary 36" sewer in detour, plug existing 36" sewer at the south invert of existing M.H. at PI 7+88, and the north invert of new M.H. at PI 11+45.
- Upon completion of permanent Fillmore Ave sewer to Expressway trunk sewer, unplug existing 36" sewer at M.H. at PI 7+88 and plug temporary 36" sewer at M.H. at PI 7+88 and PI 11+45.
- For legend see sheet 19.

FILLMORE AVE DETOUR CURVE DATA

Δ	40°-55'-34"
R	225'
T	83.96'
L	160.72'
E	16.15'
PC	DETOUR 8+18.4
	FILLMORE 6+18
PIC	DETOUR 7+76.72
	DETOUR 9+39.43
PT	FILLMORE 9+12.79-10'LL
RC	DETOUR 11+31.06
	FILLMORE 11+05.21-10'LL
PRC	DETOUR 12+92.57
RT	DETOUR 14+53.29
	FILLMORE 14+00



STRUCTURE NO. 12
FILLMORE AVENUE
OVER EXPRESSWAY

MAINTENANCE OF TRAFFIC SCHEDULE APPROVED
Edward D. Baxter
EDWARD D. BAXTER
COMM. OF PUBLIC WORKS
CITY OF BUFFALO

SCALE: 1" = 50'

FILLMORE AVENUE DETOUR

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. M.W. J.N.S.
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GENERAL NOTES - WATER

The City of Buffalo, Division of Water, will furnish at the site all pipe materials and labor to make connections to existing mains at "OPERATIONS" shown on the plans, the cost of materials and labor to be paid by the New York State Department of Public Works to the Division of Water. All water services that are to be discontinued shall be abandoned at the water main. All excavating, pumping, back-filling and resurfacing in connection with this work and of "OPERATIONS" by the Division of Water shall be done by the contractor. No separate payment will be made for temporary resurfacing or pumping. Necessary excavation will be paid for under ITEM 5.

The following work to be done and materials required to be furnished by the Contractor.

Hydrants to be relocated are to be removed and installed in new locations.

Adjust all hydrants, valve boxes and covers and manhole frames and covers to new grades.

Abandoned valve boxes shall be pulled, or top section broken off and filled in with concrete and area resurfaced.

All abandoned pipes, valves and hydrants and other appurtenances, which are not used in the new work are to remain the property of the City of Buffalo unless otherwise specified, and will be recovered by the Division of Water.

Any exposed openings created in the abandoned pipe left in place shall be plugged with brick and cement to a depth of 12" in such a manner that no material can enter. Payment will be made under Item 102.00 & 15-2A.

Contractor to excavate where ordered by the engineer to verify location of existing water lines.

Concrete Bracing and Anchor Straps shall be installed at all Bends and Plugs, including "OPERATIONS" performed by the City of Buffalo Division of Water.

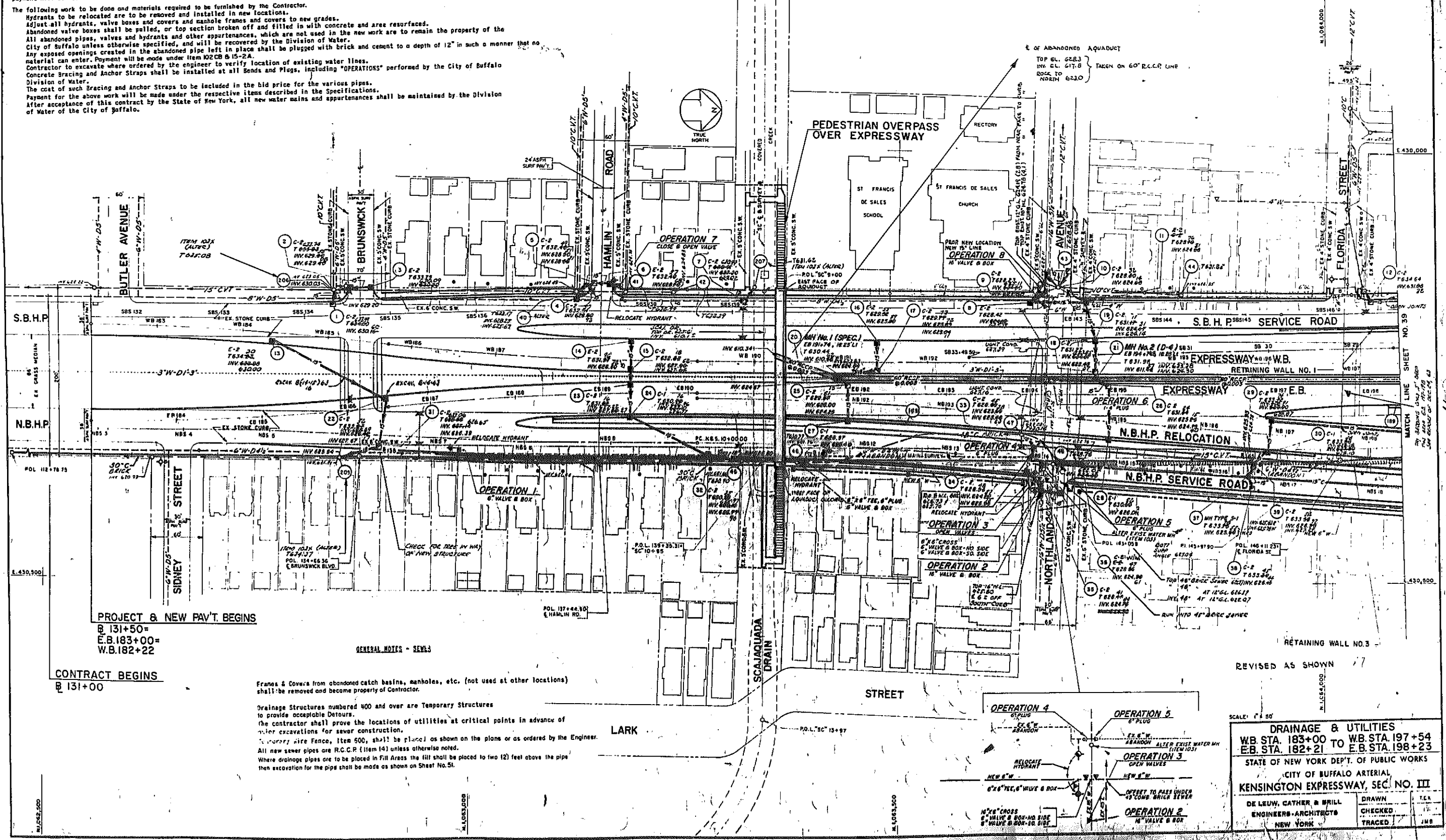
The cost of such Bracing and Anchor Straps to be included in the bid price for the various pipes.

Payment for the above work will be made under the respective items described in the Specifications.

After acceptance of this contract by the State of New York, all new water mains and appurtenances shall be maintained by the Division of Water of the City of Buffalo.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-577(25)	38RI	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PROJECT & NEW PAV'T BEGINS
@ 131+50=
E.B. 183+00=
W.B. 182+22

CONTRACT BEGINS
@ 131+00

GENERAL NOTES - SEWER

Frames & Covers from abandoned catch basins, manholes, etc. (not used at other locations) shall be removed and become property of Contractor.

Drainage Structures numbered 400 and over are Temporary Structures to provide acceptable Detours.

The contractor shall prove the locations of utilities at critical points in advance of water excavations for sewer construction.

Temporary Wire Fence, Item 500, shall be placed as shown on the plans or as ordered by the Engineer.

All new sewer pipes are R.C.C.P. (Item 14) unless otherwise noted.

Where drainage pipes are to be placed in Fill Areas the fill shall be placed to two (2) feet above the pipe then excavation for the pipe shall be made as shown on Sheet No. 51.

SCALE: 1" = 50'

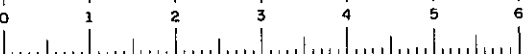
DRAINAGE & UTILITIES
WB STA. 183+00 TO WB STA. 197+54
E.B. STA. 182+21 TO E.B. STA. 198+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHEN & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

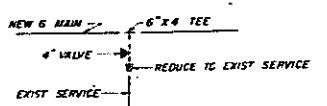
DRAWN: T.C.A.
CHECKED: J.M.B.
TRACED: J.M.B.

REVISED AS SHOWN



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	39R1	173

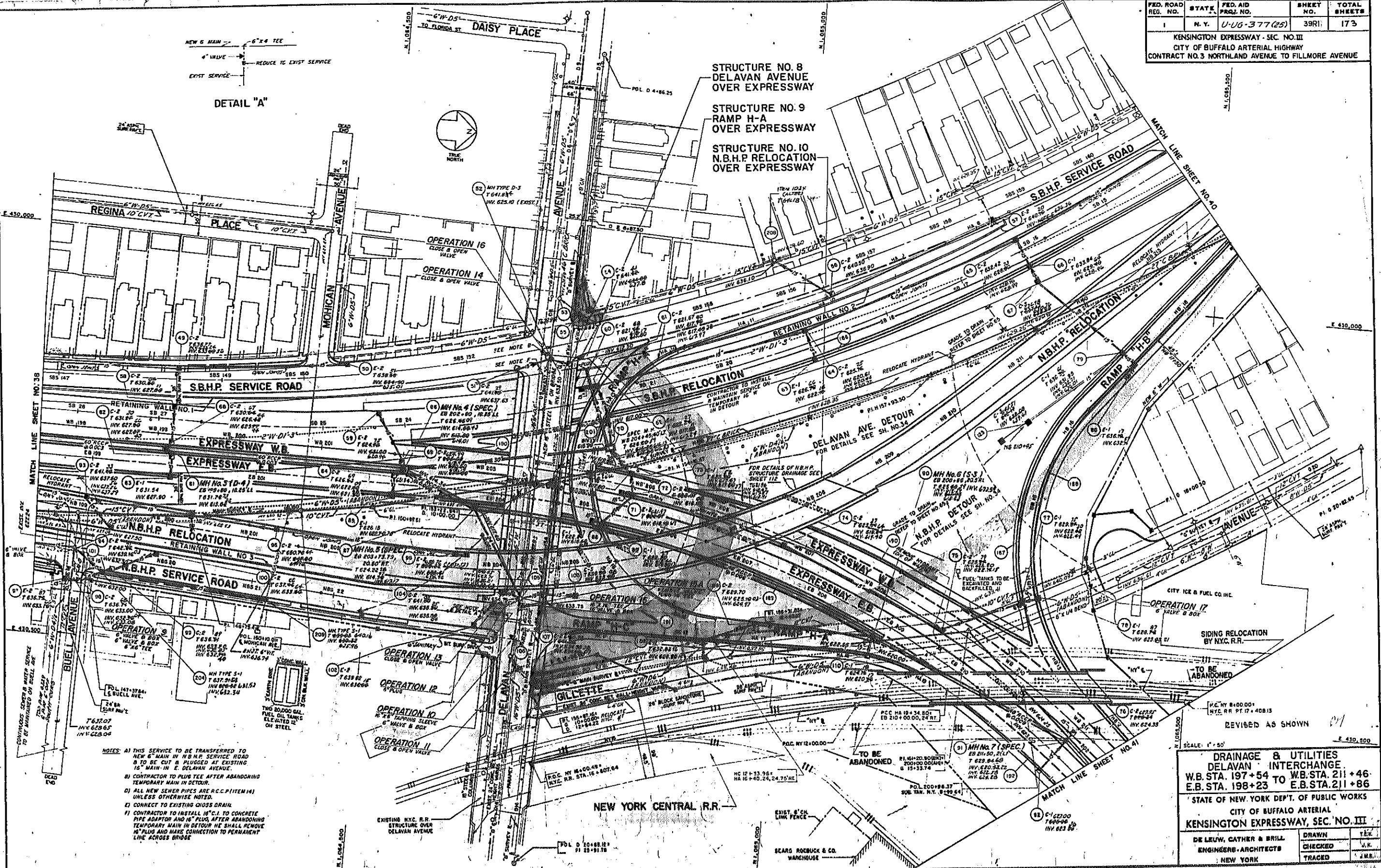
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



STRUCTURE NO. 8
DELANAV AVENUE
OVER EXPRESSWAY

STRUCTURE NO. 9
RAMP H-A
OVER EXPRESSWAY

STRUCTURE NO. 10
N.B.H.P. RELOCATION
OVER EXPRESSWAY



- NOTES:
- THIS SERVICE TO BE TRANSFERRED TO NEW 6" MAIN IN N.B.H.P. SERVICE ROAD TO BE CUT & PLUGGED AT EXISTING 16" MAIN IN E. DELAVAN AVENUE.
 - CONTRACTOR TO PLUS TEE AFTER ABANDONING TEMPORARY MAIN IN DETOUR.
 - ALL NEW SEWER PIPES ARE R.C.C. (ITEM 14) UNLESS OTHERWISE NOTED.
 - CONNECT TO EXISTING CROSS DRAIN.
 - CONTRACTOR TO INSTALL 18" C.I. TO CONCRETE PIPE ADAPTOR AND 18" PLUG, AFTER ABANDONING TEMPORARY MAIN IN DETOUR HE SHALL REMOVE 18" PLUG AND MAKE CONNECTION TO PERMANENT LINE ACROSS BRIDGE.

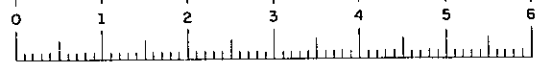
SCALE: 1" = 50'

REVISED AS SHOWN

**DRAINAGE & UTILITIES
DELANAV INTERCHANGE**
W.B. STA. 197+54 TO W.B. STA. 211+46
E.B. STA. 198+23 E.B. STA. 211+86

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

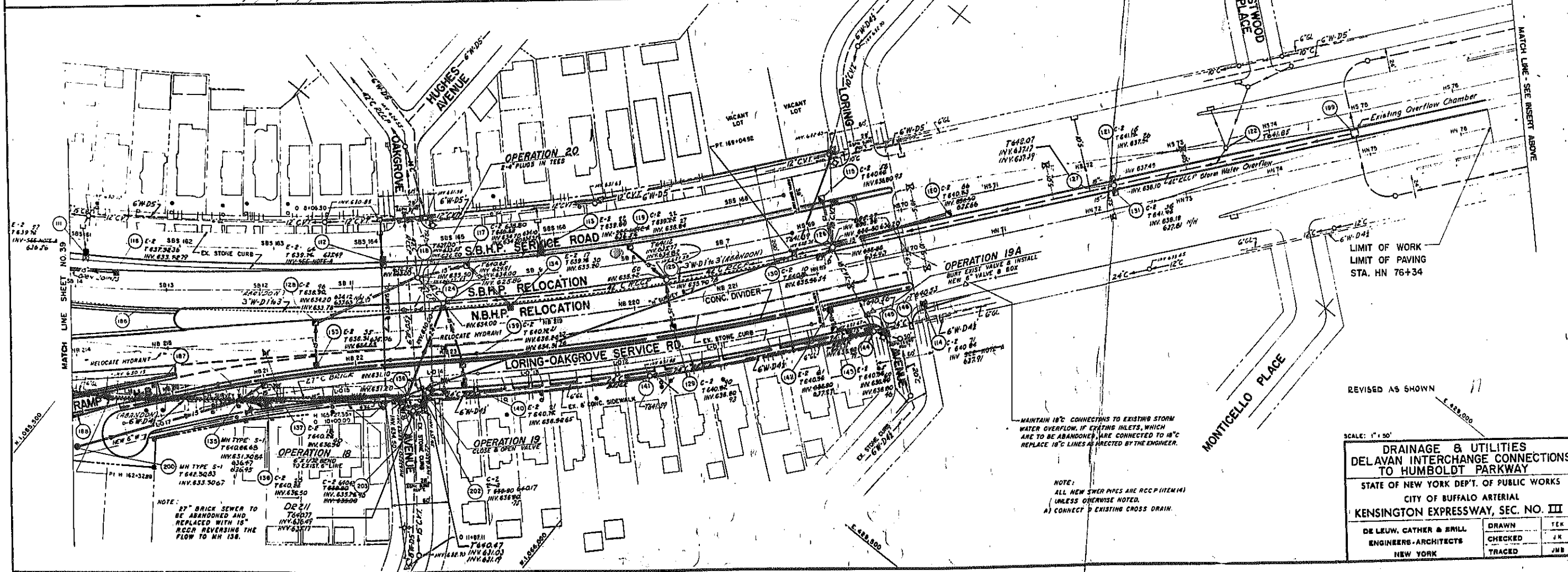
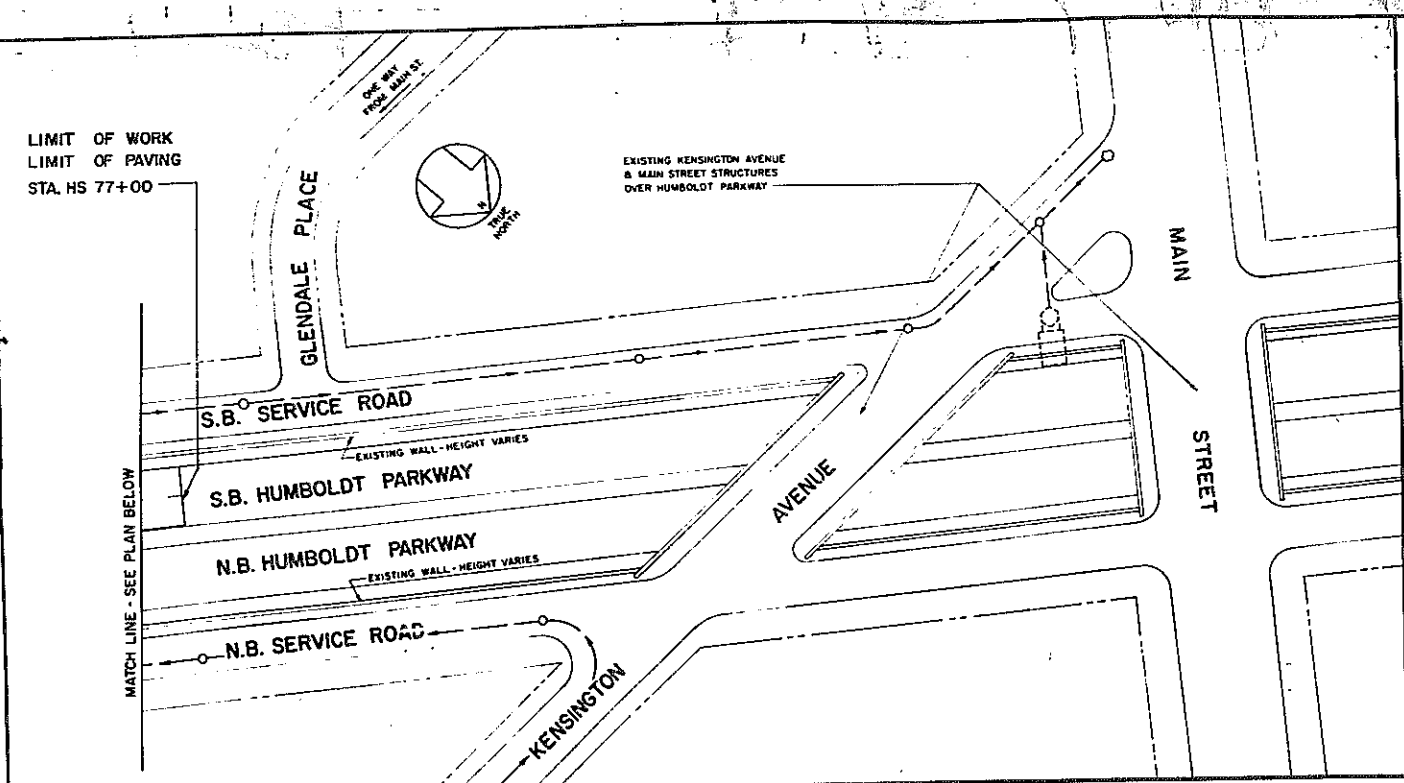
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. J.K. J.M.B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	40RL	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

LIMIT OF WORK
 LIMIT OF PAVING
 STA. HS 77+00



LIMIT OF WORK
 LIMIT OF PAVING
 STA. HN 76+34

NOTE:
 ALL NEW SWER PIPES ARE RCCP (ITEM 14)
 UNLESS OTHERWISE NOTED.
 A) CONNECT TO EXISTING CROSS DRAIN.

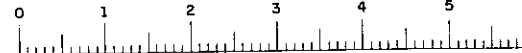
REVISED AS SHOWN

SCALE: 1" = 50'

**DRAINAGE & UTILITIES
 DELAVAN INTERCHANGE CONNECTIONS
 TO HUMBOLDT PARKWAY**

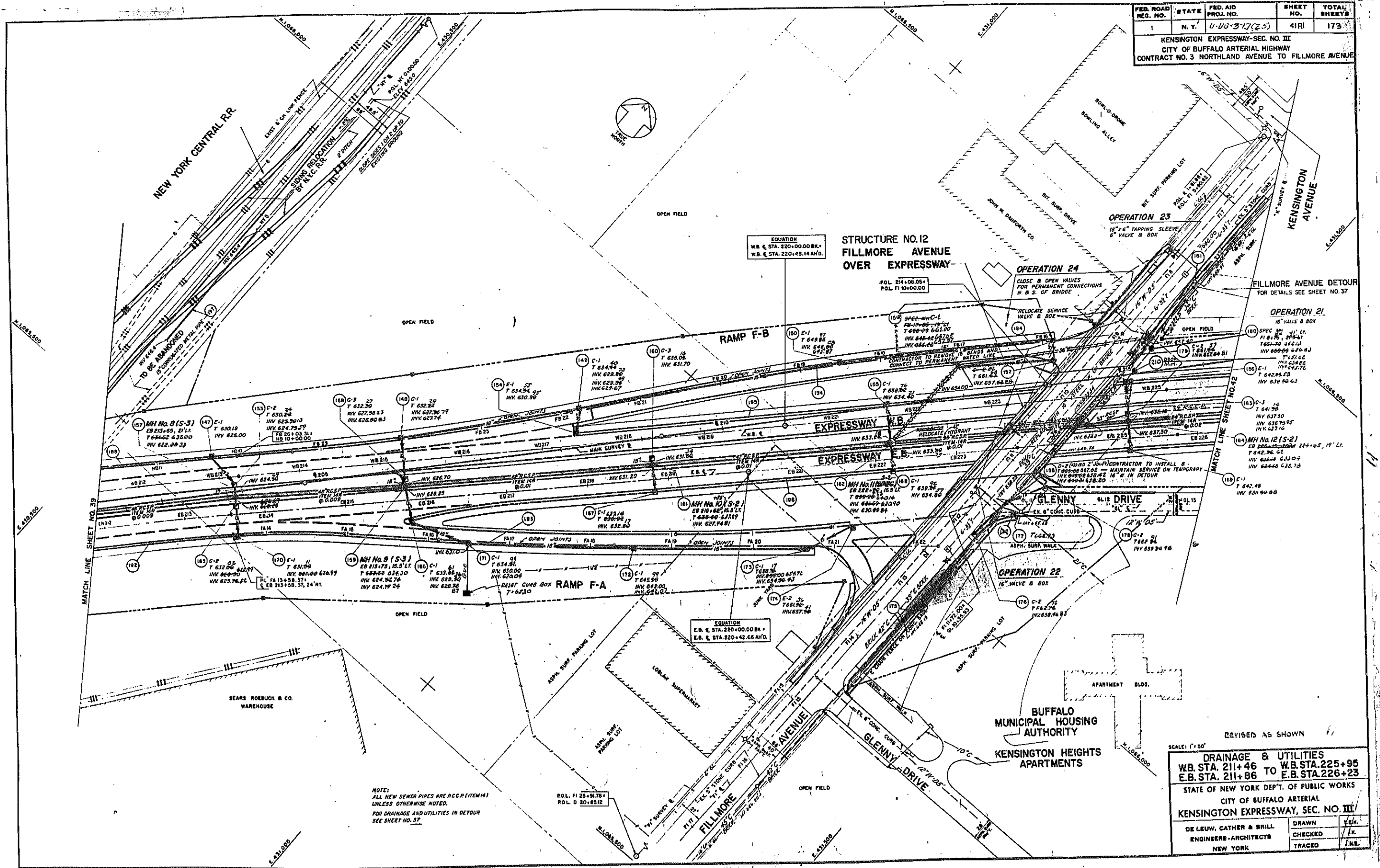
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TEK JK JMB
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	41R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

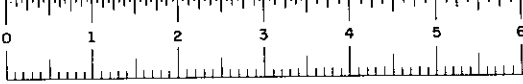


NOTE:
ALL NEW SEWER PIPES ARE ACCR. ITEM#14
UNLESS OTHERWISE NOTED.
FOR DRAINAGE AND UTILITIES IN DETOUR
SEE SHEET NO. 37.

REVISED AS SHOWN

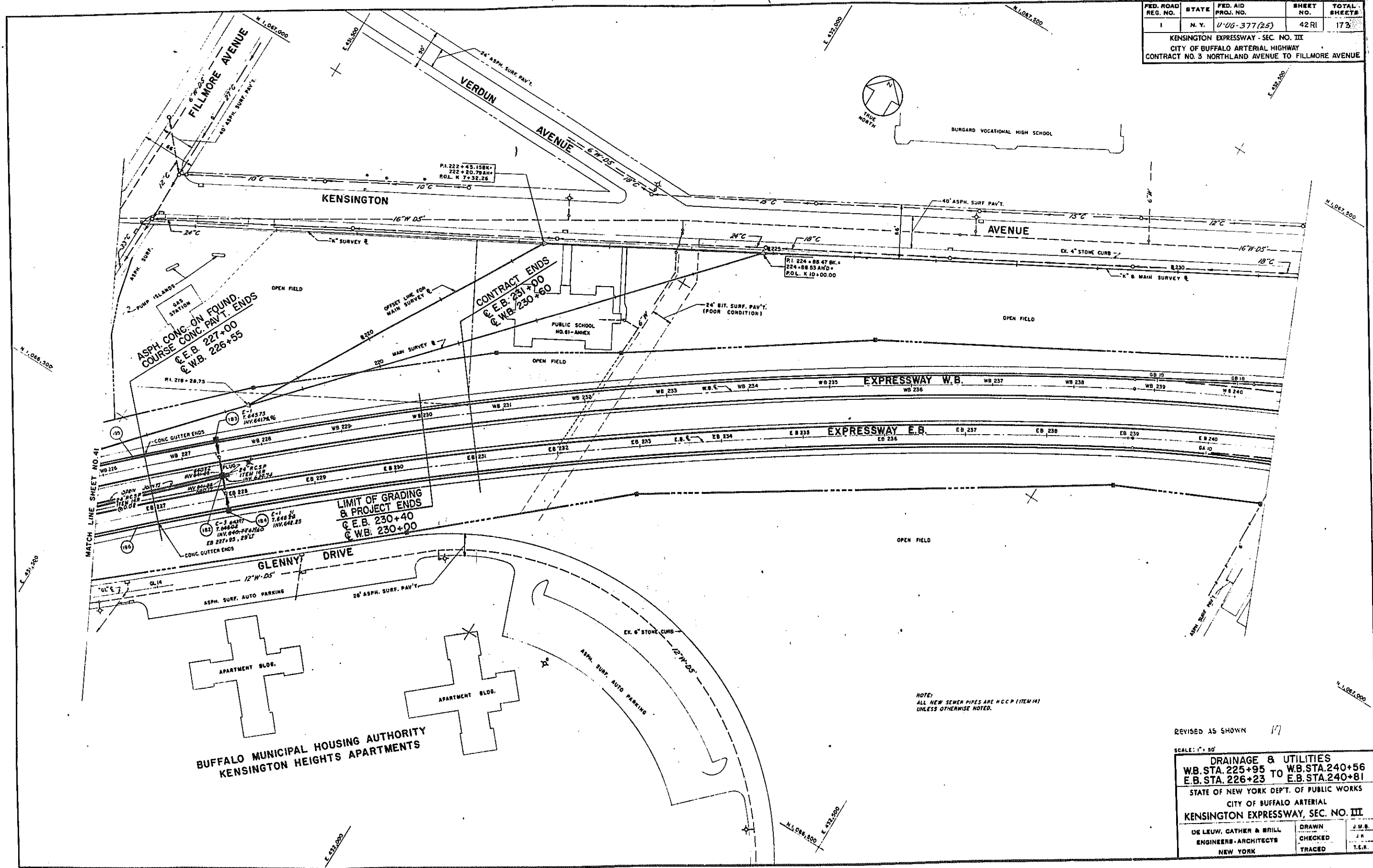
SCALE: 1"=50'

DRAINAGE & UTILITIES	
WB. STA. 211+46 TO WB. STA. 225+23	E.B. STA. 211+86 TO E.B. STA. 226+93
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL	DRAWN
ENGINEERS-ARCHITECTS	CHECKED
NEW YORK	TRACED



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	42 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



CONTRACT ENDS
 @ E.B. 231+00
 @ W.B. 230+60

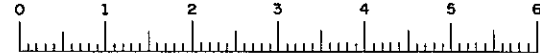
LIMIT OF GRADING
 & PROJECT ENDS
 @ E.B. 230+40
 @ W.B. 230+00

NOTE:
 ALL NEW SEWER PIPES ARE H.C.C.P. (ITEM 14)
 UNLESS OTHERWISE NOTED.

REVISED AS SHOWN 17

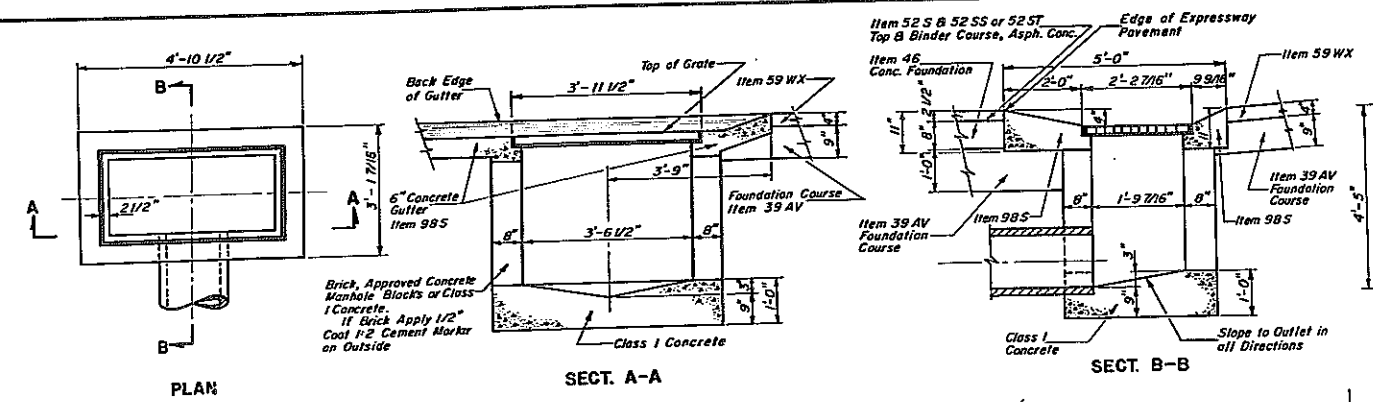
SCALE: 1" = 50'

DRAINAGE & UTILITIES	
W.B. STA. 225+95 TO	W.B. STA. 240+56
E.B. STA. 226+23	E.B. STA. 240+81
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHR & BRILL	DRAWN
ENGINEERS-ARCHITECTS	CHECKED
NEW YORK	TRACED
J.M.B.	J.M.B.
J.R.	T.E.H.

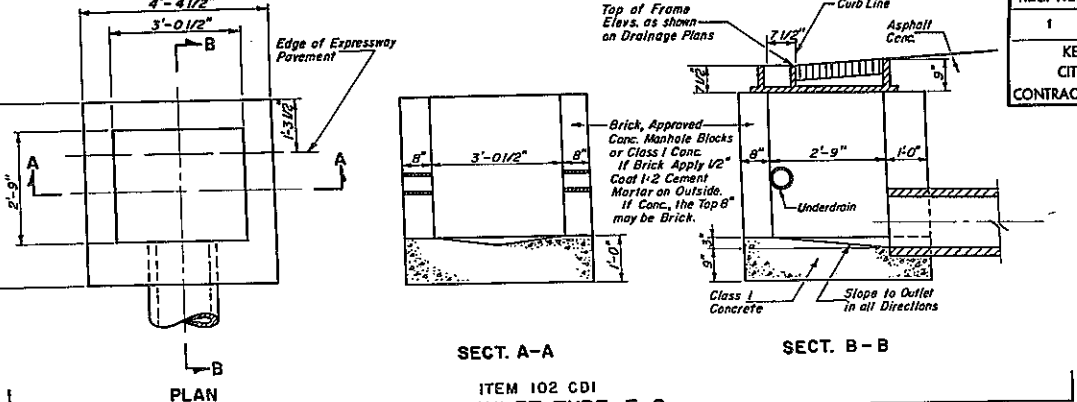


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	43	173

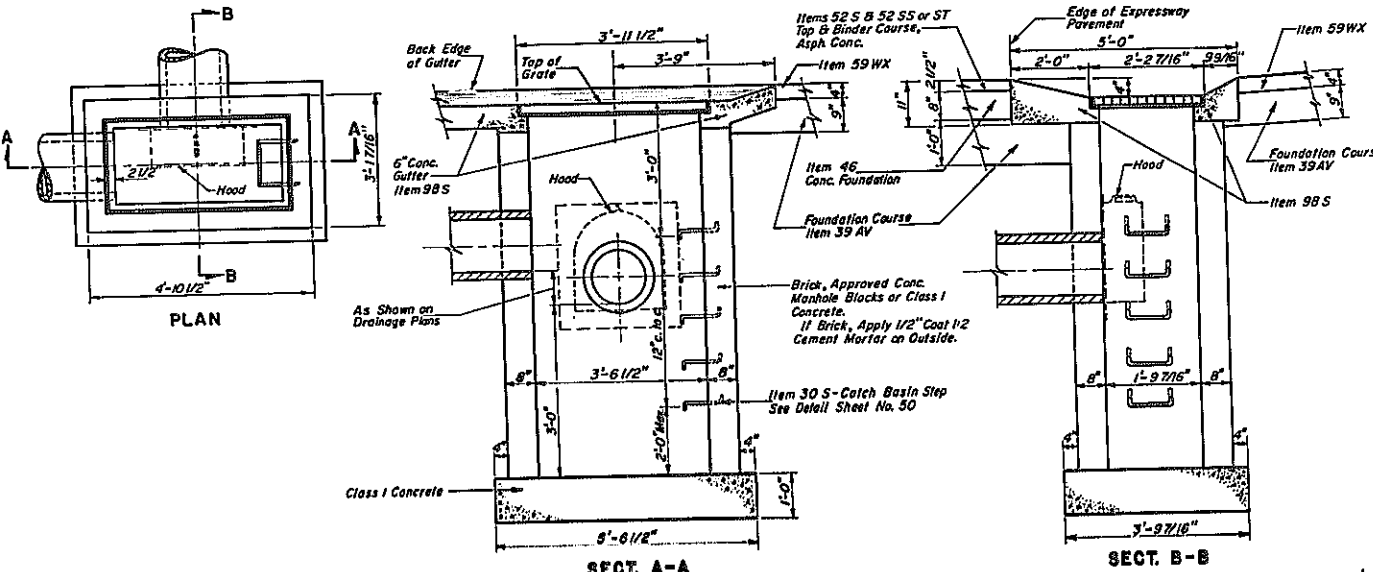
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3. NORTHLAND AVENUE TO FILLMORE AVENUE



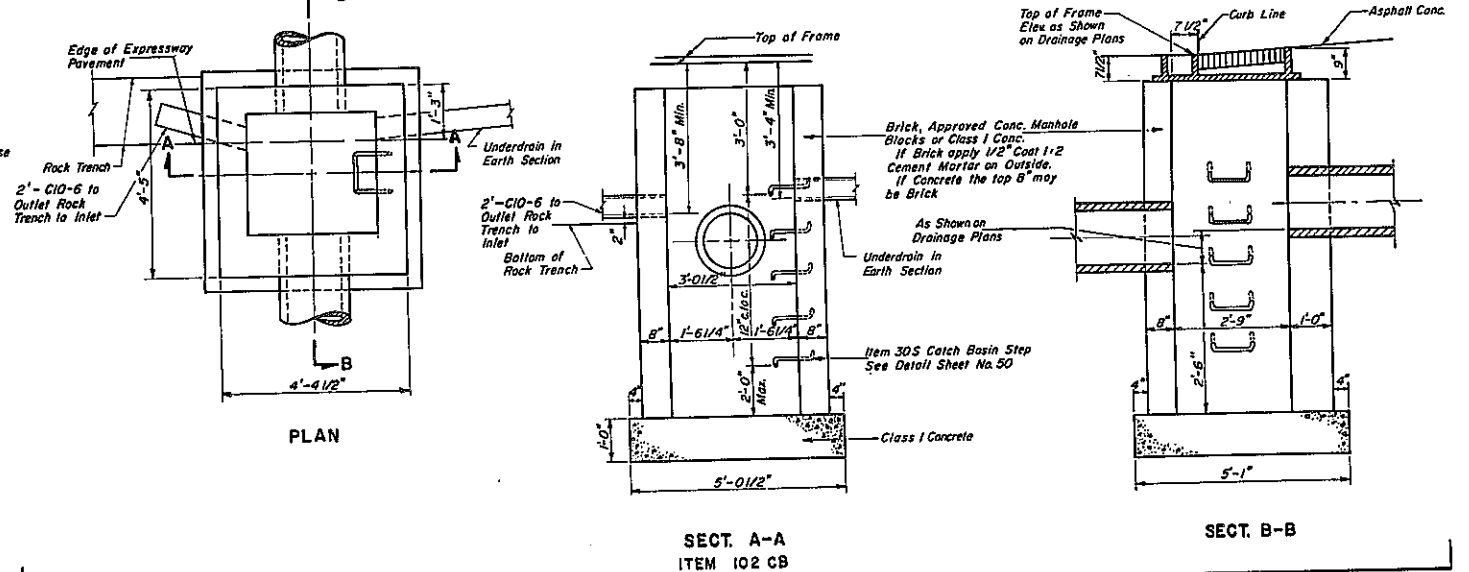
ITEM 102 CD1
DROP INLET TYPE E-1
Not to Scale



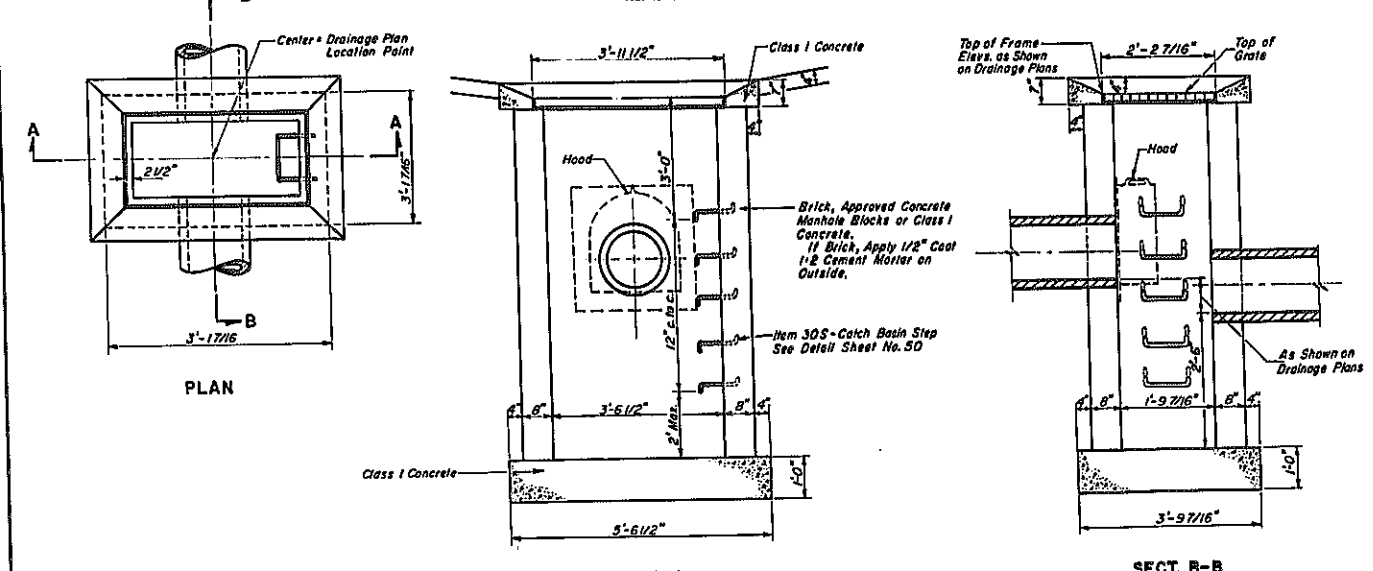
ITEM 102 CD1
DROP INLET TYPE E-2
Scale: 1/2" = 1'-0"



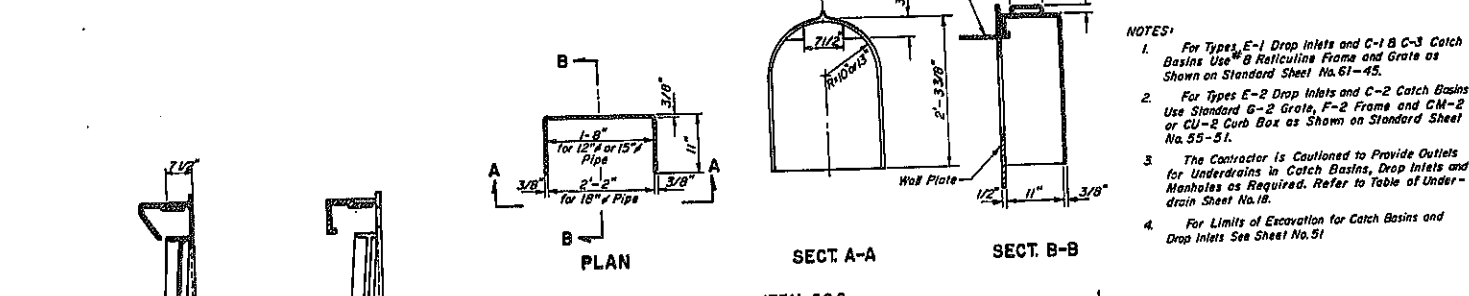
ITEM 102 CB
CATCH BASIN TYPE C-1
Not to Scale



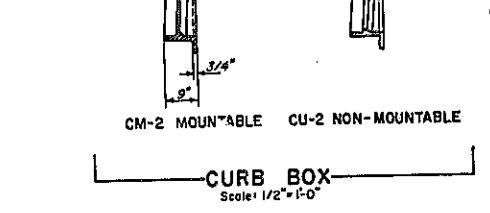
ITEM 102 CB
CATCH BASIN TYPE C-2
Scale: 1/2" = 1'-0"



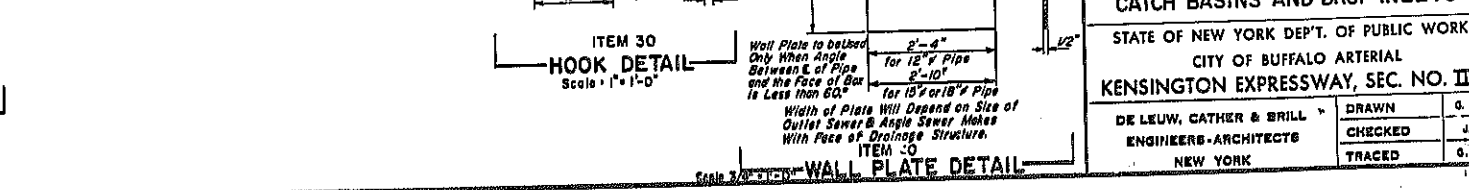
ITEM 102 CB
CATCH BASIN TYPE C-3
Not to Scale



ITEM 30 S
HOOD DETAIL
Scale: 3/4" = 1'-0"



CM-2 MOUN-TABLE CU-2 NON-MOUNTABLE
CURB BOX
Scale: 1/2" = 1'-0"



ITEM 30
HOOK DETAIL
Scale: 1" = 1'-0"

ITEM 30 WALL PLATE DETAIL
Scale: 3/4" = 1'-0"

- NOTES:
- For Types E-1 Drop Inlets and C-1 & C-3 Catch Basins Use ^{1/2}B Reflecting Frame and Grate as Shown on Standard Sheet No. 61-45.
 - For Types E-2 Drop Inlets and C-2 Catch Basins Use Standard G-2 Grate, F-2 Frame and CM-2 or CU-2 Curb Box as Shown on Standard Sheet No. 55-51.
 - The Contractor is Cautioned to Provide Outlets for Underdrains in Catch Basins, Drop Inlets and Manholes as Required. Refer to Table of Underdrain Sheet No. 18.
 - For Limits of Excavation for Catch Basins and Drop Inlets See Sheet No. 51

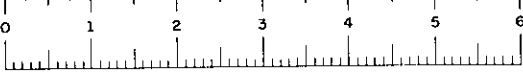
TYPE	FRAME LBS.	GRATE LBS.	CURB BOX LBS.	TOTAL EST. WGT. LBS.
E-1	70	191	—	270
C-1	70	191	—	270
C-3	70	191	—	270
E-2	505	224	221	950
C-2	505	224	221	950

**DRAINAGE STRUCTURES
CATCH BASINS AND DROP INLETS**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN G.C.R.
CHECKED G.K.
TRACED G.C.R.



FED. RD. REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	44R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

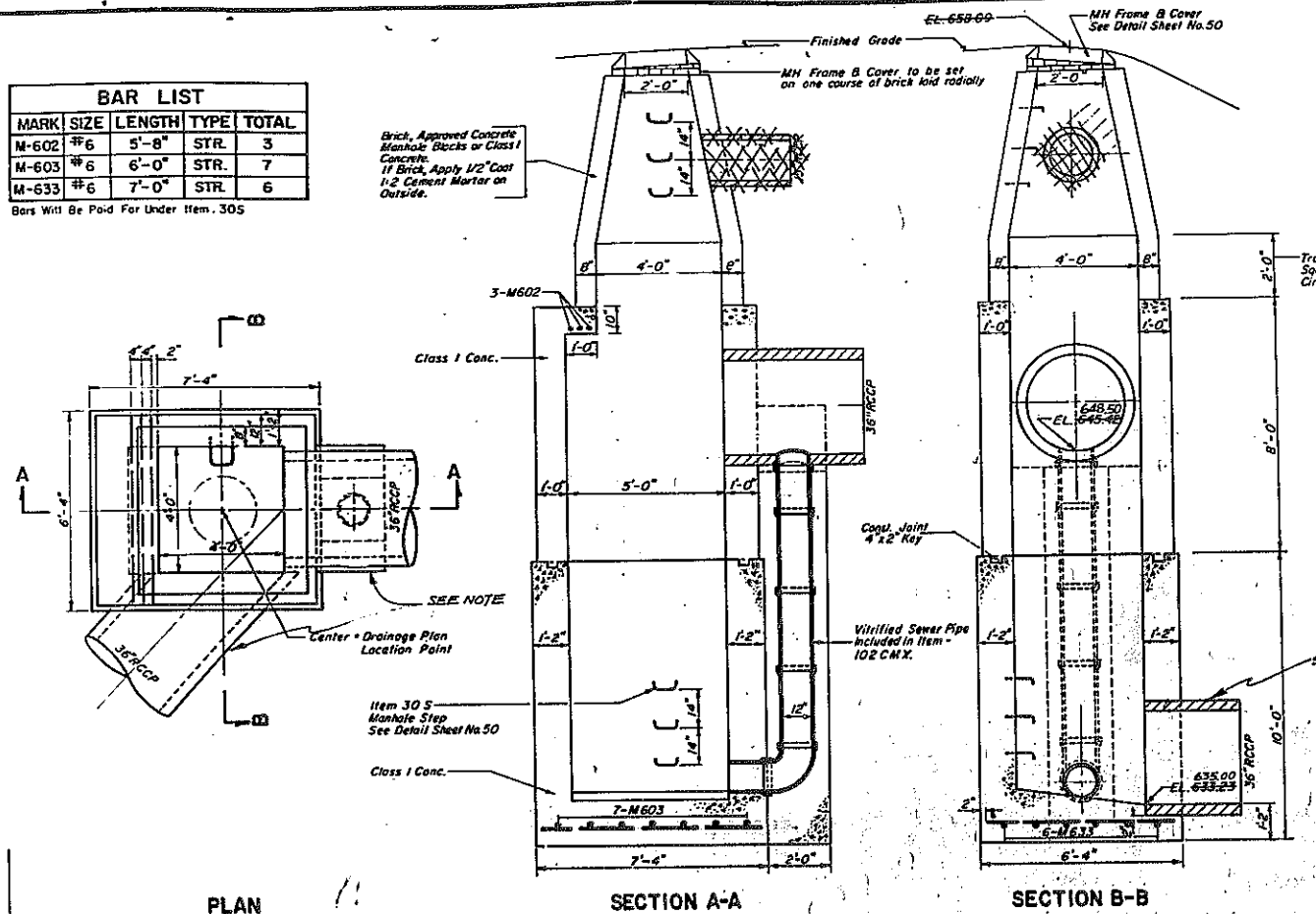
BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M-602	#6	5'-8"	STR.	3
M-603	#6	6'-0"	STR.	7
M-633	#6	7'-0"	STR.	6

Bars Will Be Paid For Under Item 305

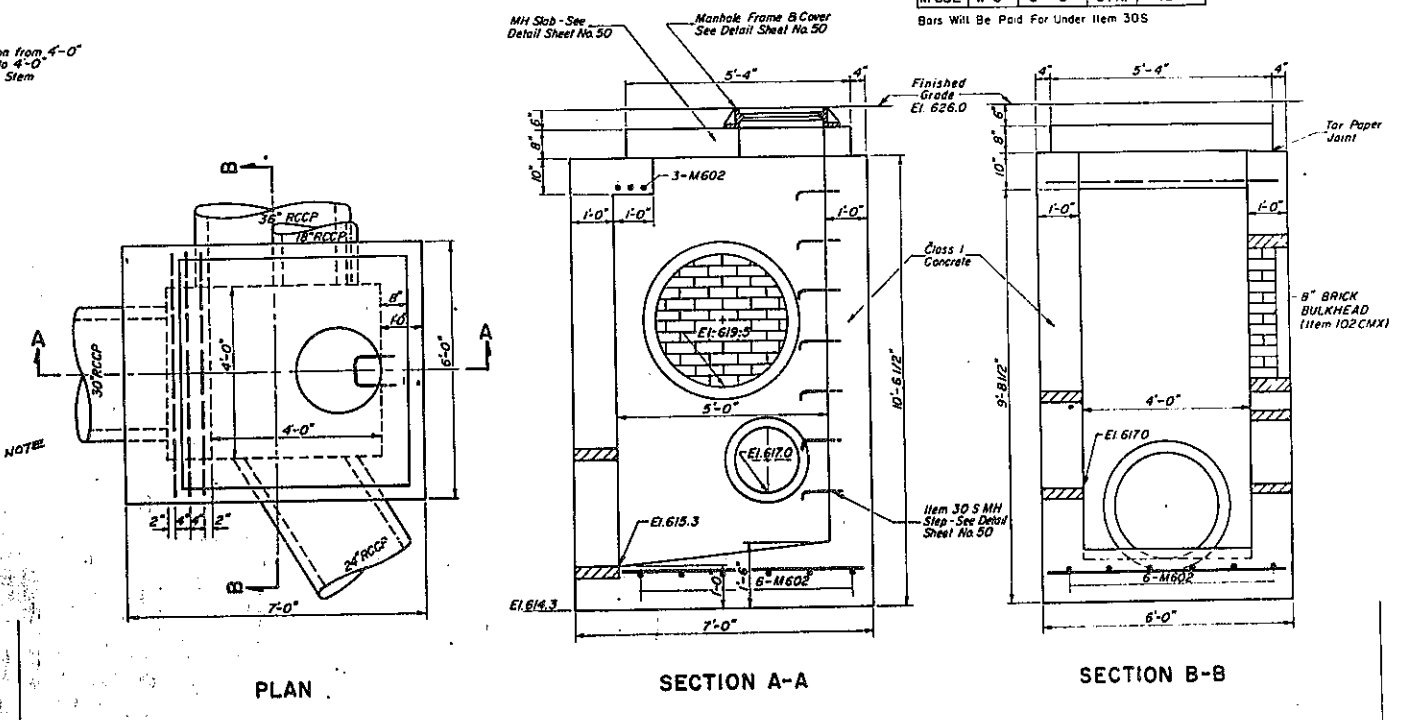
BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M602	#6	5'-8"	STR.	15

Bars Will Be Paid For Under Item 305

Note. On As Built Draw
Plates and Plate Detail
Refer to Plan Change File
Dated Jan 21, 64

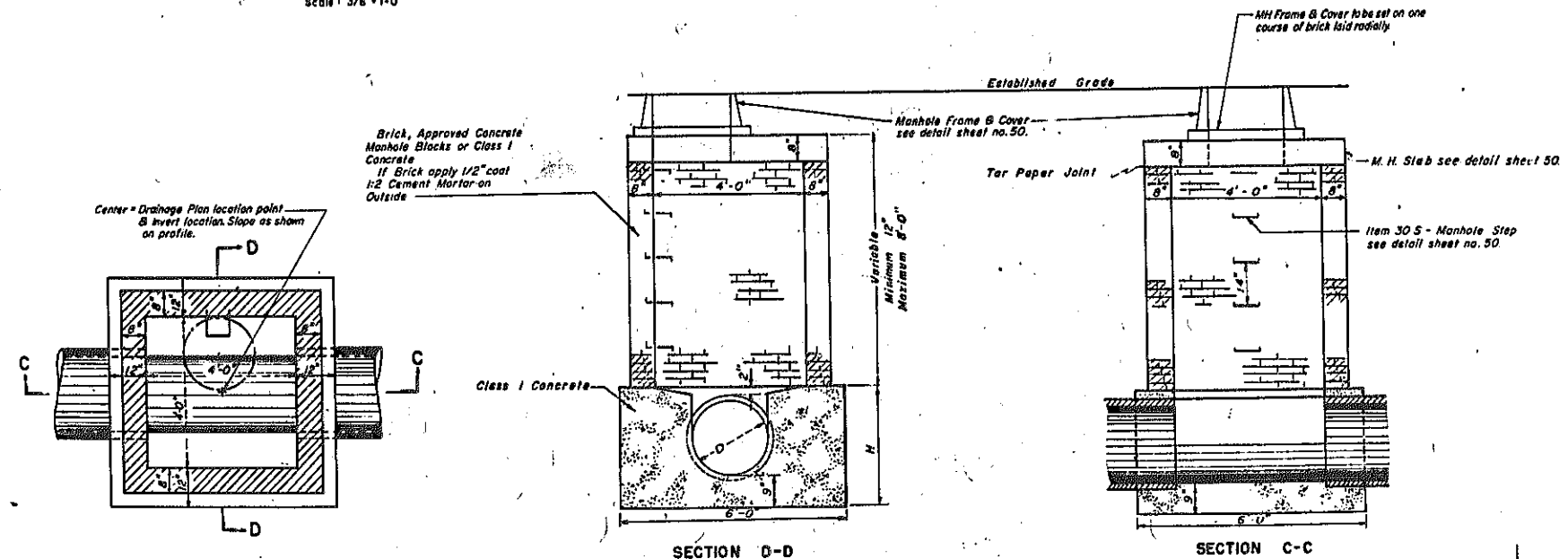


SPECIAL MANHOLE
W. B. 171-05, 15' LT.
ITEM 102 CMX
Scale: 3/8" = 1'-0"



SPECIAL MANHOLE
W. B. 204 + 45, 40' LT.
ITEM 102 CMX
Scale: 1/2" = 1'-0"

NOTE - ALIGNMENT OF PIPE DIFFERS FROM SHOWN - SEE FINISH BOOK NO. 4, SEC. 13 FOR DETAILS.



MANHOLE TYPE S-1
8" TO 24" SEWERS
ITEM 102 CMX
Scale: 1/2" = 1'-0"

D	H
8"	20"
10"	22"
12"	24"
15"	27"
18"	31"
20"	33"
22"	35"
24"	37"

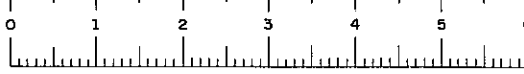
NOTE
For limits of excavation and backfill see sheet N. 51.

REVISION TO MANHOLE LOCATION

DRAINAGE STRUCTURES
MANHOLE TYPE S-1

STATE OF NEW YORK - DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. III

DE LEUW, CATHER & BRILL ENGINEERS - ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J. K. I. J. R. R. G. C. B.
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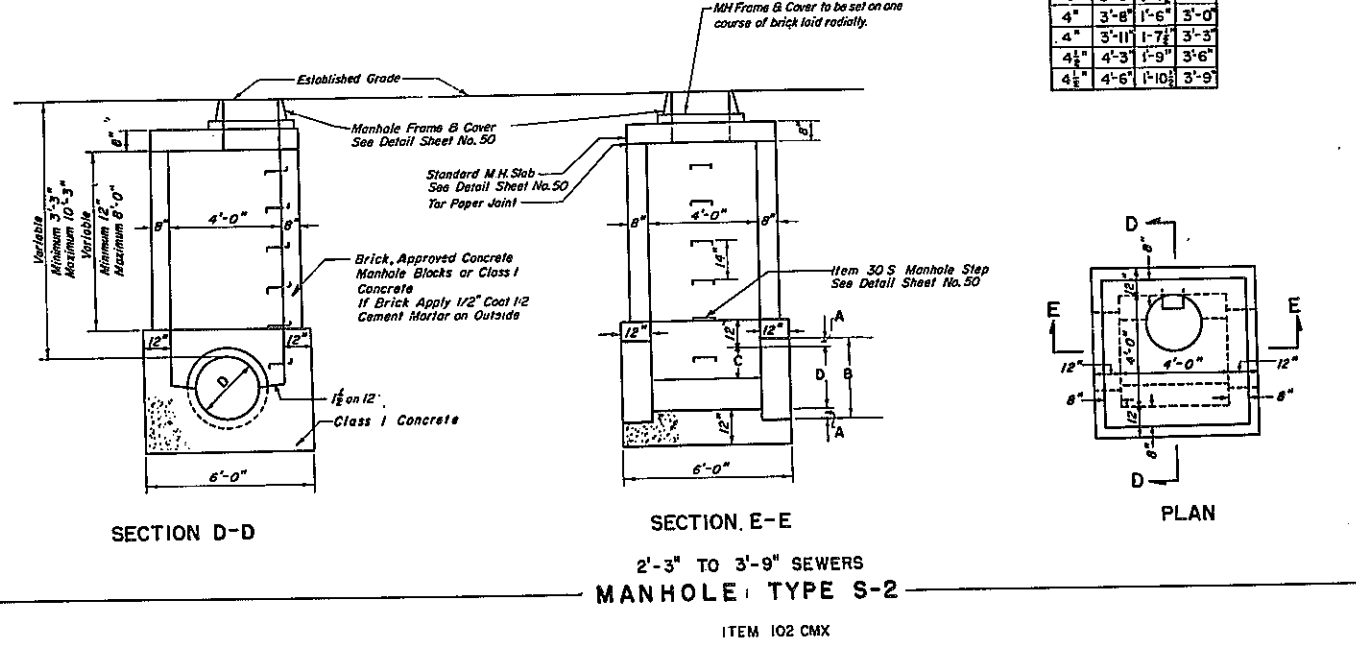
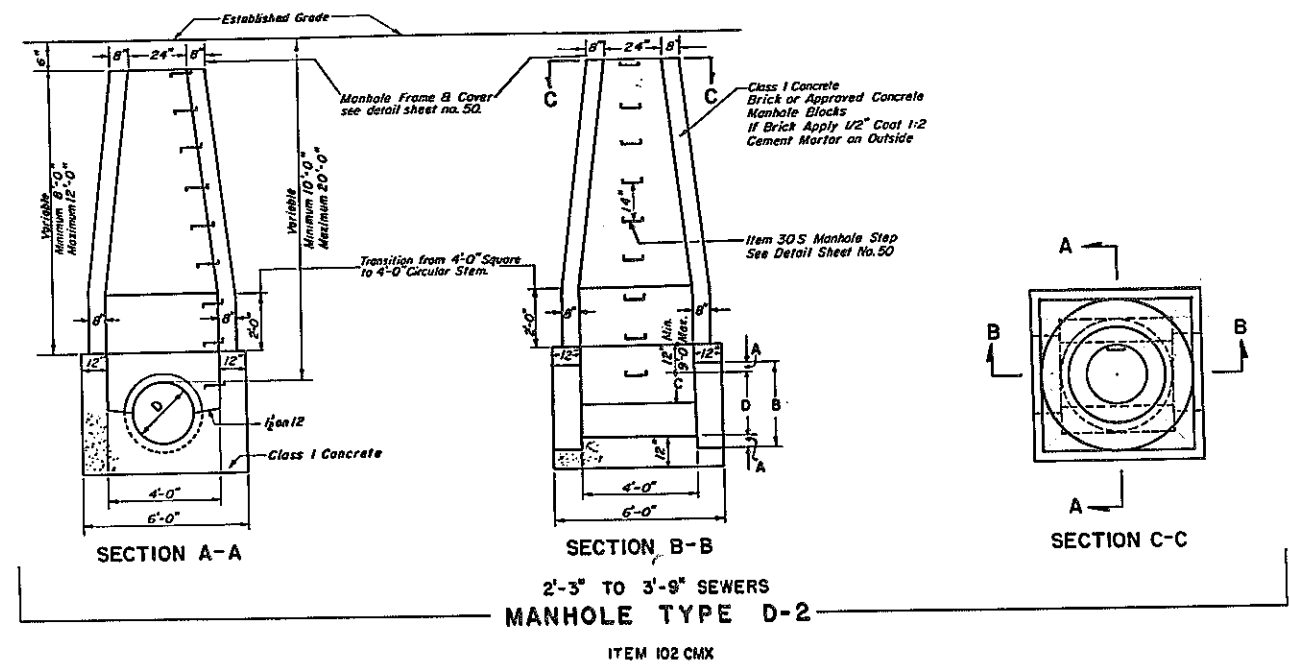


FAC 63-2

FED. RD. PROJ. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	45	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

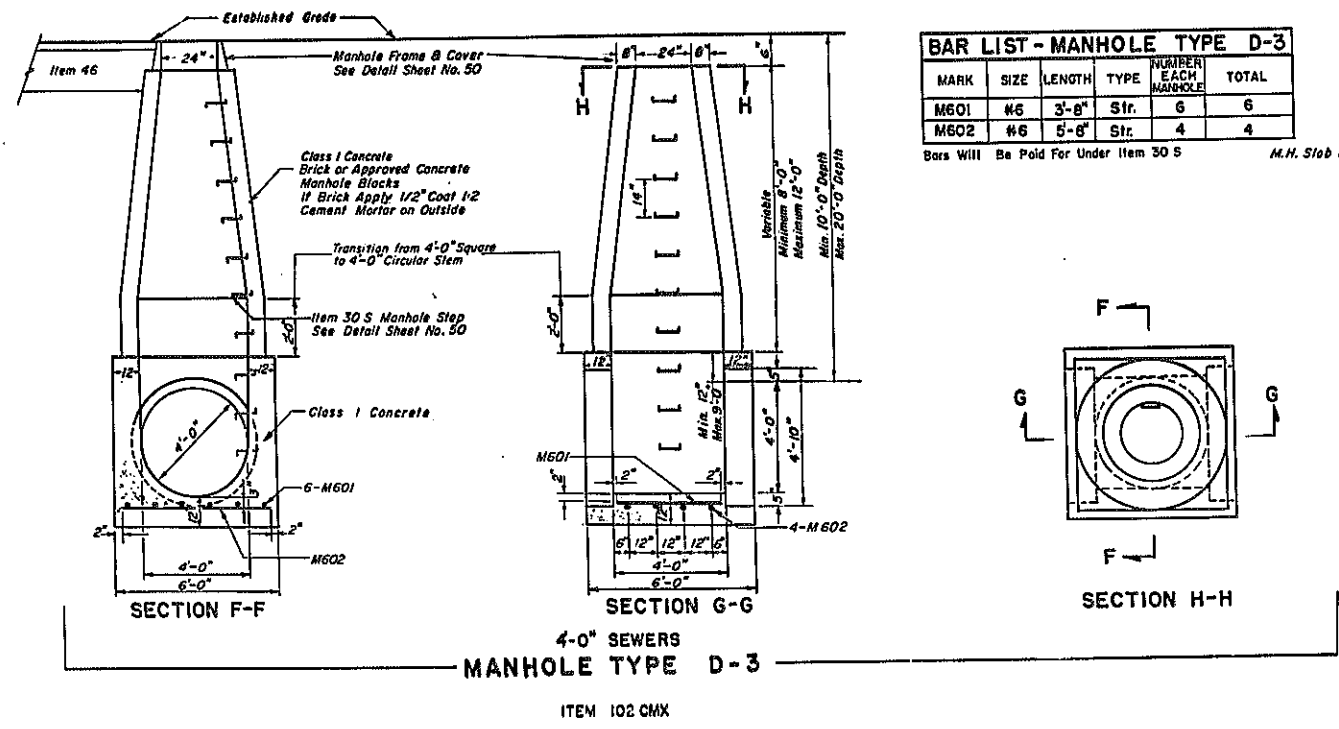
A	B	C	D
3 1/2'	2'-10"	1'-1 1/2"	2'-3"
3 1/2'	3'-1"	1'-3"	2'-6"
4'	3'-5"	1'-4 1/2"	2'-9"
4'	3'-8"	1'-6"	3'-0"
4'	3'-11"	1'-7 1/2"	3'-3"
4 1/2'	4'-3"	1'-9"	3'-6"
4 1/2'	4'-6"	1'-10 1/2"	3'-9"



BAR LIST - MANHOLE TYPE S-3

MARK	SIZE	LENGTH	TYPE	NUMBER EACH MANHOLE	TOTAL FOR 3 MH
M601	#6	3'-8"	Str.	6	18
M602	#6	5'-8"	Str.	4	12

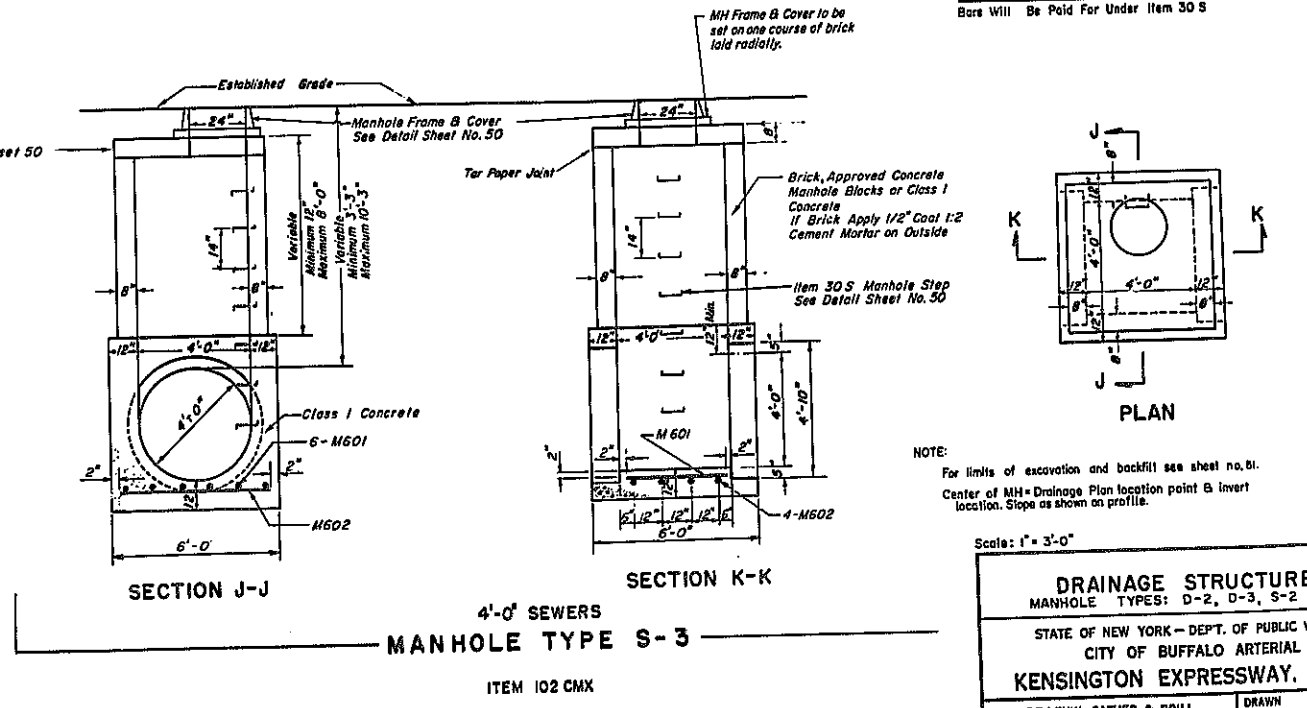
Bars Will Be Paid For Under Item 30 S



BAR LIST - MANHOLE TYPE D-3

MARK	SIZE	LENGTH	TYPE	NUMBER EACH MANHOLE	TOTAL
M601	#6	3'-8"	Str.	6	6
M602	#6	5'-8"	Str.	4	4

Bars Will Be Paid For Under Item 30 S
M.H. Slab See Detail Sheet 50



NOTE:
For limits of excavation and backfill see sheet no. 61.
Center of M.H. = Drainage Plan location point & Invert location. Slope as shown on profile.

Scale: 1" = 3'-0"

DRAINAGE STRUCTURES
MANHOLE TYPES: D-2, D-3, S-2 & S-3

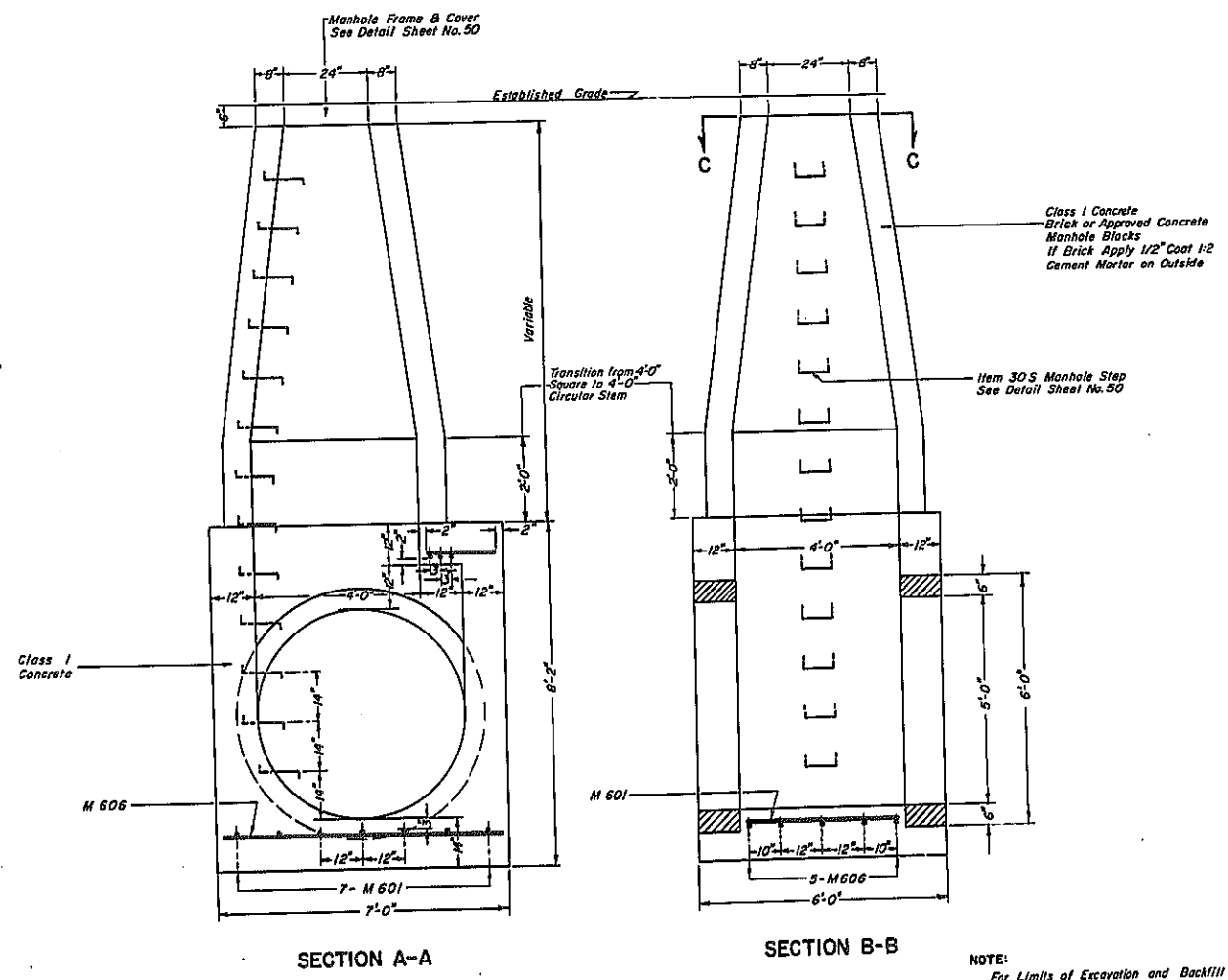
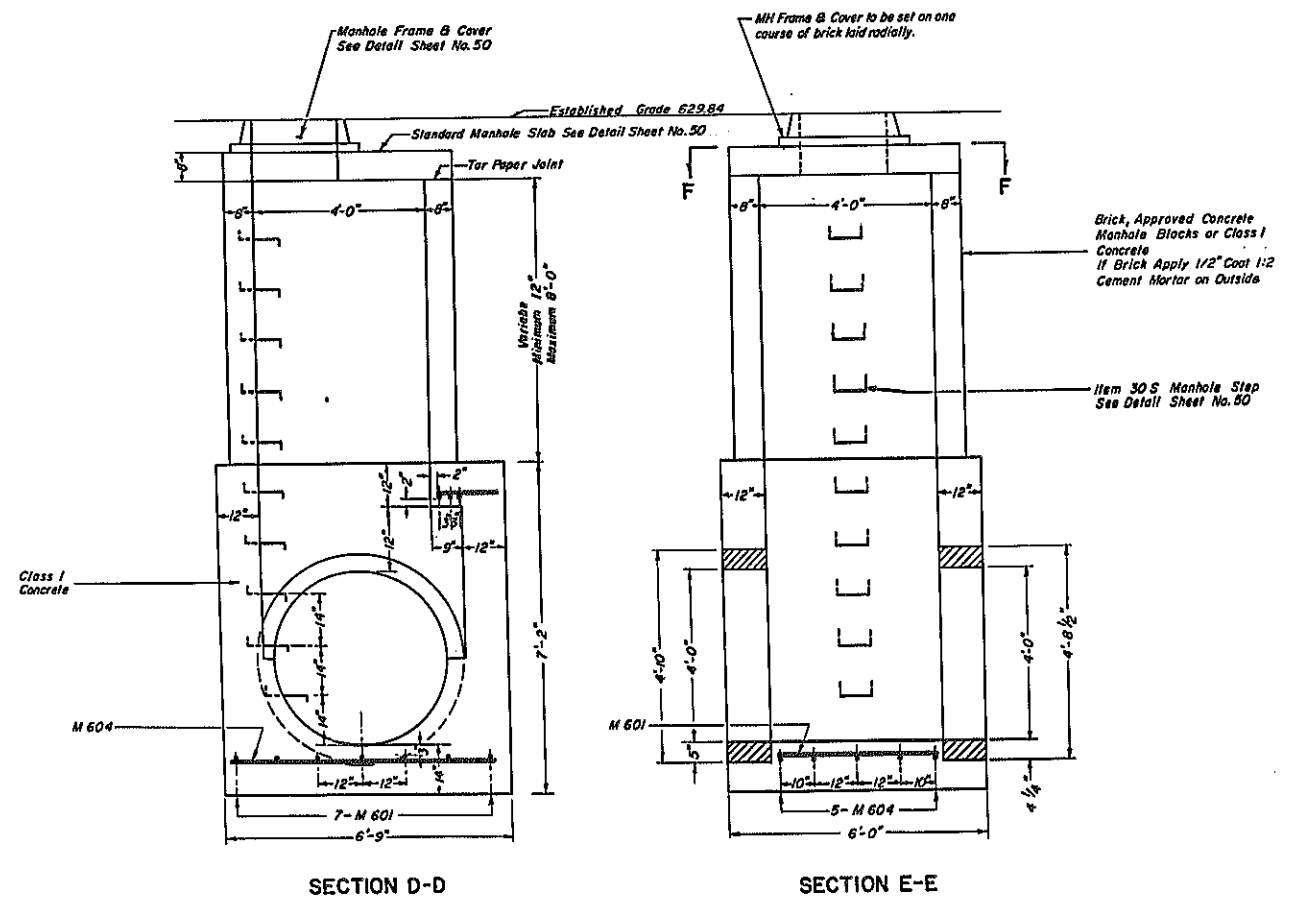
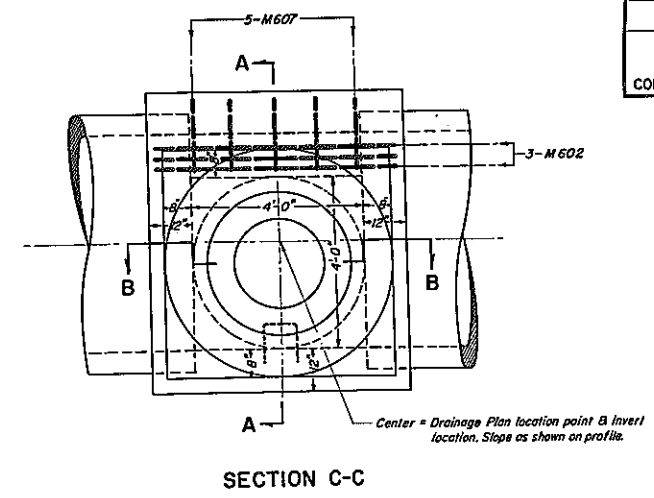
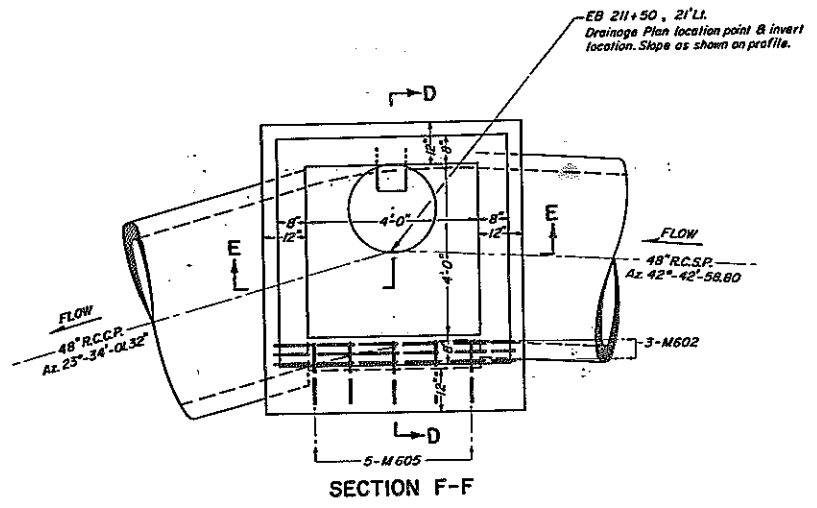
STATE OF NEW YORK - DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. III

DE LEUW, CATHER & BRILL	DRAWN	J. K.
ENGINEERS - ARCHITECTS	CHECKED	J. R. B.
NEW YORK	TRACED	G. C. R.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	46	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



MH No. 7
SPECIAL MANHOLE
EB 211+50, 21' Lt.
ITEM 102 CMX

BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M 601	# 6	3'-8"	Str.	7
M 602	# 6	5'-8"	Str.	3
M 604	# 6	6'-5"	Str.	5
M 605	# 6	1'-5"	Str.	5

Bars Will Be Paid For Under Item 308

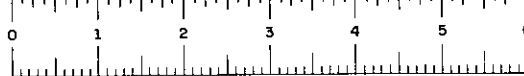
60" SEWER
MANHOLE TYPE D-4
ITEM 102 CMX

BAR LIST				
MARK	SIZE	LENGTH	NUMBER EACH MANHOLE	TOTAL FOR 2 MH
M 601	# 6	3'-8"	7	14
M 602	# 6	5'-8"	3	6
M 606	# 6	6'-8"	5	10
M 607	# 6	1'-8"	5	10

Bars Will Be Paid For Under Item 308

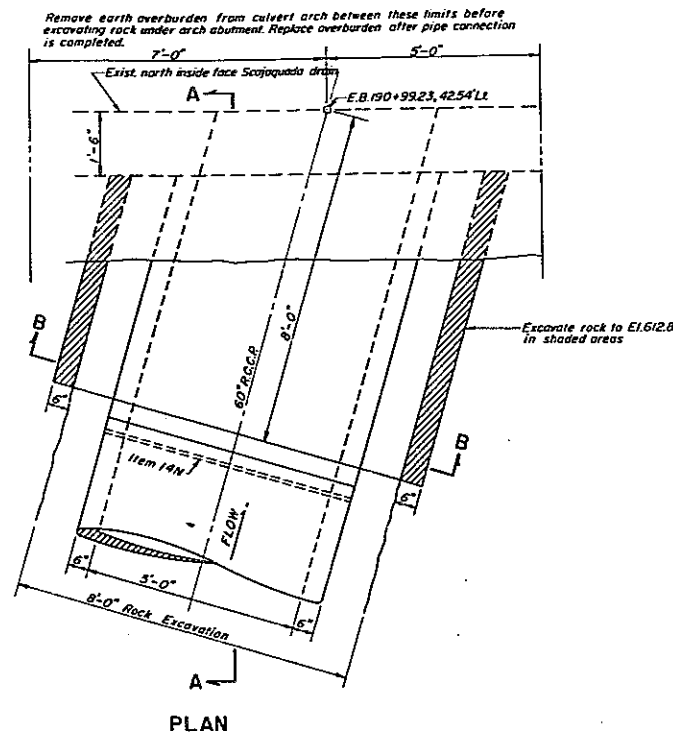
NOTE:
For Limits of Excavation and Backfill
See Sheet No. 51
SCALE 1/2" = 1'-0"

DRAINAGE STRUCTURES		
MANHOLE TYPES D-4 & No. 7 (SPECIAL)		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, GATHER & BRILL	DRAWN	G.S.P.
ENGINEERS-ARCHITECTS	CHECKED	J.K.
NEW YORK	TRACKED	G.V.B.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	47R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

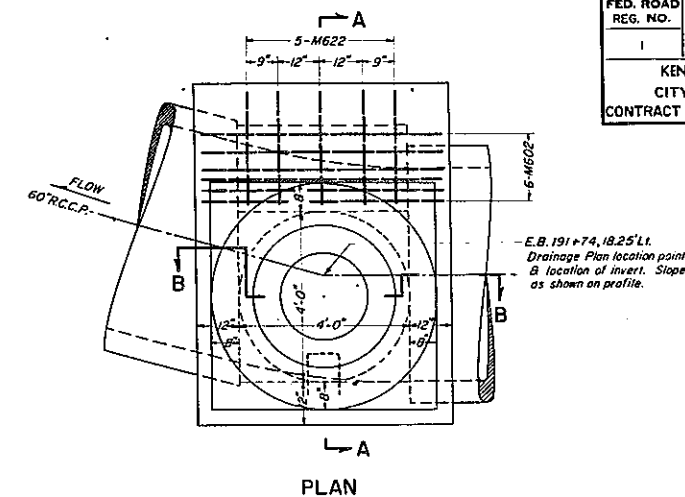


BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
A1001	No.10	8'-6"	Str.	13

Bars Will Be Paid For Under Item 305

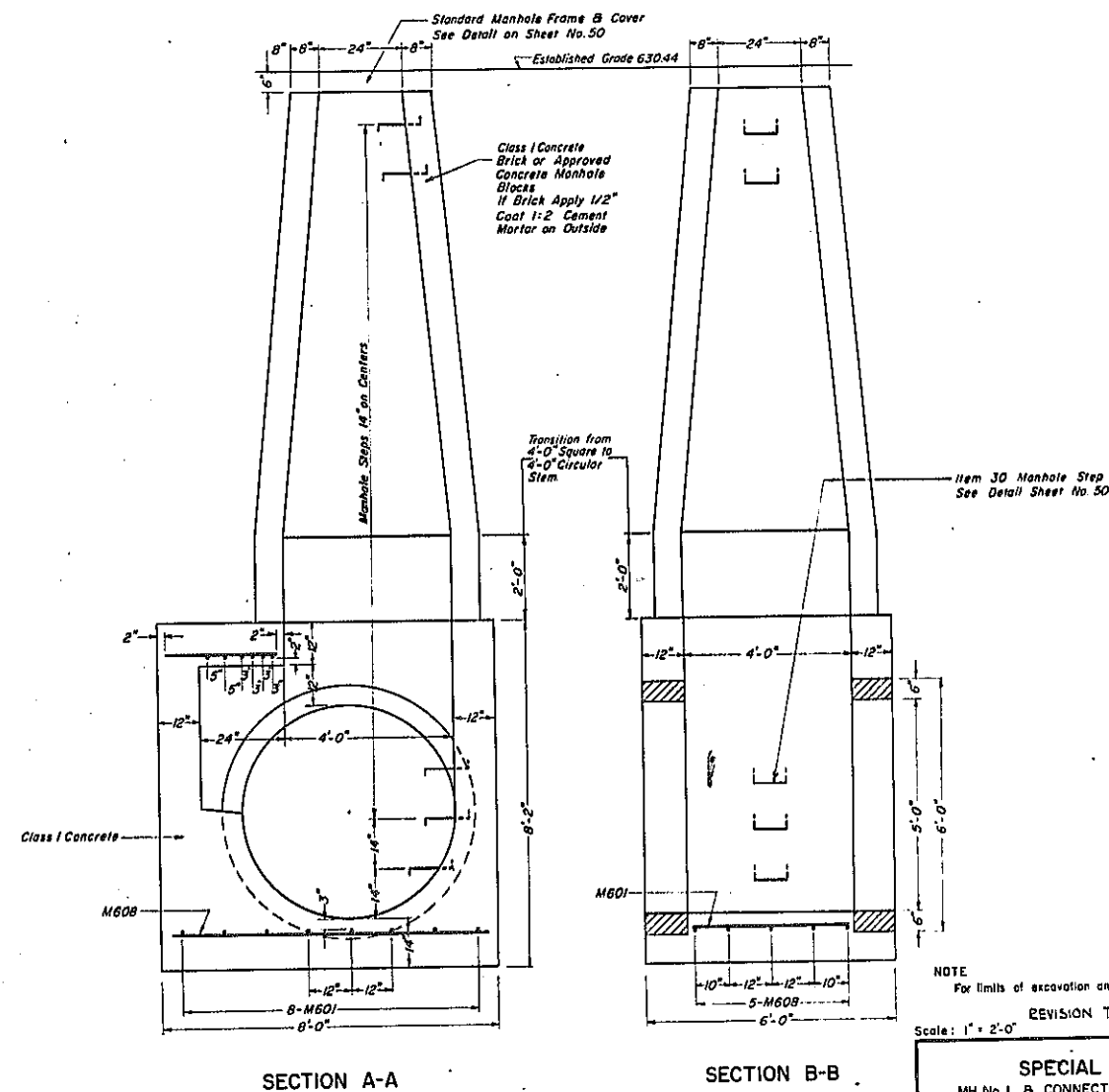
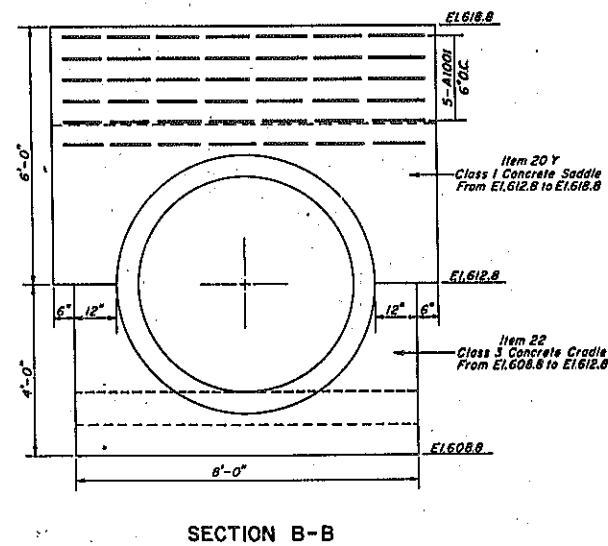
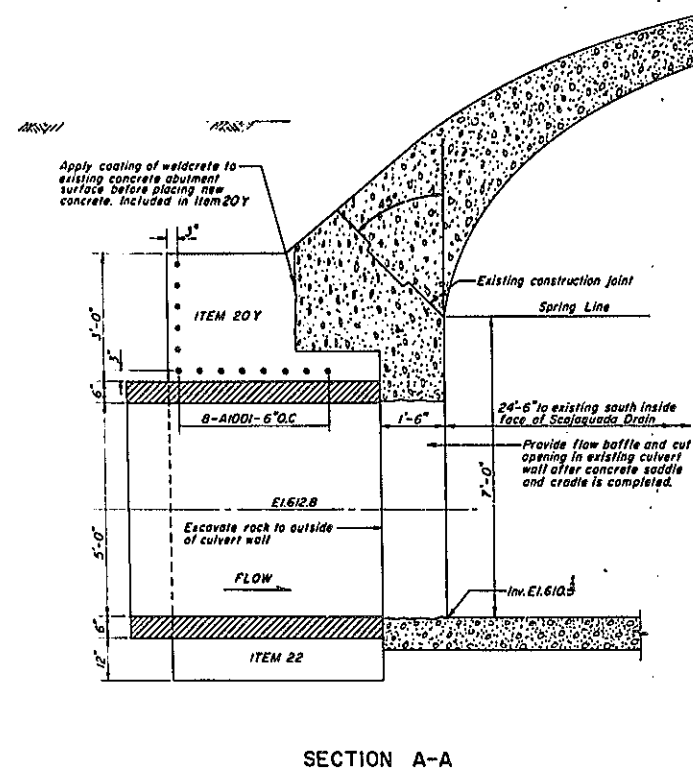
CONSTRUCTION SEQUENCE

- REMOVE EARTH OVERBURDEN FROM CULVERT ARCH BEFORE EXCAVATING ROCK UNDER ARCH ABUTMENT.
- EXCAVATE ROCK UNDER ARCH ABUTMENT TO THE ELEVATION INDICATED. THE CONTRACTOR IS CAUTIONED THAT HE WILL BE REQUIRED TO REPAIR AT HIS OWN EXPENSE ANY DAMAGE OCCURRING TO EXISTING SCAJAQUADA DRAIN AS A RESULT OF HIS EXCAVATING OPERATION.
- THE FIRST SECTION OF 60" R.C.C.P. TO FIT FLUSH AGAINST FACE OF SCAJAQUADA DRAIN AT PROPER SKEW.
- PLACE CONCRETE CRADLE AROUND PIPE TO ELEVATION 618.8
- PLACE REINFORCED CONCRETE SADDLE AROUND PIPE TO ELEVATION 618.8
- PLACE REMAINING 60" R.C.C.P. TO MANHOLE (SPEC.)
- PROVIDE TEMPORARY FLOW BAFFLE.
- CUT OPENING IN EXISTING CULVERT WALL, NO LESS THAN 28 DAYS AFTER CONCRETE SADDLE IS PLACED.
- REMOVE TEMPORARY FLOW BAFFLE.
- BACKFILL OVER EXPOSED PORTION OF CULVERT ARCH TO SUBGRADE OF WESTBOUND EXPRESSWAY



BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	6
M602	No.6	5'-8"	Str.	6
M608	No.6	7'-6"	Str.	5
M622	No.6	2'-8"	Str.	5

Bars Will Be Paid For Under Item 305



NOTE
For limits of excavation and backfill see sheet no. 51
REVISION TO INVERT ELEV.
Scale: 1" = 2'-0"

EXPRESSWAY TRUNK SEWER CONNECTION
TO SCAJAQUADA DRAIN
E.B. 190+99.23, 42.54 Lt.

MH No. 1
SPECIAL MANHOLE
E.B. 191+74, 18.25 Lt.

ITEM 102 CMX

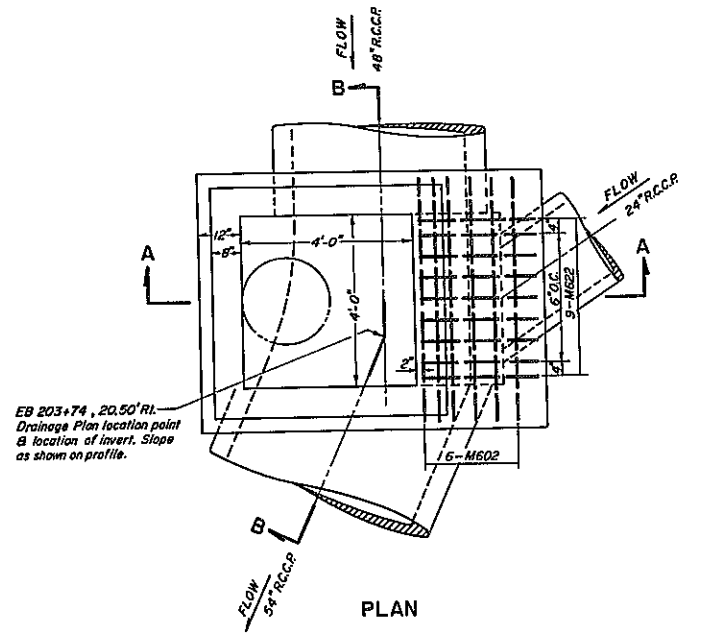
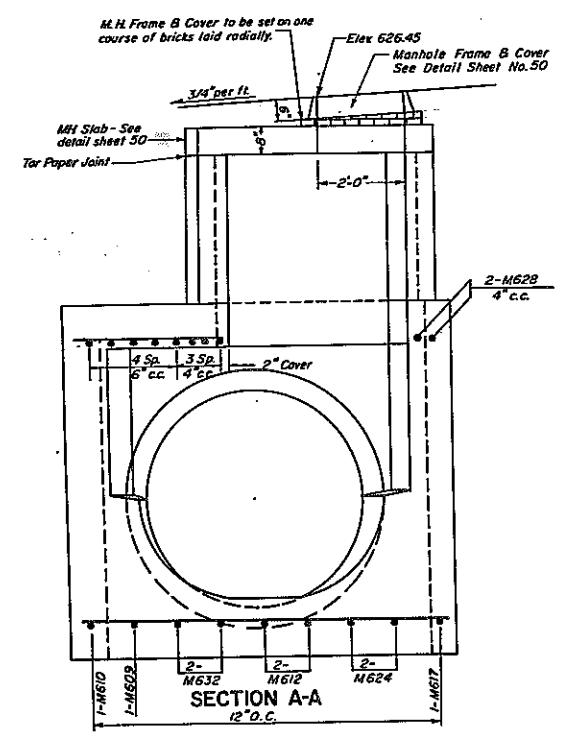
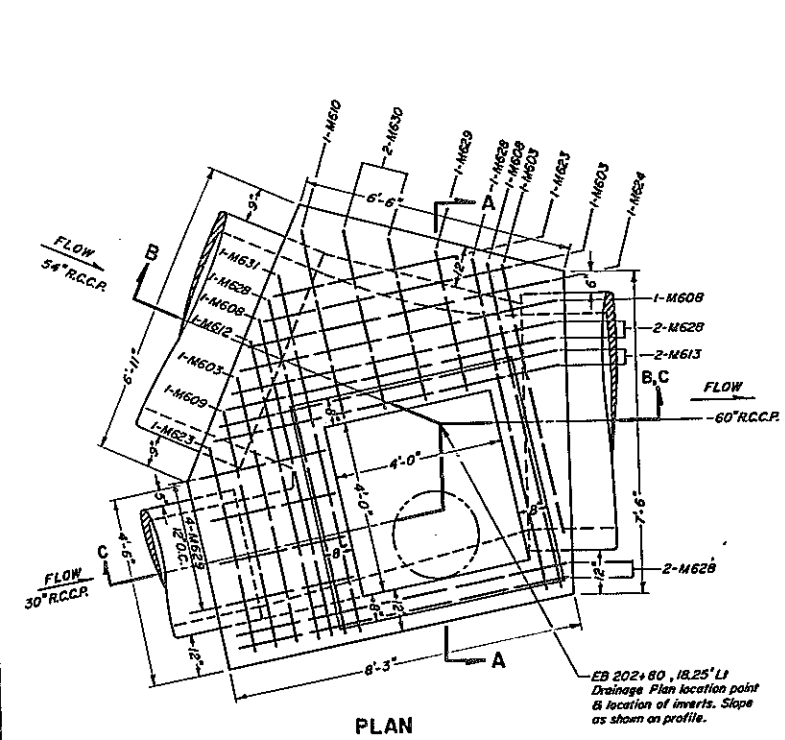
SPECIAL MANHOLES		
MH No. 1 & CONNECTION TO SCAJAQUADA DRAIN		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL	DRAWN	J.M.B.
ENGINEERS-ARCHITECTS	CHECKED	J.K.
NEW YORK	TRACED	T.E.K.



FAO 68-2

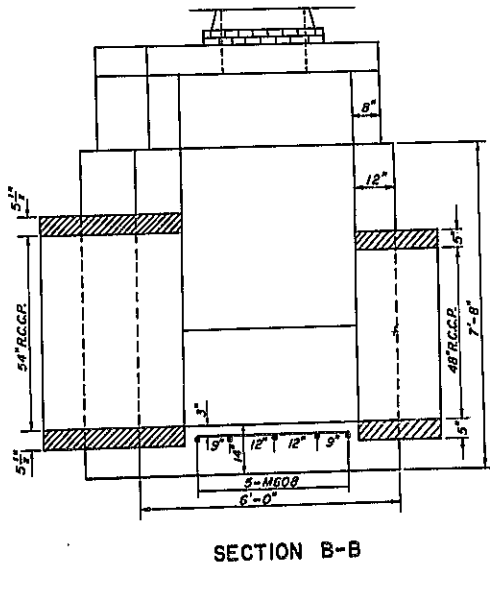
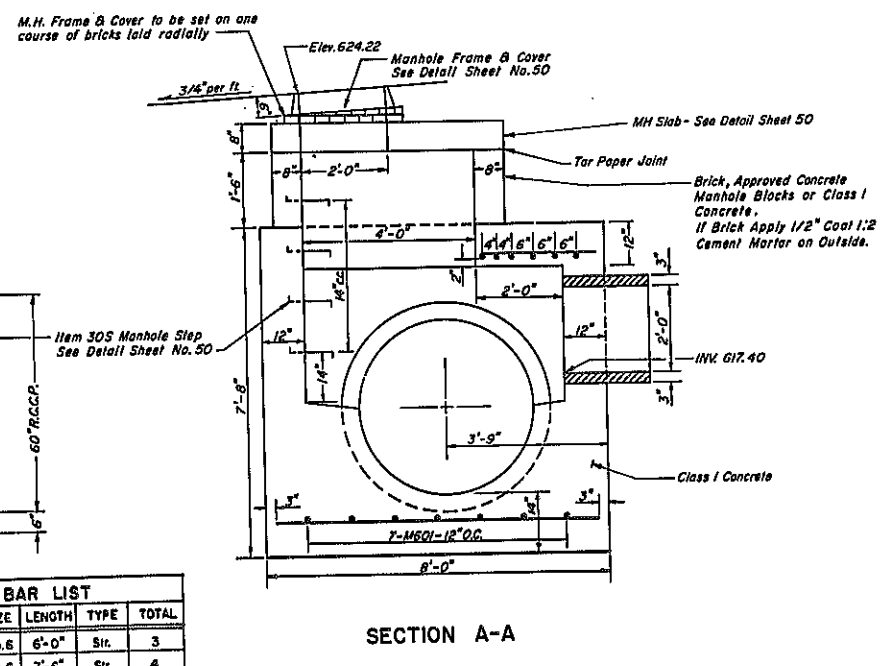
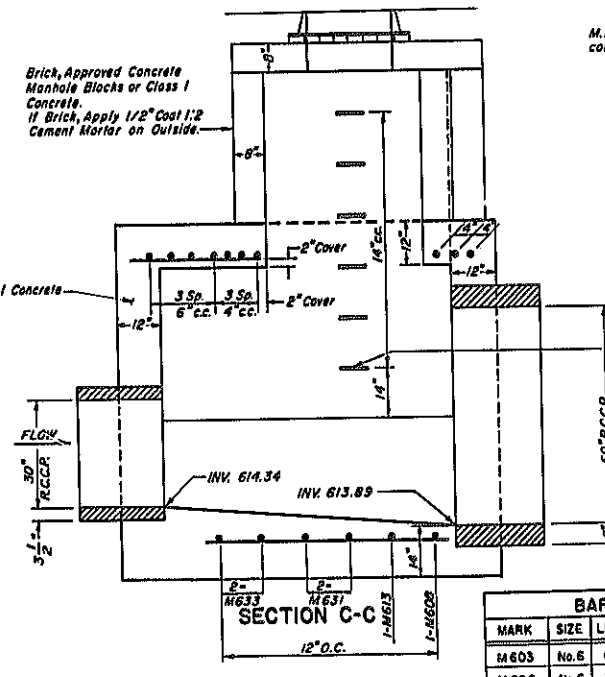
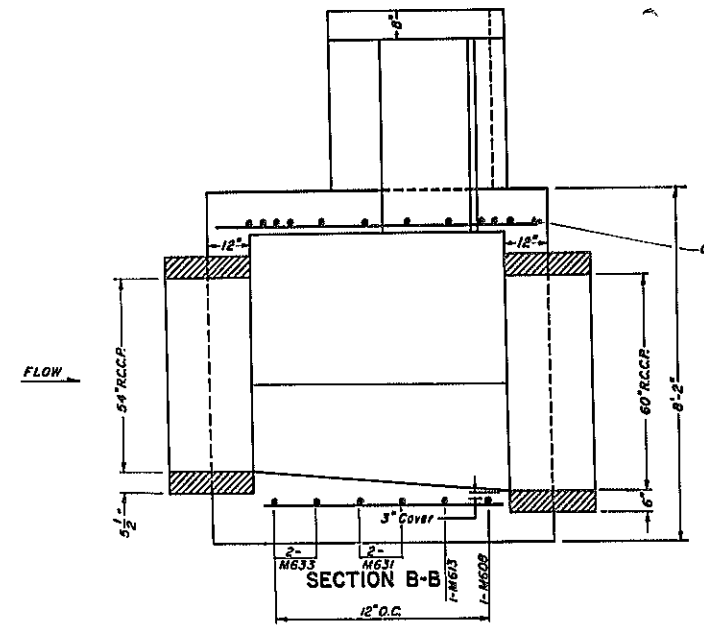
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	UG-377(25)	48	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No. 6	3'-8"	Str.	7
M602	No. 6	5'-8"	Str.	6
M608	No. 6	7'-6"	Str.	5
M622	No. 6	2'-8"	Str.	9

Bars Will Be Paid For Under Item 305



MH No. 4
SPECIAL MANHOLE
EB 202+80, 18.25' Lt.
ITEM 102 CMX

MH No. 5
SPECIAL MANHOLE
EB 203+74, 20.50' Rt.
ITEM 102 CMX

BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M603	No. 6	6'-0"	Str.	3
M608	No. 6	7'-6"	Str.	4
M609	No. 6	5'-6"	Str.	2
M610	No. 6	4'-6"	Str.	2
M612	No. 6	6'-0"	Str.	3
M613	No. 6	8'-0"	Str.	3
M617	No. 6	5'-0"	Str.	1
M623	No. 6	4'-8"	Str.	2
M624	No. 6	7'-2"	Str.	3
M628	No. 6	7'-9"	Str.	6
M629	No. 6	3'-1"	Str.	6
M630	No. 6	3'-10"	Str.	2
M631	No. 6	6'-6"	Str.	3
M632	No. 6	6'-3"	Str.	2
M633	No. 6	9'-3"	Str.	2

Bars Will Be Paid For Under Item 308

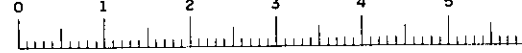
NOTE:
For limits of excavation and backfill see sheet no. 51.

Scale: 1" = 2'-0"

SPECIAL MANHOLES
MH No. 4 & MH No. 5

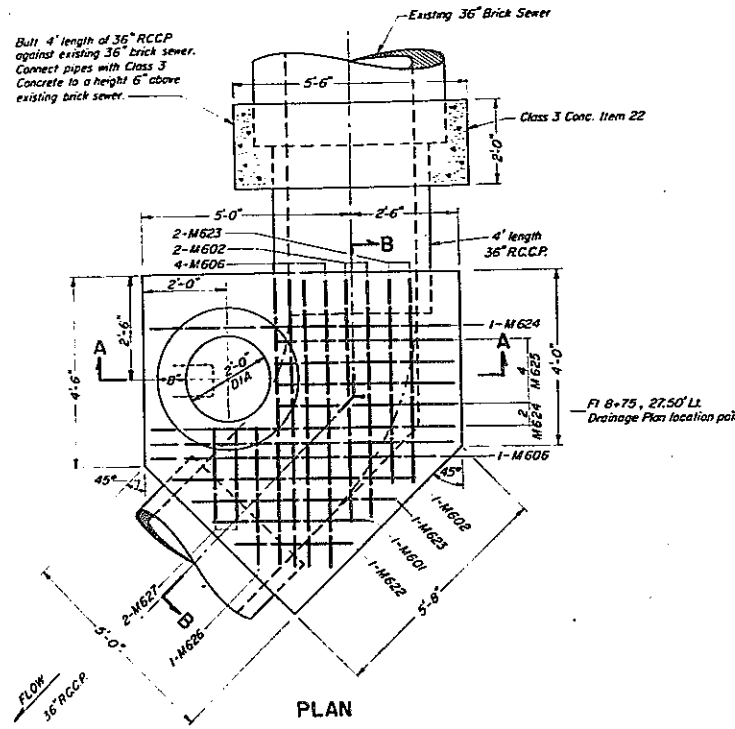
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.M.B. F.J.P. T.E.K.
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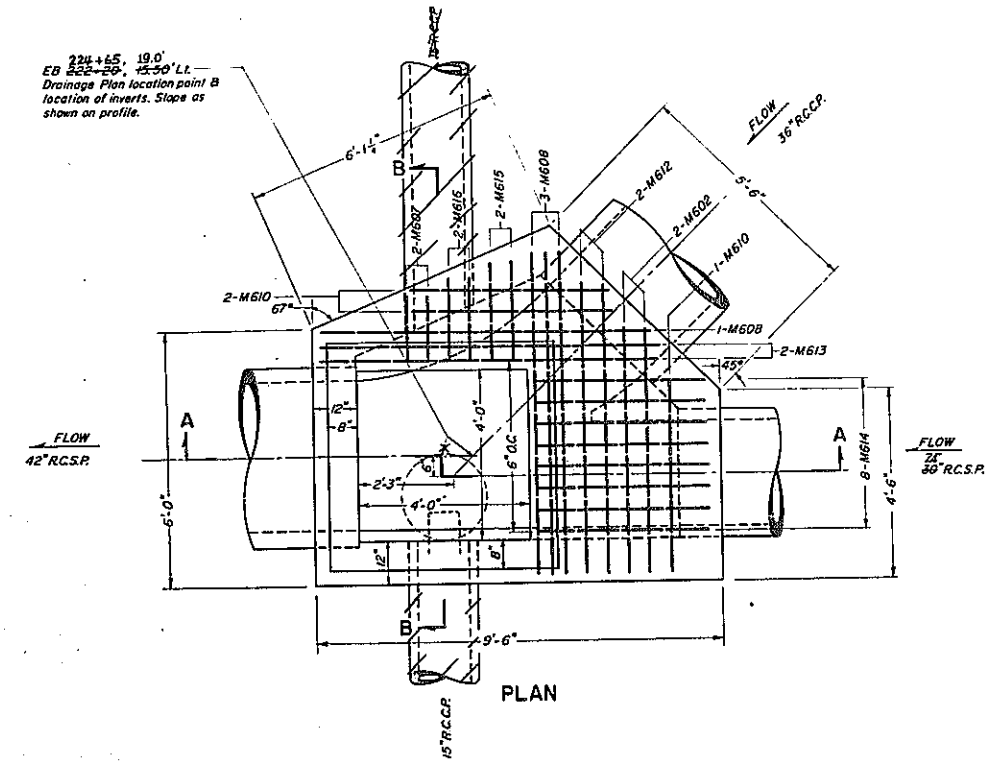
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-37725	49RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



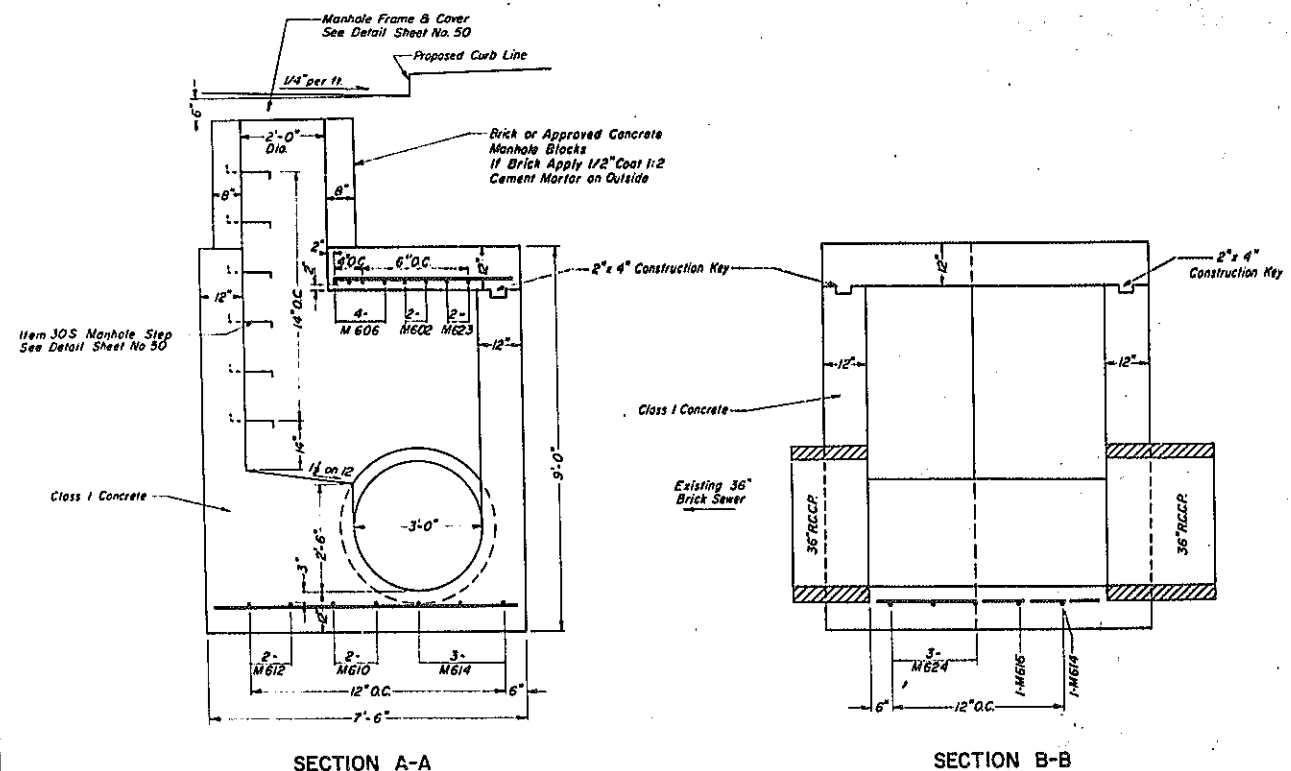
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	1
M602	No.6	5'-8"	Str.	3
M606	No.6	6'-8"	Str.	5
M622	No.6	2'-8"	Str.	1
M623	No.6	4'-8"	Str.	3
M624	No.6	7'-2"	Str.	3
M625	No.6	4'-2"	Str.	4
M626	No.6	3'-2"	Str.	1
M627	No.6	2'-2"	Str.	2

Bars Will Be Paid For Under Item 305

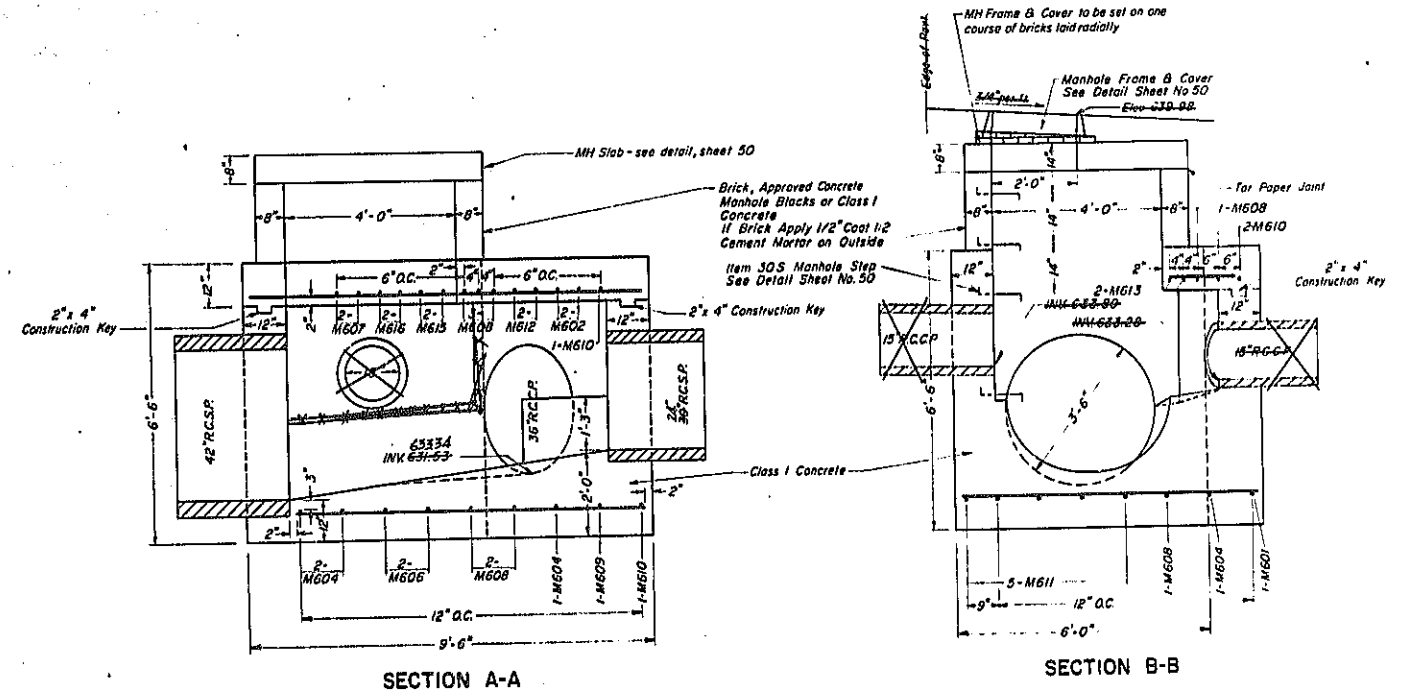


MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	1
M602	No.6	5'-8"	Str.	2
M604	No.6	6'-5"	Str.	4
M606	No.6	6'-8"	Str.	2
M607	No.6	1'-8"	Str.	2
M608	No.6	7'-6"	Str.	7
M609	No.6	5'-6"	Str.	1
M610	No.6	4'-6"	Str.	5
M611	No.6	8'-2"	Str.	5
M612	No.6	6'-9"	Str.	2
M613	No.6	8'-0"	Str.	2
M614	No.6	4'-0"	Str.	8
M615	No.6	2'-4"	Str.	2
M616	No.6	2'-0"	Str.	2

Bars Will Be Paid For Under Item 305



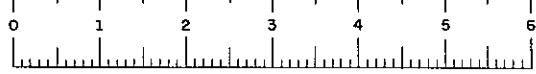
FILLMORE AVE. SEWER INTERCEPT
SPECIAL MANHOLE
FI 8+75, 27.50' Lt.
ITEM 102CMX



MH No. 11
SPECIAL MANHOLE
224 + 65, 19'
EB 222+20, 15.50' Lt.
ITEM 102CMX

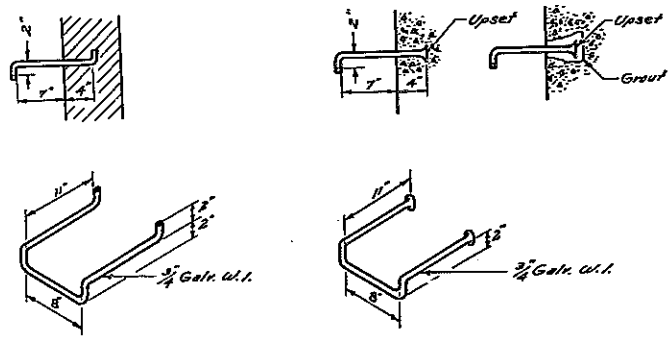
NOTE
For limits of excavation and backfill see sheet no 51
REVISION TO 15" R.C.C.P.
Scale: 1" = 2'-0"

SPECIAL MANHOLES		
MH No. 11 & FILLMORE AVE. SEWER INTERCEPT		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. F.J.P. J.M.B.



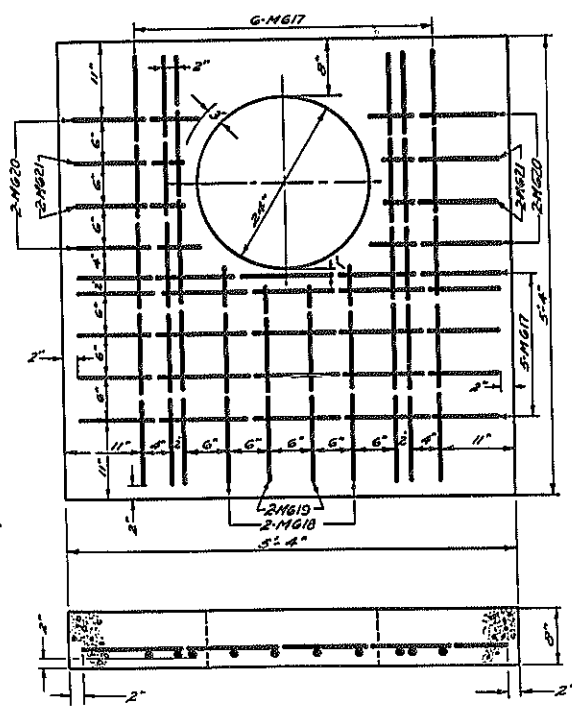
FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (28)	50	173
KENSINGTON EXPRESSWAY SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



USED IN BRICKWORK & CONCRETE MANHOLE BLOCKS

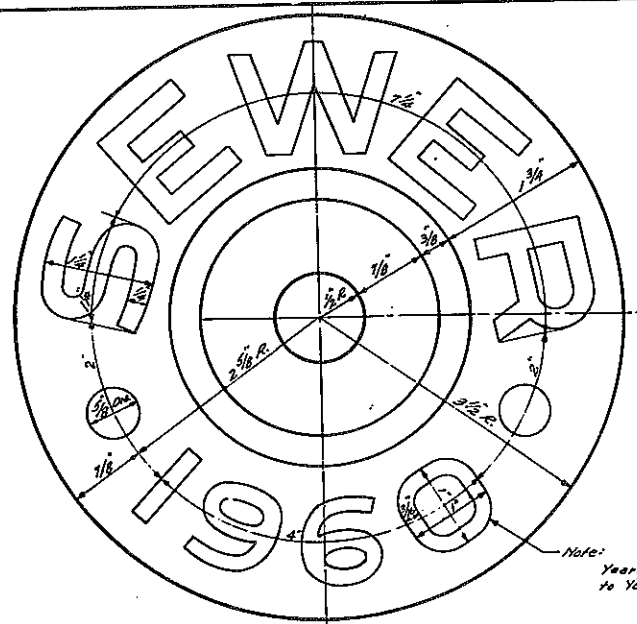
CATCH BASIN & MANHOLE STEPS
ITEM 30S



MANHOLE SLABS
SCALE: 1"=1'-0"

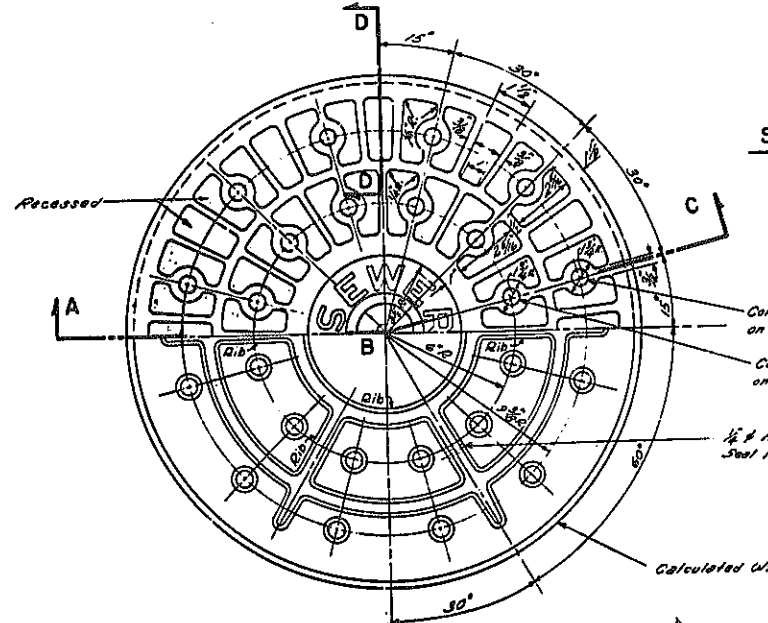
BAR LIST - MANHOLE SLABS					
MARK	SIZE	LENGTH	TYPE	NUMBER EACH SLAB	TOTAL FOR 13 MH
M617	#6	5'-0"	Str.	11	153
M618	#6	2'-6"	Str.	2	26
M619	#6	2'-3"	Str.	2	26
M620	#6	1'-6"	Str.	4	52
M621	#6	1'-3"	Str.	4	52

Bars Will Be Paid For Under Item 30S

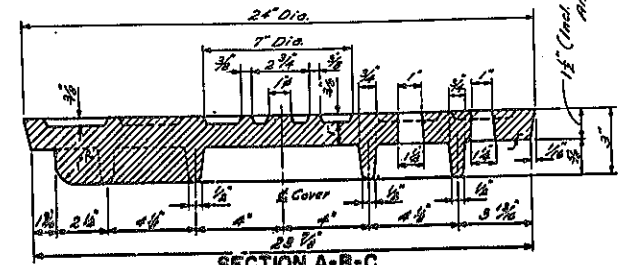


DETAIL AT CENTER OF COVER
SCALE: FULL SIZE

HALF PLAN
TOP OF COVER

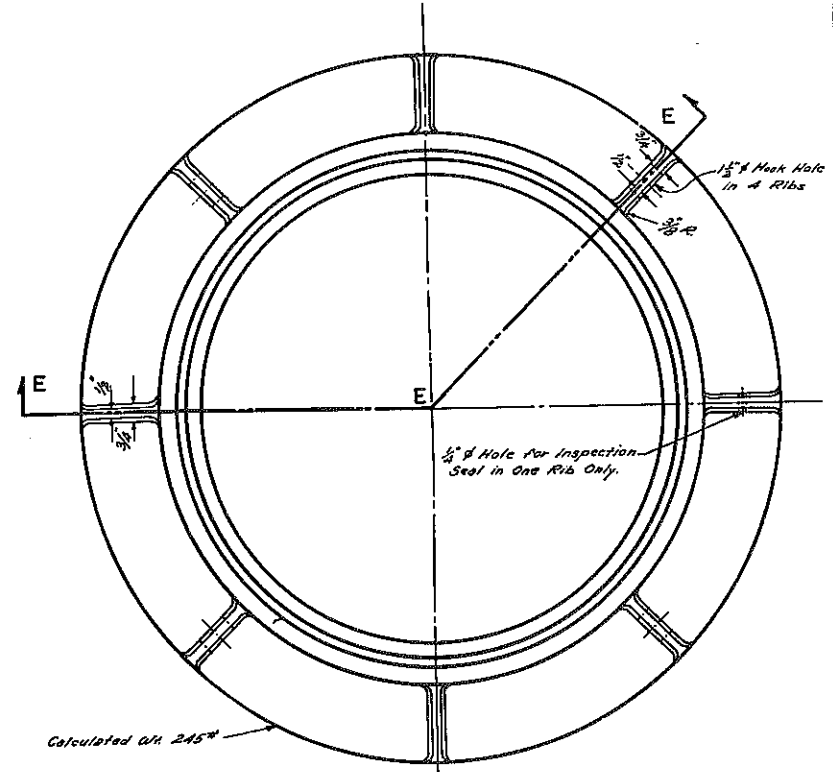


DETAIL B
HALF PLAN
UNDER SIDE OF COVER

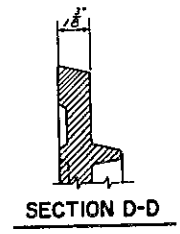


SECTION A-B-C
SCALE: 3/4"=1'-0"

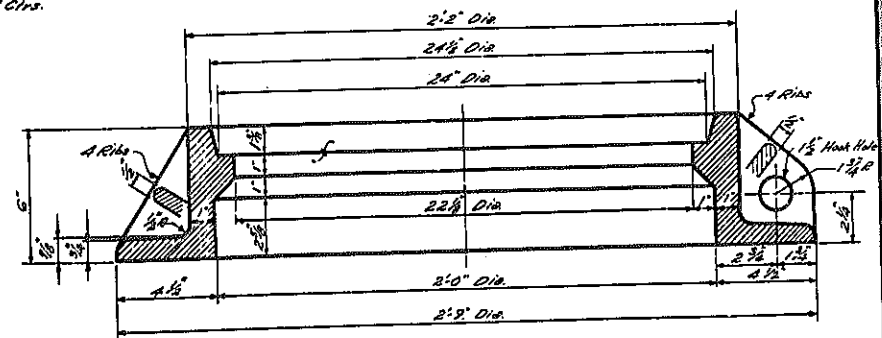
COVER PATTERN NO. B.S.A.-102



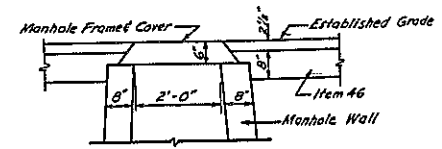
DETAIL C
PLAN
MANHOLE FRAME
SCALE: 3/4"=1'-0"



SECTION D-D

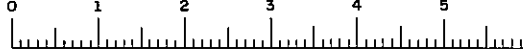


SECTION E-E-E



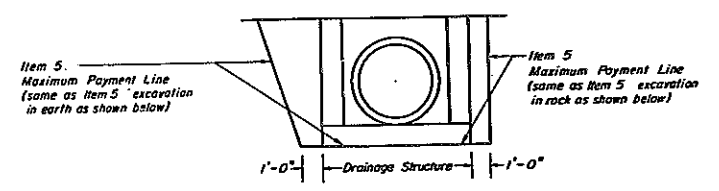
FRAME & COVER ON MANHOLE WALLS
SCALE: 1/2"=1'-0"

MANHOLE SLABS, FRAMES, COVERS & STEPS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY SEC. NO. III			
DE LEUW CATHAR & BRILL ENGINEERS - ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.K. J.R.B. S.C.R.	

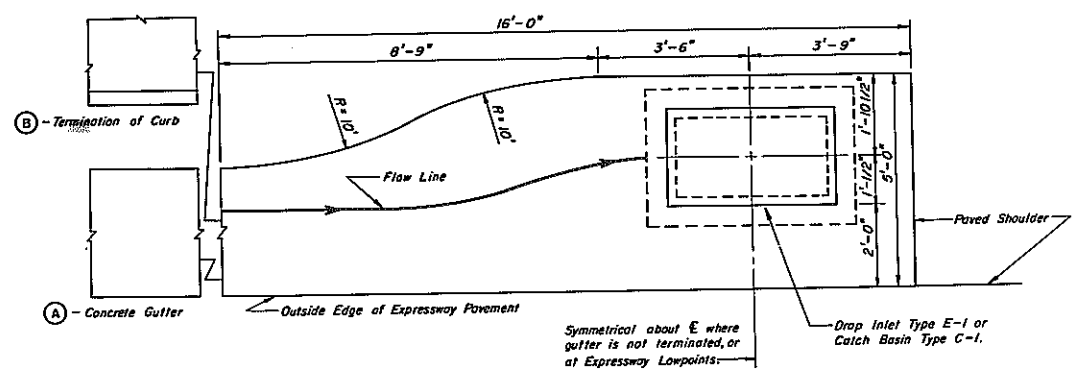


FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-U-0-377 (2x)	51	173

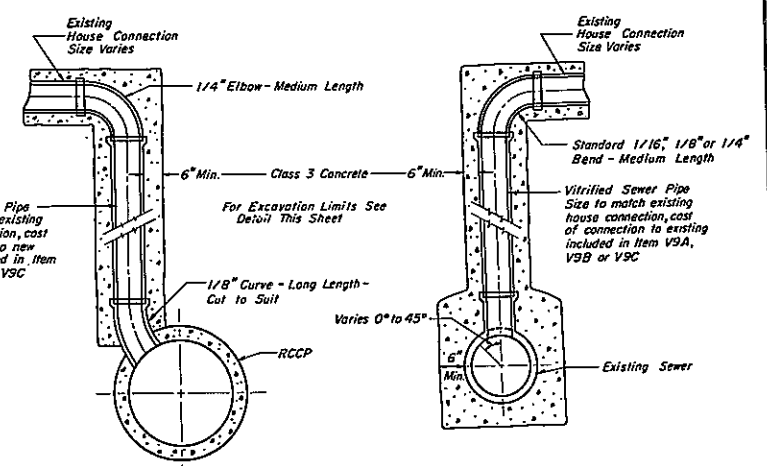
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



MANHOLES, CATCH BASINS & DROP INLET EXCAVATION
Not to Scale



CONCRETE GUTTER APRON AT CATCH BASINS & DROP INLETS
Scale: 1/2" = 1'-0"

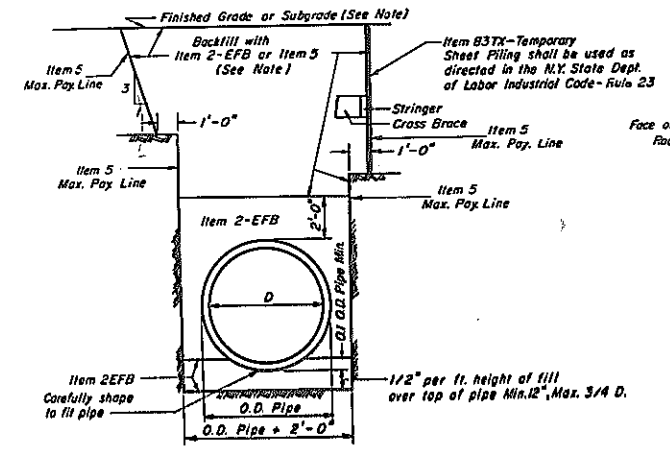


ITEM V9A, V9B or V9C STANDARD HOUSE LATERAL CONNECTION TO RCCP SEWER
Not to Scale

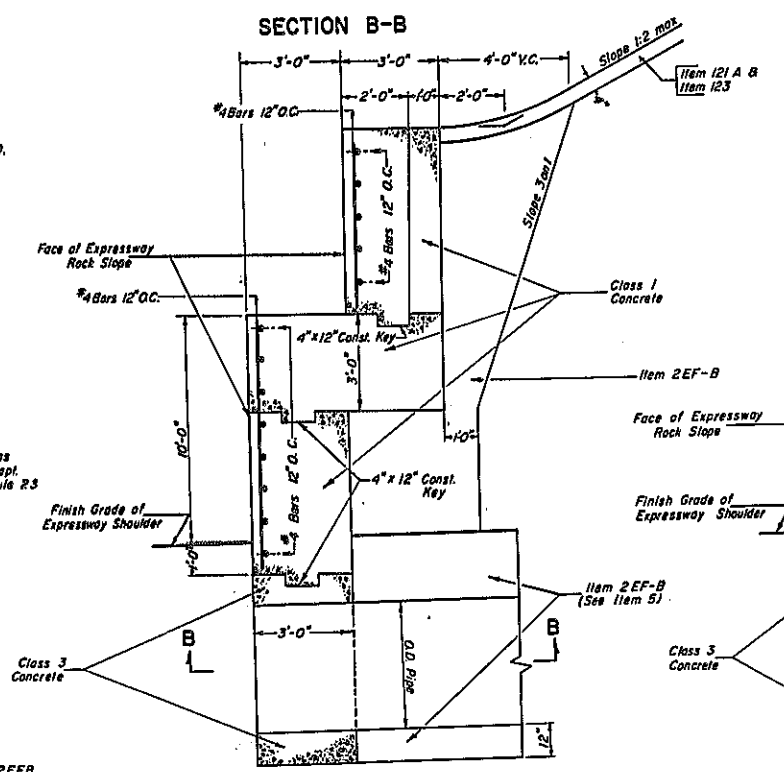
ITEM V9A, V9B or V9C STANDARD HOUSE LATERAL CONNECTION TO EXISTING SEWER
Not to Scale

LOCATION	LOCATION
E Loring - Oakgrove Ser Rd. 14 + 63	E Northbound Ser. Rd. 14 + 52
" " " " 15 + 18	" " " " 15 + 20
" " " " 15 + 84	" " " " 15 + 64
" " " " 16 + 22	" " " " 16 + 00
" " " " 16 + 47	" " " " 16 + 32
" " " " 17 + 16	" " " " 16 + 55
" " " " 17 + 80	" " " " 17 + 20

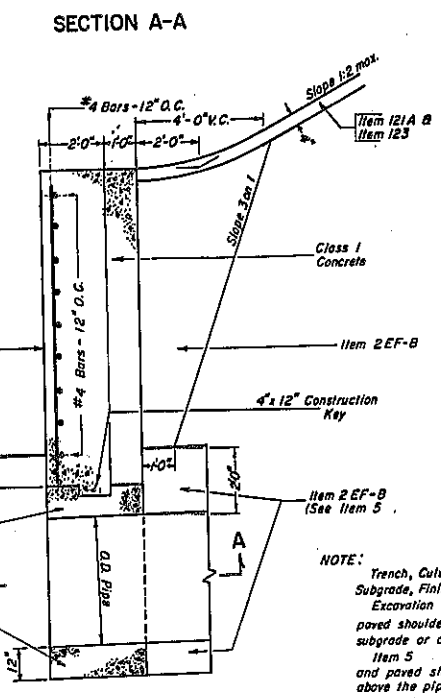
Contingent item - where construction disturbs existing connection



ITEM 5 EXCAVATION IN ROCK
Not to Scale

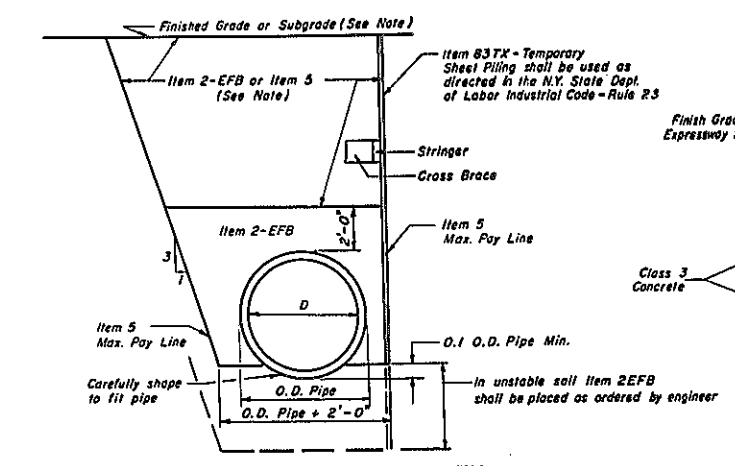


BENCHED ROCK SLOPE

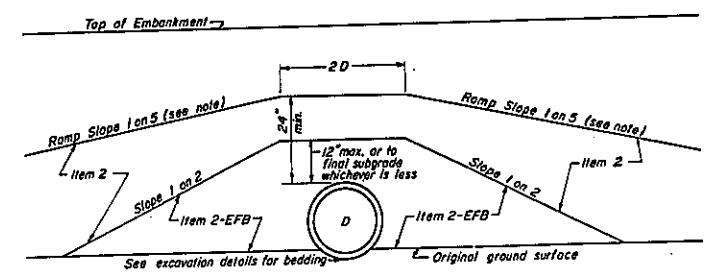


VERTICAL ROCK SLOPE

RESTORATION OF ROCK SLOPES
Scale: 3/8" = 1'-0"



ITEM 5 EXCAVATION IN EARTH
Not to Scale



PIPE PROTECTION FOR EQUIPMENT CROSSINGS
Not to Scale

NOTE: Trench, Culvert and Bridge excavation, Item 5 will be paid up to Subgrade, Finished Side Slope or Original Ground whichever is lower. Excavation under existing or new pavements, structures and paved shoulders shall be backfilled with Item 2-EFB to the pavement subgrade or as ordered by the Engineer.

Item 5 excavation not under existing or new pavements, structures and paved shoulders shall be backfilled with Item 2-EFB to 2' above the pipe.

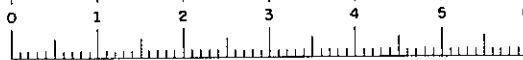
Item 5 excavation and Item 2-EFB backfill for Manhole, Drop Inlets and Catch Basins shall be the same as shown for sewers.

Item 5 excavation outside of shown payment lines shall be replaced with material as specified for backfilling and restoration of rock slopes at the contractors expense.

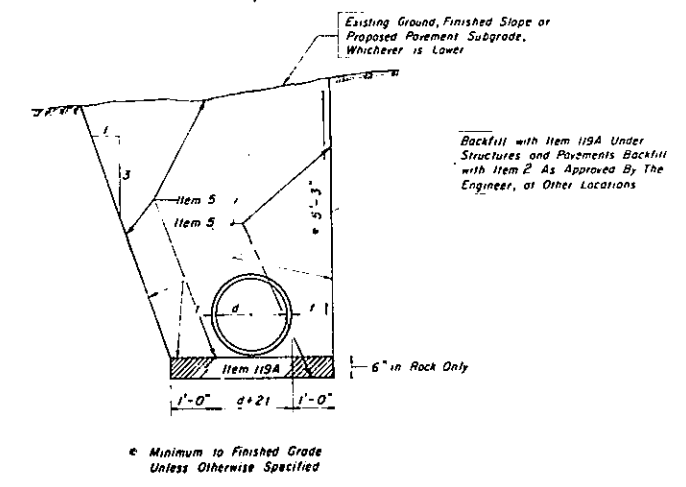
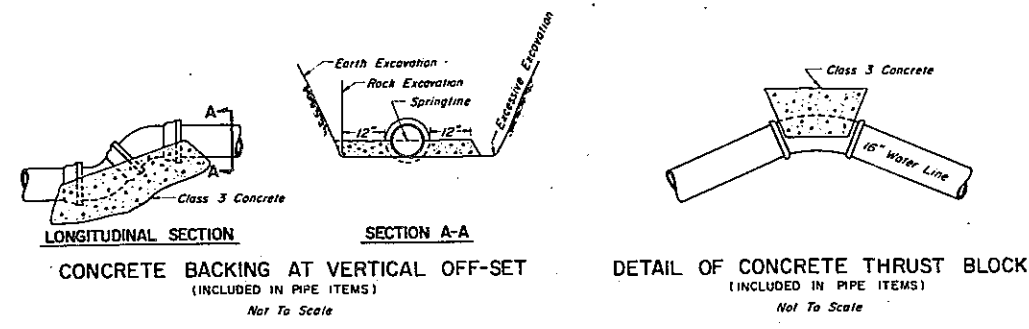
For detail of backfilling over pipe where construction equipment is to be allowed to cross over pipe installation before placement of adjacent embankment or foundation course material see Standard Sheet 61-19

For detail of pipe placed where unsuitable material is removed see Standard Sheet 61-19

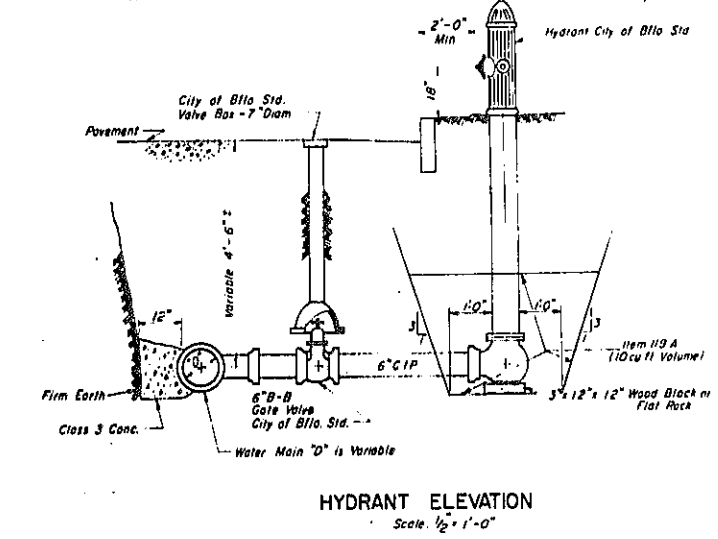
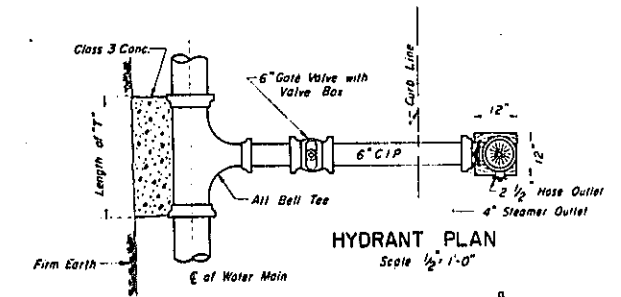
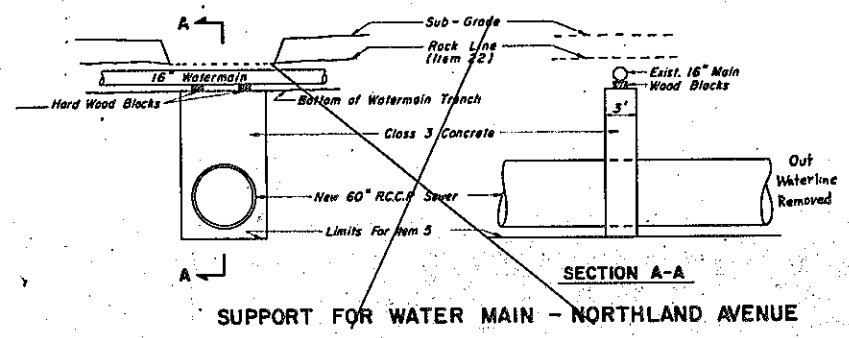
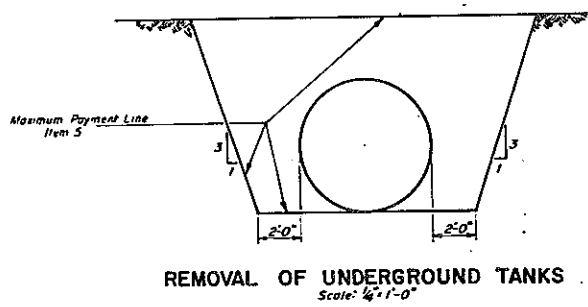
EXCAVATION LIMITS, CONCRETE GUTTER APRON, HOUSE CONNECTIONS AND RESTORATION OF ROCK SLOPES			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL HIGHWAY			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHEN & BRILL	DRAWN	J. K.	
ENGINEERS-ARCHITECTS	CHECKED	P. E. R.	
NEW YORK	TRACED	G. C. R.	



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	52R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



DETAIL OF WATER LINE EXCAVATION
Scale: 1/2" = 1'-0"



DESCRIPTION	SUMMARY OF ITEMS FOR WATER LINES											
	ITEM NO.											
	5	119A	200	202A	201B	201F	202B	205B	205C	209A	209B	5
SHEET NO. 38 - (new 6" main south of Northland Ave., new 6" main north of Northland Ave.)	809	802	-	-	705	-	4	4	-	13		
SHEET NO. 39 - (new 6" main continued to Delavan Ave., new 16" main across Delavan Ave. structure, new 6" main along Remp H-B.)	1974	768	560	1	1040	20	4	5	1	-	200 CONTINGENT	
SHEET NO. 40 - (new 6" main continued to connect with existing main in Loring - Oakgrove Service Rd.)	357	315	-	-	325	-	1	1	-	3		
SHEET NO. 41 - (new 16" main across Fillmore Ave. Structure.)	587	251	440	-	-	20	-	1	-	1		74
NEAT TOTAL	3727 C.Y.	2136 C.Y.	1000 L.F.	1 EA.	2070 L.F.	40 L.F.	9 EA.	11 EA.	1 EA.	17 EA.	200 L.F.	74 C.Y.

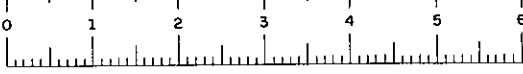
ITEM 209 A		
LOCATION	SIDE	
SBS 135 + 20	L.T.	
SBS 135 + 75	L.T.	
SBS 136 + 20	L.T.	
SBS 136 + 60	L.T.	
SBS 137 + 00	L.T.	
SBS 137 + 75	L.T.	
SBS 138 + 80	L.T.	
NBS 10 + 10	R.T.	
NBS 11 + 30	R.T.	
NBS 11 + 45	R.T.	
NBS 11 + 70	R.T.	
NBS 12 + 20	R.T.	
NBS 12 + 35	R.T.	
L-O 15 + 82	L.T.	
L-O 16 + 15	L.T.	
L-O 16 + 40	L.T.	
FI 8 + 80	R.T.	
NEAT TOTAL	17	

WATER DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

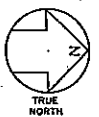
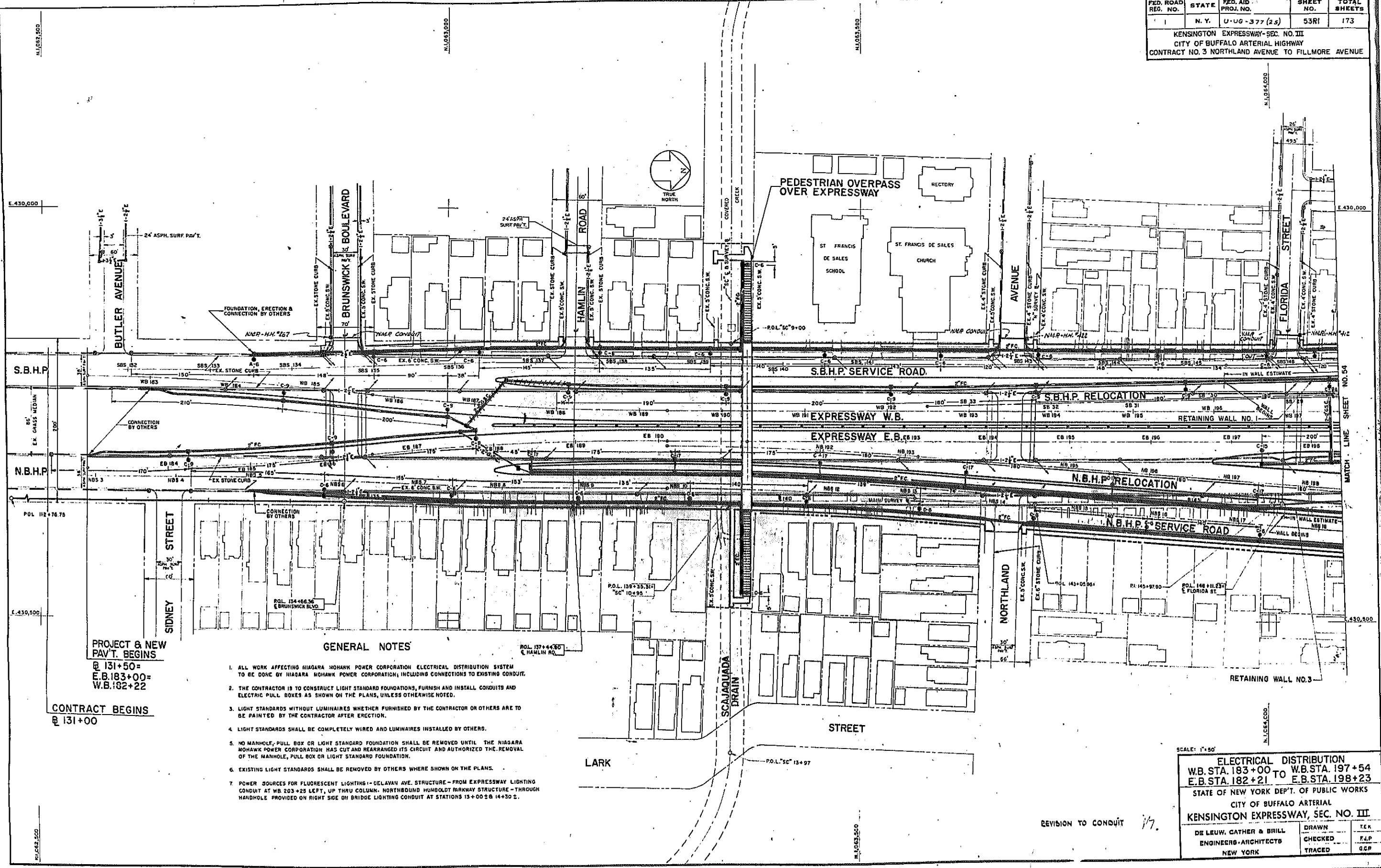
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	S.J.B. N.J.D. S.J.B.
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REVISION TO WATER MAIN 07



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	53R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PROJECT & NEW PAV'T. BEGINS @ 131+50 = E.B. 183+00 = W.B. 182+22

CONTRACT BEGINS @ 131+00

GENERAL NOTES

1. ALL WORK AFFECTING NIAGARA MOHAWK POWER CORPORATION ELECTRICAL DISTRIBUTION SYSTEM TO BE DONE BY NIAGARA MOHAWK POWER CORPORATION, INCLUDING CONNECTIONS TO EXISTING CONDUIT.
 2. THE CONTRACTOR IS TO CONSTRUCT LIGHT STANDARD FOUNDATIONS, FURNISH AND INSTALL CONDUITS AND ELECTRIC PULL BOXES AS SHOWN ON THE PLANS, UNLESS OTHERWISE NOTED.
 3. LIGHT STANDARDS WITHOUT LUMINAIRES WHETHER FURNISHED BY THE CONTRACTOR OR OTHERS ARE TO BE PAINTED BY THE CONTRACTOR AFTER ERECTION.
 4. LIGHT STANDARDS SHALL BE COMPLETELY WIRED AND LUMINAIRES INSTALLED BY OTHERS.
 5. NO MANHOLE, PULL BOX OR LIGHT STANDARD FOUNDATION SHALL BE REMOVED UNTIL THE NIAGARA MOHAWK POWER CORPORATION HAS CUT AND REARRANGED ITS CIRCUIT AND AUTHORIZED THE REMOVAL OF THE MANHOLE, PULL BOX OR LIGHT STANDARD FOUNDATION.
 6. EXISTING LIGHT STANDARDS SHALL BE REMOVED BY OTHERS WHERE SHOWN ON THE PLANS.
- POWER SOURCES FOR FLUORESCENT LIGHTING - DELAVAN AVE. STRUCTURE - FROM EXPRESSWAY LIGHTING CONDUIT AT WB 203+25 LEFT, UP THRU COLUMN. NORTHBOUND HUNDOLDT BRANWAY STRUCTURE - THROUGH HANDHOLE PROVIDED ON RIGHT SIDE ON BRIDGE LIGHTING CONDUIT AT STATIONS 13+00 & 14+30.2.

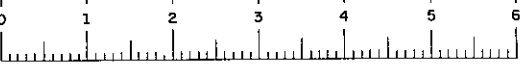
SCALE: 1"=50'

ELECTRICAL DISTRIBUTION
W.B. STA. 183+00 TO W.B. STA. 197+54
E.B. STA. 182+21 TO E.B. STA. 198+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

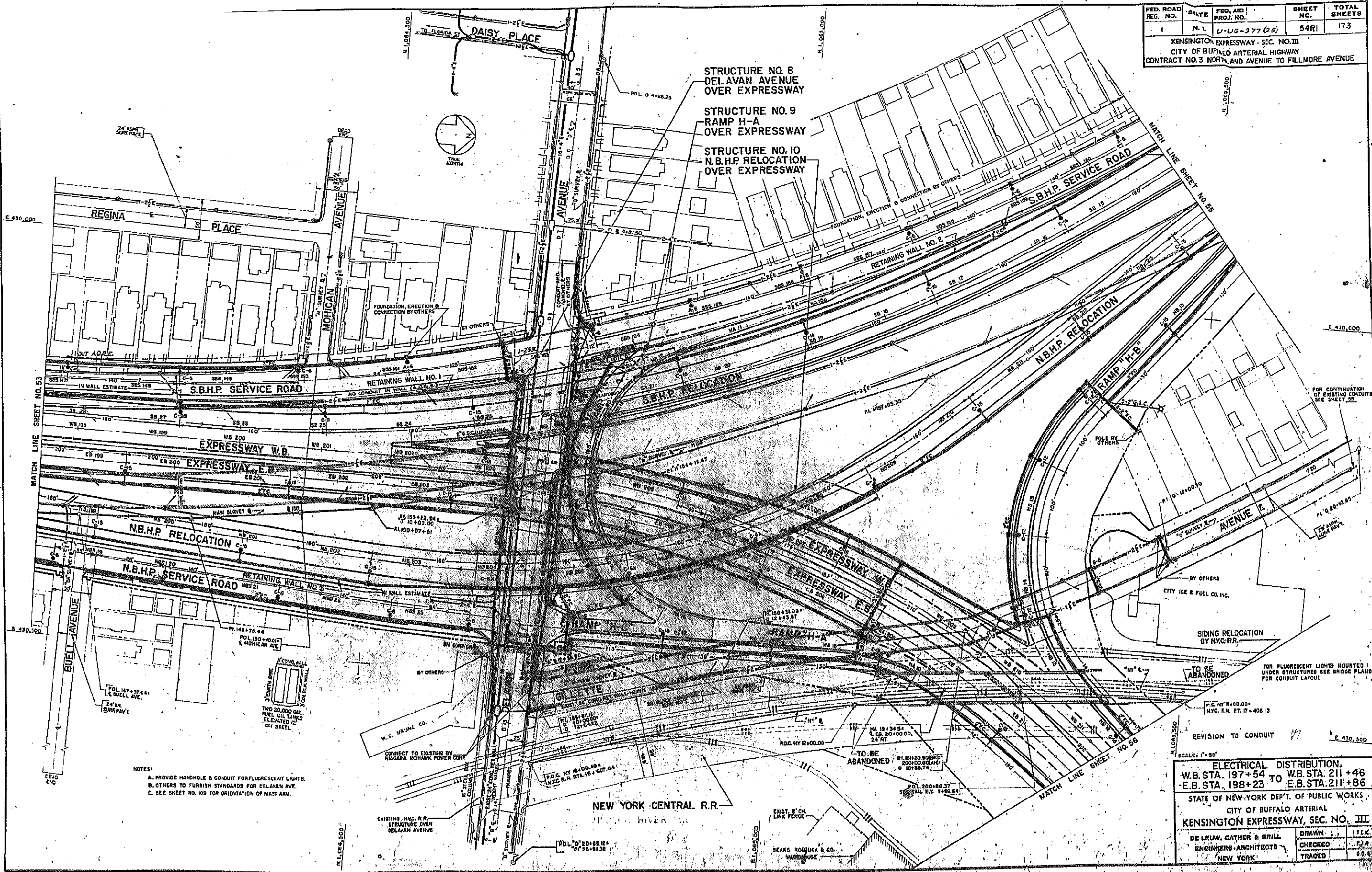
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TKR F.J.P. G.C.P.
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REVISION TO CONDUIT 17.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377 (25)	54RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTH AND AVENUE TO FILLMORE AVENUE



NOTES:
 A. PROVIDE HANDHOLE & CONDUIT FOR FLUORESCENT LIGHTS.
 B. OTHERS TO FURNISH STANDARDS FOR DELAVAN AVE.
 C. SEE SHEET NO. 109 FOR ORIENTATION OF MAST ARM.

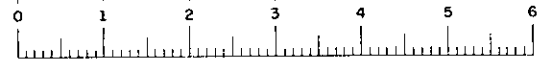
REVISION TO CONDUIT 1/1

SCALE: 1" = 50'

ELECTRICAL DISTRIBUTION.
 W.B. STA. 197+54 TO W.B. STA. 211+46
 E.B. STA. 198+23 E.B. STA. 211+86

STATE OF NEW-YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

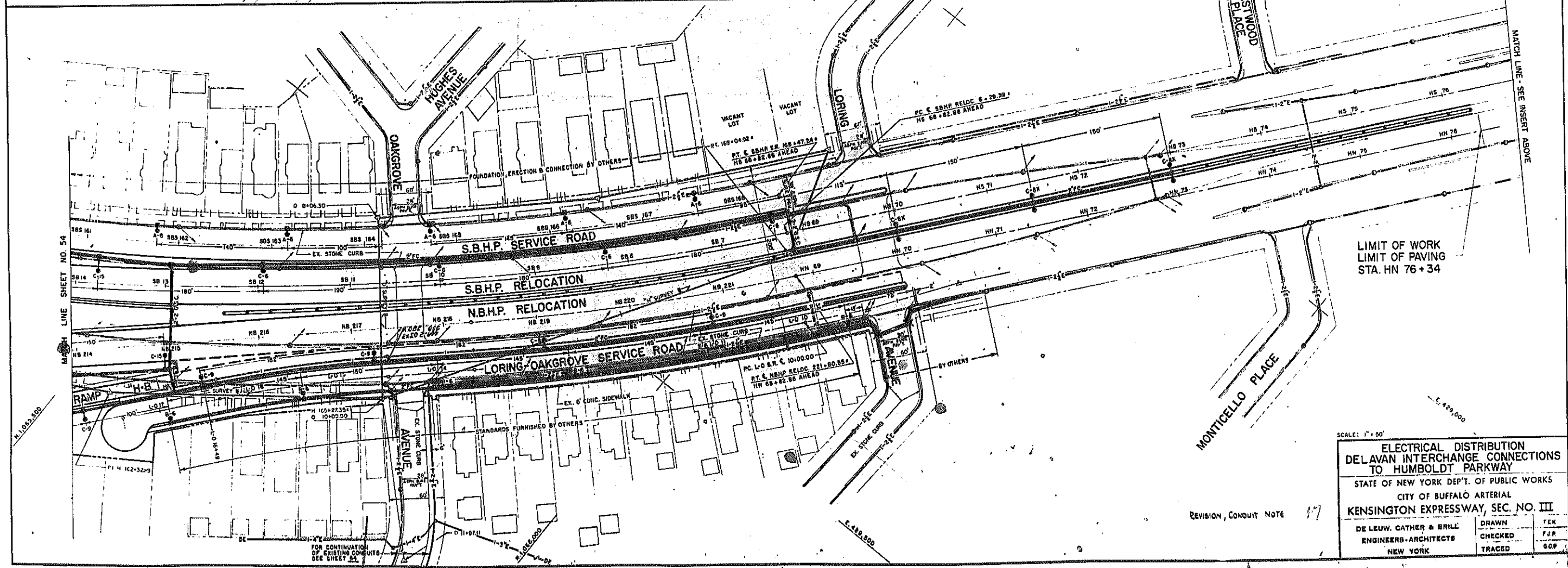
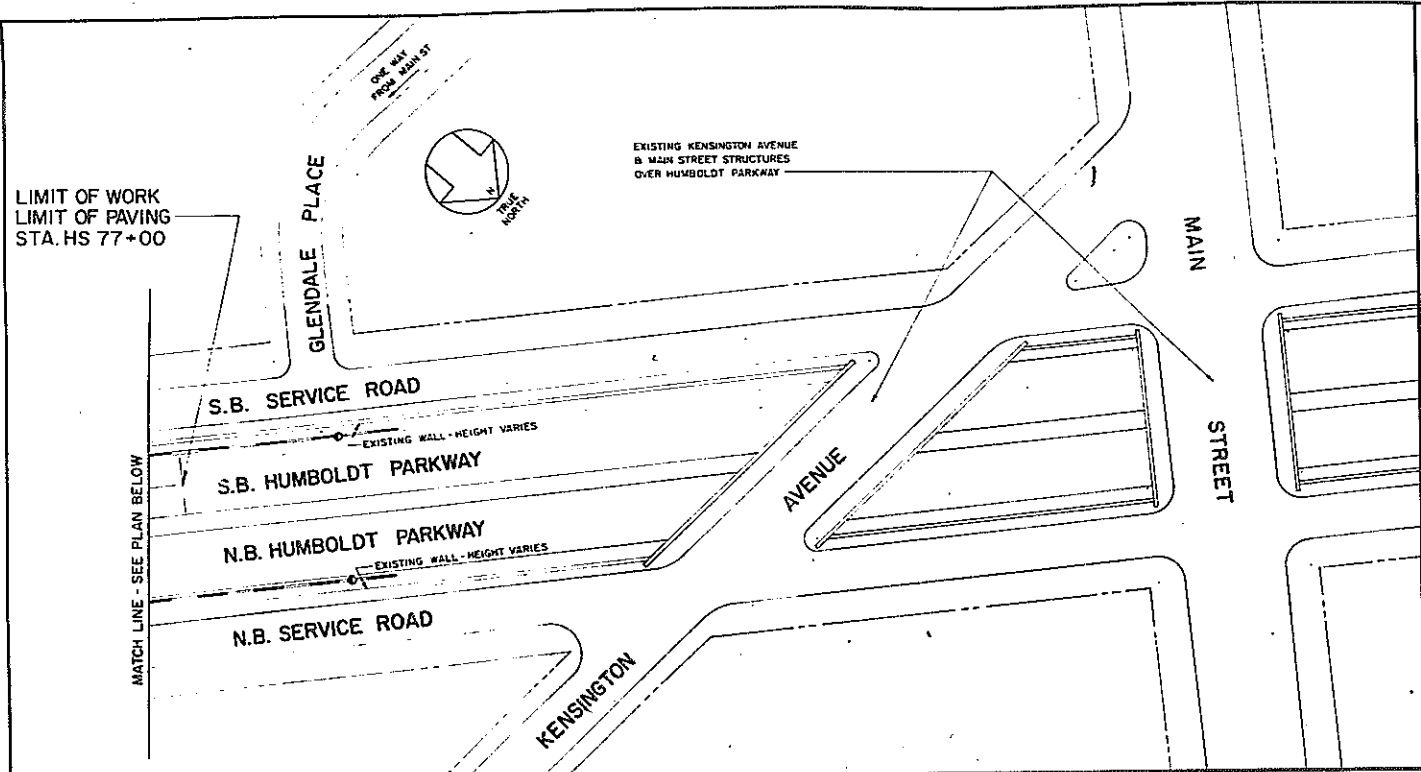
DE LEUW, CATHY & GRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	Y.T.K. R.J.P. G.P.F.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
I	N. Y.	U-UG-377 (25)	55 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

LIMIT OF WORK
 LIMIT OF PAVING
 STA. HS 77+00



LIMIT OF WORK
 LIMIT OF PAVING
 STA. HN 76+34

SCALE: 1" = 50'

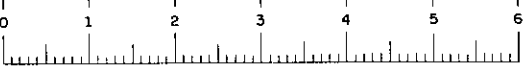
**ELECTRICAL DISTRIBUTION
 DELAVAN INTERCHANGE CONNECTIONS
 TO HUMBOLDT PARKWAY**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TEK F.J.P. G.O.P.
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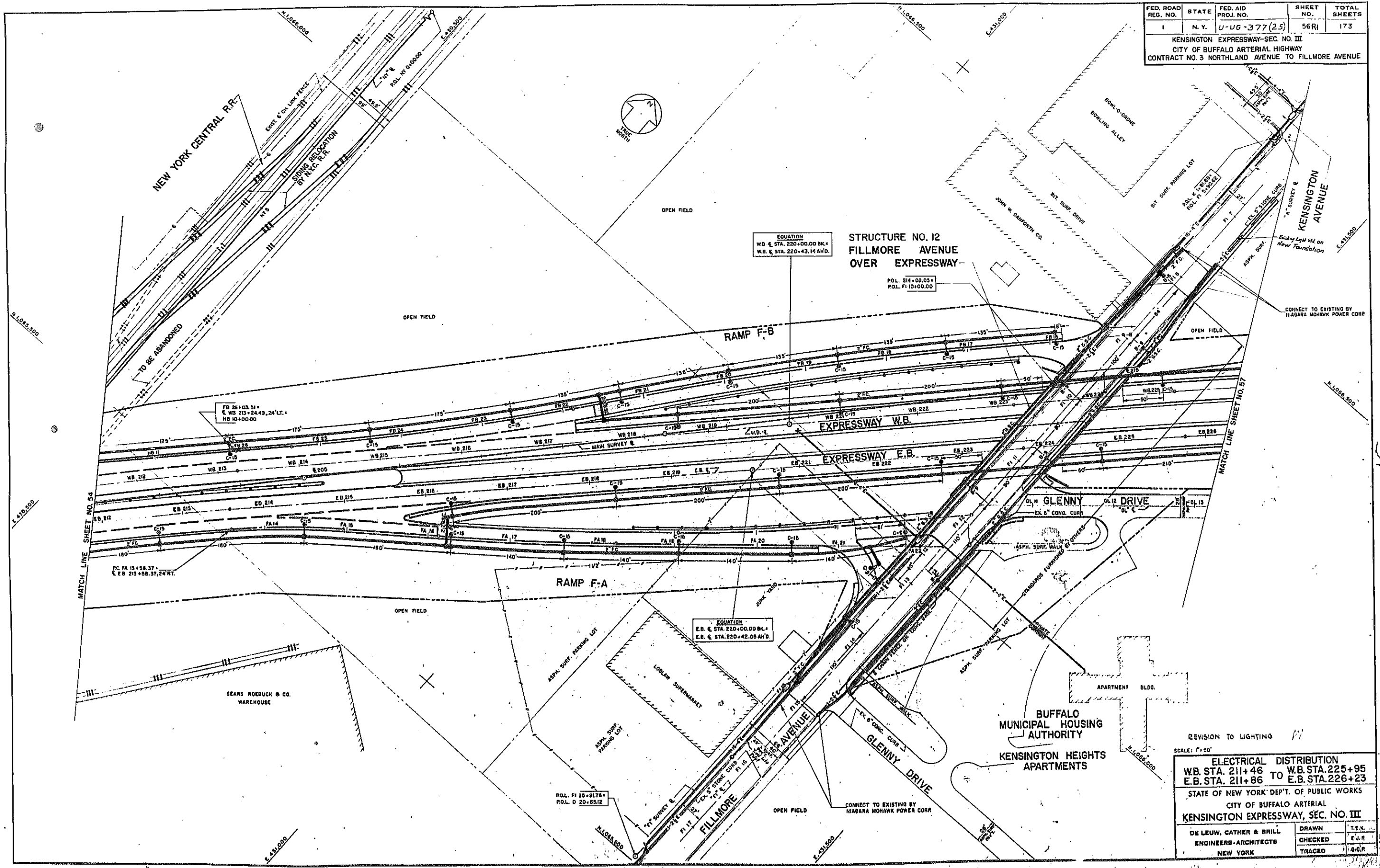
REVISION, CONDUIT NOTE 17

FOR CONTINUATION
 OF EXISTING CONDUITS
 SEE SHEET 54



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	56R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



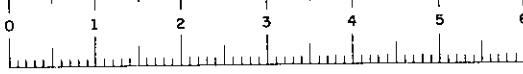
REVISION TO LIGHTING *11*

SCALE: 1" = 50'

ELECTRICAL DISTRIBUTION	
W.B. STA. 211+46	TO W.B. STA. 225+95
E.B. STA. 211+86	TO E.B. STA. 226+23

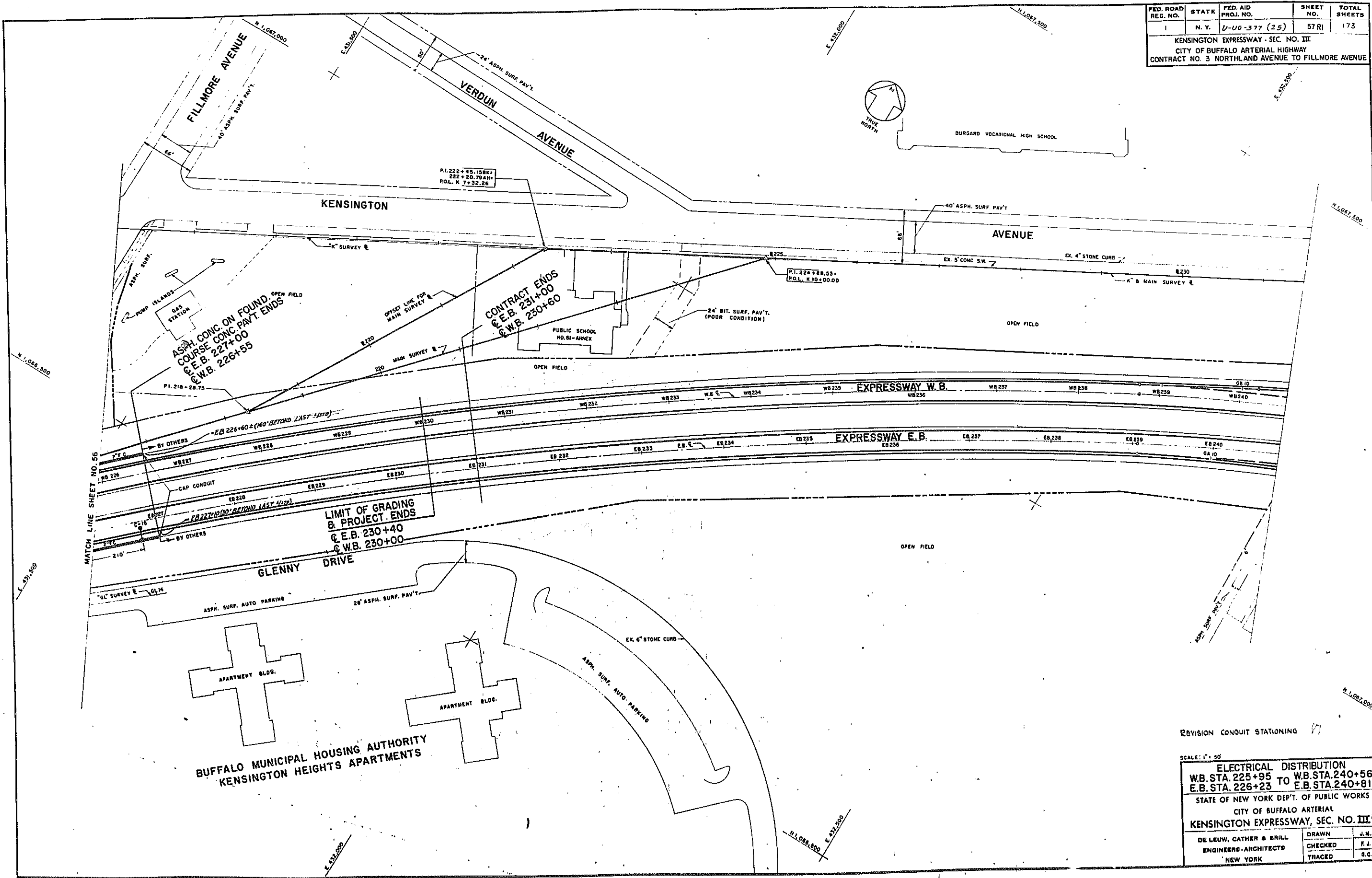
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	T.E.S.
ENGINEERS-ARCHITECTS	CHECKED	E.J.R.
NEW YORK	TRACED	A.G.R.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	57 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



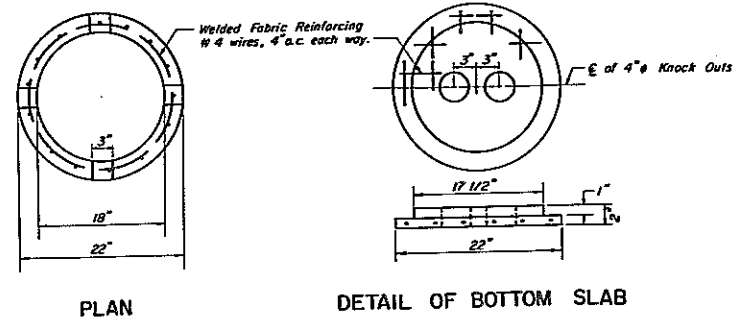
MATCH LINE SHEET NO. 56

REVISION CONDUIT STATIONING

SCALE: 1" = 50'

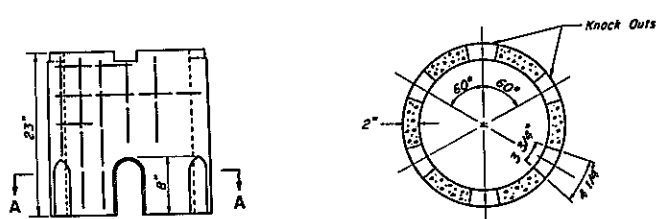
ELECTRICAL DISTRIBUTION	
WB. STA. 225+95 TO WB. STA. 240+56	WB. STA. 240+81
E.B. STA. 226+23	E.B. STA. 240+81
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL	DRAWN J.W.B.
ENGINEERS-ARCHITECTS	CHECKED F.J.R.
NEW YORK	TRACED G.C.R.

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-100-377 (25)	58	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



PLAN

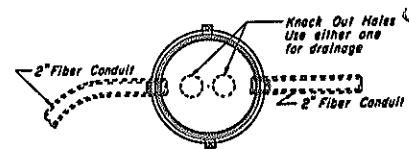
DETAIL OF BOTTOM SLAB



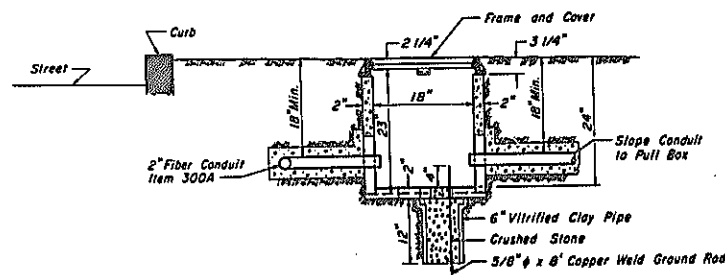
ELEVATION

SECTION A-A

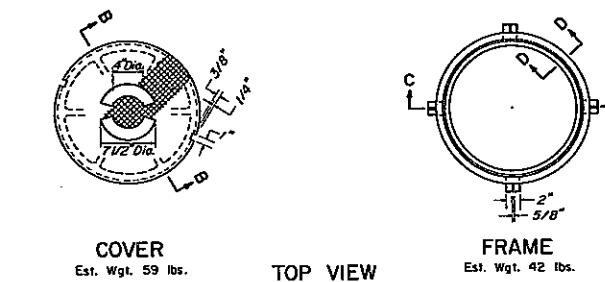
DETAILS OF 18" ROUND PRECAST PULL BOX
ITEM 304A



TOP VIEW OF PULL BOX
WITH COVER REMOVED



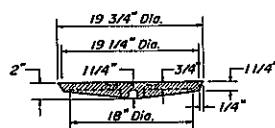
TYPICAL INSTALLATION OF
18" ROUND PRECAST PULL BOX
ITEM 304A



COVER
Est. Wgt. 59 lbs.

TOP VIEW

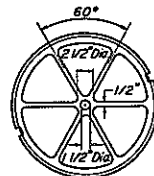
FRAME
Est. Wgt. 42 lbs.



SECTION B-B
COVER



SECTION C-C
FRAME

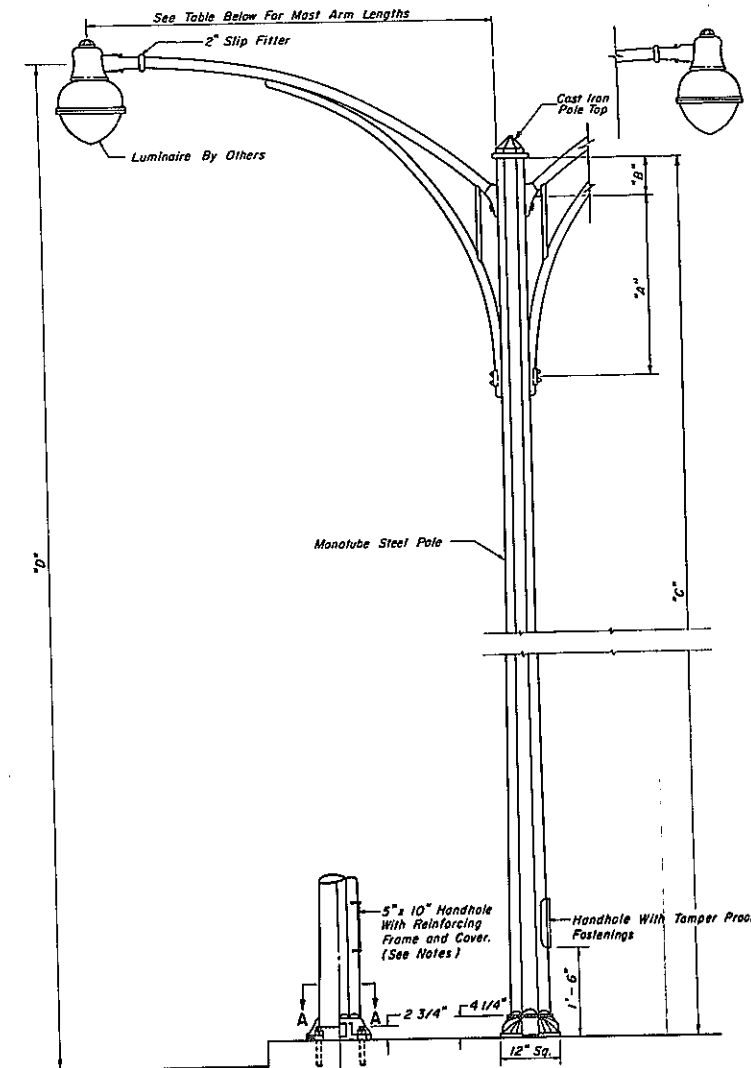


BOTTOM VIEW
COVER

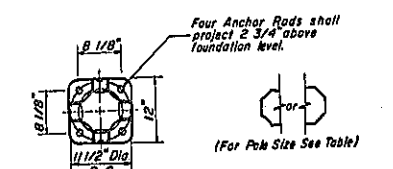


SECTION D-D
FRAME

DETAILS OF FRAME & COVER
FOR 18" ROUND PRECAST PULL BOX
ITEM 304A



LIGHT STANDARD DETAILS



SECTION A-A SECTION THRU
POLE

NOT TO SCALE

LIGHT STANDARDS DATA

ITEM NO.	QUANTITY	TYPE	MAST ARM LENGTH	DIMENSION		MIN. POLE LENGTH	MTS. HT.	POLE SIZE	LUMINAIRE		AVERAGE FT.-C. REQUIRED
				"A"	"B"				TYPE	LUMENS	
305 A	13	AE	8'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.8"	MERCURY VAPOR	20,000	0.6 or 0.8
305 B	16	B	8'-0"	AS	FURNISHED	30'-0"	30'-0"	AS FURN.	MERCURY VAPOR	20,000	0.8
305 C6	58	C6	6'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.8"	MERCURY VAPOR	20,000	0.8
305 C6X	3	C6X	8'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.8"	MERCURY VAPOR	20,000	0.8
305 C6X	3	C6X	8'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.8"	MERCURY VAPOR	20,000	0.8
305 C8	17	C8	9'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.75"	MERCURY VAPOR	20,000	0.8
305 C8A	1	C8A	9'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.75"	MERCURY VAPOR	20,000	0.8
305 C8B	1	C8B	9'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.75"	MERCURY VAPOR	20,000	0.8
305 C12	4	C12	12'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.75"	MERCURY VAPOR	20,000	0.8
305 C16	56	C16	15'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.75"	MERCURY VAPOR	20,000	0.8
305 C17	6	C17	17'-0"	33"	8"	27'-0"	30'-0"	8.5" x 4.95"	MERCURY VAPOR	20,000	0.8

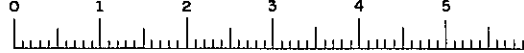
W WALL MOUNTING
ALL POLES SHALL BE 11 GAUGE EXCEPT C16 & C17 WHICH SHALL BE 7 GAUGE.

NOTES:

- Where Light Standards are located within two feet of an obstruction, the handholes shall be placed on the same side as the mast arm.
- Where Light Standards are located on structures (bridges) the handholes shall be placed on the same side as the mast arm.
- For Conduits to Fluorescent Lights under structures see bridge plans. Fluorescent Lights and their mountings furnished and erected by others.

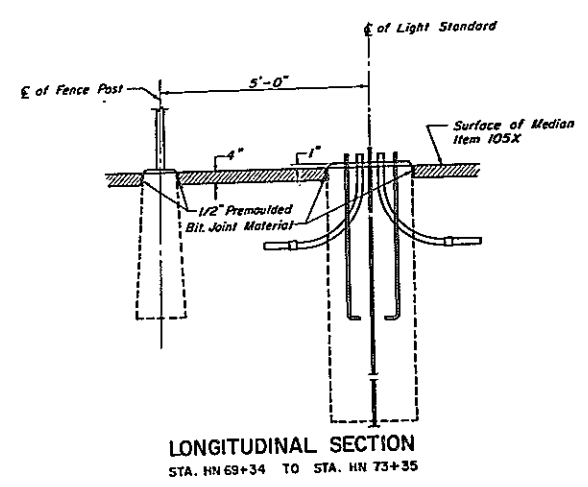
ELECTRICAL DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED
	G.J.B. J.J.R. S.V.R.

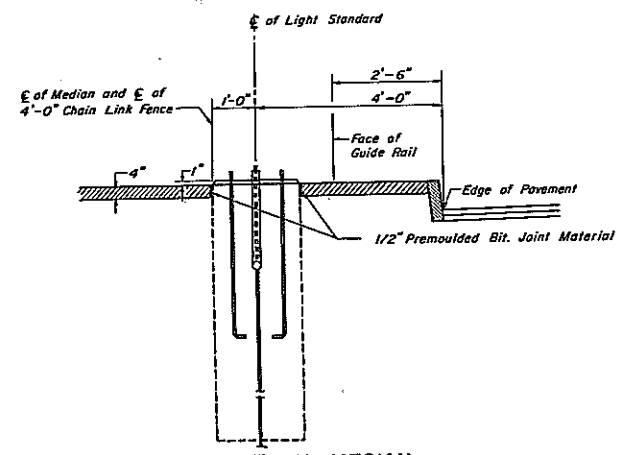


FED. ROAD RES. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UB-377 (25)	69	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

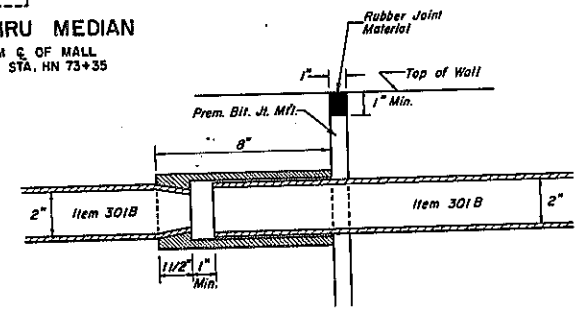


LONGITUDINAL SECTION
STA. HN 69+34 TO STA. HN 73+35

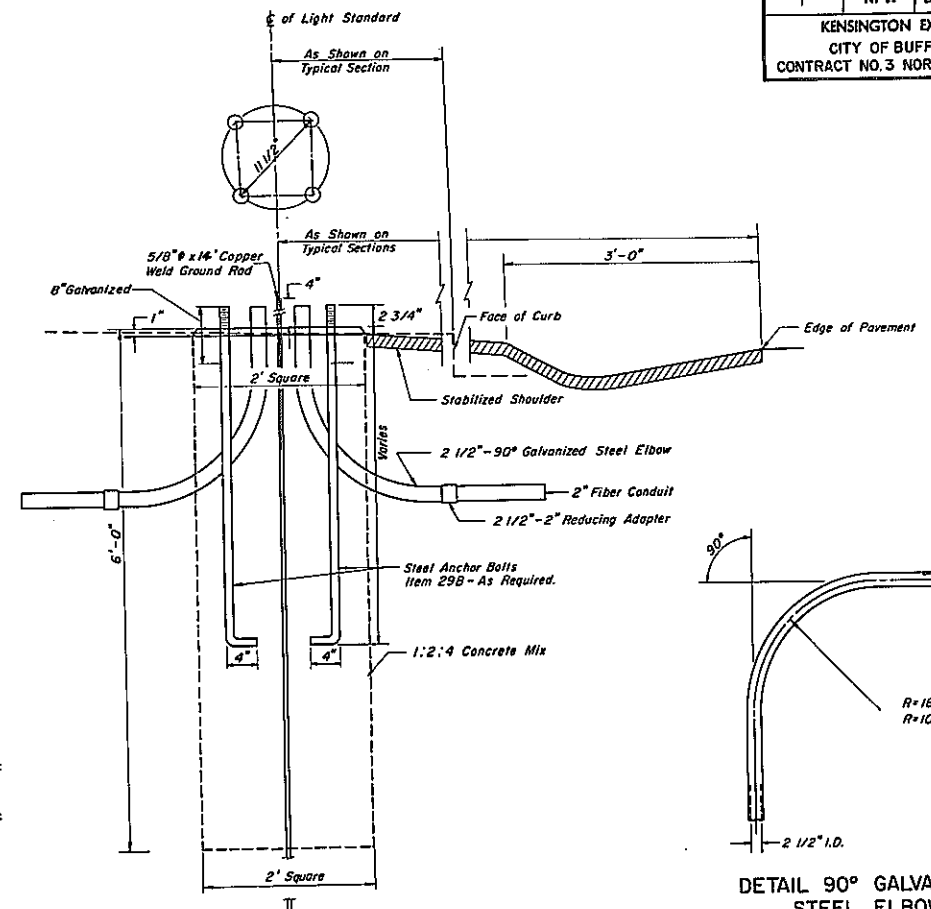


SECTION THRU MEDIAN
OFF-SET FROM E OF MALL
STA. HN 69+34 TO STA. HN 73+35

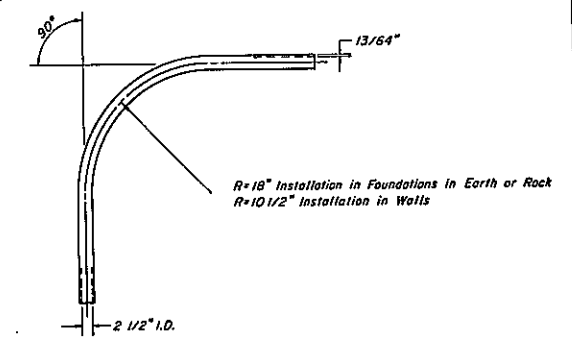
LIGHT STANDARD FOUNDATION IN MEDIAN
ITEM 302



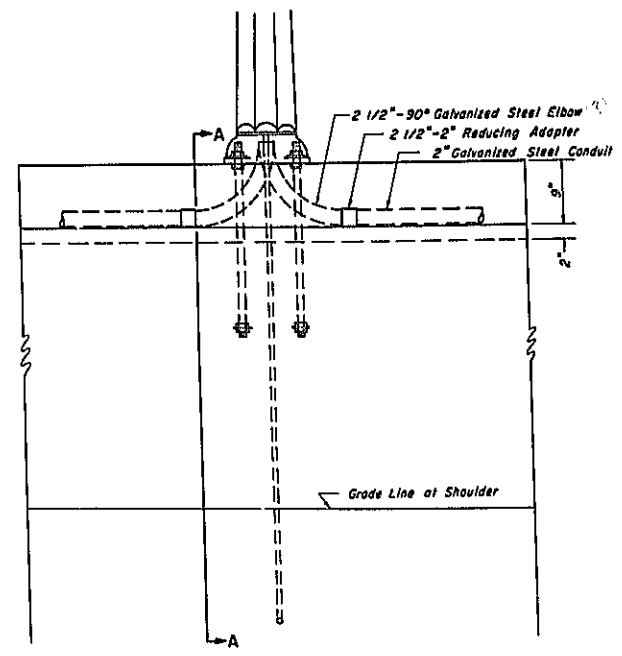
EXPANSION COUPLING



LIGHT STANDARD FOUNDATION IN EARTH
ITEM 302

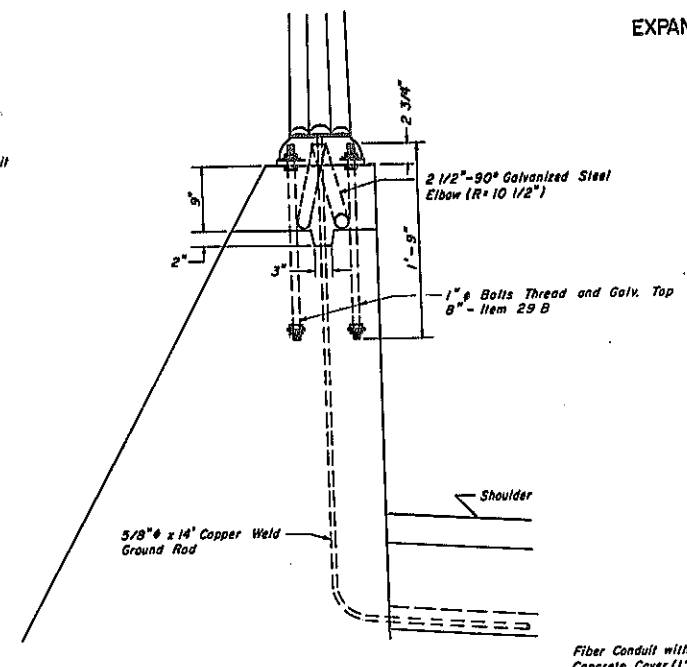


DETAIL 90° GALVANIZED STEEL ELBOW
ITEM 301C

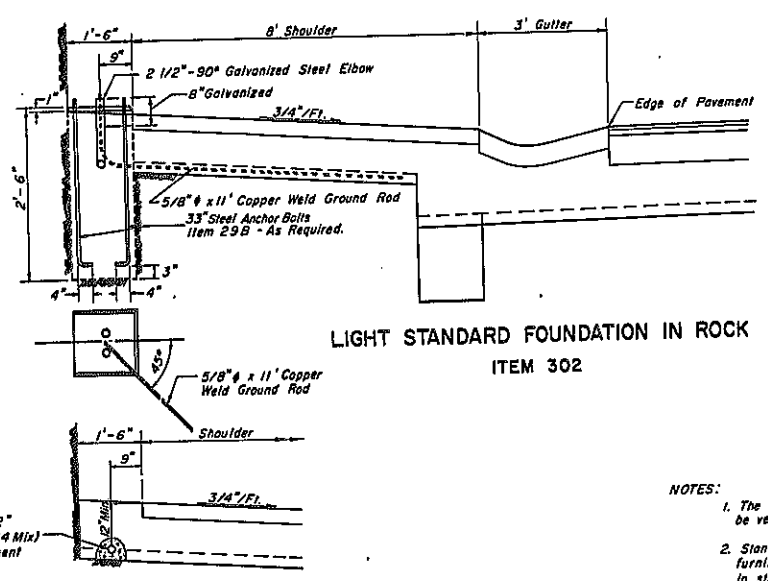


LONGITUDINAL SECTION

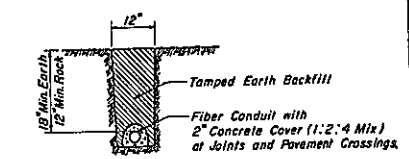
DETAIL OF LIGHT STANDARD MOUNTED ON TOP OF RETAINING WALL



SECTION A-A



FIBER CONDUIT INSTALLATION IN ROCK
ITEM 300A

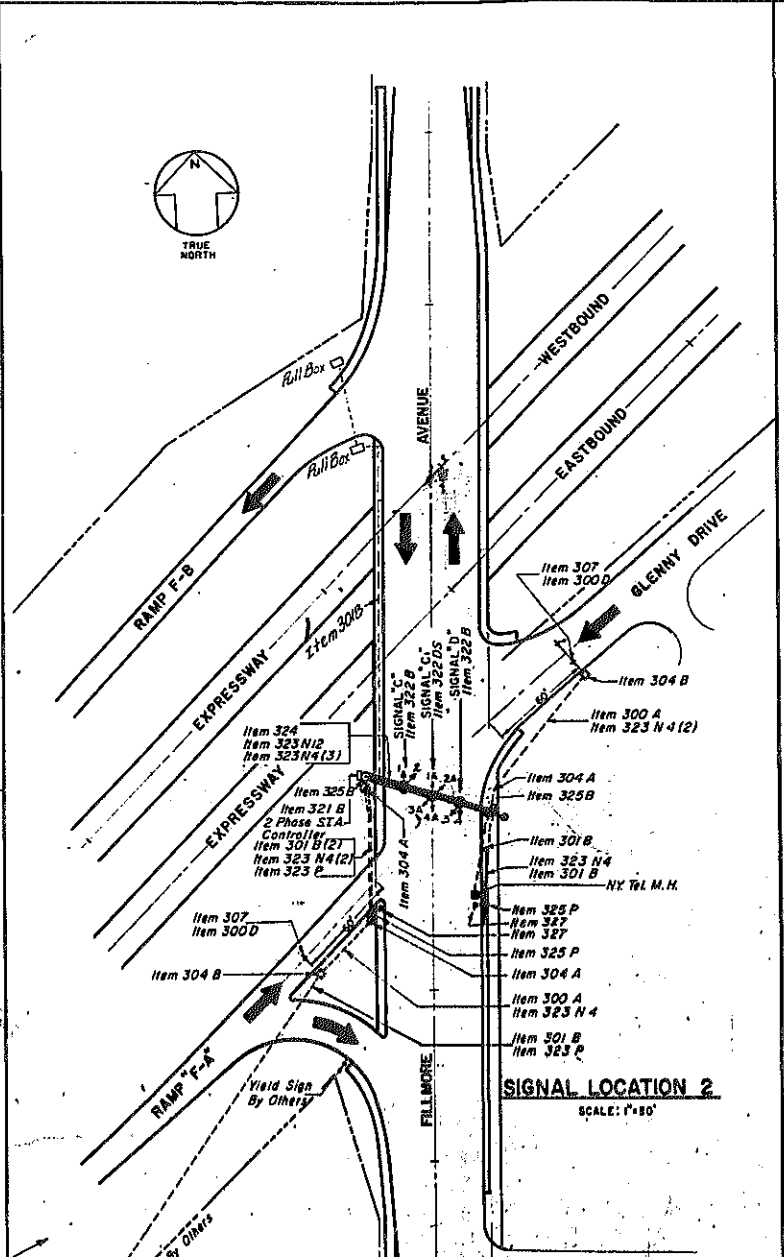
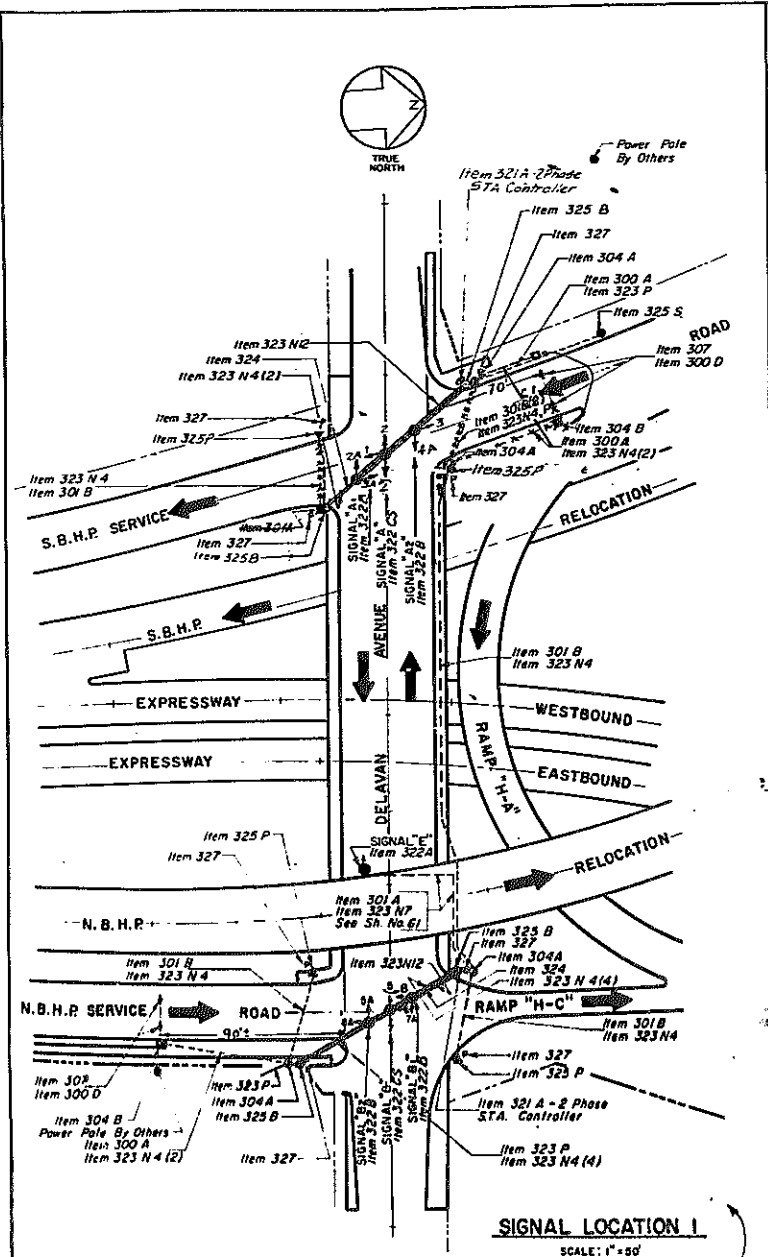
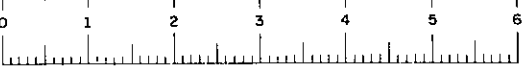


TYPICAL FIBER CONDUIT INSTALLATION IN TRENCH
ITEM 300A

- NOTES:
- The arrangement and size of the anchor bolts to be verified by the supplier of the light standards.
 - Standard conduit expansion fittings shall be furnished and installed at all expansion joints in structures and at locations where a conduit emerges from a structure.
 - When Light Standard Foundations are placed in sidewalk, the chamfer shall be eliminated from the top of the foundation and the top of the foundation shall be flush with the surface of the sidewalk.
 - All concrete Light Standard Foundations shall be Class I concrete.

NOT TO SCALE

ELECTRICAL DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G.S.B. F.S.B. C.S.



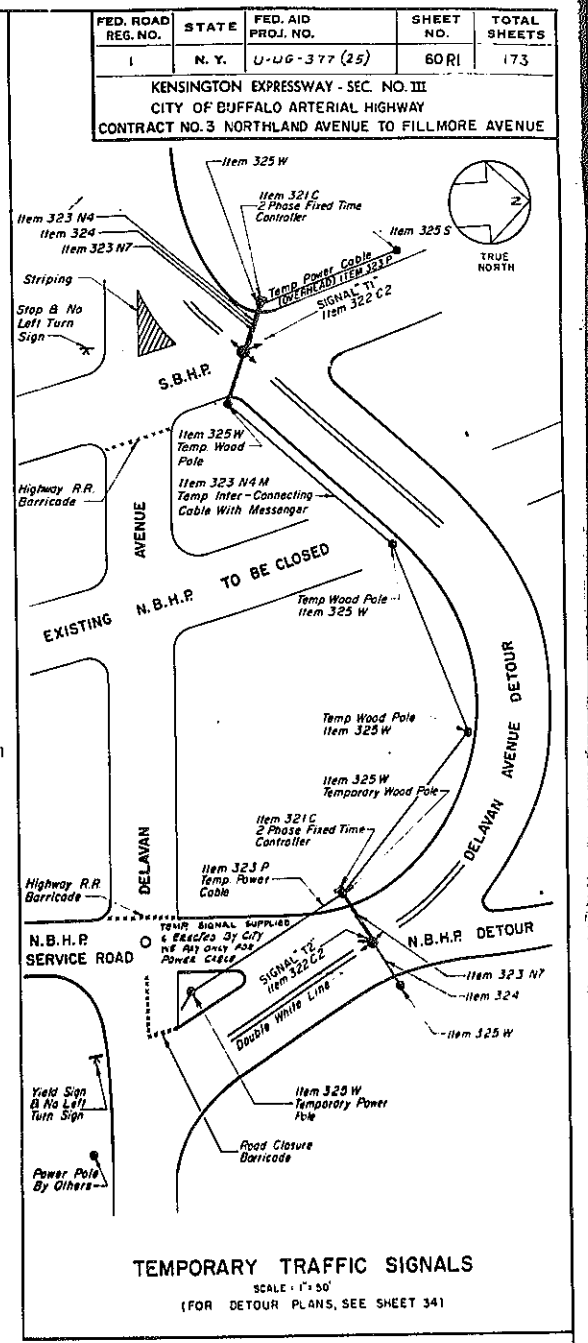
TYPE OF SIGNAL HEAD & CONTROLLER			
SIGNAL NO.	TYPE	DESCRIPTION	CONTROLLER
A	CS	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
A1	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
A2	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
B	CS	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
B1	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
B2	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
C	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
C1	DS	4 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
D	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
E	A	1 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
T1	C2	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE FIX. T.
T2	C2	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE FIX. T.

* FACES 4 & 5 TO HAVE EXTENDED GREEN WITH 12" ARROW IN BOTTOM
 ☉ FACE 3 TO HAVE EXTENDED GREEN WITH 12" ARROW IN BOTTOM

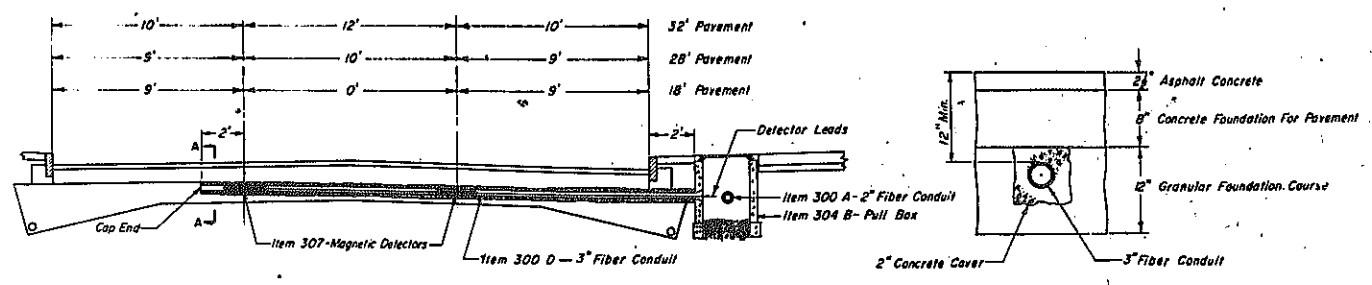
LEGEND		
SYMBOL	ITEM	DESCRIPTION
○	300 A	2" FIBER CONDUIT
○	300 D	3" FIBER CONDUIT
○	301 A	1 1/2" GALVANIZED STEEL CONDUIT
○	301 B	2" GALVANIZED STEEL CONDUIT
○	301 C	2 1/2" GALVANIZED STEEL CONDUIT
○	304 A	FURNISH & INSTALL PULL BOX
○	304 B	FURNISH & INSTALL PULL BOX (MAGNETIC DETECTOR)
○	307	MAGNETIC DETECTOR (NON DIRECTIONAL)
○	321 A, B, C	CONTROLLER
○	322	ADJUSTABLE SIGNALS
○	323	CABLES
○	324	SPAN WIRE
○	325 B	20' STEEL POLE (ANCHOR BASE TYPE)
○	325 P	PUSH BUTTON POLE
○	325 S	20' STEEL POLE (ANCHOR BASE TYPE)
○	325 W	35' TEMPORARY WOOD POLE
○	327	PEDESTRIAN PUSH BUTTON WITH SIGN

GENERAL NOTES - TRAFFIC SIGNALS

- NO DUPLICATIONS ARE PERMITTED. SPLICES SHALL BE MADE IN PULL BOXES. THE CONTRACTOR IS CAUTIONED TO ACCURATELY DETERMINE THE NECESSARY CABLE LENGTH BEFORE CUTTING CABLE.
- IF ELECTRIC POWER IS TO BE TAKEN FROM UTILITY COMPANY MANHOLE, THE CONTRACTOR SHALL EXTEND, AFTER APPROVAL FROM POWER COMPANY, THE CONDUIT AND POWER CABLE INTO MANHOLE WITH A 4' EXTENSION ON THE CABLE. THE UTILITY COMPANY SHALL MAKE ALL POWER CABLE CONNECTIONS TO LIVE WIRES.
- THE MAGNETIC DETECTORS SHALL BE LOCATED AS SHOWN ON THE PLANS. THE FINAL LOCATION OF THE DETECTOR SHALL BE DETERMINED BY PLACING THE DETECTOR AT THE LOCATION WHICH GIVES THE MAXIMUM EFFECT FOR VEHICLES TO BE DETECTED AND THE MINIMUM EFFECT FOR ALL OTHER VEHICLE MOVEMENTS.
- THE PROPOSED POLE LOCATIONS SHOWN ON PLAN ARE APPROXIMATE. THE EXACT LOCATIONS OF POLES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- FORMING OF POLE FOUNDATIONS WILL NOT BE PERMITTED.
- ANCHOR BOLTS SHALL BE AS RECOMMENDED BY THE POLE MANUFACTURER.
- ELEVATION OF TOP OF CONCRETE FOUNDATION FOR ANCHOR BASE POLES SHALL ALLOW FOR ACCESS TO ADJUSTING NUTS UNDER BASE PLATE. AFTER POLE IS ADJUSTED, FOOTING SHALL BE BROUGHT TO FINISHED GRADE WITH MORTAR.
- CONTROLLER CABINETS SHALL BE RADIALLY MOUNTED ON POLES A.O.B.E.
- SIGNALS ON FILLMORE AVENUE TO BE INTERCONNECTED WITH EXISTING KENSINGTON AVENUE AND GLENNY DRIVE SIGNALS, BY OTHERS.
- PAVEMENT MARKINGS, ROAD CLOSURE BARRICADES AND SIGNS REQUIRED IN CONJUNCTION WITH TEMPORARY DETOUR TRAFFIC SIGNALS SHALL BE FURNISHED AND ERECTED BY THE CONTRACTOR UNDER ITEM 785.
- THE CONTRACTOR WILL PROVIDE THE RIGHT OF ACCESS TO THE SIGN CONTRACTOR WORKING UNDER A SEPARATE STATE CONTRACT.
- CONDUITS SHALL BE DRAINED TO THE PULL BOXES.



TEMPORARY TRAFFIC SIGNALS
 SCALE: 1" = 50'
 (FOR DETOUR PLANS, SEE SHEET 341)



ITEM NO. 307
MAGNETIC DETECTOR - NON-DIRECTIONAL
 (LOOKING IN DIRECTION OF TRAVEL, GLENNY DRIVE & S.B.H.P. SERVICE ROAD OPPOSITE HAND)
 SCALE: 1/4" = 1'-0"

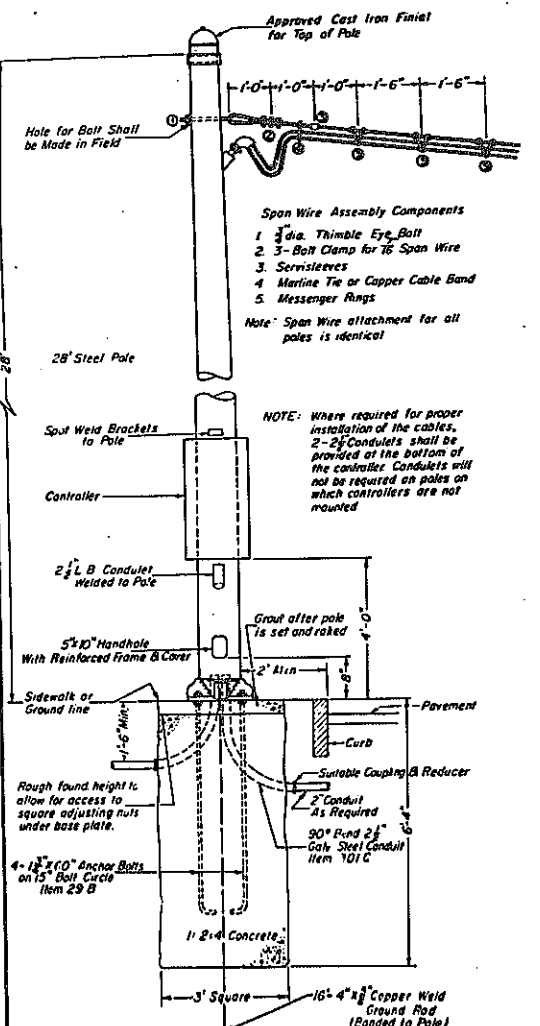
SECTION A-A

REVISED AS SHOWN 149

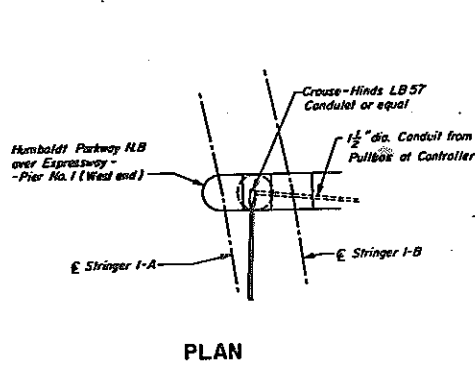
TRAFFIC SIGNAL PLANS							
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL HIGHWAY KENSINGTON EXPRESSWAY, SEC. NO. III							
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	<table border="1"> <tr> <td>DRAWN</td> <td>G.C.P.</td> </tr> <tr> <td>CHECKED</td> <td>R.J.D.</td> </tr> <tr> <td>TRACED</td> <td>G.C.P.</td> </tr> </table>	DRAWN	G.C.P.	CHECKED	R.J.D.	TRACED	G.C.P.
DRAWN	G.C.P.						
CHECKED	R.J.D.						
TRACED	G.C.P.						

FED. ROAD NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	61	173

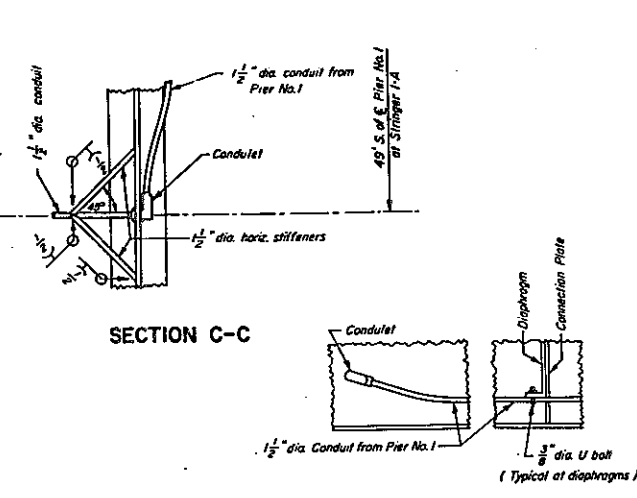
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



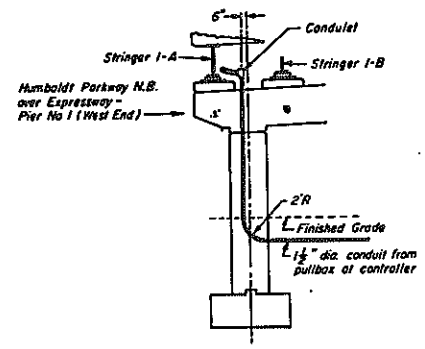
ITEM 325 B
TYPICAL TRAFFIC LIGHT STEEL POLE
INSTALLATION



PLAN

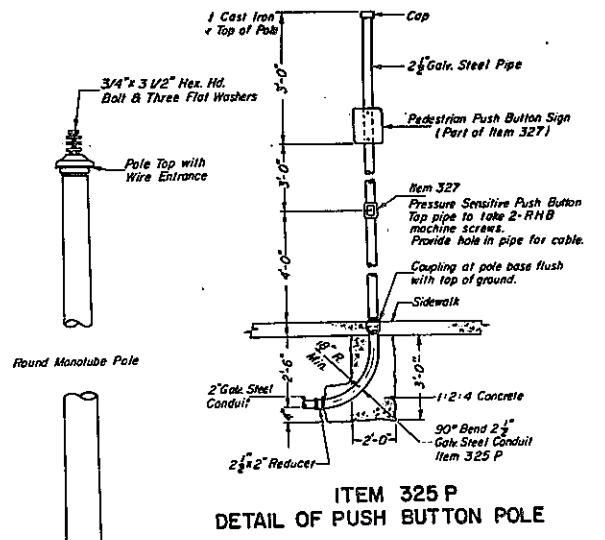


SECTION B-B

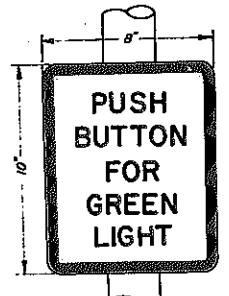


ELEVATION

SIGNAL "E" - CONDUIT & MOUNTING

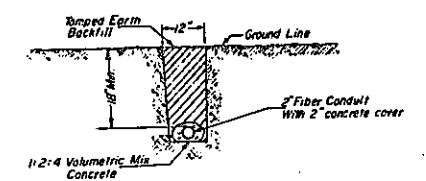


ITEM 325 P
DETAIL OF PUSH POLE

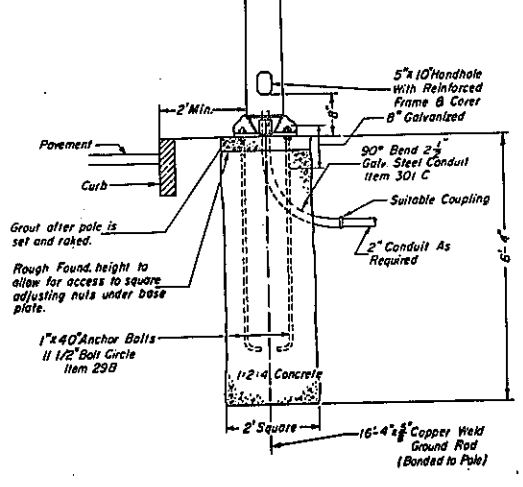


PEDESTRIAN PUSH BUTTON SIGN
(PART OF ITEM 327)

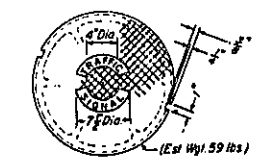
The sign to be N.Y.S. Traffic Commission sign # 55 G or equal, and be galvanized with 1-coat of Zinc Chromate primer and 2-coats of white baked enamel. Margin to be 3/8", border - 3/8" letters - 1/2".



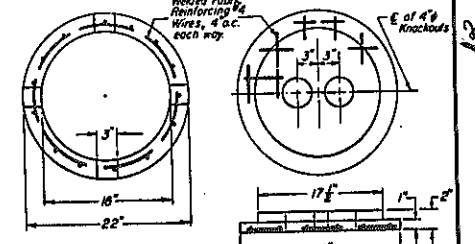
TYPICAL FIBER CONDUIT
INSTALLATION



ITEM 325 S
POWER POLE
INSTALLATION

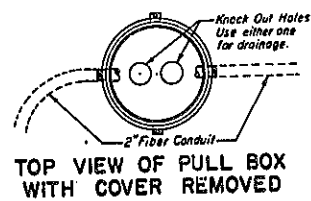


PULL BOX COVER
(PART OF ITEM 304 A)

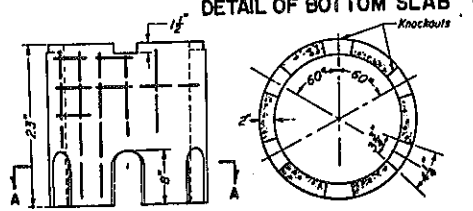


PLAN

DETAIL OF BOTTOM SLAB

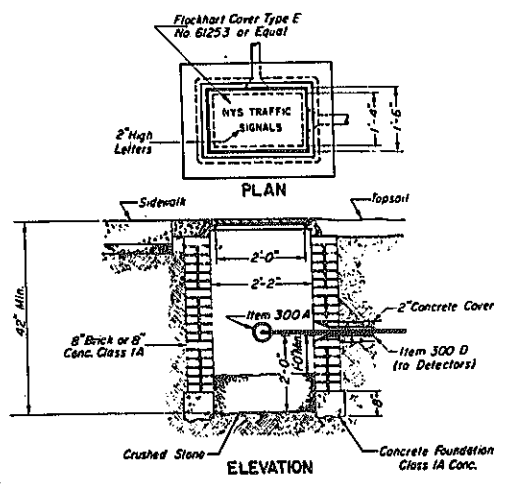


TOP VIEW OF PULL BOX
WITH COVER REMOVED

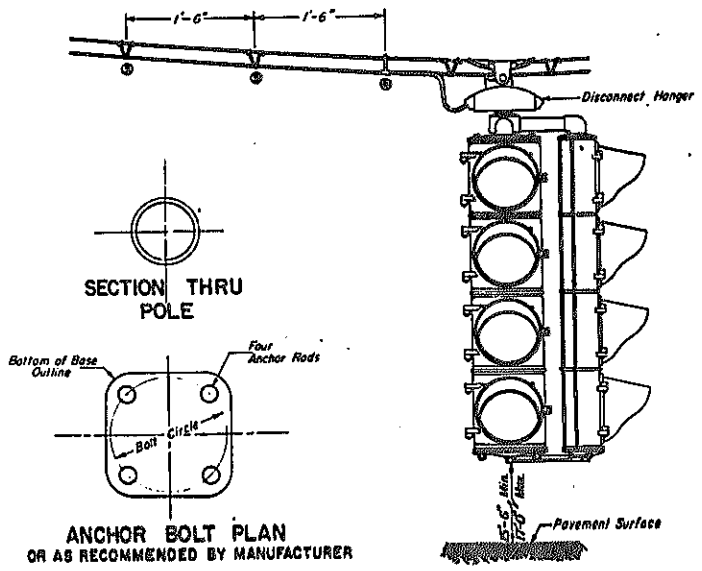


ELEVATION SECTION A-A
(REINFORCING NOT SHOWN)

ITEM 304 A
DETAILS OF 18" ROUND
PRECAST PULL BOX

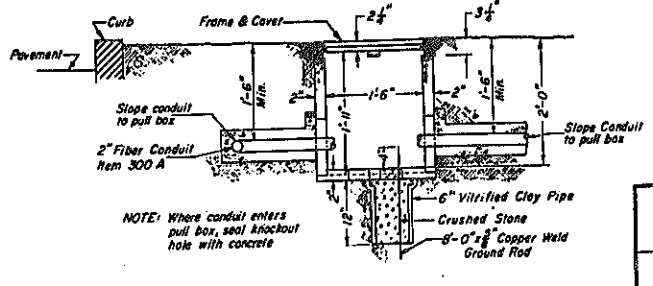


ITEM 304 B
PULL BOX DETAIL



ANCHOR BOLT PLAN
OR AS RECOMMENDED BY MANUFACTURER

TYPICAL SPAN WIRE MOUNTING



ITEM 304 A
TYPICAL INSTALLATION OF
18" ROUND PRECAST PULL BOX

TRAFFIC SIGNAL DETAILS

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

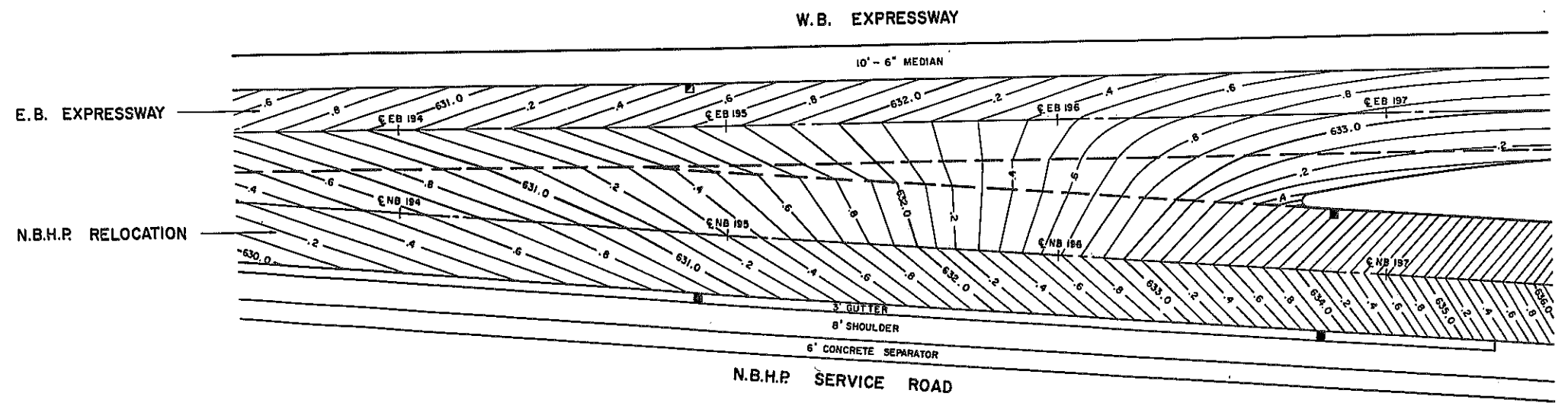
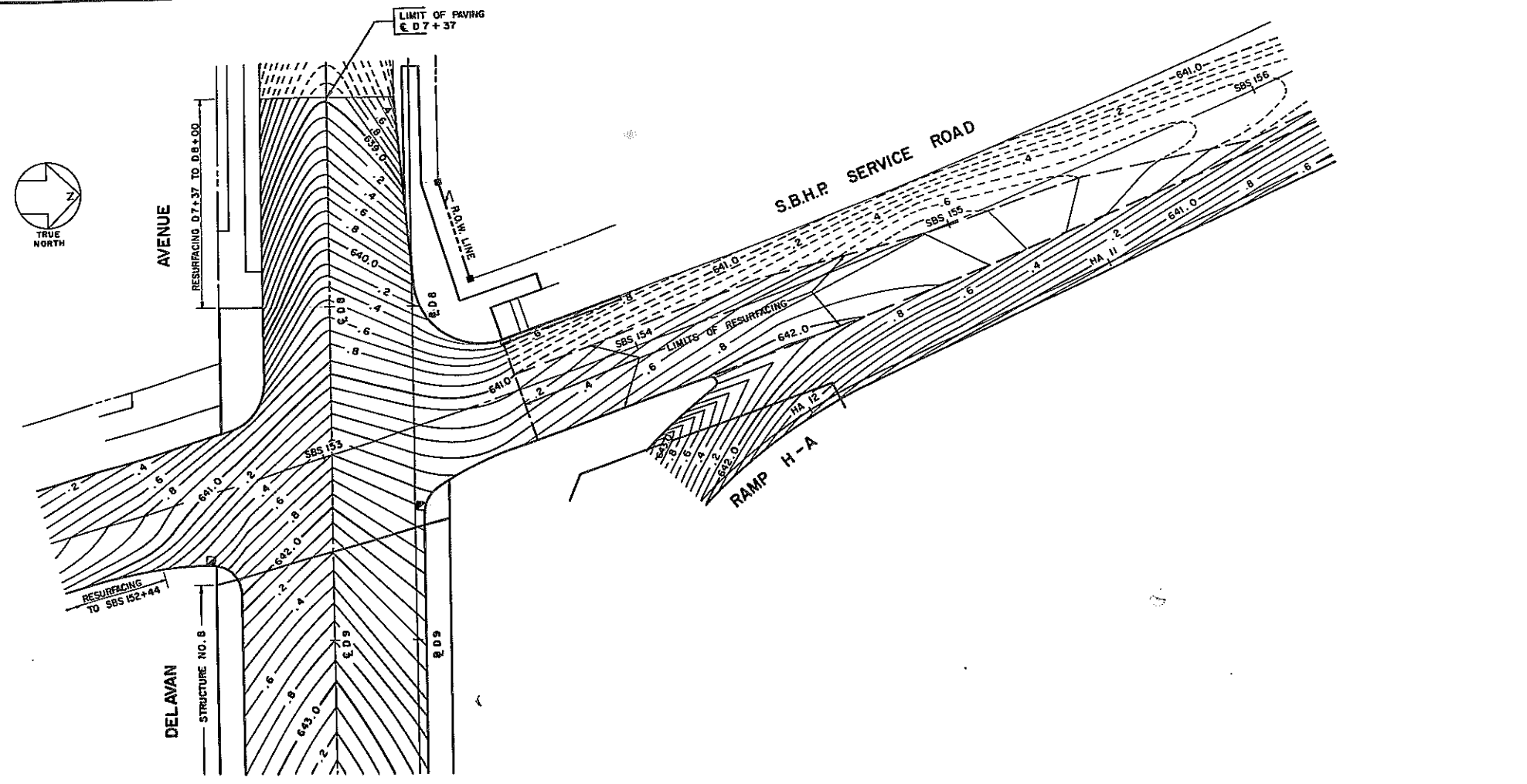
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	S. C. F. P. J. P. T. E. K.
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FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (28)	62	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

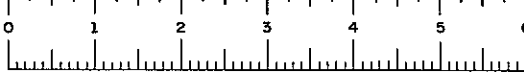


15

SCALE: 1" = 20'

INTERSECTION GRADING PLAN
DELAVAN AVE. & S.B. SERVICE RD.
E.B. EXPRESSWAY & N.B. HUMBT. PKWY.
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

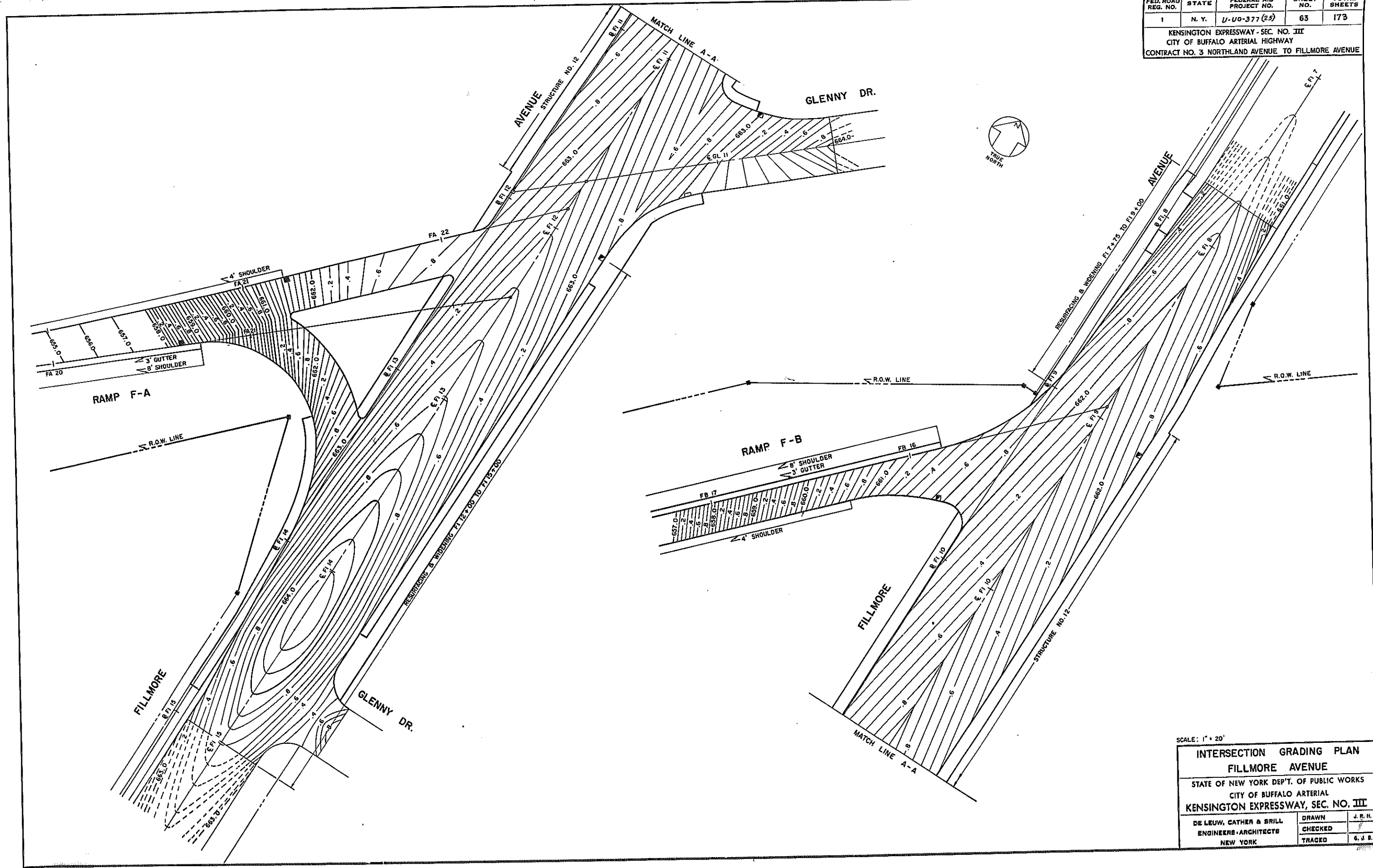
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J. R. H. G. J. E.
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FAC 63-2

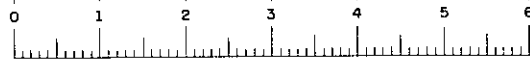
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	63	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



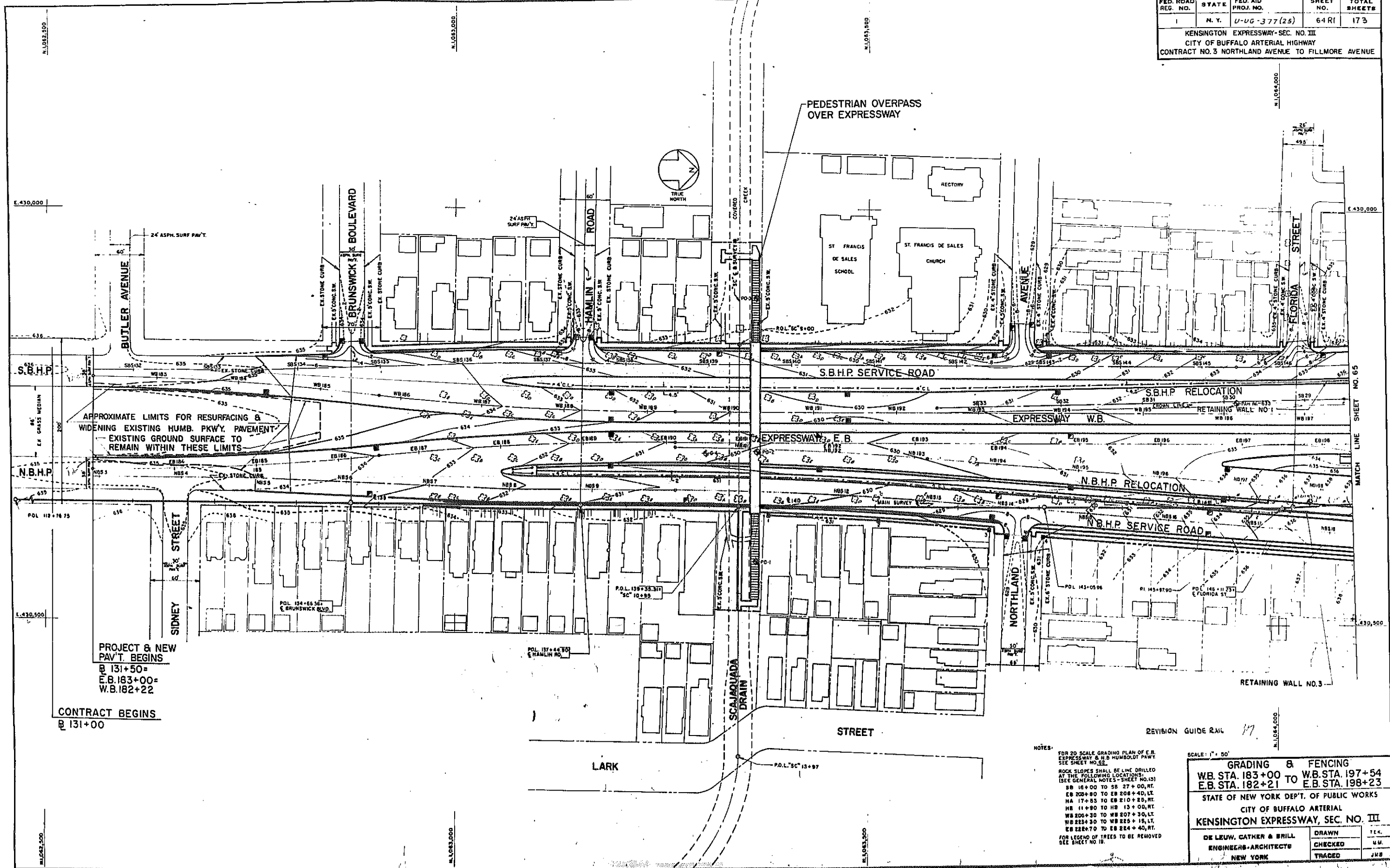
SCALE: 1" = 20'

INTERSECTION GRADING PLAN FILLMORE AVENUE			
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J. R. H. G. J. D.	



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	64 RI	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



APPROXIMATE LIMITS FOR RESURFACING & WIDENING EXISTING HUMB. PKWY. PAVEMENT - EXISTING GROUND SURFACE TO REMAIN WITHIN THESE LIMITS

PROJECT & NEW PAV'T. BEGINS
@ 131+50=
E.B. 183+00=
W.B. 182+22

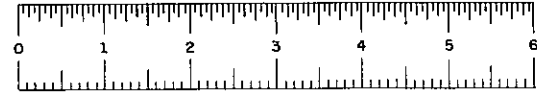
CONTRACT BEGINS
@ 131+00

NOTES:
FOR 20' SCALE GRADING PLAN OF E.B. EXPRESSWAY & H.D. HUMBOLDT PKWY. SEE SHEET NO. 62.
ROCK SLOPES SHALL BE LIME DRILLED AT THE FOLLOWING LOCATIONS: (SEE GENERAL NOTES - SHEET NO. 13)
SB 16+00 TO SB 27+00, RT.
EB 208+80 TO EB 208+40, LT.
HA 17+55 TO EB 210+25, RT.
HB 11+90 TO HB 13+00, RT.
WB 200+30 TO WB 207+30, LT.
WB 224+30 TO WB 225+15, LT.
EB 228+70 TO EB 224+40, RT.
FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 18.

SCALE: 1" = 50'

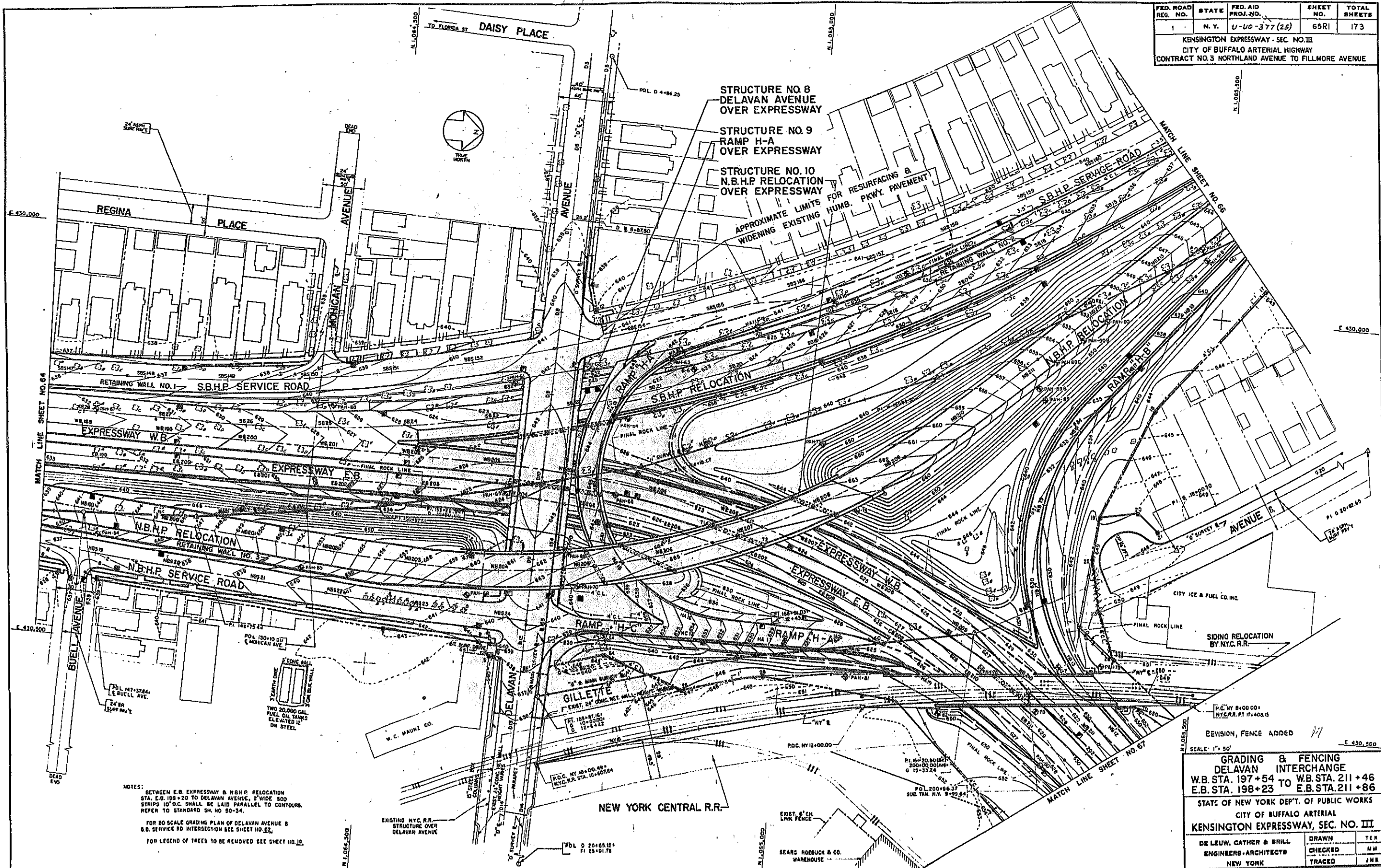
GRADING & FENCING	
WB. STA. 183+00 TO	WB. STA. 197+54
E.B. STA. 182+21	E.B. STA. 198+23
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL	DRAWN
ENGINEERS-ARCHITECTS	CHECKED
NEW YORK	TRACED
	T.E.K.
	M.M.
	J.W.B.

MATCH LINE SHEET NO. 65



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	65R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



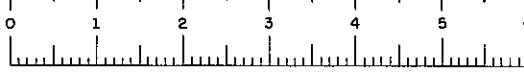
NOTES:
BETWEEN E.B. EXPRESSWAY & N.B.H.P. RELOCATION STA. E.B. 197+54 TO DELAVAN AVENUE, 2" WIDE 100 STRIPS 10' O.C. SHALL BE LAID PARALLEL TO CONTOURS. REFER TO STANDARD SH. NO. 50-34.
FOR 20 SCALE GRADING PLAN OF DELAVAN AVENUE & S.B. SERVICE RD. INTERSECTION SEE SHEET NO. 62.
FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 15.

SCALE: 1" = 50'

GRADING & FENCING
DELAVAN INTERCHANGE
W.B. STA. 197+54 TO W.B. STA. 211+46
E.B. STA. 198+23 TO E.B. STA. 211+86

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.R. M.M. J.M.B.
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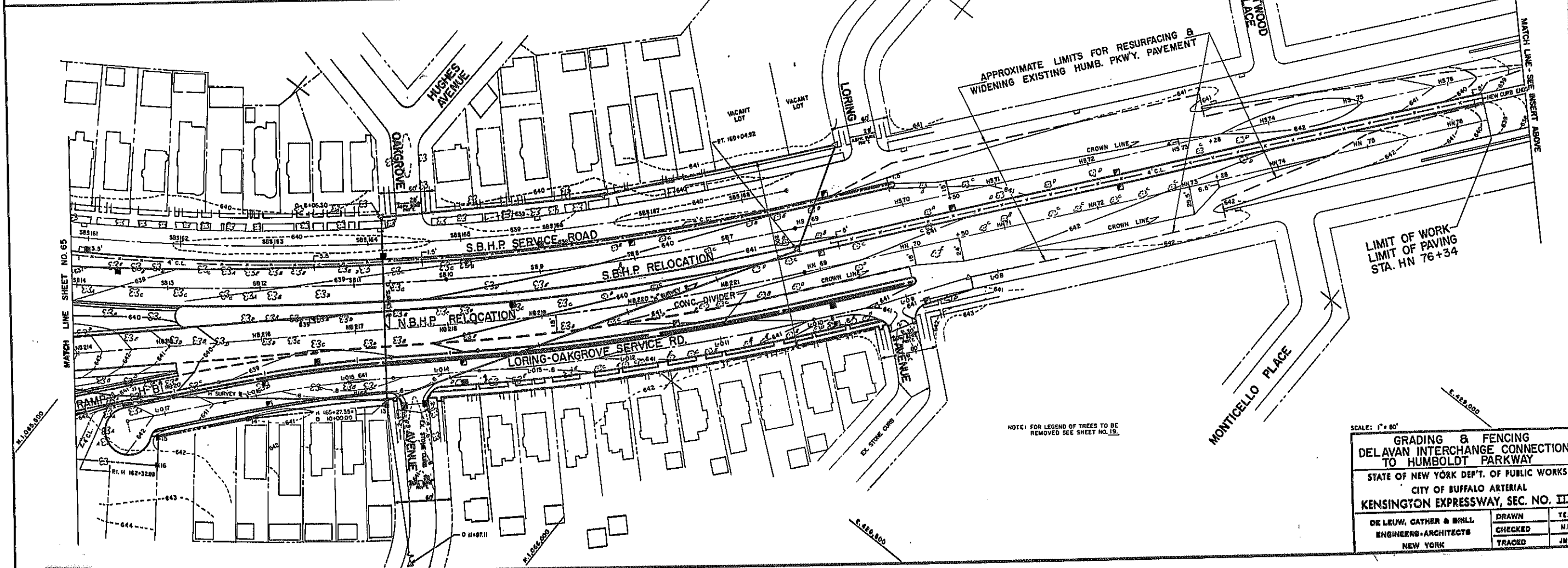
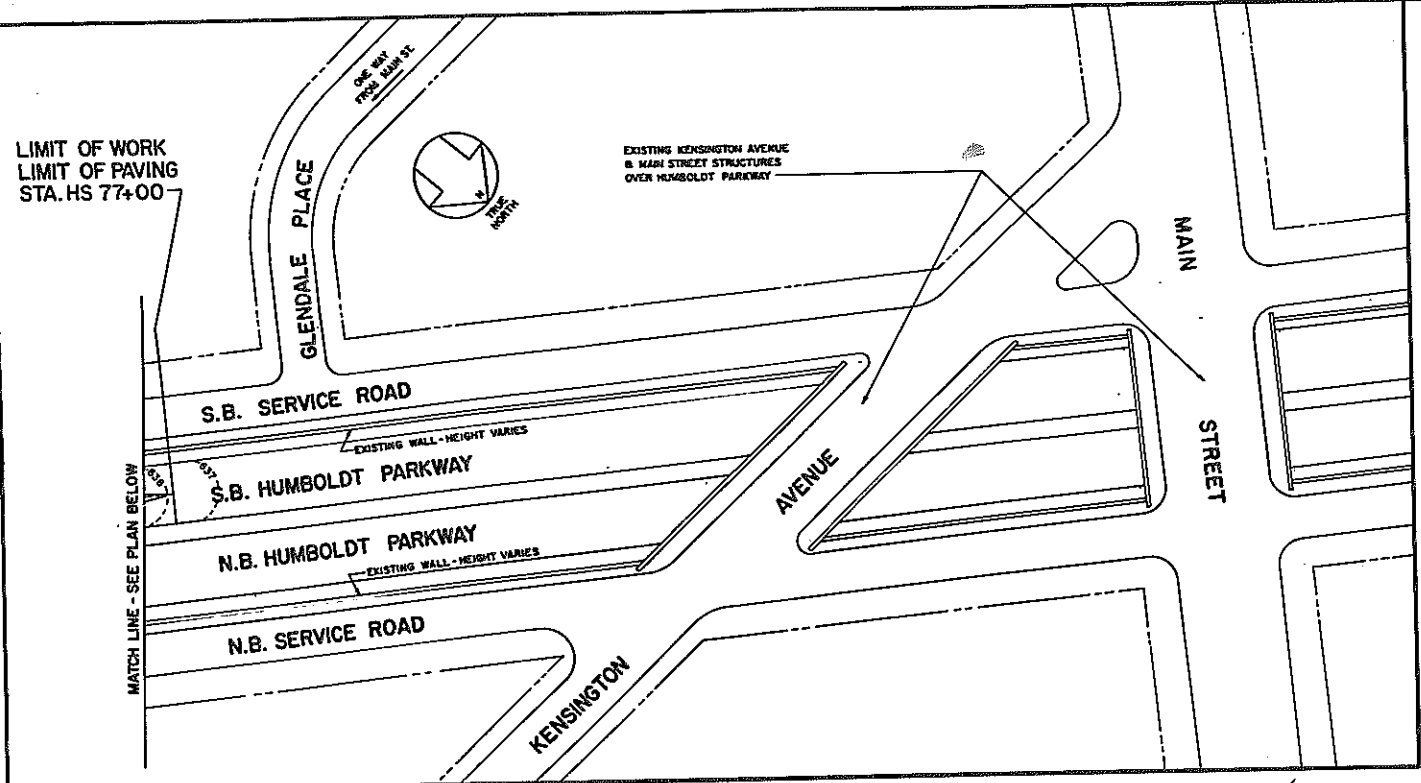


FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	66	173

KENSINGTON EXPRESSWAY - SEC. NO. II
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FALLMORE AVENUE

LIMIT OF WORK
 LIMIT OF PAVING
 STA. HS 77+00



LIMIT OF WORK
 LIMIT OF PAVING
 STA. HN 76+34

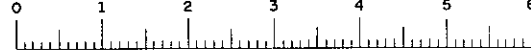
NOTE: FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 12.

SCALE: 1" = 80'

**GRADING & FENCING
 DELAVAN INTERCHANGE CONNECTIONS
 TO HUMBOLDT PARKWAY**

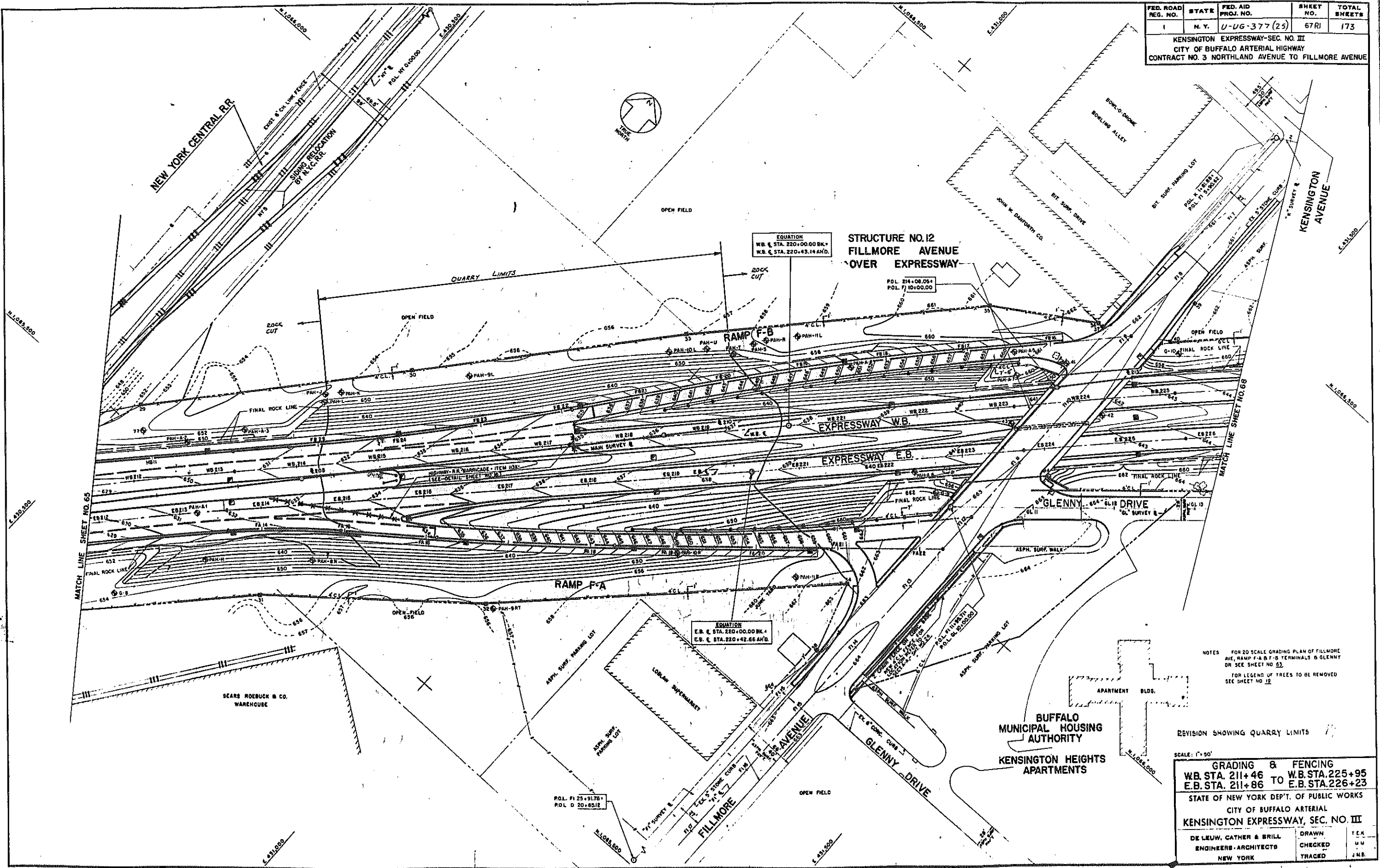
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TEA. M.H. J.M.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	67R1	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

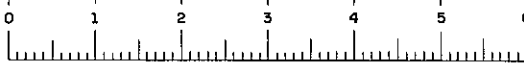


NOTES
FOR 20 SCALE GRADING PLAN OF FILLMORE AVE, RAMP F-A & F-B TERMINALS & GLENNY DR. SEE SHEET NO. 65.
FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 12.

REVISION SHOWING QUARRY LIMITS

SCALE: 1"=50'

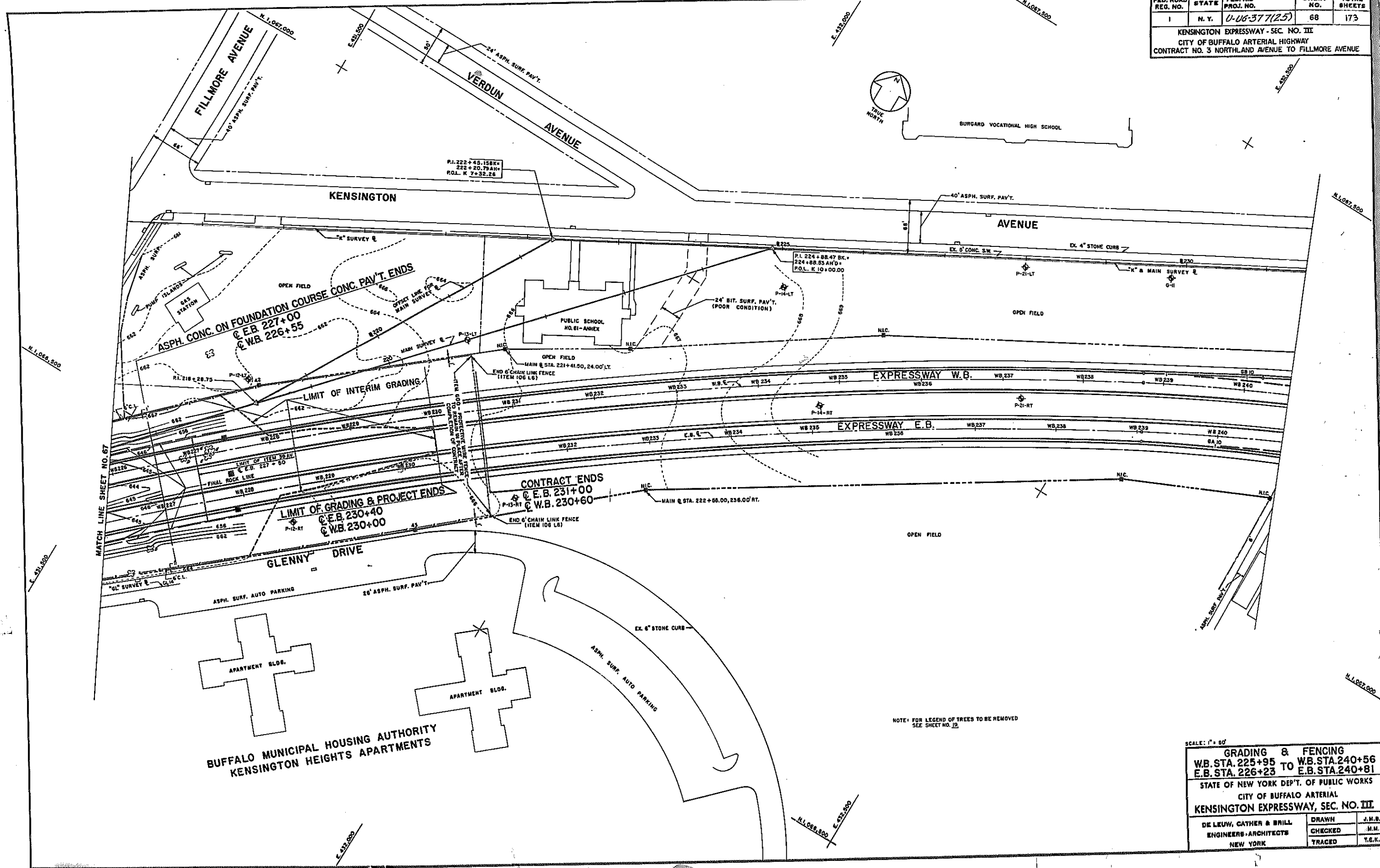
GRADING & FENCING	
WB. STA. 211+46	TO WB. STA. 225+95
E.B. STA. 211+86	TO E.B. STA. 226+23
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL	DRAWN
ENGINEERS-ARCHITECTS	CHECKED
NEW YORK	TRACED
	T.E.H.
	M.M.
	J.M.B.



FAC 63-2

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	68	173

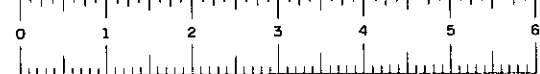
KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 19

SCALE: 1" = 50'

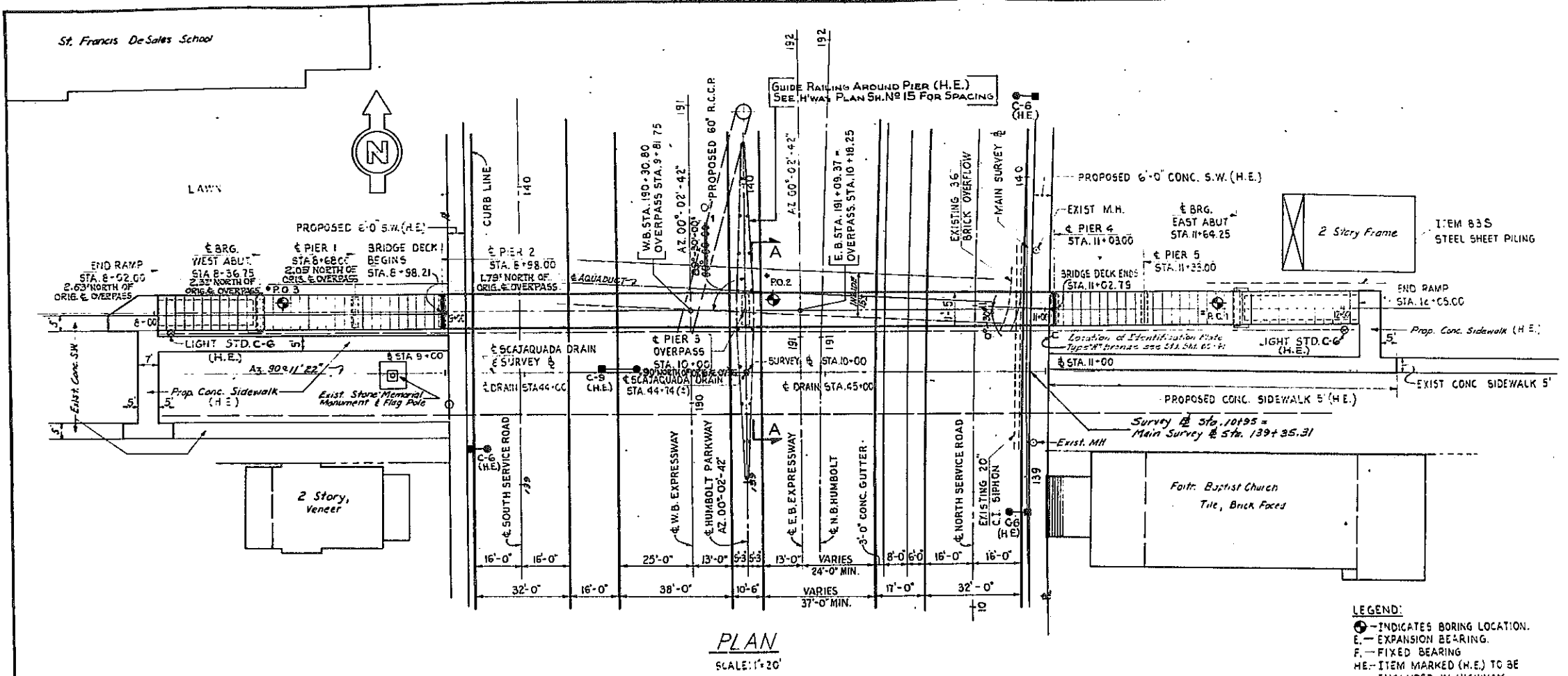
GRADING & FENCING	
W.B. STA. 225+95 TO W.B. STA. 240+81	E.B. STA. 226+23 TO E.B. STA. 240+81
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL	DRAWN J.M.B.
ENGINEERS-ARCHITECTS	CHECKED H.M.
NEW YORK	TRACED T.G.K.



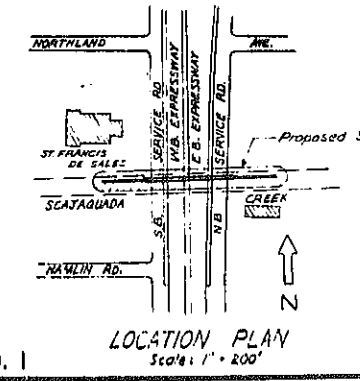
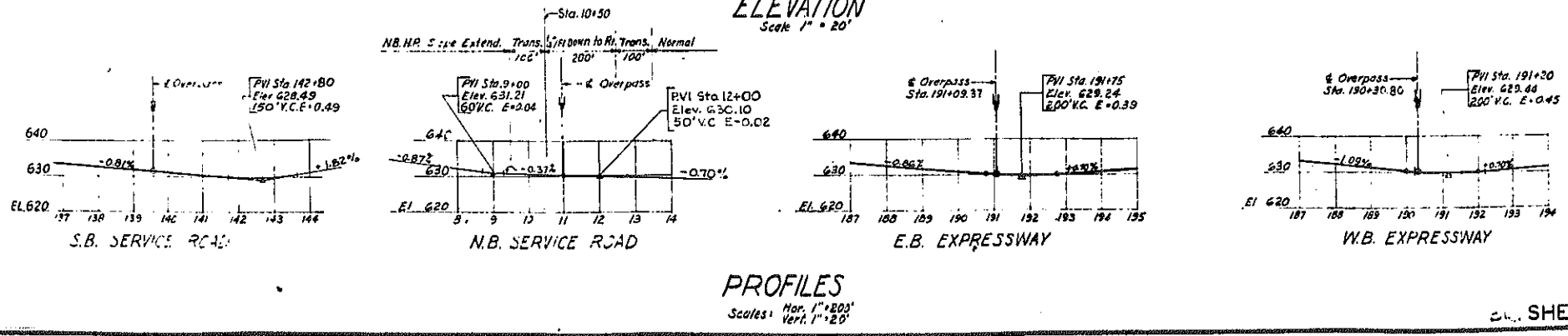
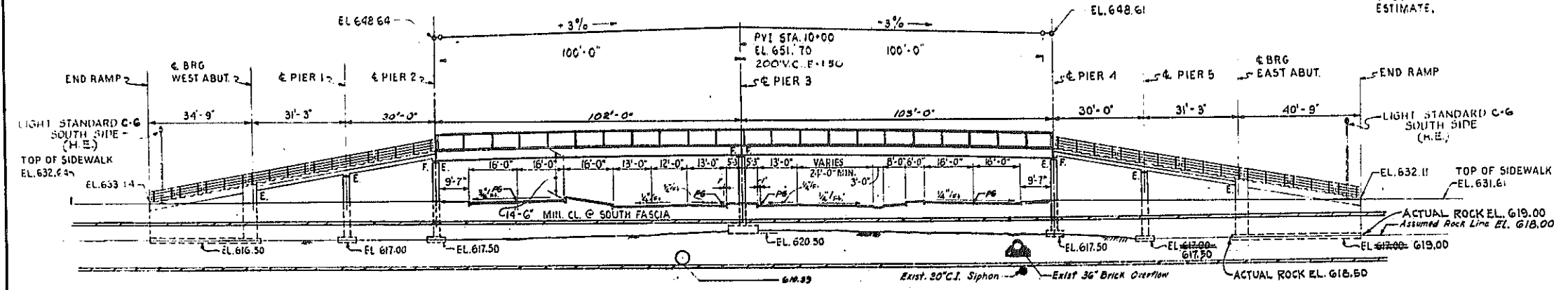
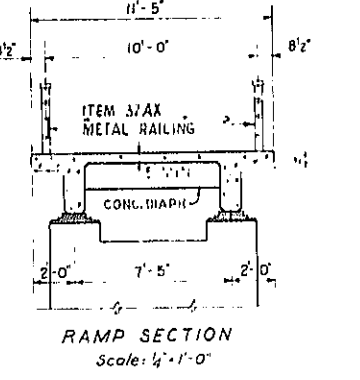
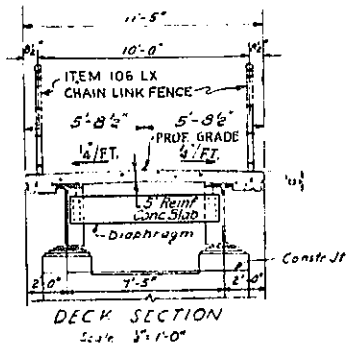
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-106-377(25)	43R1	175

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

NOTE: THIS SHEET SUPERSEDED BY FIELD CHANGE SHEET No. 695 TO CHANGE FENCING TO CAGE. WE HAVE PRINT ON RACK WITH P.E.D. OVERPASS SET.



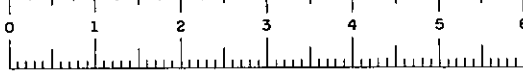
LEGEND:
 ● - INDICATES BORING LOCATION.
 E - EXPANSION BEARING.
 F - FIXED BEARING.
 HE - ITEM MARKED (H.E.) TO BE INCLUDED IN HIGHWAY ESTIMATE.



REVISED AS SHOWN

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37		
GENERAL PLAN		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHER & BRILL	DRAWN	E.S.
ENGINEERS-ARCHITECTS	CHECKED	J.A.T.
NEW YORK	TRACED	E.B.

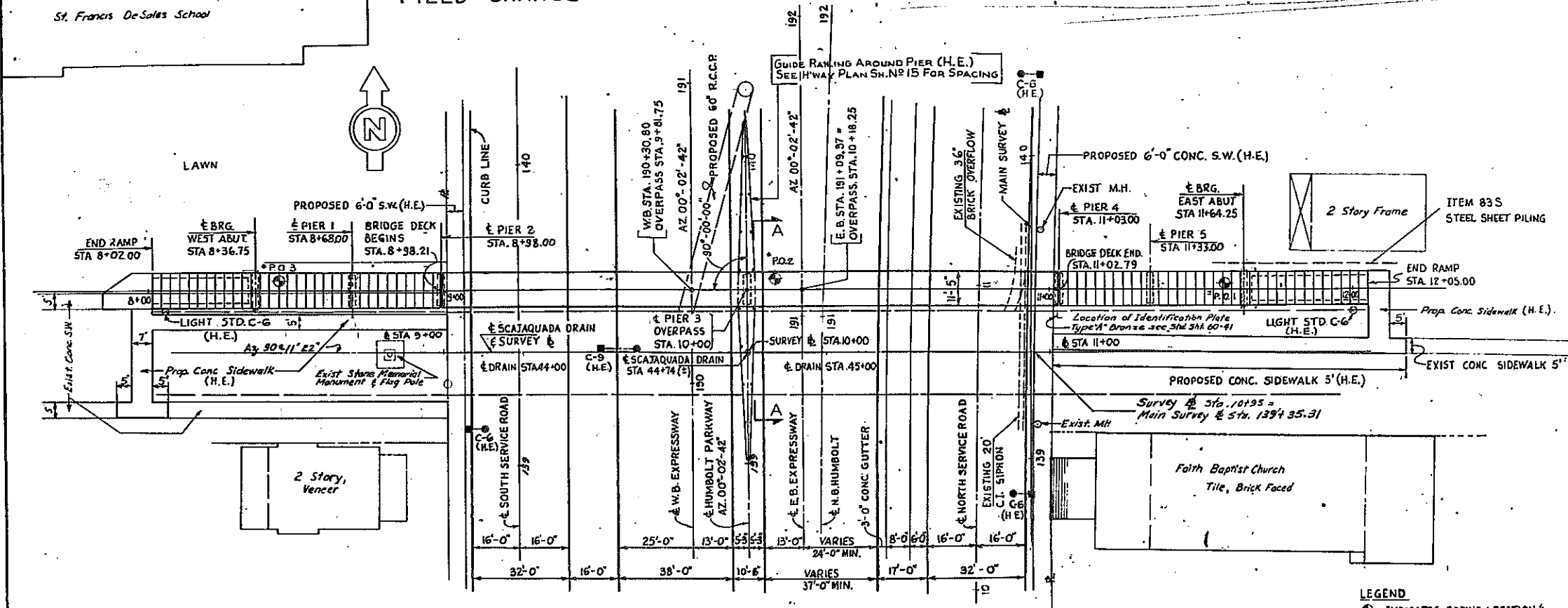
SHEET NO. 1



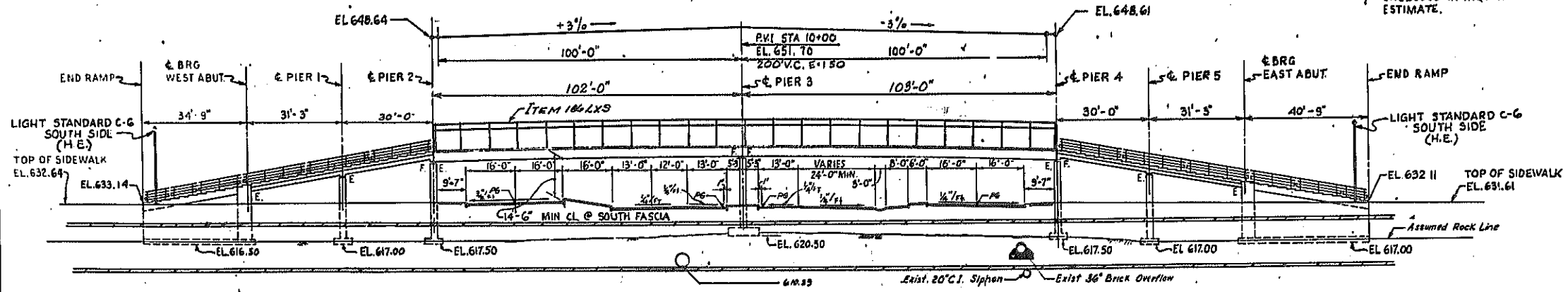
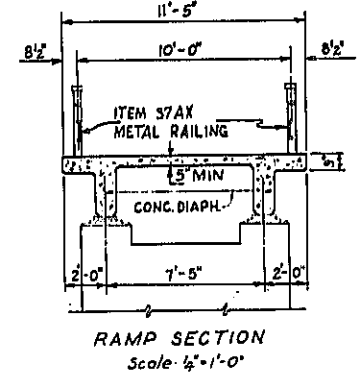
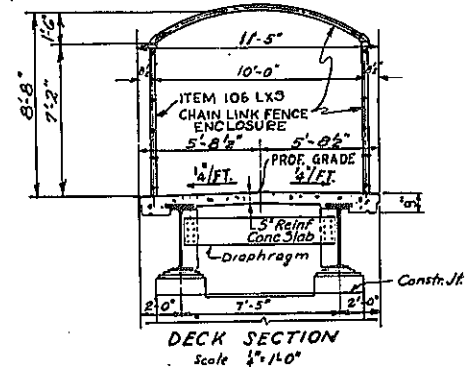
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	69	173
KENSINGTON EXPRESSWAY SEC. NO. III C.F.P.R.				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT No. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

FIELD CHANGE

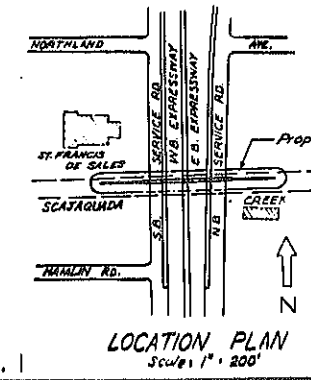
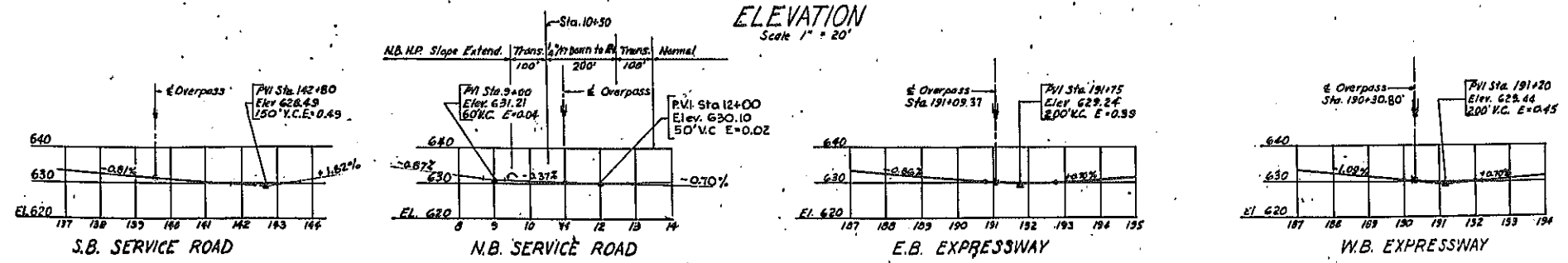


PLAN
SCALE: 1" = 20'



APPROVED *Norman H. Stoff*
V.J. BURNS
DEPUTY CHIEF ENGINEER

PREPARED PURSUANT TO THE HIGHWAY LAW AND RECOMMENDED BY *Norman H. Stoff*
Nov. 7, 1963 DIST. ENGINEER, DIST. NO. 5



BR. SHEET NO. 1

FIELD CHANGE

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37

GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS

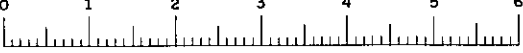
CITY OF BUFFALO ARTERIAL

KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	ENGINEERS-ARCHITECTS	NEW YORK	DRAWN	E.B.
			CHECKED	W.T.
			TRACED	F.B.

FAC 63-2

FAC 63-2



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	70R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS: A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING: PEDESTRIAN SLAB LOADING 85 LBS. PER SQ. FT.

MATERIAL FABRICATION AND MANUFACTURE: NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.
THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS: FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE: ALL CONCRETE IN THE SUPERSTRUCTURE AND THE PIERS ABOVE FOOTINGS SHALL BE ITEM 18X CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS AND WINGWALLS SHALL BE ITEM 20Y CLASS 1 CONCRETE.
CONCRETE IN ABUTMENTS CAPS ORDERED TO BE ITEM 18.

CEMENT: CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE: ONE IDENTIFICATION PLATE BRONZE, TYPE C WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STANDARD SHEET 60-41.

OTHER: REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. PEDESTRIAN SLAB SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER.

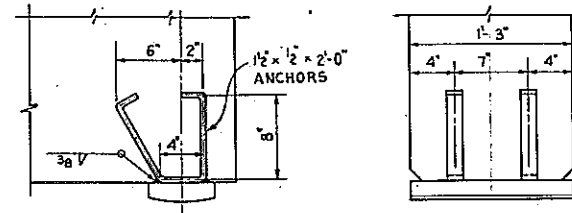
SURFACE OF PIER PEDESTALS SHALL BE POUNED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH NUMBERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.
THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS. CONSTRUCTION JOINTS OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREPARED EXPANSION JOINT FILLER, COMPRESSED ASBESTOS SHEET PACKING AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

FOOTINGS: ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

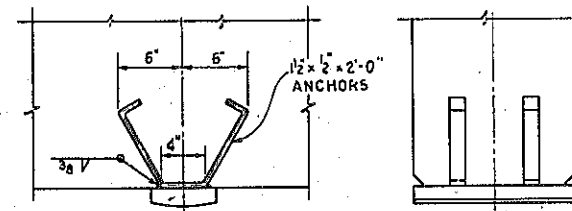
NOTE: FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

ESTIMATE OF QUANTITIES					
ITEM NO	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED	FINAL QUANTITY
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	921	955	678.7
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	252	260	260.45
20Y	CLASS 1 CONCRETE	C.Y.	334	340	293.13
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	53,000	54,000	51,904
29B	STRUCTURAL STEEL	Lb.	98,900	101,800	99,050
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	396	410	403.2
61	BITUMINOUS MATERIAL	GAL.	170	180	0.0
83S	STEEL SHEET PILING	S.F.	830	870	915
83TX	TEMPORARY SHEET PILING	S.F.	6,887	7,240	3,207
106LX	CHAIN LINK FENCE	L.F.	407	420	0.0
352XA	RUBBER JOINT MATERIAL	GAL.	2	2	2
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	10	12	5.5
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	1605	1810	1,575
106LXS	CHAIN LINK FENCING, ENCLOSURE	L.F.			203.5
106LFG	CHAIN LINK FENCE PORTAL VAN GUARD	Ea.			2
106LXSA	REPAIR OF DAMAGED POSTS ON R.O.P.	Neg.			100%

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.



SIDE ELEVATION END ELEVATION
BEAM NOT CONTINUOUS OVER SUPPORT

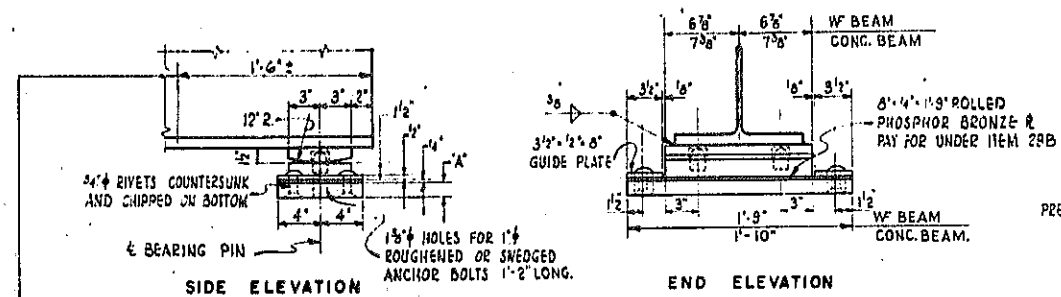


SIDE ELEVATION END ELEVATION
BEAM CONTINUOUS OVER SUPPORT

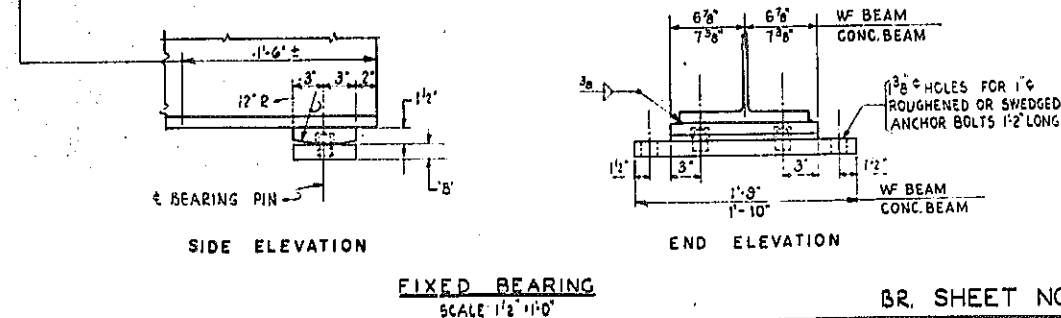
BEARING ANCHOR DETAILS FOR CONCRETE BEAMS

SCALE: 1/2" = 1'-0"

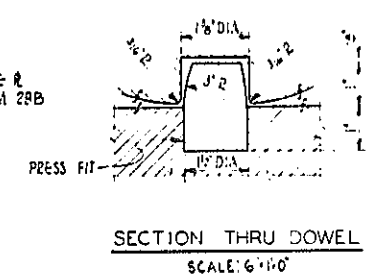
NOTE: PLACE ANCHORS SO THAT THEY WILL NOT INTERFERE WITH BEAM REINFORCEMENT.



EXPANSION BEARING
SCALE: 1/2" = 1'-0"



FIXED BEARING
SCALE: 1/2" = 1'-0"



NOTE: FOR SHOES UNDER CONCRETE BEAMS, SEE SH. NO. 8 FOR SHOES UNDER STEEL BEAMS, SEE FRAMING PLAN, SH. NO. 10.

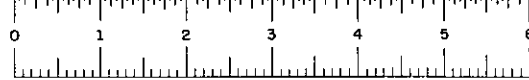
REVISED AS SHOWN, NOTE 4 ITEM 17

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37
NOTES, BEARINGS AND ESTIMATE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

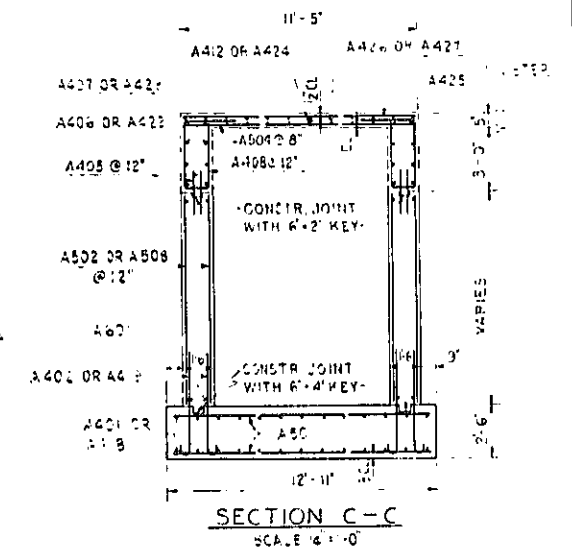
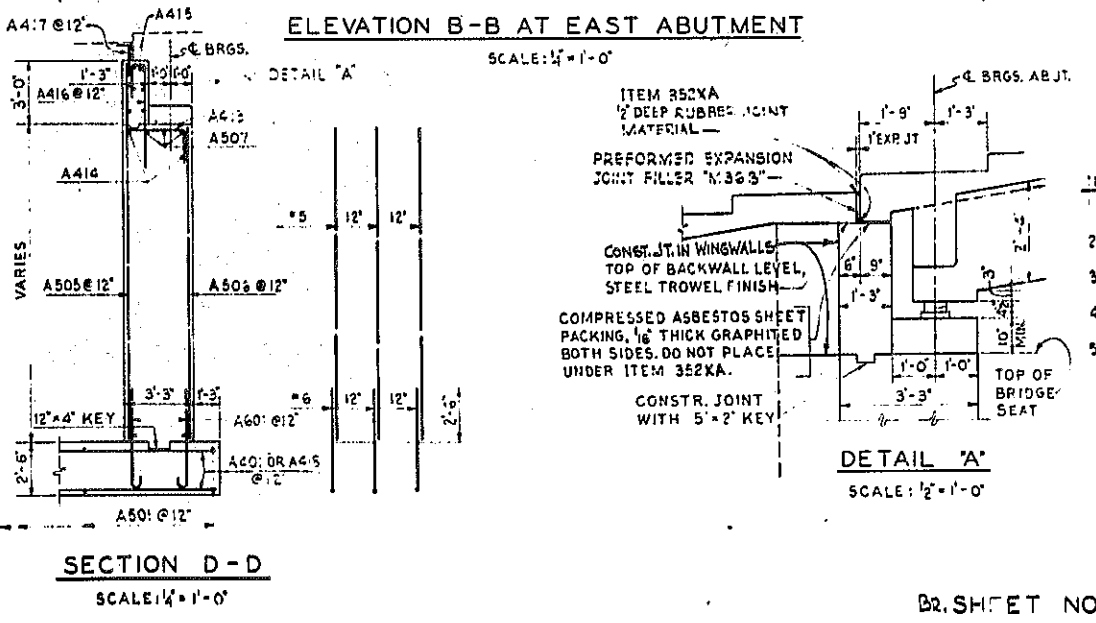
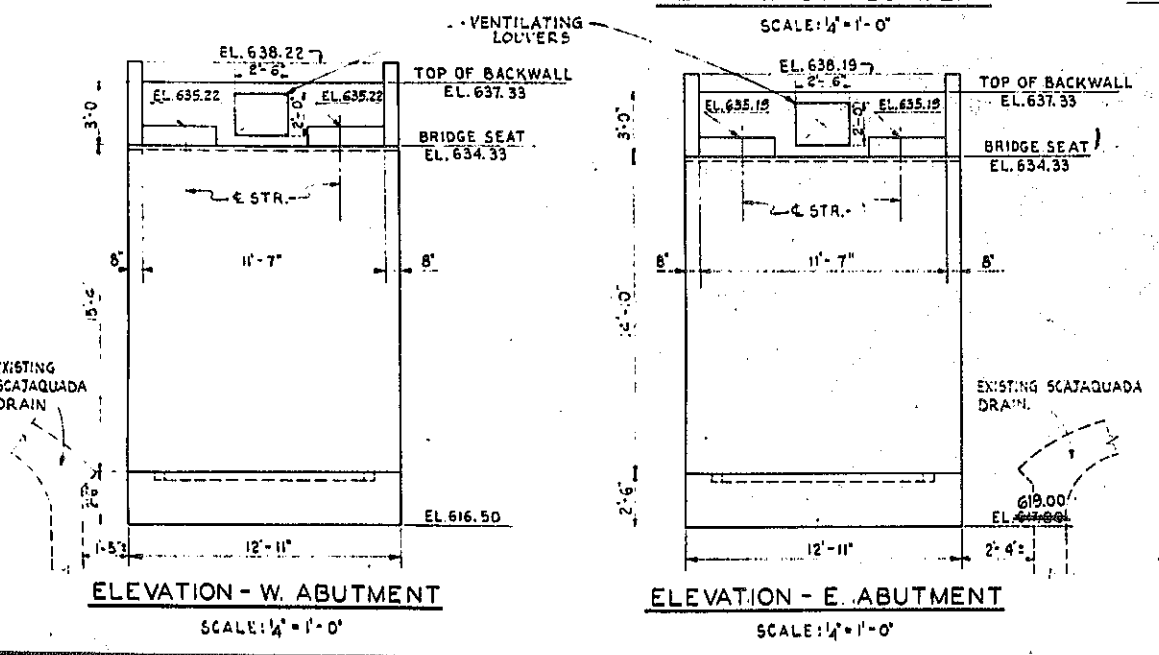
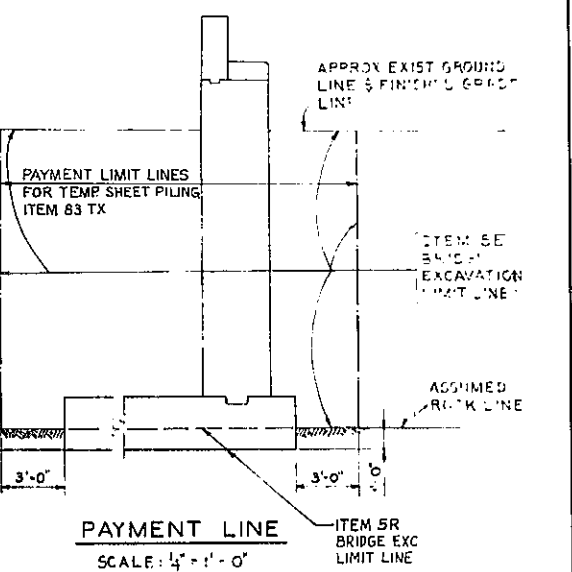
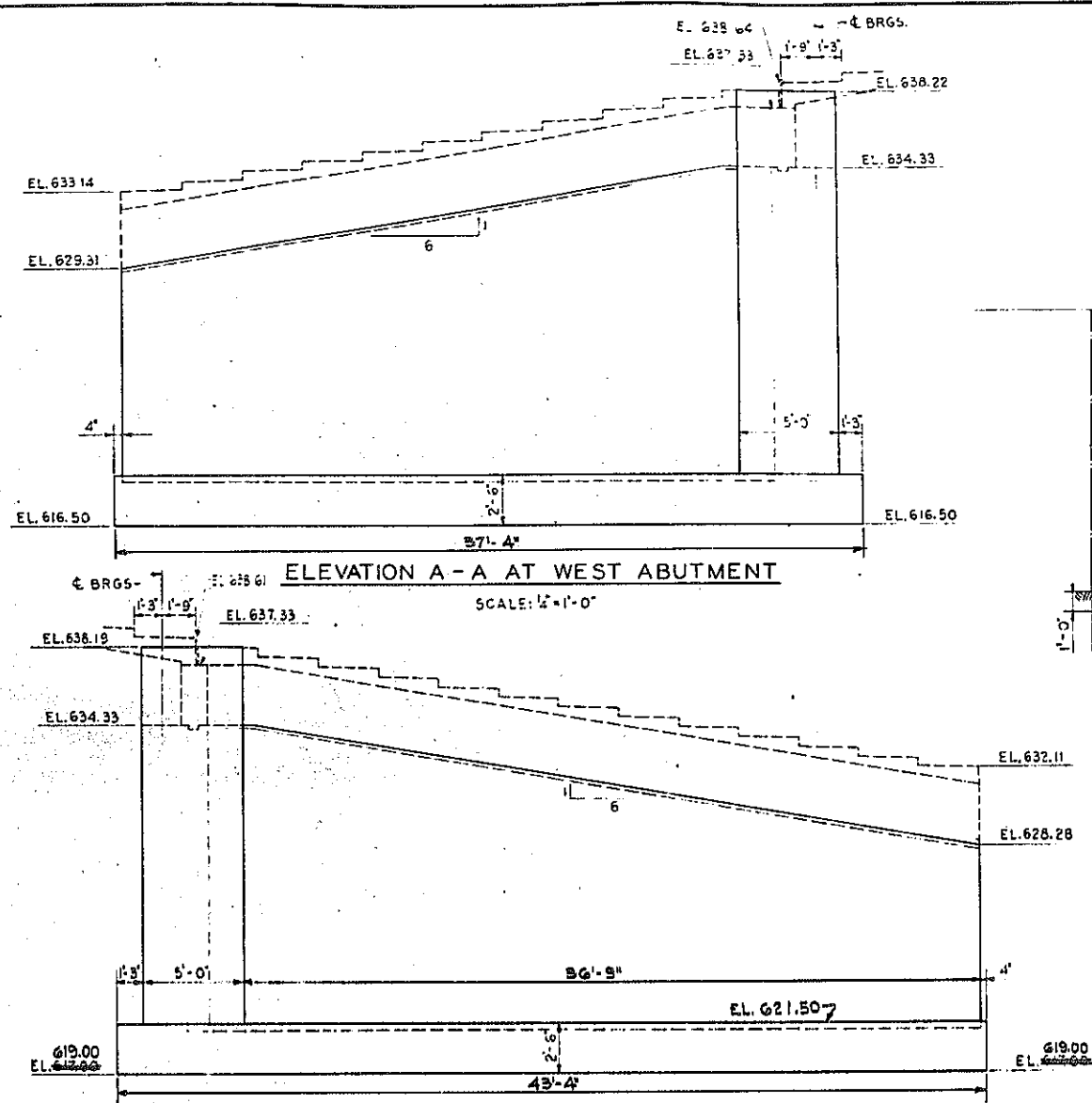
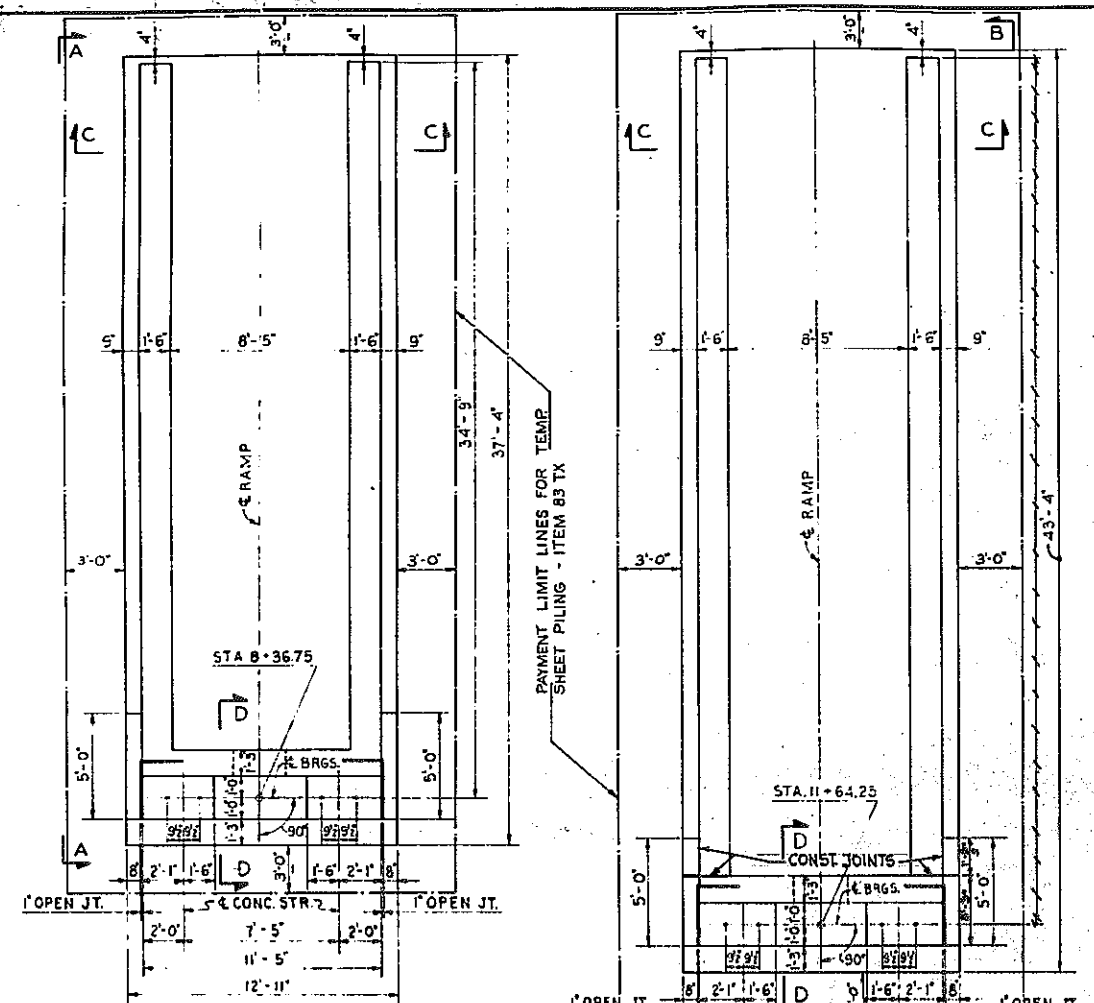
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK

DRAWN S.S.
CHECKED W.T.
TRACED S.S.



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	71R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:**
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - ALL CONCRETE IN ABUTMENT AND SIDEWALLS SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - ALL CONCRETE IN RAMPS AND BEAMS SHALL BE ITEM 8X, CLASS A CONCRETE FOR STRUCTURES.
 - 8" JYINOUS MATERIAL, ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND SIDEWALLS FROM TOP OF FOOTING TO FINISHED GRADE LINE.
 - FOR RAILING DETAILS, SEE SM. NO. 12.

REVISED AS SHOWN

PEDESTRIAN OVERPASS - E B STA 191+09.37
ABUTMENTS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN E. B.
CHECKED W. I.
TRACED E. B.

DR. SHEET NO. 3

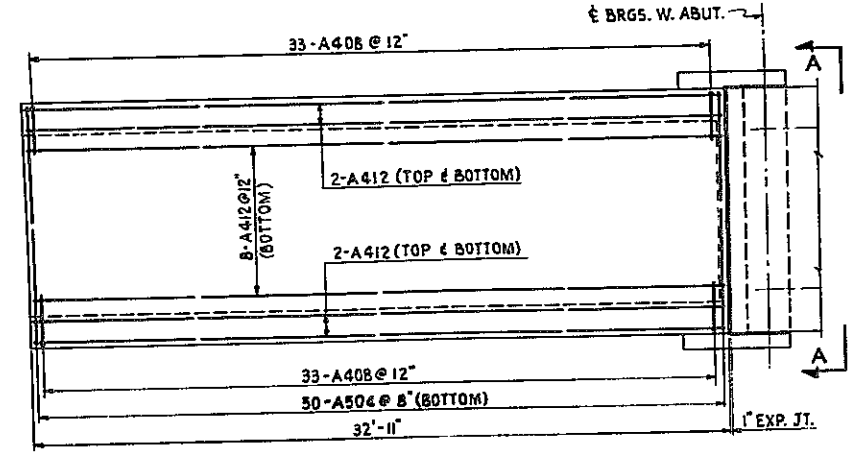


FAC 63-2

FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	72	173

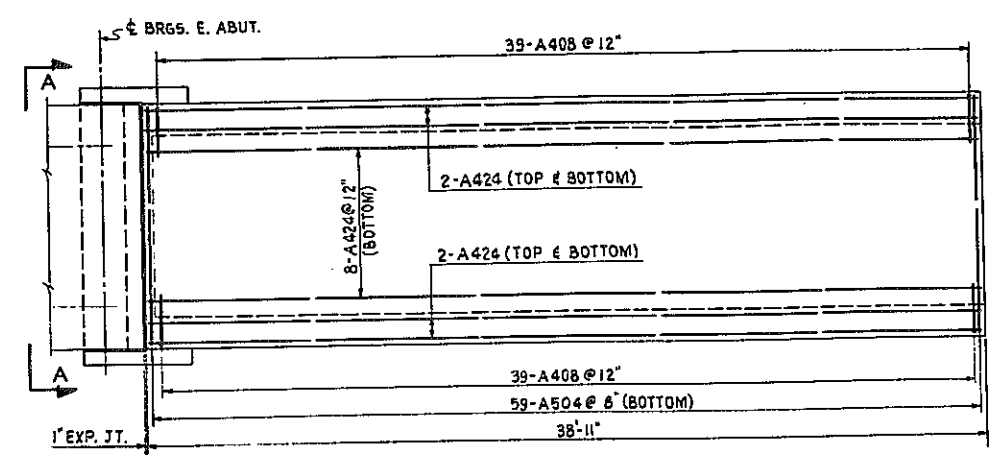
KENSINGTON EXPRESSWAY - SEC. NO. II
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

WEST ABUTMENT



SLAB PLAN
SCALE: 1/4" = 1'-0"

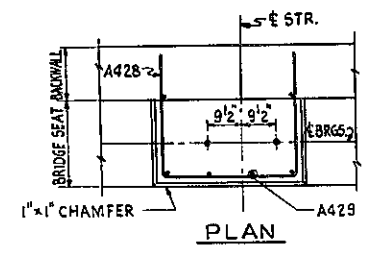
EAST ABUTMENT



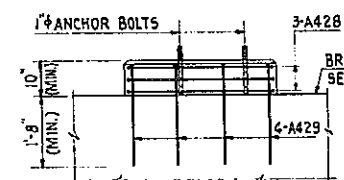
SLAB PLAN
SCALE: 1/4" = 1'-0"

STEP REINFORCEMENT NOT SHOWN
SEE STEP DETAIL.

NOTES: FOR CURTAIN WALL REINFORCEMENT
SEE DETAIL.
* FIELD BEND AS REQUIRED.

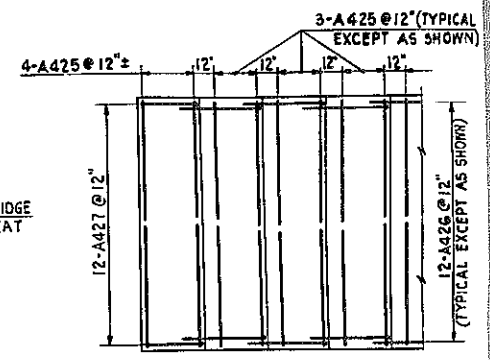


PLAN

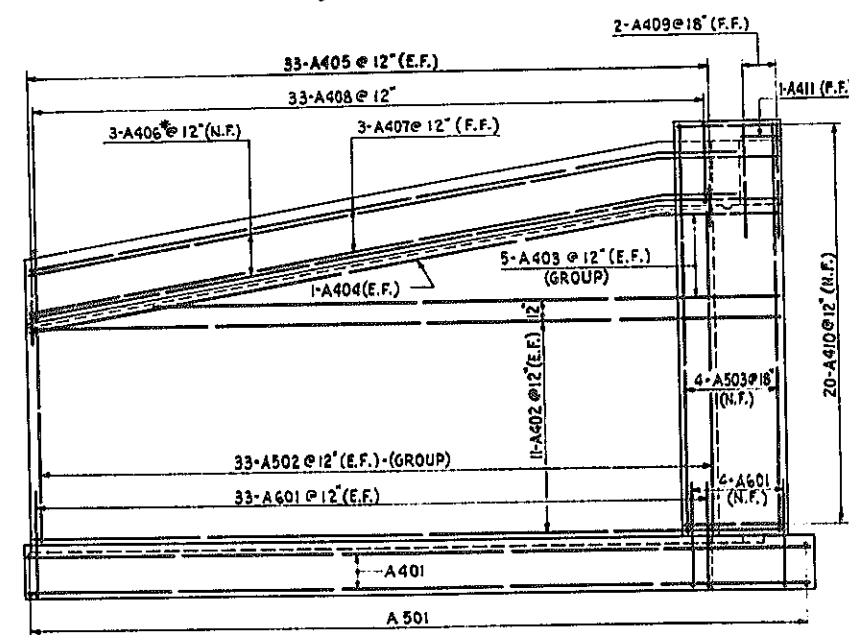


ELEVATION

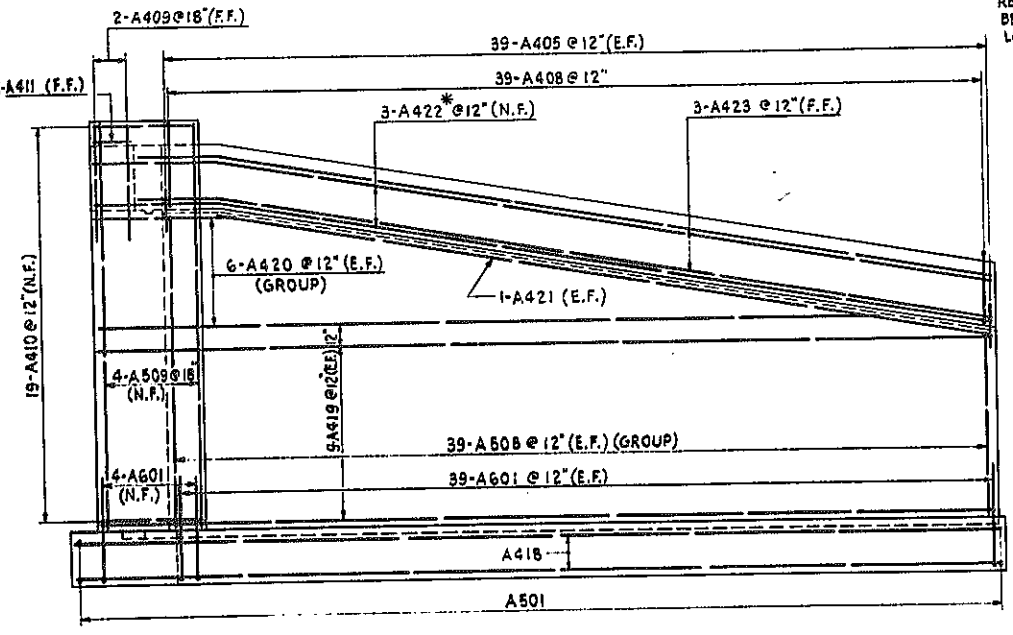
PEDESTAL DETAIL
SCALE: 1/2" = 1'-0"



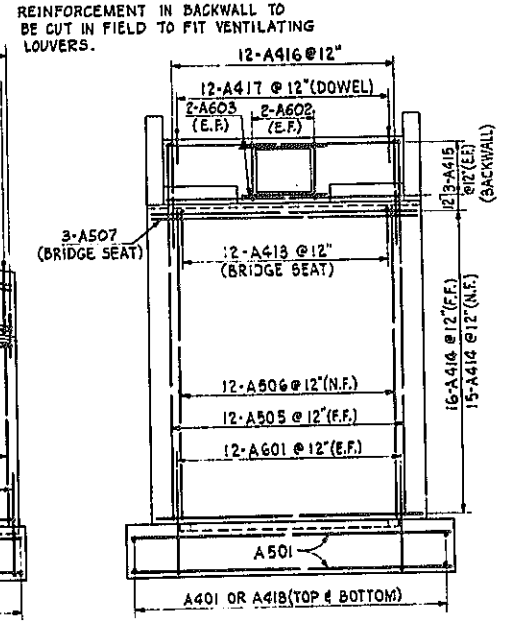
PLAN



ELEVATION
SCALE: 1/4" = 1'-0"

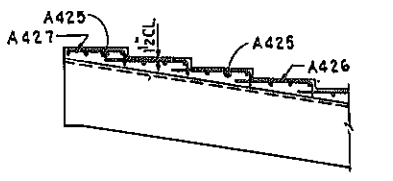


ELEVATION
SCALE: 1/4" = 1'-0"

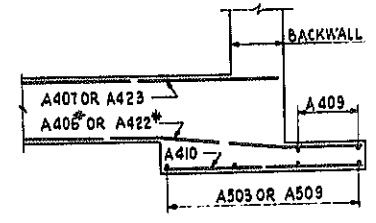


ELEVATION A-A
SCALE: 1/4" = 1'-0"

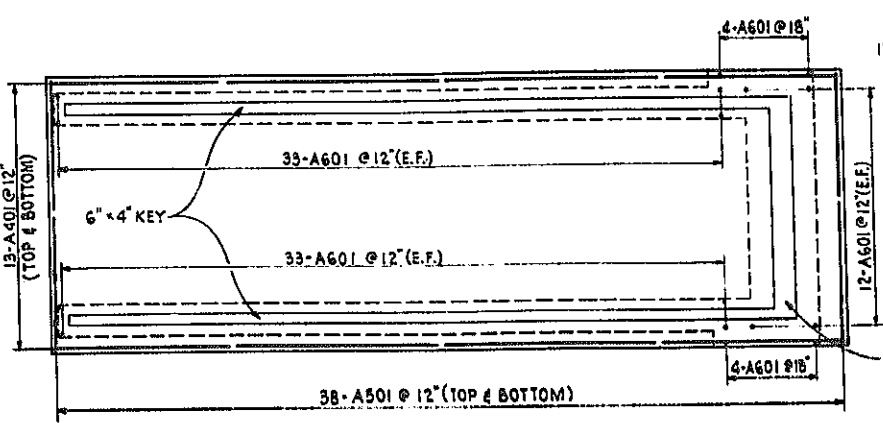
REINFORCEMENT IN BACKWALL TO BE CUT IN FIELD TO FIT VENTILATING LOUVERS.



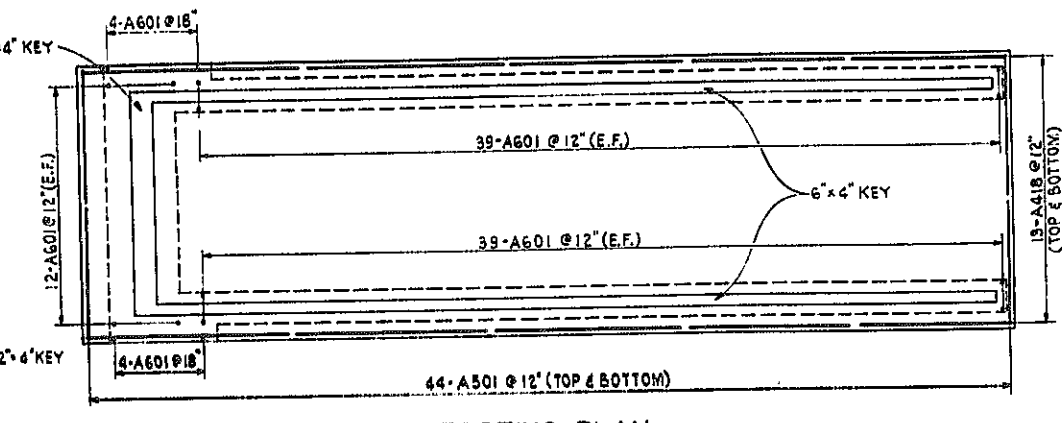
ELEVATION STEP DETAIL
SCALE: 1/4" = 1'-0"



CURTAIN WALL REINFORCEMENT DETAIL
SCALE: 1/2" = 1'-0"



FOOTING PLAN
SCALE: 1/4" = 1'-0"



FOOTING PLAN
SCALE: 1/4" = 1'-0"

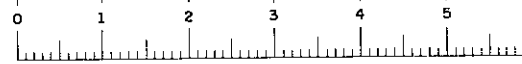
BAR REINFORCING LEGEND

NUMBER BARS	ABUTMENT	BAR SIZE	BAR MARK
12	A	4	13

NOTE: MINIMUM COVER FOR BAR REINFORCEMENT 2" UNLESS OTHERWISE NOTED.

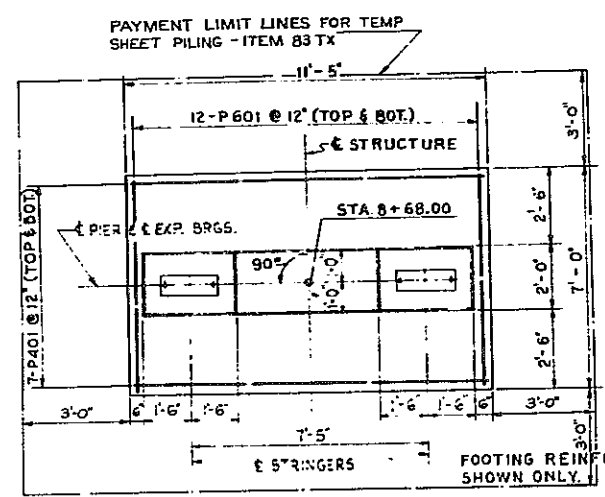
PEDESTRIAN OVERPASS-E.B. STA 191+09.37			
ABUTMENT REINFORCEMENT			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BIRLL	DRAWN	L. M.	
ENGINEERS-ARCHITECTS	CHECKED	W. T.	
NEW YORK	TRACED	L. M.	

BD. SHEET NO. 4

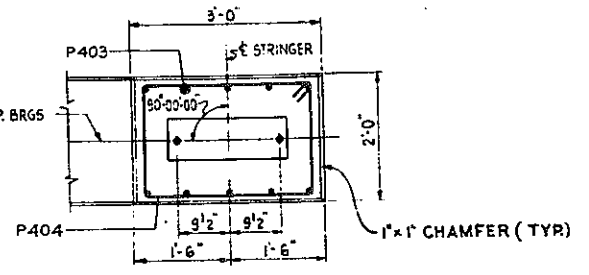


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(23)	73 RI	173

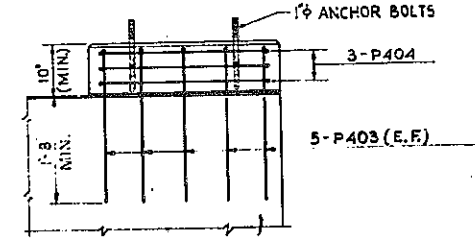
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN - PIER 1
SCALE: 3/8" = 1'-0"

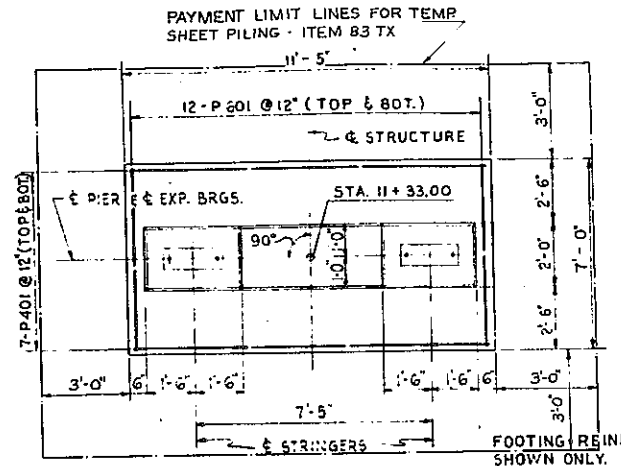


PLAN



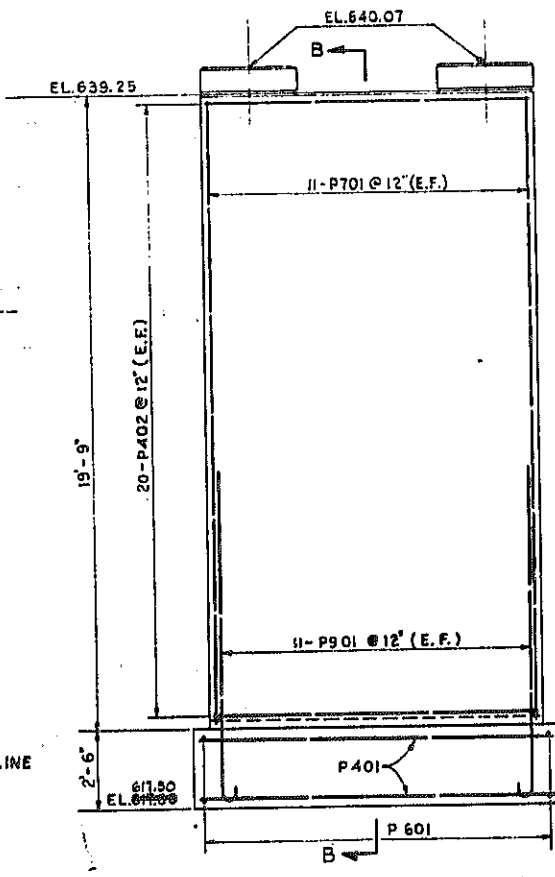
ELEVATION

PEDESTAL DETAILS
SCALE: 3/4" = 1'-0"



PLAN - PIER 5

SCALE: 3/8" = 1'-0"

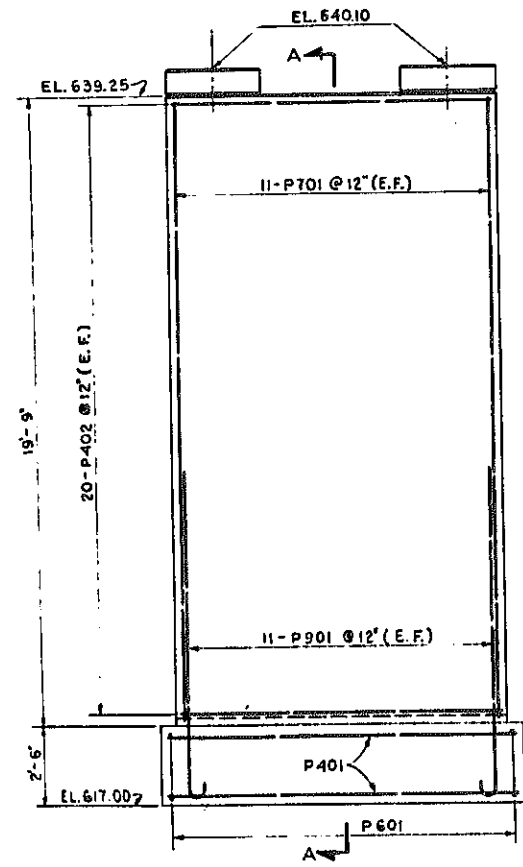


ELEVATION - PIER 5

SCALE: 3/8" = 1'-0"

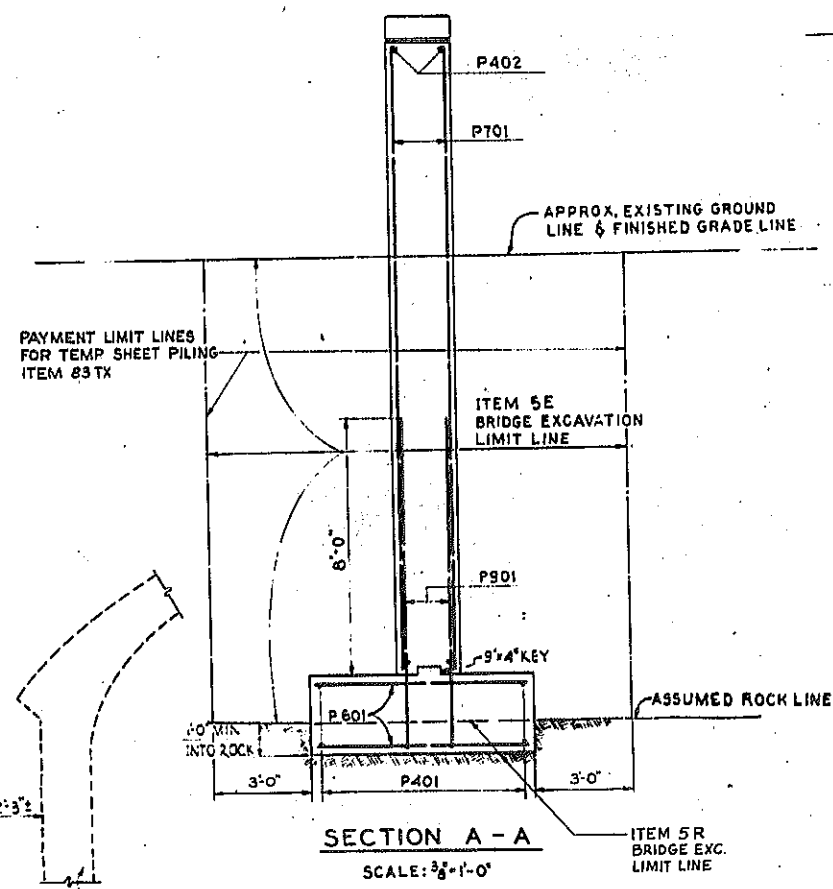
BAR REINFORCING LEGEND

NUMBER BARS	PIER	BAR SIZE	BAR MARK
12	P	6	○



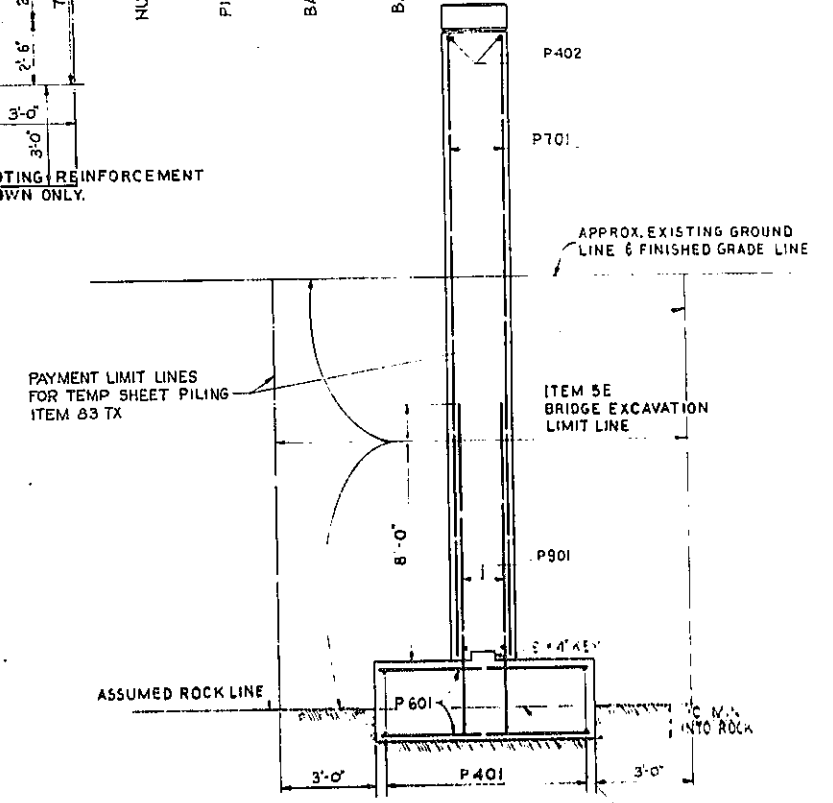
ELEVATION - PIER 1

SCALE: 3/8" = 1'-0"



SECTION A - A

SCALE: 3/8" = 1'-0"



SECTION B - B
SCALE: 3/8" = 1'-0"

NOTES:

1. FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS / SQ. FT.
2. CONCRETE IN PIER FOOTINGS SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
3. CONCRETE IN PIER STEMS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
4. ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.
5. MINIMUM COVER 2", EXCEPT FOOTING 3".

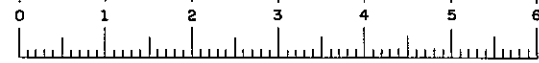
DR. SHEET NO. 5

REVISED AS SHOWN 1/7

PEDESTRIAN OVERPASS - E B STA 191+09.37
PIERS 1 & 5

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

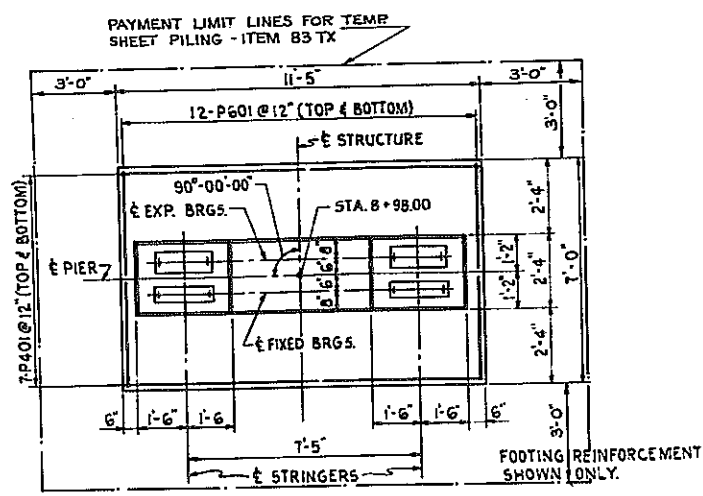
DE LEUW, CATHEN & BRILL	DRAWN	E. B.
ENGINEERS-ARCHITECTS	CHECKED	W. T.
NEW YORK	TRACED	E. B.



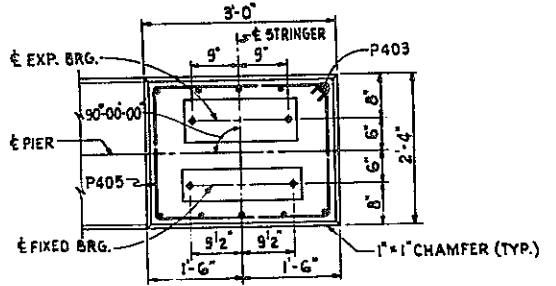
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377(23)	74	173

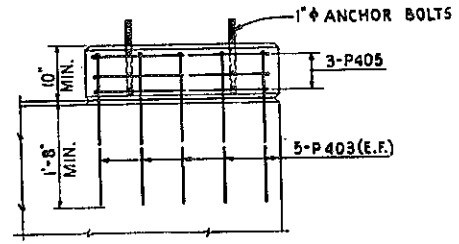
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN-PIER 2
SCALE: 3/8" = 1'-0"

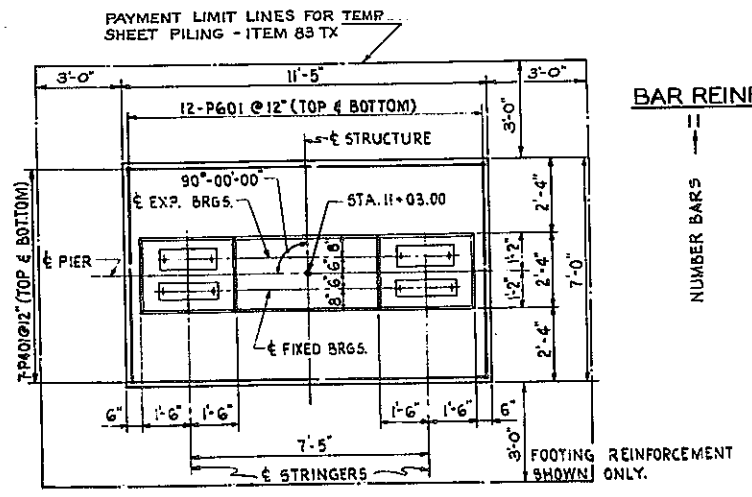


PLAN



ELEVATION

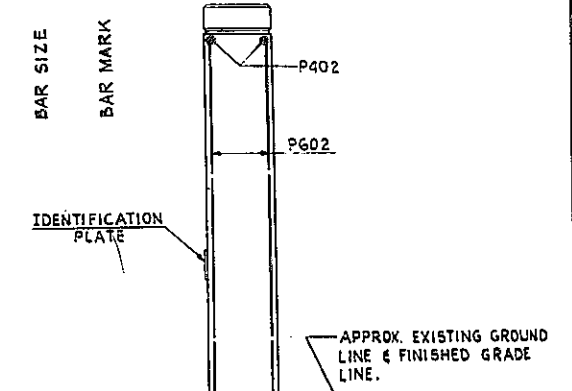
PEDESTAL DETAILS
SCALE: 3/4" = 1'-0"



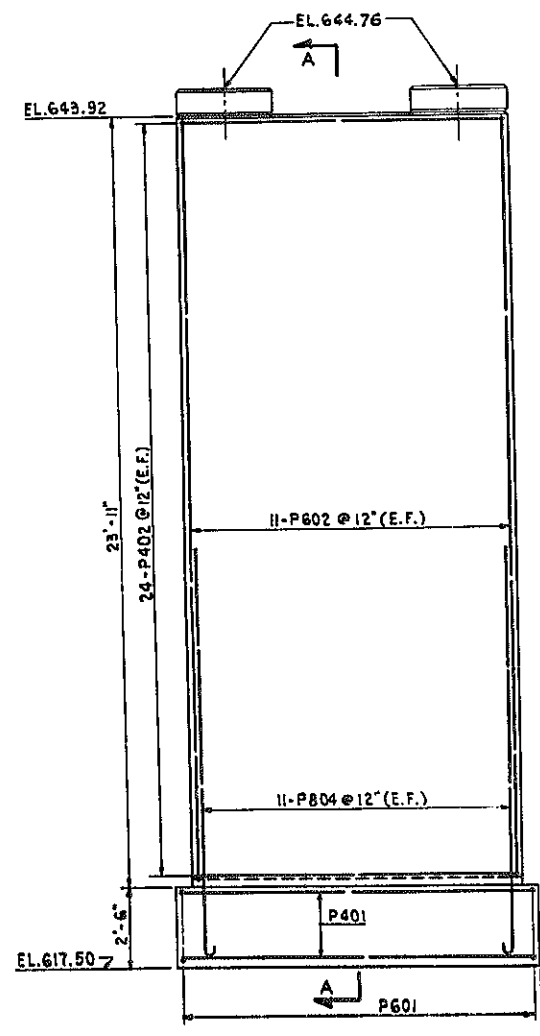
PLAN-PIER 4
SCALE: 3/8" = 1'-0"

BAR REINFORCING LEGEND

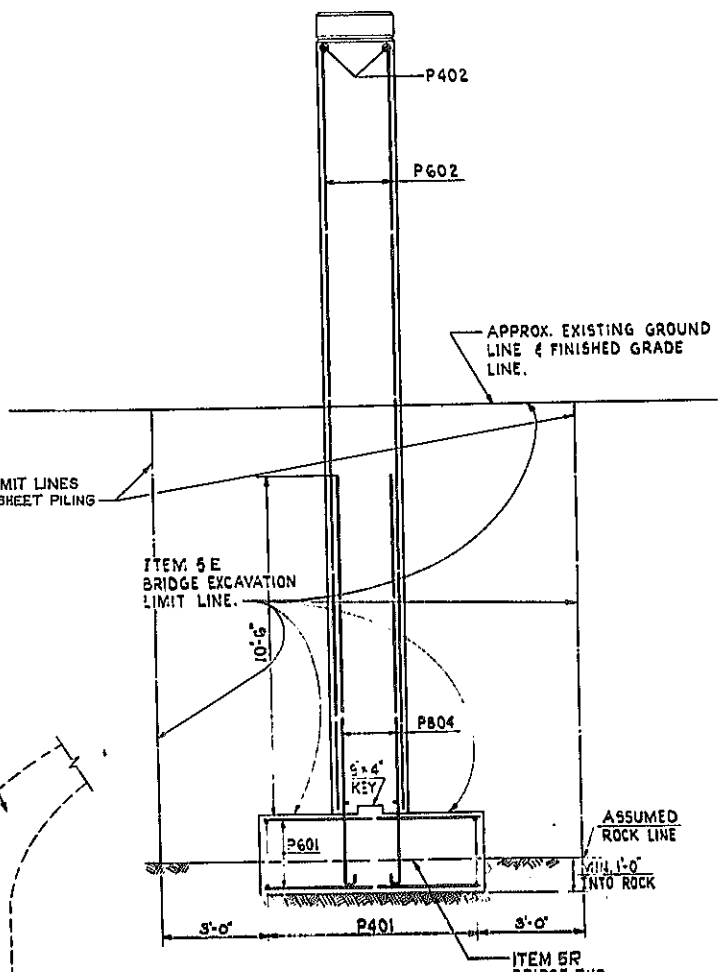
NUMBER BARS	PIER	BAR SIZE	BAR MARK
11	P	6	02



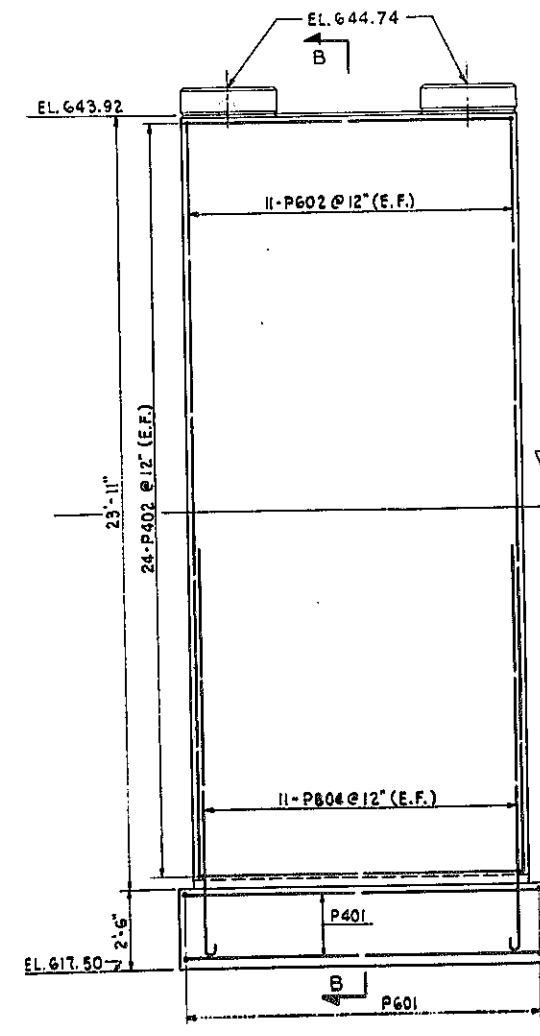
SECTION B-B
SCALE: 3/8" = 1'-0"



ELEVATION-PIER 2
SCALE: 3/8" = 1'-0"



SECTION A-A
SCALE: 3/8" = 1'-0"



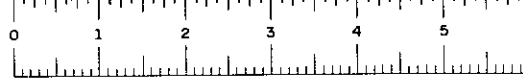
ELEVATION-PIER 4
SCALE: 3/8" = 1'-0"



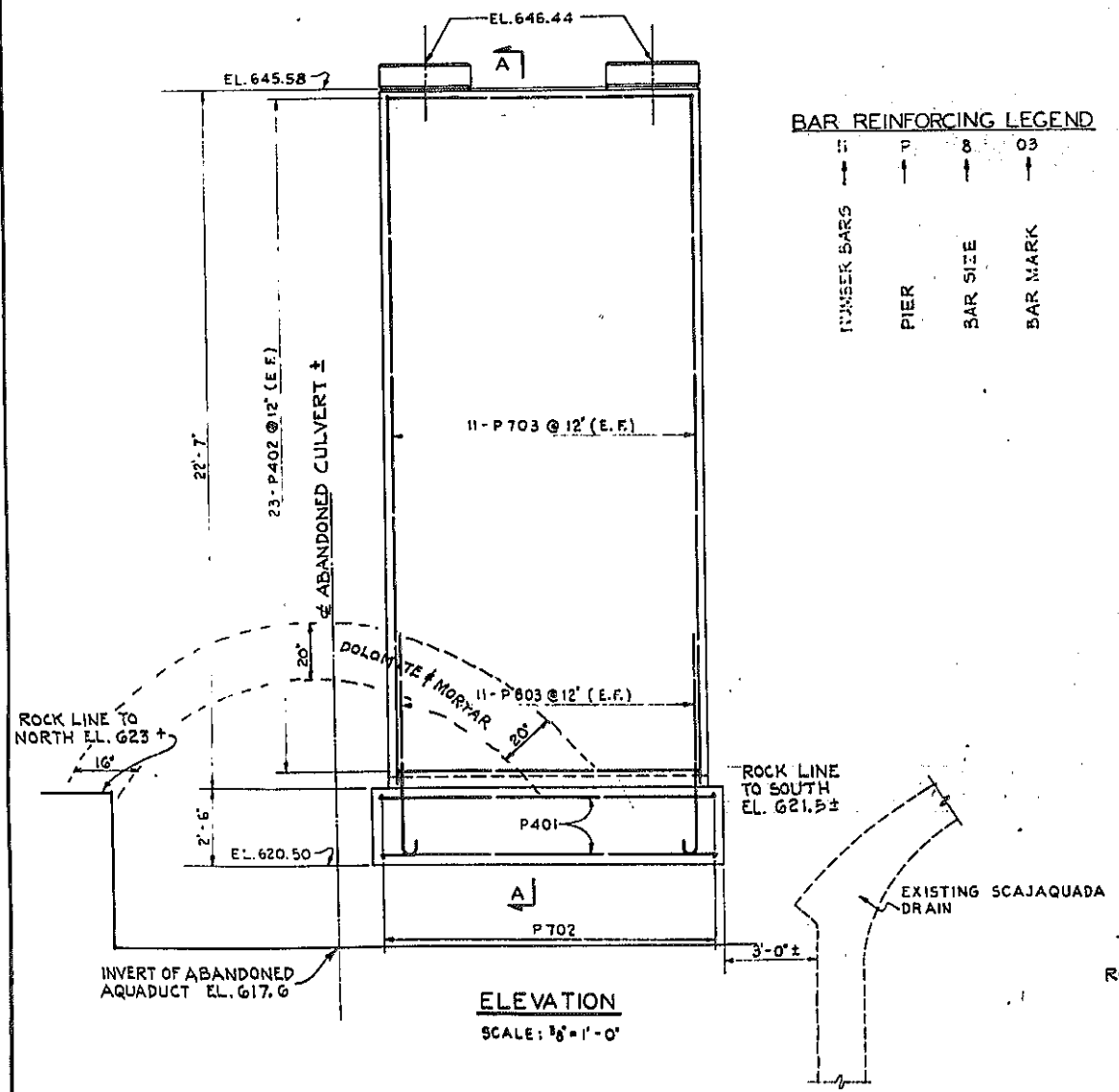
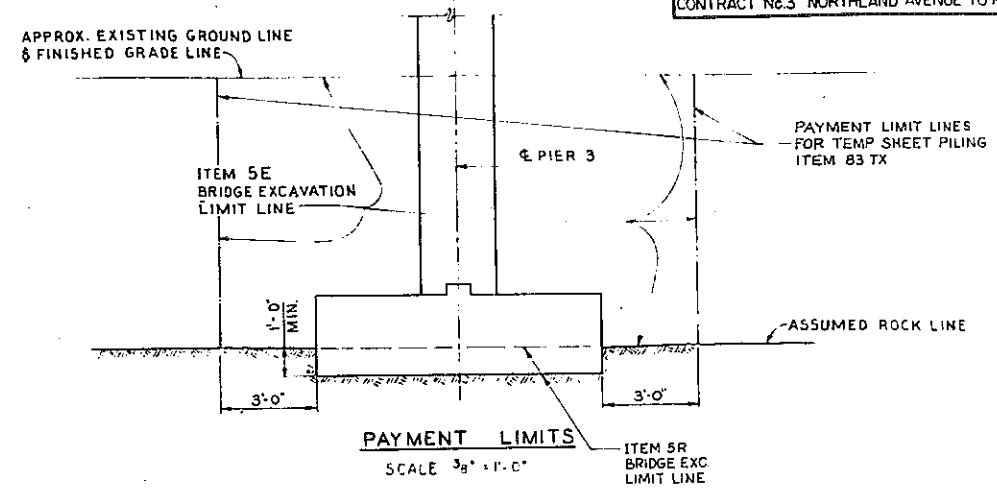
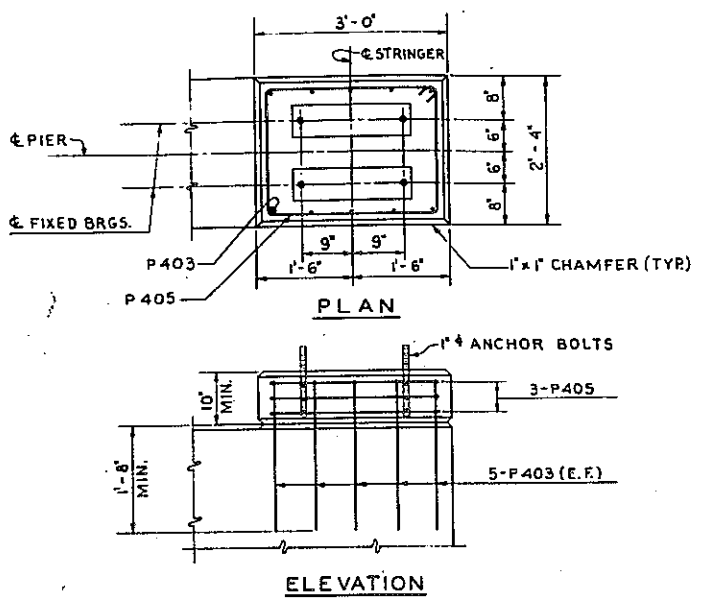
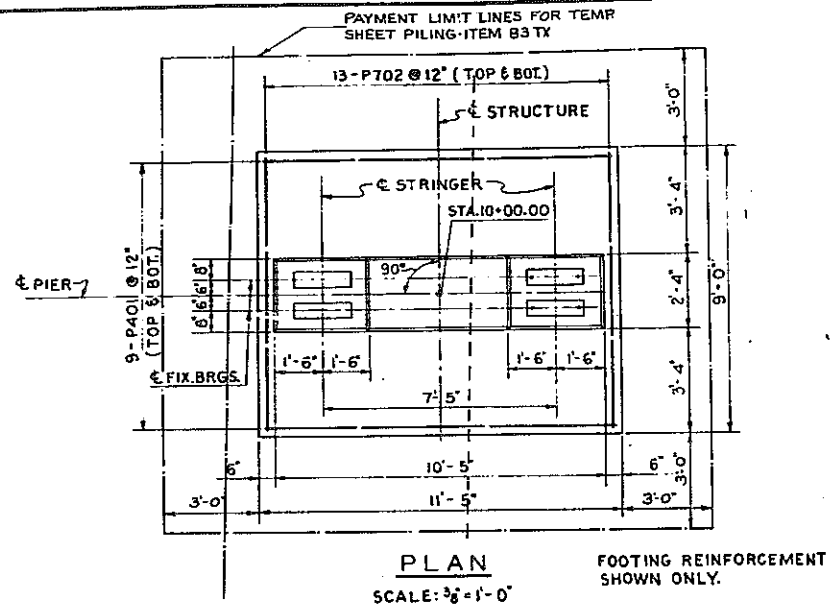
- NOTES:**
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - CONCRETE IN PIER FOOTINGS SHALL BE ITEM 20Y, CLASS I CONCRETE.
 - CONCRETE IN PIER STEMS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.
 - ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.
 - MINIMUM COVER 2", EXCEPT FOOTING 3".

PEDESTRIAN OVERPASS-E.B. STA. 191+09.37			
PIERS 2 & 4			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL	DRAWN	L.M.	
ENGINEERS-ARCHITECTS	CHECKED	W.T.	
NEW YORK	TRACED	L.M.	

DR. SHEET NO. 6

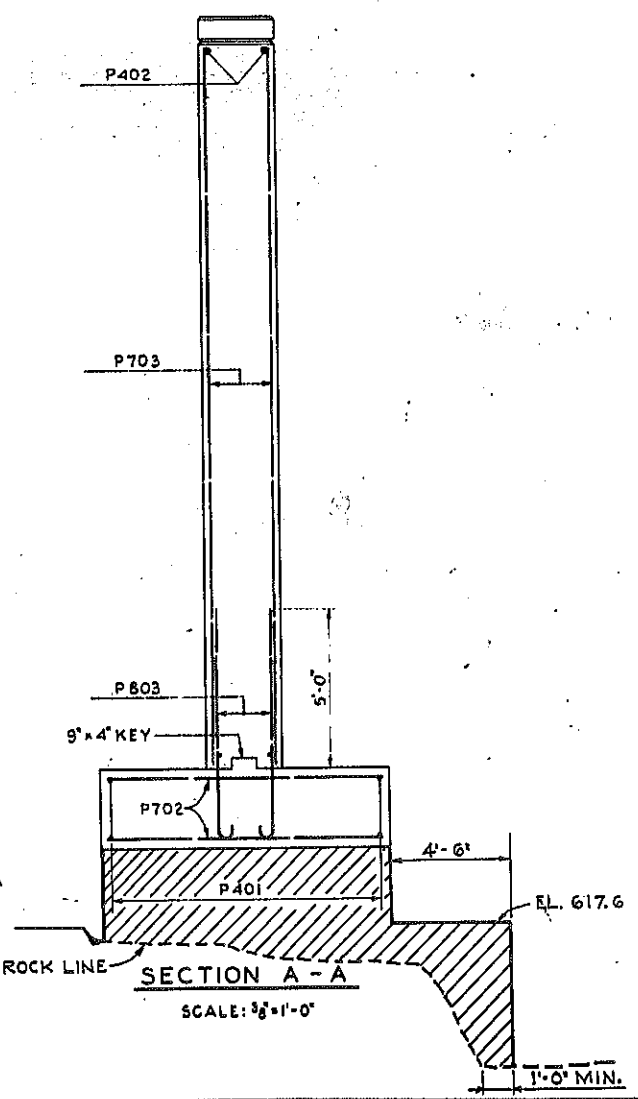


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	15 R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT No. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



BAR REINFORCING LEGEND

↑	↑	↑	↑
11	7	8	03
NUMBER BARS	PIER	BAR SIZE	BAR MARK

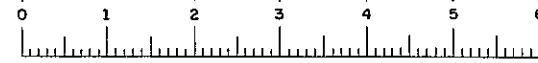


- NOTES:**
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - CONCRETE IN PIER FOOTINGS SHALL BE ITEM 207, CLASS 1 CONCRETE.
 - CONCRETE IN PIER STEMS SHALL BE ITEM 18A, CLASS 1A CONCRETE FOR STRUCTURES.
 - ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.
 - MINIMUM COVER 2", EXCEPT FOOTING 3"

REVISED AS SHOWN

PEDESTRIAN OVERPASS - E B STA 191+09.37 PIER 3			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E. B. W. T. E. B.	

BR. SHEET NO. 7

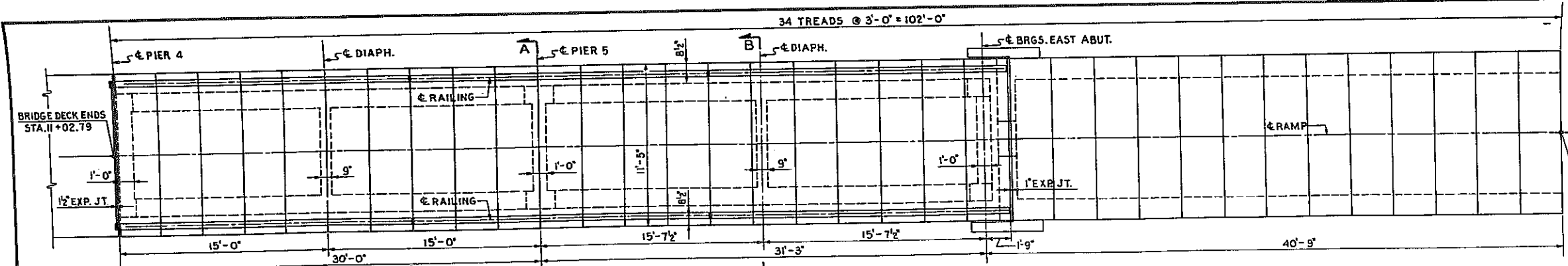


FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	76	173

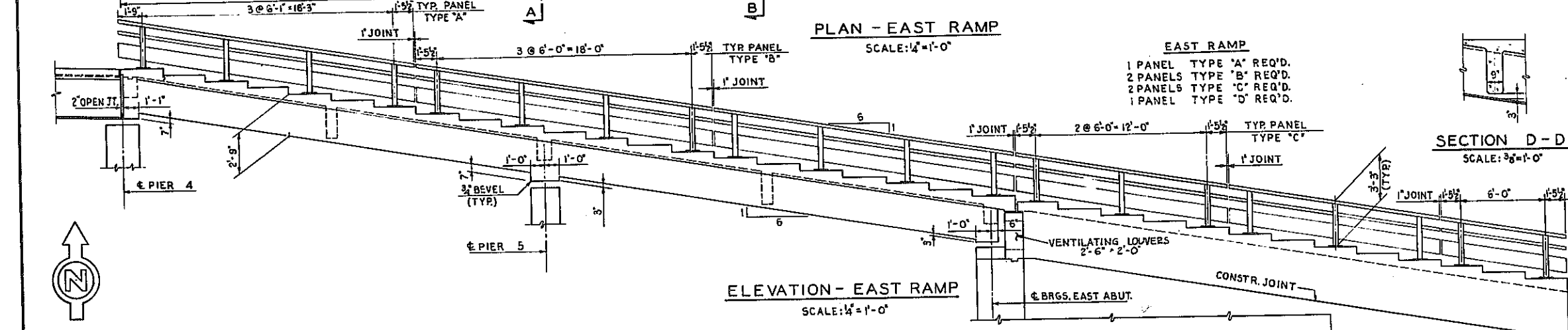
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE

BEARINGS				
EXPANSION BEARINGS				
LOCATION	STRINGER NO.	"A"	"B"	TOTAL HT.
W.ABUT.	ALL	2	1 1/2"	4 1/2"
PIER 1	ALL	2	1 1/2"	4 1/2"
PIER 5	ALL	2	1 1/2"	4 1/2"
E.ABUT.	ALL	2	1 1/2"	4 1/2"
FIXED BEARINGS				
LOCATION	STRINGER NO.	"A"	"B"	TOTAL HT.
PIER 2	ALL	2	1 1/2"	3"
PIER 4	ALL	2	1 1/2"	3"



PLAN - EAST RAMP
SCALE: 1/4" = 1'-0"

EAST RAMP
1 PANEL TYPE 'A' REQ'D.
2 PANELS TYPE 'B' REQ'D.
2 PANELS TYPE 'C' REQ'D.
1 PANEL TYPE 'D' REQ'D.



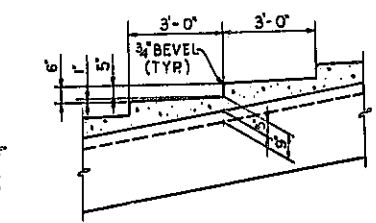
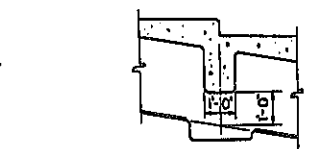
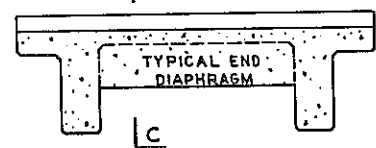
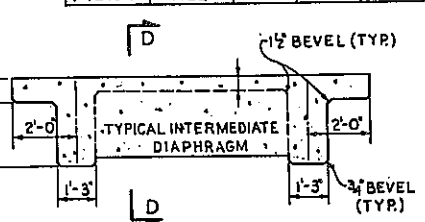
ELEVATION - EAST RAMP
SCALE: 1/4" = 1'-0"

SECTION D-D
SCALE: 3/8" = 1'-0"

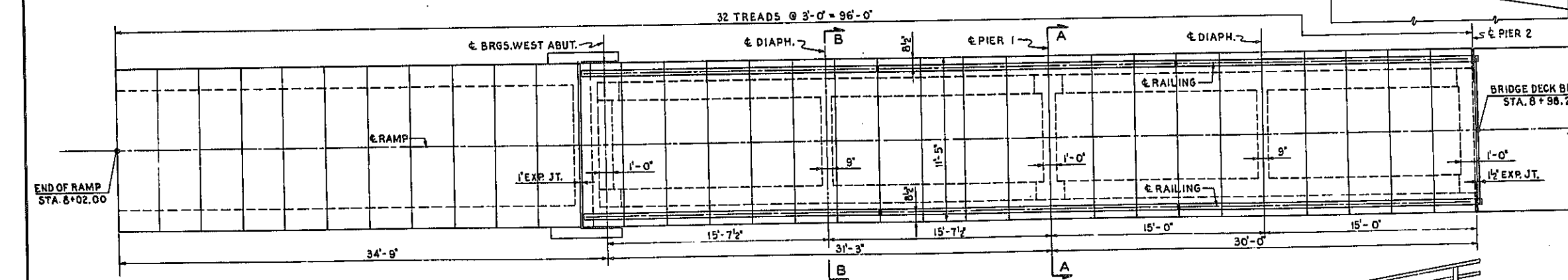
SECTION B-B
SCALE: 3/8" = 1'-0"

SECTION A-A
SCALE: 3/8" = 1'-0"

SECTION C-C
SCALE: 3/8" = 1'-0"

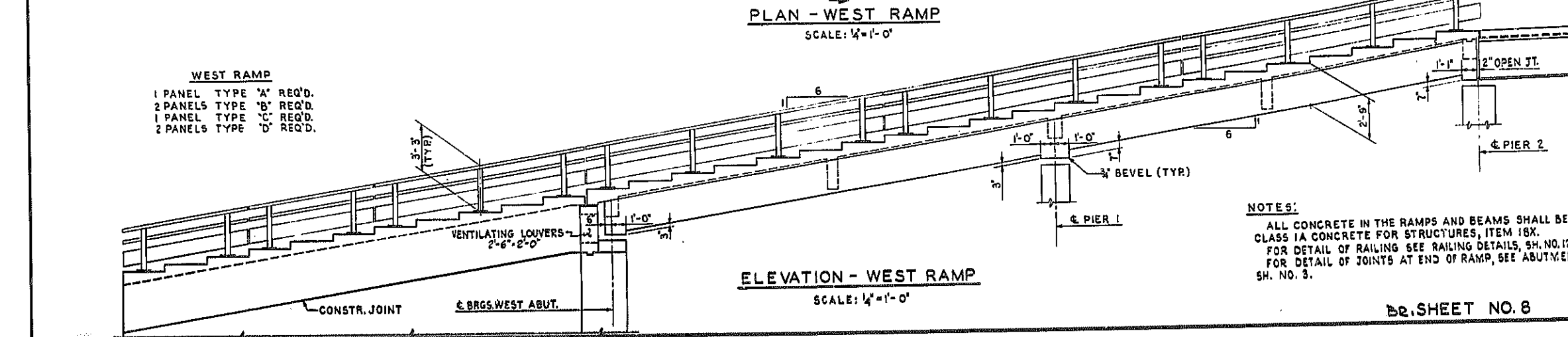


TYPICAL LONGITUDINAL SECTION
SCALE: 3/8" = 1'-0"



PLAN - WEST RAMP
SCALE: 1/4" = 1'-0"

WEST RAMP
1 PANEL TYPE 'A' REQ'D.
2 PANELS TYPE 'B' REQ'D.
1 PANEL TYPE 'C' REQ'D.
2 PANELS TYPE 'D' REQ'D.



ELEVATION - WEST RAMP
SCALE: 1/4" = 1'-0"

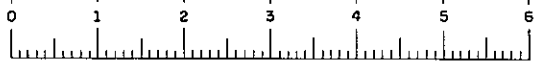
NOTES:
ALL CONCRETE IN THE RAMPS AND BEAMS SHALL BE CLASS 1A CONCRETE FOR STRUCTURES, ITEM 18X.
FOR DETAIL OF RAILING SEE RAILING DETAILS, SH. NO. 12.
FOR DETAIL OF JOINTS AT END OF RAMP, SEE ABUTMENTS SH. NO. 3.

DR. SHEET NO. 8

PEDESTRIAN OVERPASS - E. B. STA. 191+09.37
RAMPS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

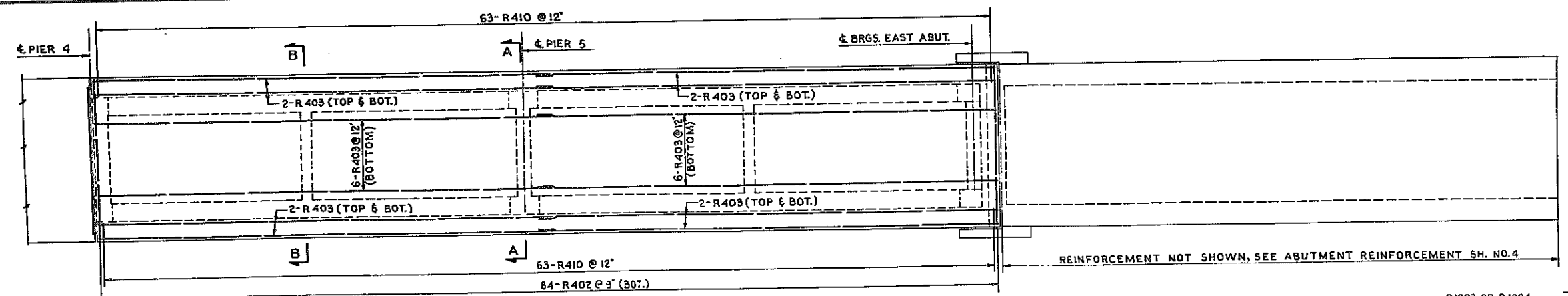
DR. LEUW, GAYHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E. B. W. T. E. B.
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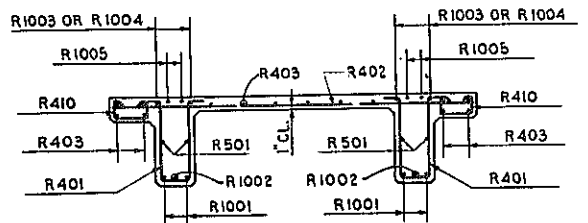
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	77	173

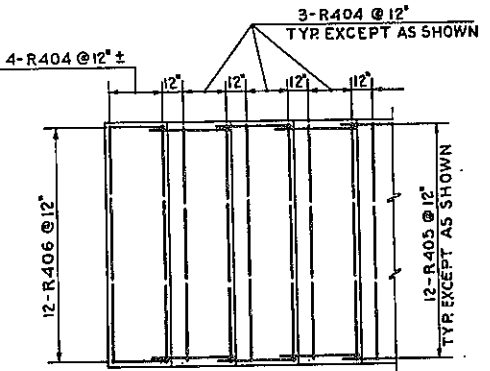
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE



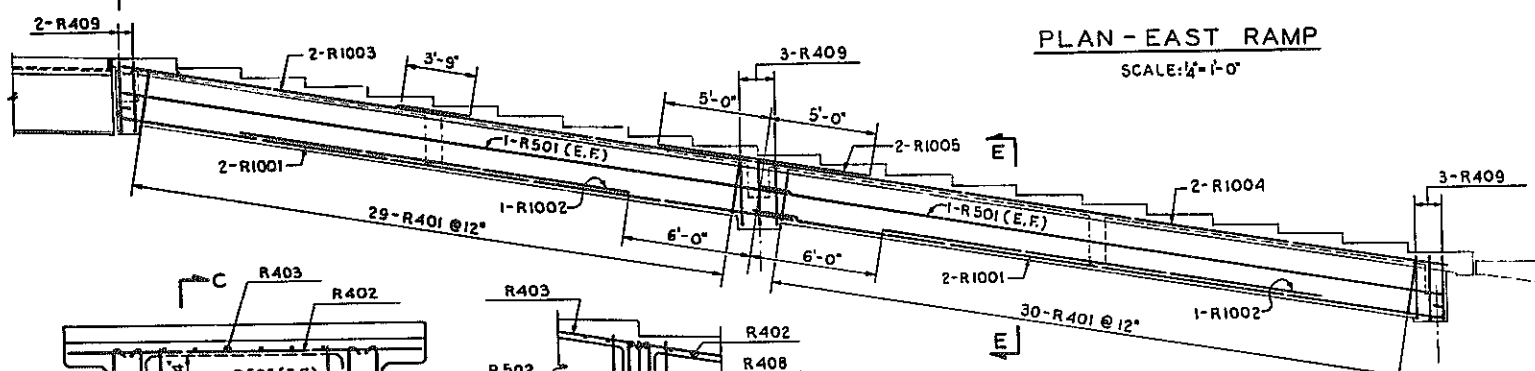
PLAN - EAST RAMP
SCALE: 1/4" = 1'-0"



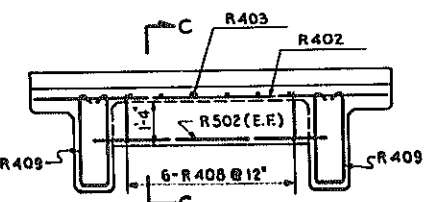
SECTION E-E
SCALE: 3/8" = 1'-0"



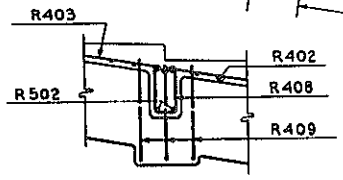
PLAN



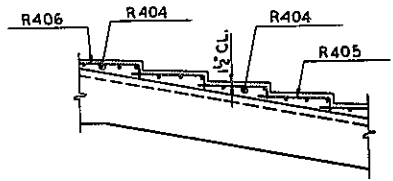
ELEVATION - EAST RAMP
SCALE: 1/4" = 1'-0"



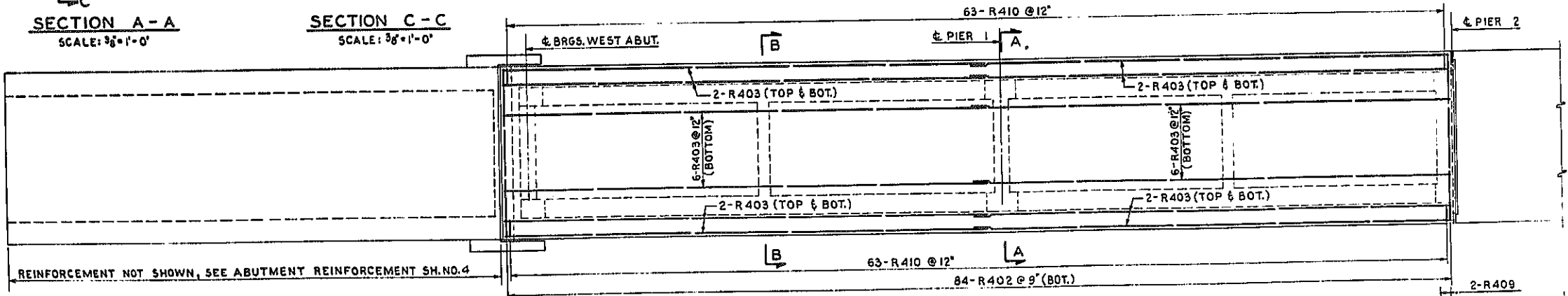
SECTION A-A
SCALE: 3/8" = 1'-0"



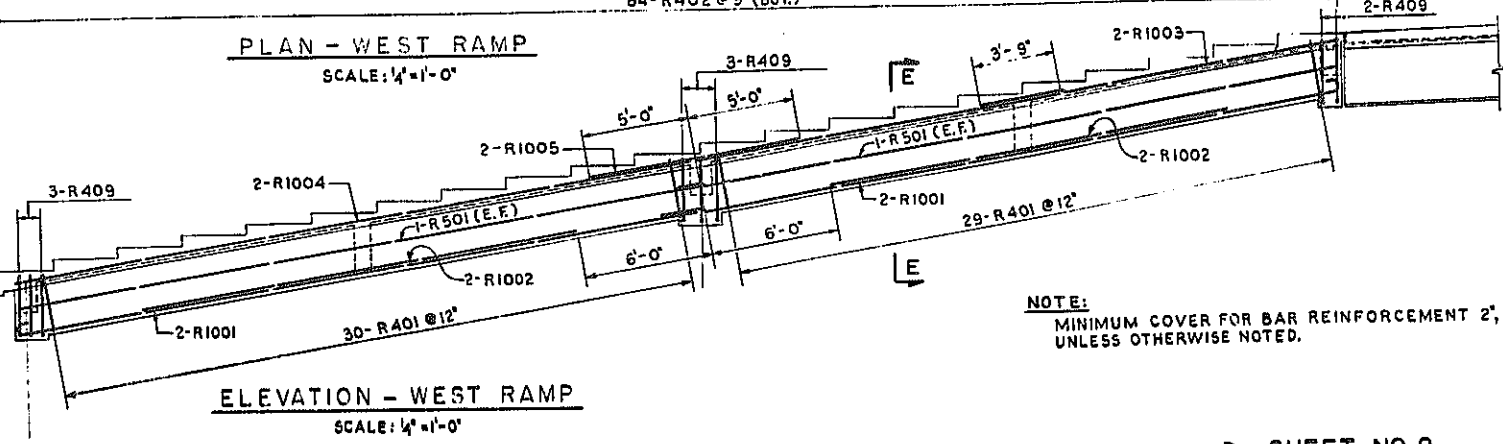
SECTION C-C
SCALE: 3/8" = 1'-0"



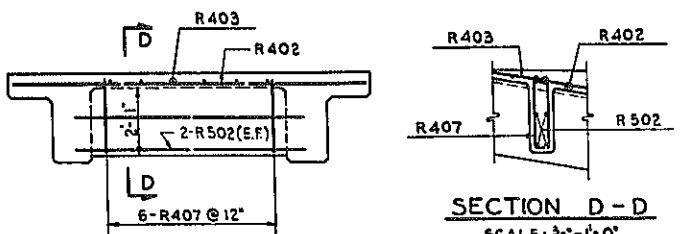
ELEVATION
STEP DETAILS
SCALE: 1/4" = 1'-0"



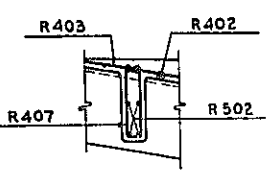
PLAN - WEST RAMP
SCALE: 1/4" = 1'-0"



ELEVATION - WEST RAMP
SCALE: 1/4" = 1'-0"



SECTION B-B
SCALE: 3/8" = 1'-0"



SECTION D-D
SCALE: 3/8" = 1'-0"

BAR REINFORCING LEGEND

NUMBER BARS	RAMP	BAR SIZE	BAR MARK
3	↑	R	4
4	↑	R	04

NOTE:
MINIMUM COVER FOR BAR REINFORCEMENT 2",
UNLESS OTHERWISE NOTED.

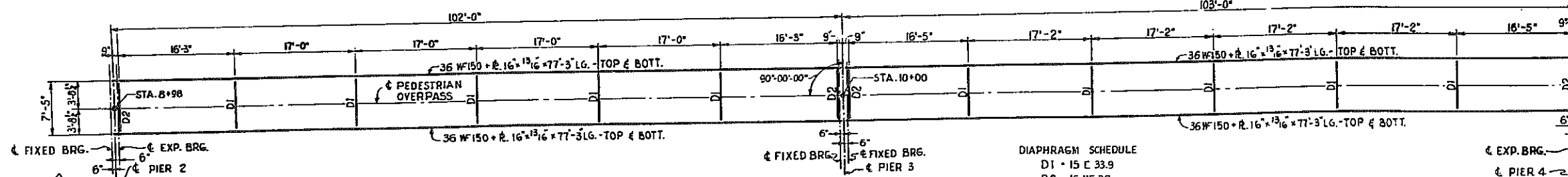
PEDESTRIAN OVERPASS - E. B. STA. 191+09.37
RAMP REINFORCEMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E. B. W. T. E. B.
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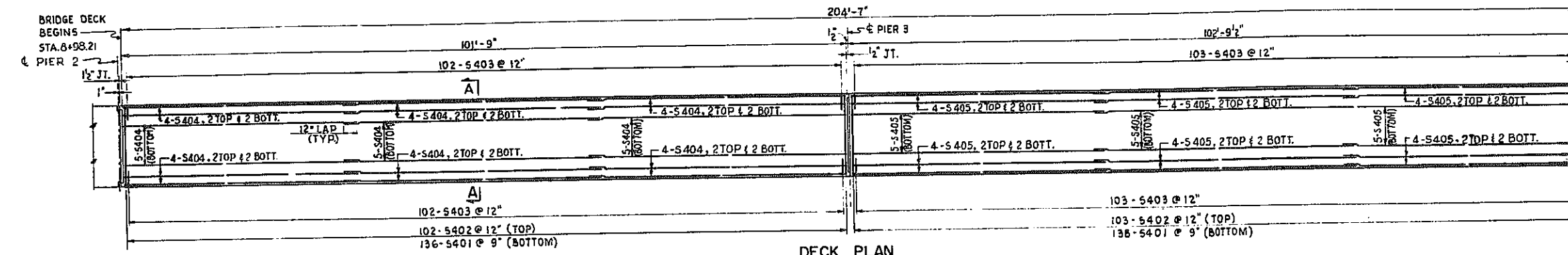
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377 (25)	76	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



BAR REINFORCING LEGEND

NUMBER BARS	SLAB	BAR SIZE	BAR MARK
4	↑	4	05
5	↑	5	05
4	↑	4	05
05	↑	05	05

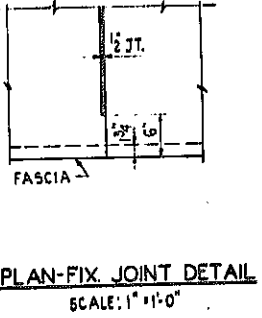
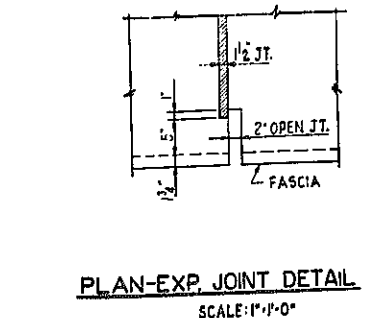
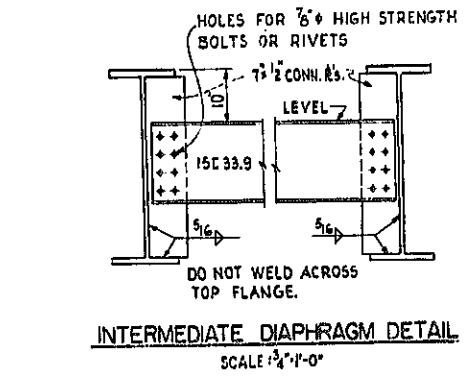
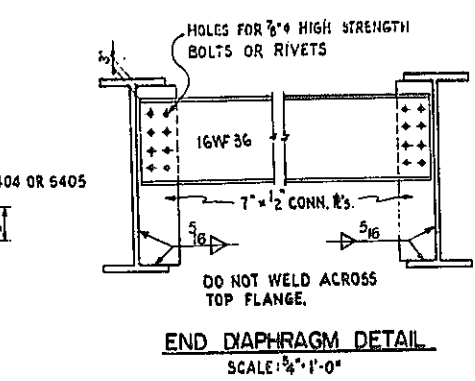
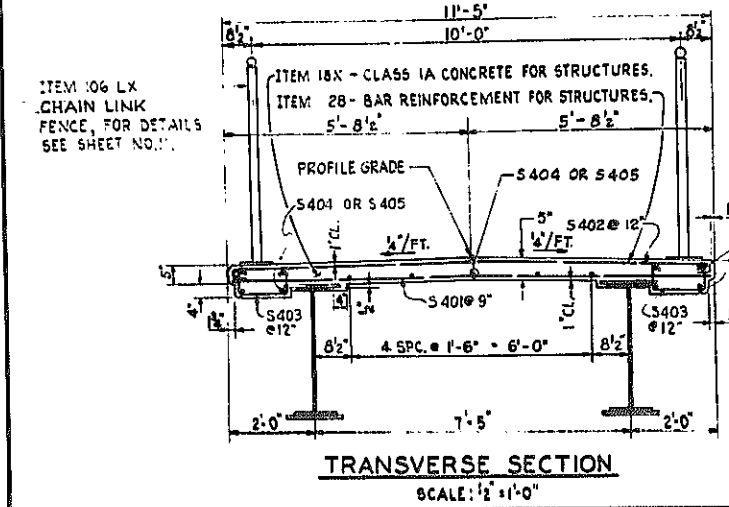
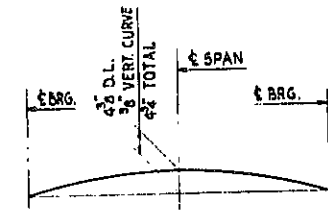
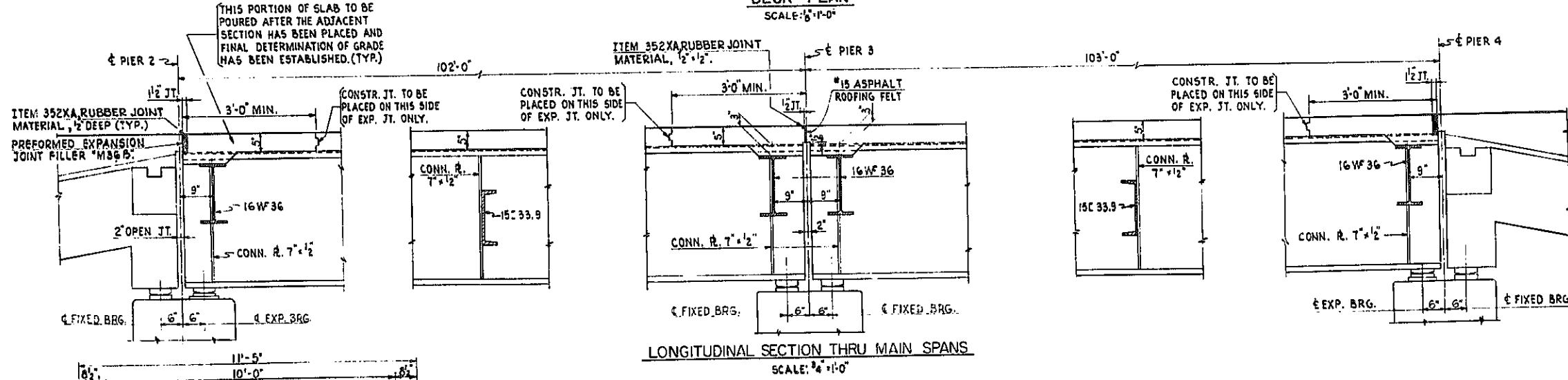


BEARINGS

EXPANSION BEARINGS				
LOCATION	STRINGER	NO.	"A"	TOTAL HT.
PIER 2	ALL	2	14"	4 1/2"
PIER 4	ALL	2	14"	4 1/2"

FIXED BEARINGS

LOCATION	STRINGER	NO.	"B"	TOTAL HT.
PIER 3	ALL	4	1 1/2"	3"



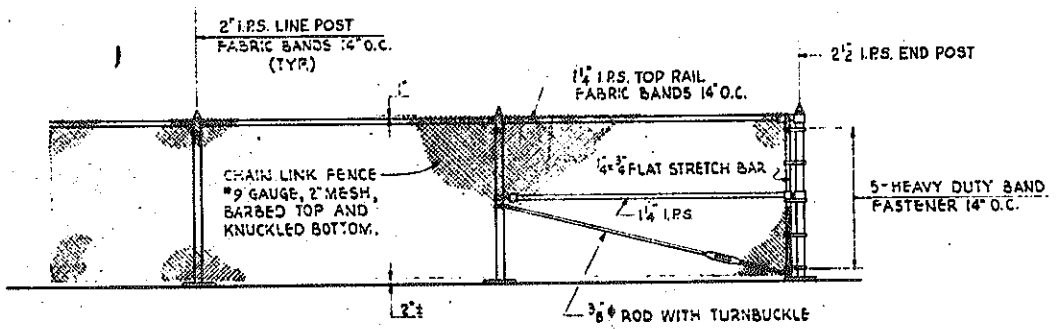
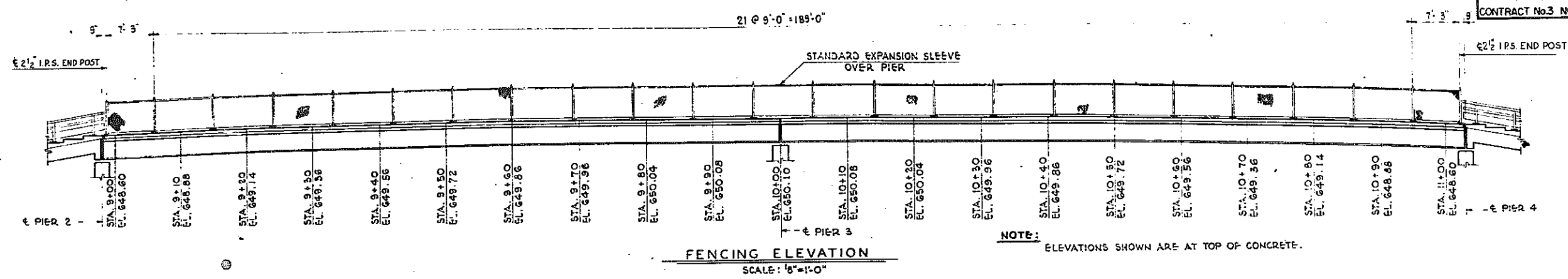
NOTE: HOLES & BOLTS IN DIAPHRAGM CONNECTIONS MAY BE OMITTED AND WELDING SUBSTITUTED IF CONTRACTOR SO ELECTS.

PEDESTRIAN OVERPASS - E. B. STA. 191+09.37
FRAMING & DECK PLAN

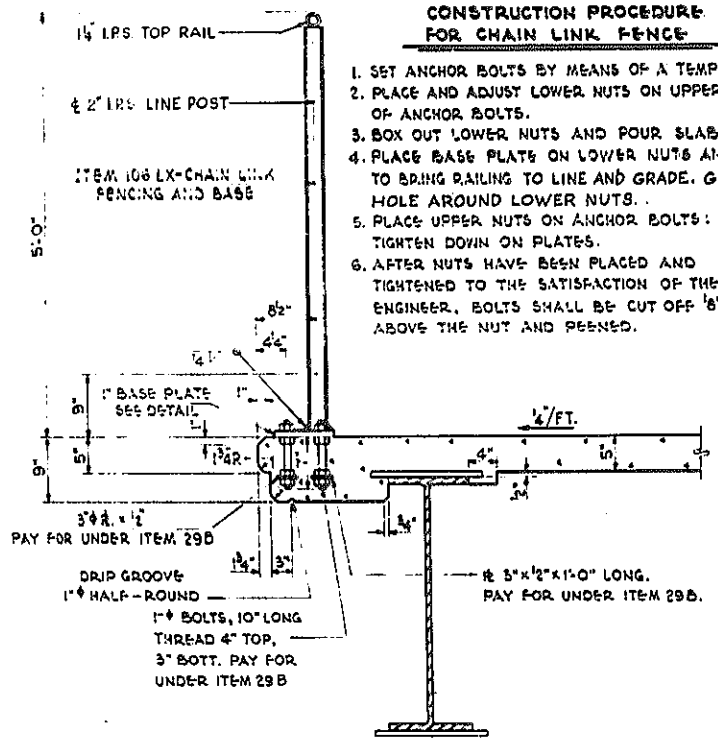
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	P.C.
ENGINEERS-ARCHITECTS	CHECKED	W.T.
NEW YORK	TRACED	P.C.

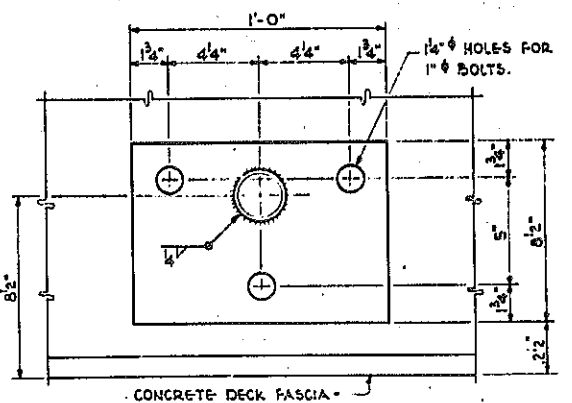
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	0-06-377(25)	79	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE				



NOTE: THIS SHEET SUPERSEDED BY FIELD CHANGE SH. No. 79 S.



PARTIAL TRANSVERSE SECTION THRU SUPERSTRUCTURE
SCALE: 1"=1'-0"



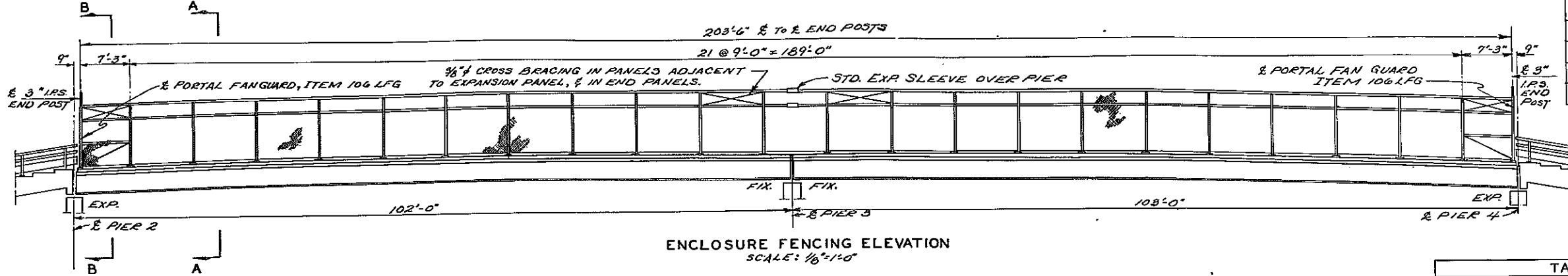
- CHAIN LINK FENCING NOTES**
1. SIZES OF RAILS AND POSTS ARE GIVEN IN NOMINAL PIPE SIZES (I.P.S.)
 2. ALL FENCING POSTS TO BE SET TRULY VERTICAL
 3. FOR REQUIREMENTS OF BASE PLATE AND CHAIN LINK FENCE, SEE SPECIAL PROVISIONS ITEM 106 LX.
 4. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
 5. PIPE RAILS AND POSTS SHALL MEET ALL THE REQUIREMENTS OF A.S.T.M. A-120-54, STANDARD WEIGHT PIPE (SCHEDULE 40).

REVISION NOTE ADDED

PEDESTRIAN OVERPASS - E. B. STA. 191+09.37		
CHAIN LINK FENCING DETAILS		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	A.C. W. T. A. C.

BR. SHEET NO. II

FIELD CHANGE

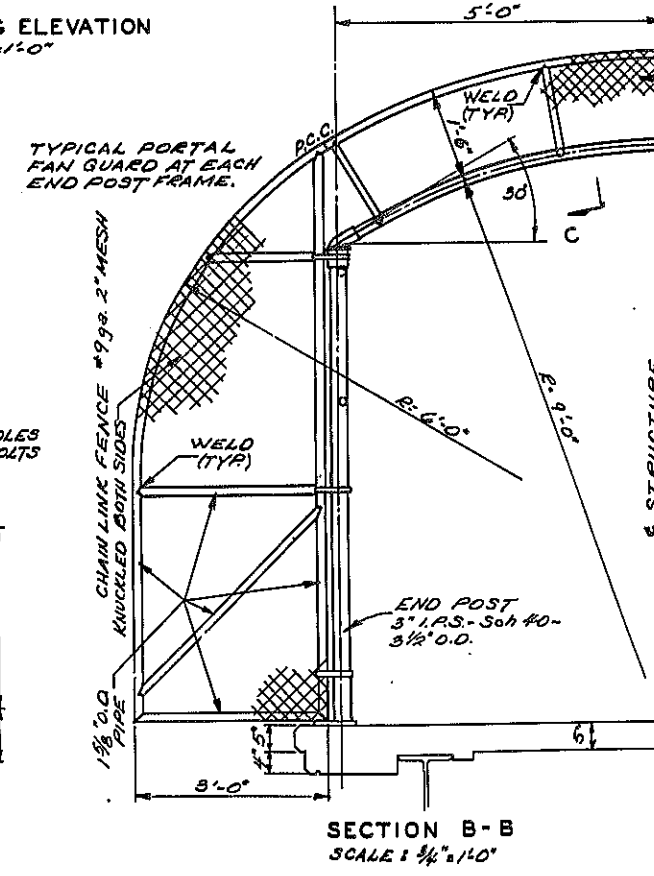
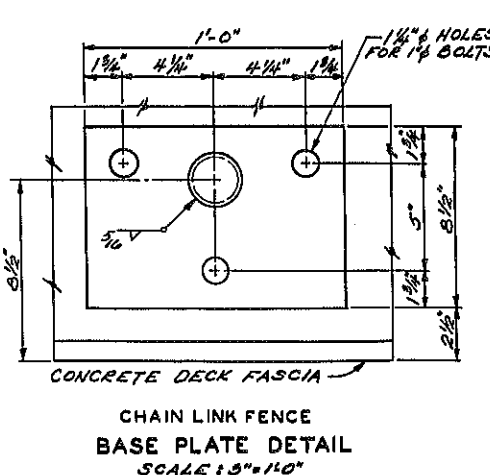
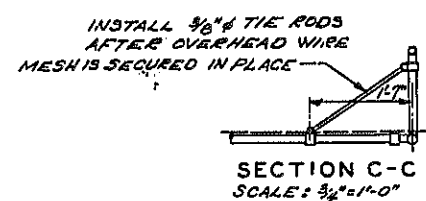
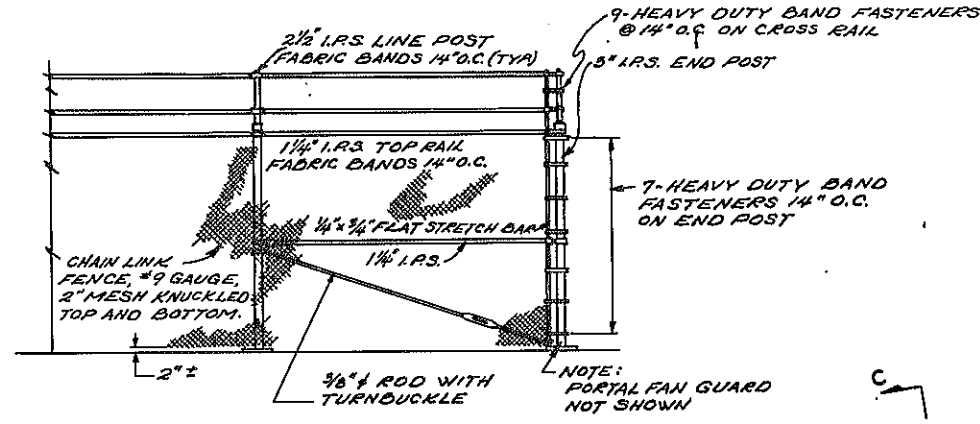
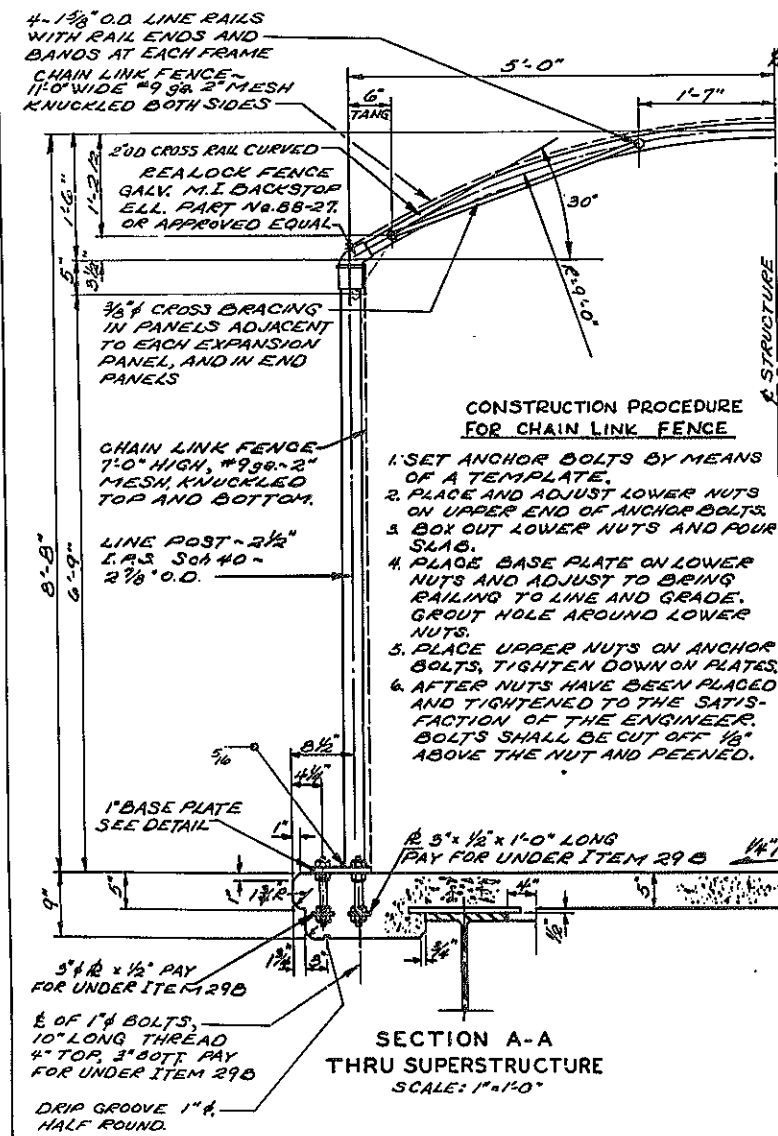


FED. RD. REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	NY	U-UG-377(25)	173	173

KENSINGTON EXPRESSWAY, SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVE. TO FILLMORE AVE.

FIELD CHANGE

ITEM	DESCRIPTION	UNIT	NEAT	APPROX.
106LXS	CHAIN LINK FENCING ENCLOSURE ON PEDESTRIAN OVERPASS.	L.F.	204'	205'
106LFG	CHAIN LINK FENCE PORTAL FAN GUARDS.	FACH	2	2



APPROVED *V.J. Burns*
V.J. BURNS
DEPUTY CHIEF ENGINEER

PREPARED PURSUANT TO THE HIGHWAY LAW AND RECOMMENDED BY *Norman W. Dwyer*
Nov. 7, 1963 DIST. ENGINEER, DIST. NO. 5

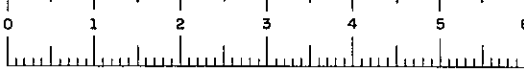
CHAIN LINK FENCING NOTES

1. SIZES OF RAILS AND POSTS ARE GIVEN IN NOMINAL PIPE SIZES (I.R.S.)
2. ALL FENCING POSTS TO BE SET TRULY VERTICAL.
3. FOR REQUIREMENTS OF BASE PLATE AND CHAIN LINK FENCE SEE SPECIAL PROVISIONS ITEM 106 LXS.
4. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29 B.
5. PIPE RAILS AND POSTS SHALL MEET ALL THE REQUIREMENTS OF A.S.T.M. A-120-54, STANDARD WEIGHT PIPE (SCHEDULE 40).

FIELD CHANGE

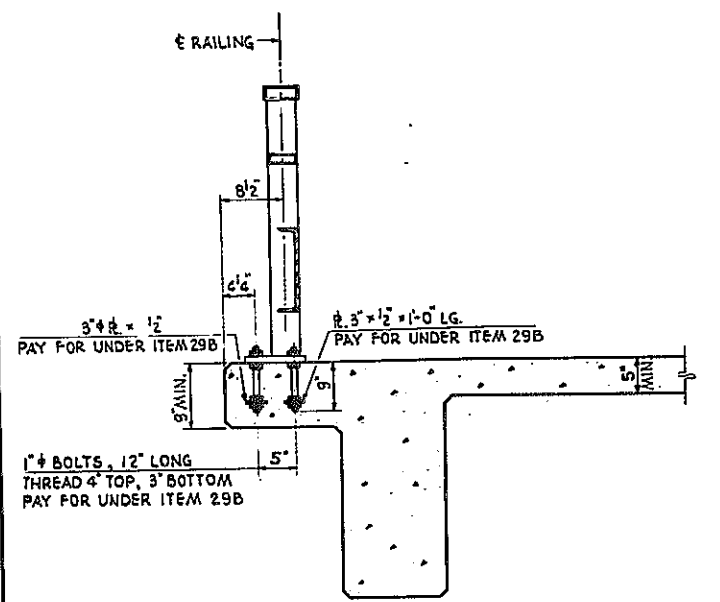
PEDESTRIAN OVERPASS - E.B. STA. 191+09.37	
CHAIN LINK FENCING DETAILS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DRAWN	A.C.
CHECKED	W.T.
TRACED	ADG.

BR. SHEET NO. IIS

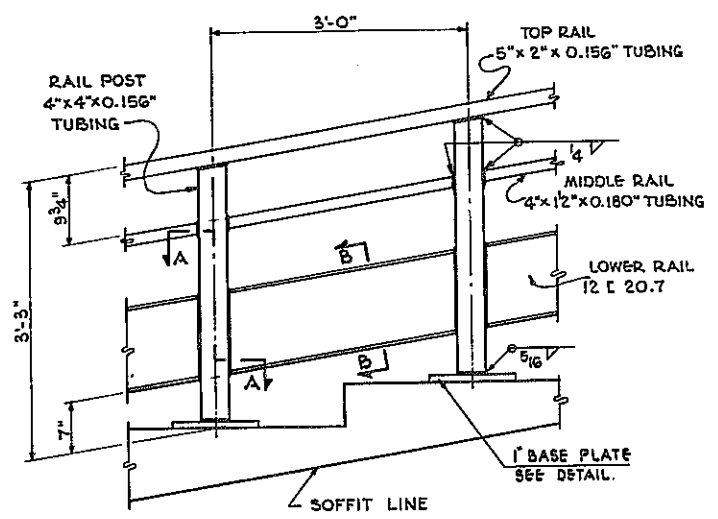


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	80	173

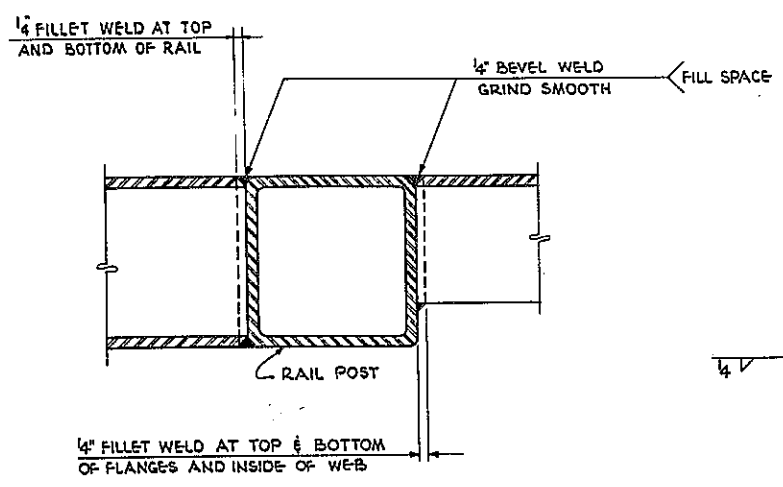
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE



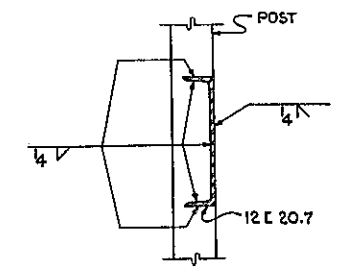
PARTIAL TRANSVERSE SECTION THRU RAMP
SCALE: 1"=1'-0"



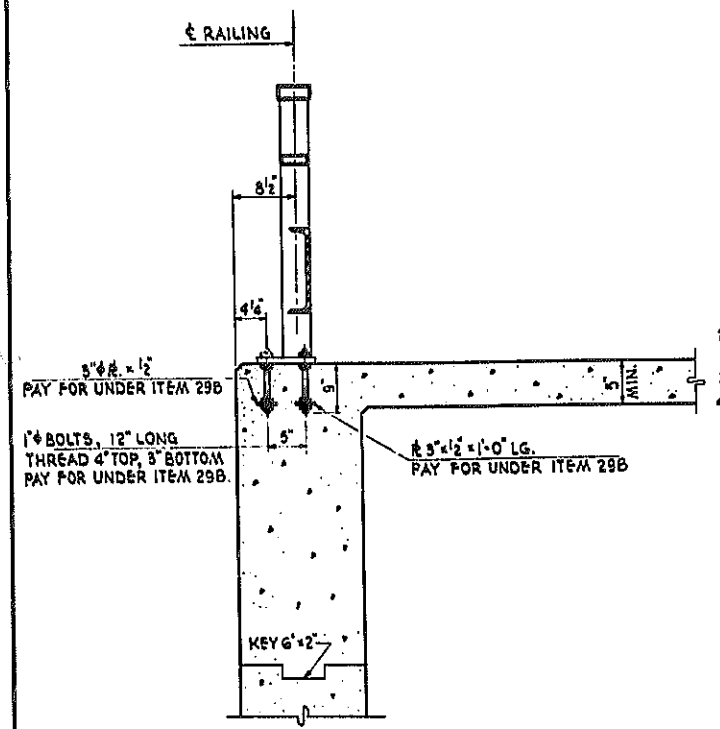
PARTIAL RAIL ELEVATION
SCALE: 1"=1'-0"



SECTION A-A
SCALE: HALF SIZE



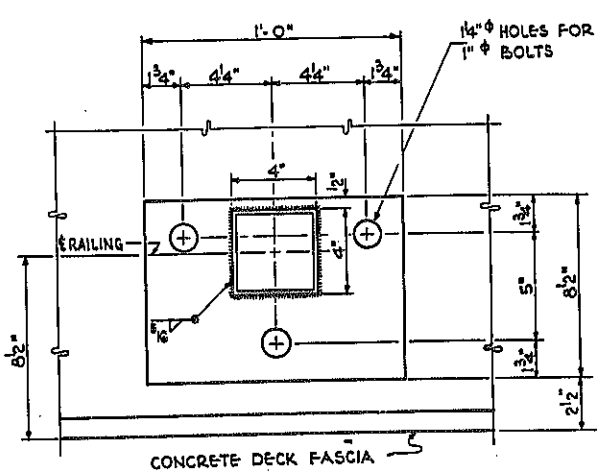
SECTION B-B
SCALE: 1/2"=1'-0"



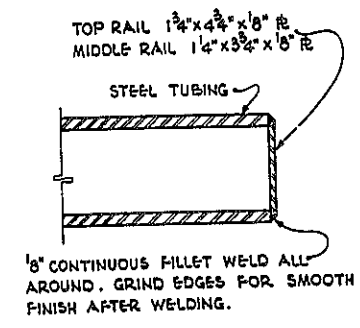
PARTIAL TRANSVERSE SECTION THRU ABUTMENT WING
SCALE: 1"=1'-0"

CONSTRUCTION PROCEDURE FOR RAILING

1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE.
2. PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
3. BOX OUT LOWER NUTS AND POUR SLAB.
4. PLACE BASE PLATE ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE. GROUT HOLE AROUND LOWER NUTS.
5. PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON PLATES.
6. AFTER NUTS HAVE BEEN PLACED AND TIGHTENED TO THE SATISFACTION OF THE ENGINEER, BOLTS SHALL BE CUT OFF 1/8" ABOVE THE NUT AND PEENED.



BASE PLATE DETAIL
SCALE: 5"=1'-0"

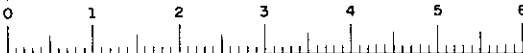


SECTION AT END OF RAILS
NOT TO SCALE

RAILING NOTES

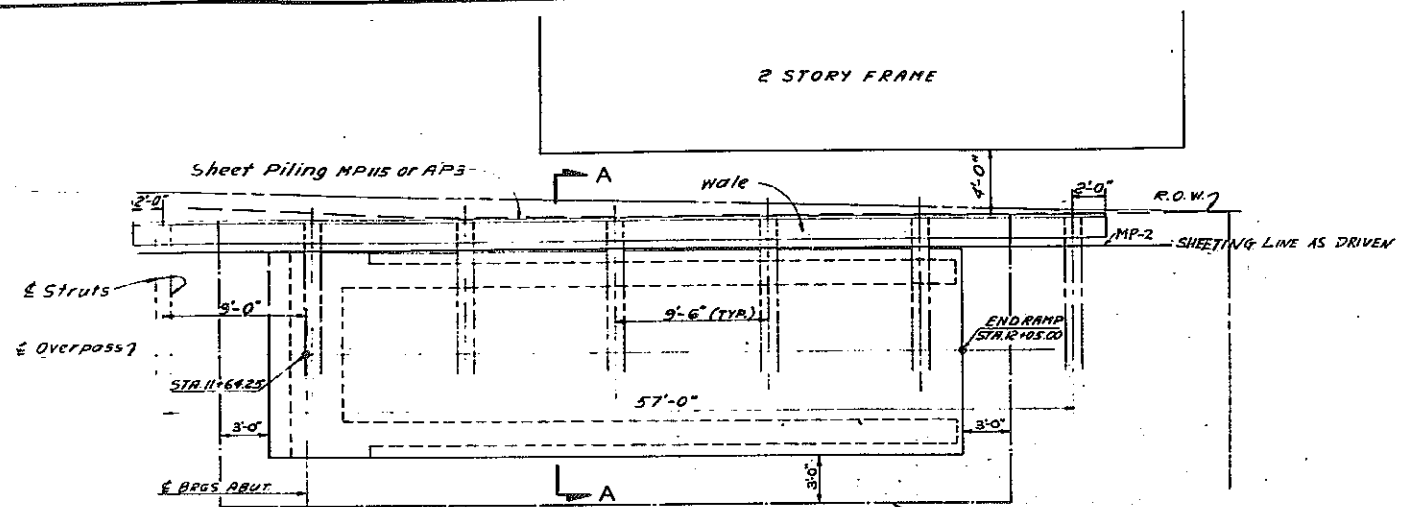
1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
2. ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE SOFFIT LINE OF STEP RAMP AND SO THAT THE POSTS ARE TRULY VERTICAL.
3. ALL SURFACES OF WELDS ON RAILING TO BE GROUND SMOOTH.
4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
7. TUBULAR AND CHANNEL RAILS, TUBULAR POSTS AND BASE PLATES WILL BE PAID FOR UNDER ITEM 27AX.

PEDESTRIAN OVERPASS - E. B. STA. 191+09.37			
RAILING DETAILS			
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHAR & BRILL	DRAWN	A. C.	
ENGINEERS-ARCHITECTS	CHECKED	W. T.	
NEW YORK	TRACED	A. C.	



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	81/1	173

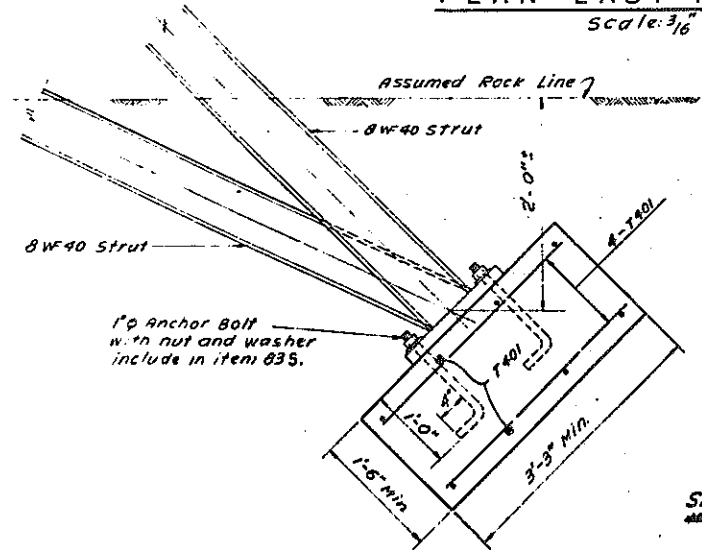
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No 3 NORTHLAND AVENUE TO FILLMORE AVENUE



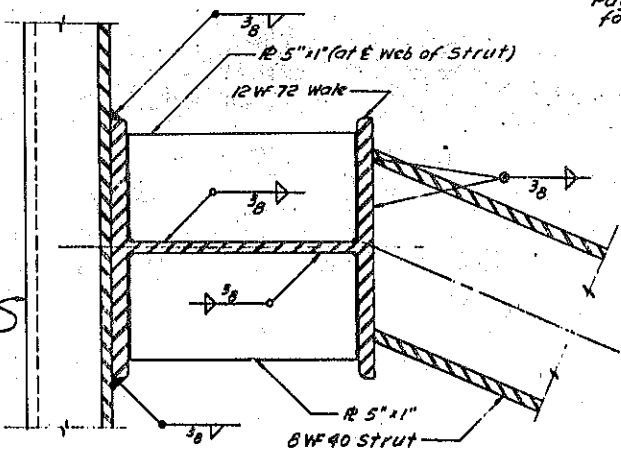
PLAN - EAST ABUTMENT
Scale: 3/16" = 1'-0"

- CONSTRUCTION PROCEDURE FOR SHEETING**
- 1- Drive steel sheet piling where and as indicated on the plans to the required depth.
 - 2- Excavate, within the payment limits shown, to a point two (2) feet below the Top Wale.
 - 3- Install Top Wale as indicated.
 - 4- Excavate, within the payment limits shown, for the installation of the struts. No excavation will be permitted at this time outside the payment limits indicated.
 - 5- Pour concrete base for struts on rock and set anchor bolts as indicated.
 - 6- Install Top Struts as indicated.
 - 7- Excavate, within the payment limits shown, to a point two (2) feet below the bottom wale. Provide temporary bracing as required during the construction operation.
 - 8- Install Bottom Wale as indicated.
 - 9- Install Bottom Struts as required.
 - 10- Complete Abut. Construction.

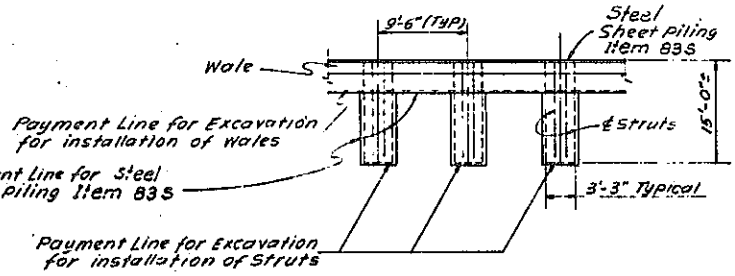
- NOTES:**
- 1- Work and material involved will be paid for under their respective items.
 - 2- Concrete and reinforcing for property protection shall conform to Item 18X and 28 respectively. Both will be paid for under item 835. Structural steel shall conform to item 29B and shall be paid for under item 835.
 - 3- Items for property protection are in the Overpass Estimate.



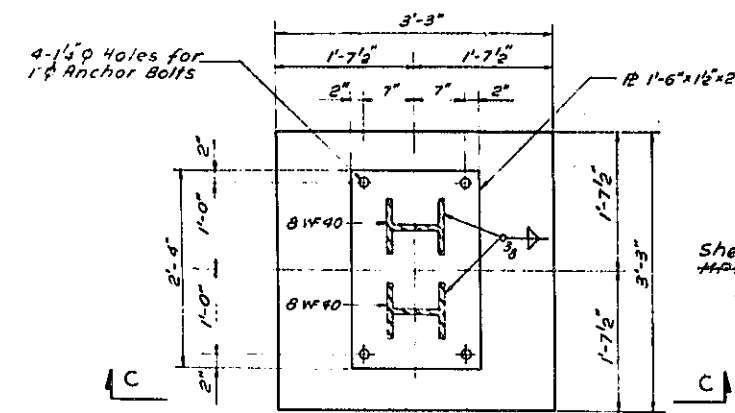
SECTION C-C
Scale: 1" = 1'-0"



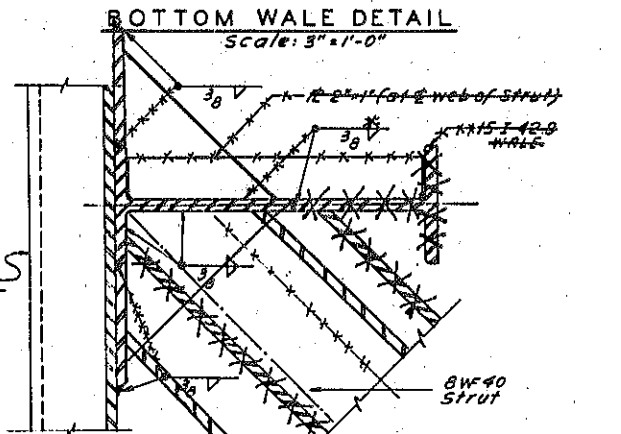
BOTTOM WALE DETAIL
Scale: 3" = 1'-0"



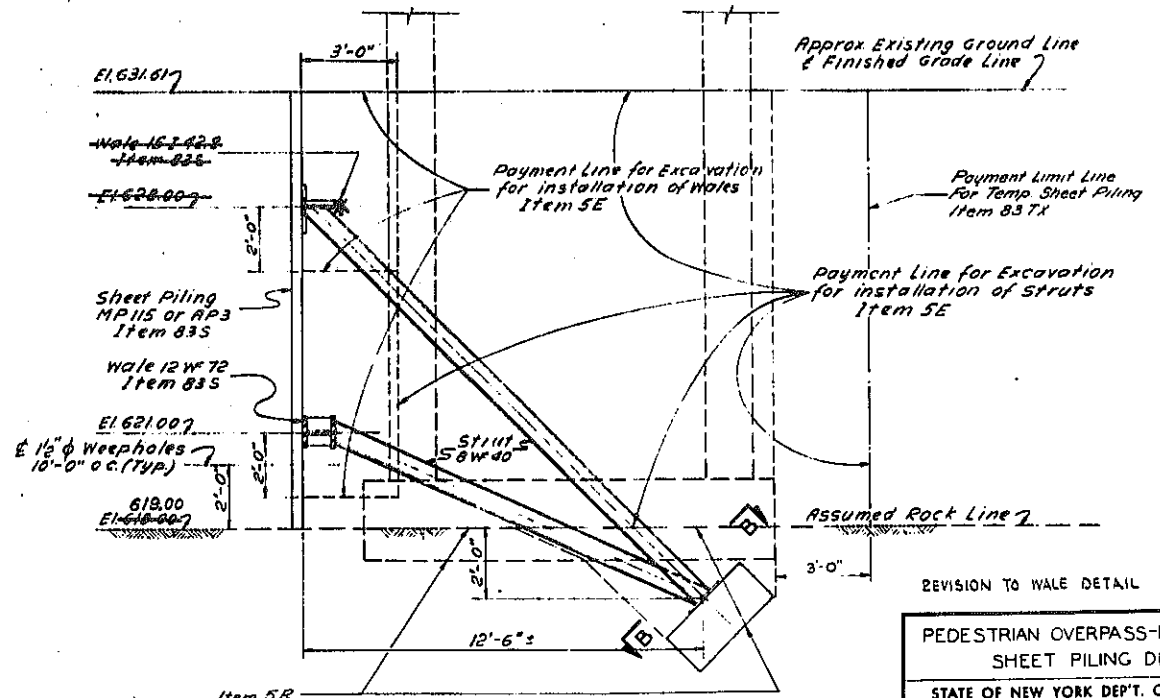
PAYMENT LINE DETAIL
Not to Scale



STRUT BASE DETAIL
SECTION B-B
Scale: 1" = 1'-0"



TOP WALE DETAIL
Scale: 3" = 1'-0"



SECTION A-A
Scale: 3/8" = 1'-0"

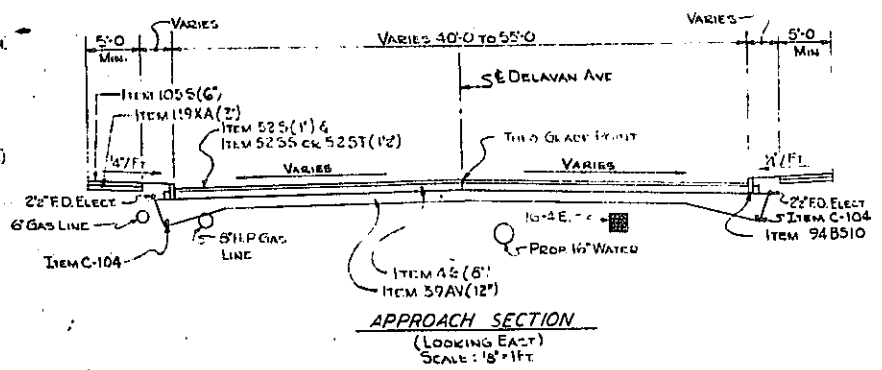
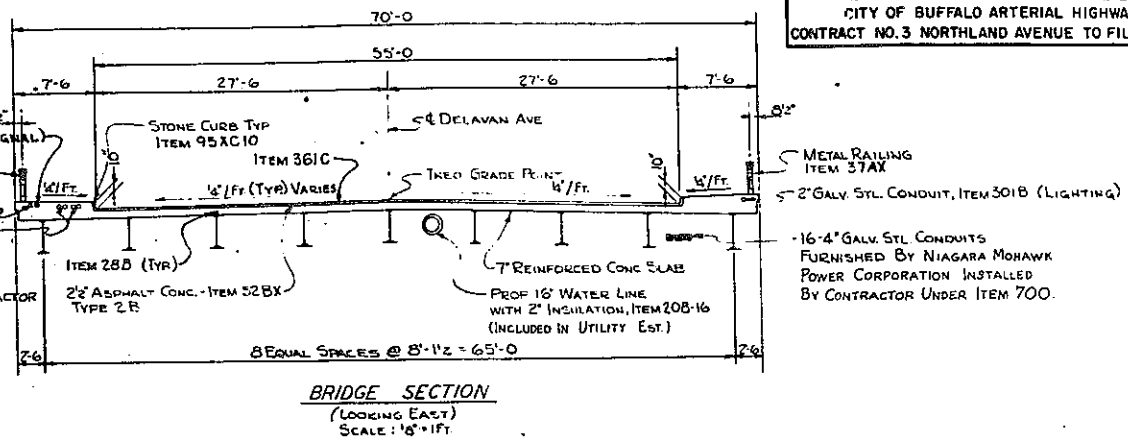
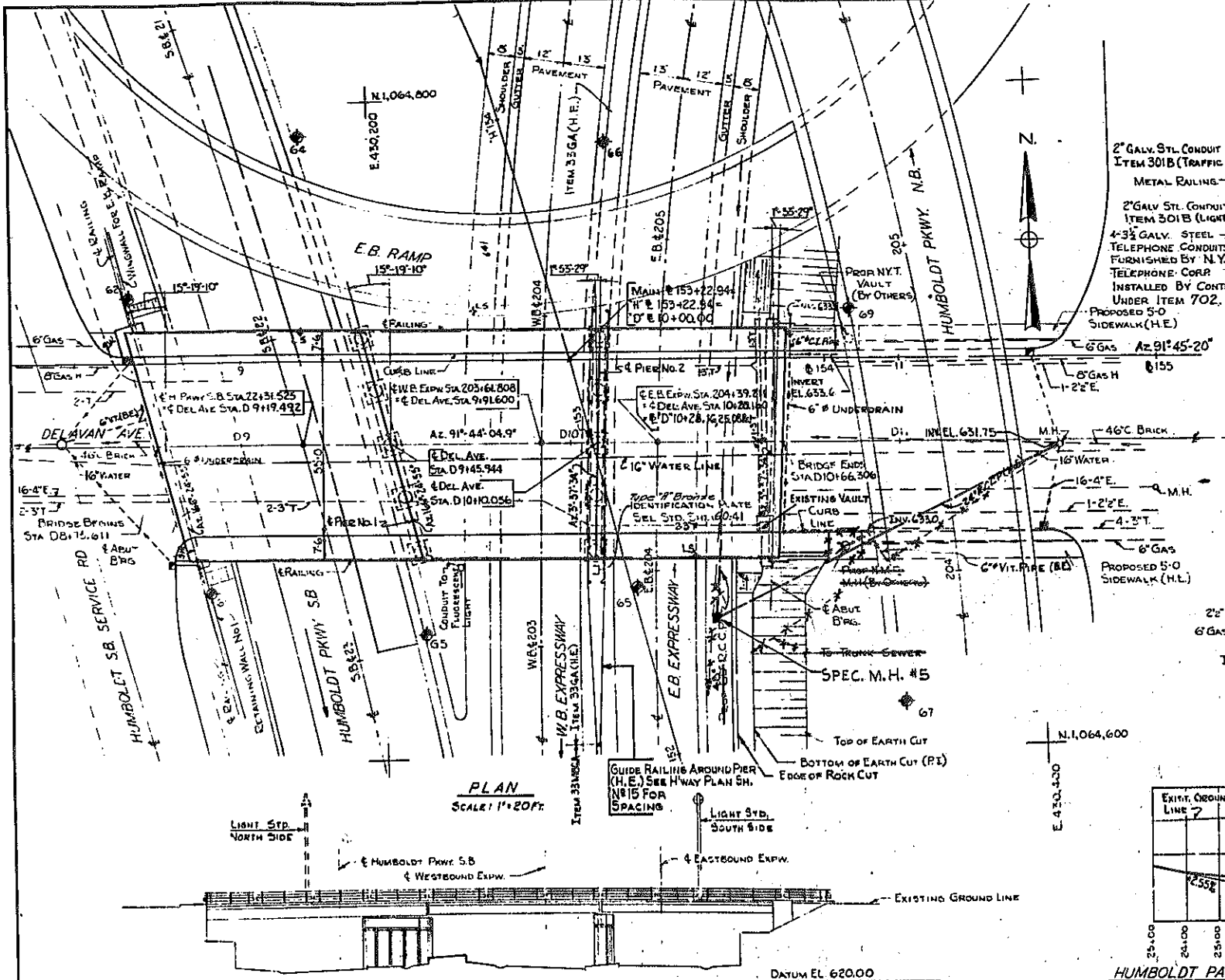
REVISION TO WALE DETAIL

PEDESTRIAN OVERPASS-E.B. STA. 19+09.37	
SHEET PILING DETAILS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LUW, CATHER & BRILL	DRAWN
ENGINEERS-ARCHITECTS	CHECKED
NEW YORK	TRACED
	W.K.
	W.T.
	W.K.

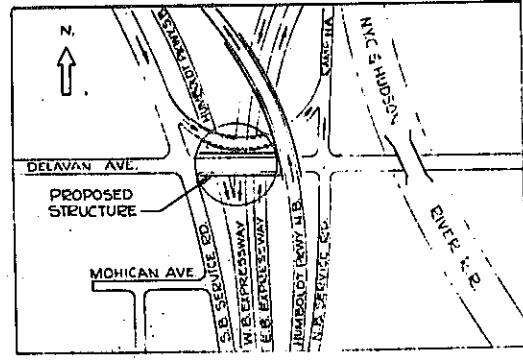
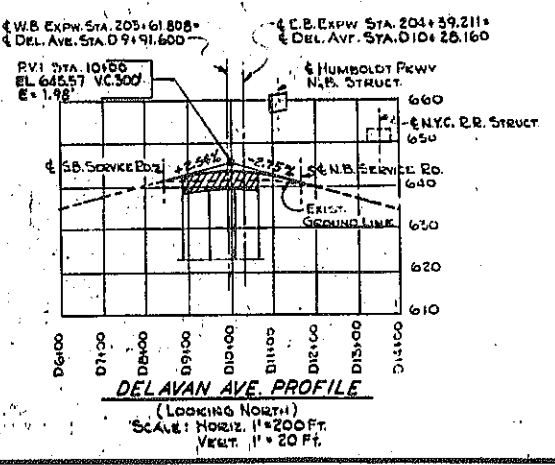
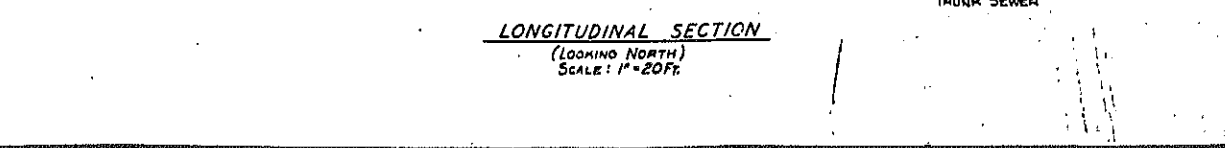
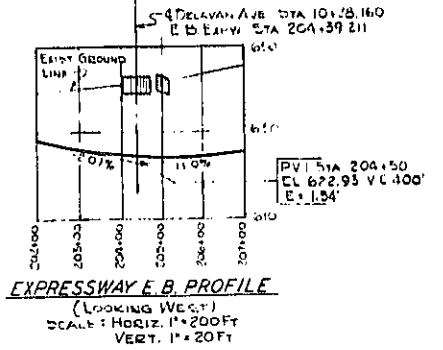
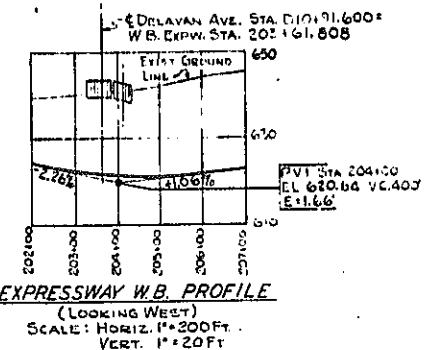
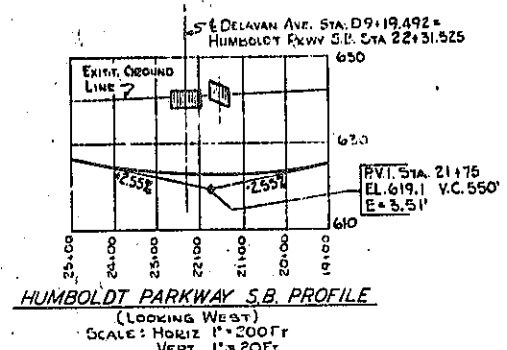
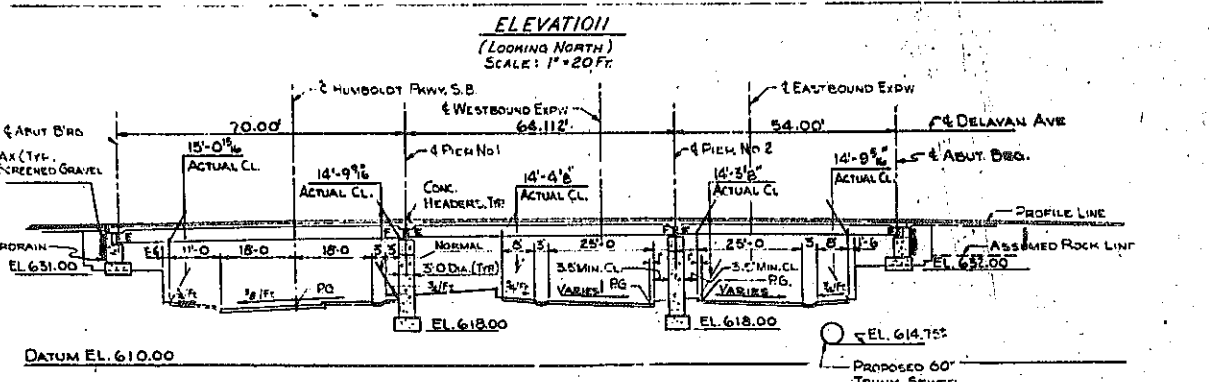


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-05-377 (25)	83RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



INDICATE BORING LOCATION
F = FIELD BEARINGS
E = EXPANSION BEARING
PG = PROFILE GRADE
UTILITIES SHOWN ON BRIDGE APPROACH ARE EXISTING, EXCEPT AS NOTED



REVISION TO 24' R.C.C.P.

DELAVAN AVE. OVER EXPRESSWAY GENERAL PLAN

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.B.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	84	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS: A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING: W20-516-44 LIVE LOAD

MATERIAL FABRICATION AND MANUFACTURE: NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.

THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS: FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK DOES NOT EXCEED 5.7 TONS PER SQUARE FOOT.

CONCRETE: ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, AND IN SIDEWALK POURS ON ABUTMENT WINGWALLS SHALL BE ITEM 18X CLASS IA CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS AND WINGWALLS (EXCEPT IN SIDEWALK POURS) SHALL BE ITEM 20Y, CLASS I, CONCRETE. WHEN PORTLAND CEMENT TYPE 2 IS REQUIRED TO REDUCE THE PERCENTAGE OF ENTRAINED AIR, THE PAYMENT FOR TYPE 2 CEMENT REQUIRED WILL BE MADE AT THE CONTRACT PRICE FOR TYPE 2A CEMENT.

CEMENT: CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A. ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE: ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STANDARD STRUCTURE SHEET 60-41.

OTHER: REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED. SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE Poured 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS. THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS Poured, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 28 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREMOULDED BITUMINOUS JOINT, COMPRESSED ASBESTOS SHEET PACKING, PREF. EXR JOINT FILLER AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

EPOXY: EPOXY PROTECTIVE COATING FOR CONCRETE SHALL BE APPLIED TO THE FOLLOWING SURFACES:
 ABUTMENTS - ALL EXPOSED PEDESTAL SURFACES, BRIDGE SEATS, EXPOSED VERTICAL SURFACES OF BACKWALL AND CURTAIN WALLS FACING THE SUPERSTRUCTURE.
 PIERS - ALL PEDESTAL SURFACES AND THE TOP SURFACE OF PIER BETWEEN PEDESTALS INCLUDING THE EDGE CHAMFER AT TOP EDGE OF PIER.

ABUTMENT FOOTINGS: THE ELEVATION OF SOUND ROCK FOR ABUTMENT FOOTINGS IS TENTATIVE AND ACTUAL FINDINGS IN THE FIELD MAY REQUIRE. IF SO ORDERED BY THE DEPUTY CHIEF ENGINEER, THAT THE ABUTMENT DETAILS BE MODIFIED, OR REVISED. WHERE NECESSARY TO EXCAVATE BELOW THE ASSUMED ELEVATION OF BOTTOM OF FOOTINGS OF ABUTMENTS TO OBTAIN A BASE ON SOUND ROCK, SUCH EXCAVATION SHALL BE MADE AND LOOSE ROCK REMOVED TO THE SATISFACTION OF THE ENGINEER. THE AREA BETWEEN THE TOP OF SOUND ROCK AND THE ASSUMED BOTTOM OF ABUTMENT FOOTINGS SHALL BE FILLED WITH ITEM 20Y, CLASS I CONCRETE. ROCK REMOVED BEYOND THE LINES AND LEVELS ORDERED BY THE ENGINEER WILL NOT BE PAID FOR AND THE AREA SHALL BE BACKFILLED WITH CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE. EXCAVATION AS ORDERED OR SHOWN ON THE PLANS SHALL BE ITEMS 5E AND 5R TRENCH, CULVERT AND BRIDGE EXCAVATION.

SHEAR CONNECTORS: THE MATERIALS TO BE USED FOR STUD SHEAR CONNECTORS SHALL BE GRADE DESIGNATION C1015 OR C1020.

WELDED JOINTS SHALL BE RADIOGRAPHED IN CONFORMANCE WITH THE REQUIREMENTS OF THE NEW YORK STATE SPECIFICATION FOR RADIOGRAPHIC INSPECTION. THE JOINTS TO BE RADIOGRAPHED ARE DESCRIBED THEREIN.

WHERE MATERIAL CONFORMING TO SPECIFICATION ITEM M-41X APPEARS ON THE DRAWINGS, STANDARD DRAWINGS, OR THE SPECIFICATION MATERIAL CONFORMING TO SPECIFICATIONS, ITEM M-41XA SHALL BE USED.

CORRUGATED METAL FORMS: APPROVED CORRUGATED METAL FORMS MAY BE USED FOR THE STRUCTURAL SLAB PROVIDED THE COVER FOR THE REINFORCING STEEL IS MAINTAINED ABOVE THE CREST OF THE CORRUGATIONS.

DETAILS FOR CORRUGATED METAL FORMS SHALL BE SUBMITTED TO THE DEPUTY CHIEF ENGINEER (BRIDGES) FOR APPROVAL, PRIOR TO THEIR USE. THE CONTRACTOR'S ATTENTION IS DIRECTED TO ADDITIONAL NOTES APPEARING IN THE PROPOSAL RELATIVE TO CORRUGATED METAL FORMS.
 No payment will be made for the concrete and cement in structural slab below the crest of corrugated forms.

TABLE OF BEARINGS						1" ANCHOR BOLTS		
LOCATION	STRINGER	No.	A	B	C	D	No.	LENGTH
EXPANSION BEARINGS								
WEST. ABUT.	1A-1J	9	1 3/8		4 5/8		18	1'-2"
PIER No. 1	2A-2J	9	1 3/8		4 5/8		18	1'-2"
PIER No. 2	3A-3J	9	1 3/8		4 5/8		18	1'-2"
FIXED BEARINGS								
PIER No. 1	1A	1		3/8		4 5/8	2	1'-3"
	1B	1		3/8		4 5/8	2	1'-3"
	1C	1		3		4 1/2	2	1'-3"
	1D	1		2 7/8		4 3/8	2	1'-3"
	1E	1		2 1/16		4 3/16	2	1'-3"
	1F	1		2 1/16		4 3/16	2	1'-3"
	1G	1		2 7/8		4 3/8	2	1'-3"
	1H	1		2 7/8		4 3/8	2	1'-3"
	1J	1		2 7/8		4 3/8	2	1'-3"
PIER No. 2	2A	1		2 1/16		4 3/16	2	1'-3"
PIER No. 2	2B-2H	7		1 5/8		3 3/8	14	1'-2"
PIER No. 2	2J	1		3 3/16		4 1/16	2	1'-3"
EAST. ABUT.	3A-3J	9		1 5/8		3 3/8	18	1'-2"

AMENDMENTS TO PAINT NOTES SPECS.

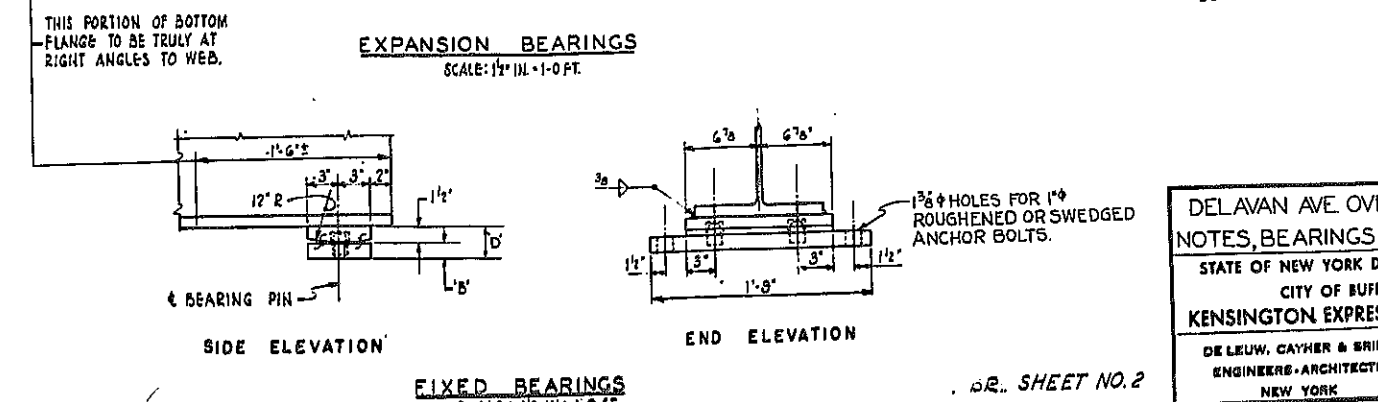
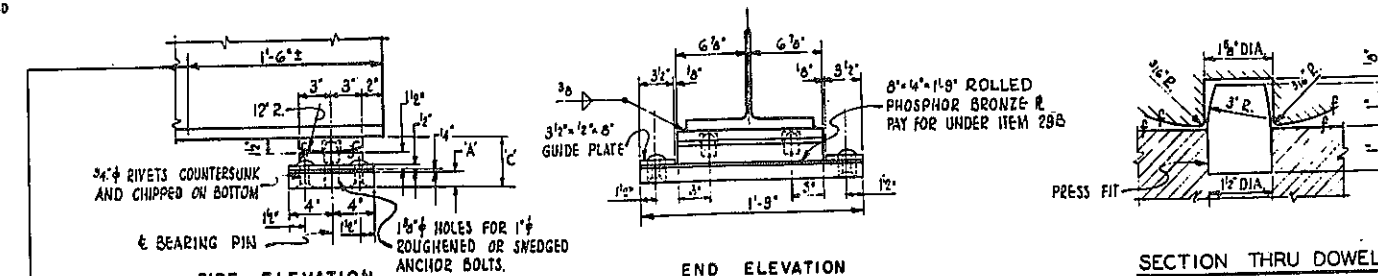
THE DRIER CATALYST SHALL BE ZIRCONIUM, MANGANESE AND COBALT METALS OF THE FOLLOWING TABULATED PERCENTAGES BASED ON THE NON-VOLATILE VEHICLE BY WEIGHT INTRODUCED THEREIN AS SOLUBLE ORGANIC METAL SALTS

PAINT ITEMS	ZIRCONIUM %	MANGANESE %	COBALT %
M-19A MAROON PRIMER	0.14	0.04	
M-20A ORANGE PRIMER	0.14	0.04	
M-21A BLACK PAINT	0.15	0.01	0.02
M-22A STAIN RESISTANT WHITE PAINT	0.10	0.02	
M-23A GRAY PAINT	0.10	0.02	
M-24A GRAY-GREEN PAINT	0.15	0.04	

NORMAL SPREADING RATES FOR PAINTS M-19A THROUGH M-24A SHALL BE IN THE RANGE OF 400 TO 500 SQUARE FEET PER GALLON SO AS TO PROVIDE WET FILM THICKNESS OF 2.5 TO 3.5 MILS. PAINT SHALL BE APPLIED ONLY WHEN THE AIR TEMPERATURE IS AT OR ABOVE 40° F. AND RELATIVE HUMIDITY IS LESS THAN 85%. MAROON PRIMER, ITEM M-19A, WILL NOT BE APPROVED FOR USE ON THIS CONTRACT.

ESTIMATE OF QUANTITIES				
ITEM No.	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED
2EFB	SELECTED FILL	C.Y.	229	250
5	TRENCH, CULVERT & BRIDGE EXC.	C.Y.	144	160
11 H	PERF. CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	200	205
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	1,344	1,390
18X	CLASS IA CONCRETE FOR STRUCTURES	C.Y.	529	540
20Y	CLASS I CONCRETE	C.Y.	321	330
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	44	48
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	115,880	117,200
28B	SHEAR CONNECTORS	L.F.	1,698	1,750
29B	STRUCTURAL STEEL	Lb.	396,344	408,200
30S	MISCELLANEOUS METALS	Lb.	304	310
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	415	430
52BX	ASPHALT CONCRETE, TYPE 2B (2z)	Ton	165	170
61	BITUMINOUS MATERIAL	GAL.	103	110
95XC10	10" STONE CURB (BRIDGE)	L.F.	432	440
301B	2" GALV. STEEL CONDUIT	L.F.	650	670
301C	2 1/2" GALV. STEEL CONDUIT	L.F.	10	10
303B	INSTALL LIGHT STANDARD, TYPE B	Ea.	2	2
323M	FOUR CONDUCTOR CABLES	L.F.	222	230
352XA	RUBBER JOINT MATERIAL	GAL.	8	9
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	143	150
9VB	SEWER PIPE (VITRIFIED) 6"	L.F.	40	50
13B	CAST IRON PIPE, 6" DIA.	L.F.	5	5
20B-1G	FURNISH & INSTALL 16" INSULATED STL. WATER PIPE	L.F.	192	195
83TX	TEMPORARY SHEET PILING	S.F.	1537	1,610
700	INSTALL N.M.P. CORR. COND. ON DELAVAN AVE. STRUCT.	L.S.	NEC.	NEC.
702	INSTALL N.Y. TEL. CO. COND. ON DELAVAN AVE. STRUCT.	L.S.	NEC.	NEC.
363 C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	24	25

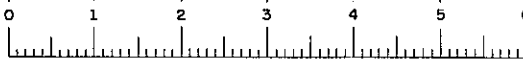
* INCLUDED IN PUBLICLY OWNED WATER WORKS ESTIMATE.
 ** PAID FOR BY UTILITIES INVOLVED
 *** Includes portion for water utility (Total)



DELAVAN AVE OVER EXPRESSWAY
 NOTES, BEARINGS AND ESTIMATE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

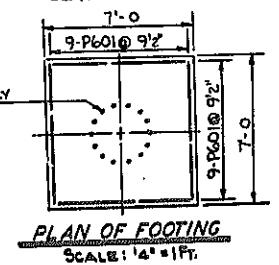
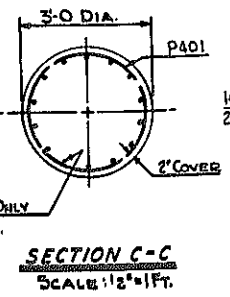
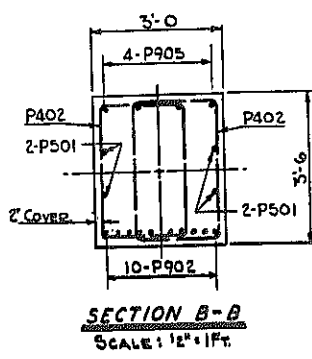
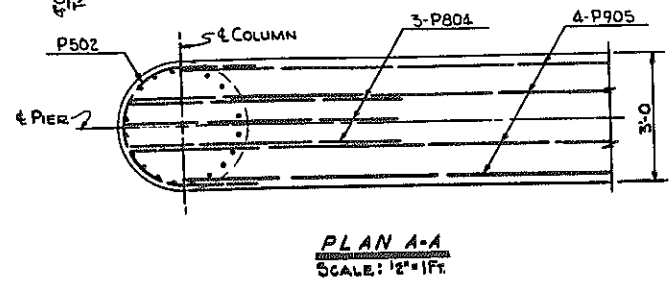
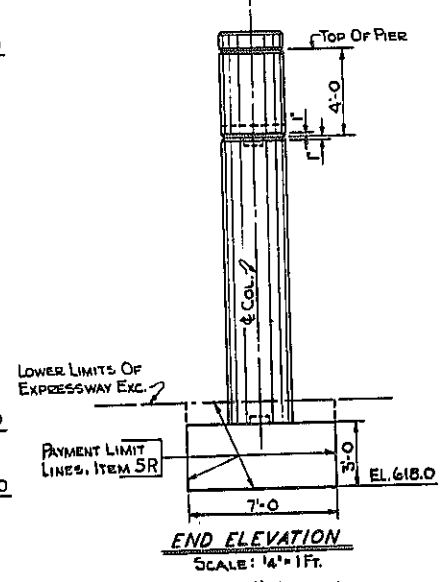
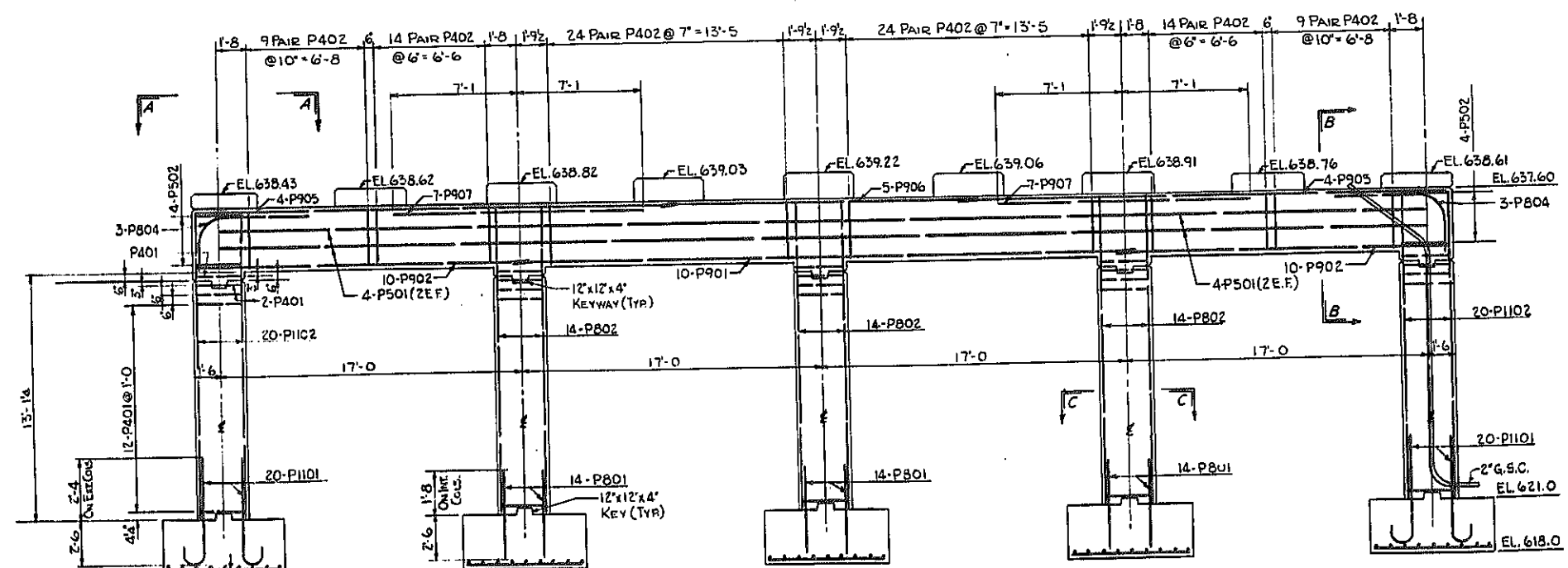
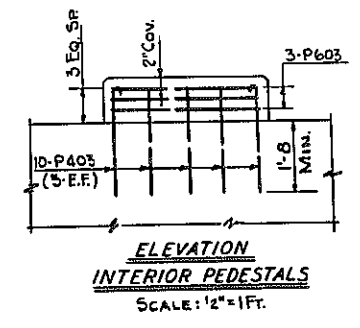
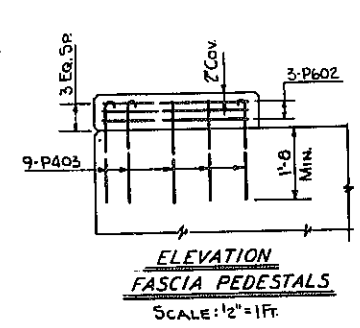
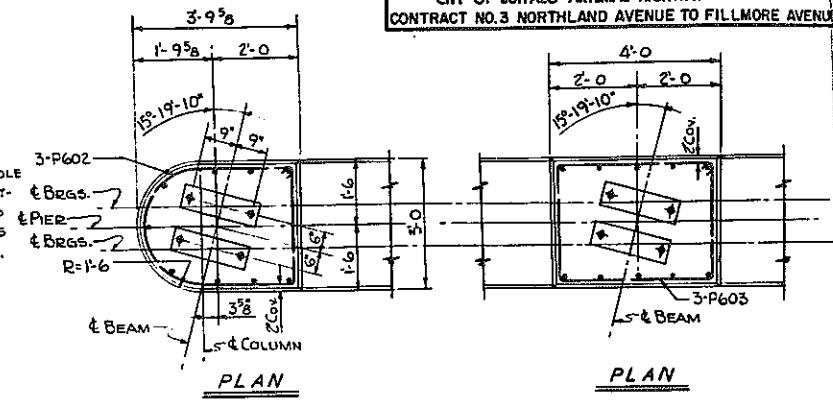
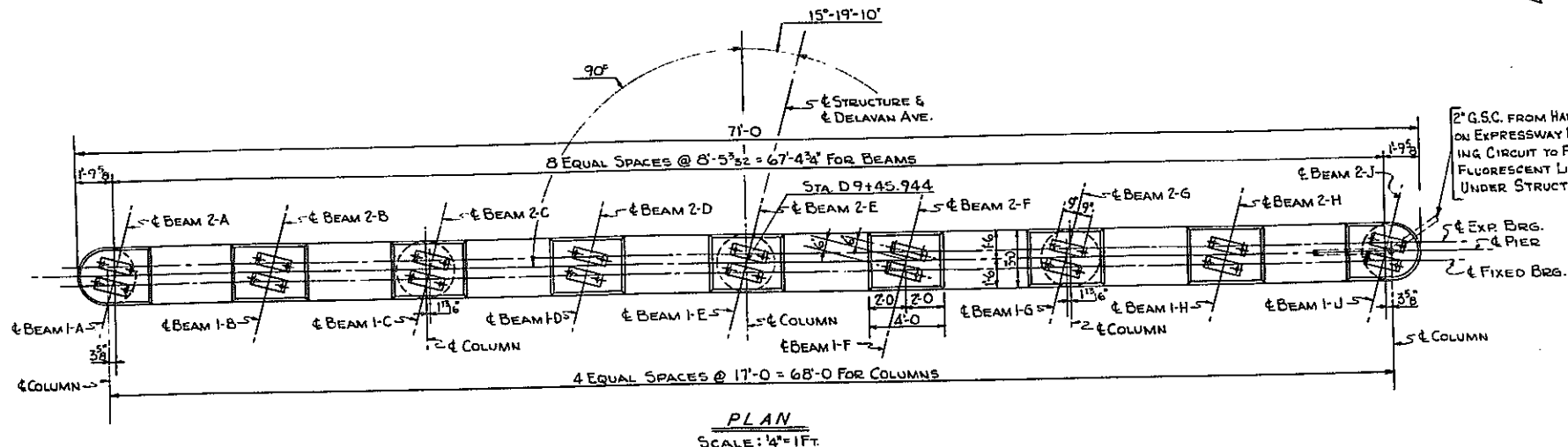
DE LEUW, CAYHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	85	173

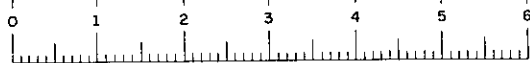
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5.7 TONS PER SQ. FT.
 - CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 - CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS AND SET 10" INTO MASONRY
 - SEE SHEET NO. 2 FOR ANCHOR BOLT LENGTHS.
 - BAR NUMBER SYSTEM:
PIER BAR NUMBERS ARE PREFIXED WITH LETTER P. SIZE OF BAR IS INDICATED BY NUMBER IN THE HUNDREDS COLUMN.

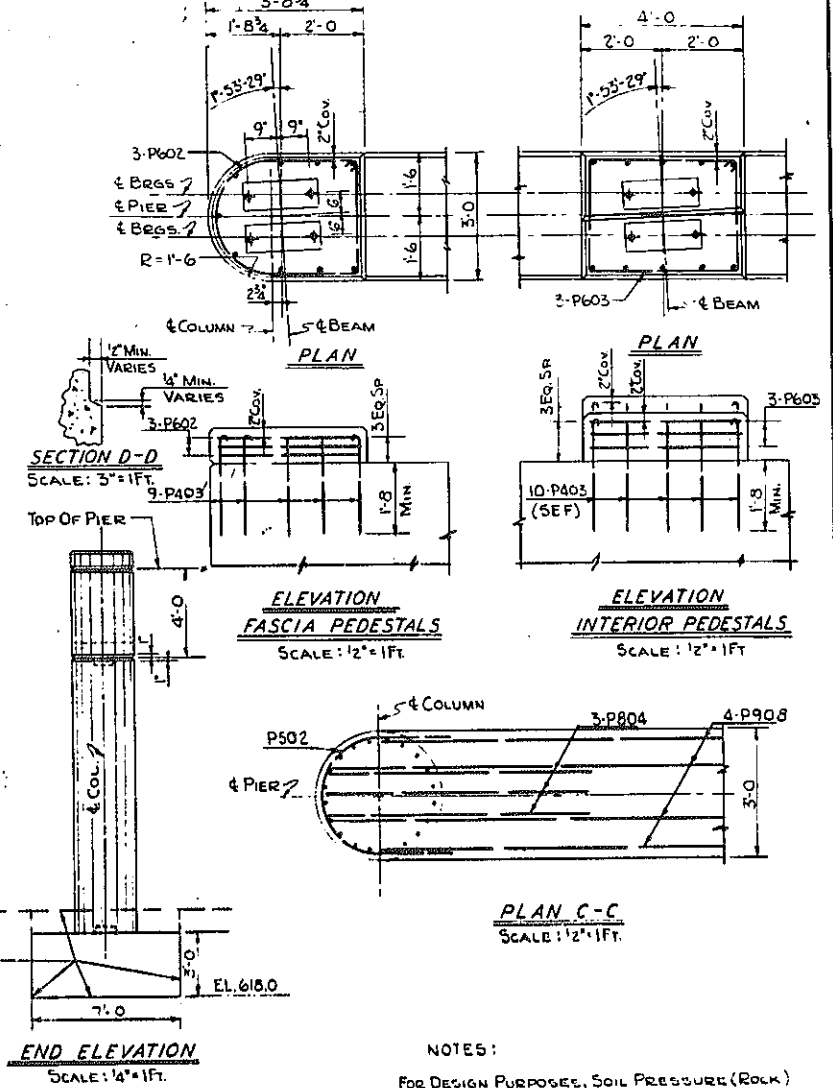
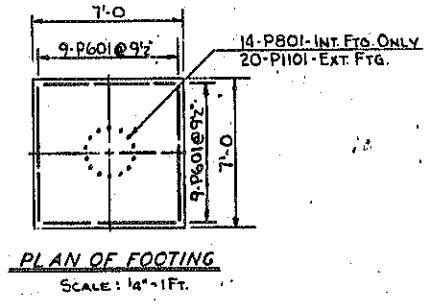
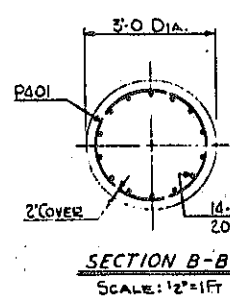
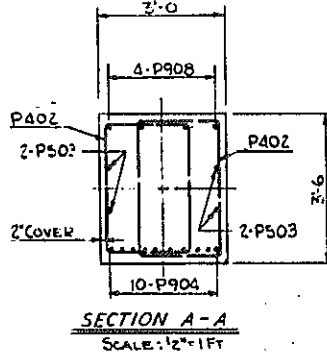
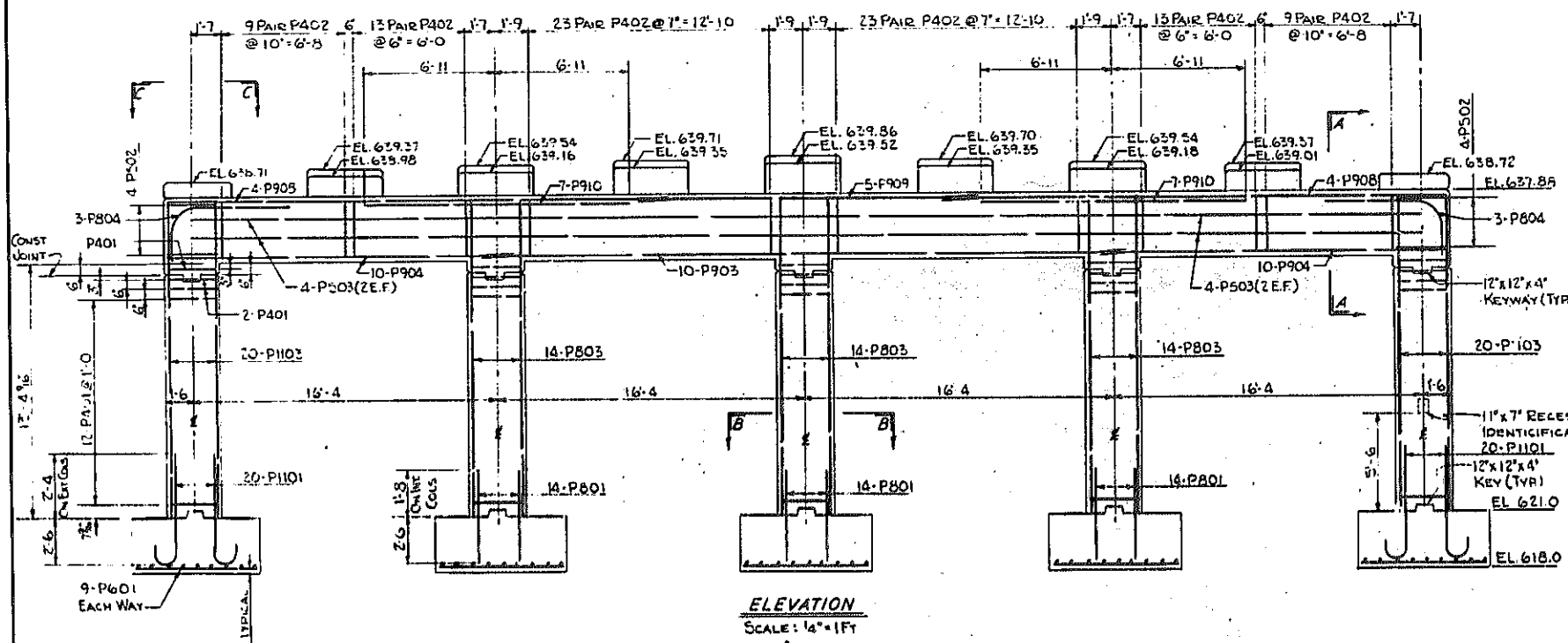
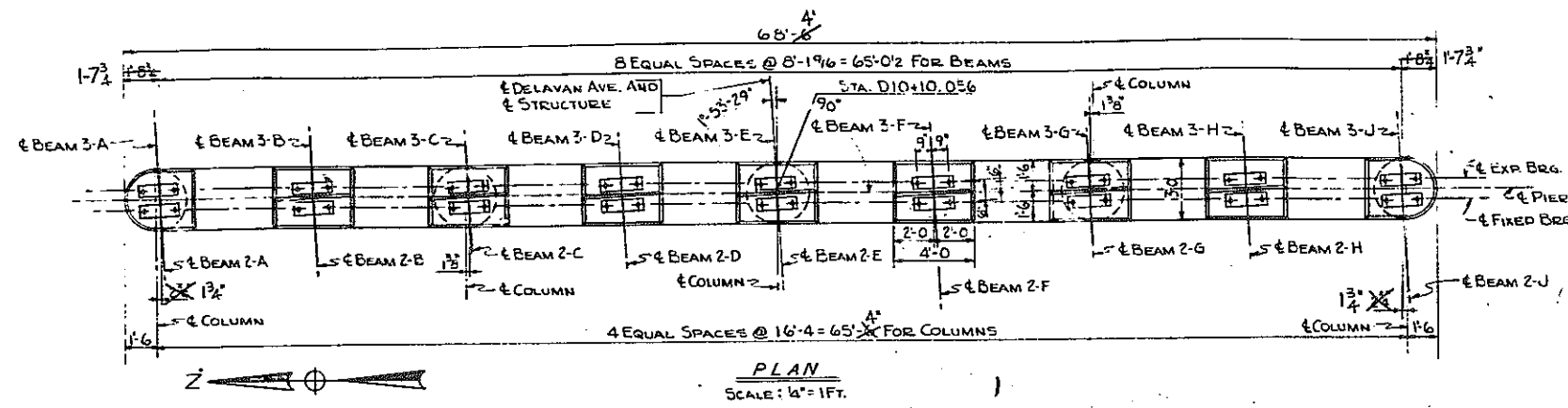
DELAVAN AVE. OVER EXPRESSWAY PIER NO. 1		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.

BR. SHEET NO. 3



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	26R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5.7 TONS PER SQ. FT. AT PIERS.

CONCRETE IN PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS AND SET 10" INTO MASONRY.

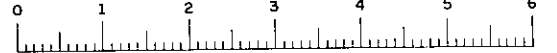
SEE SHEET NO. 2 FOR ANCHOR BOLT LENGTHS.

BAR NUMBER SYSTEM:
PIER BAR NUMBERS ARE PREFIXED BY LETTER P
SIZE OF BAR IS INDICATED BY NUMBER IN HUNDREDS COLUMN.

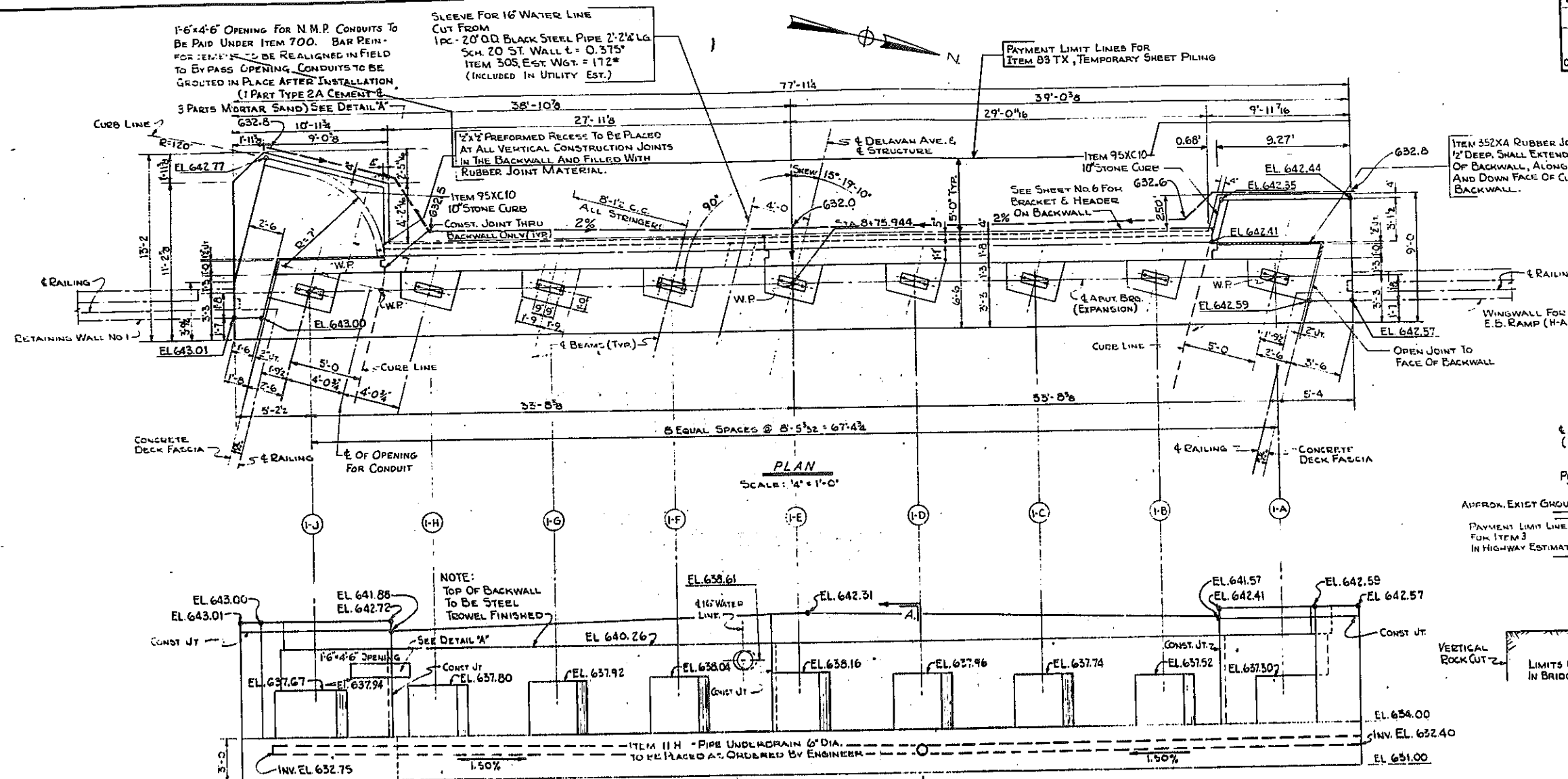
REVISION TO PIER LENGTH DIMENSION

BR SHEET NO. 4

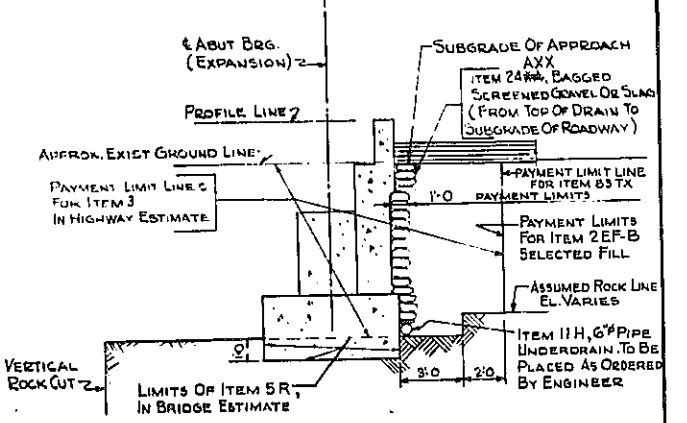
DELAVAN AVE. OVER EXPRESSWAY PIER NO 2			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.	



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	8TRI	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



ITEM 352XA RUBBER JOINT MATERIAL 1/2" DEEP, SHALL EXTEND FROM THE FACE OF BACKWALL, ALONG TOP OF SIDEWALK AND DOWN FACE OF CURB TO TOP OF BACKWALL.



SECTION A-A
SCALE: 1/4" = 1'-0"

NOTES:

CONCRETE IN ABUTMENT SHALL BE ITEM 10Y, CLASS I CONCRETE.

CONCRETE IN HEADER & SUBSTRUCTURE SIDEWALKS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 57 TONS PER SQ FT.

FOR SECTIONS AND REINFORCING, SEE SHT NO 6

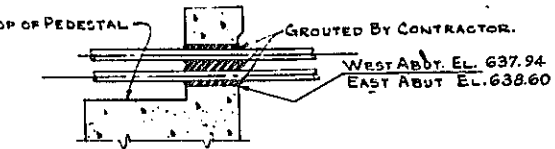
FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO 2.

BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTING TO THE BOTTOM OF PAVEMENT.

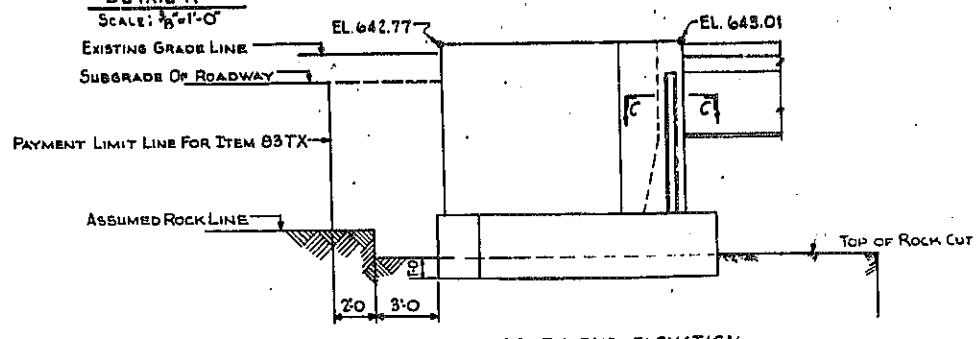
REVISION TO UNDERDRAIN

DELAVAN AVE. OVER EXPRESSWAY WEST ABUTMENT		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL HIGHWAY		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DR. LEUW, CATHAR & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.B.

BR. SHEET NO. 5

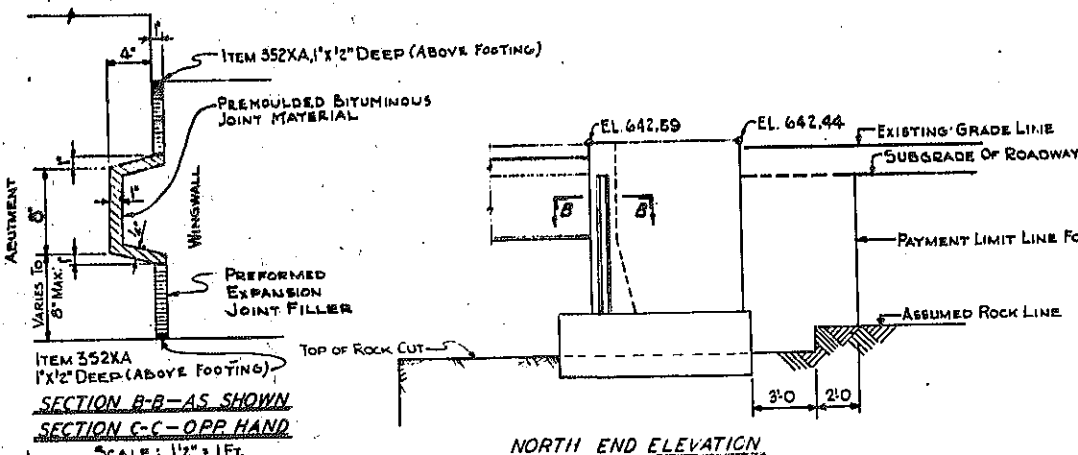


DETAIL A
SCALE: 1/2" = 1'-0"



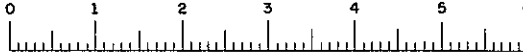
SOUTH END ELEVATION
SCALE: 1/4" = 1'-0"

ELEVATION
(LOOKING WEST)
SCALE: 1/4" = 1'-0"



NORTH END ELEVATION
SCALE: 1/4" = 1'-0"

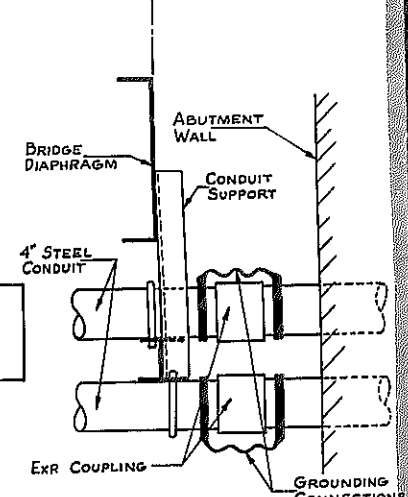
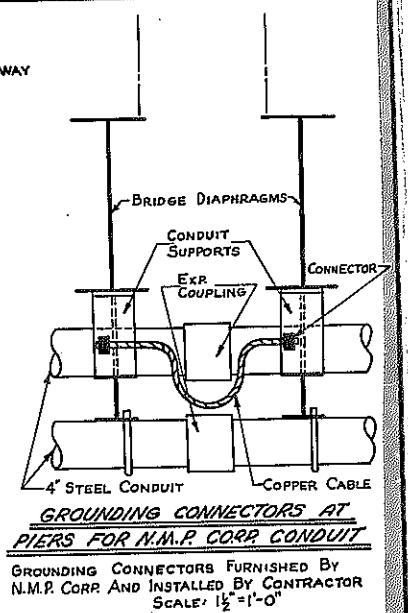
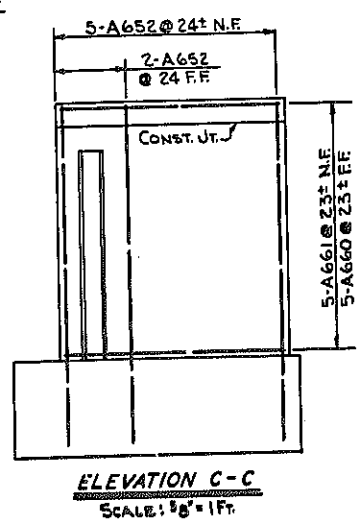
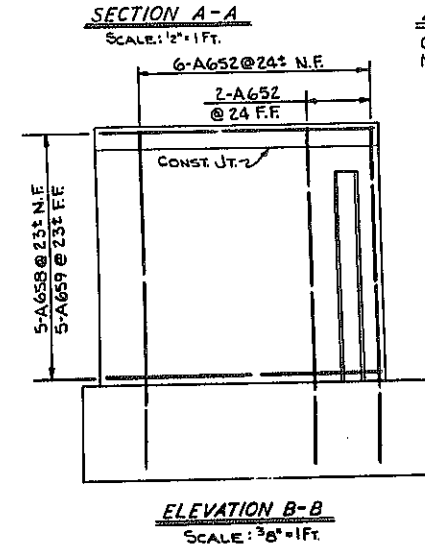
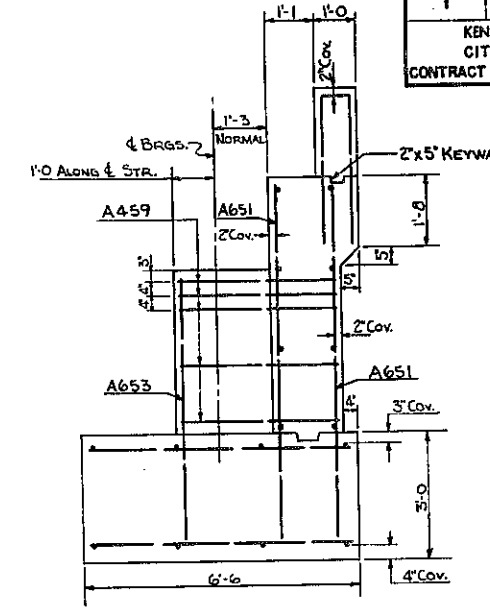
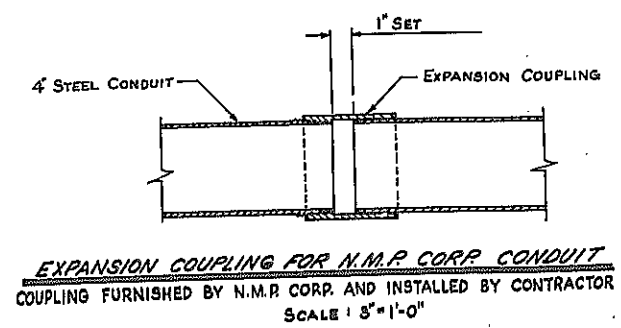
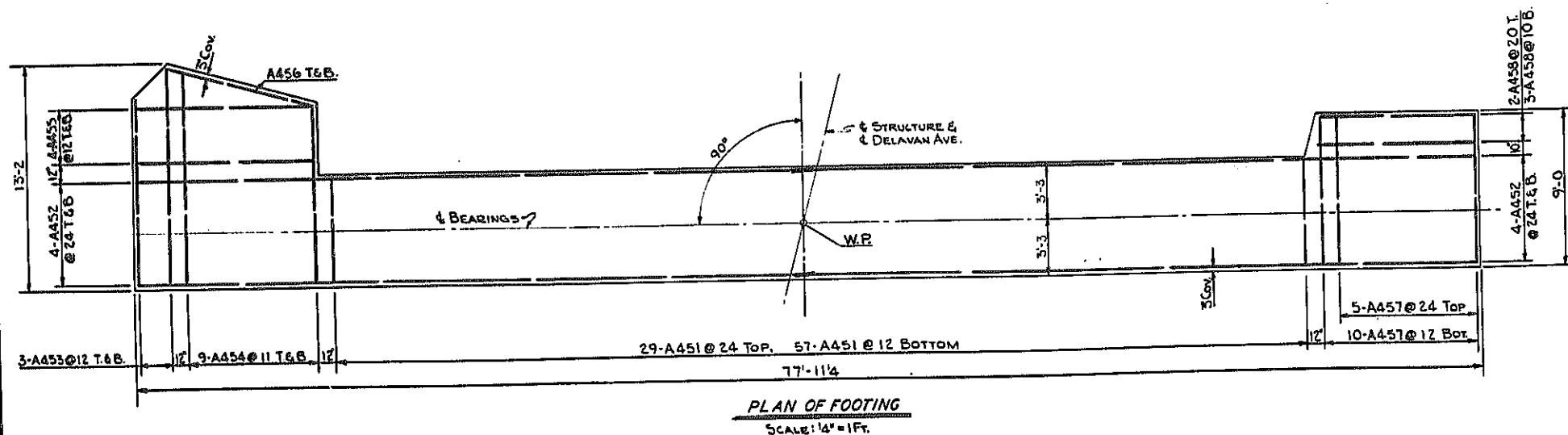
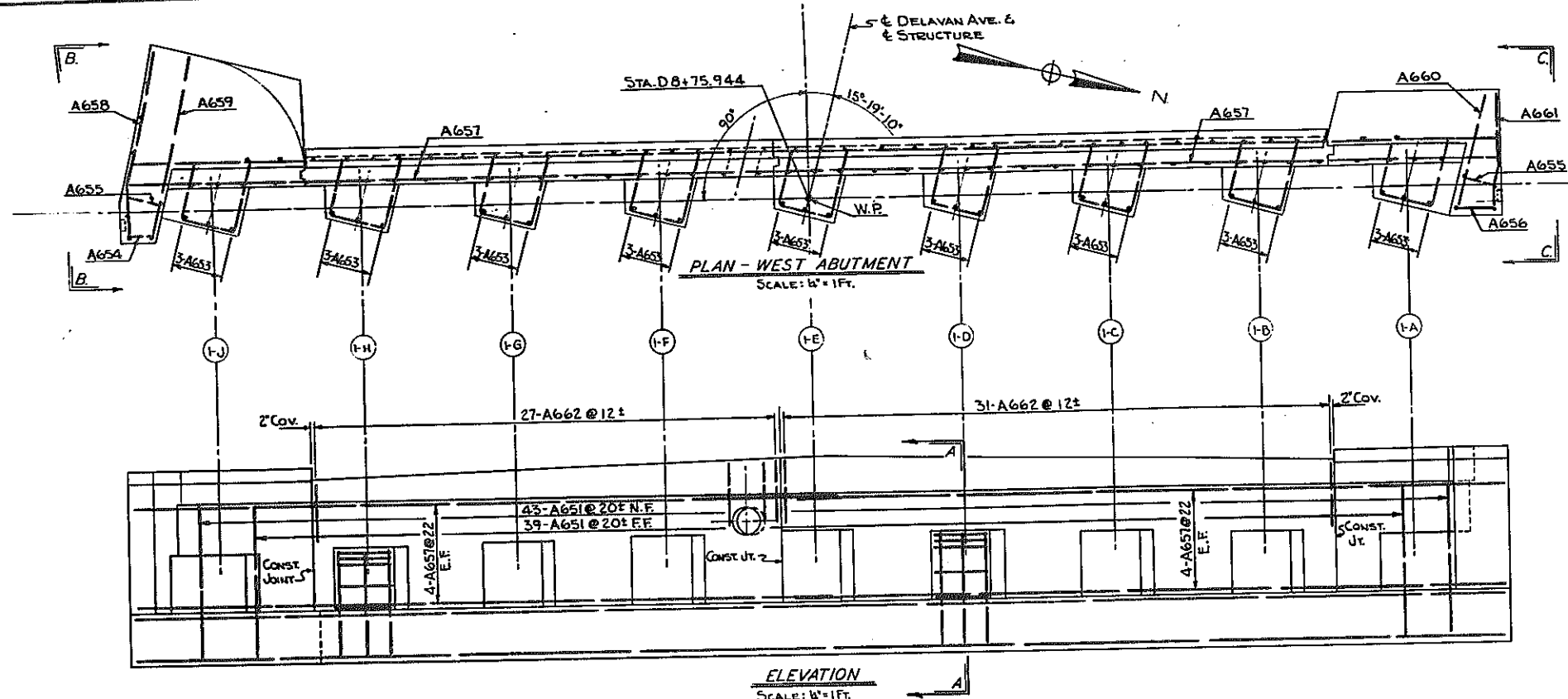
NOTE: USE P.B.J.M. IN FOOTING KEY TO EXTEND FROM TOP OF FOOTING TO A POINT 1'-0" BELOW TOP OF WALL. BAR REINFORCEMENT NOT SHOWN.



FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	88	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



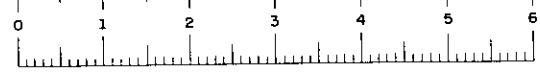
BAR NUMBER SYSTEM
ABUTMENT BAR NUMBERS ARE PREFIXED BY LETTER A. SIZE OF BAR IS INDICATED BY NUMBER IN THE HUNDREDS COLUMN.

DEHAVAN AVE. OVER EXPRESSWAY
WEST ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

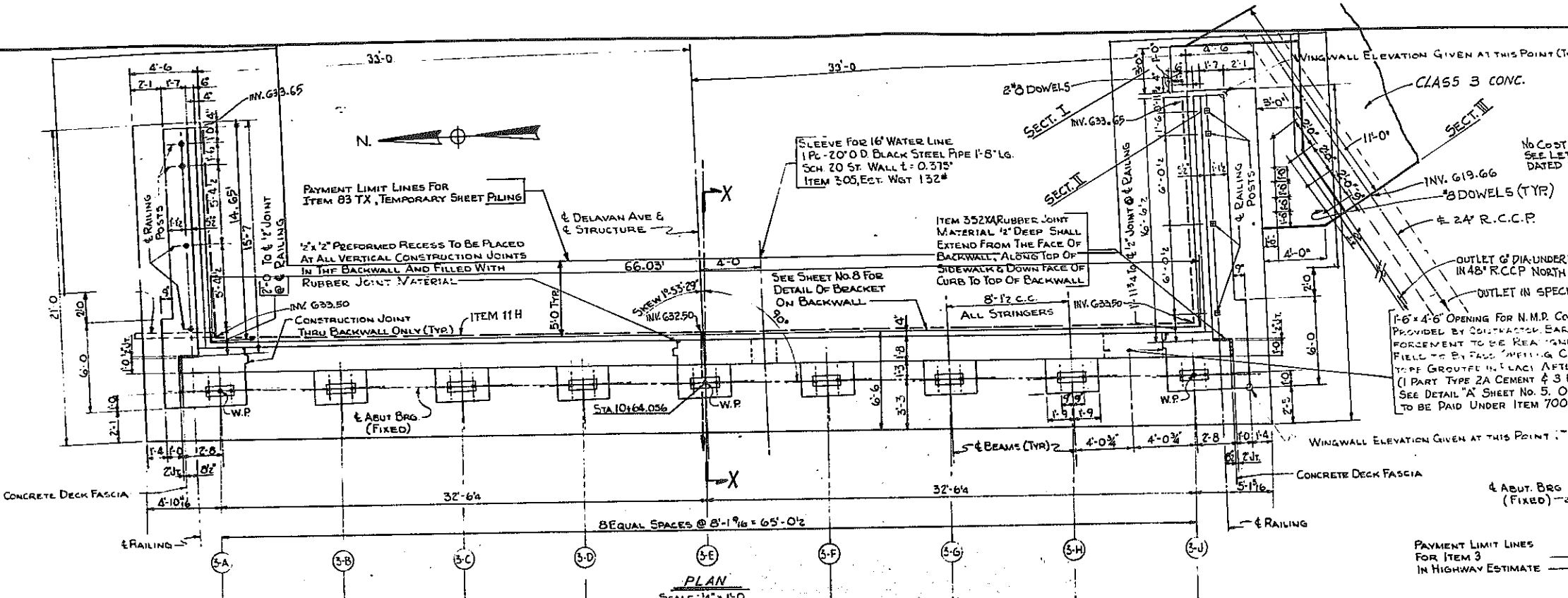
DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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BR. SHEET NO. 6

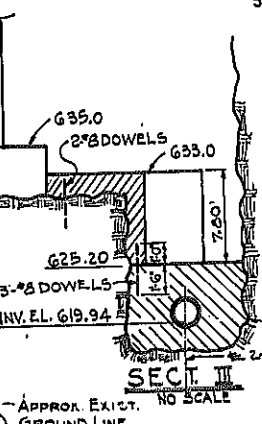
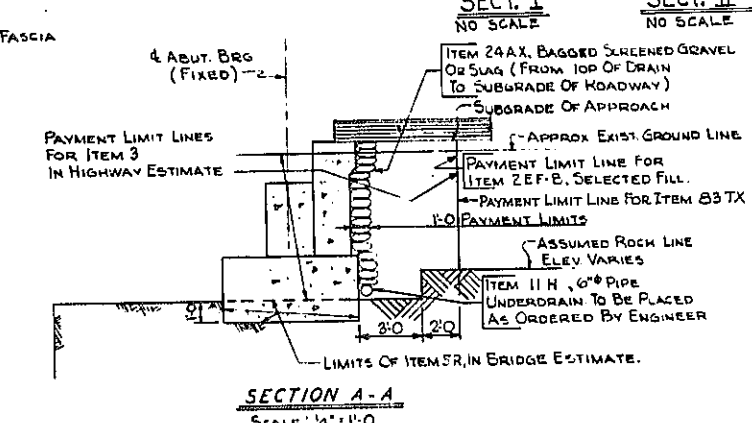
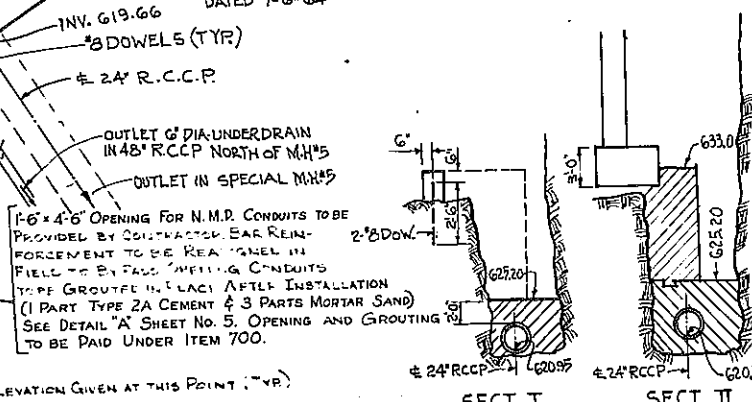
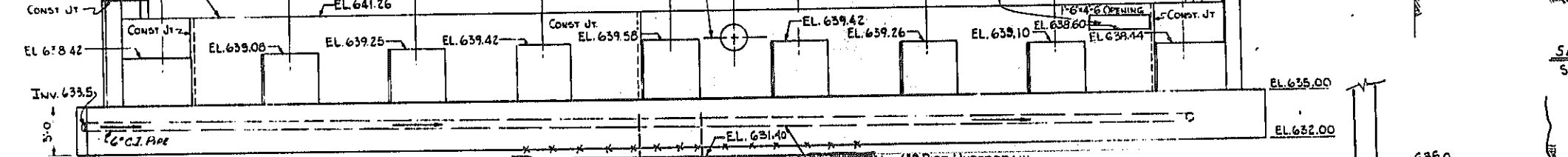


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	N. Y.	U-UG-377(25)	89RI	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

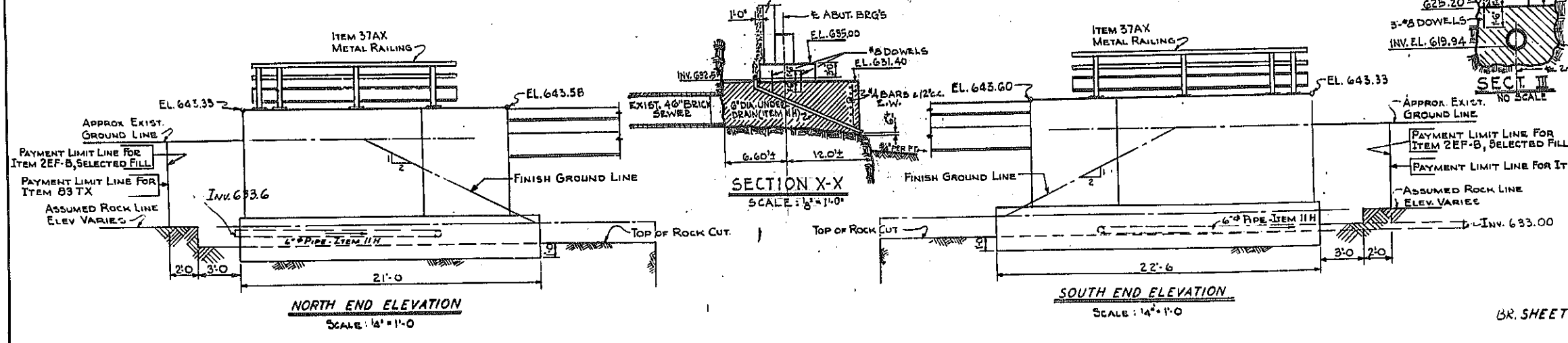
No COST TO STATE
SEE LETTER TO CONTRACTOR
DATED 7-6-64



NOTE
TOP OF BACKWALL
TO BE STEEL TROWEL
FINISHED
EL. 643.58

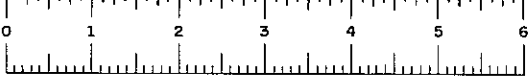


- NOTES:
- CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - CONCRETE IN WINGWALL FASCIAE SHALL BE ITEM 15X, CLASS 1A CONCRETE FOR STRUCTURES
 - FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5.7 TONS PER SQ FT
 - FOR SECTIONS AND REINFORCING, SEE SHEET NO. 8
 - FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 2
 - BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT



DELAVAN AVE OVER EXPRESSWAY EAST ABUTMENT		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.

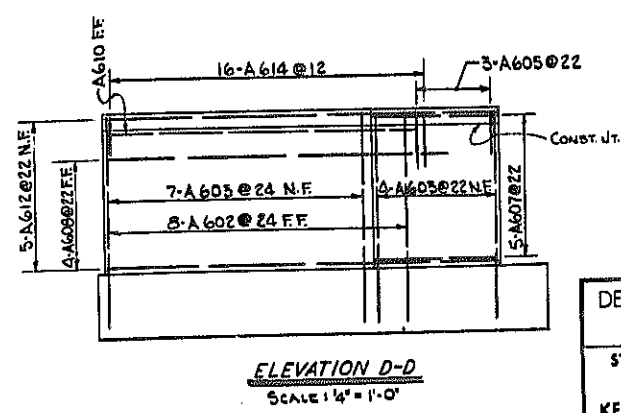
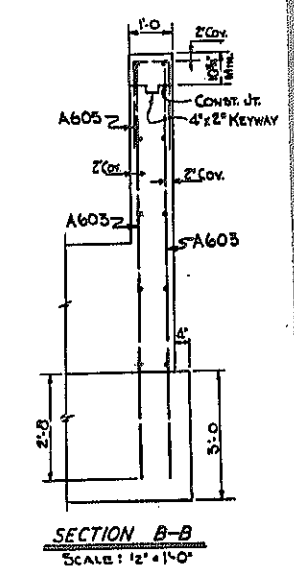
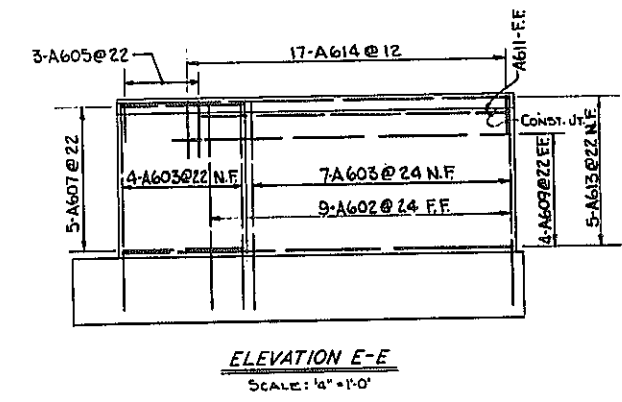
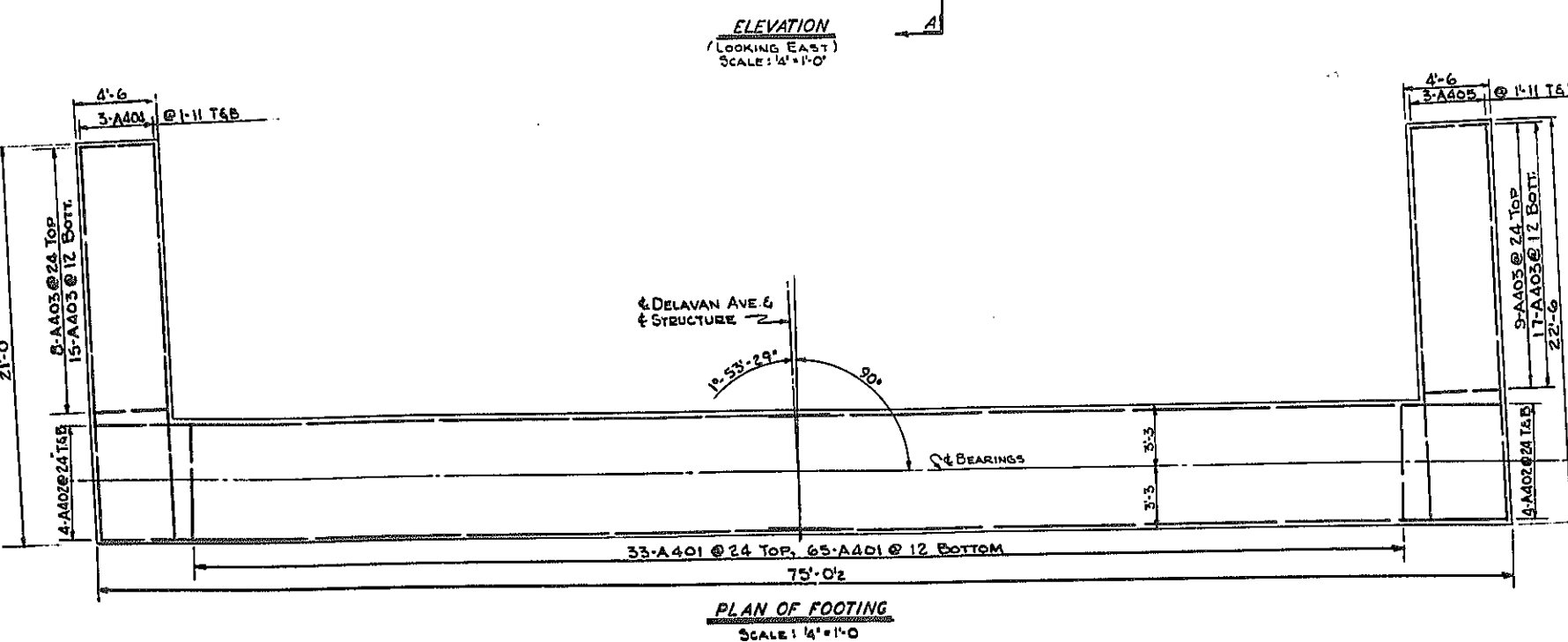
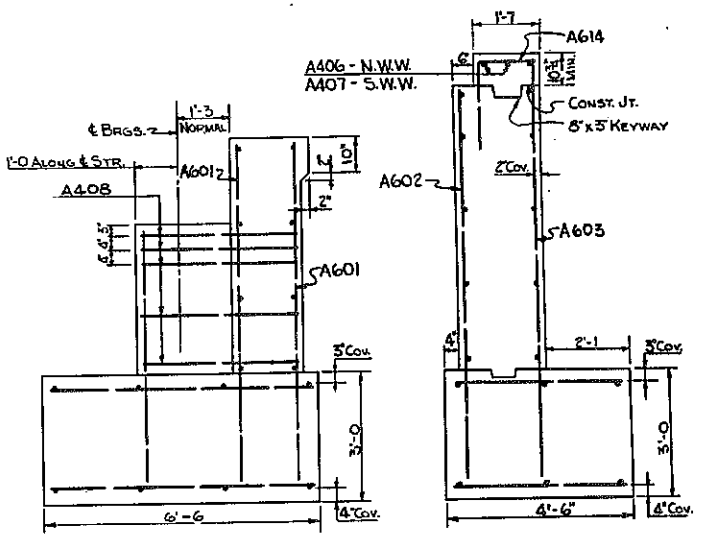
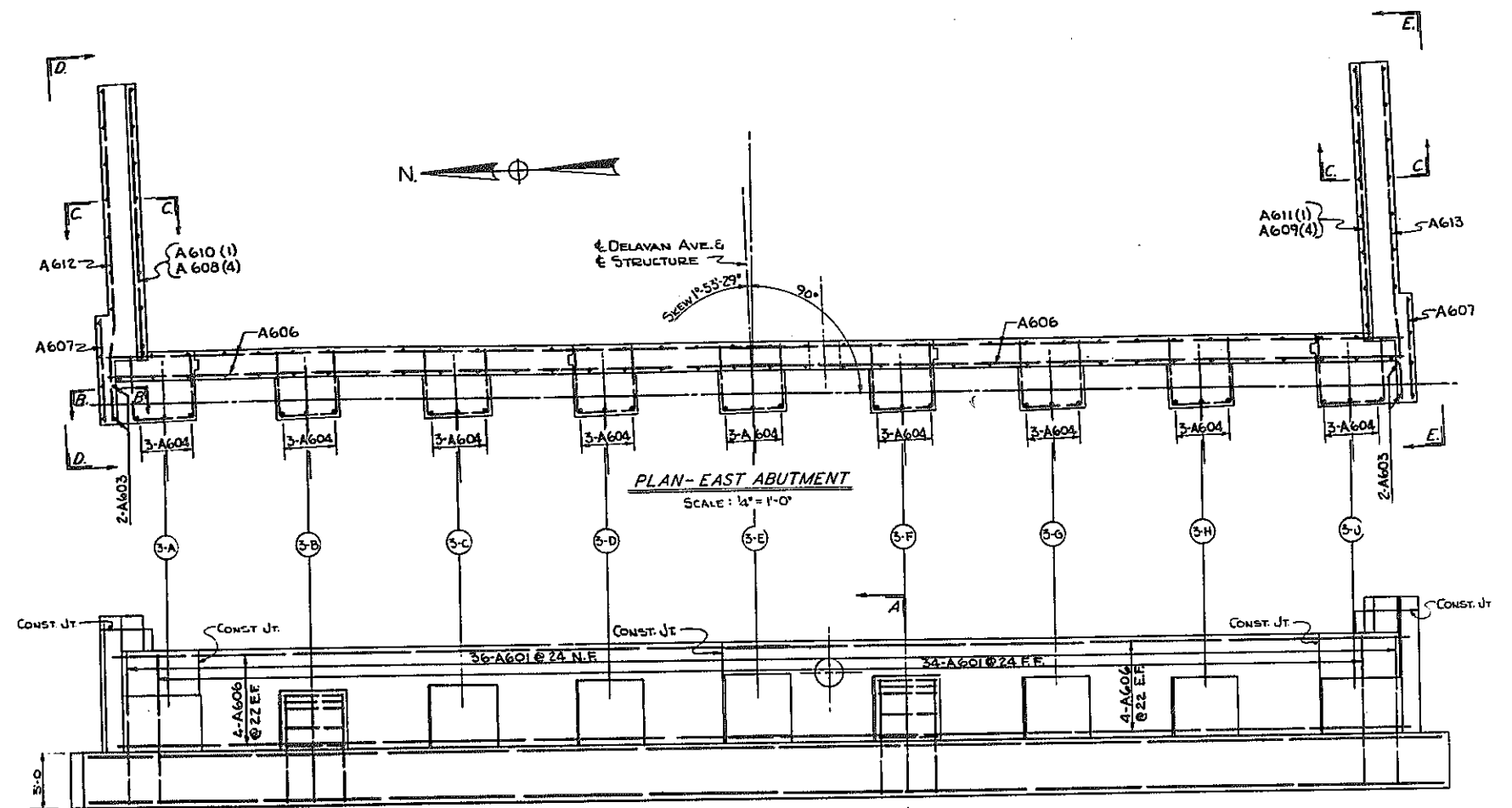
BR. SHEET NO 7



FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	90	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

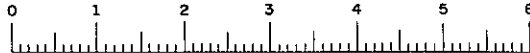


DELAVAN AVE. OVER EXPRESSWAY
EAST ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

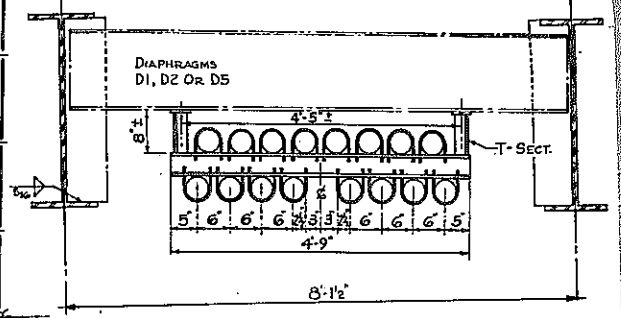
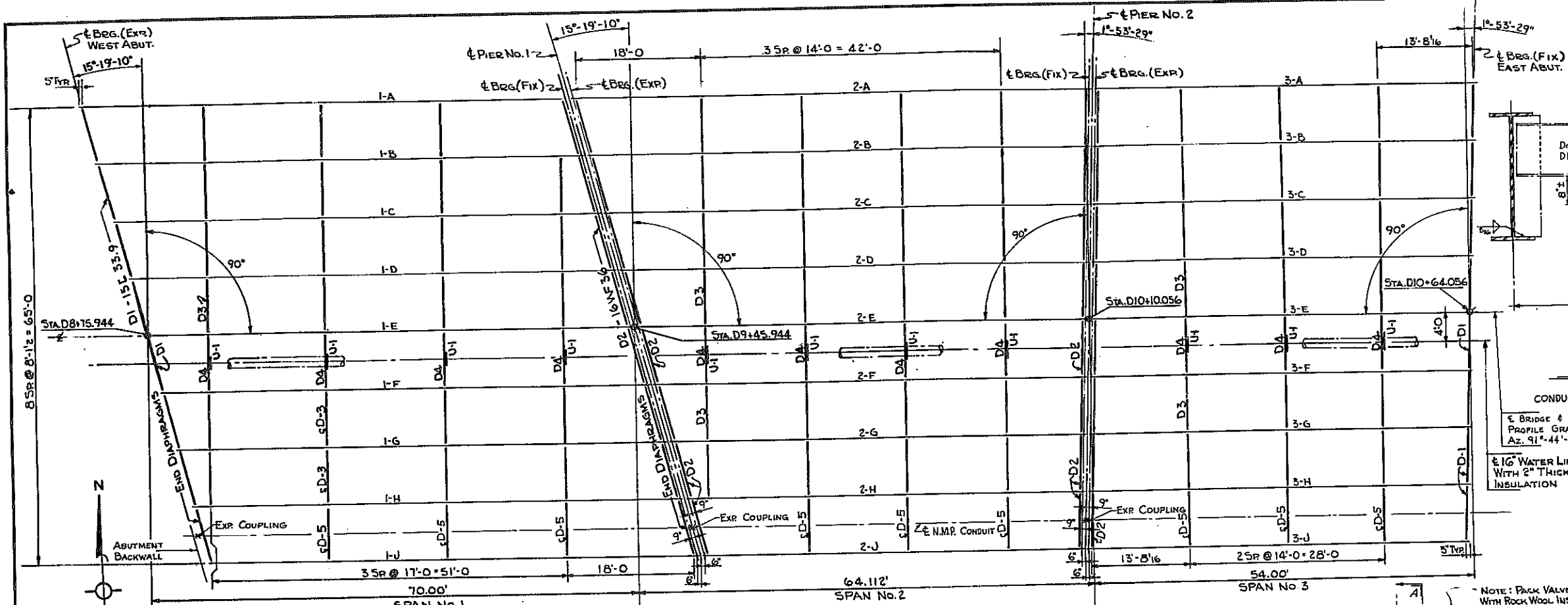
DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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BR. SHEET NO. 8



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	91	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE



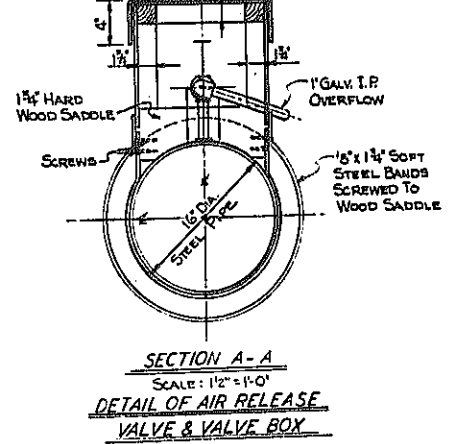
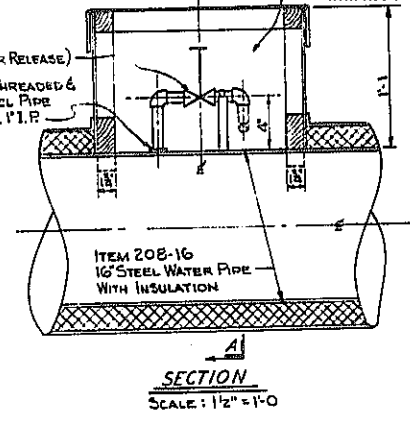
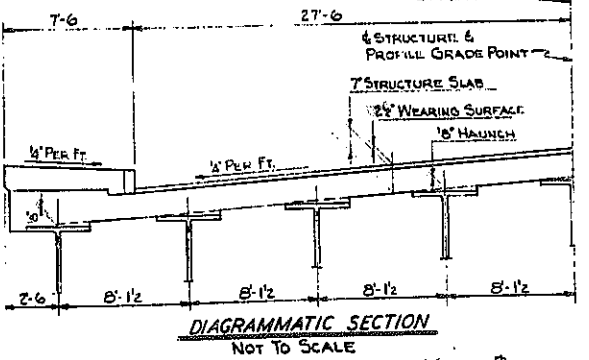
CONDUIT SUPPORTS FURNISHED BY N.M.P. CORR. - INSTALLED BY CONTRACTOR

NOTE: BRIDGE & DELAVAN AVE. PROFILE GRADE LINE AZ. 91°-44'-04.9"

NOTES:
PIPE INSULATION TO BE FURNISHED WITH VAPOR BARRIER JACKET OF TOUGH KRAFT FOL LAMINATE. JACKETED PIPE INSULATION SHALL BE COVERED WITH ALUMINUM WEATHER-PROOF JACKETING AS MFGD. BY CHILDERS MFG. CO. OR EQUAL.
INSULATION SHALL BE GLASS FIBER PIPE INSULATION IN ONE PIECE MOLDED SECTIONS 2" THICK, AS MFGD. BY GUSTIN-BACON MFG. CO. OR EQUAL.

NOTE: FOR DETAILS OF END DIAPHRAGMS D1, D2 AND INTERMEDIATE DIAPHRAGMS D3, AND D4, IN PANEL E-F ONLY, AND D5, IN PANEL H-J ONLY, SEE SHEET NO. 11.

FRAMING PLAN
SCALE: 1/8" = 1'-0"

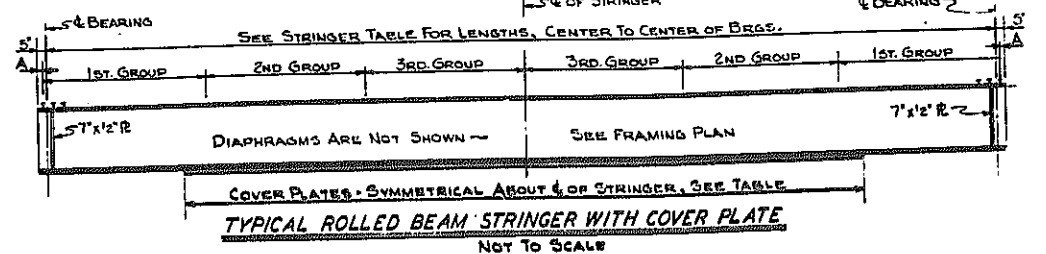


STRINGER SCHEDULE

SPAN	STRINGER NO.	MK	SIZE	CENTER TO CENTER BRGS.	BOTTOM COVER	CAMBER	STUD SHEAR CONNECTOR SPACING: 25 STUDS PER ROW		
							GROUP 1	GROUP 2	GROUP 3
1	1-A	36WF180	69'-6"	16x1	50'-0"	2"	28 @ 5' = 11'-8"	17 @ 7'-2" = 10'-7 1/2"	15 @ 10' = 12'-6"
	1-B	36WF180	69'-6"	16x1	50'-0"				
	1-C	36WF180	69'-6"	16x1	50'-0"				
	1-D	36WF180	69'-6"	16x1	50'-0"				
	1-E	36WF180	69'-6"	16x1 1/2	50'-0"				
	1-F	36WF170	69'-6"	16x1 1/2	50'-0"				
	1-G	36WF160	69'-6"	16x1	50'-0"				
	1-H	36WF160	69'-6"	16x1	50'-0"				
	1-I	36WF160	69'-6"	16x1	50'-0"				
	1-J	36WF160	69'-6"	16x1	50'-0"				
2	2-A	36WF170	73.088	17x1 1/2	55'-0"	3 3/4"	30 @ 5' = 12'-6"	20 @ 7'-2" = 12'-6"	14 @ 10' = 11'-8"
	2-B	36WF170	70.594	16x1 1/2	52'-6"	3 3/4"	29 @ 5' = 12'-1"	20 @ 7'-2" = 12'-6"	13 @ 10' = 10'-10"
	2-C	36WF160	68.100	16x7/8	50'-0"	2 7/8"	27 @ 5' = 11'-3"	18 @ 7'-2" = 11'-3"	14 @ 10' = 11'-8"
	2-D	36WF150	65.606	16x7/8	47'-6"	2 7/8"	26 @ 5' = 10'-10"	18 @ 7'-2" = 11'-3"	13 @ 10' = 10'-10"
	2-E	36WF150	63.112	16x7/8	45'-0"	2 7/8"	25 @ 5' = 10'-5"	18 @ 7'-2" = 11'-3"	12 @ 10' = 10'-0"
	2-F	36WF150	60.618	16x3/4	42'-6"	2"	25 @ 5' = 10'-5"	16 @ 7'-2" = 10'-0"	12 @ 10' = 10'-0"
	2-G	36WF150	58.124	14x3/4	40'-0"	1 3/4"	24 @ 5' = 10'-0"	16 @ 7'-2" = 10'-0"	11 @ 10' = 9'-2"
	2-H	36WF150	55.630	14x3/4	37'-6"	1 3/4"	23 @ 5' = 9'-7"	16 @ 7'-2" = 10'-0"	10 @ 10' = 8'-4"
	2-I	36WF150	53.136	14x3/4	35'-0"	1 3/4"	21 @ 5' = 8'-9"	14 @ 7'-2" = 8'-9"	11 @ 10' = 9'-2"
	2-J	36WF150	50.642	14x3/4	32'-6"	1 3/4"	20 @ 5' = 8'-4"	14 @ 7'-2" = 8'-8"	13 @ 9' = 9'-9"
3	3-A	30WF150	53'-6"	9x7/8	21'-0"	1 1/2"	25 @ 4' = 8'-4"	16 @ 6'-2" = 8'-8"	13 @ 9' = 9'-9"
	3-B	30WF116	53'-6"	14x7/8	39'-6"	1 3/4"			
	3-C	30WF116	53'-6"	14x7/8	39'-6"				
	3-D	30WF116	53'-6"	14x7/8	39'-6"				
	3-E	30WF124	53'-6"	14x7/8	39'-6"				
	3-F	30WF124	53'-6"	14x7/8	39'-6"				
	3-G	30WF116	53'-6"	14x7/8	39'-6"				
	3-H	30WF116	53'-6"	14x7/8	39'-6"				
	3-I	30WF116	53'-6"	14x7/8	39'-6"				
	3-J	30WF150	53'-6"	9x7/8	21'-0"	1 1/2"			

NOTE: CAMBER OF BEAM SHALL BE MEASURED WITH BEAM LYING ON ITS SIDE.

PLAN SECTION AT END OF COVER PLATE
SCALE: 3/4" = 1'-0"



NOTE: AIR RELEASE VALVE AND VALVE BOX TO BE LOCATED AT HIGH POINT OF EXPOSED WATER LINE UNDER STRUCTURE. PAYMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM 208-16

NOTE: FOR SPACING OF STUD SHEAR CONNECTORS SEE TABLE.

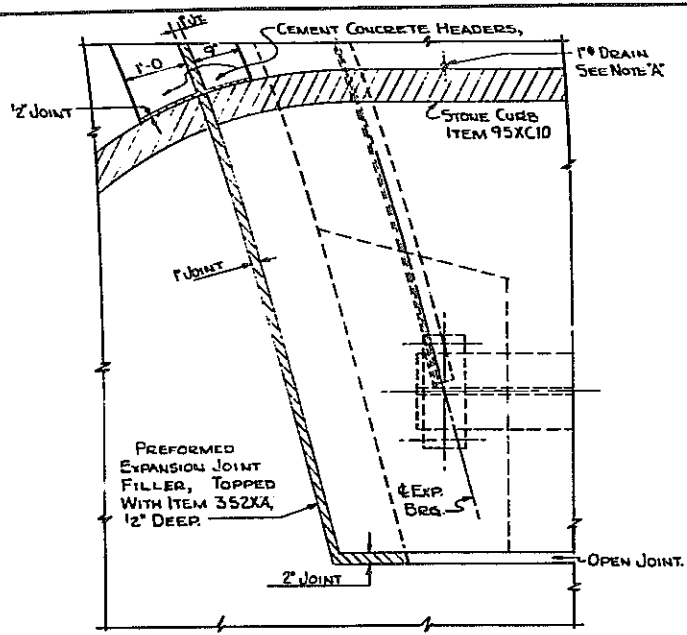
NOTE: FOR ALTERNATE DETAIL FOR SHEAR CONNECTOR SEE SHEET NO. 105.

DELAVAN AVE. OVER EXPRESSWAY FRAMING PLAN

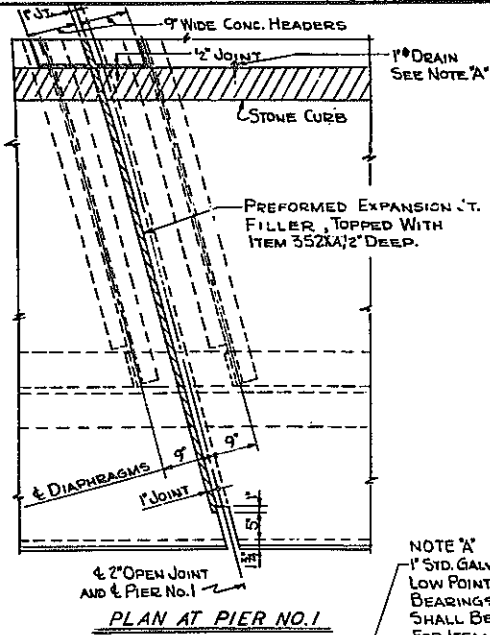
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED
R.B. C.E. R.B.	

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	92	173

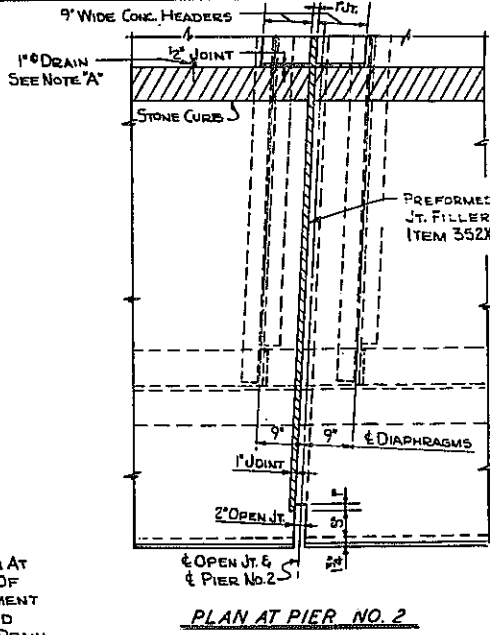
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



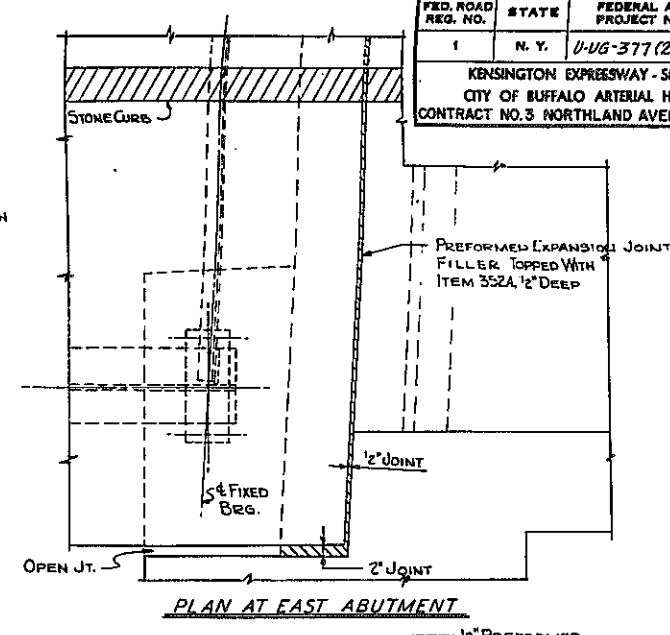
PLAN AT WEST ABUTMENT



PLAN AT PIER NO. 1

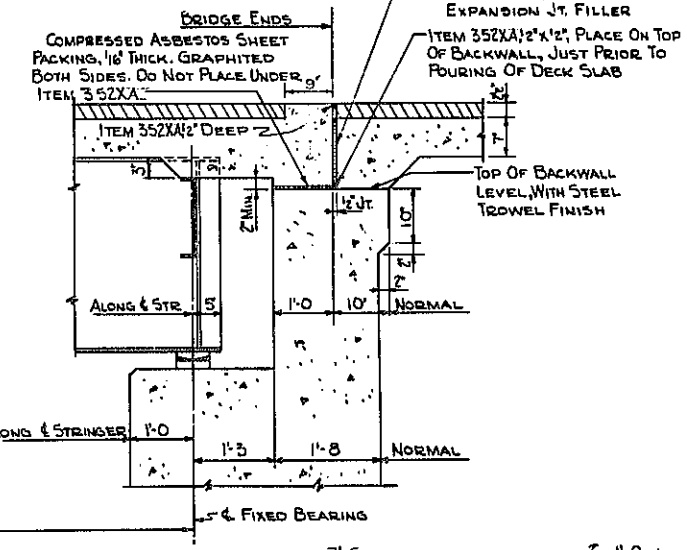
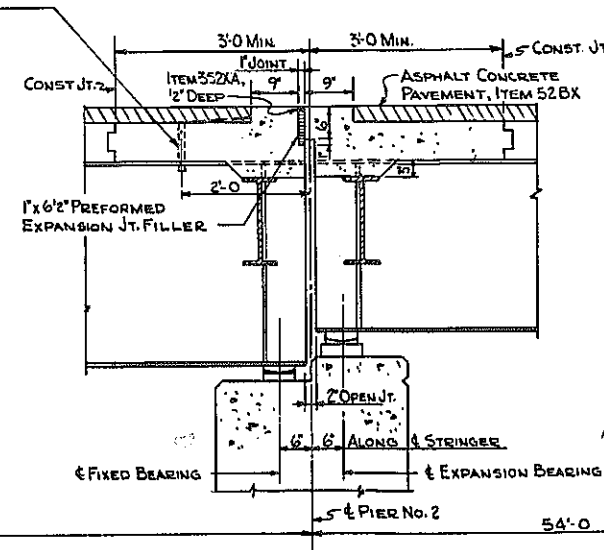
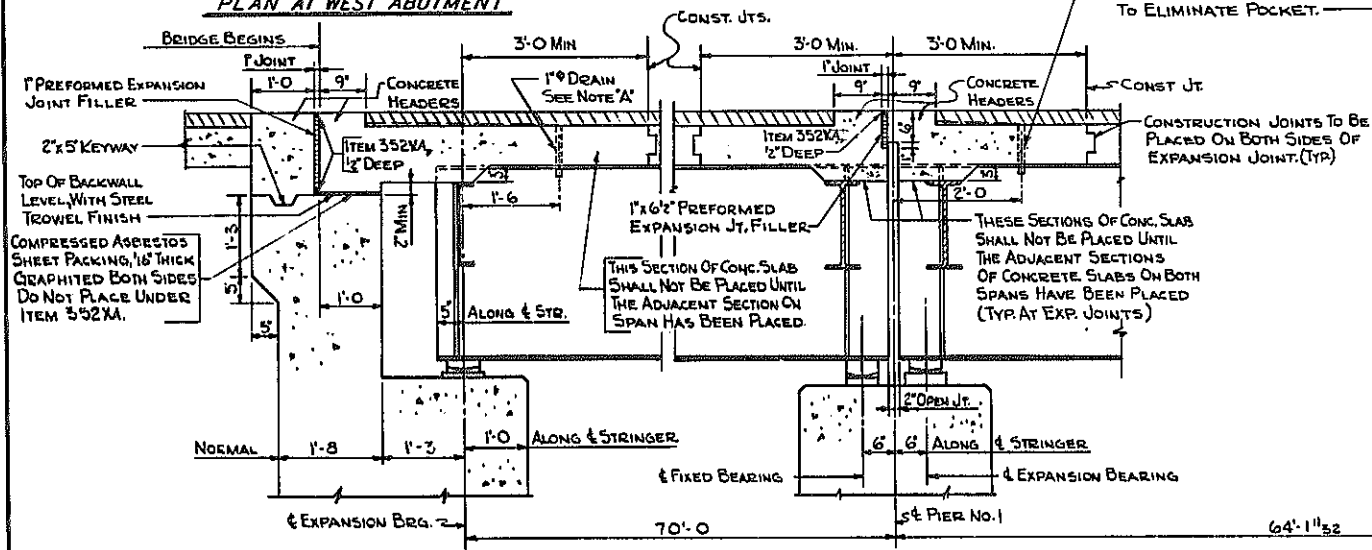


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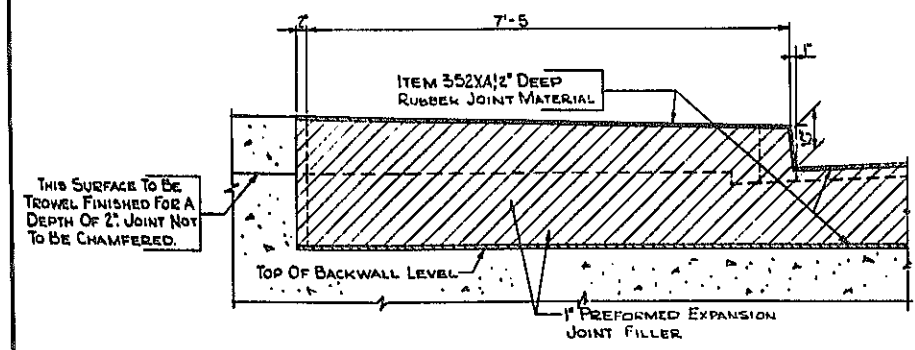


PLAN AT EAST ABUTMENT

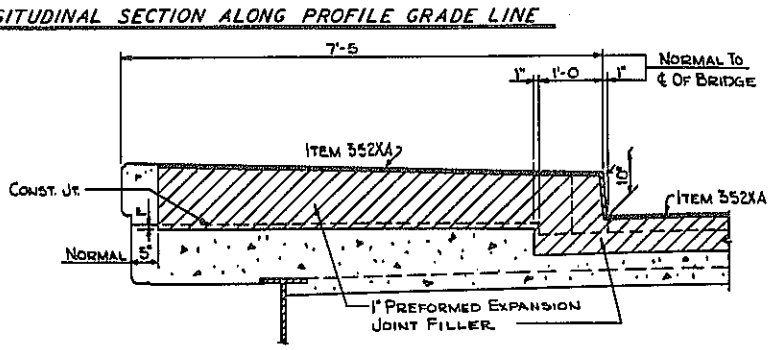
NOTE 'A'
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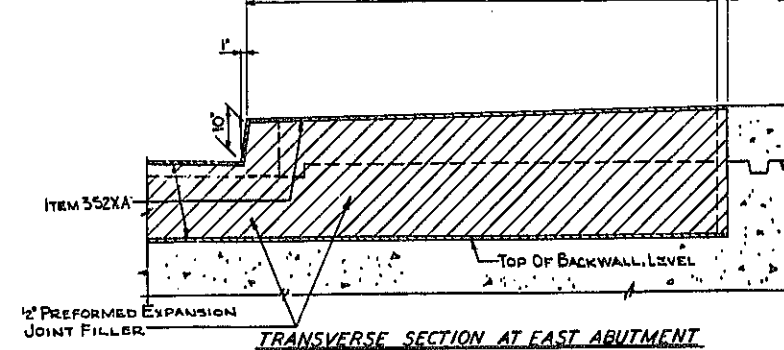
LONGITUDINAL SECTION ALONG PROFILE GRADE LINE



TRANSVERSE SECTION AT WEST ABUTMENT



TRANSVERSE SECTION AT PIERS

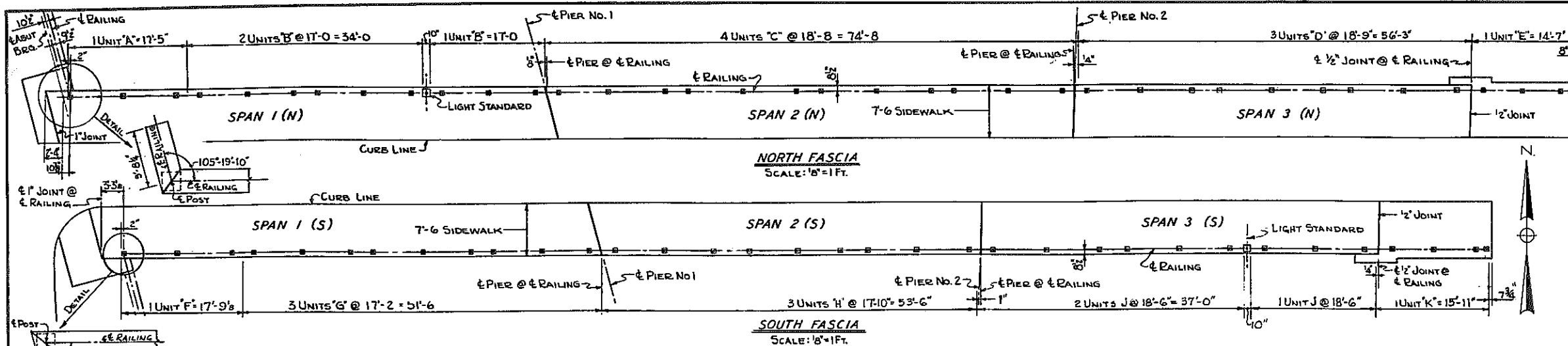
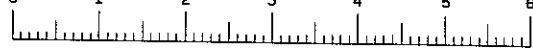


TRANSVERSE SECTION AT EAST ABUTMENT

SCALE: 3/4\"/>

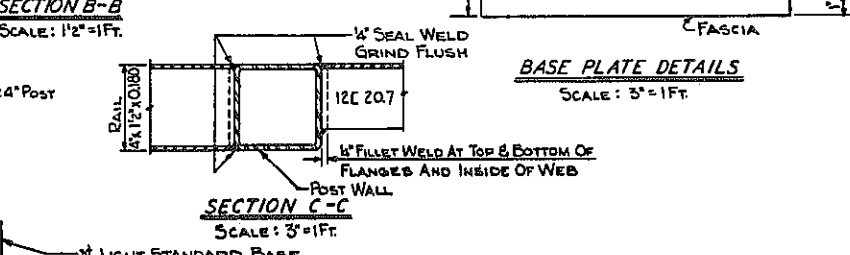
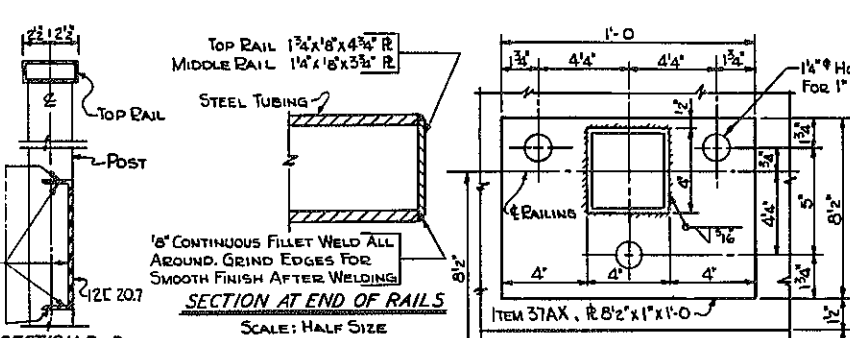
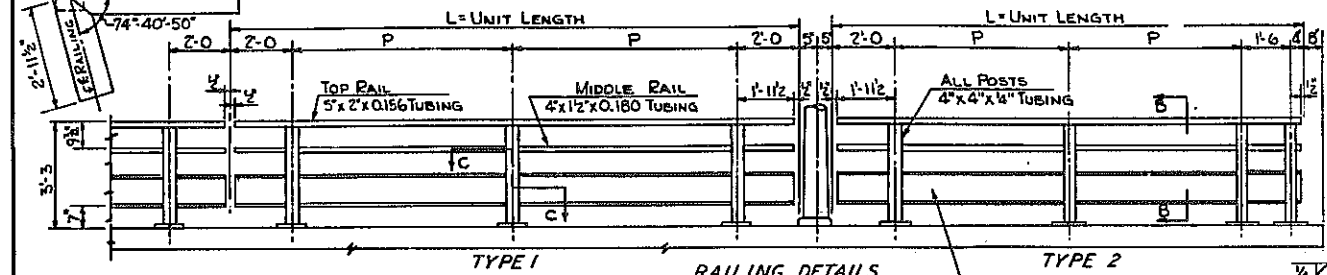
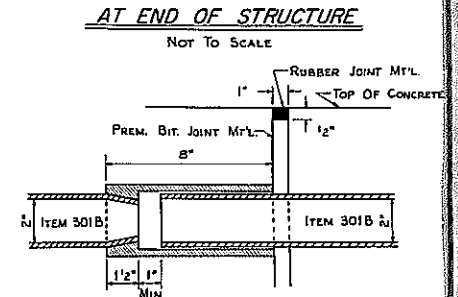
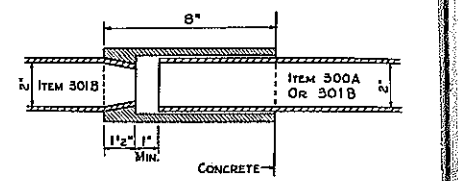
BR. SHEET NO. 10

DELAVAN AVE. OVER EXPRESSWAY LONGITUDINAL SECTION & DETAILS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.	



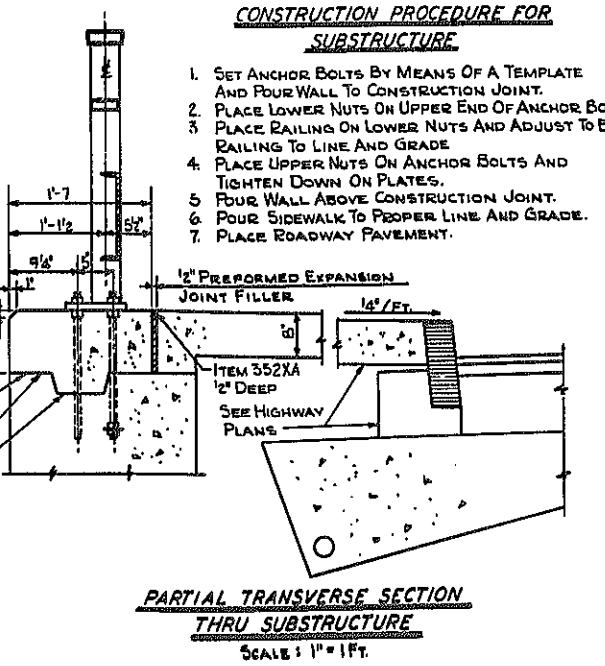
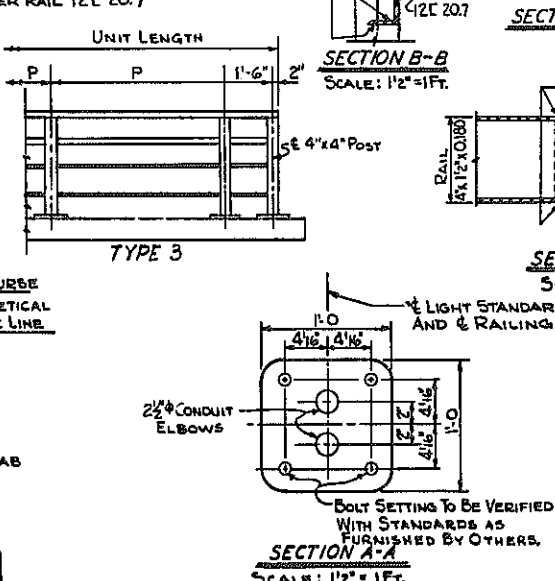
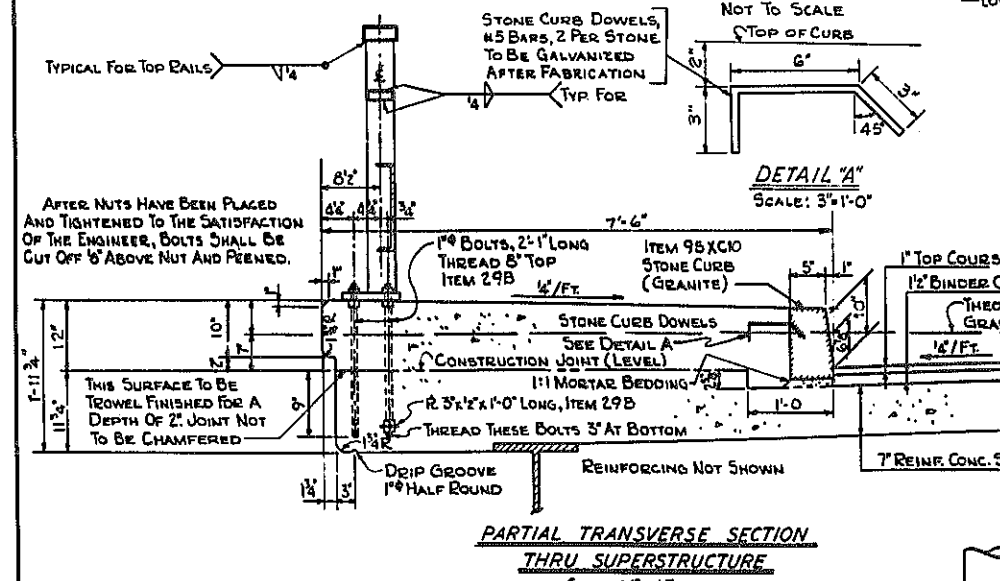
FED. ROAD PROJ. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	94	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



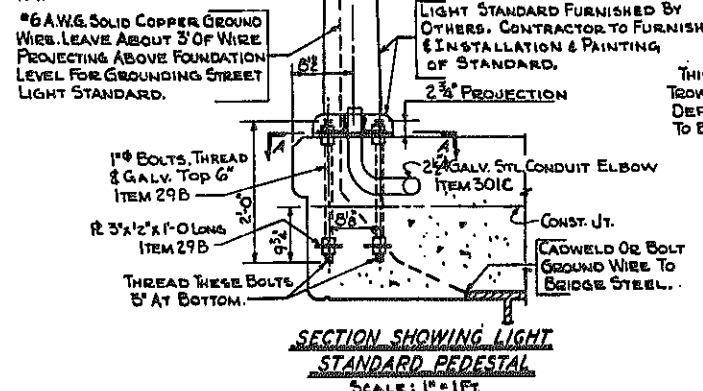
METAL RAILING - ITEM 37AX

LOCATION	UNIT	No.	P	L	TYPE
SPAN 1 (N)	A	1	6'-10 1/2"	17'-5"	3
SPAN 1 (N)	B	3	6'-6"	17'-0"	1
SPAN 2 (N)	C	4	7'-4"	18'-8"	1
SPAN 3 (N)	D	3	7'-4 1/2"	18'-9"	1
N.E. WALL	E	1	5'-4 1/2"	14'-7"	2
SPAN 1 (S)	F	1	7'-0 1/2"	17'-9 1/2"	3
SPAN 1 (S)	G	3	6'-7"	17'-2"	1
SPAN 2 (S)	H	3	6'-11"	17'-10"	1
SPAN 3 (S)	J	3	7'-3"	18'-6"	1
S.E. WALL	K	1	6'-0 1/2"	15'-11"	2



- CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR WALL TO CONSTRUCTION JOINT.
 2. PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 3. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 4. PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON PLATES.
 5. POUR WALL ABOVE CONSTRUCTION JOINT.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. PLACE ROADWAY PAVEMENT.
- RAILING NOTES**
1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
 2. ALL RAILINGS ARE TO BE FABRICATED AND ERRECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF FASCIA AND SO THAT THE POSTS ARE TRULY VERTICAL.
 3. ALL SURFACES OF WELDS ON RAILING TO BE GROUND SMOOTH.
 4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
 5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
 6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
 7. RAILS, TUBULAR POSTS AND BASE PLATES WILL BE PAID FOR UNDER ITEM 37AX.

- CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR SLAB, BROOM FINISH TOP OF SLAB BETWEEN CURBS.
 2. PLACE ITEM 95XC10, STONE CURB (GRANITE).
 3. PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 4. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 5. PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON PLATES.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. BROOMED SURFACE OF SLAB TO BE GIVEN A WATERPROOFING, ITEM 361C.
 8. PLACE ROADWAY PAVEMENT.

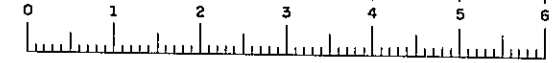


DE LAVAN AVE. OVER EXPRESSWAY
RAILING LAYOUT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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BR. SHEET NO. 12



BAR LIST

MARK	5507	5508	5512	5513
TYPE	I	II	I	II
A	3'-11"	3'-11"	3'-7 1/2"	3'-7 1/2"
B	2'-4"	2'-4"	2'-3"	2'-3"
C	33'-8"	25'-3"	32'-6"	24'-4 1/2"
D	38'-9"	34'-8"	37'-6"	33'-5"
L	40'-10"	36'-7"	39'-7"	35'-4"

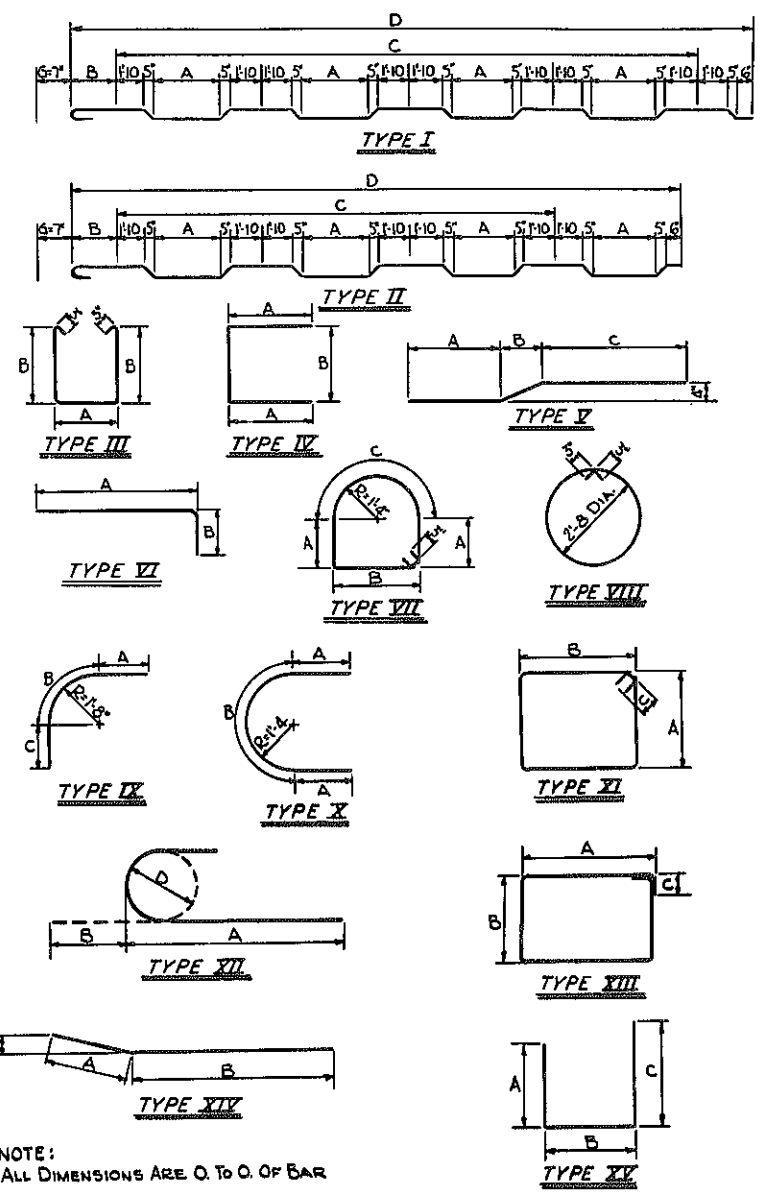
L = TOTAL LENGTH

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	95	173

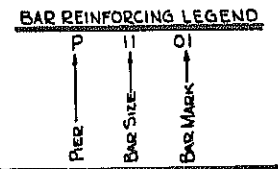
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

ABUTMENTS (2)									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
EAST ABUTMENT - FOOTING									
A401	#4	6'-0"	STR.	98				TRANSVERSE BARS ABUT. FTG.	
A402	#4	37'-8"	STR.	16				LONGITUDINAL BARS ABUT. FTG.	
A403	#4	4'-0"	STR.	49				TRANSVERSE BARS WINGWALL FTGS.	
A404	#4	20'-6"	STR.	6				LONGITUDINAL BARS N. WINGWALL FTG.	
A405	#4	21'-10"	STR.	6				LONGITUDINAL BARS S. WINGWALL FTG.	
EAST ABUTMENT - WALLS									
A601	#6	8'-9"	STR.	70				VERTICAL BARS ABUT. WALL	
A602	#6	9'-11"	STR.	17				VERTICAL BARS WINGWALL	
A603	#6	10'-10"	STR.	26				VERT. BARS WINGWALL & CURTAIN WALL	
A604	#6	5'-11"	STR.	27				VERTICAL BARS PEDESTALS	
A605	#6	4'-6"	IV	6	2'-0"	8'		U-BARS CURTAIN WALL	
A606	#6	36'-8"	STR.	16				HORIZONTAL BARS ABUT. WALL	
A607	#6	5'-8"	STR.	10				HORIZONTAL BARS CURTAIN WALL	
A608	#6	15'-10"	STR.	4				HORIZONTAL BARS N. WINGWALL	
A609	#6	17'-2"	STR.	4				HORIZONTAL BARS S. WINGWALL	
A610	#6	14'-2"	STR.	1				HORIZONTAL BARS N. WINGWALL	
A611	#6	15'-6"	STR.	1				HORIZONTAL BARS S. WINGWALL	
A612	#6	18'-3"	V	5	3'-3"	2'-5"	12'-7"	HORIZONTAL BARS N. WINGWALL	
A613	#6	19'-5"	V	5	3'-3"	2'-5"	13'-9"	HORIZONTAL BARS S. WINGWALL	
A614	#6	3'-2"	VI	33	1'-3"	2'-0"		VERTICAL BARS WINGWALLS	
A406	#4	14'-2"	STR.	2				LONGITUDINAL BARS N. WINGWALL	
A407	#4	15'-6"	STR.	2				LONGITUDINAL BARS S. WINGWALL	
A408	#4	10'-0"	IV	45	3'-6"	3'-2"		HORIZONTAL BARS PEDESTALS	
WEST ABUTMENT - FOOTING									
A451	#4	6'-0"	STR.	86				TRANSVERSE BARS ABUT. FTG.	
A452	#4	39'-2"	STR.	16				LONGITUDINAL BARS ABUT. FTG.	
A453	#4	Avg. 17'-8"	STR.	6				LONGITUDINAL BARS S. WINGWALL FTG. T&B VARIES 10'-8" TO 12'-8" INCREMENT	
A454	#4	Avg. 11'-2"	STR.	18				LONGITUDINAL BARS S. WINGWALL FTG. T&B VARIES 10'-2" TO 12'-2" INCREMENT	
A455	#4	10'-5"	STR.	8				TRANSVERSE BARS S. WINGWALL FTG.	
A456	#4	9'-0"	STR.	2				TRANSVERSE BARS S. WINGWALL FTG.	
A457	#4	8'-6"	STR.	15				LONGITUDINAL BARS N. WINGWALL FTG.	
A458	#4	8'-9"	STR.	5				TRANSVERSE BARS N. WINGWALL FTG.	
WEST ABUTMENT - WALLS									
A651	#6	8'-9"	STR.	82				VERTICAL BARS ABUT. WALL	
A652	#6	10'-11"	STR.	15				VERTICAL BARS WINGWALL	
A653	#6	5'-9"	STR.	27				VERTICAL BARS PEDESTALS	
A654	#6	5'-6"	IV	1	2'-0"	1'-8"		U-BARS CURTAIN WALL	
A655	#6	6'-0"	IV	2	2'-0"	2'-2"		U-BARS CURTAIN WALL	
A656	#6	6'-5"	IV	1	2'-0"	2'-7"		U-BARS CURTAIN WALL	
A657	#6	39'-5"	STR.	16				HORIZONTAL BARS ABUT. WALL	
A658	#6	12'-0"	XIV	5	4'-4"	7'-8"	1'-2"	HORIZONTAL BARS S. CURTAIN WALL	
A659	#6	10'-0"	STR.	5				HORIZONTAL BARS S. CURTAIN WALL	
A660	#6	7'-0"	STR.	5				HORIZONTAL BARS N. CURTAIN WALL	
A661	#6	8'-2"	V	5	2'-11"	1'-3"	4'-0"	HORIZONTAL BARS N. CURTAIN WALL	
A662	#6	6'-0"	IV	58	2'-9"	8'		U-BARS CONCRETE HEADER	
A459	#4	10'-4"	XV	45	3'-3"	3'-2"	4'-1"	HORIZONTAL BARS PEDESTALS	

PIERS (2)										
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	DESCRIPTION
				PIER 1	PIER 2	TOTAL				
FOOTINGS										
P601	#6	6'-6"	STR.	90	90	180				HORIZONTAL BARS
P801	#8	4'-2"	STR.	42	42	84				DOWELS INTERIOR COLUMNS
P1101	#11	6'-5"	XII	40	40	80	4'-10"	1'-7"		DOWELS EXTERIOR COLUMNS
COLUMNS & CAP BEAM										
P802	#8	16'-4"	STR.	42		42				VERTICAL BARS
P803	#8	16'-7"	STR.			42				VERTICAL BARS
P1102	#11	16'-4"	STR.	40		40				VERTICAL BARS
P1103	#11	16'-7"	STR.			40				VERTICAL BARS
P401	#4	8'-9"	VIII	75	75	150				COLUMN TIES
P901	#9	36'-0"	STR.	10		10				HORIZONTAL BARS BOTTOM
P902	#9	19'-4"	STR.	20		20				HORIZONTAL BARS BOTTOM
P903	#9	34'-8"	STR.			10				HORIZONTAL BARS BOTTOM
P904	#9	18'-8"	STR.			20				HORIZONTAL BARS BOTTOM
P905	#9	27'-10"	STR.	8		8				HORIZONTAL BARS TOP
P906	#9	19'-0"	STR.	5		5				HORIZONTAL BARS TOP
P907	#9	14'-2"	STR.	14		14				HORIZONTAL BARS TOP
P908	#9	26'-10"	STR.	8		8				HORIZONTAL BARS TOP
P909	#9	18'-4"	STR.	5		5				HORIZONTAL BARS TOP
P910	#9	13'-10"	STR.	14		14				HORIZONTAL BARS TOP
P804	#8	9'-9"	IX	6	6	12	5'-6"	2'-7 1/2"	1'-7 1/2"	CORNER BARS CAP
P501	#5	34'-6"	STR.	8		8				HORIZONTAL BARS SIDES
P502	#5	6'-2"	X	8	8	16	1'-0 1/2"	4'-1"		HORIZONTAL BARS FASCIA ENDS
P402	#4	10'-8"	XI	188	180	368	3'-2"	2'-0"	4'-2"	CAP STIRRUPS
P403	#4	3'-8"	XII	88	88	176	3'-2"	6"		CAP PEDESTALS DOWELS
P602	#6	11'-3"	VII	6	6	12	2'-1"	2'-8"	4'-1"	HORIZONTAL BARS FASCIA PEDESTALS
P603	#6	14'-10"	XIII	21	21	42	3'-8"	2'-8"	1'-3 1/2"	HORIZONTAL BARS INTERIOR PED.
P503	#5	33'-2"	STR.	8		8				HORIZONTAL BARS SIDES
SLAB										
S501	#5	36'-6"	STR.	184		184				LONGITUDINAL BARS SLAB & SIDEWALK
S502	#5	Avg. 32'-0"	STR.			184				LONGITUDINAL BARS SLAB & SIDEWALK VARIES 27'-6" TO 38'-2" INCREMENT
S503	#5	28'-6"	STR.			184	184			LONGITUDINAL BARS SLAB & SIDEWALK
S504	#5	36'-6"	STR.	174	2	176				TRANSVERSE BARS SLAB
S505	#5	40'-9"	STR.	87	1	88				TRANSVERSE BARS SLAB
S506	#5	32'-4"	STR.	87	1	88				TRANSVERSE BARS SLAB
S507	#5	40'-10"	I	86		86				TRANSVERSE BARS BENT SLAB
S508	#5	36'-7"	II	86		86				TRANSVERSE BARS BENT SLAB
S509	#5	35'-3"	STR.	128	136	264				TRANSVERSE BARS SLAB
S510	#5	39'-4"	STR.	64	68	132				TRANSVERSE BARS SLAB
S511	#5	31'-2"	STR.	64	68	132				TRANSVERSE BARS SLAB
S512	#5	39'-7"	I	65	67	130				TRANSVERSE BARS BENT SLAB
S513	#5	35'-4"	II	65	67	130				TRANSVERSE BARS BENT SLAB
S514	#5	Avg. 35'-4"	STR.	102		102				TRANSVERSE BARS SLAB VARIES 1'-1 1/2" TO 69'-4 1/2" INCREMENT
S401	#4	4'-8"	III	146	152	114	4'-12"	1'-8"		TRANSVERSE HOOPS SIDEWALK
S402	#4	7'-1"	STR.	146		146				TRANSVERSE BARS SIDEWALK
S403	#4	6'-10"	STR.		152	114				TRANSVERSE BARS SIDEWALK



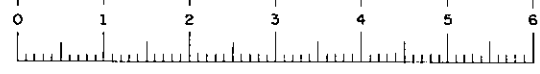
NUMBER	SIZE
4	1/2"
5	3/8"
6	3/4"
8	1"
9	1 1/4"
11	1 3/4"



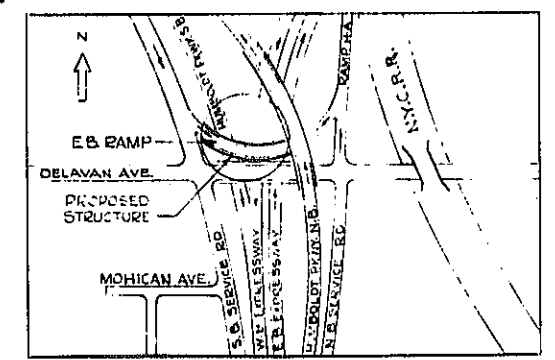
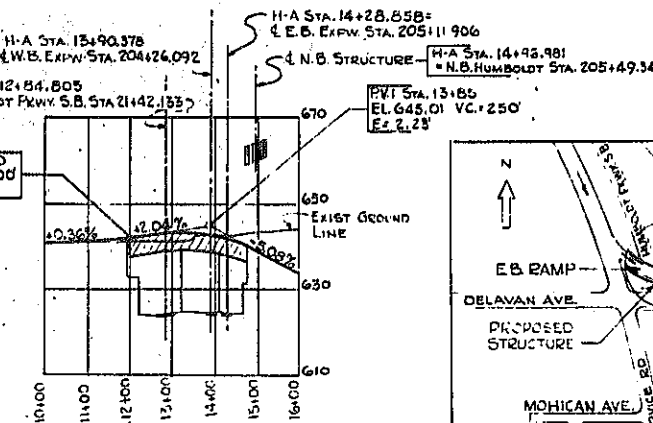
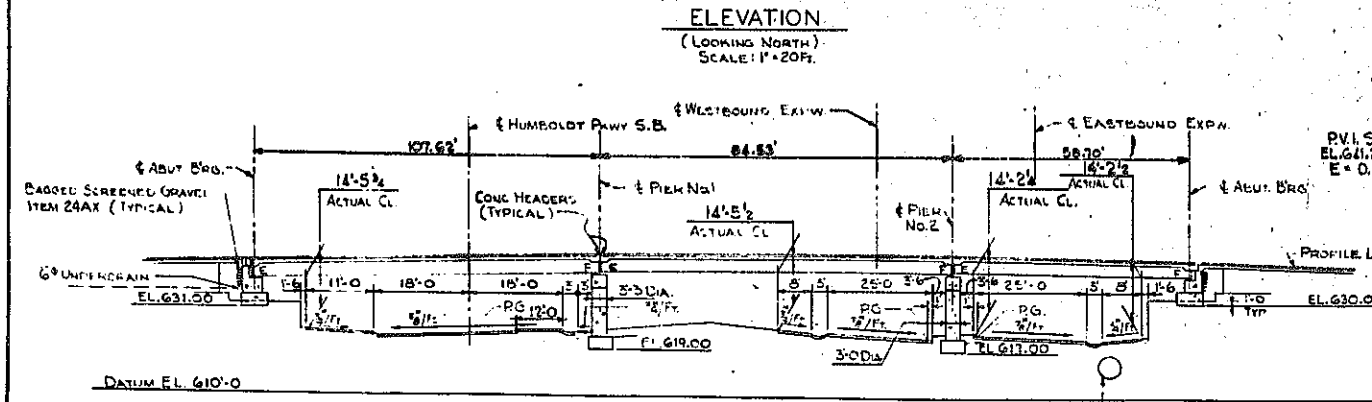
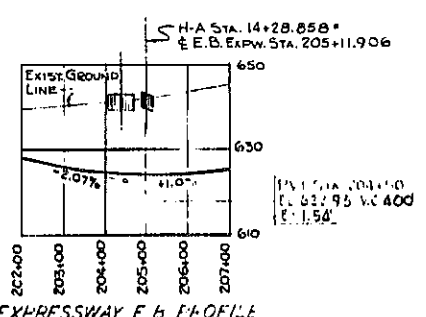
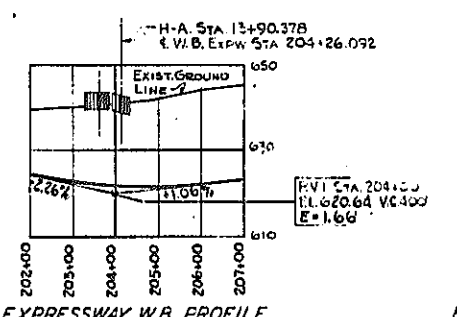
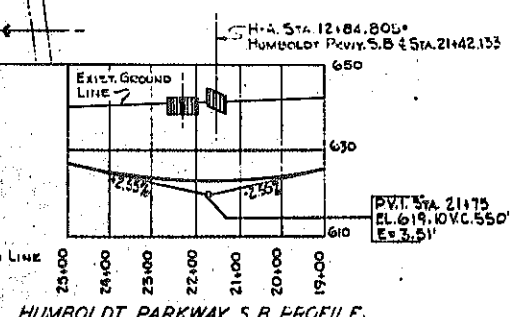
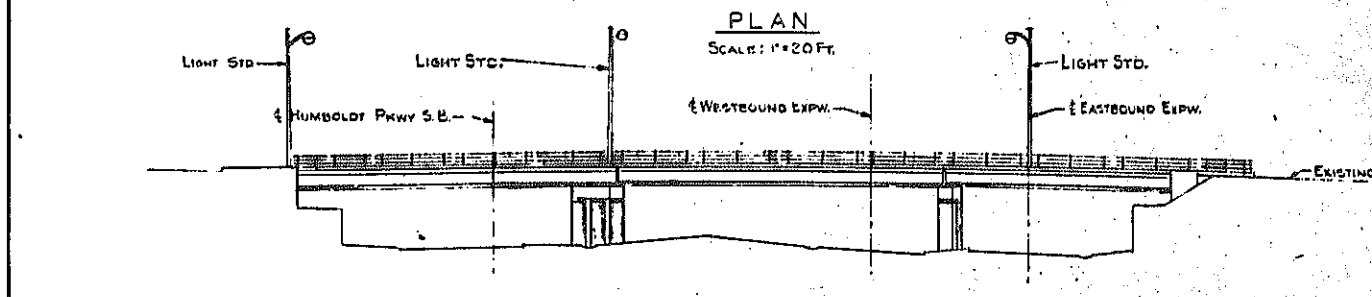
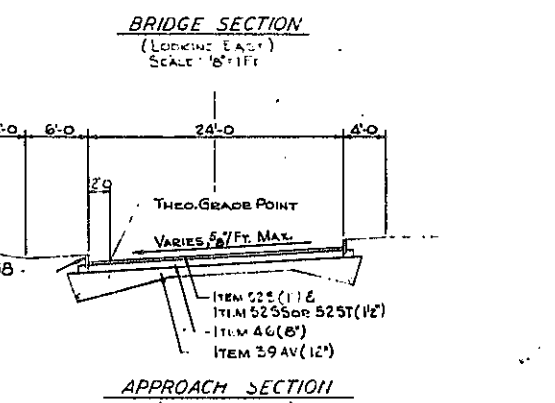
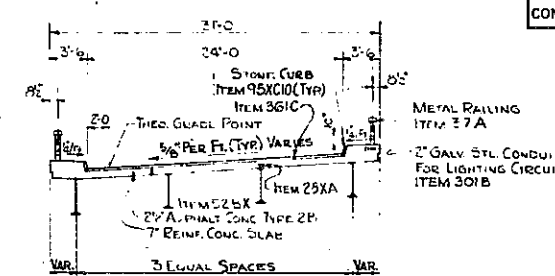
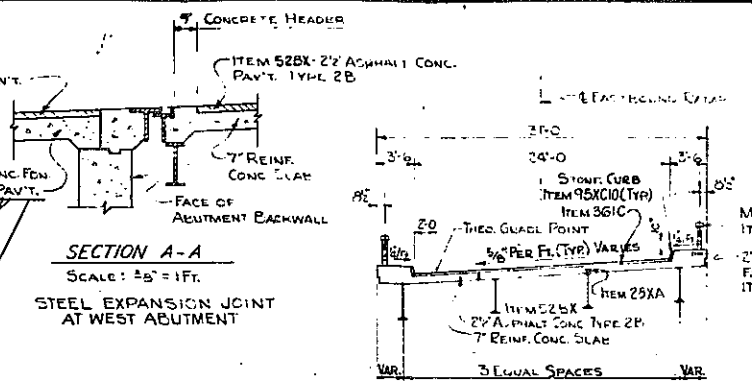
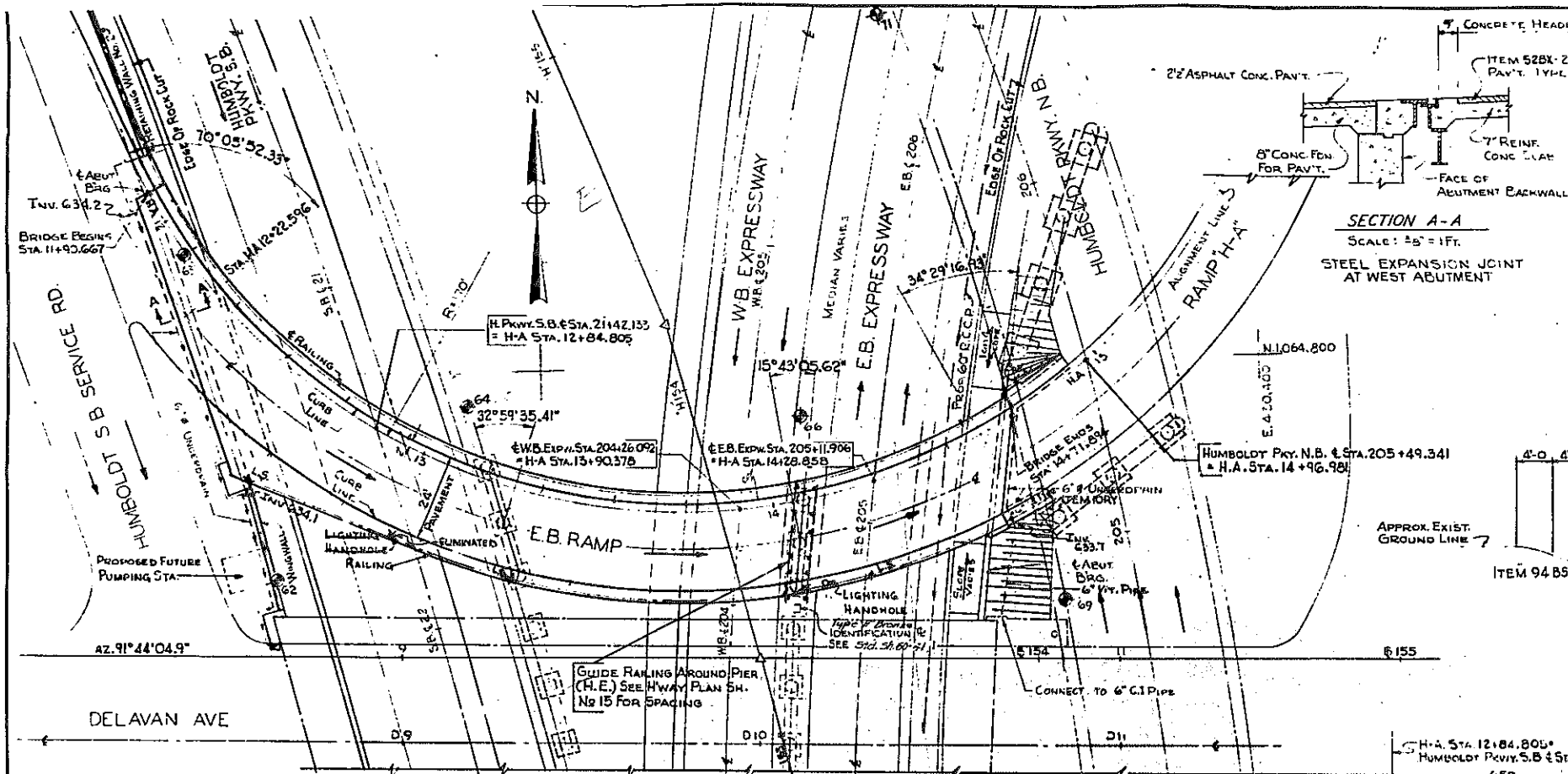
DELAVAN AVE. OVER EXPRESSWAY
BAR LIST

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	N. Y.	U-UG-377(25)	96R1	173
KENSINGTON EXPRESSWAY - SEC NO III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



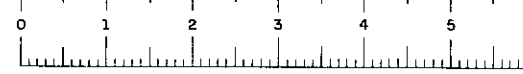
REVISION TO HANDHOLE 17
(RAMP H-A)

E B RAMP OVER EXPRESSWAY
GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DR. LEW. CATHAR & BRILL ENGINEERS-ARCHITECTS
DRAWN BY R.B.
CHECKED BY C.E.
NEW YORK

38. SHEET NO. 1



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	97 RI	173

KENSINGTON EXPRESSWAY, SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.N.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : H20-S16-44 LIVE LOAD

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, OR THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.

THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, ABUTMENT SIDE WALK POURS AND WINGWALL (BETWEEN E.B. RAMP & DELAVAN AVE.) SHALL BE ITEM 16X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS (EXCEPT IN SIDEWALK POURS) AND ABUTMENT WINGWALL SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

CEMENT

CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A, ITEMS 16X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE: ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET GO-41.

OTHER

REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL THROGEL FINISHED.

SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE POURED 1/8" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAINGED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.

THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 20 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREMOULDED BITUMINOUS JOINT, COMPRESSED ASBESTOS SHEET PACKING, PREF. EXP. JT. FILLER AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

FOOTINGS : ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES, SPECIFICATIONS SEE SHEET NO. B4.

TABLE OF BEARINGS

LOCATION	STRINGER	Nº	A	B	C	D	1" ANCHOR BOLTS
EXPANSION BEARINGS							
WEST ABUT.	1A to 1H		1 3/8"		4 5/8"		16 1'-2"
PIER No. 1	2A to 2D		1 3/8"		4 5/8"		8 1'-2"
	2-E		2 3/8"		5 3/8"		2 1'-3"
PIER No. 2	3A to 3D		2 3/8"		5 3/8"		8 1'-3"
FIXED BEARINGS							
PIER No. 1	1A to 1D			3 3/8"		4 5/8"	8 1'-3"
	1-E			1 5/8"		3 3/8"	2 1'-2"
PIER No. 2	2A to 2D			1 5/8"		3 3/8"	8 1'-2"
EAST ABUT.	3A to 3D			1 5/8"		3 3/8"	8 1'-2"

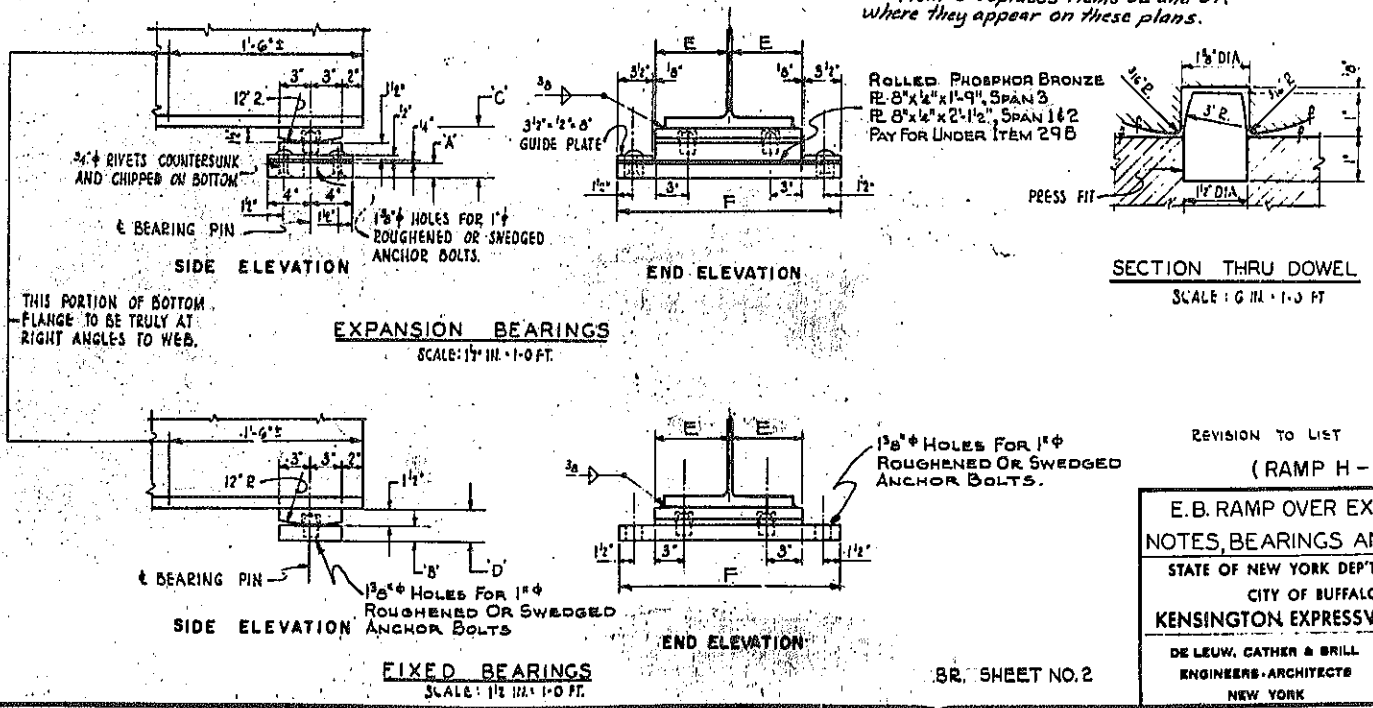
BEARING DIMENSIONS

STRINGER	E	F
1A to 1E	9 1/8"	2'-1 1/2"
1F to 1H	6 3/8"	1'-9"
2A to 2D	9 3/8"	2'-1 1/2"
2E	6 3/8"	1'-9"
3A to 3D	6 3/8"	1'-9"

ESTIMATE OF QUANTITIES

ITEM NO	DESCRIPTION	UNIT	TOTAL NET	TOTAL ROUNDED	FINAL
2EF-B	SELECTED FILL	C.Y.	314	350	252.2
5	TRENCH, CULVERT & BRIDGE EXC.	C.Y.	87	95	118.04
11 H	PERF. CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	208	215	203
152A	PORTLAND CEMENT TYPE 2A	Bbl.	1088	1,130	931.9
16X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	361	370	350.1
20Y	CLASS 1 CONCRETE	C.Y.	329	340	305.22
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	54	59	52.52
2B	BAR REINFORCEMENT FOR STRUCTURES	Lb.	84,843	86,500	86,981
28B	SHEAR CONNECTORS	L.F.	1,174	1,250	1,171
29B	STRUCTURAL STEEL	Lb.	351,761	362,300	351,326
37 A	METAL RAILING	L.F.	529	540	531.3
52BX	ASPHALT CONCRETE TYPE 2B (2 1/2")	Ton	98	100	84.5
61	BITUMINOUS MATERIAL	GAL.	131	140	83.0
95XC10	10" STONE CURB (BRIDGE)	L.F.	560	570	538.2
301B	2" GALVANIZED STEEL CONDUIT	L.F.	241	250	255.5
303CG	FURNISH & INSTALL LIGHT STDS. 30' MTG. HT.	EA.	2	2	2.00
9VB	SEWER PIPE (VITRIFIED) 6" DIA.	L.F.	25	30	0.0
13 B	CAST IRON PIPE, 6" DIA.	L.F.	6	6	0.0
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	49	50	45.6
305	MISCELLANEOUS METALS	Lb.	150	160	100
301G	2 1/2" GALVANIZED STEEL CONDUIT	L.F.	15	15	20.0
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	18	20	20.50
303CB	FURNISH & INSTALL LIGHT STDS. 30' MTG. HT.	EA.	1	1	1.000
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	78	80	151.0
352XA	RUBBER JOINT MATERIAL	GAL.	9	10	17.82
63TX	TEMPORARY SHEET PILING	SF	1021	1070	0

Notes:
Item 5 replaces Items 5E and 5F where they appear on these plans.

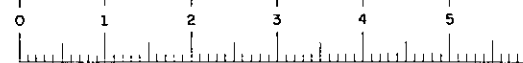


E.B. RAMP OVER EXPRESSWAY
NOTES, BEARINGS AND ESTIMATE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

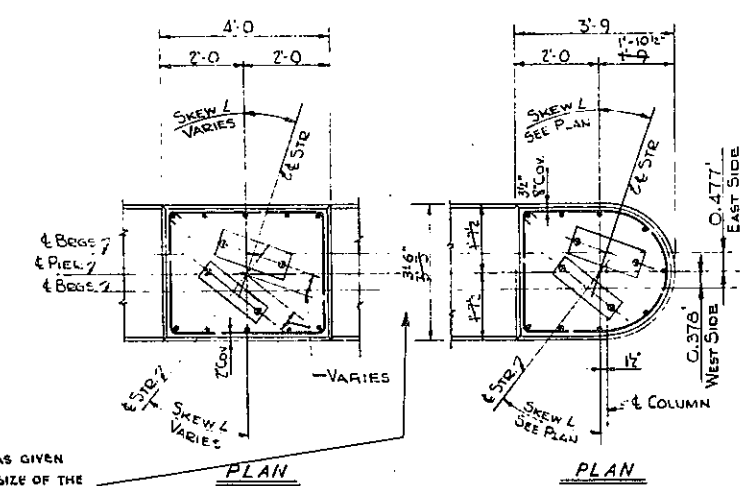
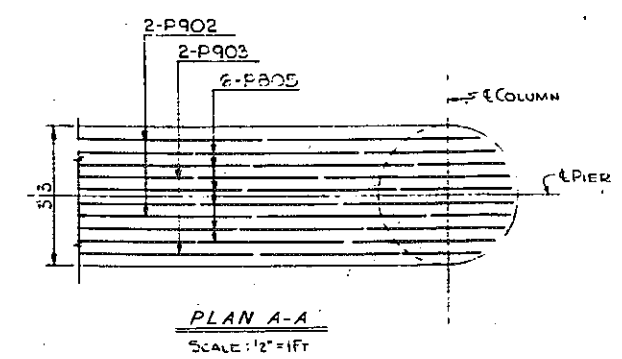
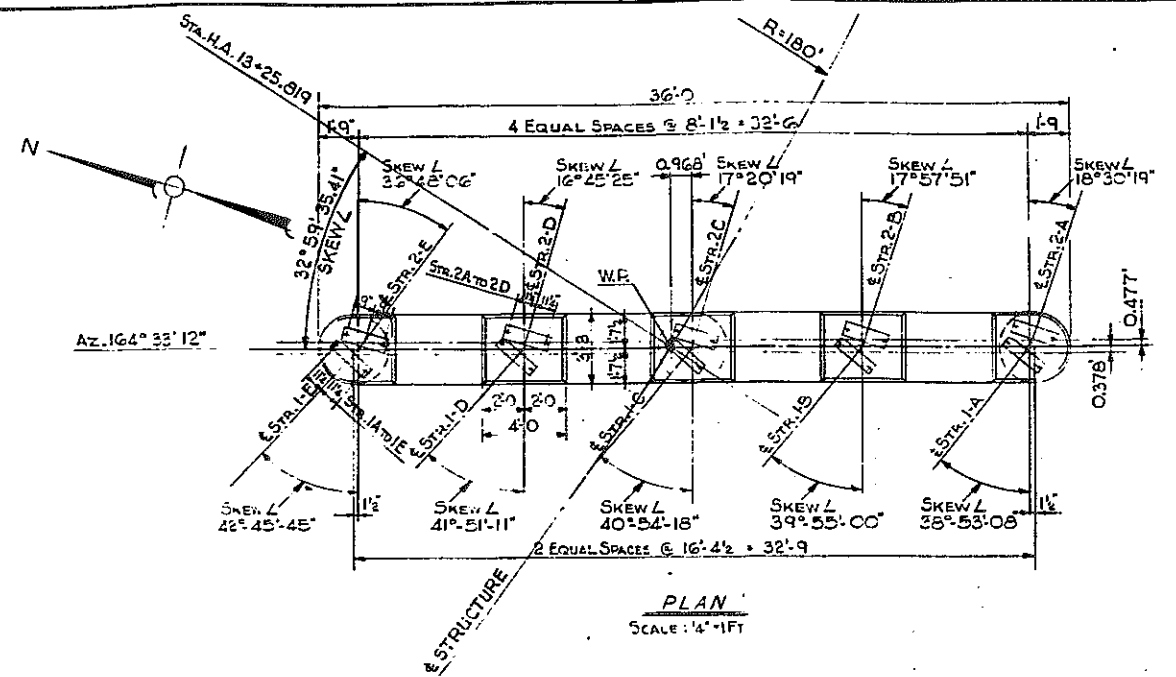
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.S. D.E.L. J.S.
--	----------------------------	------------------------

BR. SHEET NO. 2

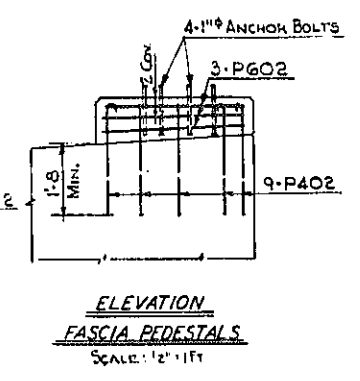
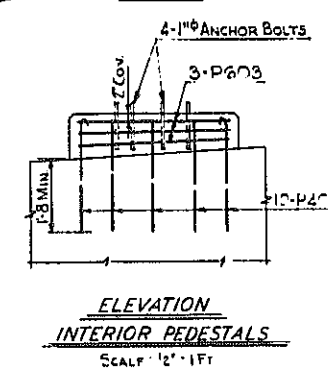
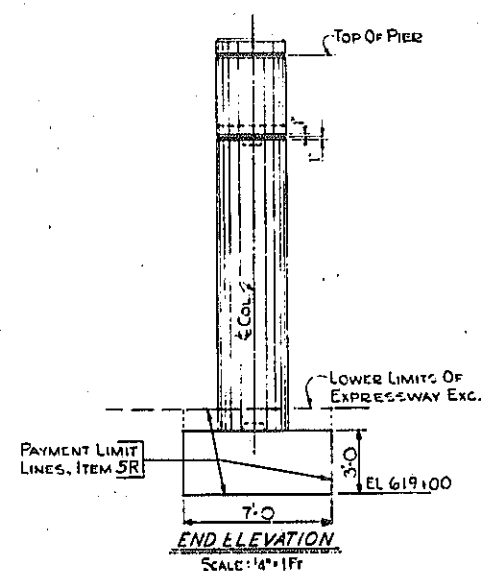
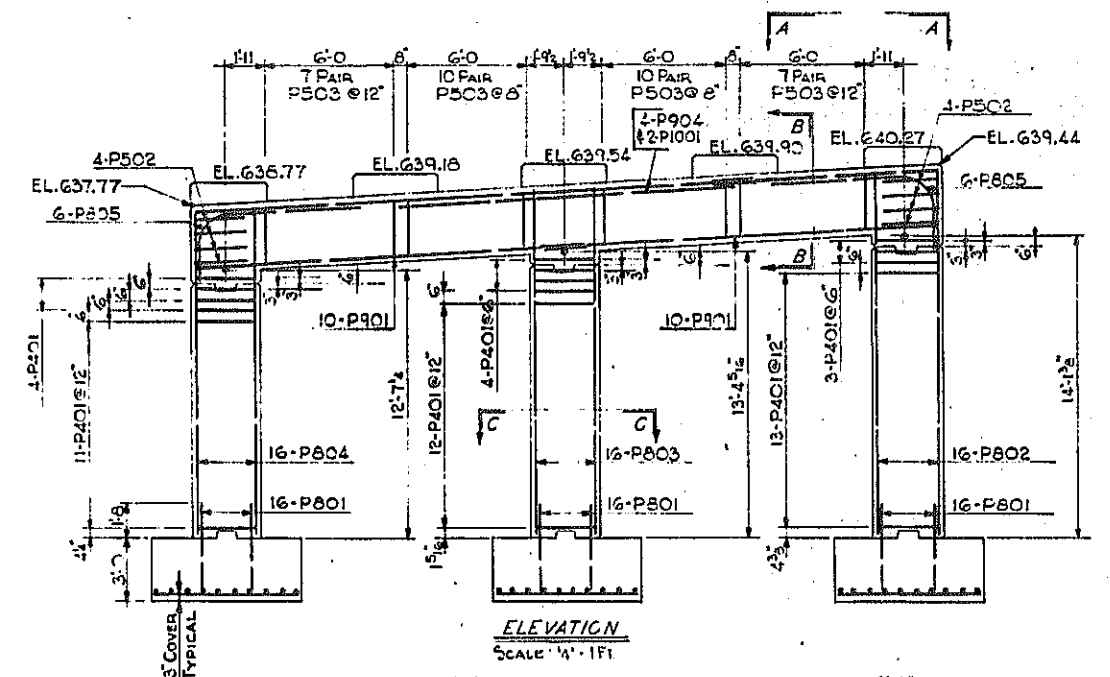


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	95 RI	173

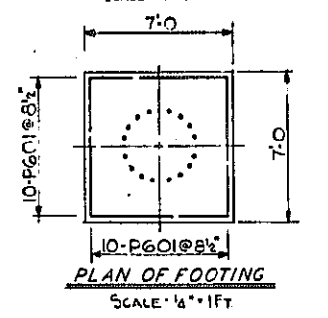
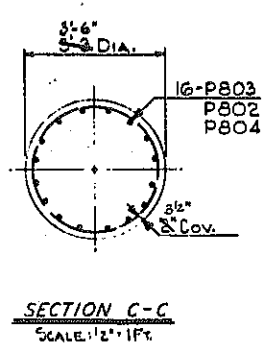
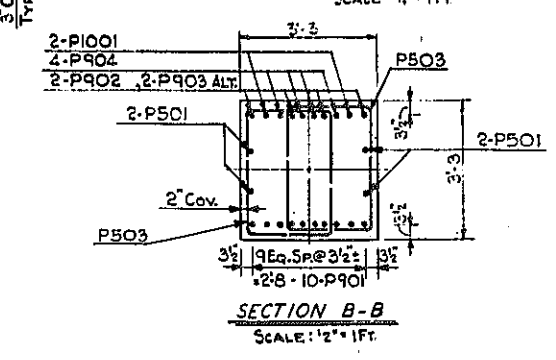
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: S.J. GRAVES & SONS WAS GIVEN PERMISSION TO INCREASE THE SIZE OF THE PIER COLUMNS & BEAM TO 3'-6" AT NO EXTRA COST TO THE STATE. (PAY AS PER PLAN SIZES.)



NOTES:
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18Y, CLASS 1A CONCRETE FOR STRUCTURES.
CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS AND SHALL BE ACCURATELY PLACED BY MEANS OF A TEMPLATE AND SET 10' INTO MASONRY.
SEE SHEET NO 2 FOR ANCHOR BOLT LENGTHS.



REVISION TO PIER WIDTH (7)

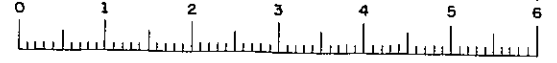
(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY PIER NO. I

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN C.H.B.
	CHECKED C.E.
	TRACED

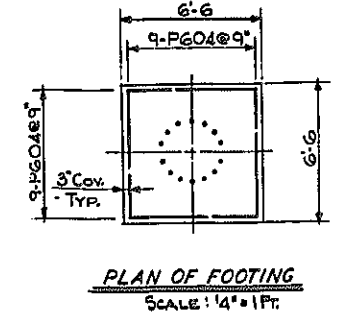
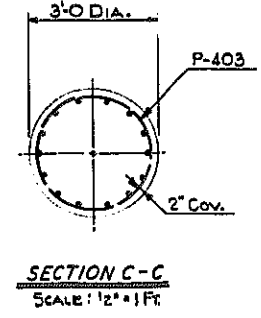
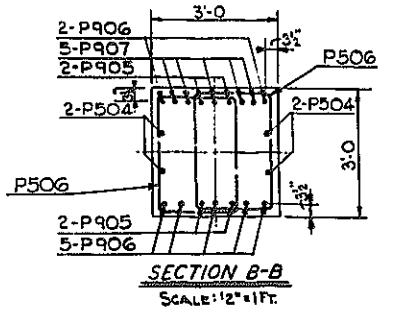
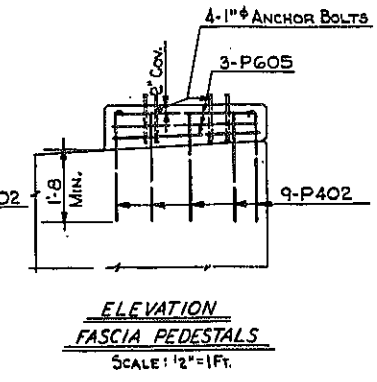
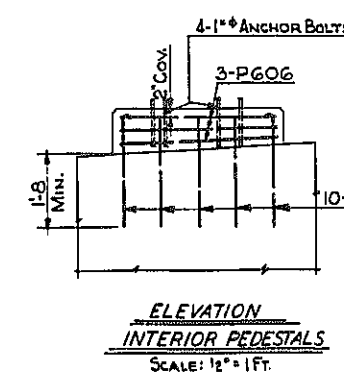
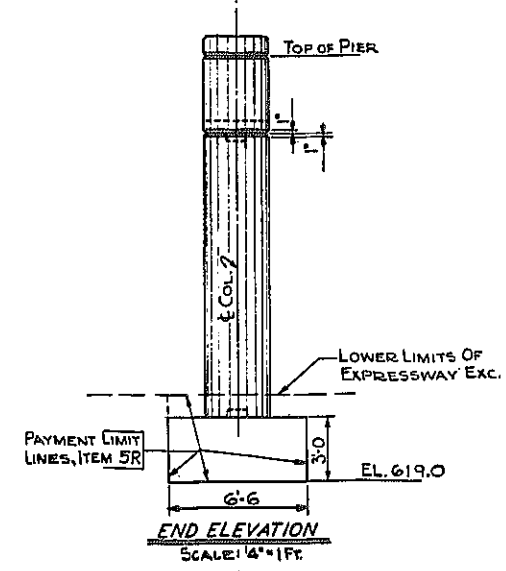
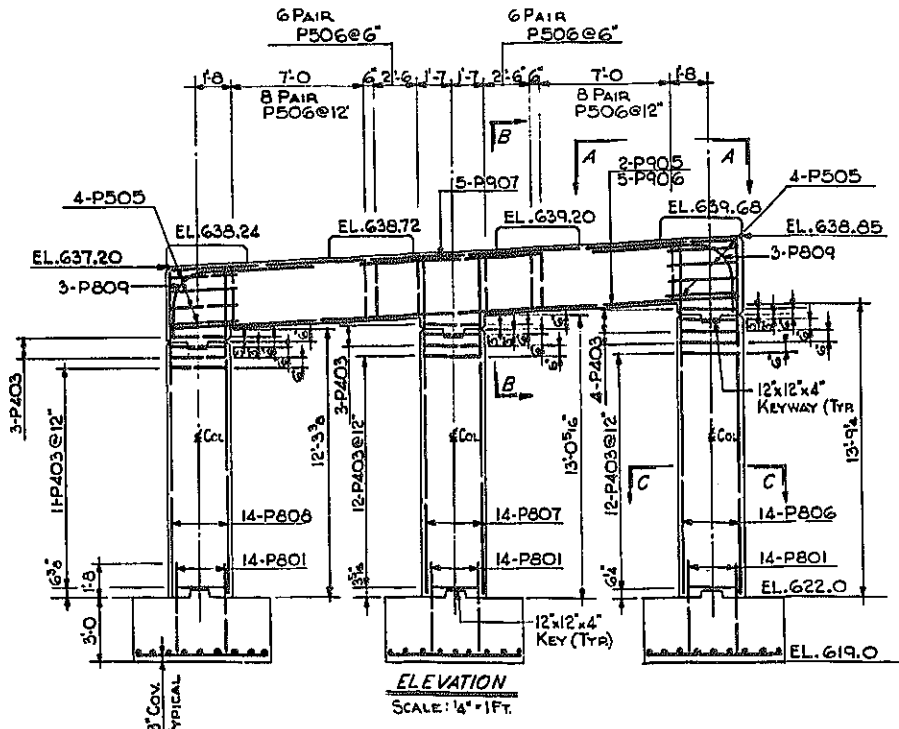
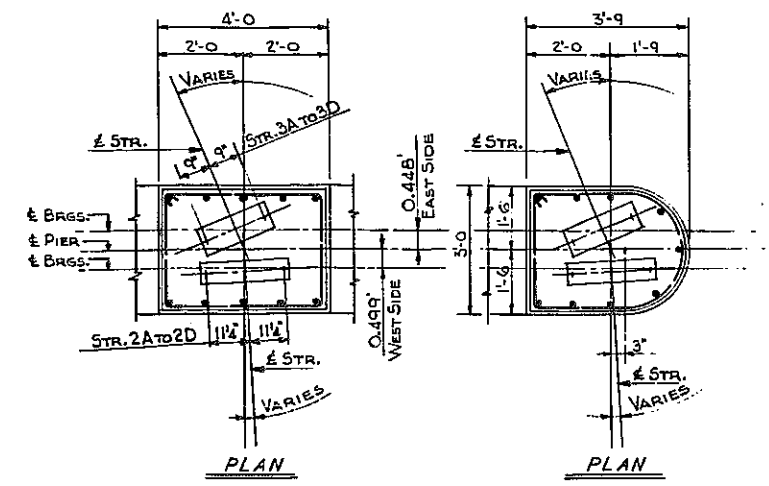
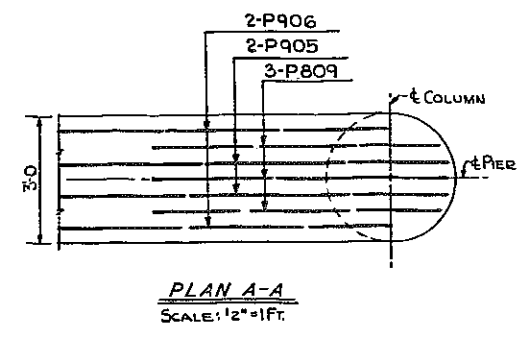
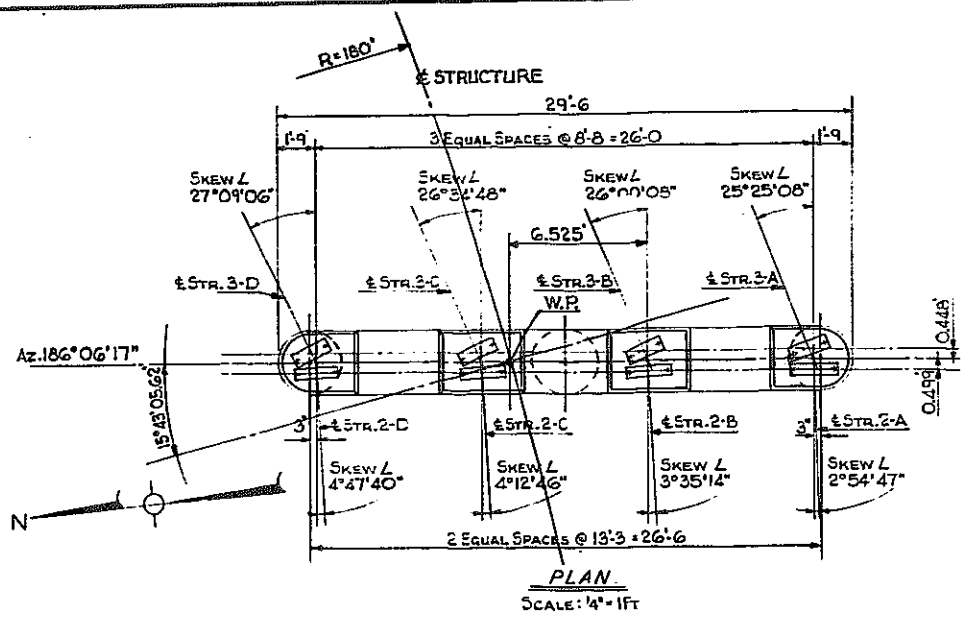
BR. SHEET NO 3



FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	99	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.
CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS I CONCRETE.
ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS AND SHALL BE ACCURATELY PLACED BY MEANS OF A TEMPLATE AND SET 10' INTO MASONRY.
SEE SHEET NO. 2 FOR ANCHOR BOLT LENGTHS.

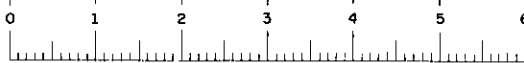
(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY PIER NO. 2

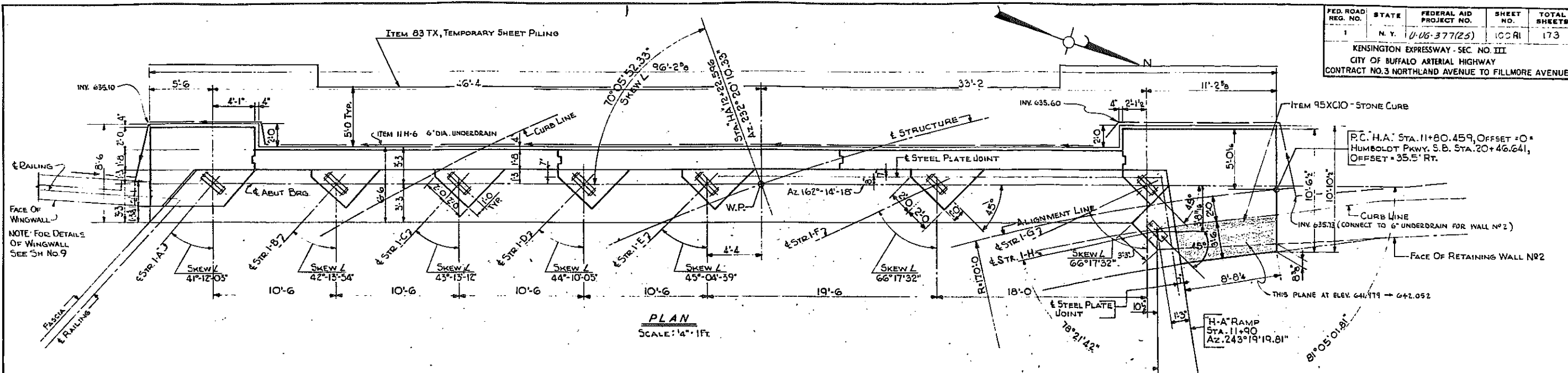
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHEN & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.B.
NEW YORK	TRACED	

BR. SHEET NO. 4



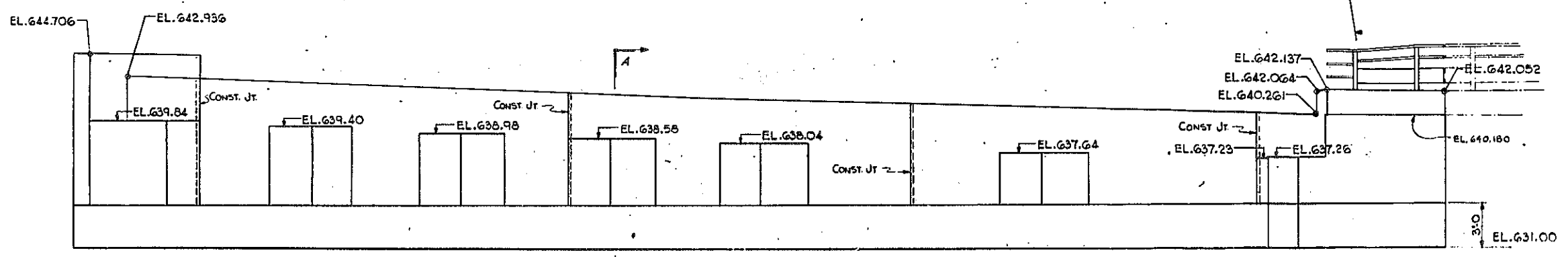
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	100 RI	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



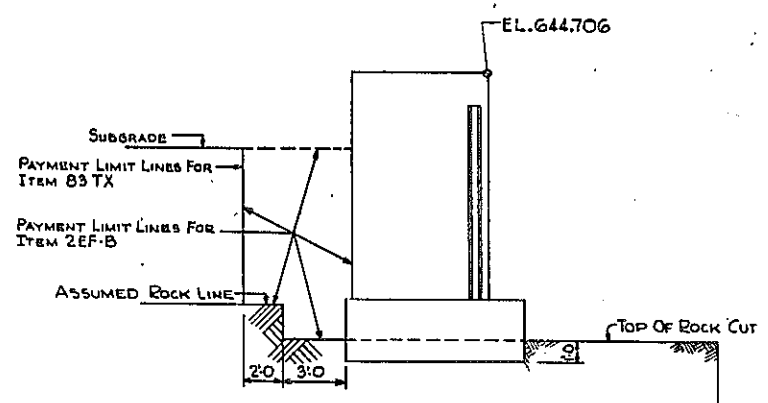
NOTE: FOR DETAILS OF WINGWALL SEE SH NO 9

P.C. H.A. STA. 11+80.459, OFFSET = 0'
HUMBOLDT PKWY, S.B. STA. 20+46.641,
OFFSET = 35.5' RT.

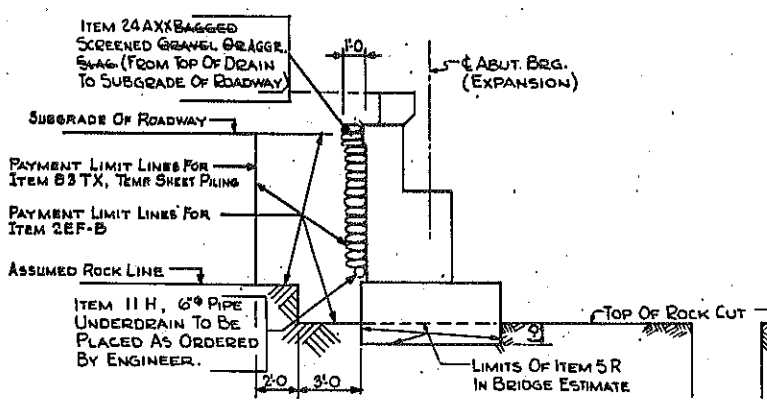
PLAN
SCALE: 1/4" = 1 FT



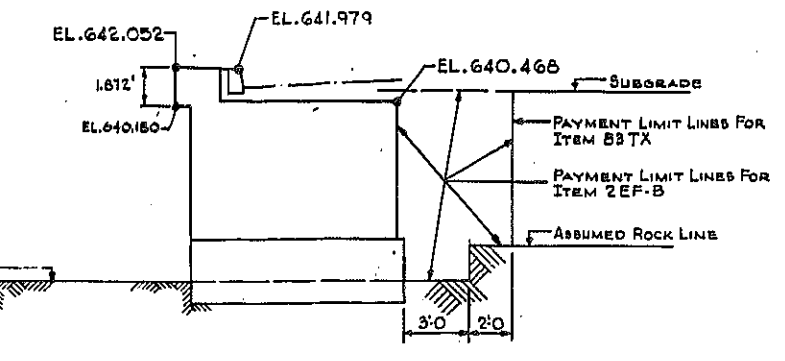
ELEVATION
SCALE: 1/4" = 1 FT



SOUTH END ELEVATION
SCALE: 1/4" = 1 FT
LOOKING NORTH



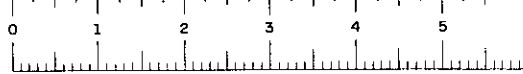
SECTION A-A
SCALE: 1/4" = 1 FT
LOOKING NORTH



NORTH END ELEVATION
SCALE: 1/4" = 1 FT
LOOKING SOUTH

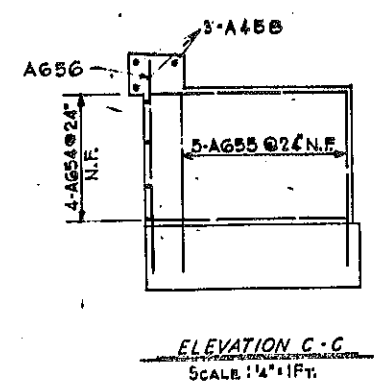
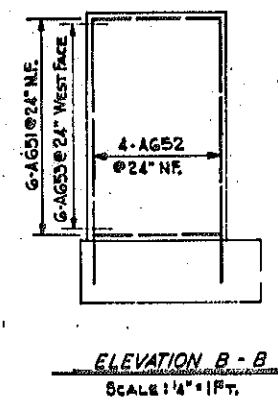
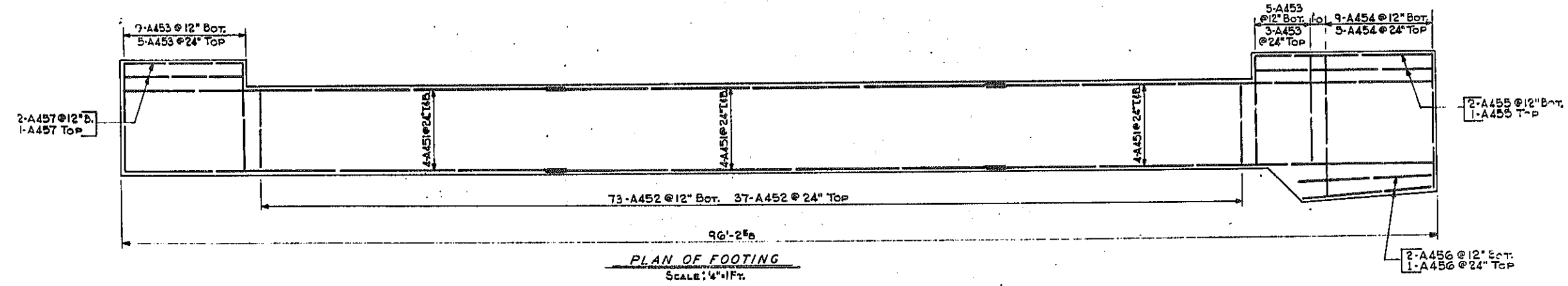
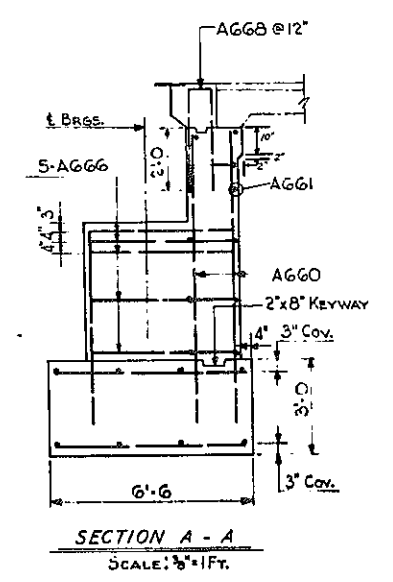
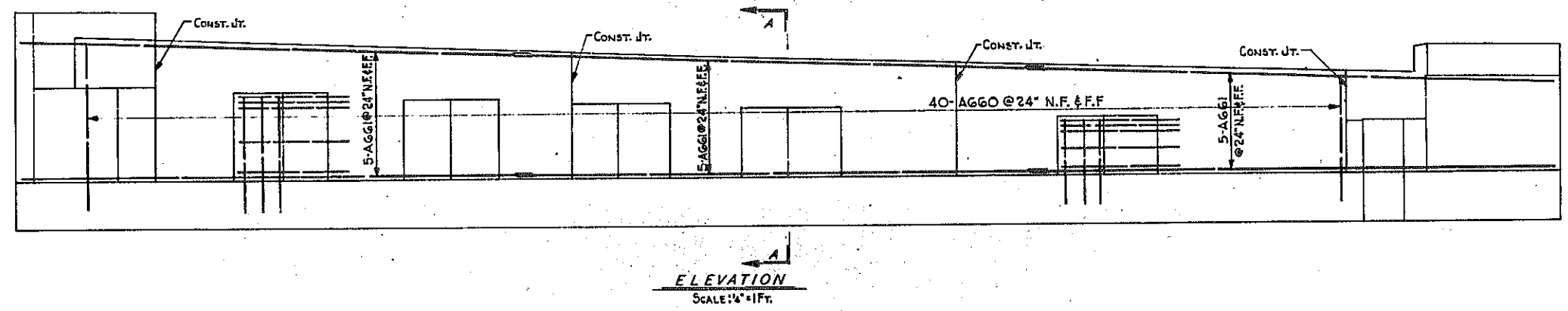
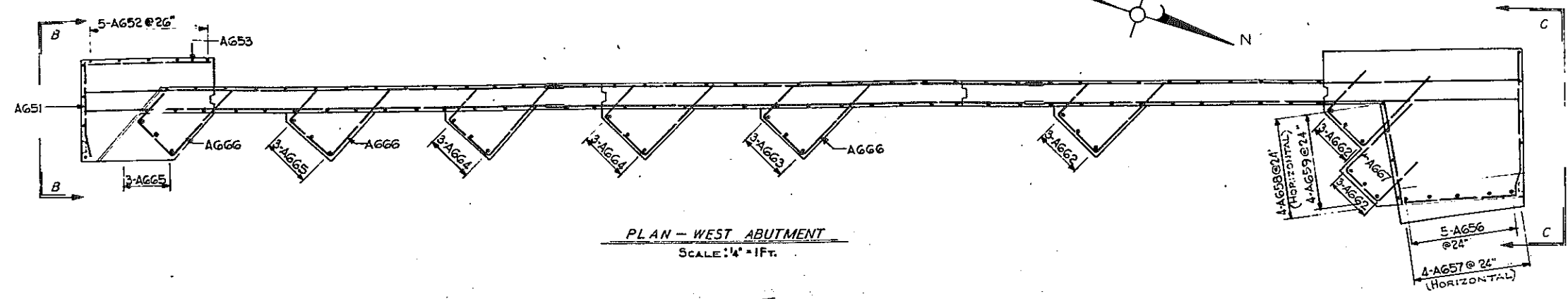
REVISION TO UNDERDRAIN (1)
(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY WEST ABUTMENT		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL ENGINEERS - ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	101	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



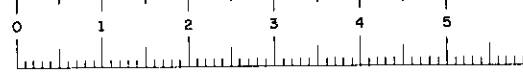
REVISION TO ABUT SECTION
(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY
WEST ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

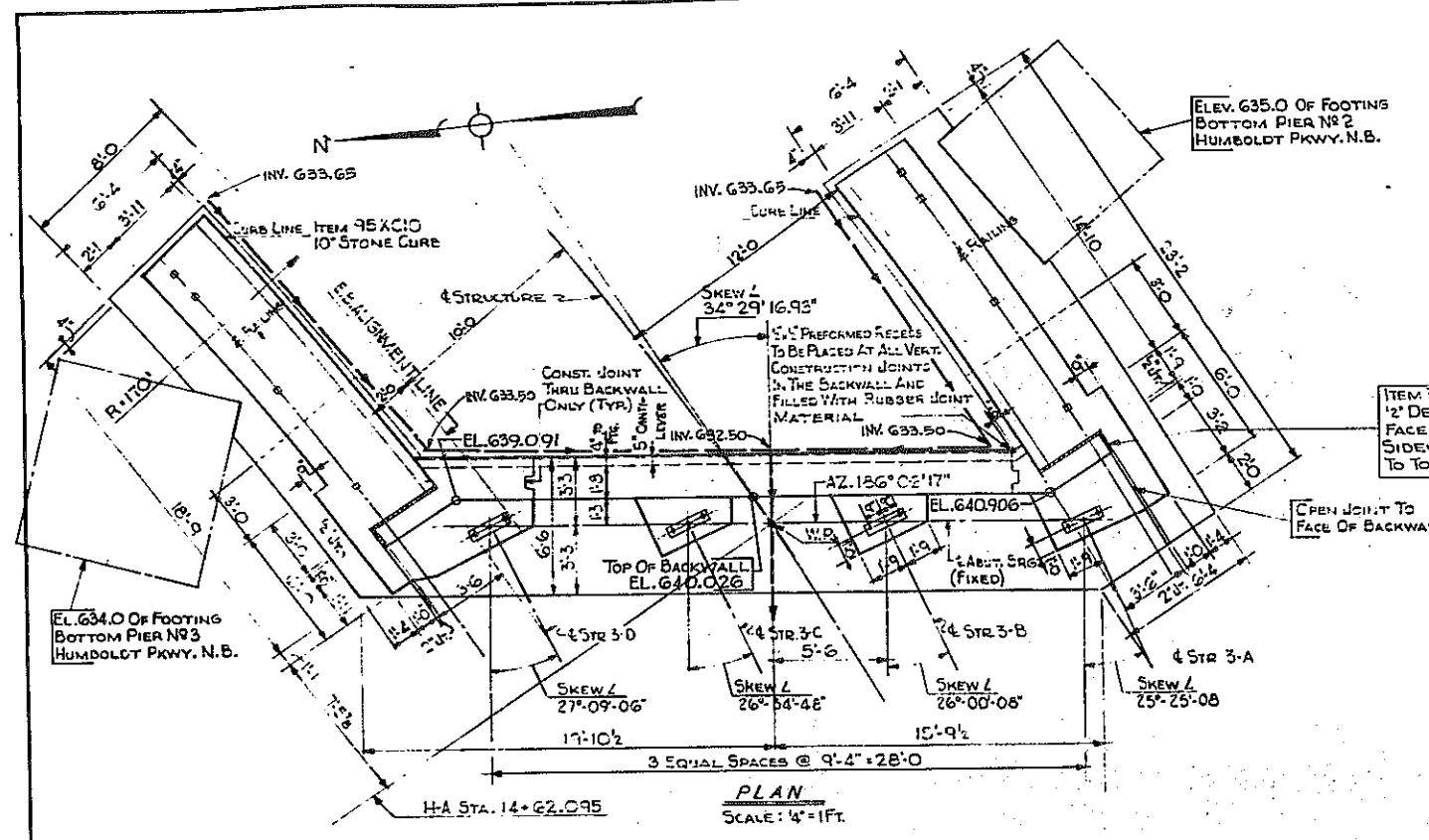
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.S.K. C.E.
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BR. SHEET NO. 6



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	102R1	173

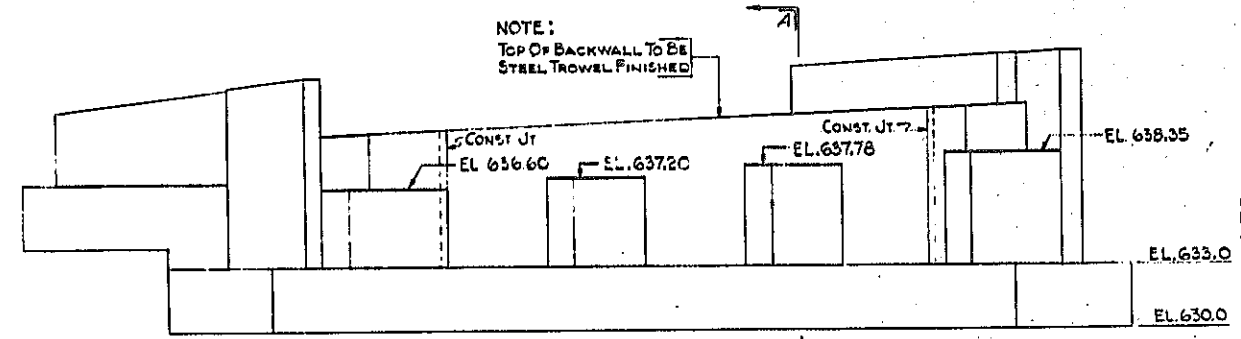
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



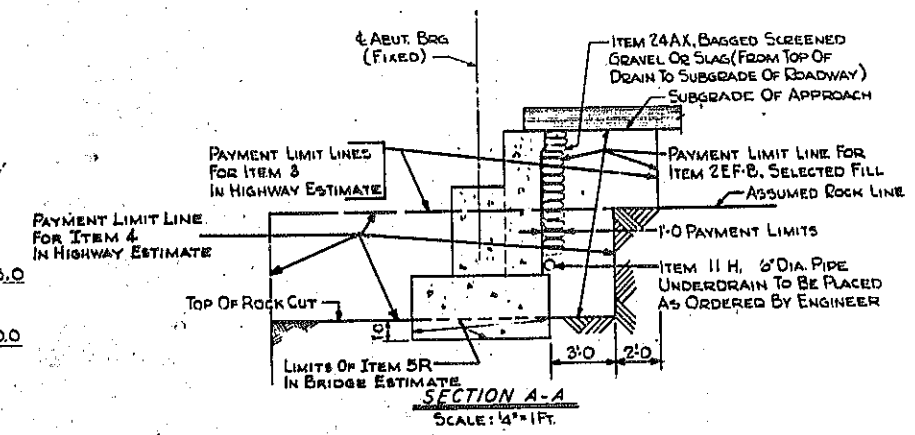
ITEM 352XA RUBBER JOINT MATERIAL 1/2\"/>

OPEN JOINT TO FACE OF BACKWALL

NOTE:
TOP OF BACKWALL TO BE STEEL TROWEL FINISHED

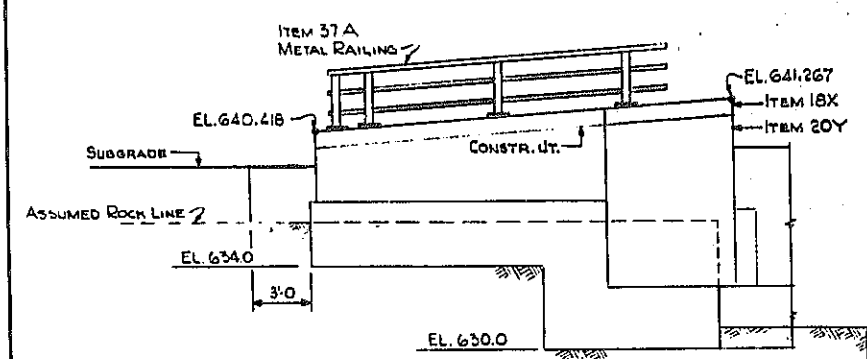


ELEVATION
(LOOKING EAST)
SCALE: 1/4\"/>

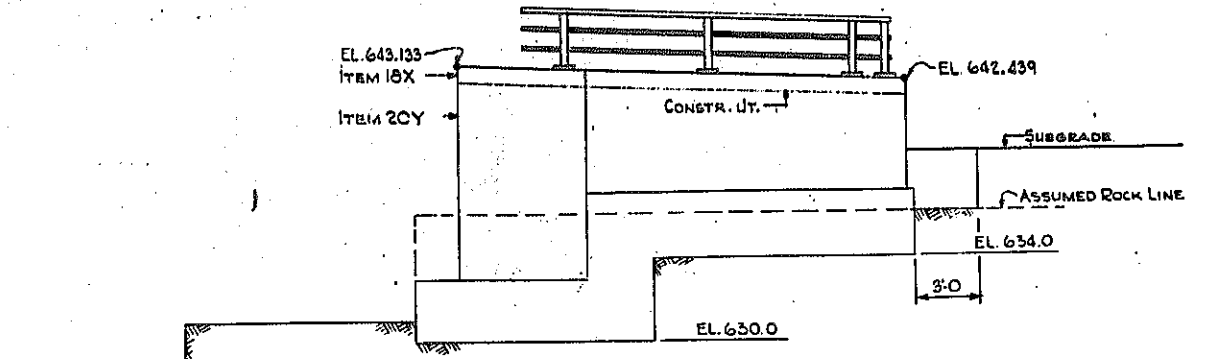


SECTION A-A
SCALE: 1/4\"/>

NOTES:
CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
CONCRETE IN WINGWALL FASCIA SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
FOR SECTIONS AND REINFORCING, SEE SHEET NO. B
FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 2.
BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.



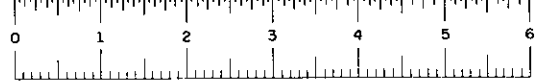
NORTH END ELEVATION
SCALE: 1/4\"/>



SOUTH END ELEVATION
SCALE: 1/4\"/>

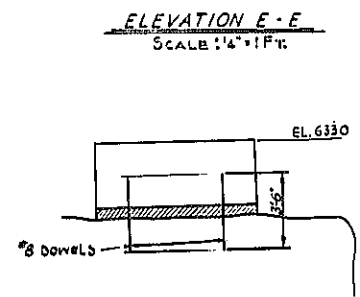
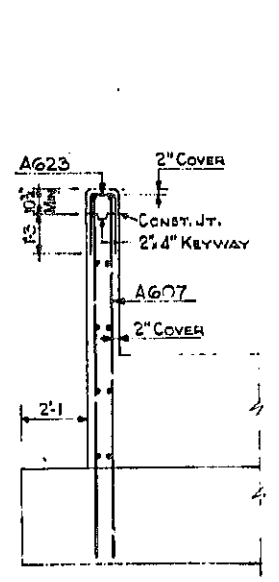
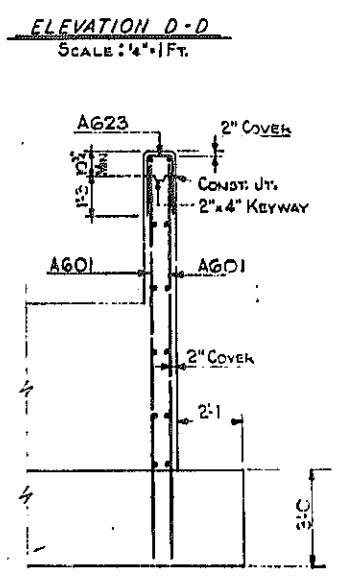
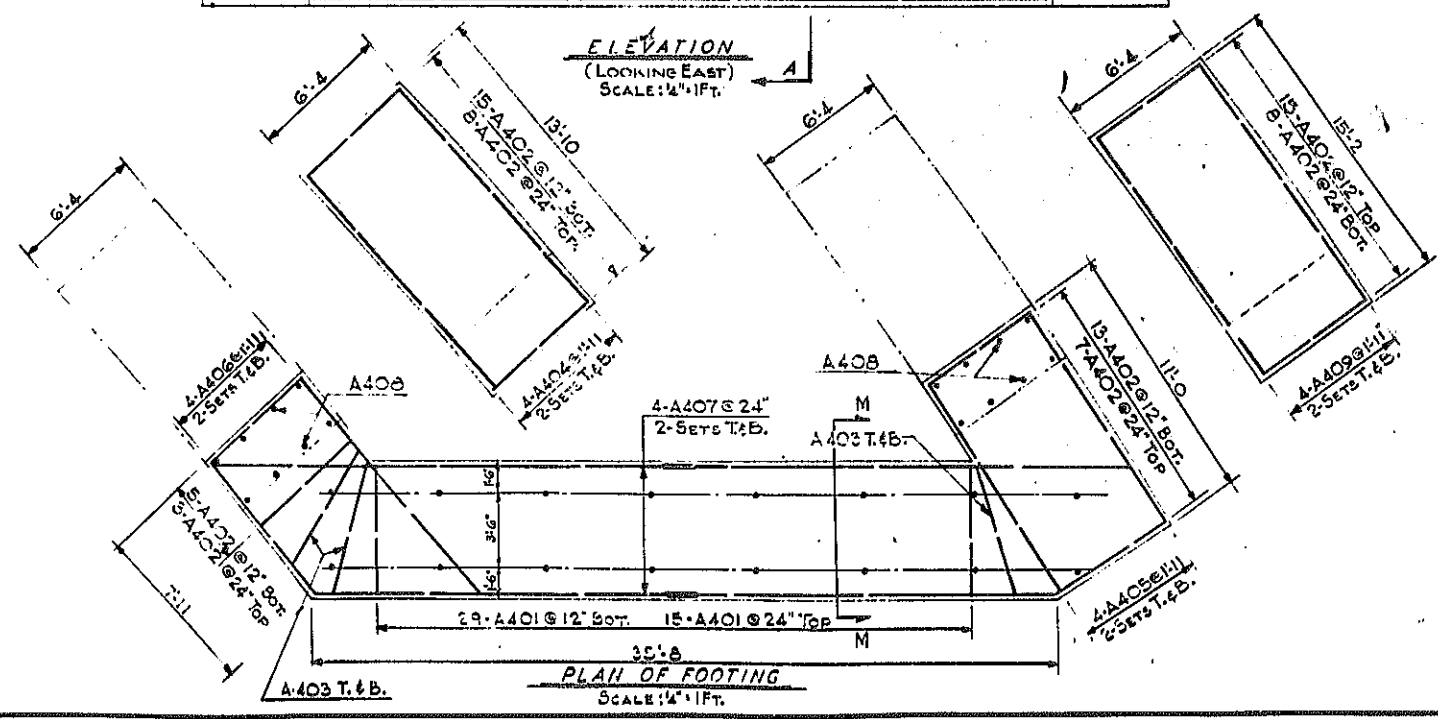
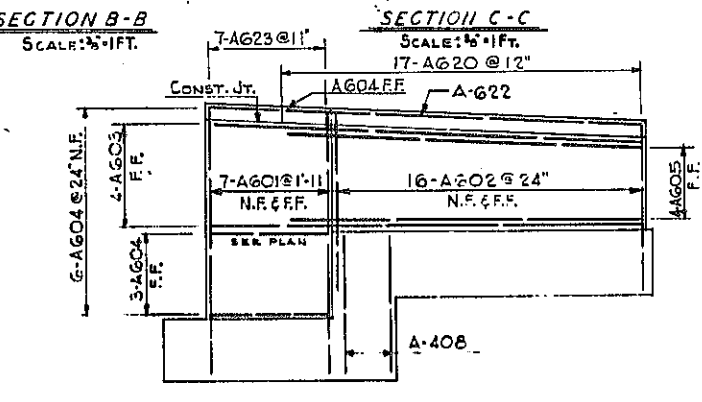
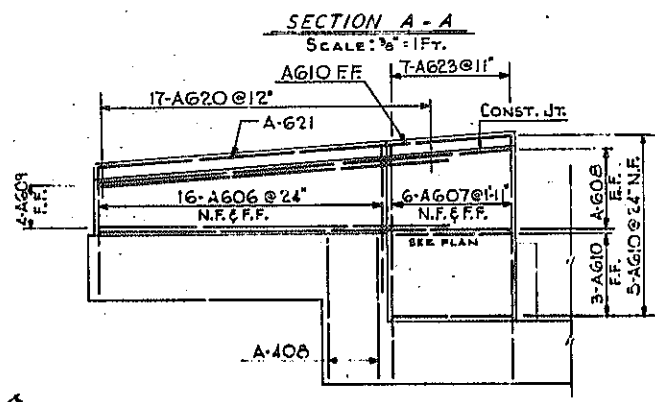
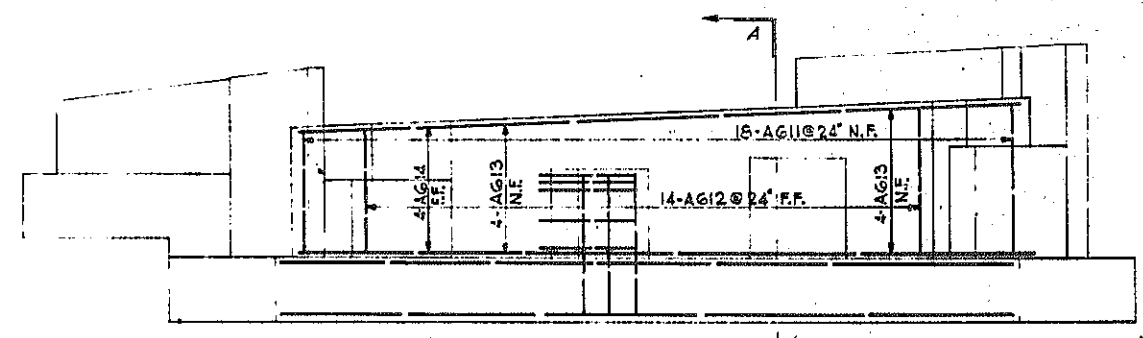
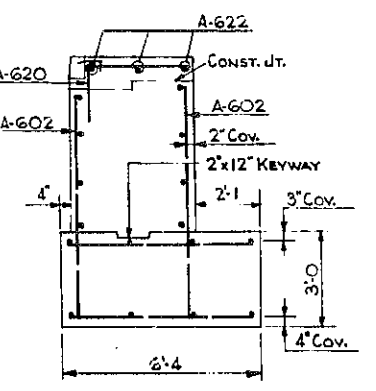
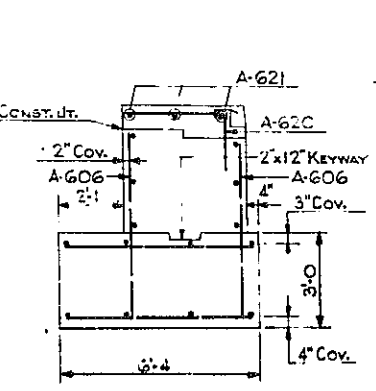
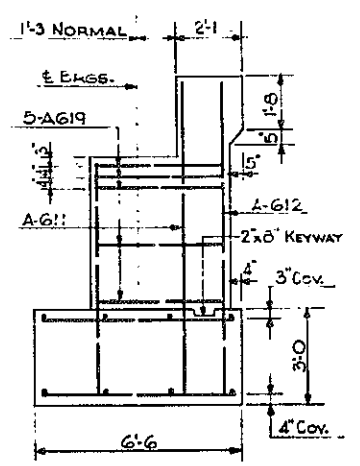
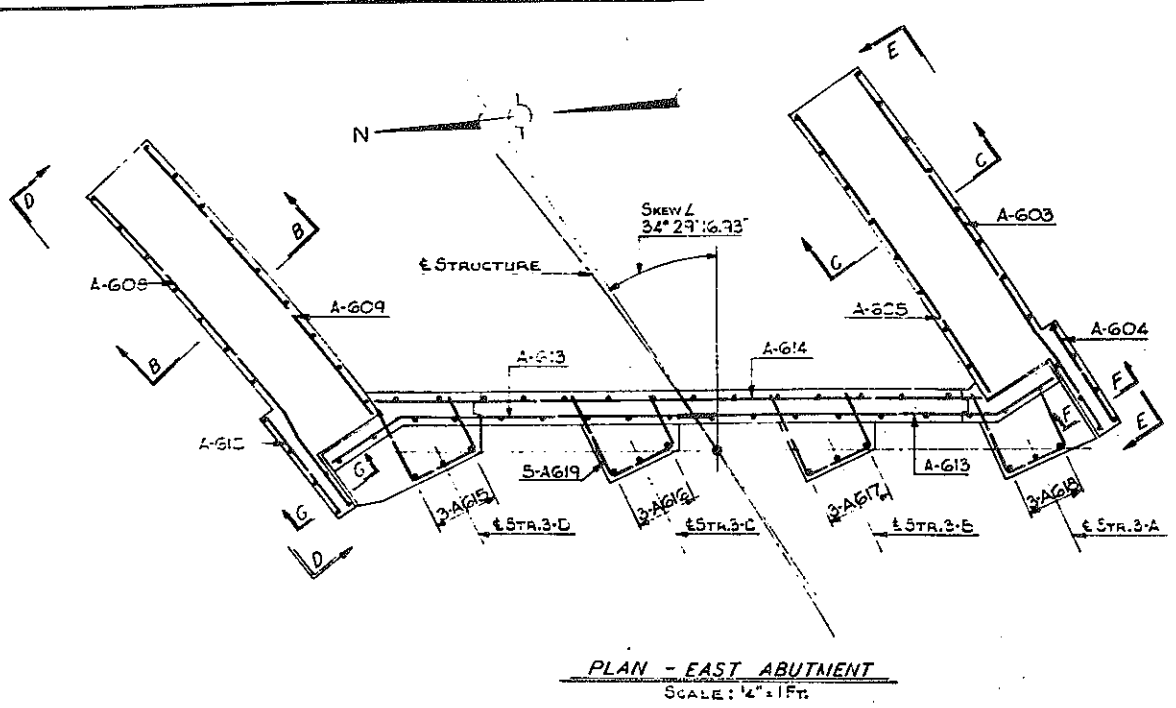
REVISION TO UNDERDRAIN
(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY EAST ABUTMENT	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED
R.B. C.E.	



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	103R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SECTION F-F
SCALE: 3/8" = 1 FT.

SECTION G-G
SCALE: 3/8" = 1 FT.

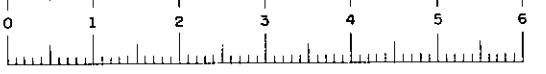
BR SHEET NO. 8

SEE LETTER TO CONTRACTOR DATED 7-3-64
REVISION TO FOOTING REINF.
(RAMP H-A)

**E.B. RAMP OVER EXPRESSWAY
EAST ABUTMENT DETAILS**

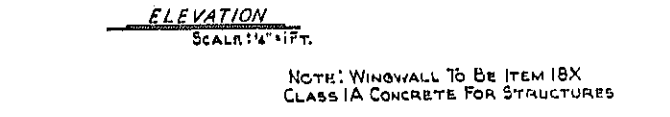
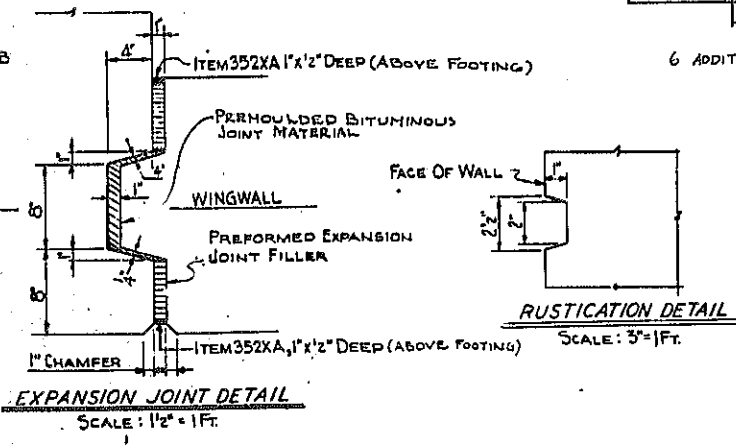
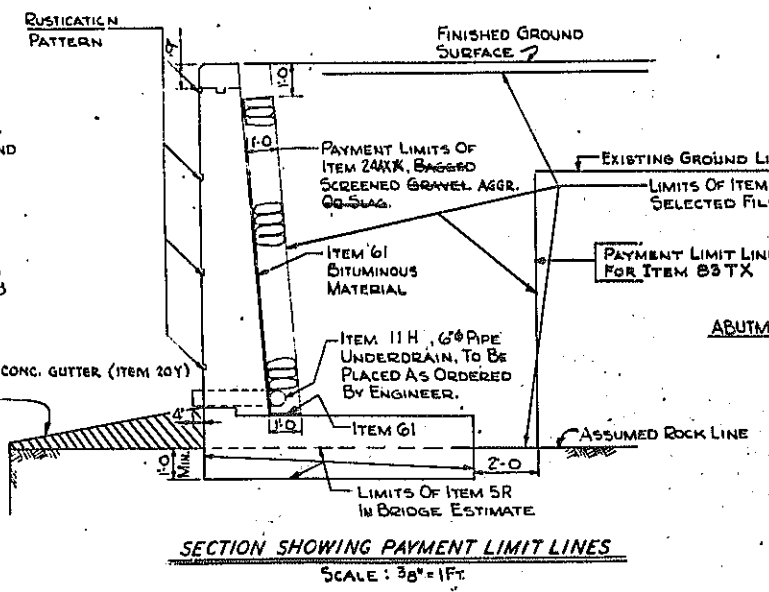
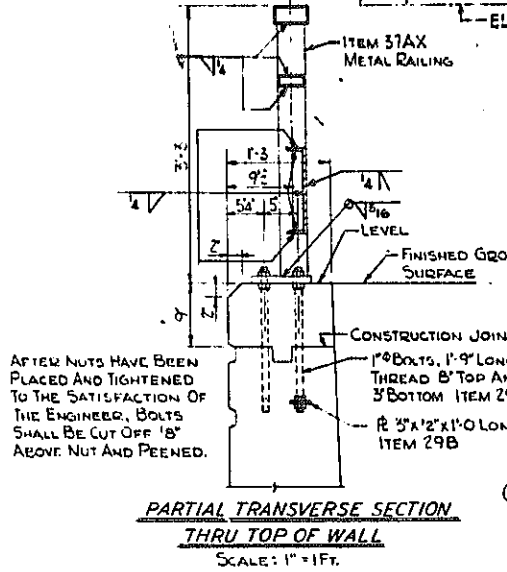
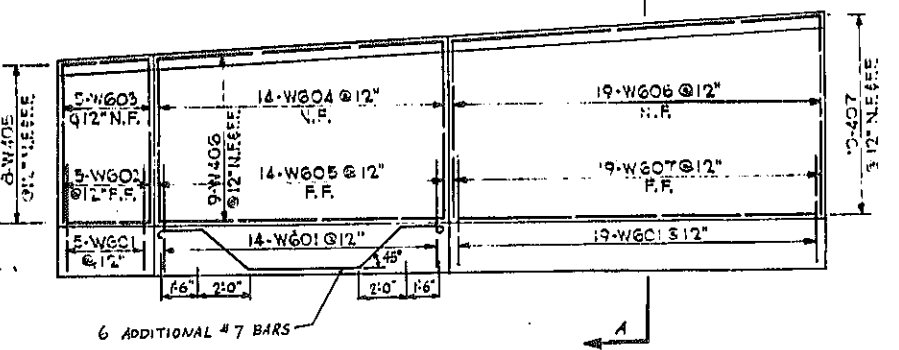
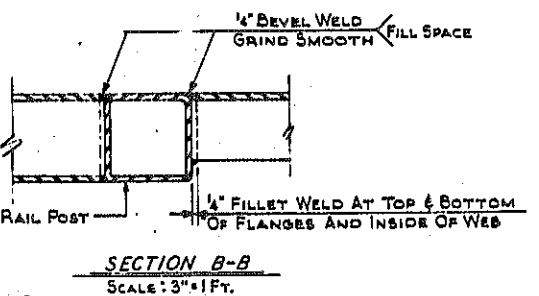
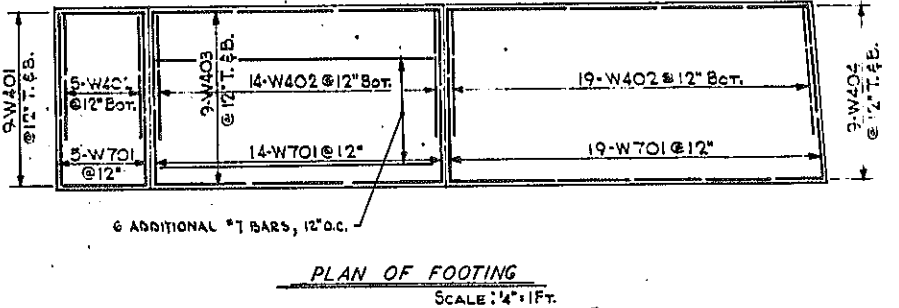
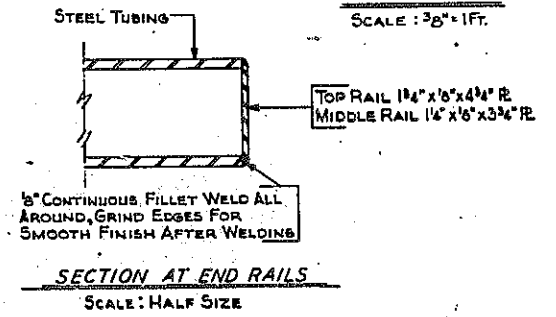
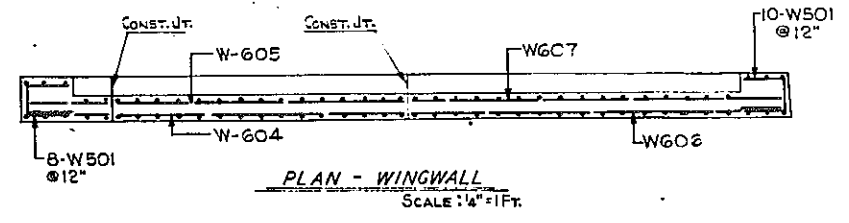
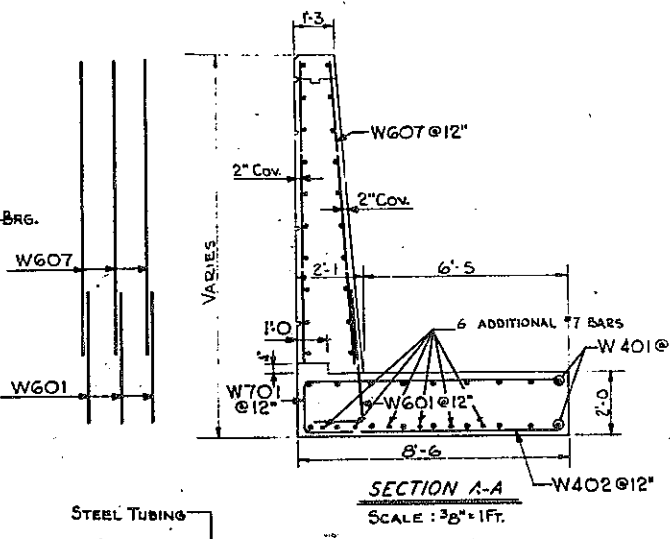
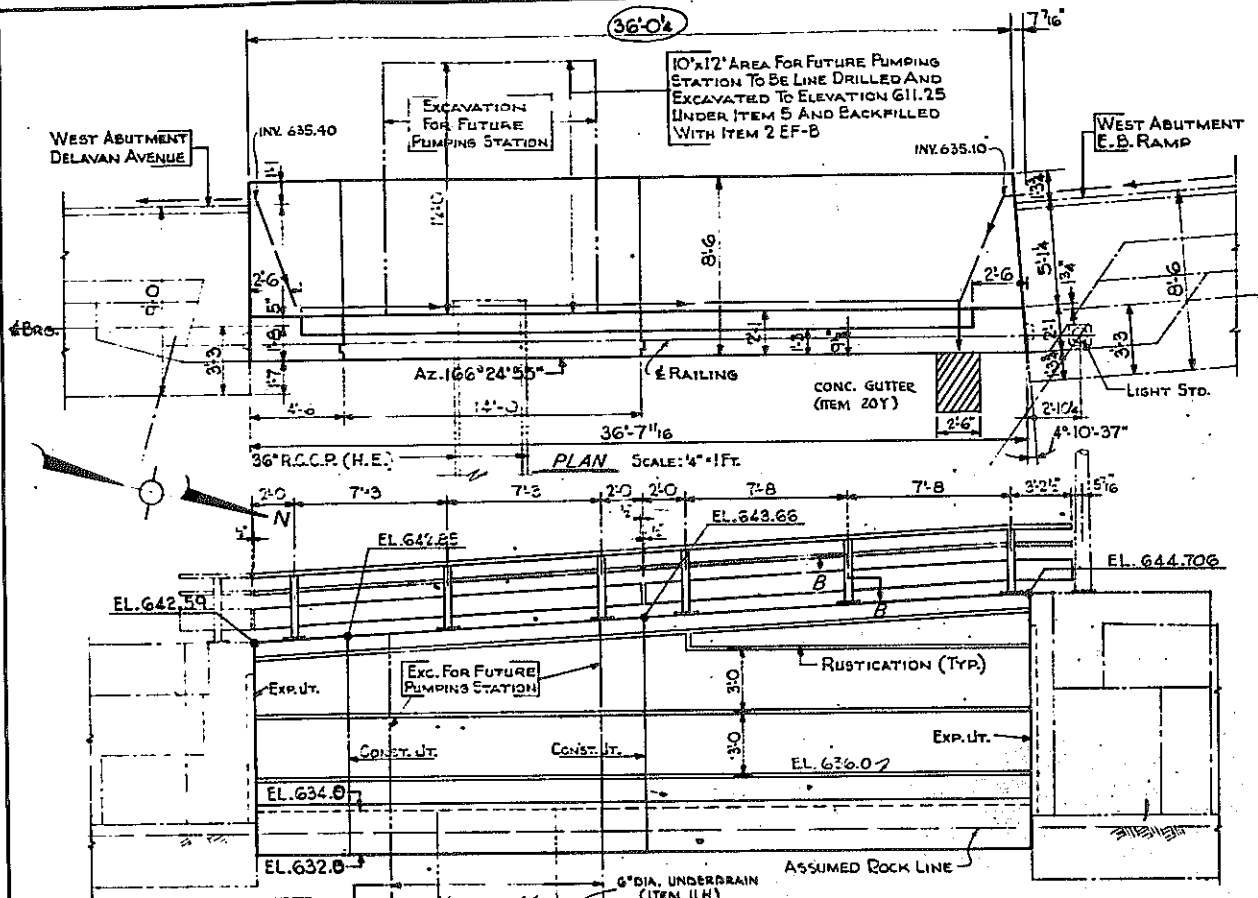
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	24-R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE:
 FOR FABRICATION AND ERECTION DETAILS
 AND NOTES FOR RAILING SEE SHEET NO. 14

NOTE: WINGWALL TO BE ITEM 18X
 CLASS IA CONCRETE FOR STRUCTURES

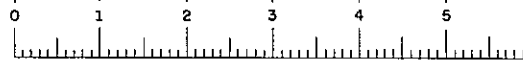
REVISION TO DETAIL UNDER DRAIN
 BARS-SECT A-A

NOTE: USE R.B.J.M. IN FOOTING.

RAMP H-A
 E B RAMP OVER EXPRESSWAY
 WINGWALL
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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BR. SHEET NO. 9



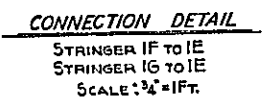
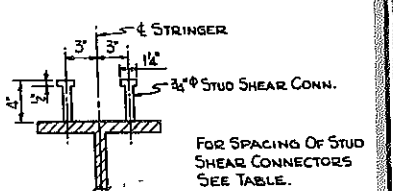
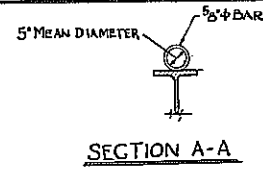
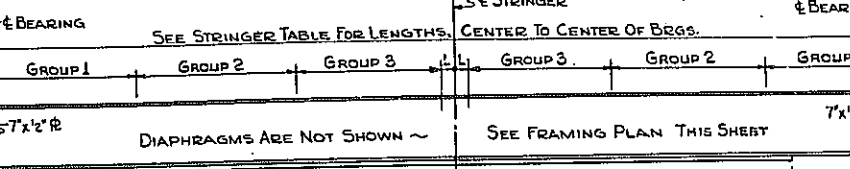
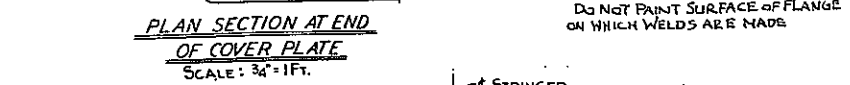
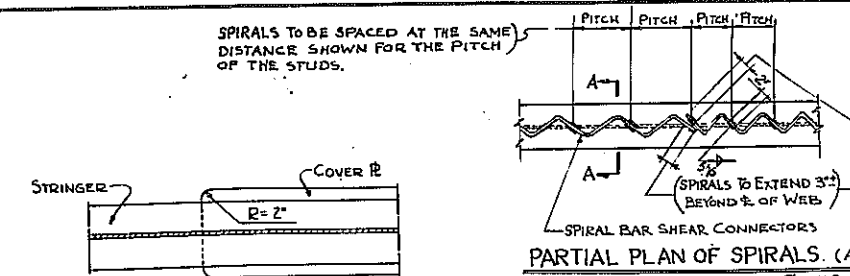
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	105	173

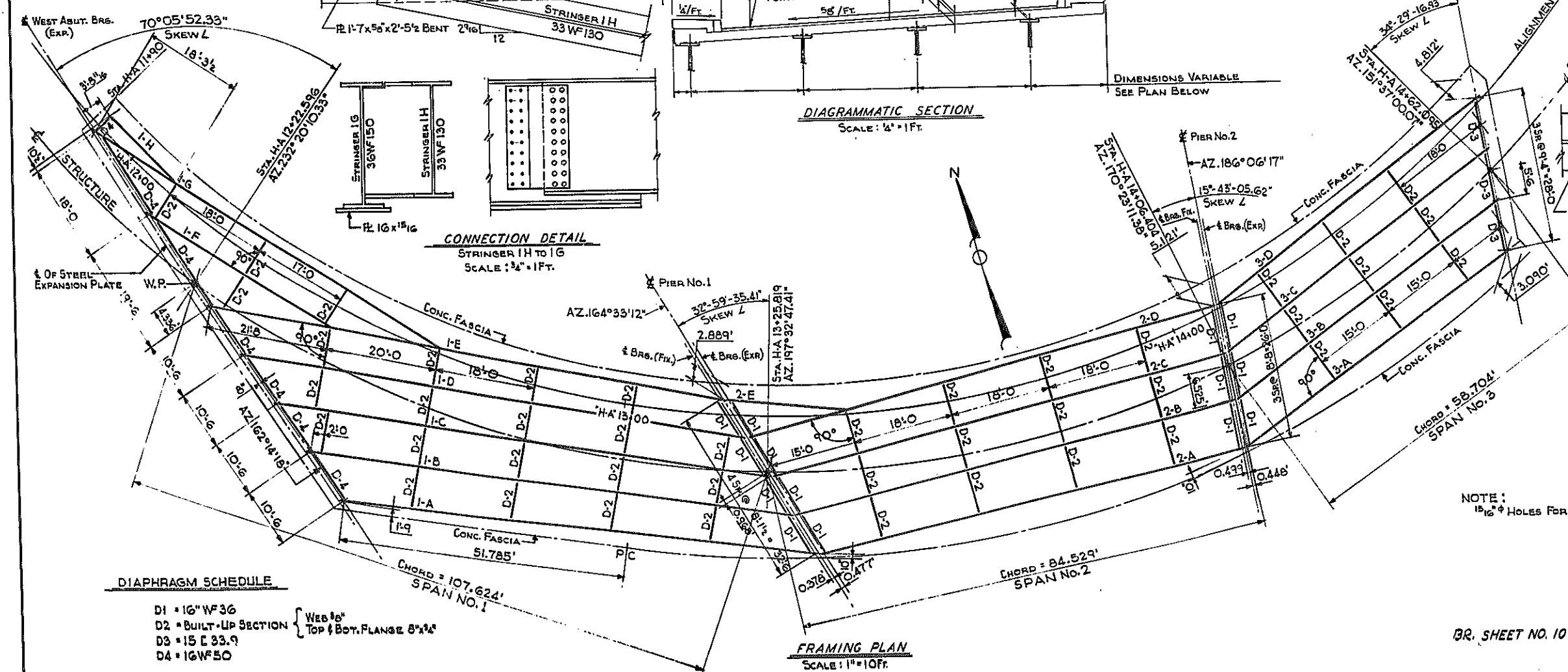
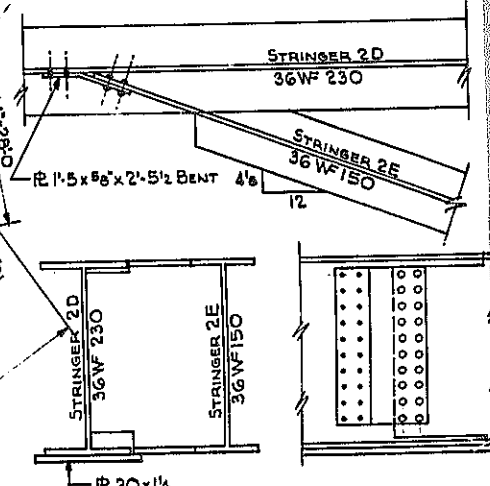
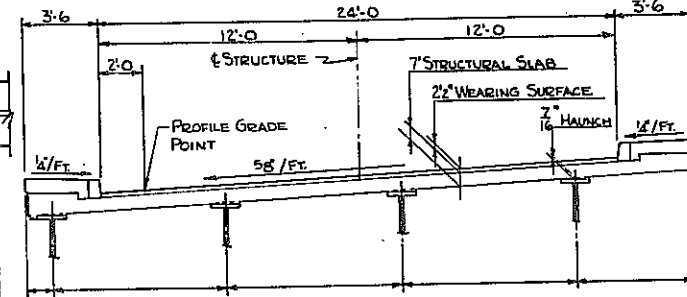
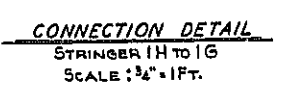
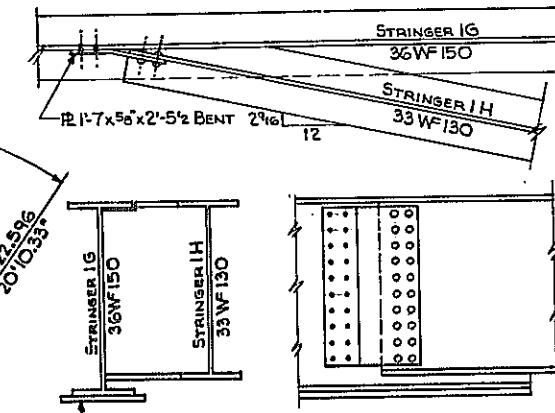
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

SPAN	STRINGER	BOTTOM COVER	CAMBER	STUD SHEAR CONNECTOR SPACING: 2 STUDS PER ROW	GROUP			L		
					GROUP 1	GROUP 2	GROUP 3			
1	1-A	36WF230	85.264	18x1	59'-0"	1 1/2"	29 5P@6'-14'-6"	18 5P@9'-13'-6"	14 5P@12'-14'-0"	.632
	1-B	36WF230	87.087	18x1	61'-0"	1 1/2"	29 5P@6'-14'-6"	18 5P@9'-13'-6"	15 5P@12'-15'-0"	.543
	1-C	36WF230	88.937	20x1	63'-0"	1 1/2"	31 5P@6'-15'-6"	18 5P@9'-13'-6"	15 5P@12'-15'-0"	.468
	1-D	36WF230	90.812	20x1 1/2	65'-0"	1 1/2"	30 5P@6'-15'-0"	20 5P@9'-15'-0"	15 5P@12'-15'-0"	.406
	1-E	36WF300	92.711	20x1 1/2	67'-0"	1 1/2"	36 5P@5'-15'-0"	24 5P@7'-15'-0"	16 5P@12'-16'-0"	.355
	1-F	36WF150	88.053	NONE	NONE	-15"	21 5P@6'-10'-6"	8 5P@12'-8'-0"	12 5P@12'-12'-0"	.526
2	2-A	36WF230	77.243	20x1	54'-0"	1 1/2"	30 5P@5'-12'-6"	21 5P@7'-13'-1 1/2"	15 5P@10'-12'-6"	.496
	2-B	36WF230	80.287	20x1	57'-0"	1 1/2"	33 5P@5'-13'-9"	20 5P@7'-13'-9"	16 5P@10'-13'-4"	.560
	2-C	36WF230	83.342	20x1 1/2	60'-0"	2"	32 5P@5'-13'-4"	22 5P@7'-13'-1 1/2"	17 5P@10'-14'-2"	.421
	2-D	36WF230	86.406	20x1 1/2	63'-0"	2"	35 5P@5'-14'-7"	21 5P@7'-13'-1 1/2"	18 5P@10'-15'-0"	.495
	2-E	36WF150	82.699	NONE	NONE	0"	11 5P@7'-10'-1 1/2"	5 5P@10'-4'-2"	NONE	.308
	2-F	36WF150	82.699	NONE	NONE	0"	23 5P@5'-9'-7"	16 5P@7'-10'-0"	11 5P@10'-9'-2"	.430
3	3-A	33WF130	58.361	16x7 1/2	44'-0"	7"	26 5P@4'-9'-9"	17 5P@7'-9'-11"	11 5P@10'-9'-2"	.490
	3-B	33WF130	58.647	16x7 1/2	44'-0"	7"	26 5P@4'-9'-9"	17 5P@7'-9'-11"	11 5P@10'-9'-2"	.637
	3-C	33WF130	58.940	16x7 1/2	44'-0"	15 1/2"	26 5P@4'-9'-9"	17 5P@7'-9'-11"	11 5P@10'-9'-2"	.637
	3-D	33WF130	59.239	16x7 1/2	44'-0"	1"	24 5P@5'-10'-0"	16 5P@7'-10'-0"	11 5P@10'-9'-2"	.453

NOTE: CAMBER OF BEAM SHALL BE MEASURED WITH BEAM LYING ON ITS SIDE.



* STRINGER 1E HAS ALSO TOP COVER PLATE 15"x14"x48" O.L.



D1	16" WF 36	Web 8" Top & Bot. Flange 8"x3"
D2	BUILT-UP SECTION	
D3	15" C 33.9	
D4	16" WF 50	

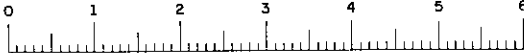
NOTE: 1 1/2" HOLES FOR 7/8" RIVETS (TYP) (RAMP H-A)

E.B. RAMP OVER EXPRESSWAY FRAMING PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

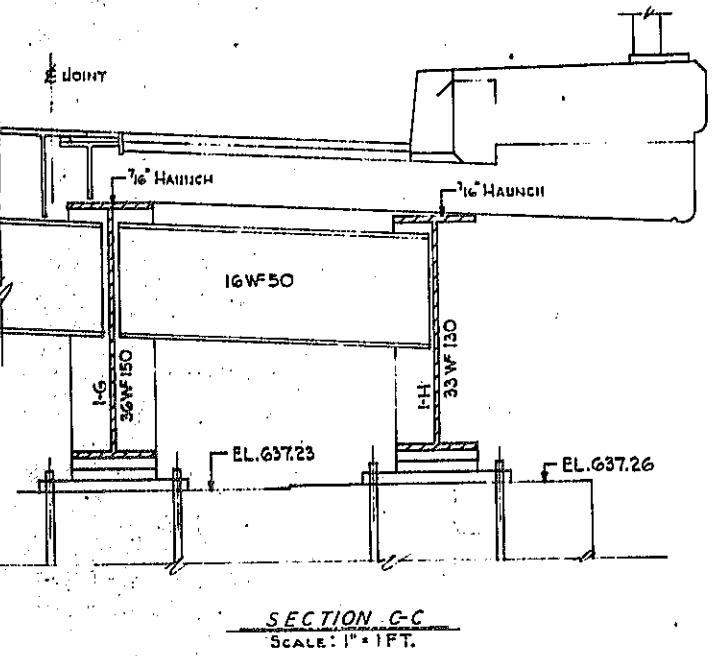
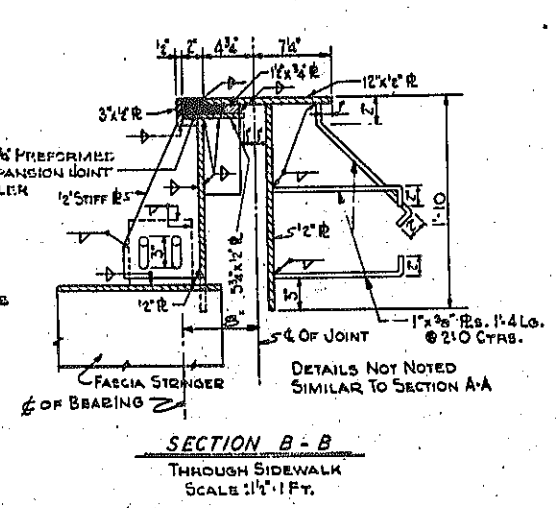
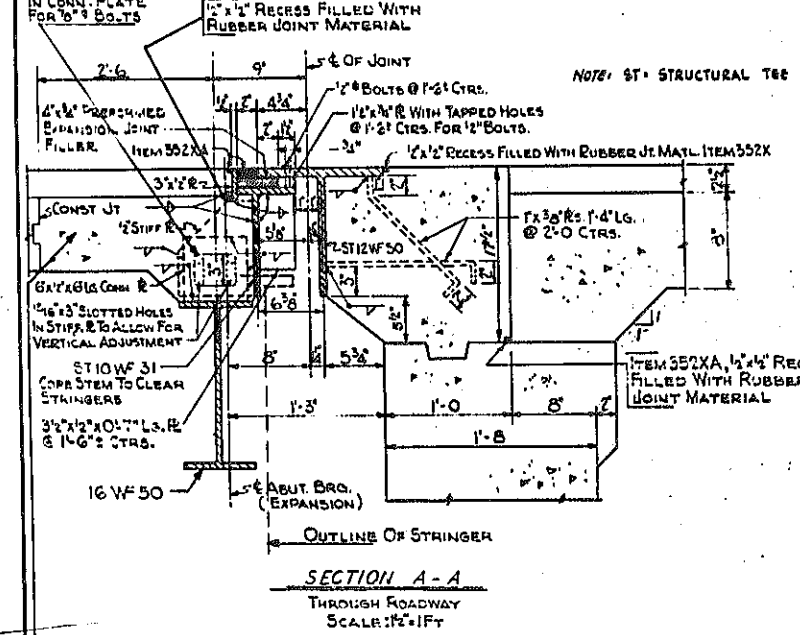
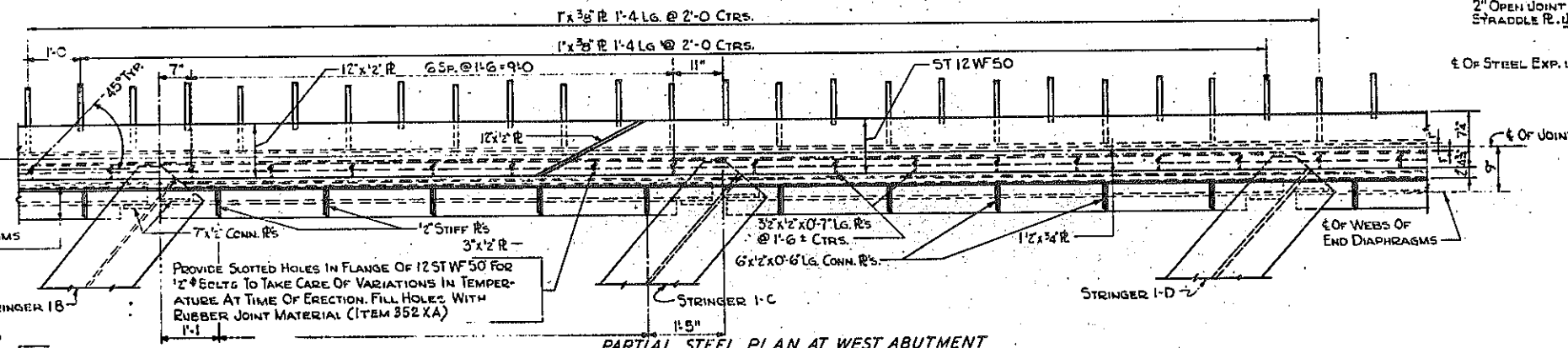
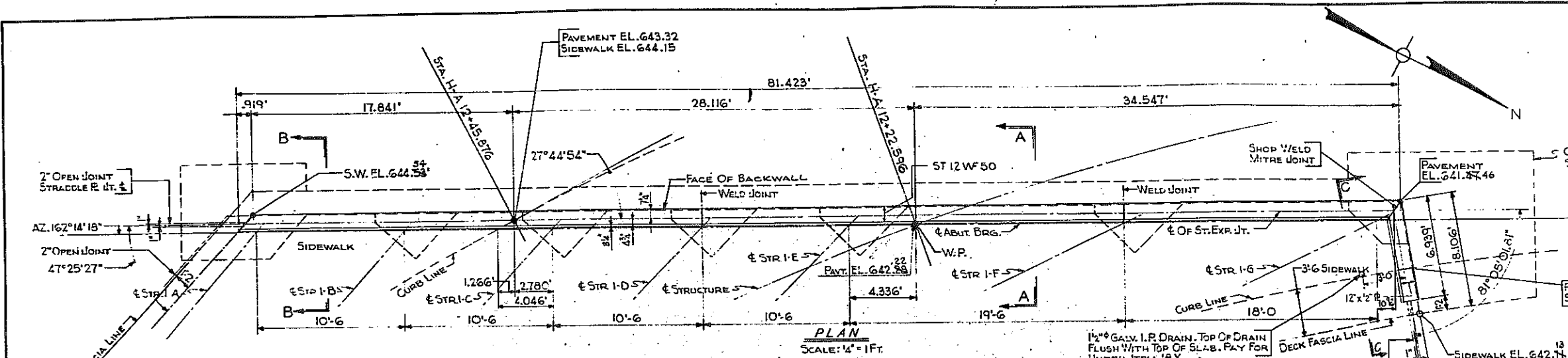
BR. SHEET NO. 10

DE LEUW, CATHER & BRILL	DRAWN	R.E.K.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	106R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
ALL FILLET WELDS ARE 5/16"

TO PREVENT BOND OF JOINT SEALING MATERIAL AT BOTTOM SURFACES, WAX PAPER, "BEAR TAPE" # 131-A1445 LB MANUFACTURED BY BEHR MANNING CO., TROY, N.Y. OR ANY APPROVED EQUAL SHALL BE APPLIED.

FIELD REVISIONS:
REFER TO LETTER DATED APRIL 29, 1964.

REVISED AS SHOWN

(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY STEEL EXPANSION JOINT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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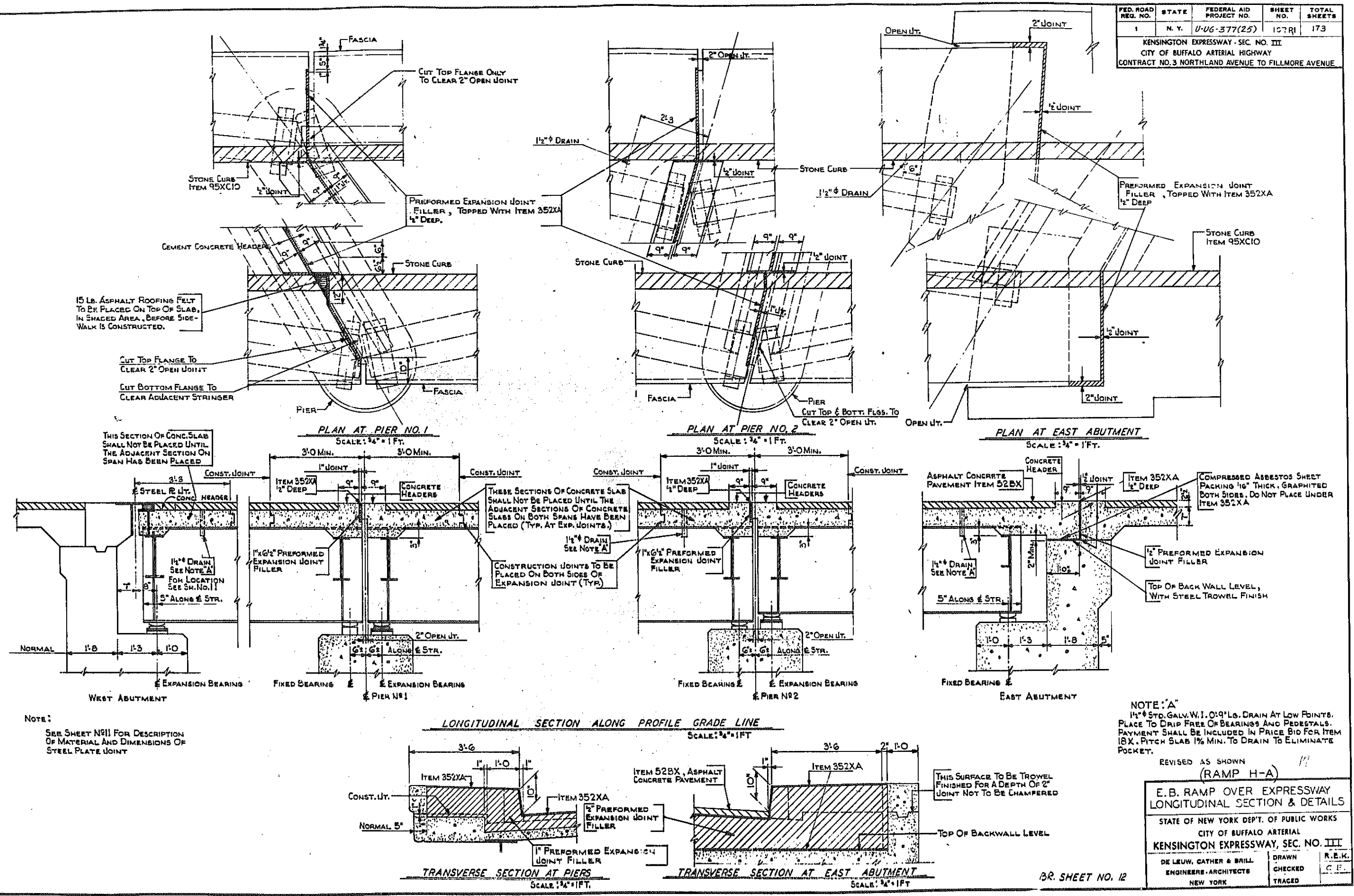
BR. SHEET NO. 11

THIS SECTION OF CONCRETE SLAB TO BE FORMED AFTER ADJACENT SECTIONS HAVE BEEN PLACED, AND THE JOINTS HAVE BEEN ERECTED TO THEIR CORRECT POSITIONS, BASED ON THE FINAL DETERMINATION OF ROADWAY GRADES.



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	107R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE:
 SEE SHEET NG11 FOR DESCRIPTION OF MATERIAL AND DIMENSIONS OF STEEL PLATE JOINT

NOTE 'A'
 1 1/2" STD. GALV. W. I. O. 9" LG. DRAIN AT LOW POINTS. PLACE TO DRIP FREE OF BEARINGS AND PEDESTALS. PAYMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM 18X. PITCH SLAB 1% MIN. TO DRAIN TO ELIMINATE POCKET.

REVISED AS SHOWN
 (RAMP H-A)

**E. B. RAMP OVER EXPRESSWAY
 LONGITUDINAL SECTION & DETAILS**

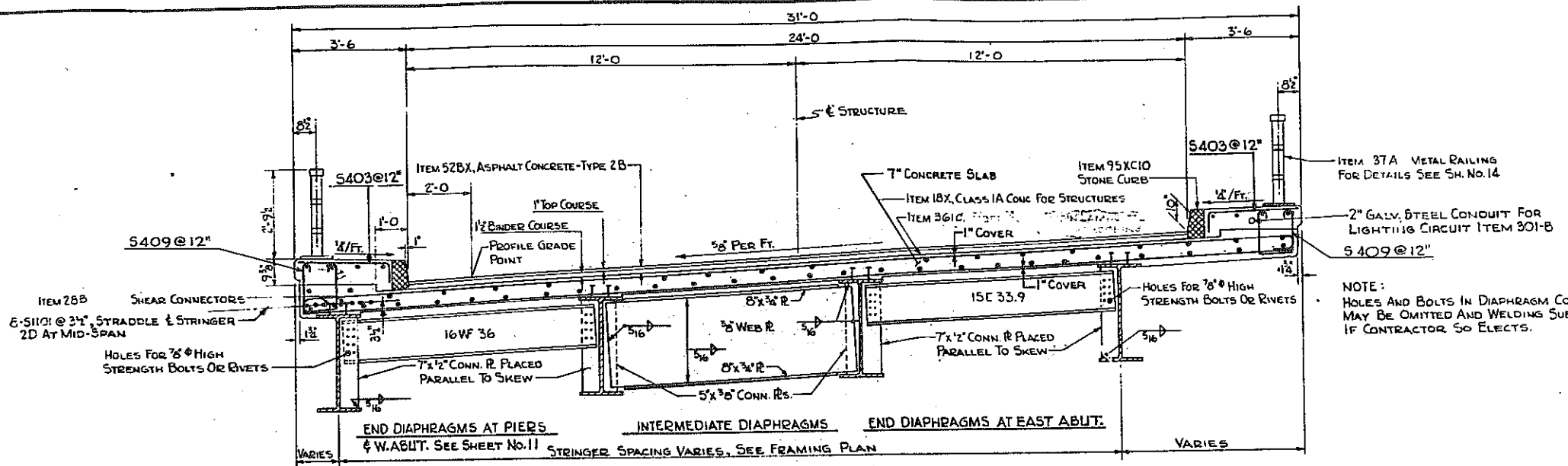
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R. E. K. C. F.
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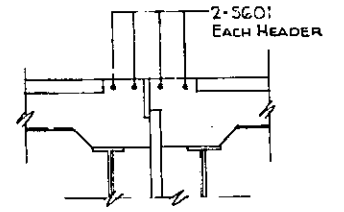


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	108 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

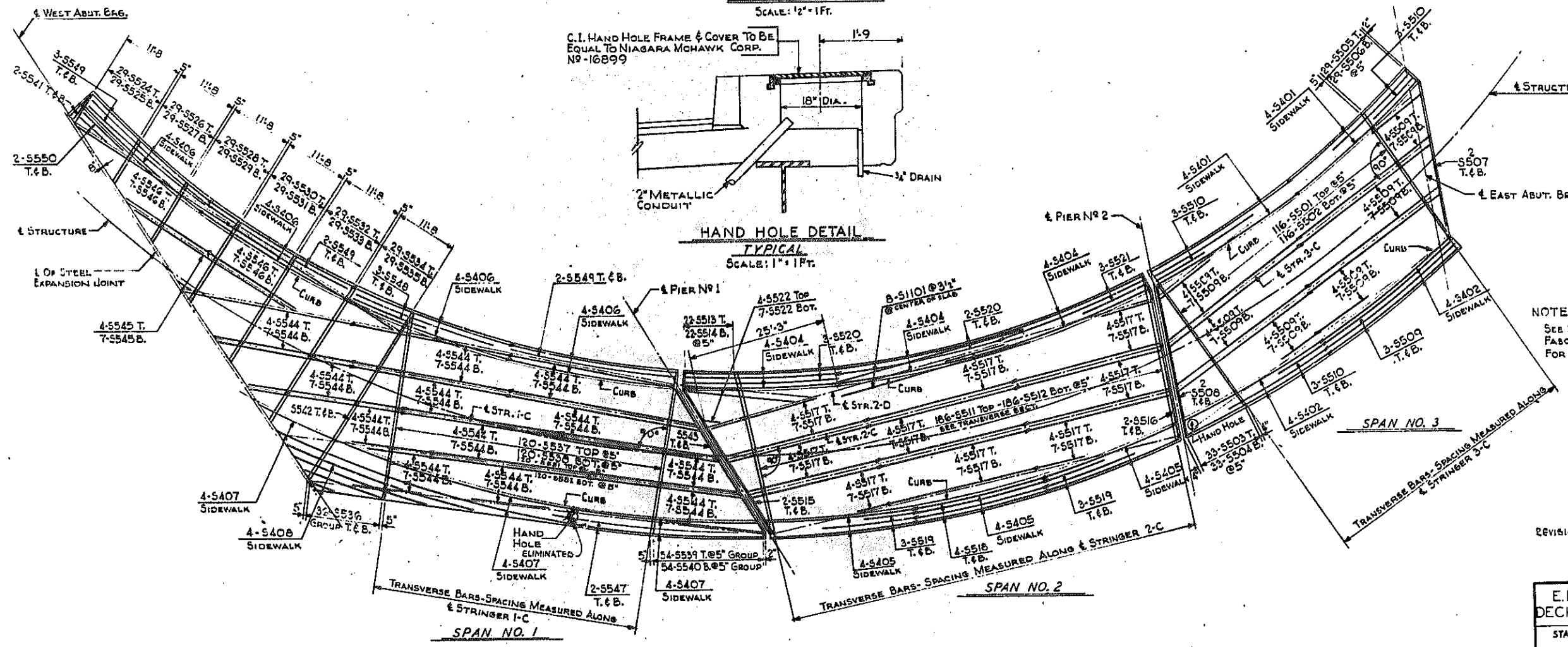


NOTE:
HOLES AND BOLTS IN DIAPHRAGM CONNECTIONS MAY BE OMITTED AND WELDING SUBSTITUTED IF CONTRACTOR SO ELECTS.



TRANSVERSE SECTION
SCALE: 1/2" = 1 FT.

CONCRETE HEADER
SCALE: 3/4" = 1 FT.



NOTE:
SEE SHEET NO. 10 FOR DIMENSIONS AT FASCIA, ROADWAY LINES AND STRINGER, FOR SPANS 1, 2 & 3.

DECK PLAN
SCALE: 1" = 10 FT.

REVISION TO HANDHOLE

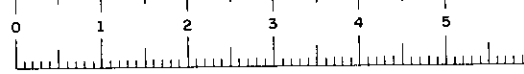
(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY DECK PLAN LAYOUT AND SECTION

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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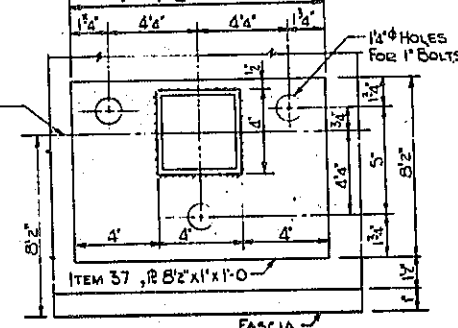
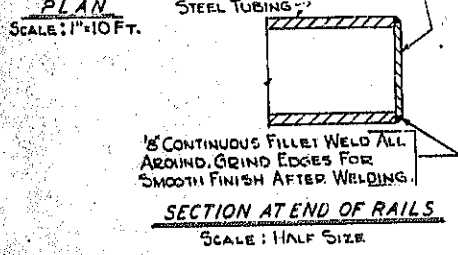
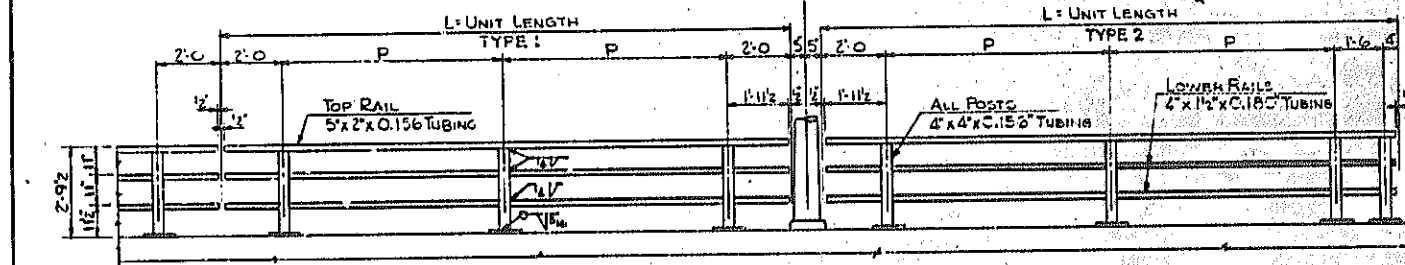
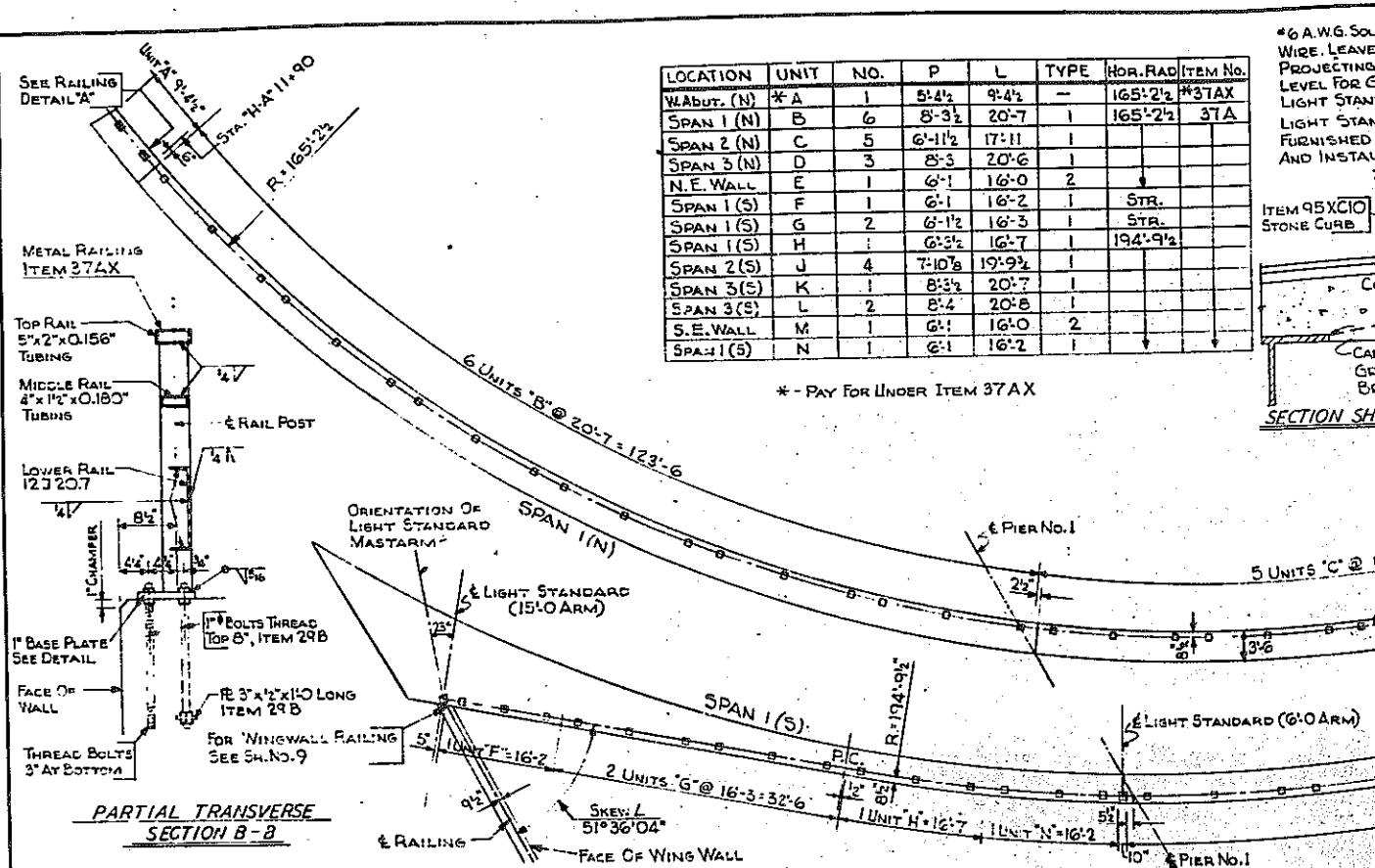
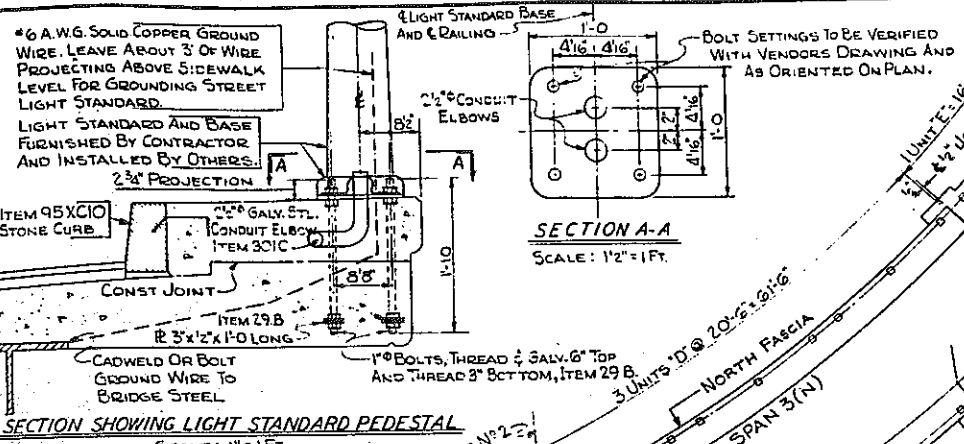
BR. SHEET NO. 13



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
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KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

LOCATION	UNIT	NO.	P	L	TYPE	HOR. RAD	ITEM NO.
W. ABUT. (N)	A	1	5'-4 1/2	9'-4 1/2	-	165'-2 1/2	#31AX
SPAN 1 (N)	B	6	8'-3 1/2	20'-7	1	165'-2 1/2	37A
SPAN 2 (N)	C	5	6'-11 1/2	17'-11	1		
SPAN 3 (N)	D	3	8'-3	20'-6	1		
N.E. WALL	E	1	6'-1	16'-0	2		
SPAN 1 (S)	F	1	6'-1	16'-2	1	STR.	
SPAN 1 (S)	G	2	6'-1 1/2	16'-3	1	STR.	
SPAN 1 (S)	H	1	6'-3 1/2	16'-7	1	194'-9 1/2	
SPAN 2 (S)	J	4	7'-10 3/8	19'-9 1/2	1		
SPAN 3 (S)	K	1	8'-3 1/2	20'-7	1		
SPAN 3 (S)	L	2	8'-4	20'-8	1		
S.E. WALL	M	1	6'-1	16'-0	2		
SPAN 1 (S)	N	1	6'-1	16'-2	1		

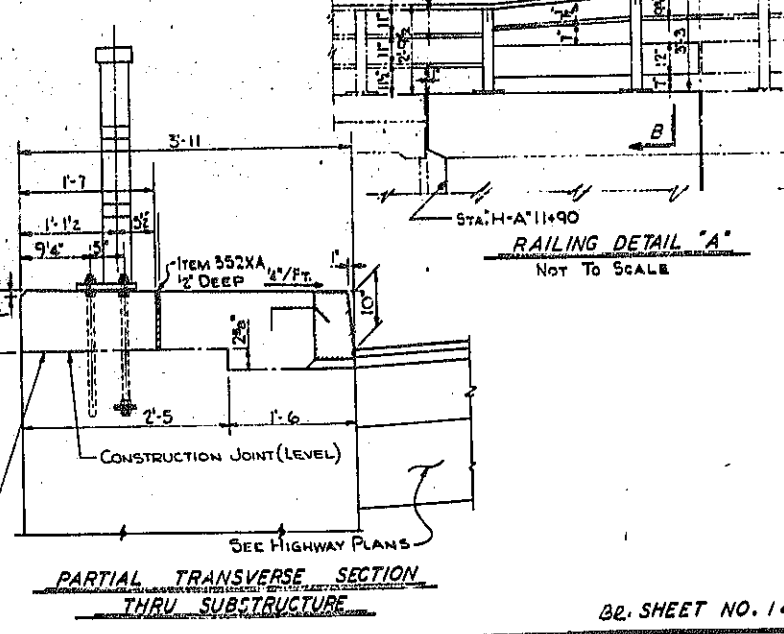
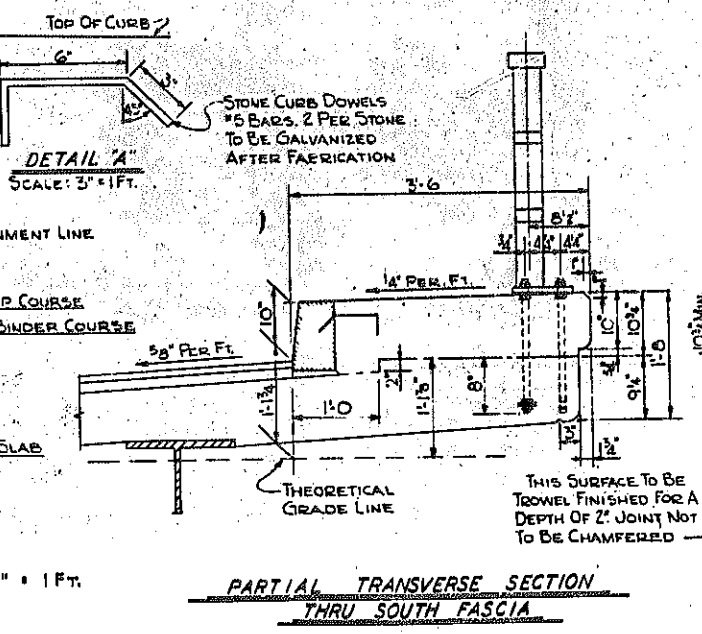
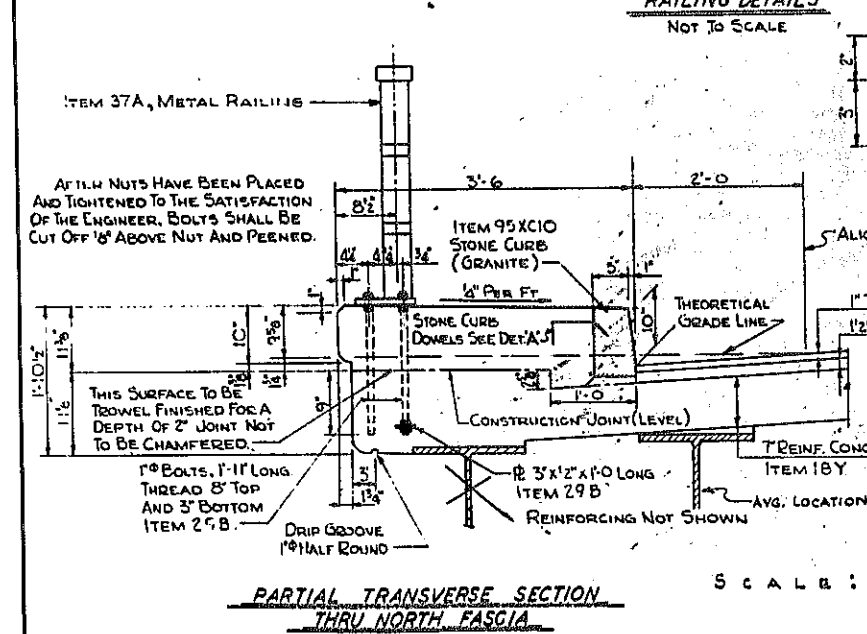
* - PAY FOR UNDER ITEM 37 AX



- CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR SLAB. BROOM FINISH TOP OF SLAB BETWEEN CURBS.
 2. PLACE ITEM 95XC10, STONE CURB (GRANITE).
 3. PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 4. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 5. PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON PLATES.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. BROOMED SURFACE OF SLAB TO BE GIVEN A FINER SLAB WATERPROOFING ITEM 450 B.
 8. PLACE ROADWAY PAVEMENT.

- CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR WALL TO CONSTRUCTION JOINT.
 2. PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 3. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 4. PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON PLATES.
 5. POUR WALL ABOVE CONSTRUCTION JOINT.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. PLACE ROADWAY PAVEMENT.

- GENERAL NOTES**
1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
 2. ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF FASCIA AND SO THAT THE POSTS ARE TRULY VERTICAL.
 3. ALL SURFACES OF WELDS ON RAILING TO BE GROUND SMOOTH.
 4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
 5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
 6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29 B.
 7. TUBULAR RAILS, TUBULAR POST AND BASE PLATES WILL BE PAID FOR UNDER ITEMS 37 A & 37 AX

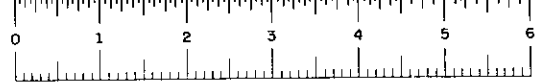


REVISION TO FASCIA BEAM
(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY
RAILING LAYOUT

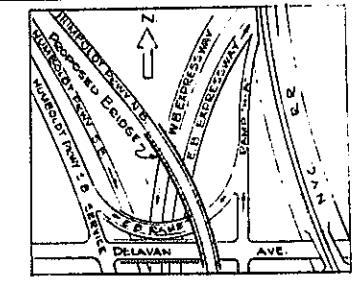
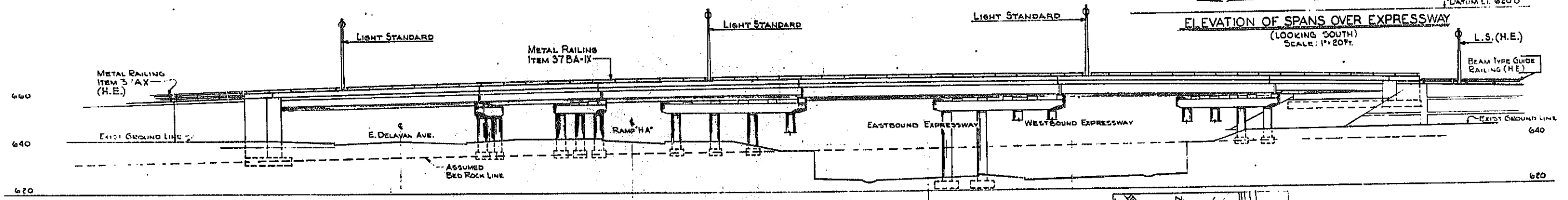
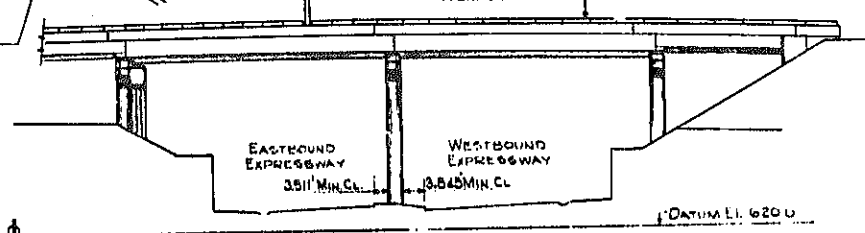
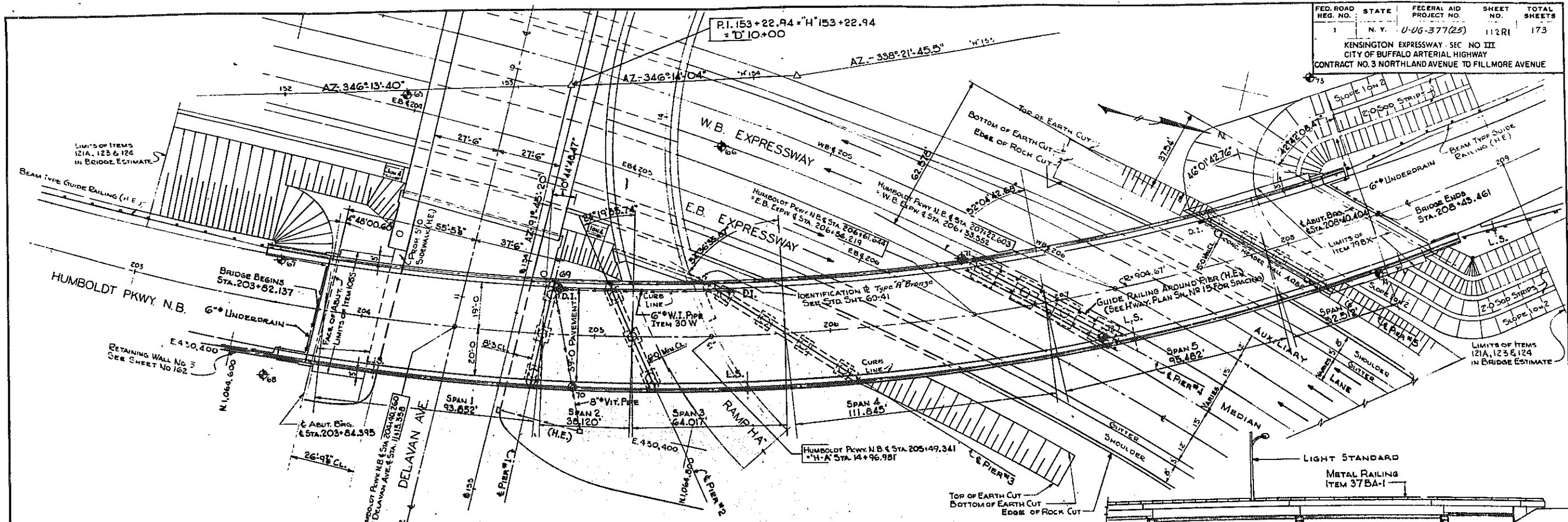
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	112R1	173

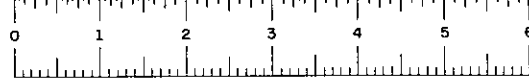
KENSINGTON EXPRESSWAY - SEC NO III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



REVISION, CONC. HEADER - (A.O.B.E.)

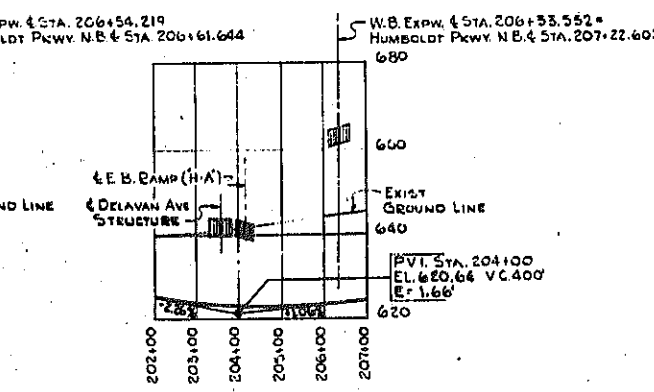
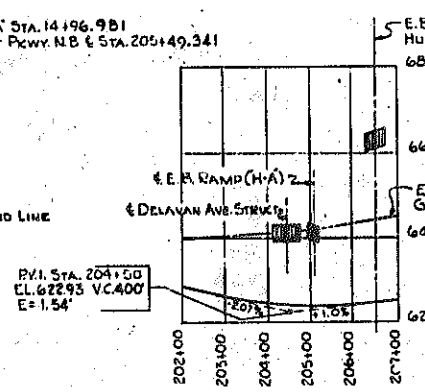
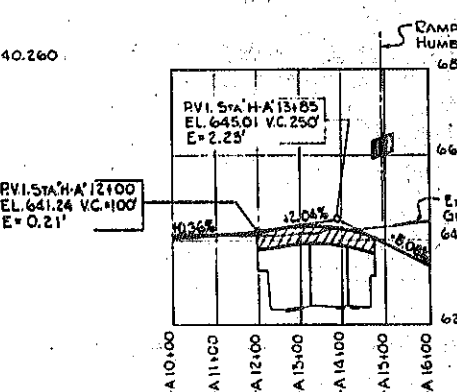
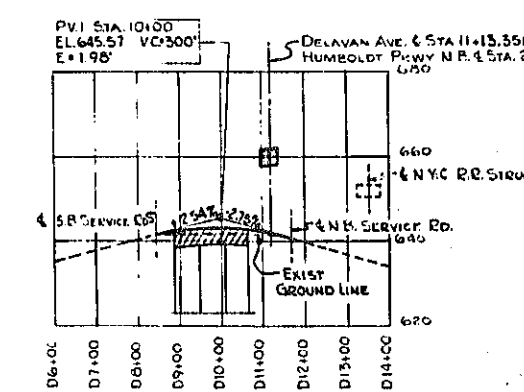
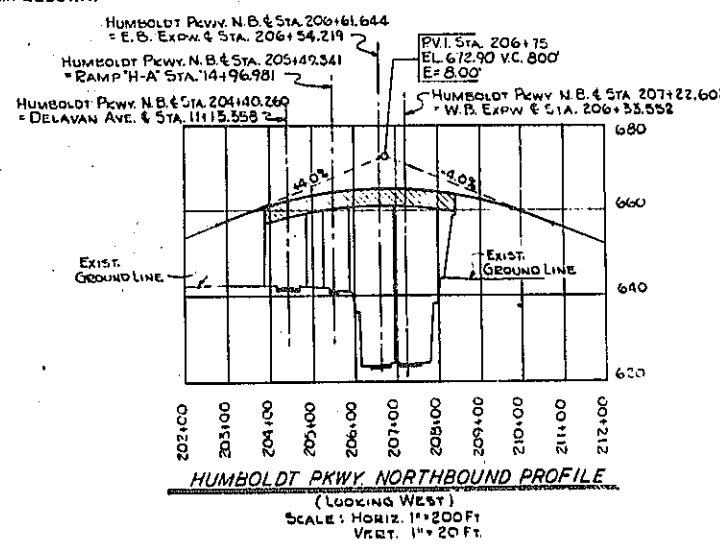
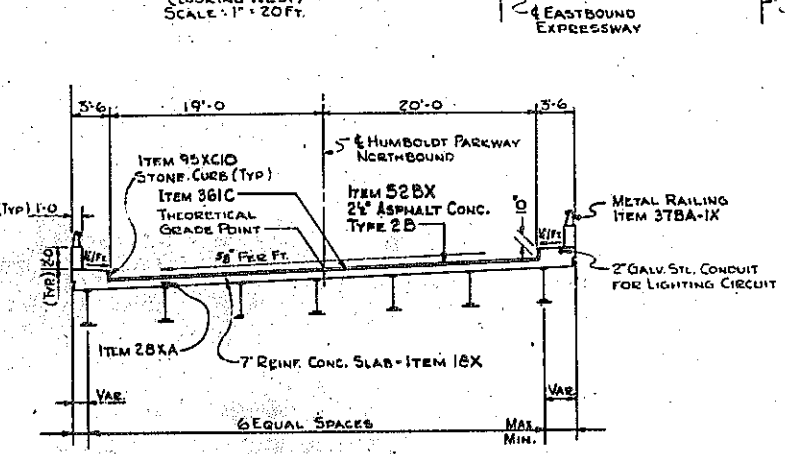
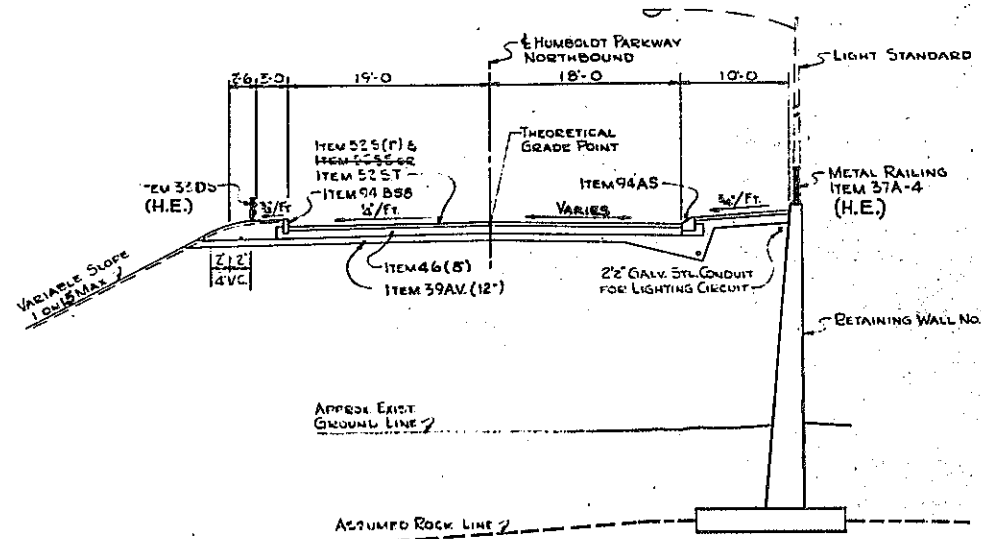
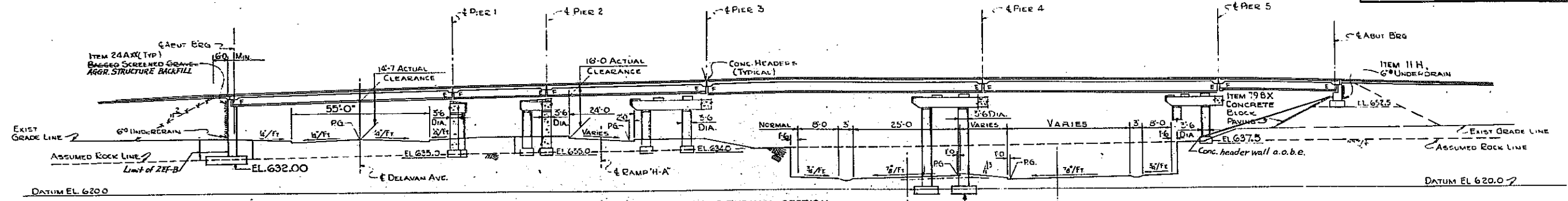
HUMBOLDT PKWY. N.B. OVER E XPW. GENERAL PLAN		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUV, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.

BR. SHEET NO. 1



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	113 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



LEGEND
 ⊕ INDICATES BORING LOCATIONS.
 F = FIXED BEARINGS.
 E = EXPANSION BEARINGS.
 P.G. = PROFILE GRADE.
 ITEMS MARKED (H.E.) ARE TO BE INCLUDED IN HIGHWAY ESTIMATE

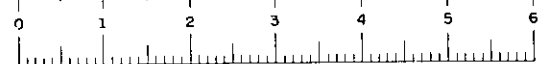
REVISED AS SHOWN 17

HUMBOLDT PKWY N.B. OVER EXPW PROFILES & SECTIONS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.
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B2 SHEET NO. 2



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	114-R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS: A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING: W20-316-14 LIVE LOAD OR THE ALTERNATE LOADING OF TABLE 29, 000 LB. AXLES SPACED AT 4 FT. CENTERS.

MATERIAL FABRICATION AND MANUFACTURE: NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.
THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS: FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENT DOES NOT EXCEED 5 TONS PER SQUARE FOOT, AND DOES NOT EXCEED 1.5 TONS PER SQUARE FOOT ON EARTH.

CONCRETE: ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, IN SOUTH ABUTMENT, AND IN SIDEWALK POURS ON ALL ABUTMENT WINGWALLS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, NORTH ABUTMENT AND WINGWALLS (EXCEPT IN SIDEWALK POURS) SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

CEMENT: CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A. ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE: ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET 60-41.

OTHER: REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SPLICERS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED.
SURFACE OF ABUTMENT AND PIER PEDICESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND DUGH MAINTAINED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.
THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 28 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREMOULDED BITUMINOUS JOINT, COMPRESSED ASBESTOS SHEET PACKING, PREPARED FILLER AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

FOOTINGS: ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 50Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE. EXCAVATION AS ORDERED OR SHOWN ON THE PLANS SHALL BE ITEMS 5E AND 5R TRENCH, CULVERT AND BRIDGE EXCAVATION.

WELDING: WELDED JOINTS SHALL BE RADIOGRAPHED IN CONFORMANCE WITH THE REQUIREMENTS OF THE NEW YORK STATE SPECIFICATION FOR RADIOGRAPHIC INSPECTION. THE JOINTS TO BE RADIOGRAPHED ARE DESCRIBED THEREIN. ALL WELDING SHALL CONFORM TO THE CURRENT "STANDARD SPECIFICATIONS FOR WELDED HIGHWAY AND RAILROAD BRIDGES" OF THE AMERICAN WELDING SOCIETY.

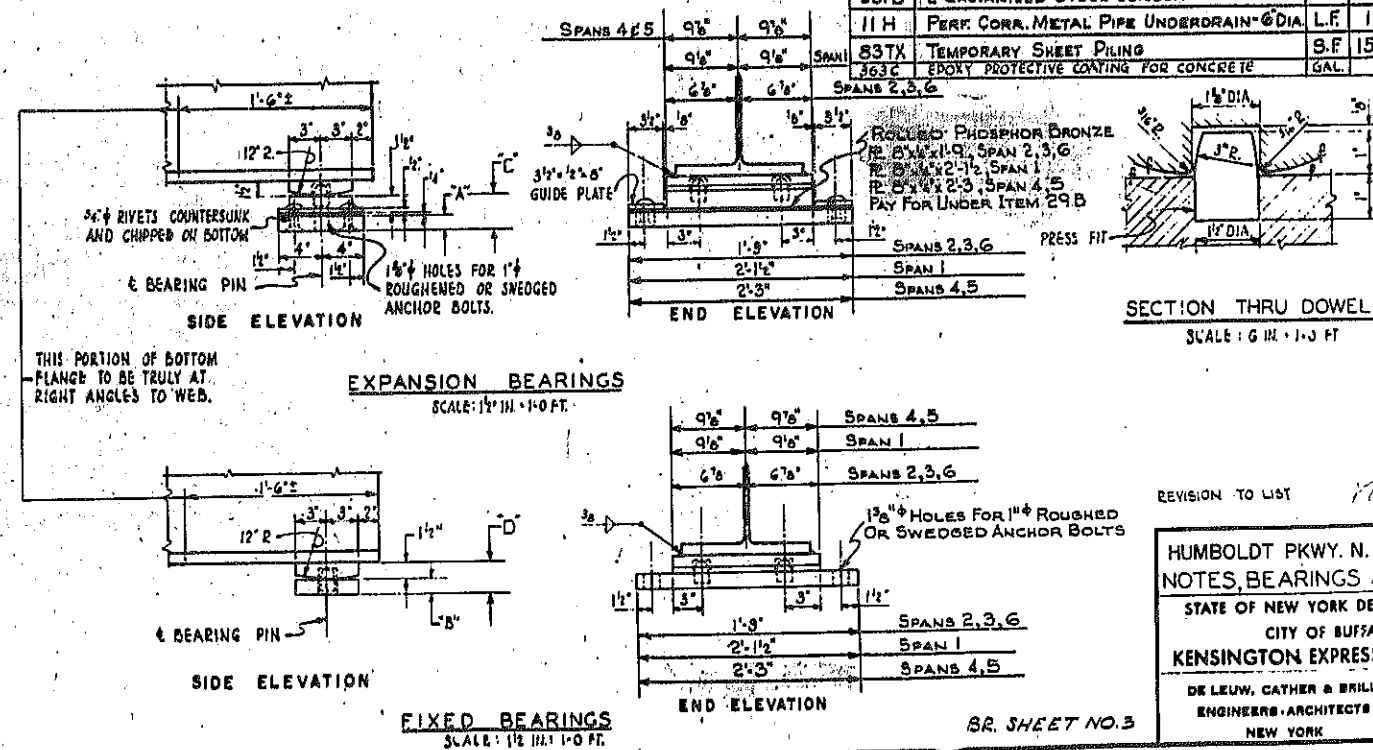
FORMS: APPROVED CORRUGATED METAL FORM MAY BE USED FOR THE STRUCTURAL SLAB, PROVIDED THE COVER FOR THE REINFORCING STEEL IS MAINTAINED ABOVE THE CREST OF THE CORRUGATION.

NOTE: FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

TABLE OF BEARINGS						
LOCATION	STRINGER NO.	A	B	C	D	NO. ANCHOR BOLTS
EXPANSION BEARINGS						
PIER No. 1	1A-1G	7	1 3/8"	4 5/8"		14 1'-2"
PIER No. 2	2A-2G	7	1 3/8"	4 5/8"		14 1'-2"
PIER No. 3	3A-3G	7	1 3/8"	4 5/8"		14 1'-2"
PIER No. 4	4A-4G	7	1 3/8"	4 5/8"		14 1'-2"
PIER No. 5	5A-5G	7	1 3/8"	4 5/8"		14 1'-2"
NORTH ABUT.	6A-6G	7	1 3/8"	4 5/8"		14 1'-2"
FIXED BEARINGS						
SOUTH ABUT.	1A-1G		1 3/8"	3 3/8"	14	1'-2"
PIER No. 1	2A-2G		3 3/8"	5 3/8"	14	1'-4"
PIER No. 2	3A-3E		3 3/8"	4 1/2"	10	1'-4"
PIER No. 2	3F		3"	4 1/2"	2	1'-3"
PIER No. 2	3G		2 5/8"	4 5/8"	2	1'-3"
PIER No. 3	4A-4G		1 3/8"	3 3/8"	14	1'-2"
PIER No. 4	5A-5G		3 1/2"	5"	14	1'-4"
PIER No. 5	6A-6G		1 3/8"	3 3/8"	14	1'-2"

ESTIMATE OF QUANTITIES					Final
ITEM NO.	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED	
2EF-B	SELECTED FILL	C.Y.	1,862	2,050	354.44
5	TRENCH, CULVERT & BRIDGE EXC.		607	670	158.05
13A	CAST IRON PIPE 6" DIA.	L.F.	20	21	9.7
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	2,640	2,750	254.5
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	1,379	1,420	137.19
20Y	CLASS 1 CONCRETE	C.Y.	279	290	281.0
24AX	BAGGED SCREENED GRAVEL OR SLAB	C.Y.	57	60	59.8
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	281,430	287,000	282.36
28B	SHEAR CONNECTORS	L.F.	3,202	3,300	3,200
29B	STRUCTURAL STEEL	Lb.	905,603	932,700	888.23
30S	MISCELLANEOUS METAL	Lb.	540	560	540
30W	MISC. METAL - W. I. PIPE 6" DIA.	Lb.	8,802	9,000	8,854
37BA-1	METAL RAILING - TYPE B-1 RAIL	L.F.	989	1,020	980
52BX	ASPHALT CONCRETE TYPE 2 B (2 1/2)	TON	273	290	273.0
61	BITUMINOUS MATERIAL	GAL.	194	200	162.5
79BX	CONCRETE BLOCK PAVING	S.Y.	162	170	364.7
95XC10	10" STONE CURB (BRIDGE)	L.F.	989	1,010	1,015.5
121A	TOPSOIL FURNISHED & PLACED	C.Y.	102	107	102.0
123	SEEDING	Acres	0.21	0.22	0.210
124	SODDING	S.Y.	98	100	124.4
301C	2 1/2 GALVANIZED STEEL CONDUIT	L.F.	15	15	28.2
303C6	FURNISH & INSTALL LIGHT STDS. 30' HT. 6" ARM	EA.	3	3	3,000
352XA	RUBBER JOINT MATERIAL	GAL.	19	21	37.80
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	237	250	426.0
9VC	SEWER PIPE VITRIFIED	L.F.	57	60	51.0
301B	2" GALVANIZED STEEL CONDUIT	L.F.	518	530	501.6
11H	PERF. CORR. METAL PIPE UNDERDRAIN 6" DIA.	L.F.	168	178	154
83TX	TEMPORARY SHEET PILING	S.F.	1507	1580	0
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	24	25	57.0



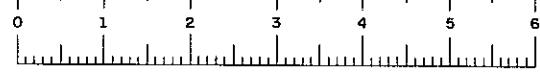
REVISION TO LIST

HUMBOLDT PKWY. N.B. OVER EXPW.
NOTES, BEARINGS AND ESTIMATE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	S.S. C.E. S.S.
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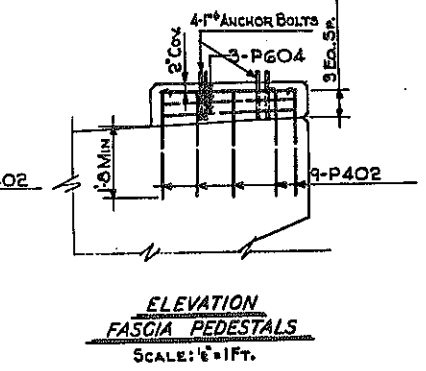
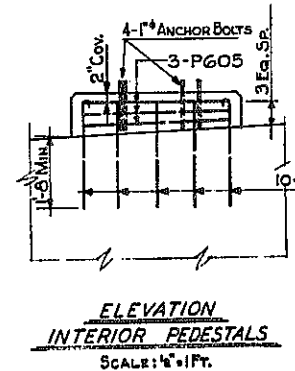
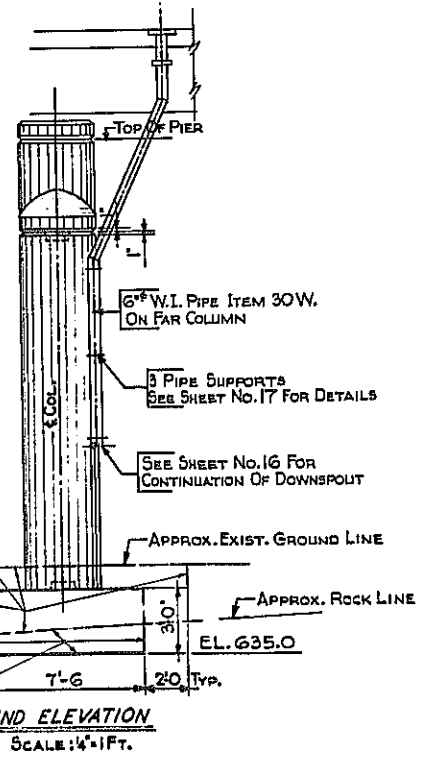
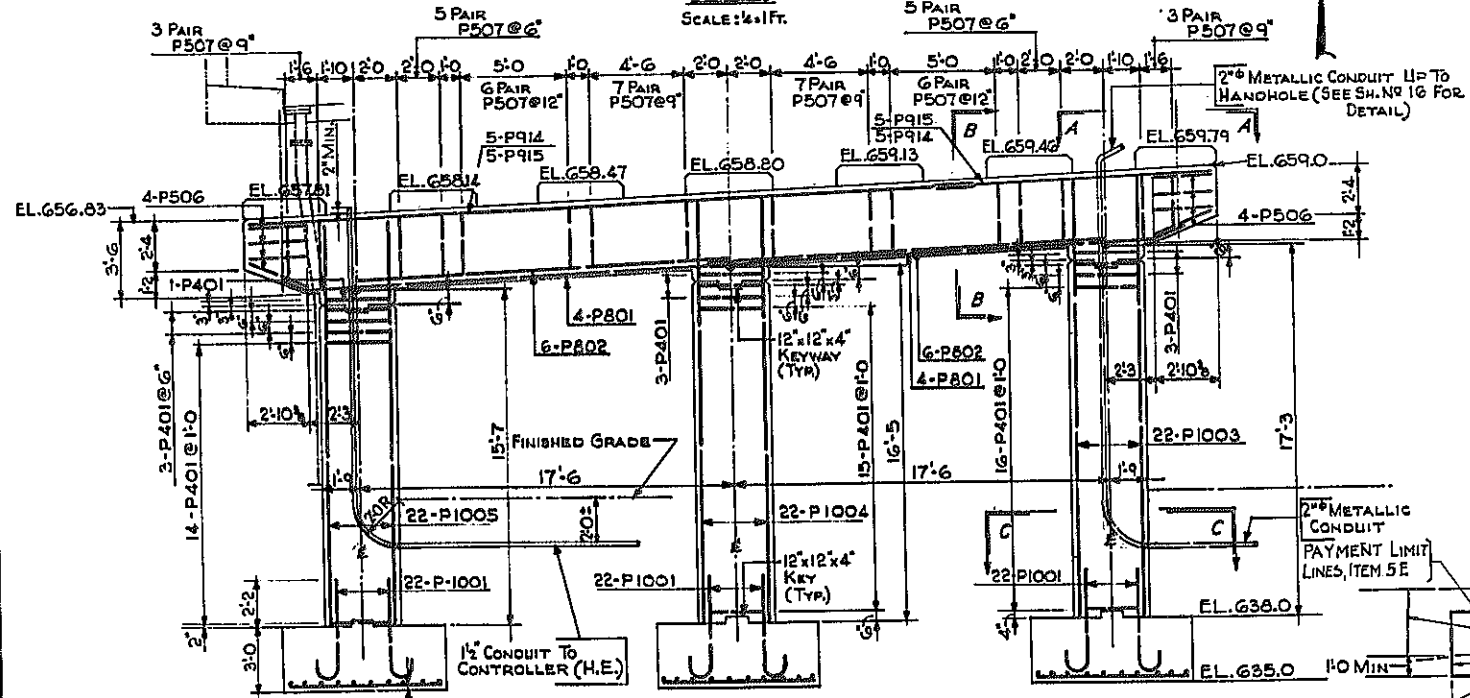
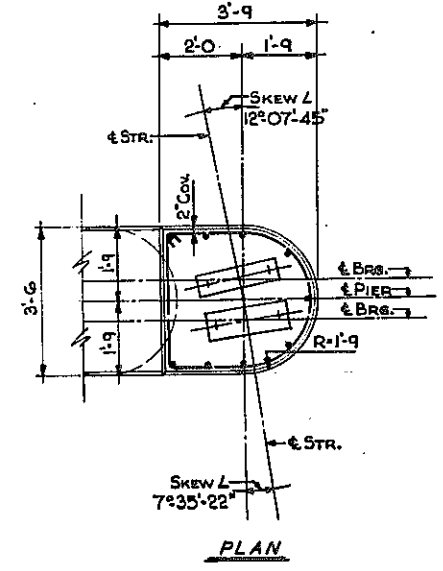
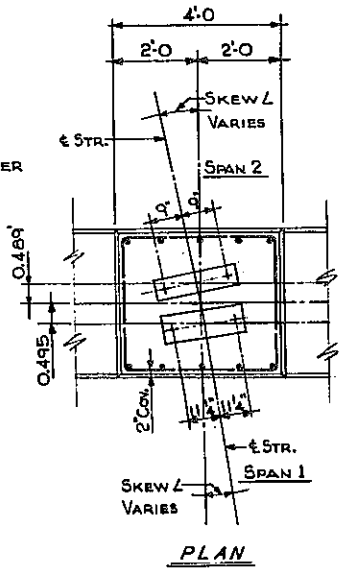
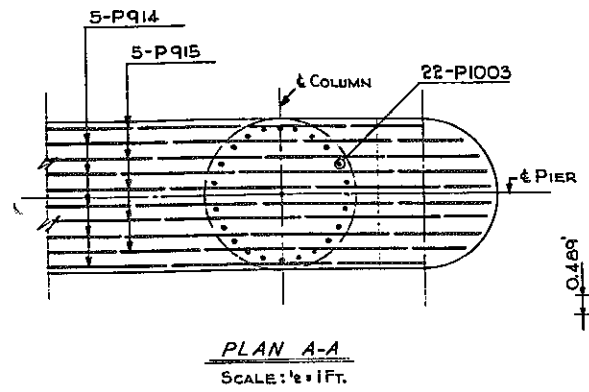
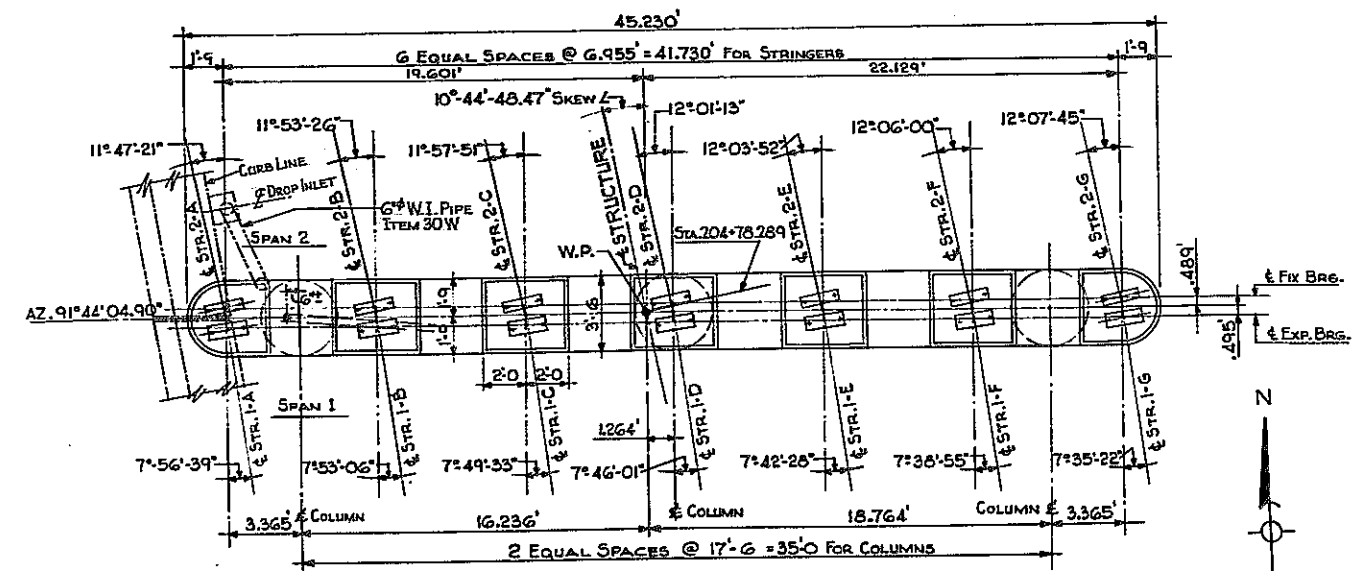
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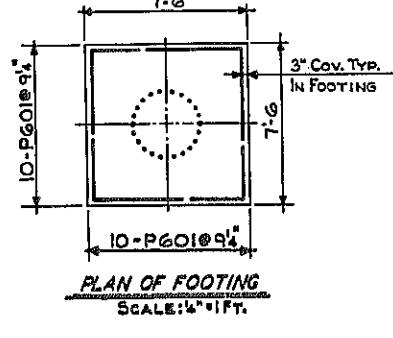
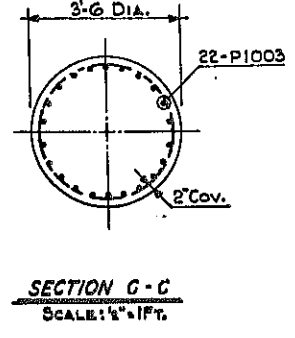
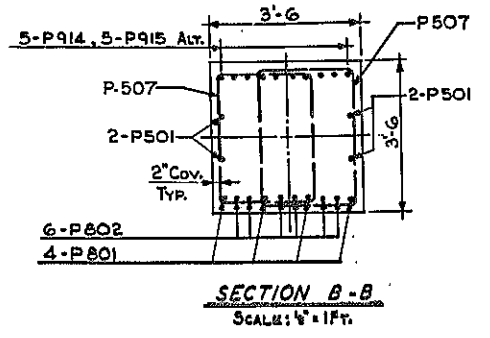
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	115	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18 X, CLASS 1A CONCRETE FOR STRUCTURES.
CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20 Y, CLASS I CONCRETE.
ANCHOR BOLTS FOR BEARINGS SHALL BE 1" φ ROUGHENED OR SWEDGED BOLTS. SET 10" INTO MASONRY.
SEE SHEET NO. 3 FOR ANCHOR BOLT LENGTHS.



BAR REINFORCING LEGEND

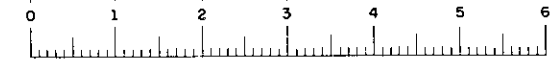
4	P	9	O2
↑	↑	↑	↑
NUMBER OF BARS	PIER	SIZE OF BARS	BAR MARK

HUMBOLDT PKWY. N.B. OVER EXPW. PIER NO. 1

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

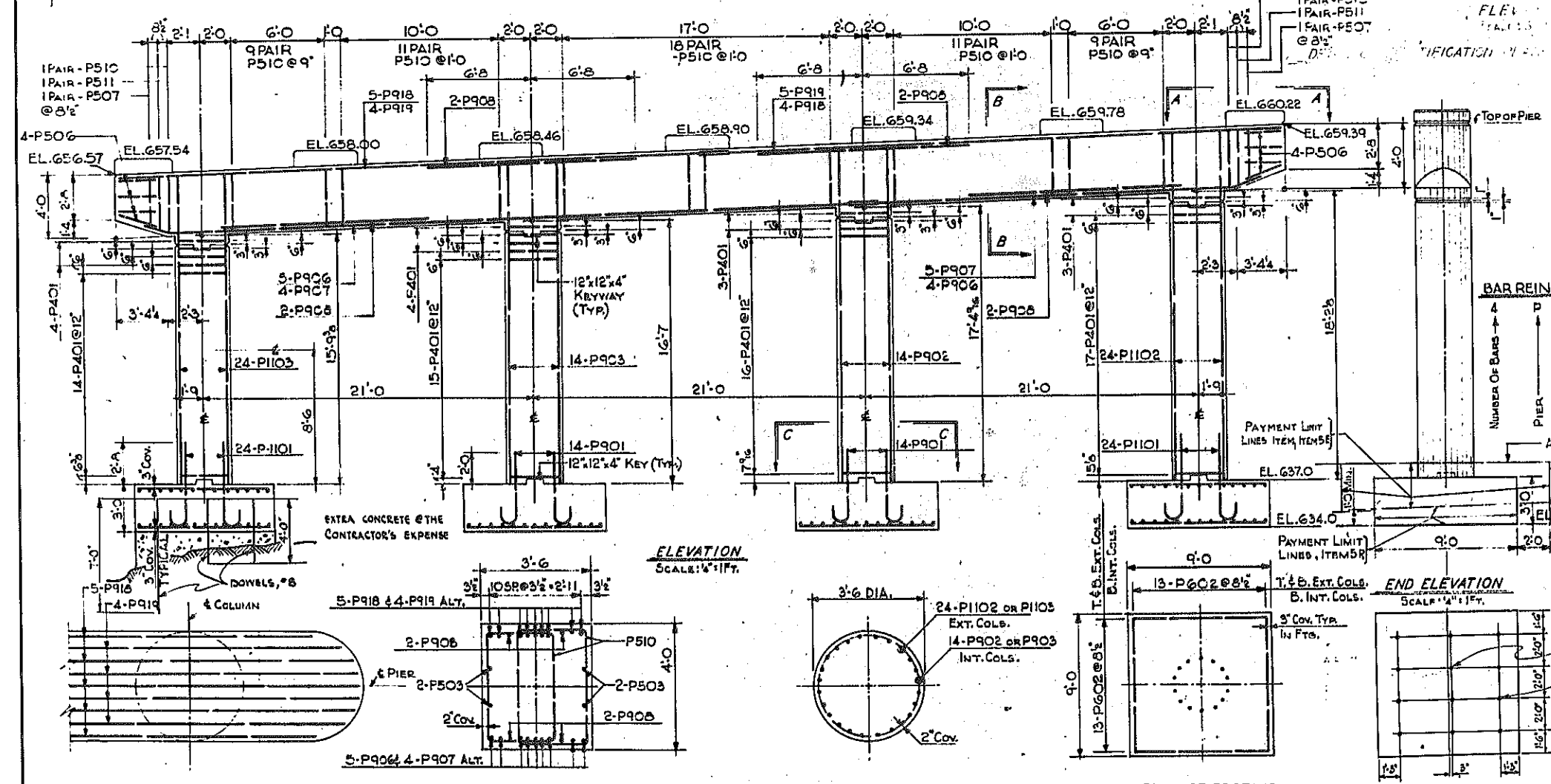
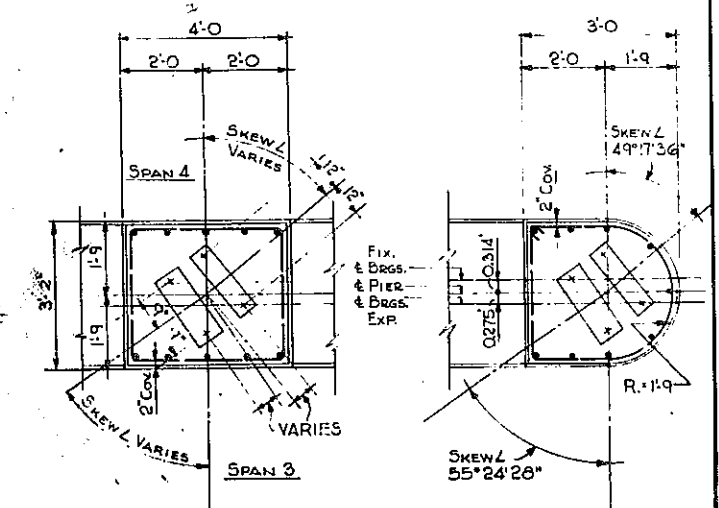
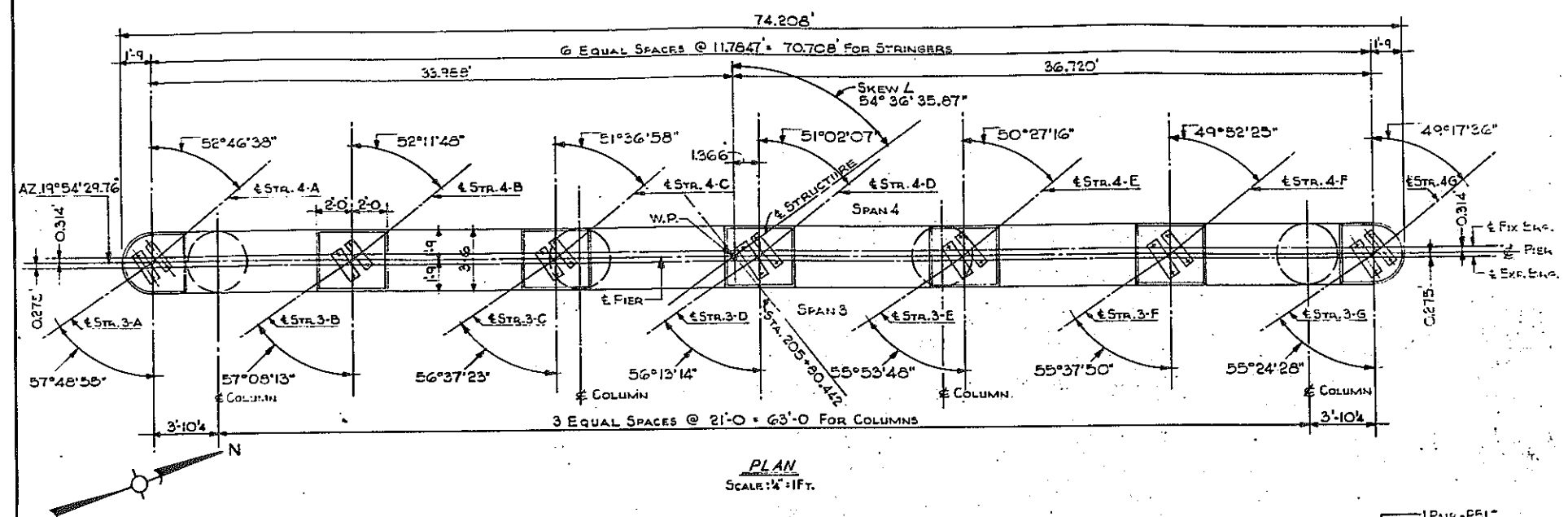
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BR SHEET NO. 4



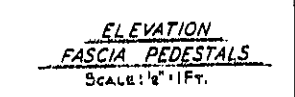
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1	N. Y.	UUG-377(25)	1171	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



BAR REINFORCING LEGEND

↑	4	Number of Bars
↑	P	PIER
↑	Q	SIZE OF BARS
↑	O2	BAR MARK



NOTES

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.

CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

CONCRETE IN THE PIER FOOTING, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS, SET 10" INTO MASONRY.

SEE SHEET NO. 3 FOR ANCHOR BOLT LENGTHS.

PLAN A-A
SCALE: 1/4" = 1 FT.

SECTION B-B
SCALE: 1/4" = 1 FT.

SECTION C-C
SCALE: 1/4" = 1 FT.

PLAN OF FOOTING
SCALE: 1/4" = 1 FT.

PLAN OF DOWELS ADDED IN MOST SOUTHERLY FOOTING

BR. SHEET NO 6

REVISION TO FOOTING REINF.

HUMBOLDT PKWY. N.B. OVER EXPW.
PIER NO. 3

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

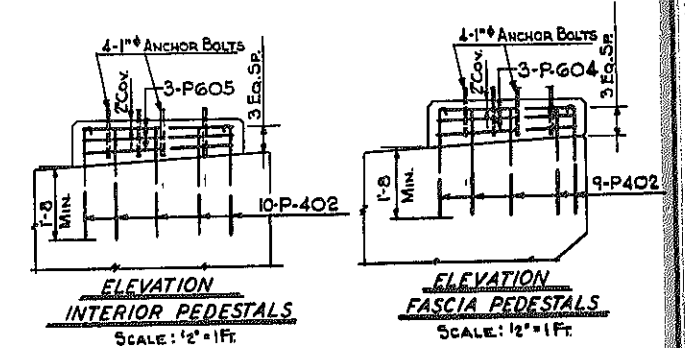
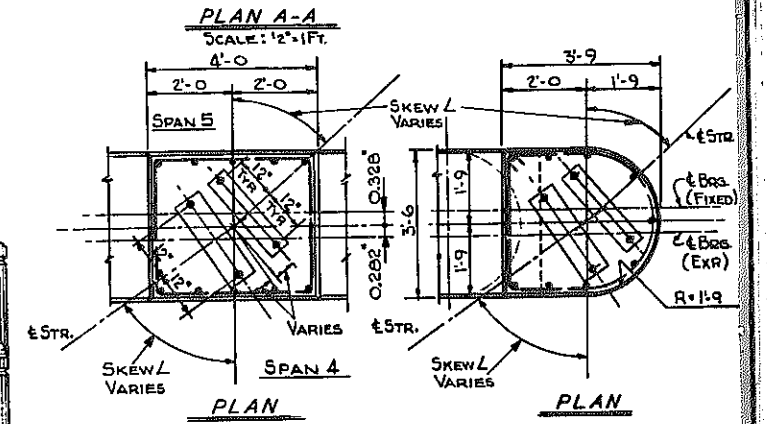
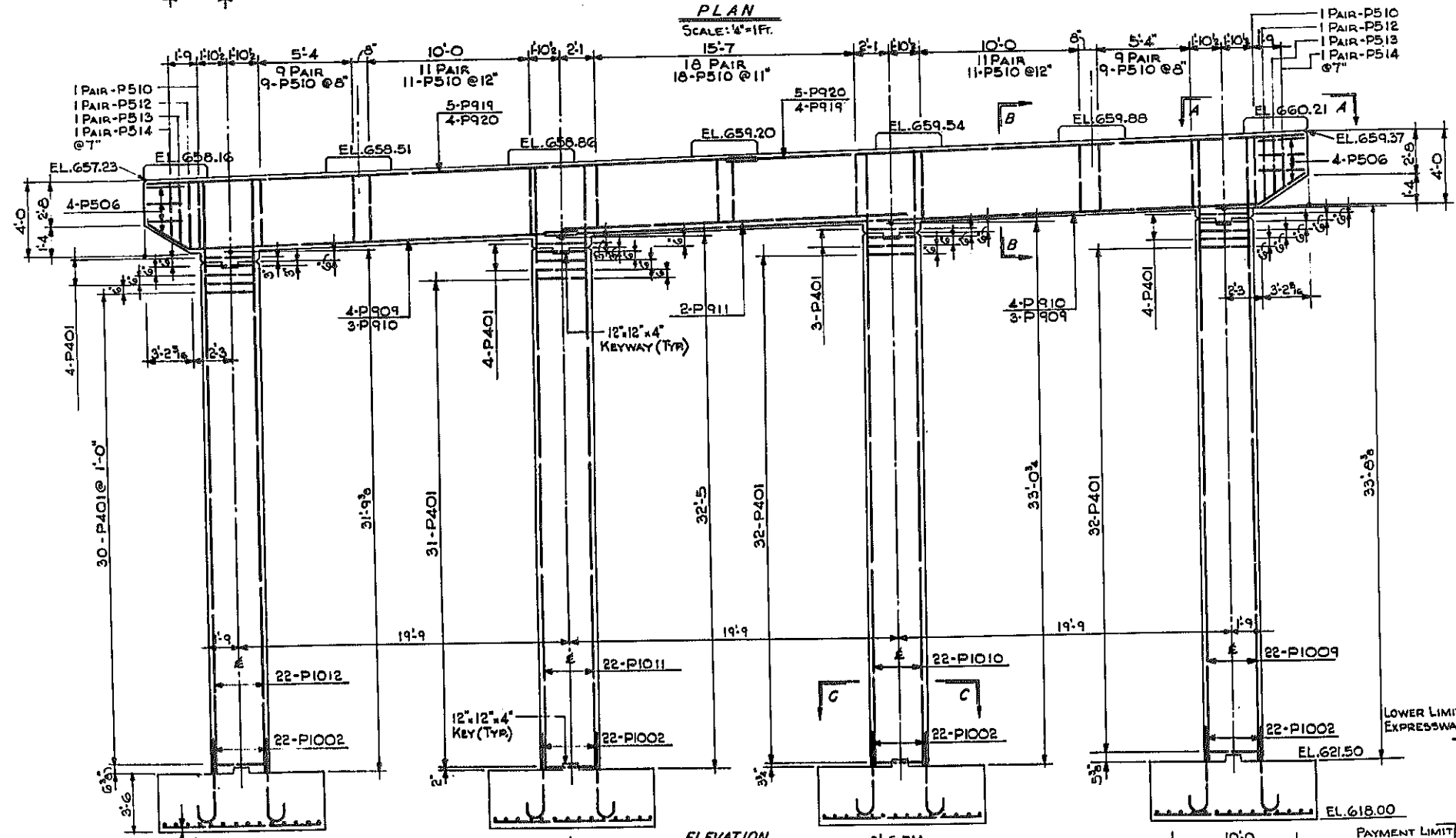
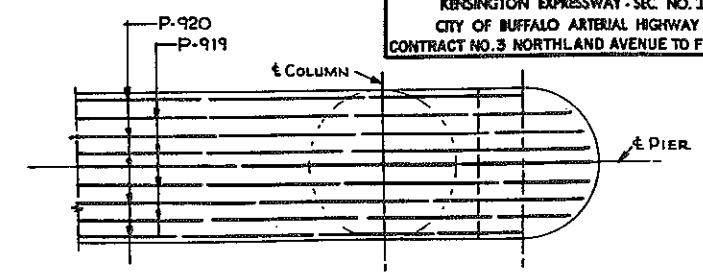
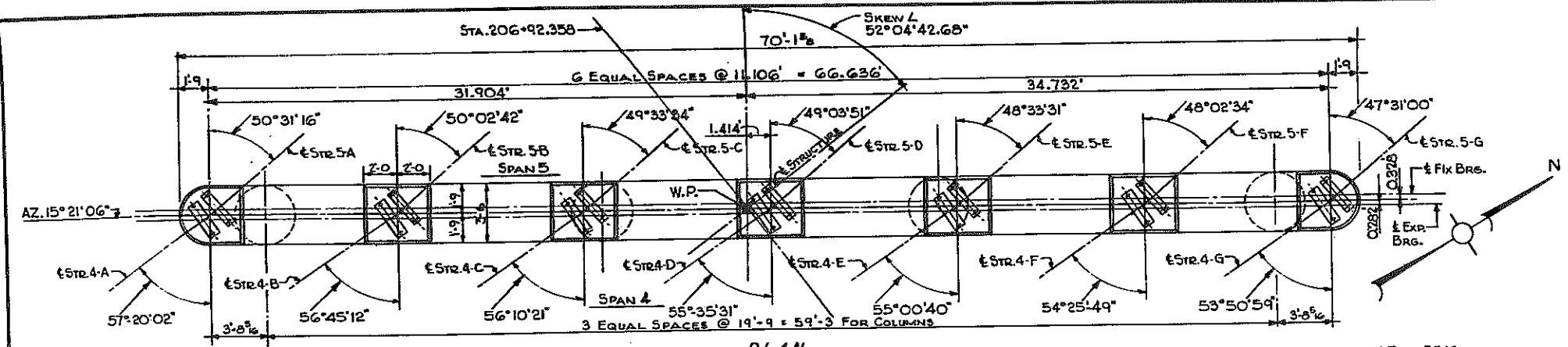
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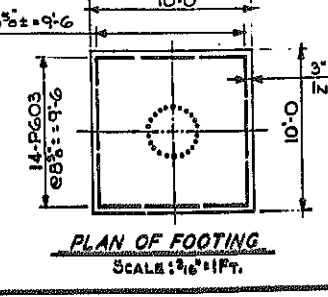
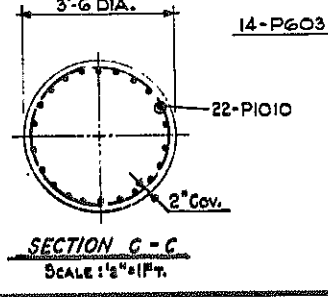
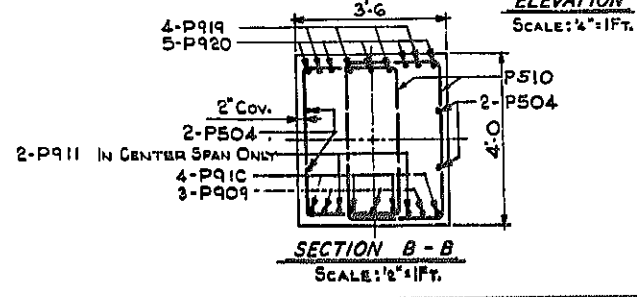
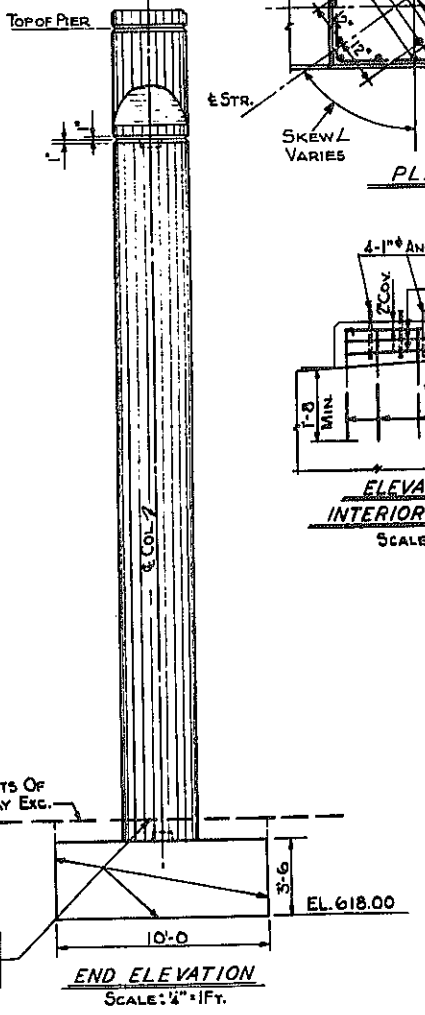
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	118	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS. SET 10" INTO MASONRY.
SEE SHEET NO. 3 FOR ANCHOR BOLT LENGTHS



BAR REINFORCING LEGEND

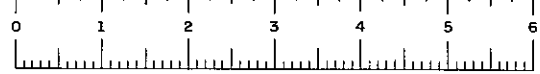
Number of Bars	Pier	Size of Bars	Bar Mark
4	P	3	O2

BR. SHEET NO. 7

HUMBOLDT PKWY. N. B. OVER EXPW. PIER NO. 4

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

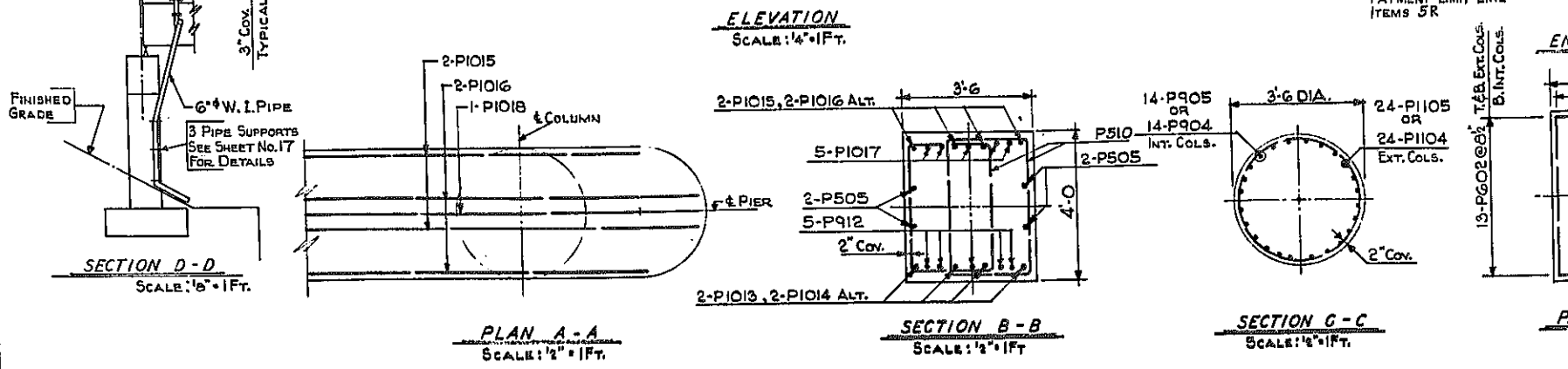
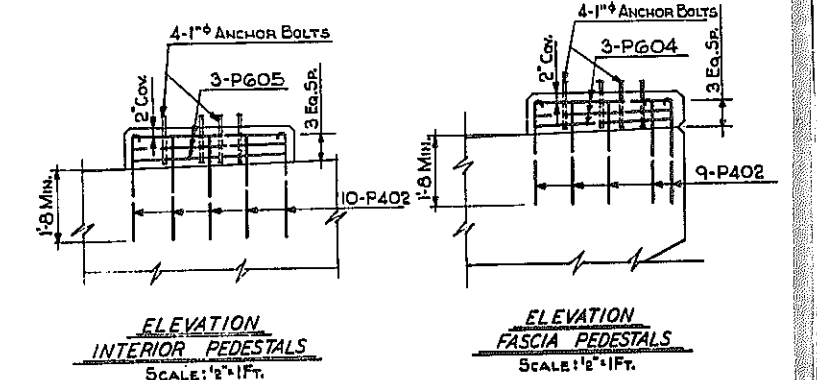
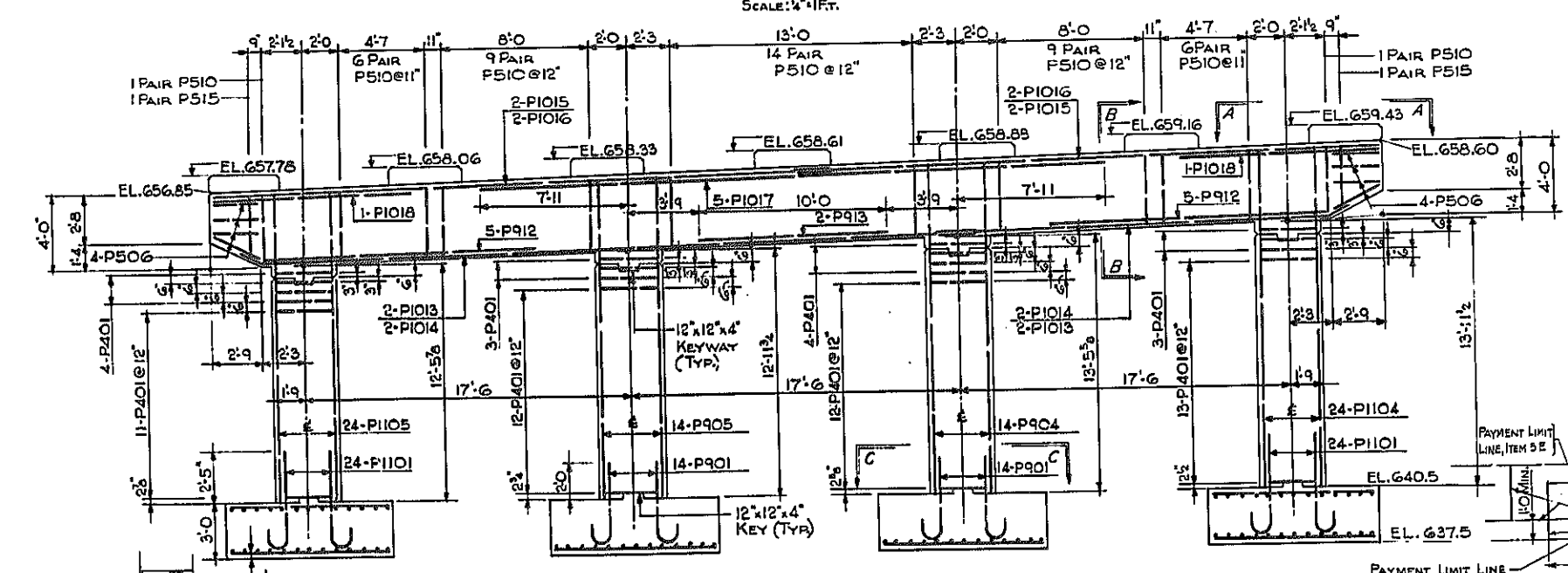
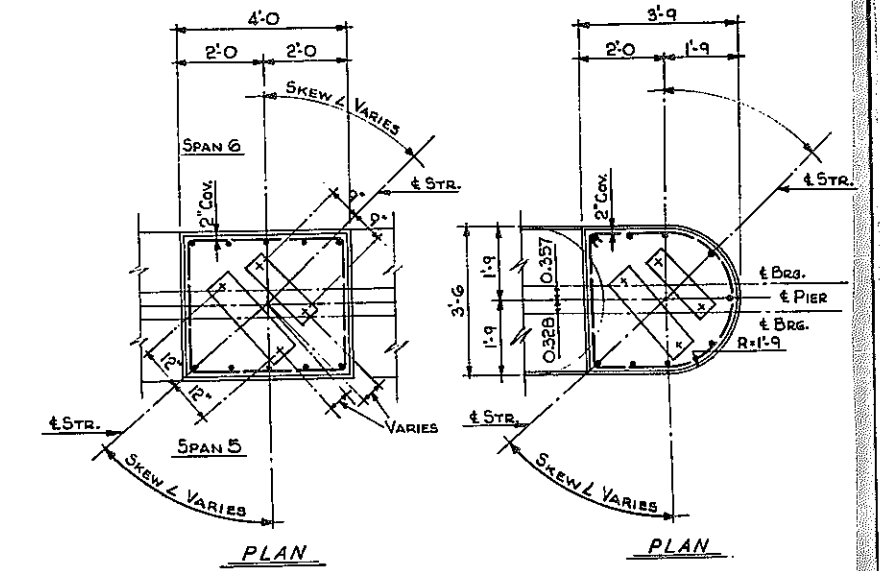
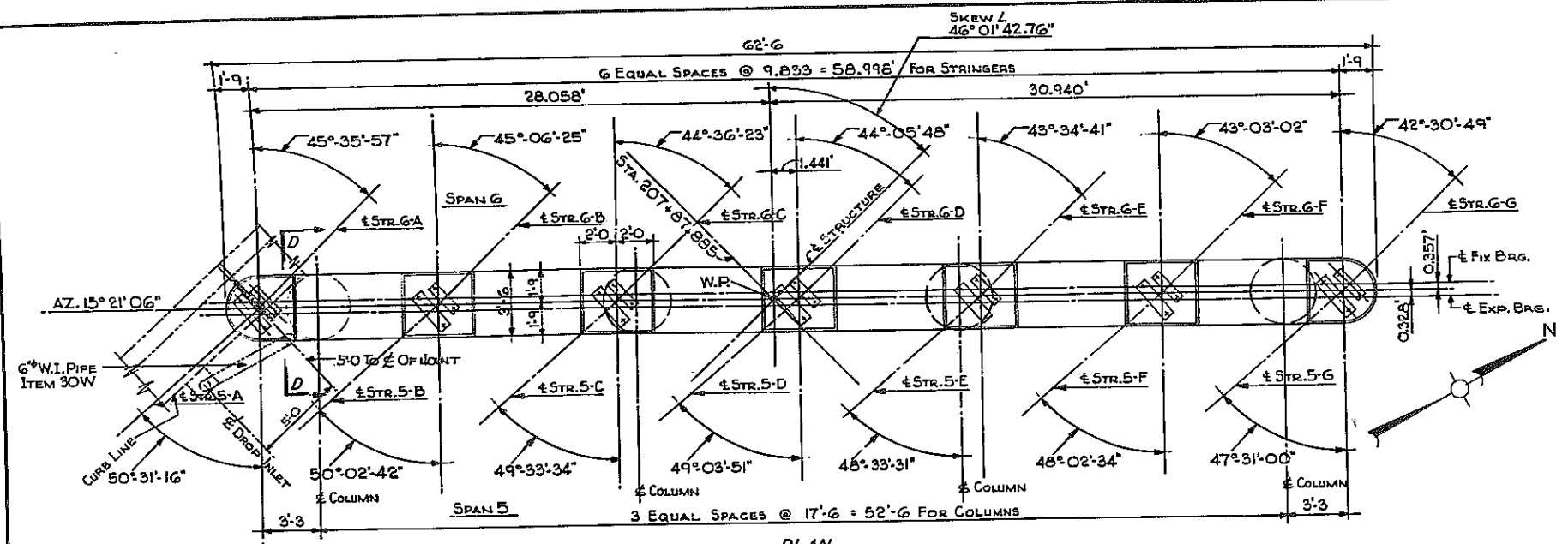
DR. LEW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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BAO 60-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	119	173

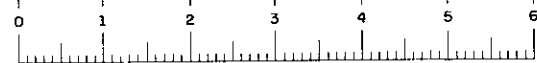
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.
CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS I CONCRETE.
ANCHOR BOLTS FOR BEARINGS SHALL BE 1" DIA. ROUGHENED OR SWEDGED BOLTS, SET 10" INTO MASONRY.
SEE SHEET NO. 3 FOR ANCHOR BOLT LENGTHS.

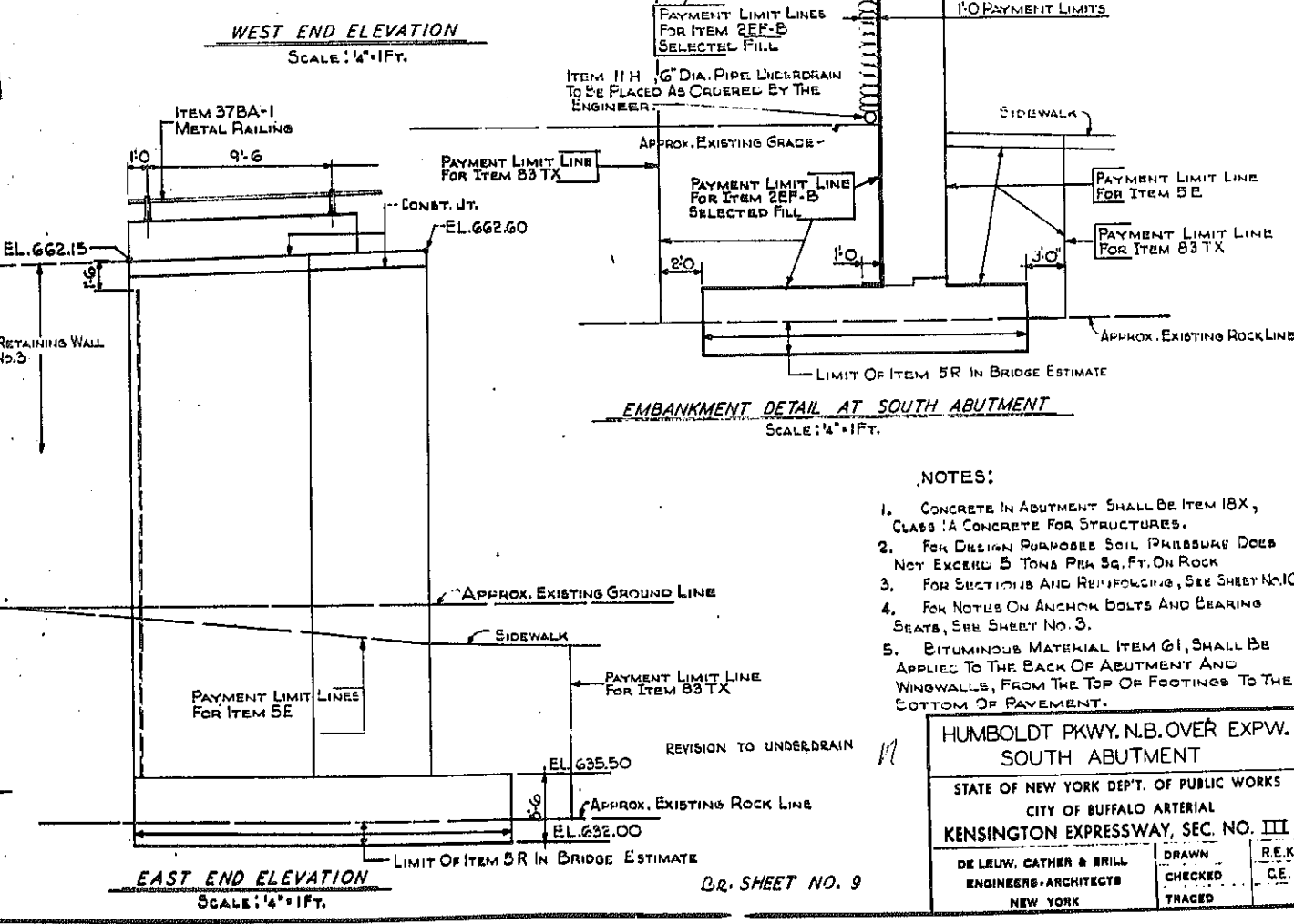
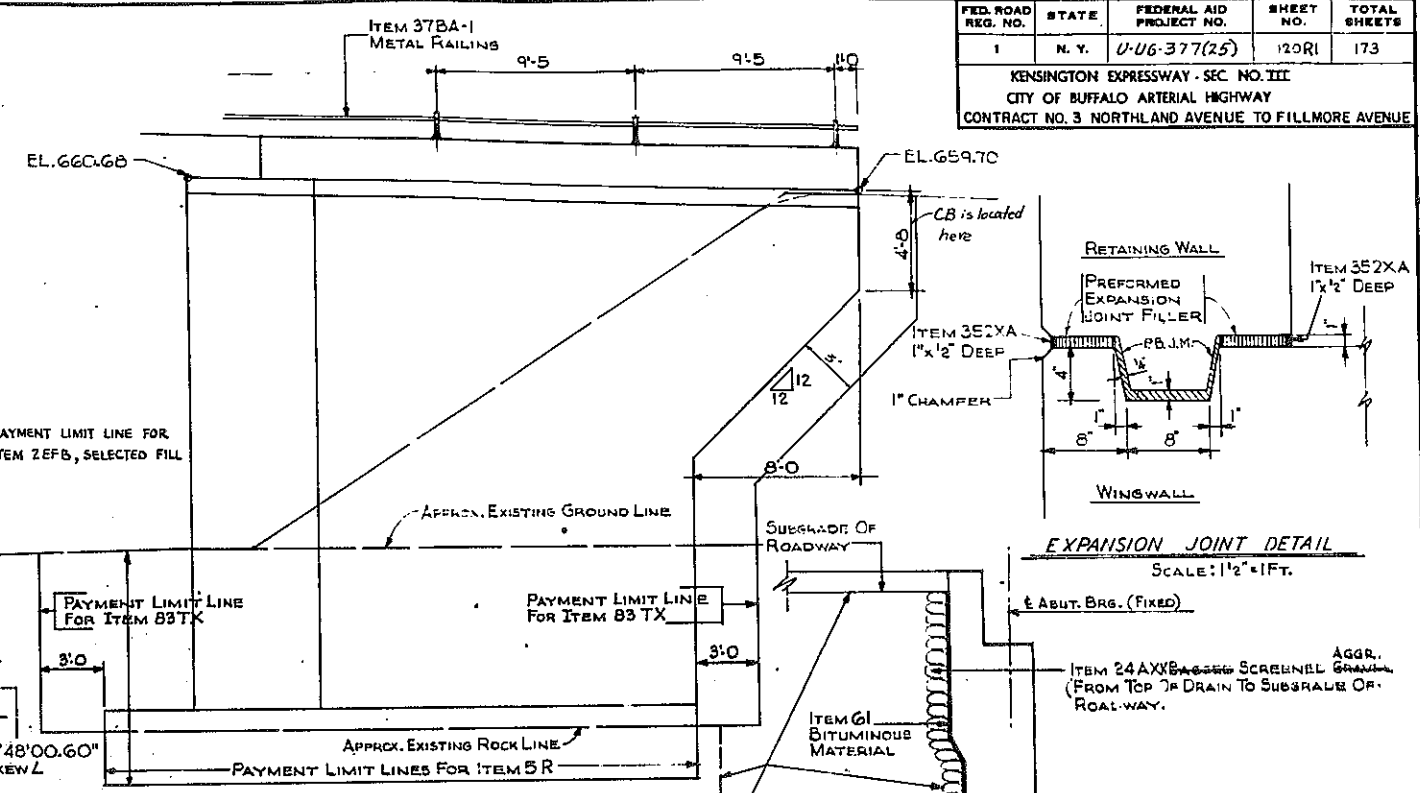
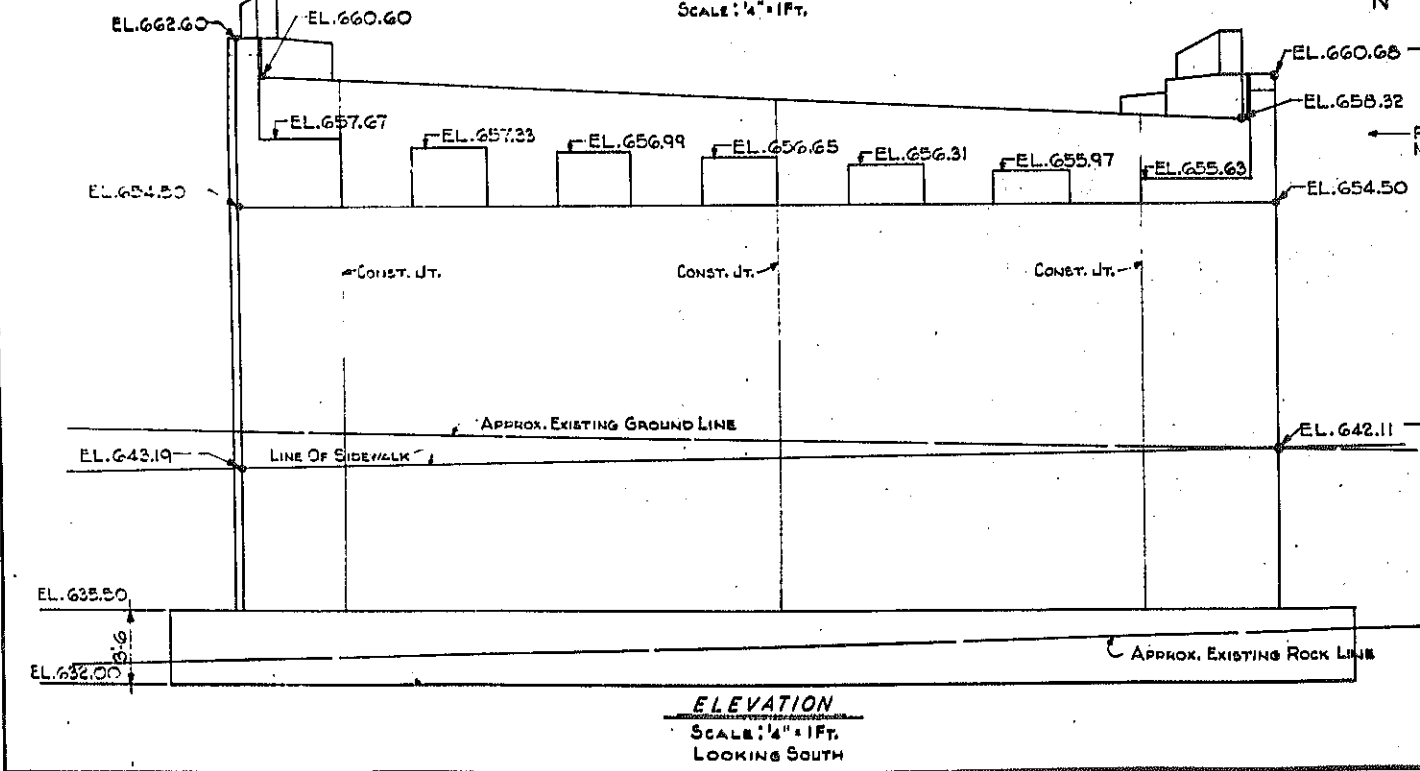
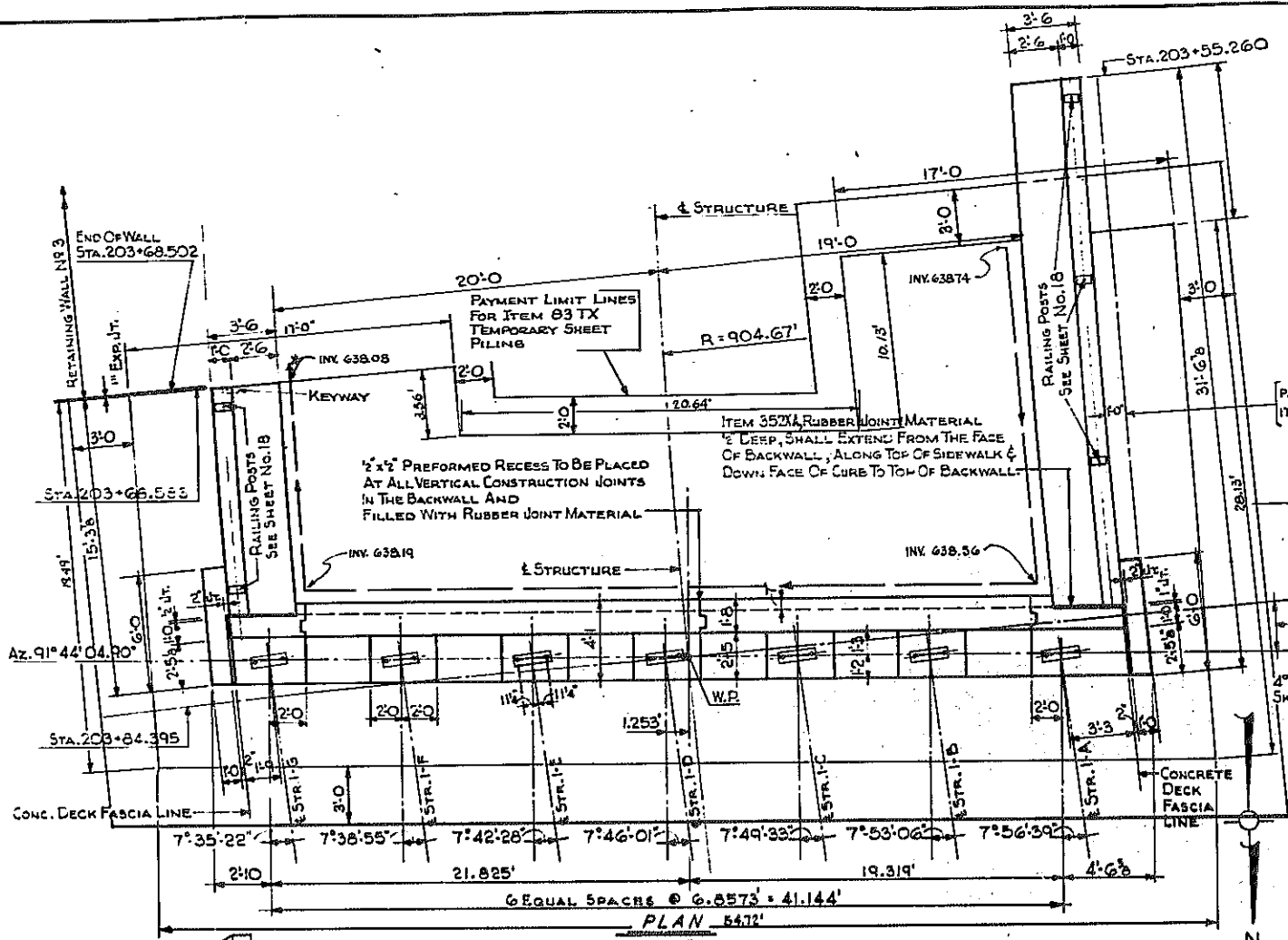
HUMBOLDT PKWY. N. B. OVER EXPW. PIER NO. 5

STATE OF NEW YORK DEPT. OF PUBLIC WORKS		DRAWN R.E.	
CITY OF BUFFALO ARTERIAL		CHECKED C.E.	
KENSINGTON EXPRESSWAY, SEC. NO. III		TRACED	
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK			



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	120R1	173

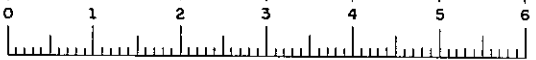
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
1. CONCRETE IN ABUTMENT SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 2. FOR DESIGN PURPOSES SOIL PRESSURE DOES NOT EXCEED 5 TONS PER SQ. FT. ON ROCK
 3. FOR SECTIONS AND REINFORCING, SEE SHEET NO. 10
 4. FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 3.
 5. BITUMINOUS MATERIAL ITEM G1, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.

HUMBOLDT PKWY. N.B. OVER EXPW. SOUTH ABUTMENT
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK
DRAWN CHECKED TRACED
R.E.K. C.E.

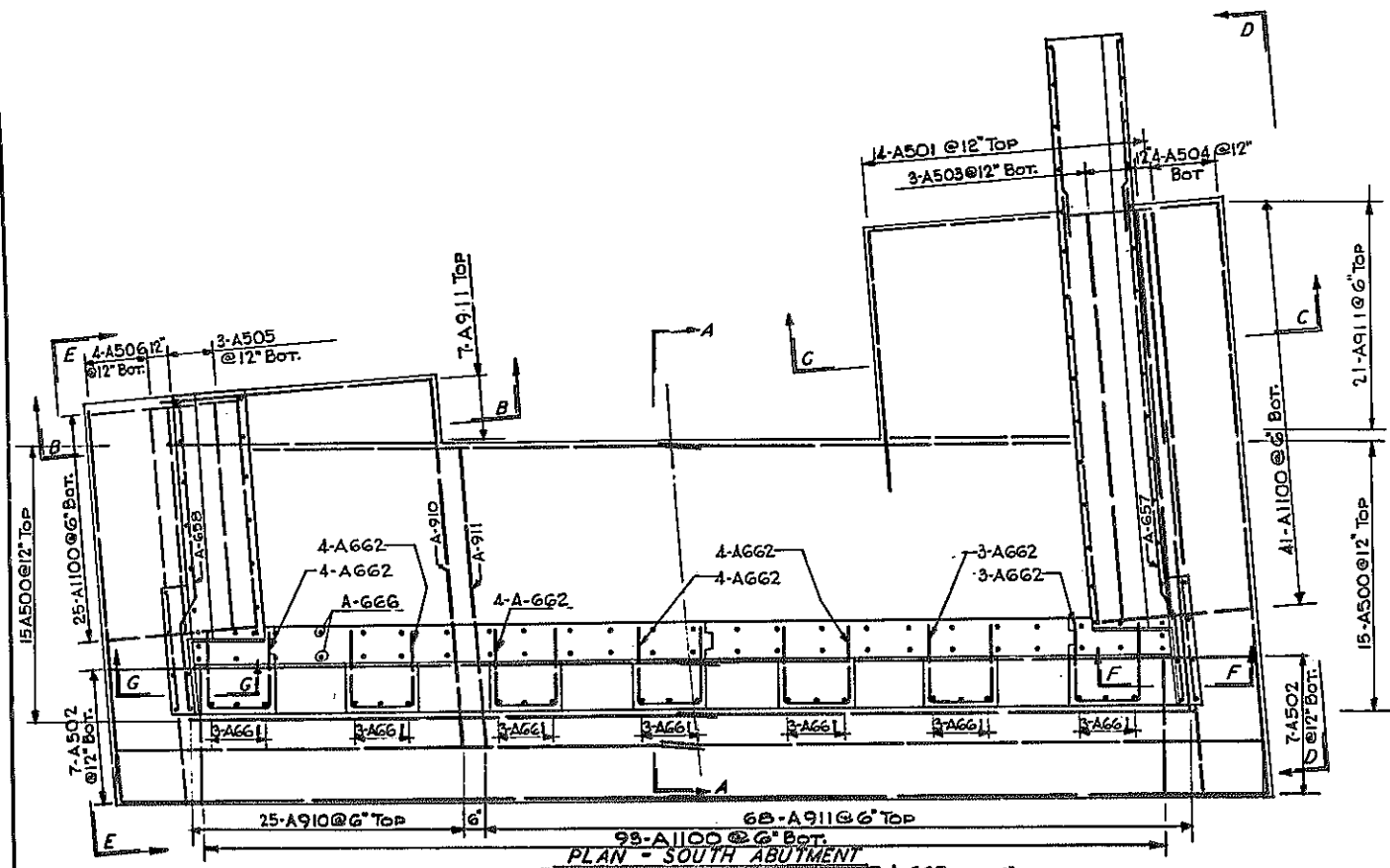
D.R. SHEET NO. 9



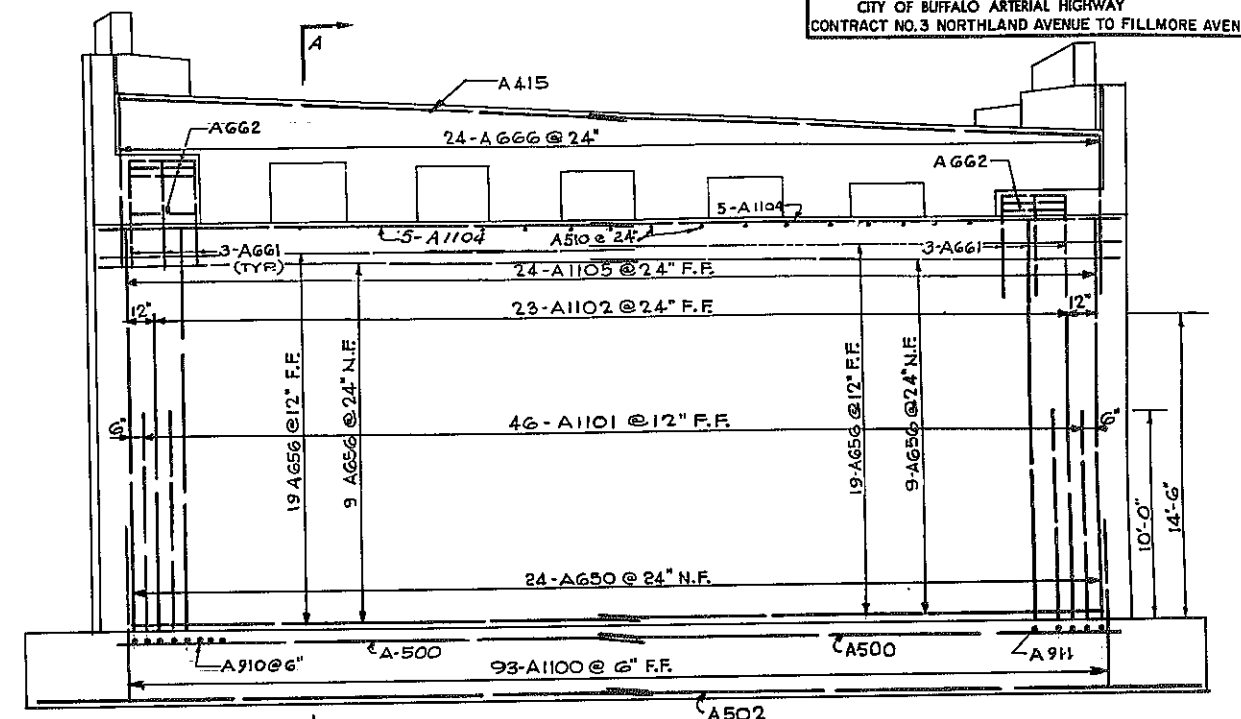
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	1-116-377(25)	121	173

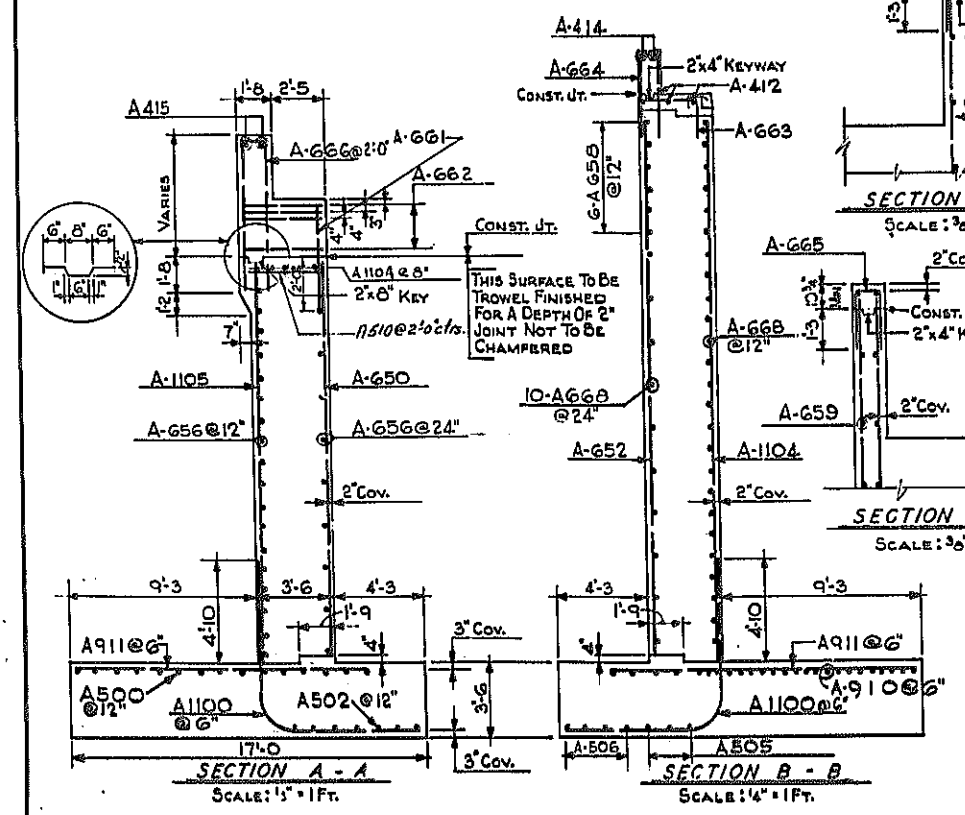
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN OF FOOTING
SCALE: 1/4" = 1 FT.

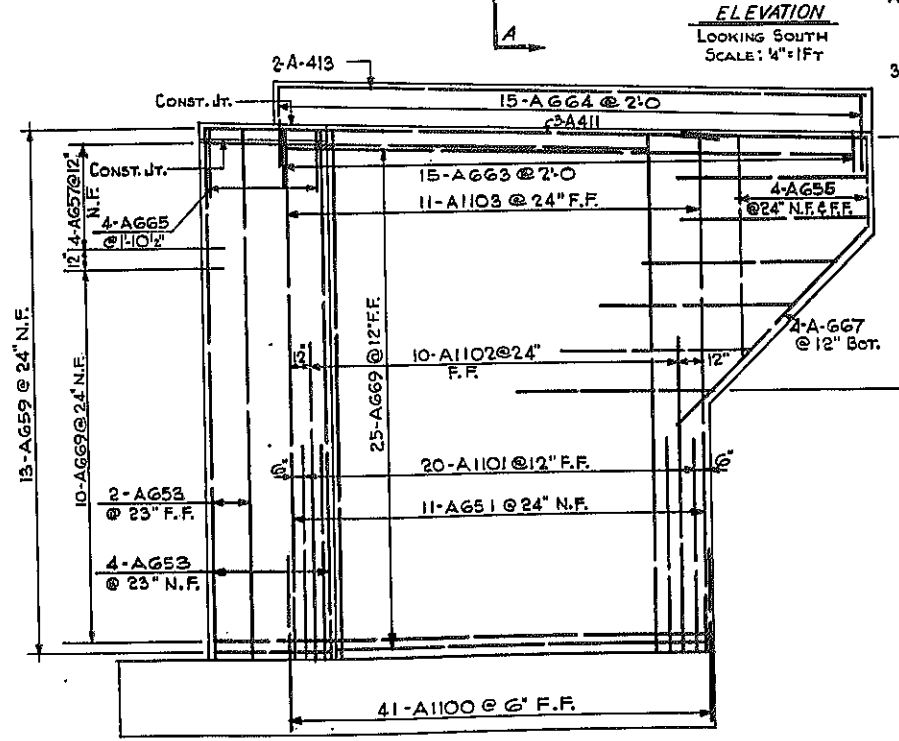


ELEVATION A-A
LOOKING SOUTH
SCALE: 1/4" = 1 FT.

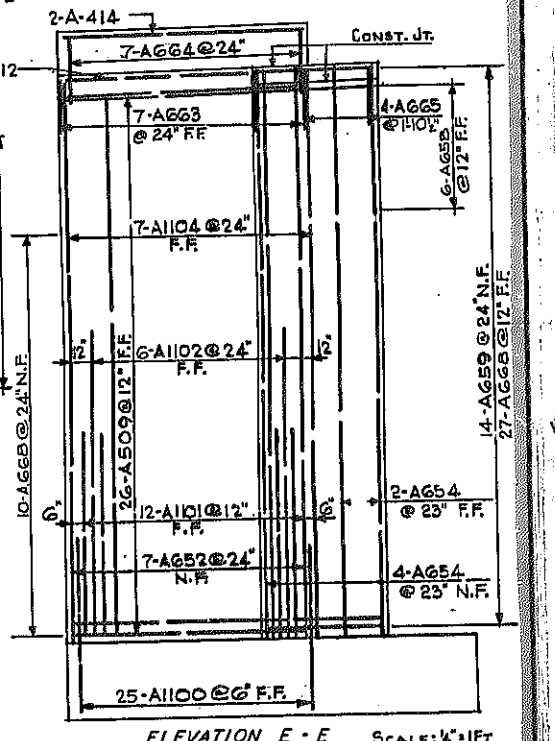


SECTION F-F
SCALE: 3/8" = 1 FT.

SECTION G-G
SCALE: 3/8" = 1 FT.



ELEVATION D-D
SCALE: 1/4" = 1 FT.



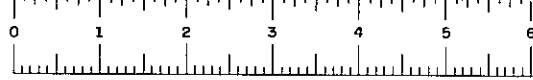
ELEVATION E-E
SCALE: 1/4" = 1 FT.

HUMBOLDT PKWY. N.B. OVER EXPW.
SOUTH ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REK. C.E.
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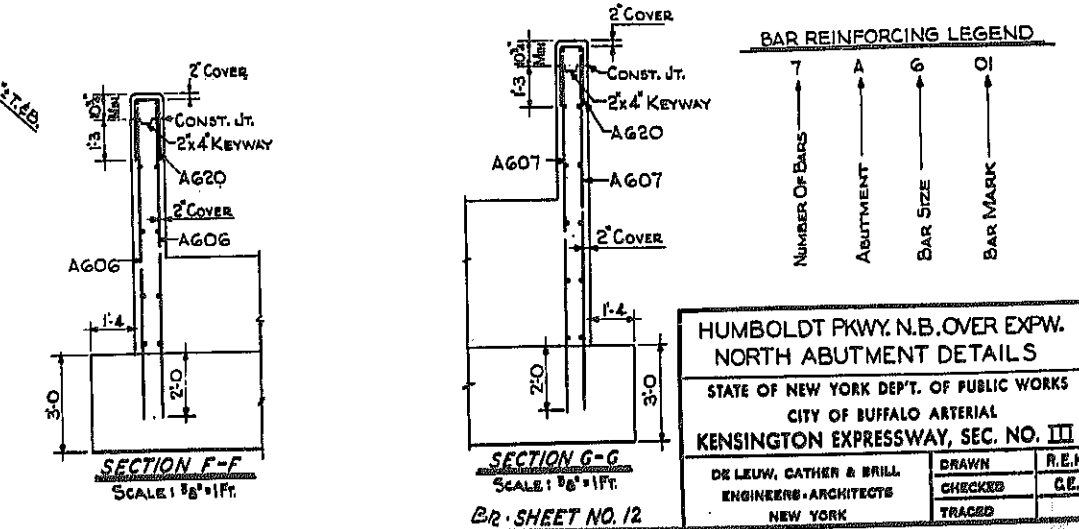
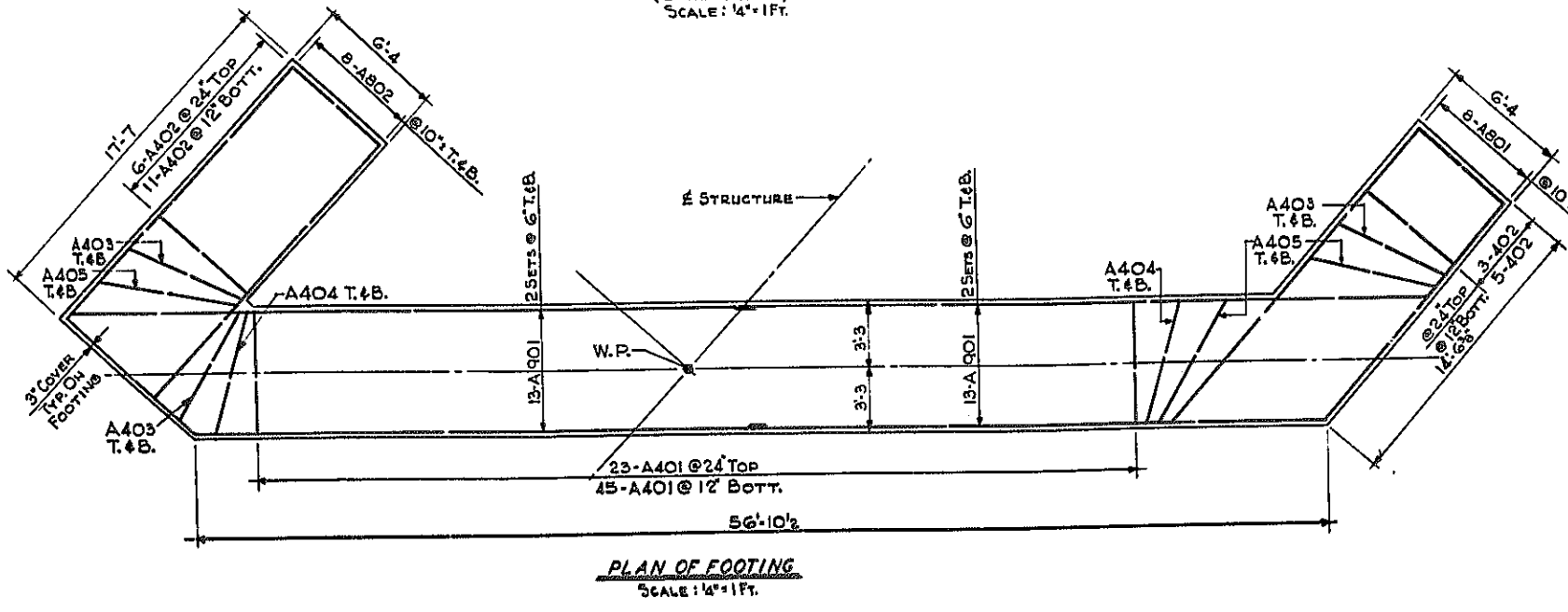
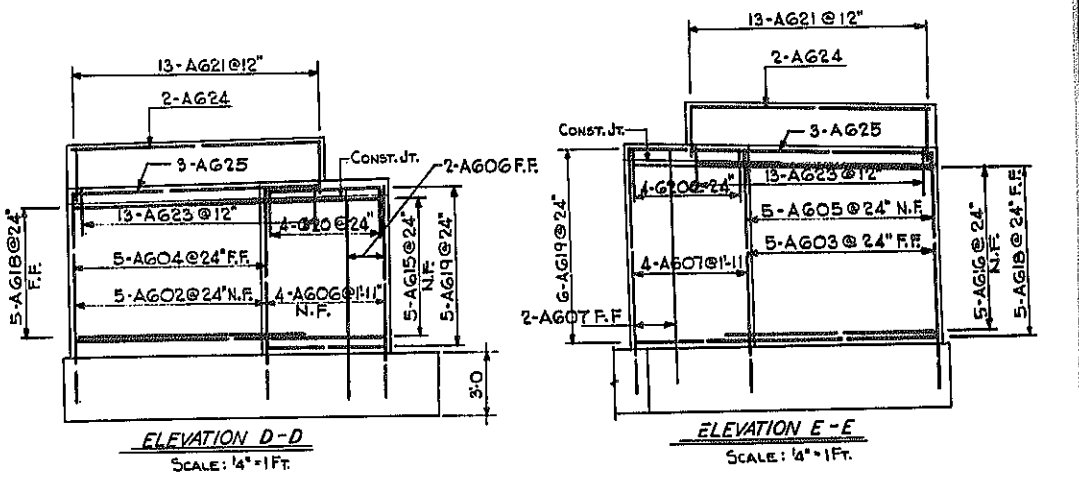
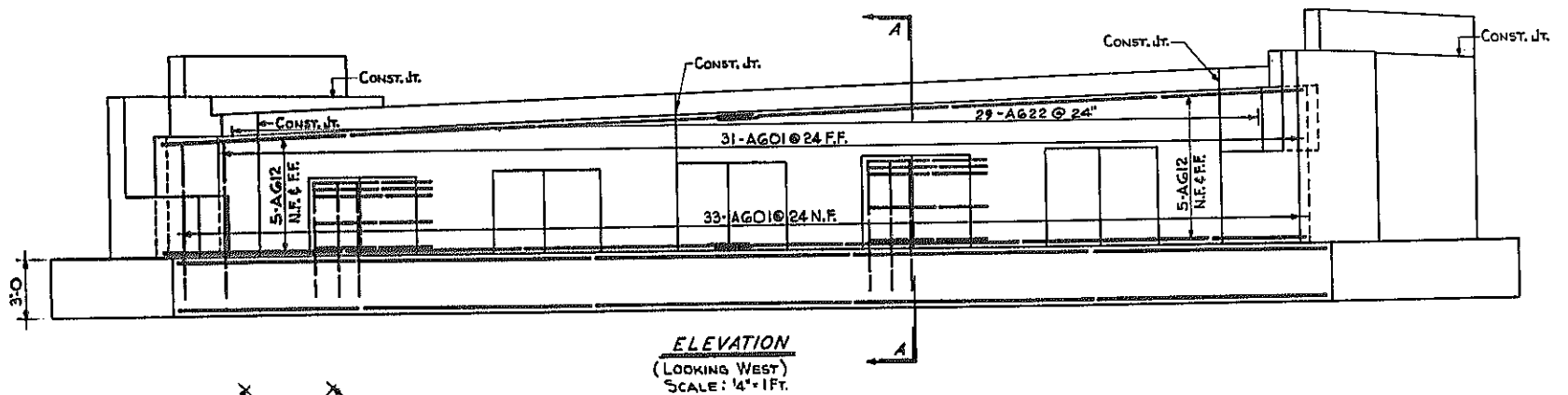
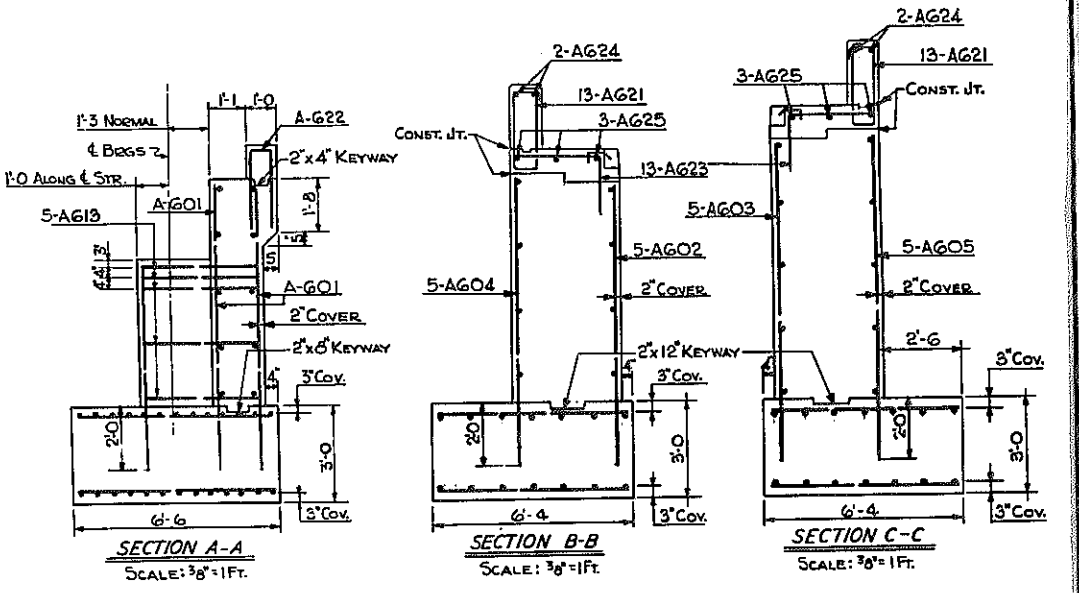
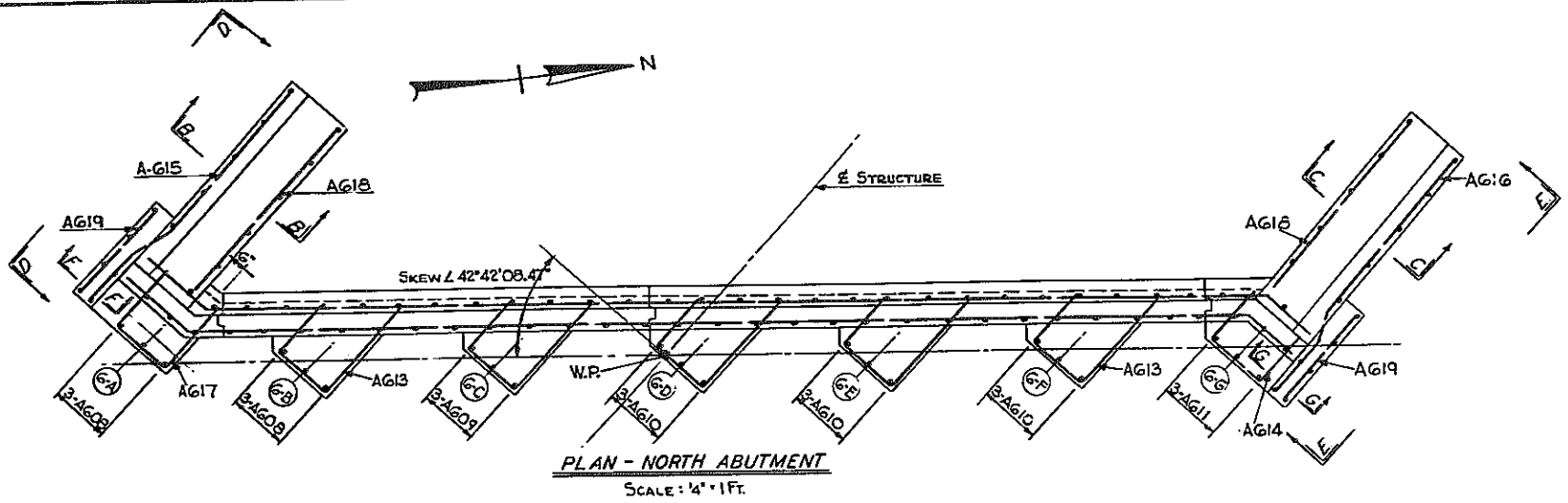
DR. SHEET NO 10



740 60-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	129	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 5 NORTHLAND AVENUE TO FILLMORE AVENUE

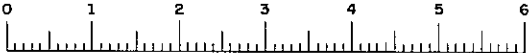


BAR REINFORCING LEGEND

NUMBER OF BARS	ABUTMENT	BAR SIZE	BAR MARK
7	A	G	O1

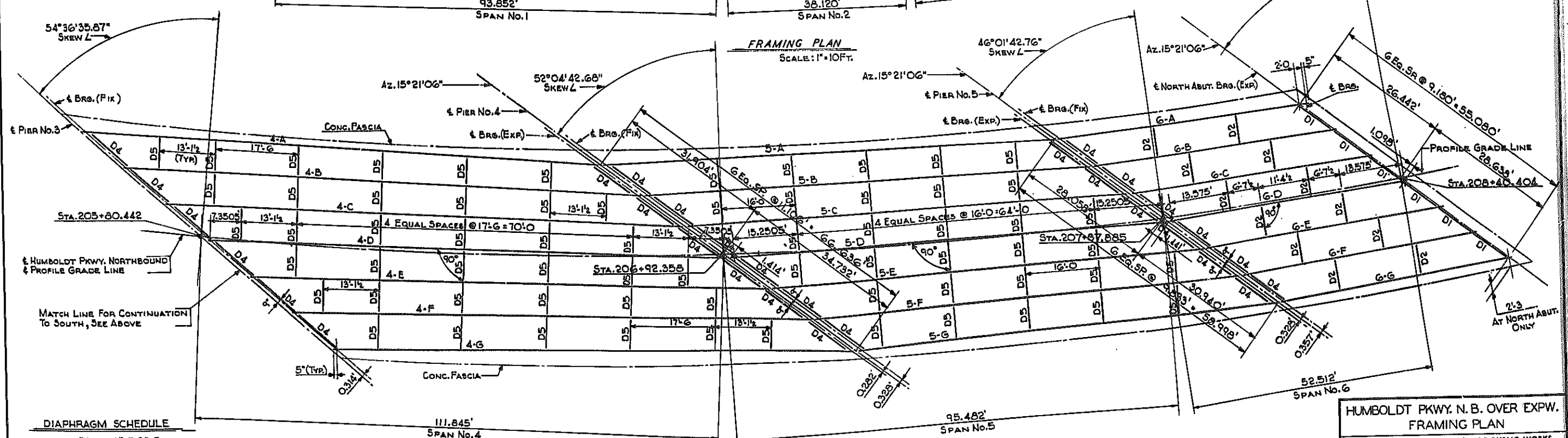
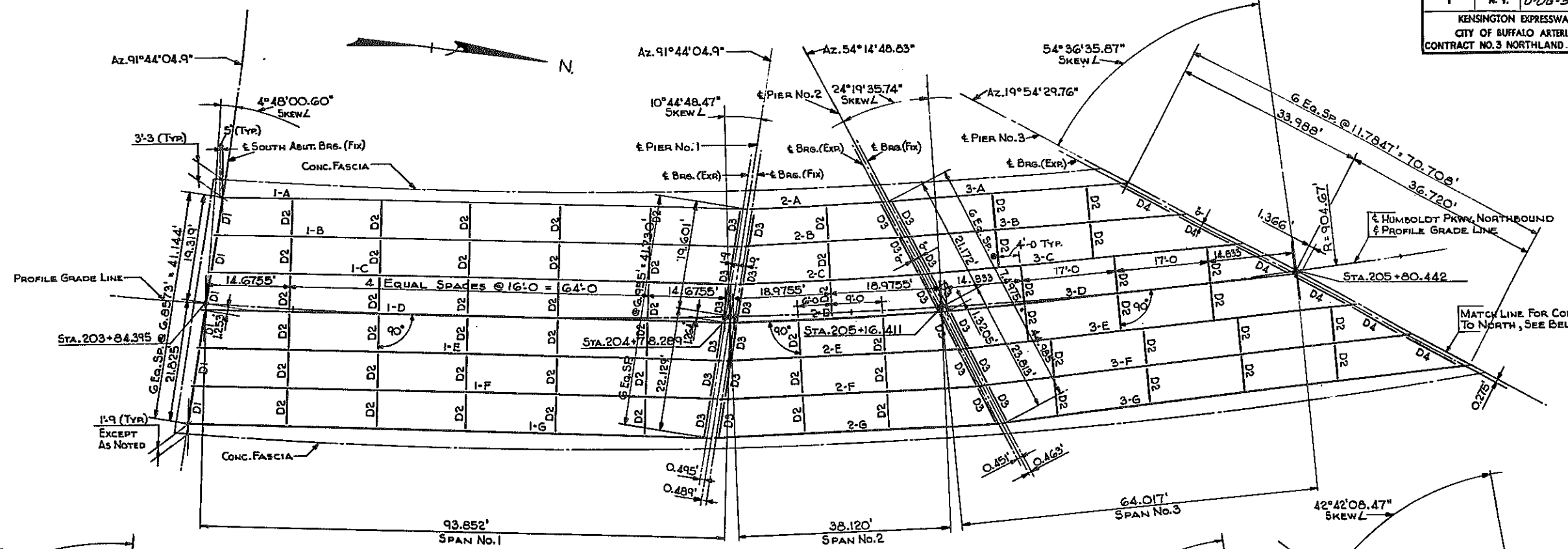
HUMBOLDT PKWY. N.B. OVER EXPW.
NORTH ABUTMENT DETAILS
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DR. LEUW, GATHER & BRILL
ENGINEERS-ARCHITECTS
NEW YORK
DRAWN: R.E.K.
CHECKED: G.E.
TRACED

BR SHEET NO. 12



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377(25)	124	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



DIAPHRAGM SCHEDULE

D1 = 15 E 33.9
D2 = 18 E 42.9

D3 = 16 W 36

END D4 = } BUILT-UP SECTION
INT. D5 = }

TOP & BOTTOM ST 8W 32
2- DIAGONALS = 2 L 6x4x3/8
GUSSET PL. 8x16x110

FRAMING PLAN SCALE: 1" = 10' FT.

HUMBOLDT PKWY. N. B. OVER EXPW.
FRAMING PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

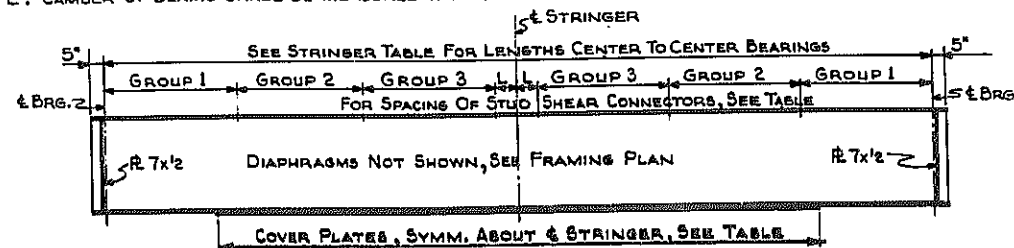
DE LEUW, CATHEN & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN: R.E.K.
CHECKED: C.E.
TRACES

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	125	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

STRINGER SCHEDULE												
MARK	STRINGER SIZE	LGT. FT. C. TO C. BRGS.	BOTTOM COVER PL.			VERT. CURV. TOTAL	STUD SHEAR CONNECTOR SPACING			L		
			SIZE	LENGTH	DL.		GROUP 1	GROUP 2	GROUP 3			
1-A	36WF245	93.391	20x1 1/4	66'-0"	3/16	5/8	3 1/16	31@6'-15"-6"	22@9'-16"-6"	13@13'-14"-1"	612	
1-B		93.377	20x1 1/4	66'-0"	3/8	9/16	3 1/16				608	
1-C		93.364	20x1 1/4	66'-0"	3/8	9/16	3 1/16				599	
1-D		93.351	20x1 1/4	66'-0"	3/8	9/16	3 1/16				592	
1-E		93.338	20x1 1/4	66'-0"	3/8	9/16	3 1/16				586	
1-F		93.325	20x1 1/4	66'-0"	3/8	9/16	3 1/16				579	
1-G		93.312	20x1 1/4	66'-0"	3/8	1/2	3/8				573	
2-A	36WF150	23.933	NONE		1/8	1/8	1/8	14@10'-11"-8"			300	
2-B		23.906	NONE		1/8	1/8	1/8	4@7'-2"-4"	14@10'-11"-8"		308	
2-C		23.278	NONE		1/8	1/8	3/16	8@7'-4"-8"	14@10'-11"-8"		306	
2-D		57.951	NONE		3/16	1/8	5/16	12@7'-7"-0"	14@10'-11"-8"		309	
2-E		42.623	NONE		5/16	1/8	7/16	5@5'-2"-3"-2"	12@7'-7"-0"	14@10'-11"-8"	353	
2-F		47.296	NONE		7/16	1/8	9/16	10@5'-4"-7"	12@7'-7"-0"	14@10'-11"-8"	398	
2-G		51.968	NONE		5/8	3/16	13/16	15@5'-6"-10"-2"	12@7'-7"-0"	14@10'-11"-8"	442	
3-A	36WF150	42.652	NONE		5/16	1/8	7/16	14@6'-7"-0"	11@8'-7"-4"	8@10'-6"-8"	326	
3-B		49.655	NONE		1/2	3/16	11/16	17@6'-8"-6"	14@8'-9"-4"	8@10'-6"-8"	327	
3-C		56.659	8x1 1/2	36'-0"	3/4	1/4	1	18@6'-9"-0"	15@8'-10'-0"	9@12'-9"-0"	329	
3-D		63.666	10x1 1/2	43'-0"	1 1/8	1/4	1 1/8	24@5'-11"-0"	14@9'-10'-6"	10@12'-10'-0"	333	
3-E		70.673	16x1 1/2	50'-0"	1 1/8	1/4	1 1/8	26@5'-11"-11"	16@9'-12'-0"	11@12'-11"-0"	420	
3-F	36WF170	77.681	16x1 1/2	57'-0"	2	3/8	2 3/8	28@5'-12"-0"	18@9'-13'-6"	12@12'-12"-0"	507	
3-G	36WF194	84.690	16x1 1/2	64'-0"	2 1/8	3/8	3 1/8	30@5'-13'-9"	20@9'-15'-0"	13@12'-13'-0"	599	
4-A		110.960			3	7/8	3 3/8	32@7'-2"-0"	17@12'-17'-0"	12@18'-18'-0"	480	
4-B		110.946			3 1/8	1 1/8	3 3/8				473	
4-C		110.943			3 1/8	1 1/8	3 3/8				471	
4-D		110.951			3 1/8	1 1/8	3 3/8				475	
4-E		110.970			3 1/8	3/4	3 3/8				485	
4-F		111.000			3 1/8	3/4	3 3/8				500	
4-G		111.041			3 1/8	3/4	3 3/8				520	
5-A		97.387			2 3/8	1 1/8	3 1/8	25@8'-16"-6"	17@11'-15"-7"	12@16'-16"-0"	444	
5-B		96.418			2 1/4	5/8	2 1/8	24@8'-16"-0"			426	
5-C		95.456			2 1/8	5/8	2 3/8	23@8'-15"-4"			411	
5-D		94.501			2 1/8	9/16	2 1/8	23@8'-15"-4"			334	
5-E		93.553			1 11/16	9/16	2 1/2	22@8'-14"-8"			526	
5-F		92.612			1 7/8	1/2	2 3/8	21@8'-14"-0"			723	
5-G		91.679			1 7/8	1/2	2 3/8	20@8'-13'-4"			923	
6-A	36WF150	55.144	8x1 1/2	28'-0"	3/4	3/16	1 1/16	21@6'-10"-6"	13@8'-8"-8"	8@12'-5'-0"	405	
6-B		52.684	NONE		1/8	3/16	3/8	18@6'-9'-0"			475	
6-C		52.227	NONE		5/8	3/16	1 1/16	18@6'-9'-0"			447	
6-D		51.775	NONE		5/8	3/16	1 1/16	17@6'-8'-6"			721	
6-E		51.327	NONE		9/16	3/16	3/4	17@6'-8'-6"			497	
6-F		50.884	NONE		9/16	3/16	3/4	16@6'-8'-0"			775	
6-G		50.444	NONE		9/16	3/16	3/4	16@6'-8'-0"			959	

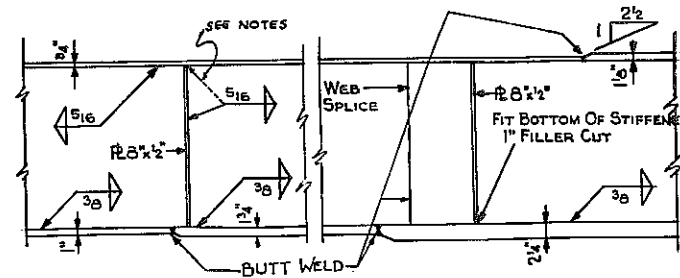
NOTE: CAMBER OF BEAMS SHALL BE MEASURED WITH BEAMS LYING ON THEIR SIDES.



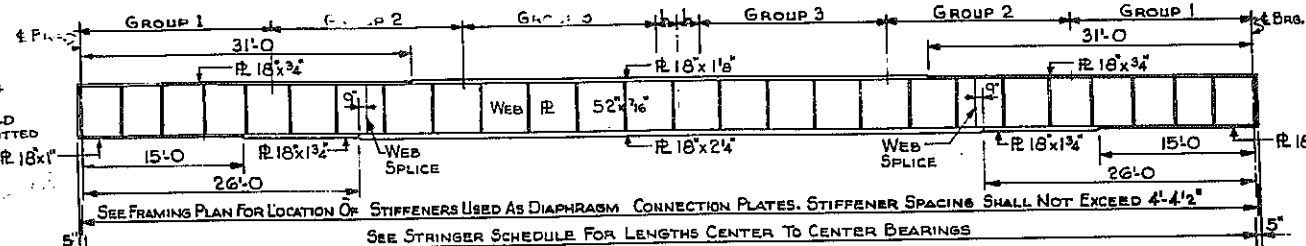
STRINGER DETAILS - SPANS 1, 2, 3, 6
NOT TO SCALE

INTERMEDIATE STIFFENERS
2-R 8x1/2" - ON INTERIOR STRINGERS
1-R 8x1/2" - ON INSIDE FACE OF EXTERIOR STRINGERS

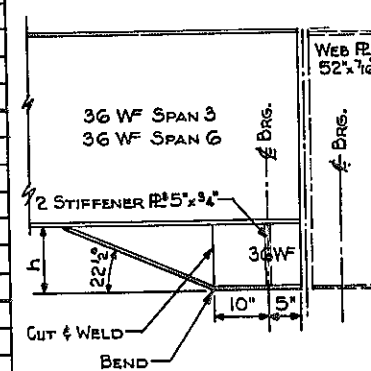
TYPICAL STRINGER DETAIL
SPAN NO. 4
NOT TO SCALE



END STIFFENER:
2-R 8x1/2" WITH TEE WELD AT BOTTOM FITTED AT TOP.

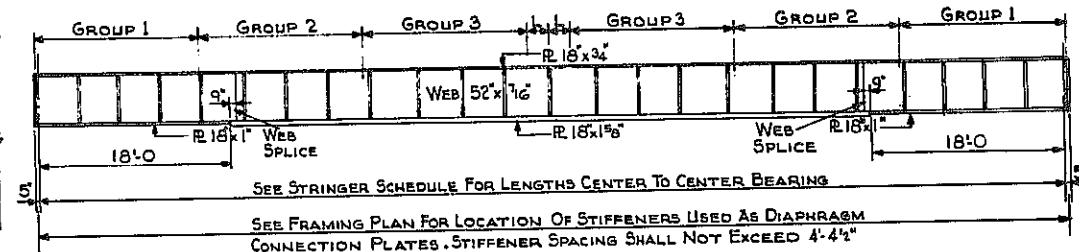


STRINGER DETAILS - SPAN NO. 4
NOT TO SCALE

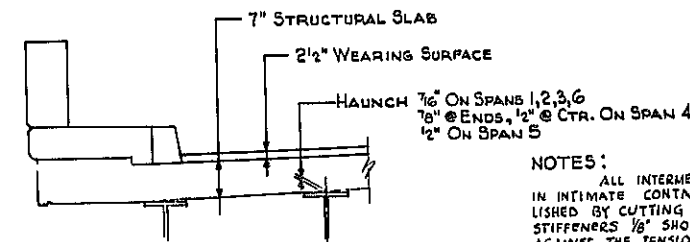


DETAIL FAIRING AT END STRINGERS
SPANS NO. 3 & NO. 6
SCALE: 3/4" = 1 FT.

SPAN	STRNG	FAIRING MAT'L	h
3	3-A	R 36WF150	16 3/4"
3	3-B	R 36WF150	16 3/4"
3	3-C	R 36WF150	16 3/4"
3	3-D	R 36WF150	16 3/4"
3	3-E	R 36WF150	16 3/4"
3	3-F	R 36WF170	16 7/16"
3	3-G	R 36WF194	16 1/8"
6	6-A	R 36WF150	19 3/8"
6	6-B	R 36WF150	19 3/8"
6	6-C	R 36WF150	19 3/8"
6	6-D	R 36WF150	19 3/8"
6	6-E	R 36WF150	19 3/8"
6	6-F	R 36WF150	19 3/8"
6	6-G	R 36WF150	19 3/8"



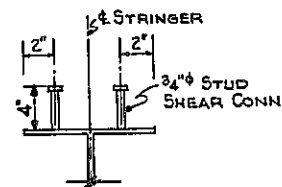
STRINGER DETAILS SPAN NO. 5
NOT TO SCALE



DIAGRAMMATIC SECTION
NOT TO SCALE

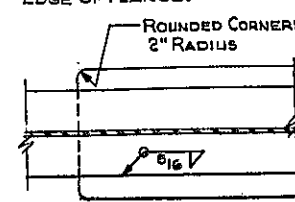
NOTE: SPECIAL PRECAUTION MUST BE EXERCISED WHERE WELDING CROSSES EDGE OF FLANGE TO AVOID ANY POSSIBILITY OF UNDERCUT OR "NICKS" ON EDGE OF FLANGE.

NOTES:
ALL INTERMEDIATE STIFFENERS USED IN PAIRS SHALL BE PLACED IN INTIMATE CONTACT WITH BOTH FLANGES. THIS MAY BE ACCOMPLISHED BY CUTTING THE STIFFENERS TO FIT OR BY CUTTING THE STIFFENERS 1/8" SHORT AND THEN PLACING THE STIFFENER TIGHTLY AGAINST THE TENSION FLANGE AND WELDING THE OPPOSITE END TO THE COMPRESSION FLANGE. WHEN INTERMEDIATE STIFFENERS USED IN PAIRS ARE CUT TO FIT THEY SHALL BE WELDED TO THE WEB ONLY.
ALL BEARING STIFFENERS SHALL BE USED IN PAIRS (ONE ON EACH SIDE OF WEB). BEARING STIFFENERS SHALL BE WELDED TO WEB AND TO THE FLANGE AT THE BEARING END OF THE STIFFENER. THE WELD CONNECTING THE BEARING STIFFENER TO THE FLANGE SHALL FULLY DEVELOPE THE BEARING STIFFENER. THE OTHER END OF THE STIFFENER SHALL BE TIGHTLY FITTED TO THE FLANGE.
ALL INTERMEDIATE STIFFENERS USED SINGLY SHALL BE PLACED TIGHTLY AGAINST THE TENSION FLANGE AND WELDED TO THE COMPRESSION FLANGE AND WEB.



DETAIL OF STUD SHEAR CONNECTOR
SCALE: 1 1/2" = 1 FT.

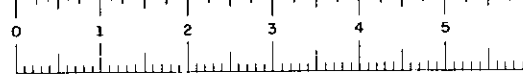
NOTE: FOR ALTERNATE DETAIL FOR SPIRAL SHEAR CONNECTOR, SEE SHEET NO. 105.



COVER PLATE DETAIL
SCALE: 1" = 1 FT.

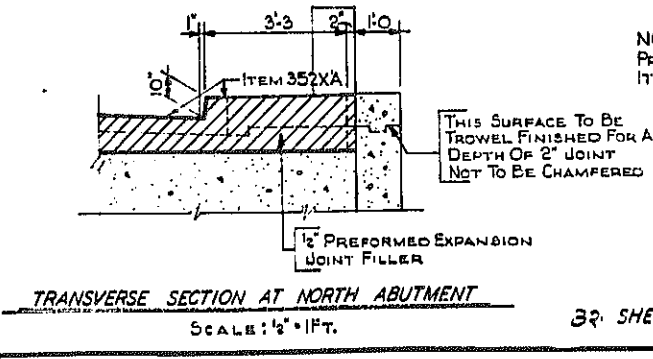
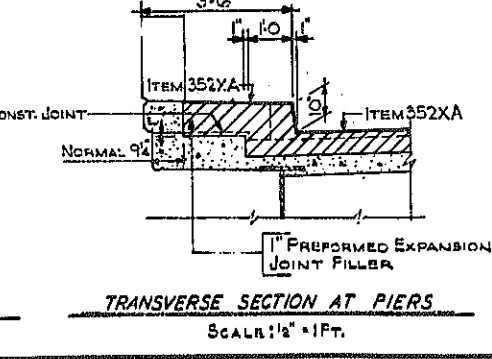
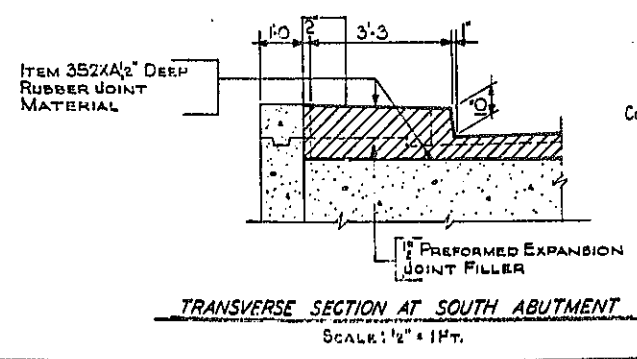
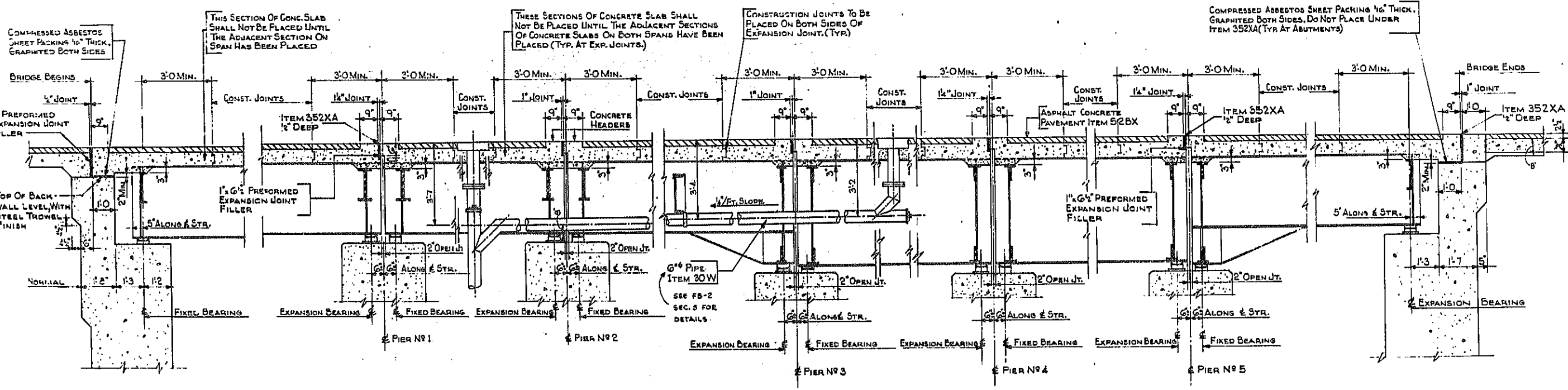
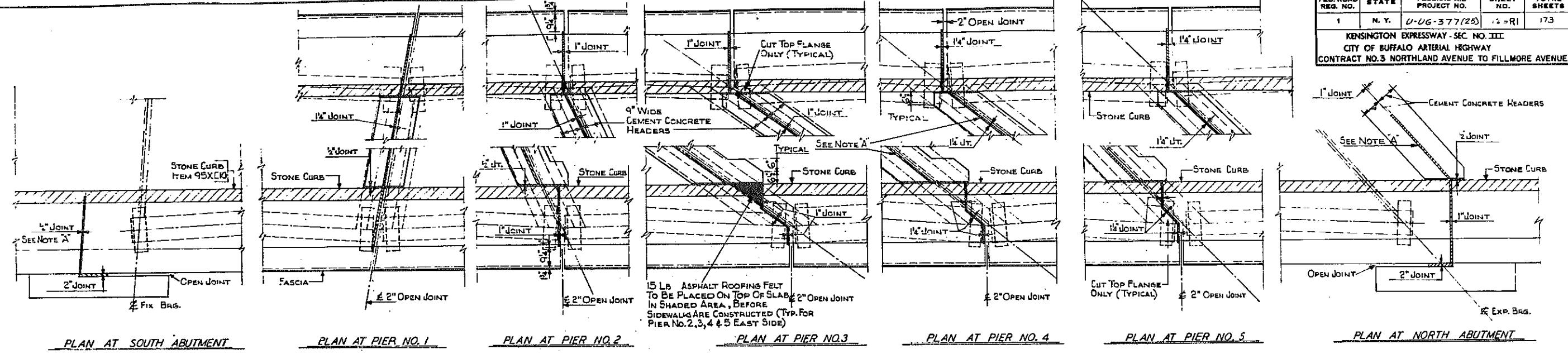
B2. SHEET NO. 14

HUMBOLDT PKWY. N.B. OVER EXPW. STRINGERS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, GATHEN & BULL	DRAWN	R.E.K.	
ENGINEERS-ARCHITECTS	CHECKED	G.E.	
NEW YORK	TRACED		



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	1291	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



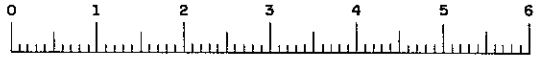
NOTE "A"
PREFORMED EXPANSION JOINT FILLER, TOPPED WITH ITEM 352XA, 1/2" DEEP.
REVISION TO APPR. SLAB DEPTH

THIS SURFACE TO BE TROWEL FINISHED FOR A DEPTH OF 2" JOINT NOT TO BE CHAMFERED

HUMBOLDT PKWY. N. B. OVER EXPW.
LONGITUDINAL SECTION & DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

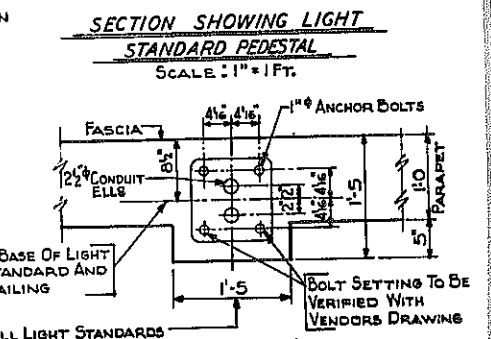
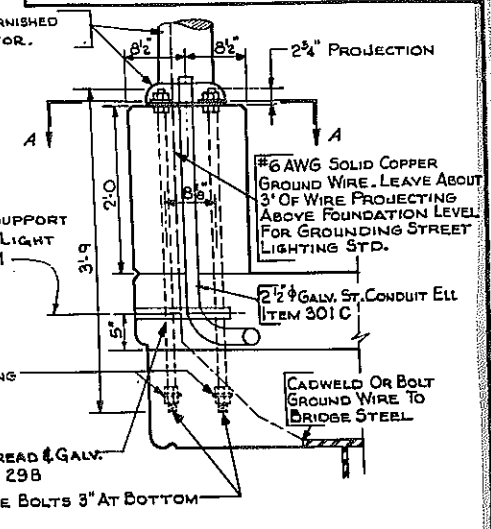
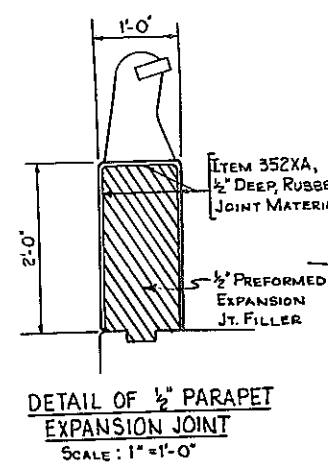
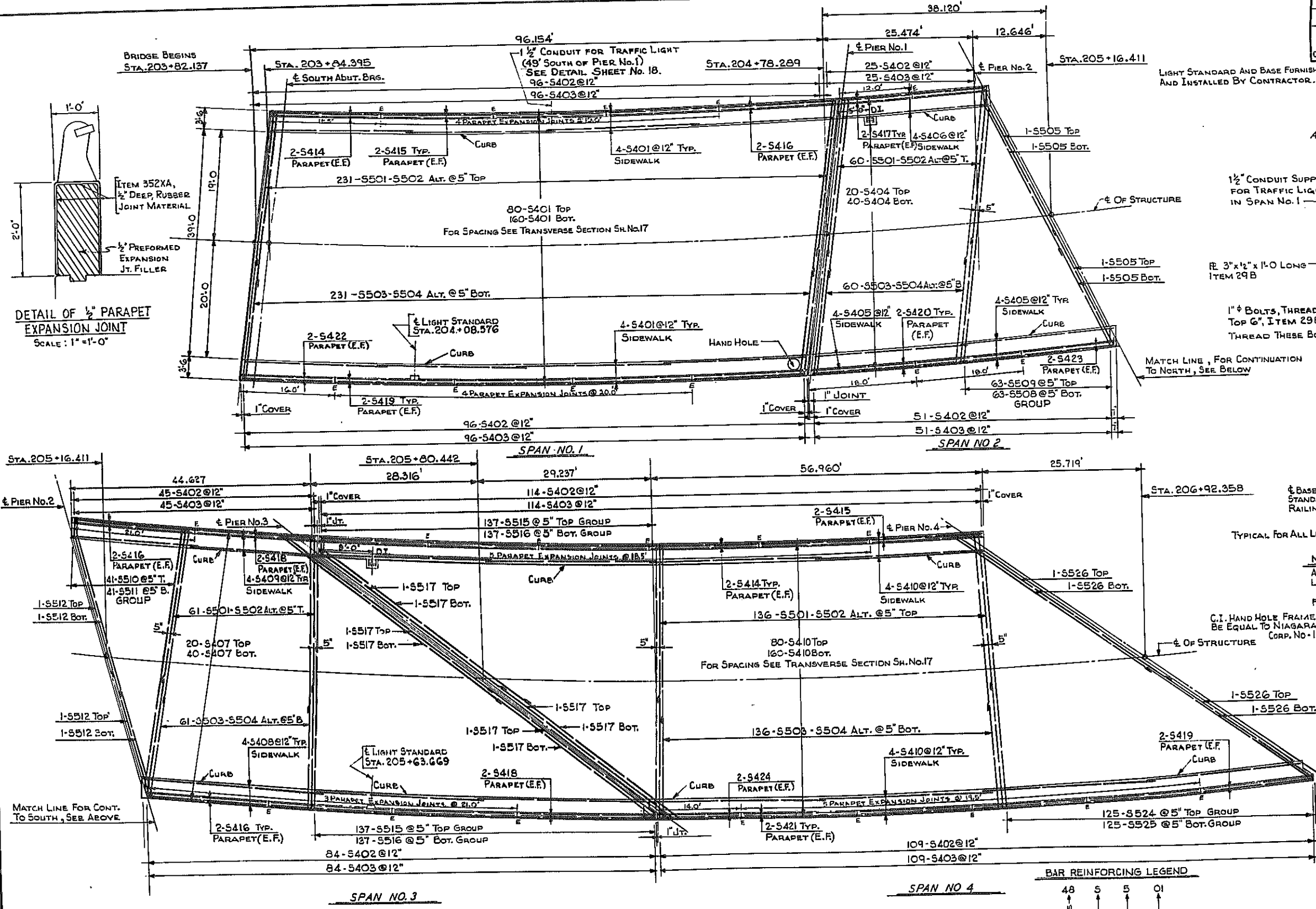
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FAC 68-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	127	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



BAR REINFORCING LEGEND

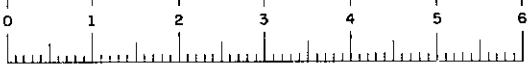
48	S	S	O1
↑	↑	↑	↑
NUMBER OF BARS	SLAB	BAR SIZE	BAR MARK

B2 SHEET NO. 16

HUMBOLDT PKWY. N.B. OVER EXPW. DECK PLAN - SPANS 1-4

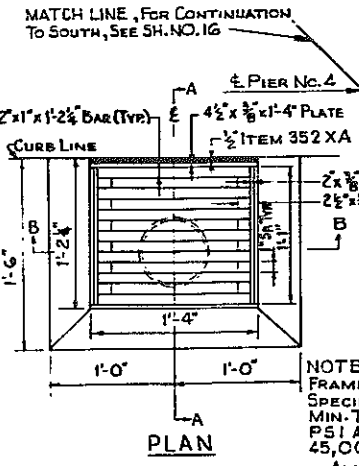
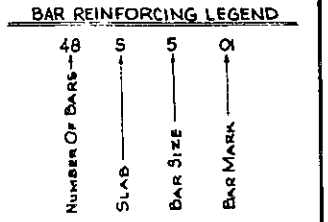
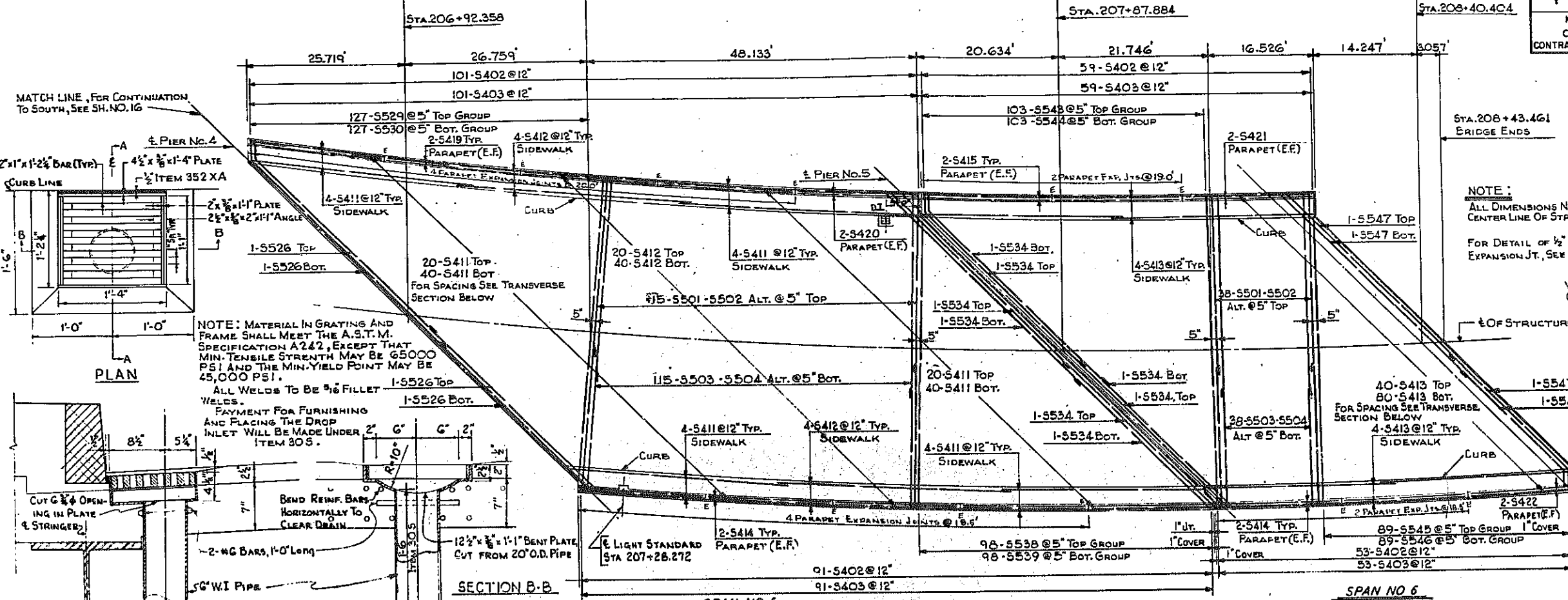
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

DR. LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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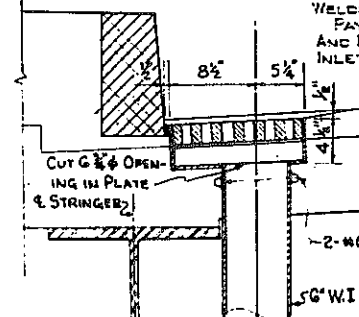


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377 (25)	128R1	173

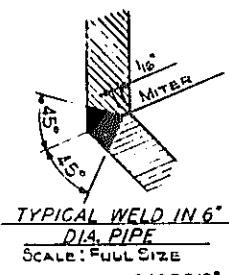
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



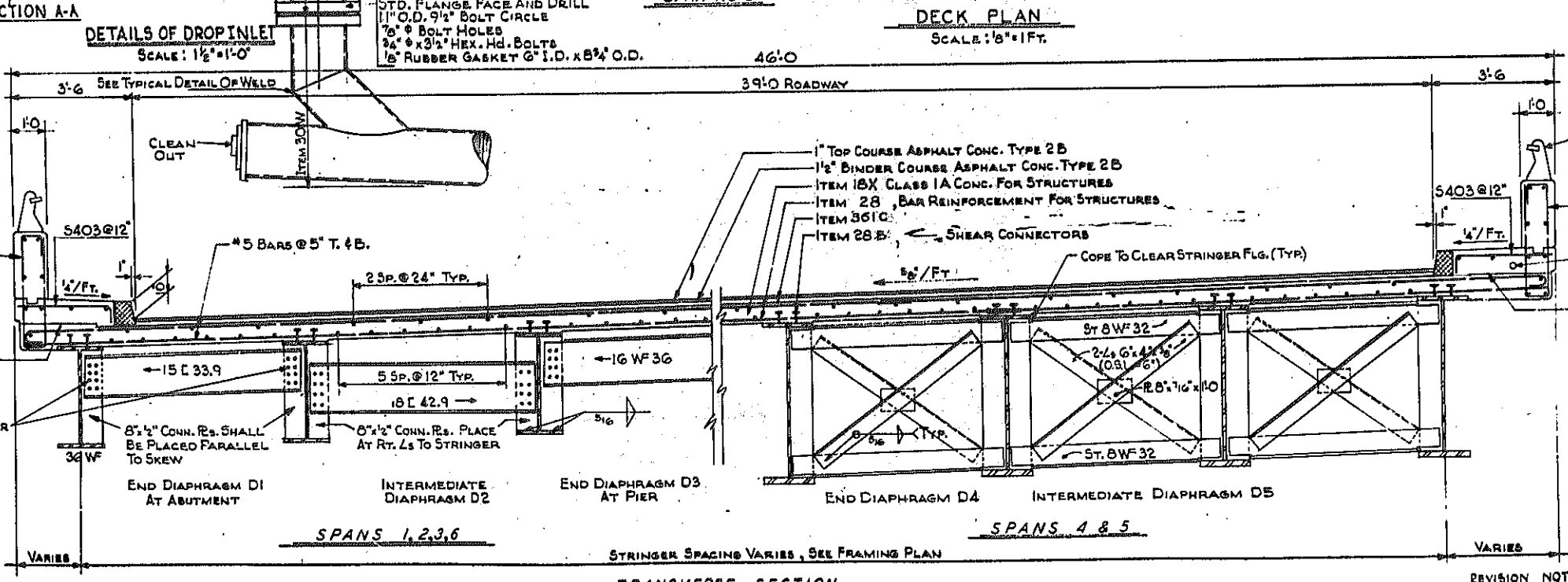
NOTE: MATERIAL IN GRATING AND FRAME SHALL MEET THE A.S.T.M. SPECIFICATION A242, EXCEPT THAT MIN. TENSILE STRENGTH MAY BE 65000 PSI AND THE MIN. YIELD POINT MAY BE 45,000 PSI.
ALL WELDS TO BE 3/8" FILLET WELDS.
PAYMENT FOR FURNISHING AND PLACING THE DROP INLET WILL BE MADE UNDER ITEM 305.



DETAILS OF DROP INLET
SCALE: 1/2" = 1'-0"

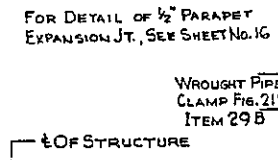


TYPICAL WELD IN 6" DIA. PIPE
SCALE: FULL SIZE

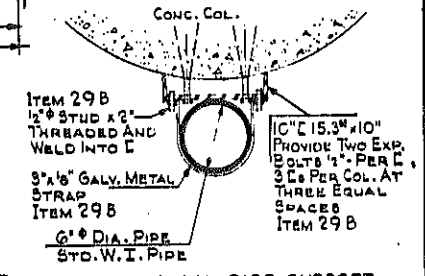


TRANSVERSE SECTION
SCALE: 1/2" = 1'-0"

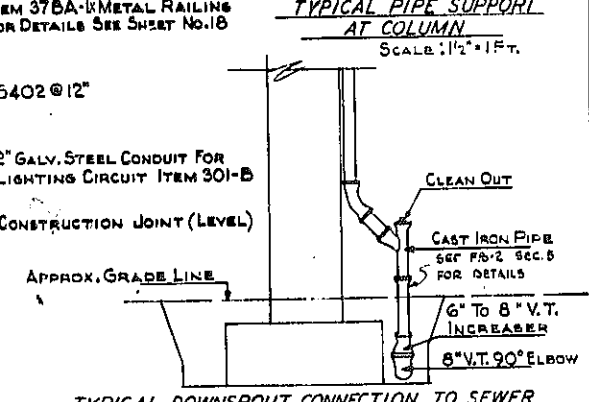
NOTE: ALL DIMENSIONS NOTED ARE TAKEN ALONG CENTER LINE OF STRUCTURE.



TYPICAL PIPE HANGER
SCALE: 1" = 1'-0"



TYPICAL PIPE SUPPORT AT COLUMN
SCALE: 1/2" = 1'-0"



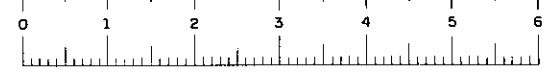
TYPICAL DOWNSPOUT CONNECTION TO SEWER
SCALE: 1/4" = 1'-0"

HUMBOLDT PKWY. N.B. OVER EXPW.
DECK PLAN AND SECTION

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

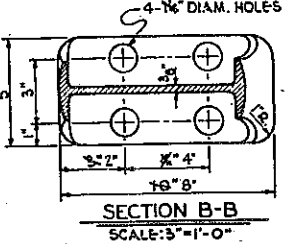
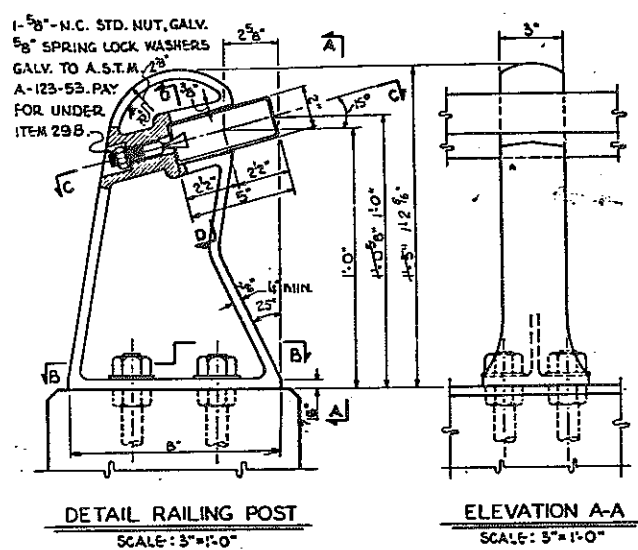
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REK C.E.
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REVISION NOTE ADDED
SHEET NO. 17



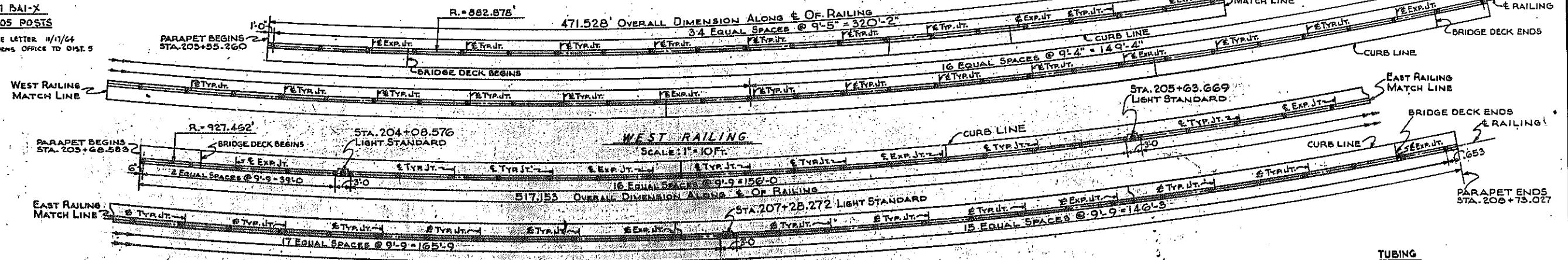
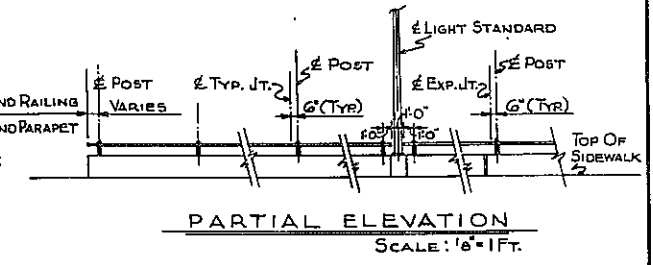
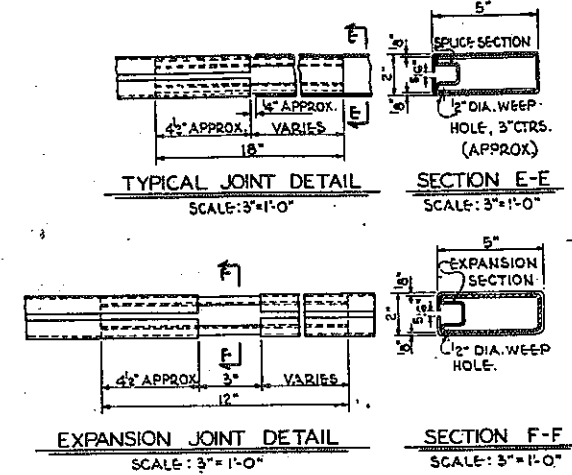
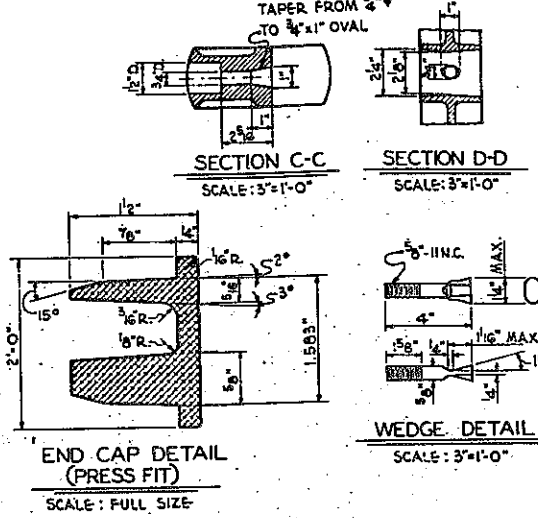
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	129 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

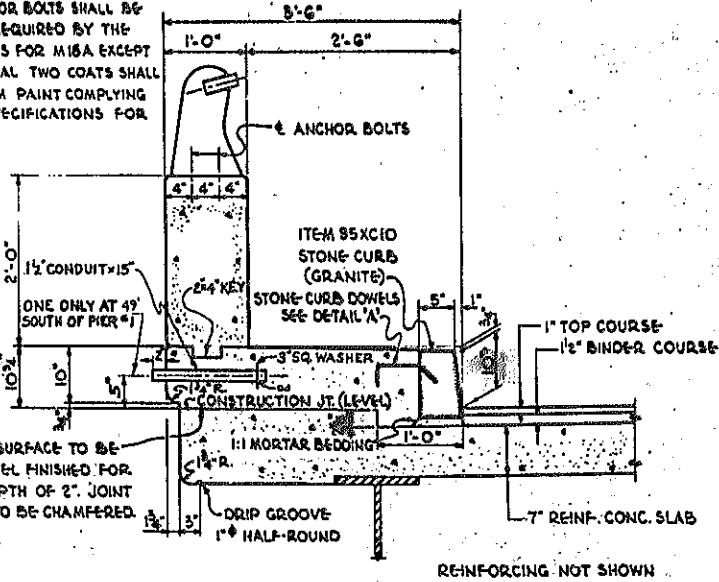


4- 1" DIA. x 24" LG. GALV. PLAIN RODS TH'D 8" EACH END WITH 1" THD. 16-1" A.S.H. GALV. HEX. NUTS, BOLTS & NUTS GALVANIZED TO A.S.T.M. A-153.
4- GALV. ROD WASHERS 2 1/2" O.D. x 1/4" THICK GALV. TO A.S.T.M. A-123-53.

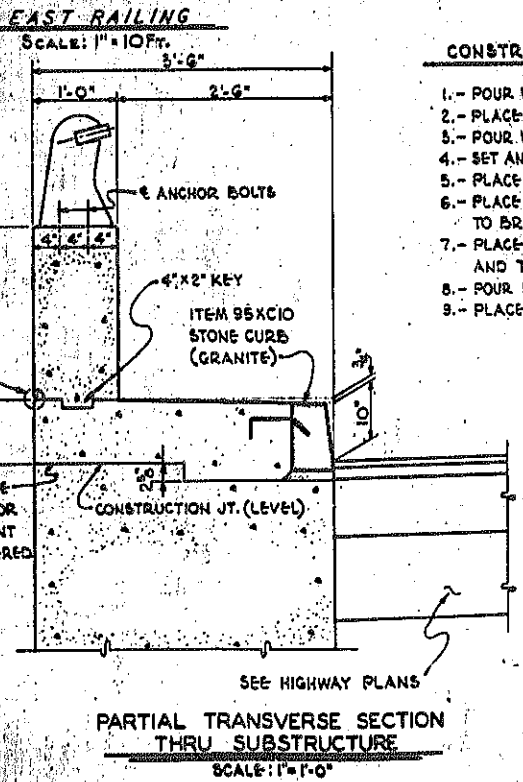
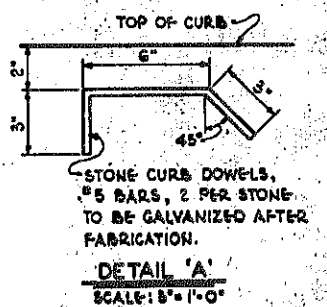
POST
MALLEABLE IRON CASTINGS A.S.T.M. A-47, GRADE 35018.
GALVANIZED TO A.S.T.M. A-123-53.



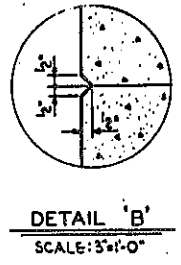
NOTE:
ANCHOR BOLTS SHALL BE CUT OFF 1/2" ABOVE NUT AND PRESSED, THE CUT TOPS OF ANCHOR BOLTS SHALL BE PAINTED AS REQUIRED BY THE SPECIFICATIONS FOR M18A EXCEPT THAT THE FINAL TWO COATS SHALL BE ALUMINUM PAINT COMPLYING WITH THE SPECIFICATIONS FOR M25.



- CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE**
- 1.- POUR SLAB & BROOM FINISH TOP OF SLAB BETWEEN CURBS.
 - 2.- PLACE ITEM 85XC10 STONE CURB (GRANITE).
 - 3.- POUR SIDEWALK TO PROPER LINE AND GRADE.
 - 4.- SET ANCHOR BOLTS BY MEANS OF A TEMPLATE.
 - 5.- PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 - 6.- PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 - 7.- PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON WASHERS.
 - 8.- POUR PARAPET TO PROPER LINE AND GRADE.
 - 9.- BROOMED SURFACE OF SLAB TO BE GIVEN A WATERPROOFING, ITEM 3610.
 - 10.- PLACE ROADWAY PAVEMENT.



- CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE**
- 1.- POUR WALL UP TO CONSTRUCTION JOINT.
 - 2.- PLACE STONE CURB (GRANITE)
 - 3.- POUR WALL ABOVE CONSTRUCTION JOINT.
 - 4.- SET ANCHOR BOLTS BY MEANS OF A TEMPLATE.
 - 5.- PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 - 6.- PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 - 7.- PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON WASHERS.
 - 8.- POUR PARAPET TO PROPER LINE AND GRADE.
 - 9.- PLACE ROADWAY PAVEMENT.



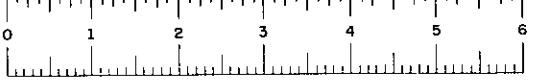
- RAILING NOTES:**
- 1.- ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE CONCENTRIC TO THE TOP OF THE PARAPET AND SO THAT THE POSTS ARE TRULY VERTICAL.
 - 2.- SINCE THE FINISHED RAILINGS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT, GRADE AND VERTICALITY OF POST TO THE FULL SATISFACTION OF THE ENGINEER IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION IS PERFORMED.
 - 3.- TUBULAR RAILS, MALLEABLE IRON POST, INSERTS, WEDGES AND END CAPS WILL BE PAID FOR UNDER ITEM 37BA-1.
 - 4.- ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
 - 5.- ALL SHIMS REQUIRED TO ALIGN THE POSTS SHALL BE PAID FOR UNDER ITEM 37BA-1 ALL SHIMS TO BE GALVANIZED TO A.S.T.M. A-153.

HUMBOLDT PKWY. N. B. OVER EXPW.
RAILING LAYOUT

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	R.E.K.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.E.K.

REVISION TO DETAIL RAILING POST
82, SHEET NO. 18



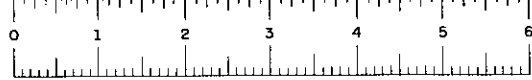
BAR LIST

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	130 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

ABUTMENTS (2)									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
NORTH ABUTMENT - FOOTING									
A401	#4	6'-0"	STR.	68				TRANSVERSE BARS	
A402		5'-10"		25				TRANSVERSE BARS WINGWALL	
A403		6'-2"		6				TRANSVERSE BARS	
A404		6'-4"		4				TRANSVERSE BARS	
A405		7'-0"		6				TRANSVERSE BARS	
A801	#8	16'-4"		16				LONG. BARS E. & W.W. VARIES 14'-0" TO 19'-2"	
A802	#8	17'-1"		16				LONGITUDINAL BARS	
A901	#9	33'-5"		52				LONG. BARS VARIES 29'-5" TO 35'-1"	
NORTH ABUTMENT - WALL									
A601	#6	8'-7"	STR.	64				VERT. BARS VARIES 7'-10" TO 9'-4" - ABUT.	
A602		9'-2"		5				VERTICAL BARS W. W.W.	
A603		10'-4"		5				" " " E. W.W.	
A604		9'-2"		5				" " " W. W.W.	
A605		10'-4"		5				" " " E. W.W.	
A606		10'-1"		6				" " " W. W.W.	
A607		11'-6"		6				" " " E. W.W.	
A608		4'-11"		6				" " " PEDESTAL	
A609		5'-5"		3				" " " "	
A610		5'-11"		9				" " " "	
A611		6'-5"		3				" " " "	
A612		30'-2"		20				HORIZONTAL BARS ABUTMENT	
A613		11'-6"	I	25	3'-2"	3'-0"	5'-6"	HORIZONTAL BARS PEDESTAL	
A614		9'-0"	II	5	3'-2"	3'-0"		" " " " "	
A615		14'-8"	III	6	9'-4"	2'-0"	3'-4"	" " " W. W.W.	
A616		14'-10"	III	6	9'-6"	2'-0"	3'-4"	" " " E. W.W.	
A617		11'-0"	II	5	3'-2"	4'-0"		" " " PEDESTAL	
A618		11'-10"	STR.	10				" " " E. & W. W.W.	
A619		5'-8"	STR.	10				" " " "	
A620		4'-6"	II	8	8"	2'-0"		" " " U BARS CURTAIN WALL	
A621		7'-5"	IV	26	8"	2'-6"		" " " U BARS PARAPET E. & W. W.W.	
A622		5'-2"	II	29	8"	2'-4"		" " " U BARS CONC. HEADER	
A623		4'-5"	V	26	2'-8"	1'-10"		" " " CURB BARS	
A624		12'-0"	STR.	4				" " " HORIZONTAL BARS E. & W. PARAPET	
A625		11'-10"	STR.	6				" " " " " W.W.	
SOUTH ABUTMENT - FOOTING									
A500	#5	24'-2"	STR.	30				LONGITUDINAL BARS ABUTMENT	
A501		12'-0"		14				" " " W. W.W.	
A502		27'-9"		14				" " " ABUTMENT	
A503		19'-7"		3				" " " W. W.W.	
A504		23'-1"		4				" " " "	
A505		11'-4"		3				" " " E. W.W.	
A506		14'-10"		4				" " " "	
A910	#9	15'-4"		25				TRANSVERSE ABUTMENT	
A911	#9	14'-0"		96				TRANSVERSE ABUT. & W.W.	
A1100	#11	15'-0"	STR.	159	7'-1"	1'-7"	6'-4"	DOWELS	
SOUTH ABUTMENT - WALL									
A411	#4	27'-9"	STR.	3				LONGITUDINAL BARS W. W.W.	
A412		11'-8"		3				" " " E. W.W.	
A413		27'-9"		2				" " " PARAPET W. W.W.	
A414		11'-8"		2				" " " " " E. W.W.	
A415		23'-5"		4				" " " Abut. Backwall	
A650	#6	18'-6"		24				VERTICAL BARS ABUTMENT	
A651		24'-2"		11				" " " W. W.W.	
A652		25'-4"		7				" " " E. W.W.	
A653		25'-0"		6				" " " W. CURTAIN WALL	
A654		26'-4"		6				" " " E. " "	
A655		7'-8"		8				" " " W. W.W. VAR. 10'-6" TO 4'-6"	
A656		22'-9"		56				HORIZONTAL BARS ABUTMENT	
A657		23'-2"	III	4	4'-2"	1'-0"	18'-0"	" " " W. W.W.	
A658		14'-8"	III	6	4'-2"	1'-0"	9'-6"	" " " E. W.W.	
A659		5'-8"	STR.	27				" " " E. & W. CURTAIN WALL	
A660		8'-8"		14				" " " W. W.W.	
A661		5'-0"		21				VERTICAL BARS PEDESTALS	
A662		11'-0"	II	26	3'-8"	3'-9"		HORIZONTAL BARS PEDESTALS	
A663		5'-4"	V	22	2'-8"	2'-9"		SIDEWALK BARS E. & W. W.W.	
A664		7'-10"	II	22	8"	3'-8"		U BARS PARAPET E. & W. W.W.	
A665		4'-6"	II	8	8"	2'-0"		U BARS CURTAIN WALLS	
A666		16'-10"	II	24	1'-4"	7'-10"		U BARS CONC. HEADER	
A667		12'-10"	STR.	4				LONGITUDINAL BARS W. W.W.	
A1101	#11	10'-0"		78				VERTICAL BARS ABUT. & W.W.	
A1102		14'-6"		39				" " " "	
A1103		24'-4"		11				" " " W. W.W.	
A1104		25'-6"		17				" " " E. W.W. & DR. SEAT	
A1105		18'-10"		24				" " " ABUTMENT	
A668	#6	11'-6"	STR.	37				HORIZONTAL BARS E. W.W.	
A669	#6	15'-8"	STR.	35				" " " W. W.W.	
A670	#8	3'-7"	STR.	14				" " " U BARS CURTAIN WALL	

PIERS (5)															
MARK	SIZE	LEN.	TYPE	NUMBER						A	B	C	DESCRIPTION		
				PIER 1	PIER 2	PIER 3	PIER 4	PIER 5	TOTAL						
FOOTINGS															
P601	#6	7'-0"	STR.	60	60					120					HORIZONTAL BARS
P602	#6	8'-6"	STR.			156				312					" " "
P603	#6	9'-6"	STR.				112			112					" " "
P901	#9	5'-11"	X			28				28	4'-8"	1'-3"	1'-11"		DOWELS - INT. COLS.
P1001	#10	6'-3"	X	66	66					132	4'-10"	1'-5"	1'-10 1/2"		" " "
P1002	#10	6'-9"	X				88			88	5'-4"	1'-5"	1'-10 1/2"		" " "
P1101	#11	6'-8"	X			48				48	5'-1"	1'-7"	1'-2"		" - EXT. COLS.
COLUMNS & BEAMS															
P401	#4	10'-9"	VII	55	57	76	140	62	390	3'-2"					COLUMN TIES
P402	#4	3'-0"	XI	68	68	68	68	68	340	2'-6"	6"				CAP PEDESTALS - DOWELS
P501	#5	41'-9"	STR.	4					4						HORIZONTAL BARS SIDES
P502		45'-0"			4				4						" " "
P503		25'-10"				8			8						" " "
P504		33'-10"					8		8						" " "
P505		30'-0"						8	8						" " "
P506		7'-1"	IX	8	8	8	8	8	40	1'-1"	4'-11"	1'-1"			FASCIA ENDS
P507		(11'-1")	XIV	84	76	4			164	3'-2"	2'-2 1/2"	4'-1/2"			CAP STIRRUPS
P508		10'-9"	XIV		4				4	3'-0"	2'-2 1/2"	4'-1/2"			" " "
P509		10'-6"	XIV		4				4	2'-8 1/2"	2'-2 1/2"	4'-1/2"			" " "
P510		12'-1"	XIV			120	120	92	332	3'-8"	2'-2 1/2"	4'-1/2"			" " "
P511	#5	11'-8"	XIV			4			4	3'-5 1/2"	2'-2 1/2"	4'-1/2"			" " "
P512		11'-11"	XIV			4			4	3'-7"	2'-2 1/2"	4'-1/2"			" " "
P513		11'-5"	XIV			4			4	3'-4"	2'-2 1/2"	4'-1/2"			" " "
P514		10'-11"	XIV			4			4	3'-1"	2'-2 1/2"	4'-1/2"			" " "
P515		(11'-6")	XIV			4			4	3'-4 1/2"	2'-2 1/2"	4'-1/2"			" " "
P604	#6	12'-0"	XII	6	6	6	6	6	30	1'-10"	3'-2"	4'-10 1/2"			HORIZONTAL BARS FASCIA PEDESTALS
P605	#6	15'-4"	XIII	8	15	15	15	15	75	3'-8"	3'-2"	1'-0 1/2"			" " " INT. PEDESTALS
P801	#8	23'-5"	VIII	8					8	1'-2 1/2"	20'-7"	6'-1/2"			HORIZONTAL BARS BOTTOM
P802		17'-6"	STR.	12					12						" " "
P803		23'-7"	VIII		8				8	1'-7"	22'-0"	7'-1/2"			" " "
P804		37'-6"	STR.	5					5						" " "
P902	#9	21'-0"				14			14						VERTICAL BARS INT. COL.
P903		20'-4"				14			14						" " " " " "
P904		17'-2"						14	14						" " " " " "
P905		16'-8"						14	14						" " " " " "
P906		47'-0"	VIII		9				9	1'-9"	45'-3"	9"			HORIZONTAL BARS BOTTOM
P907		26'-0"	VIII		9				9	1'-9"	24'-3"	9"			" " " " " "
P908		13'-4"	STR.		8				8						" " " " " " & TOP
P909		44'-4"	VIII			7			7	1'-7"	42'-9"	8'-1/2"			" " " " " "
P910		24'-7"	VIII			7			7	1'-7"	23'-0"	8'-1/2"			" " " " " "
P911		21'-9"	STR.			2			2						" " " " " "
P912		14'-10"						10	10						" " " " " "
P913		10'-10"						2	2						" " " " " "
P914		33'-10"				10			10						" " " " " TOP
P915		13'-0"				10			10						" " " " " "
P916		36'-5"				10			10						" " " " " "
P917		13'-9"				10			10						" " " " " "
P918		38'-11"					9		9						" " " " " "
P919		36'-11"													



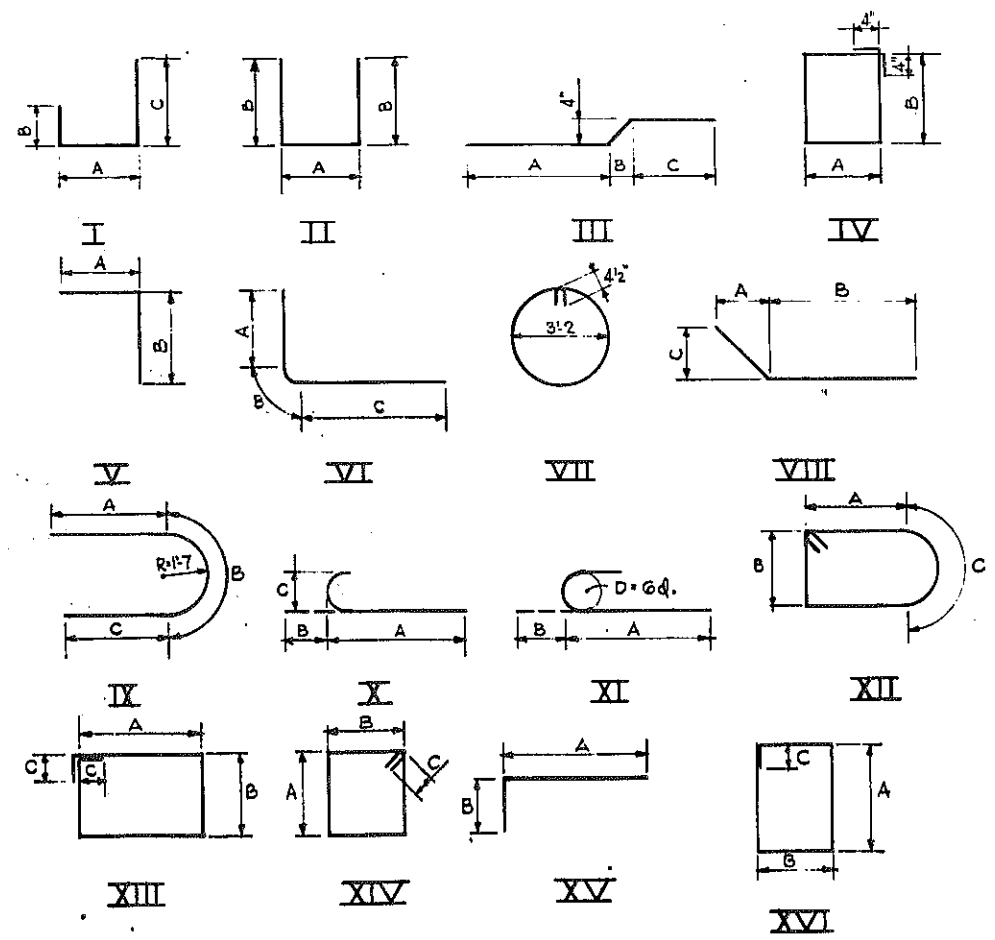
BAR LIST

DECK SLABS

MARK	SIZE	LEN.	TYPE	NUMBER						A	B	C	DESCRIPTION
				SPAN1	SPAN2	SPAN3	SPAN4	SPAN5	SPAN6				
5401	#4	24'-8"	STR.	272									LONG. BARS - DECK SLAB, SV
5402		8'-8"	XVI	192	76	129	223	192	112	92.4	3'-4"	8"	BENT BARS SIDEWALK & PARAPET
5403		3'-4"	XV	192	76	129	223	192	112	92.4	2'-8"	8"	TRANSVERSE SIDE WALK
5404		26'-11"	STR.		60					60			LONGITUDINAL BARS - VARIES 24'-0" TO 52'-10"
5405		26'-11"			8'					8			LONG. BARS SIDEWALK & PARAPET
5406		23'-6"			4					4			"
5407		64'-4"				60				60			LONGITUDINAL BARS - VARIES 43'-4" TO 85'-4"
5408		29'-0"				12				12			LONG. BARS SIDEWALK
5409		22'-2"				8				8			"
5410		28'-8"					272			272			LONGITUDINAL BARS
5411		34'-0"						126		126			"
5412		25'-6"						68		68			" VARIES 27'-0" TO 34'-0"
5413		28'-6"							136	136			"
5414		18'-2"			4		20	8		52			LONGITUDINAL BARS - PARAPET
5415		18'-8"			12		4			24			"
5416		20'-8"			4	16				20			"
5417		11'-10"				8				8			"
5418		22'-2"				8				8			"
5419		19'-8"			16		4	16		36			"
5420		17'-8"				8		4		12			"
5421		19'-2"					16		4	20			"
5422		15'-8"				4			4	8			"
5423		17'-0"				4				4			"
5424		13'-8"				4				4			"
5501	#5	28'-3"	XI	231	60	61	136	115	38	641	26'-8"	7"	TRANSVERSE BARS TOP
5502		20'-6"	XI	231	60	61	136	115	38	641	19'-11"	7"	"
5503		30'-0"	STR.	231	60	61	136	115	38	641			" BOT.
5504		16'-6"	STR.	231	60	61	136	115	38	641			" TOP & BOT.
5505		25'-6"	STR.	4						4			"
5506													"
5507													"
5508		23'-2"	STR.		63					63			" BOT. VAR. 3'-4" TO 43'-0"
5509		20'-6"	XI		63					63	23'-2"	7"	" TOP 'A' VAR. 3'-4" TO 43'-2"
5510		24'-6"	XI			41				41	23'-8"	7"	" TOP 'A' VAR. 3'-4" TO 44'-0"
5511		23'-8"	STR.			41				41			" BOT. VAR. 3'-4" TO 44'-0"
5512		24'-9"	STR.			4				4			" TOP & BOT.
5513													"
5514													"
5515		23'-3"	XI			137	137			274	22'-10"	7"	" TOP 'A' VAR. 3'-4" TO 42'-4"
5516		22'-10"	STR.			137	137			274			" BOT. VAR. 3'-4" TO 42'-4"
5517		39'-6"	STR.			4	4			8			" TOP & BOT.
5518													"
5519													"
5520													"
5521													"
5522													"
5523													"
5524		22'-10"	XI				125			125	22'-10"	7"	" TOP 'A' VAR. 3'-4" TO 42'-4"
5525		22'-10"	STR.				125			125			" BOT. VAR. 3'-4" TO 42'-4"
5526		37'-6"	STR.				4	4		8			" TOP & BOT.
5527													"
5528													"
5529		24'-6"	XI					127		127	23'-8"	7"	" TOP 'A' VAR. 3'-4" TO 44'-0"
5530		25'-8"	STR.					127		127			" TOP VAR. 3'-4" TO 44'-0"
5531													"
5532													"
5533													"
5534		33'-3"	STR.					4	4	8			" TOP & BOT.
5535													"
5536													"
5537													"
5538		23'-8"	XI					98		98	22'-10"	7"	" TOP 'A' VAR. 3'-4" TO 42'-4"
5539		22'-10"	STR.					98		98			" BOT. VAR. 3'-4" TO 42'-4"
5540													"
5541													"
5542													"
5543		25'-1"	XI						103	103	24'-6"	7"	" TOP 'A' VAR. 3'-4" TO 45'-8"
5544		24'-6"	STR.						103	103			" BOT. VAR. 3'-4" TO 45'-8"
5545		21'-1"	XI						89	89	22'-8"	7"	" TOP 'A' VAR. 3'-4" TO 42'-2"
5546		21'-8"	STR.						89	89			" BOT. VAR. 3'-4" TO 42'-2"
5547		31'-6"	STR.						4	4			" TOP & BOT.

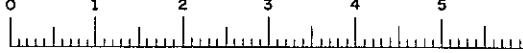
* SEE STR. 10, BK 1.
Pg. 130
SPANS 1, 2 & 3 ONLY

FED. ROAD REG. NO. 1 STATE N. Y. FEDERAL AID PROJECT NO. U-06-377(25) SHEET NO. 131 R TOTAL SHEETS 173
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



REVISION TO BAR LIST 17

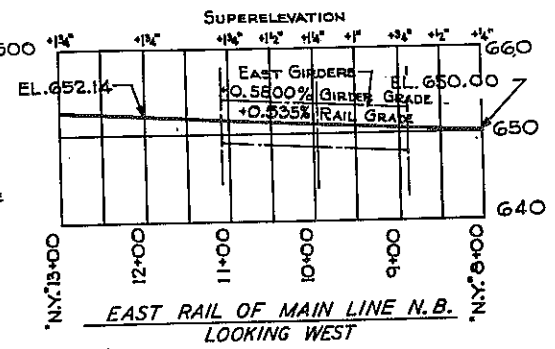
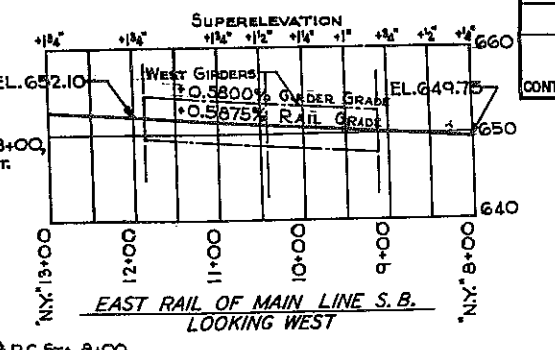
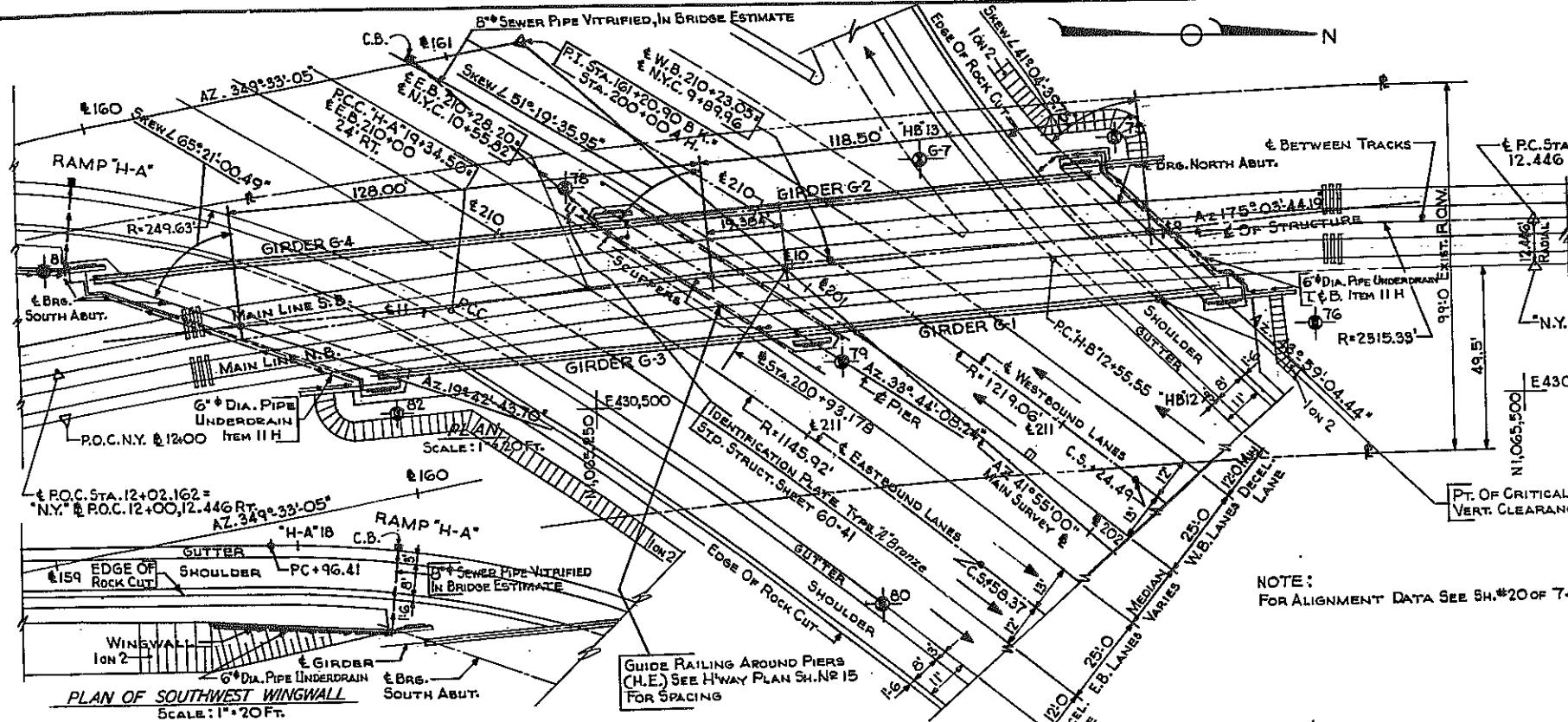
HUMBOLDT PKWY. N.B. OVER EXPW.
BAR LIST CONT'D.
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK
DRAWN R.E.K.
CHECKED C.E.
TRACED



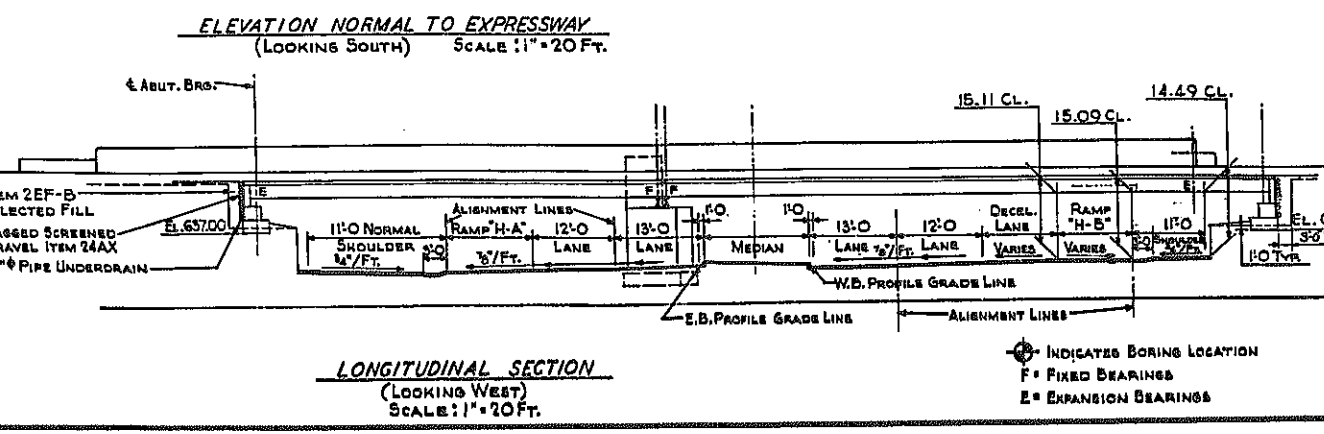
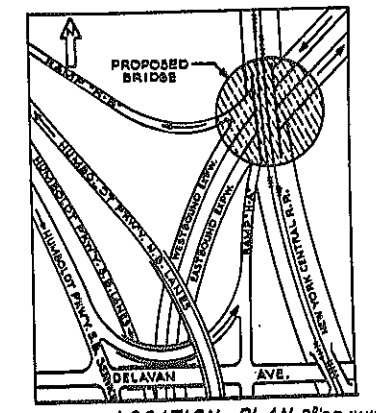
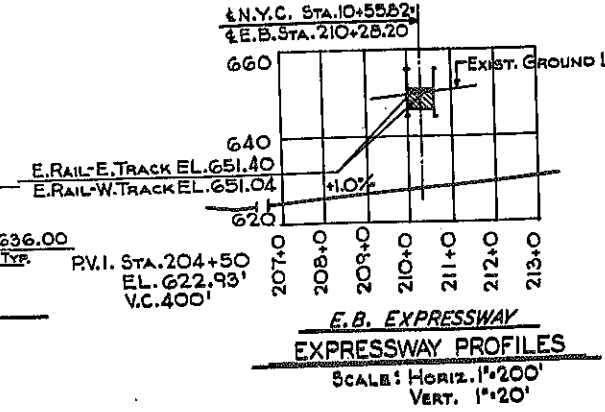
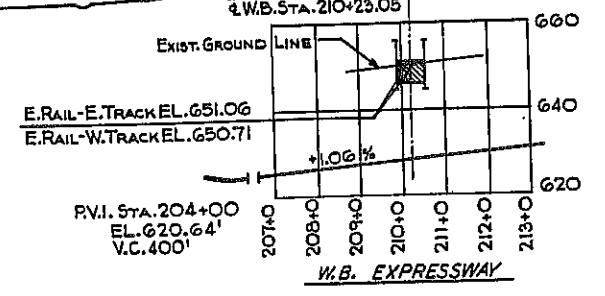
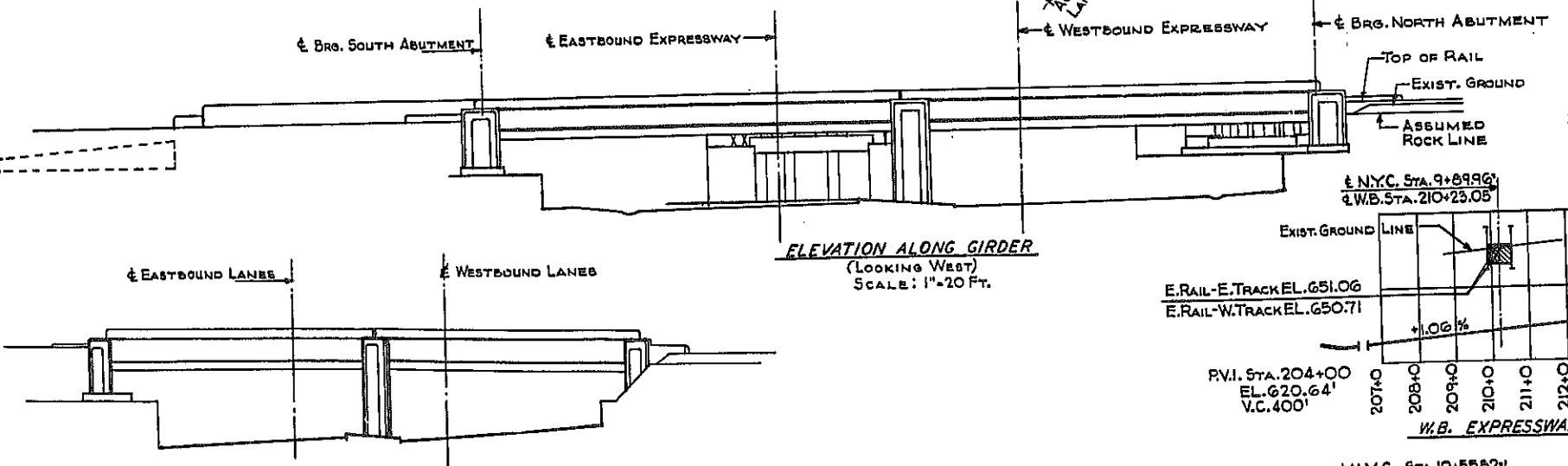
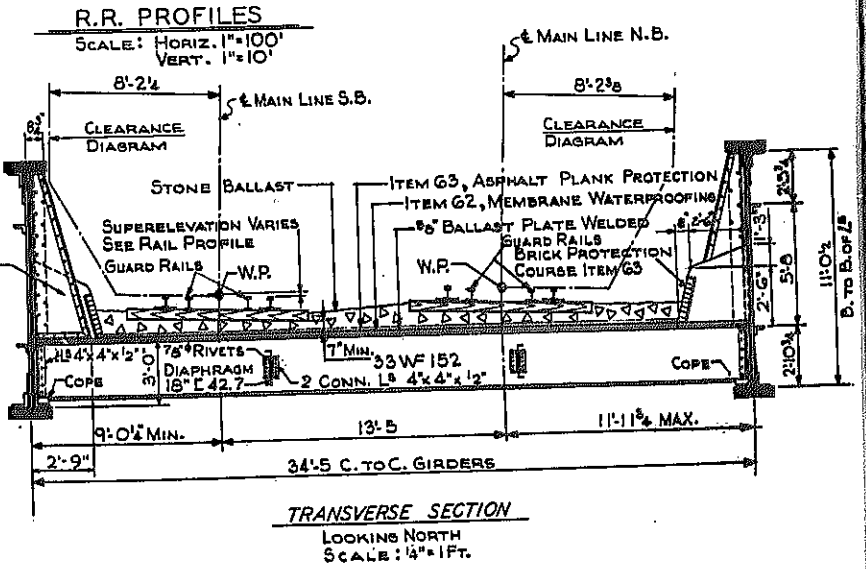
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	132	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



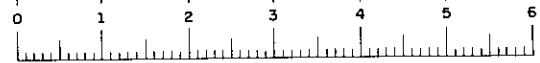
NOTE:
FOR ALIGNMENT DATA SEE SH. #20 OF 74



⊙ INDICATES BORING LOCATION
F = FIXED BEARINGS
E = EXPANSION BEARINGS

N.Y.C.R.R. OVER EXPRESSWAY
GENERAL PLAN
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

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CHECKED	C.E.
TRACED	



SUBSTRUCTURE NOTES

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	N. Y.	U-UG-377(25)	133 RI	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

LOADING : COOPER E72 WITH DIESEL IMPACT.

DESIGN SPECIFICATION : DESIGN AND FABRICATION TO BE IN ACCORDANCE WITH THE 1961 A.R.E.A. SPECIFICATIONS WITH CURRENT REVISIONS FOR STEEL RAILWAY BRIDGES.

MATERIALS : N.Y. STATE DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957, AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION. THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

FOUNDATION LOADS : FOR DESIGN PURPOSES, THE ASSUMED FOUNDATIONS LOAD ON ROCK FOR PIER, ABUTMENTS AND WINGWALLS DOES NOT EXCEED 8 TONS PER SQUARE FOOT.

CEMENT : CEMENT USED IN CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A. ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

CONCRETE : CONCRETE IN PIER BENT ABOVE FOOTINGS AND IN SUPERSTRUCTURE SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES. CONCRETE IN ABUTMENTS, WINGWALLS AND FOOTINGS FOR PIER SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

RAILROAD DETOUR : FOR RAILROAD DETOUR, TEMPORARY AND PERMANENT TRACKAGE, SEE SHEET NO. 33. BALLAST WILL BE FURNISHED AND PLACED BY THE N.Y.C.R.R. BUT SHALL BE REMOVED BY THE CONTRACTOR UNDER ITEM 2. TEMPORARY TRACK HARDWARE AND TIES WILL BE FURNISHED, INSTALLED, MAINTAINED AND REMOVED BY THE N.Y.C.R.R.

ELEVATIONS : ALL ELEVATIONS REFER TO U.S.C.G.S. DATUM. SUBTRACT 1.904' TO REFER TO N.Y.C.R.R. DATUM.
U.S.C.G.S. DATUM = CITY DATUM + 575.454'
N.Y.C.R.R. DATUM = CITY DATUM + 573.55'
U.S.C.G.S. DATUM - 1.904' = N.Y.C.R.R. DATUM
NEW YORK CENTRAL RAILROAD DATUM 0.00 EQUALS MEAN HIGH WATER AT THE FOOT OF EAST 26th STREET, NEW YORK CITY, WHICH IS 2.75 FEET ABOVE MEAN TIDE AT SANDY HOOK.

MAINTENANCE : THE DIVISION OF MAINTENANCE FOR THE VARIOUS PORTIONS OF THIS CONTRACT SHALL BE AS FOLLOWS:
THE BRIDGE AND ITS SUPPORTING ABUTMENTS AND PIER CARRYING THE N.Y.C.R.R. TRACKS OVER KENSINGTON EXPRESSWAY SHALL BE MAINTAINED BY THE NEW YORK CENTRAL RAILROAD COMPANY.
ALL EXPRESSWAY CURBS, SHOULDERS AND DRAINAGE PIPES SHALL BE MAINTAINED AND KEPT IN REPAIR BY THE STATE OF NEW YORK.
ALL COMBINED SEWERS SHALL BE MAINTAINED AND KEPT IN REPAIR BY THE BUFFALO SEWER AUTHORITY.

OTHERS : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOP OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED. SURFACE OF ABUTMENTS AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATIONS SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATIONS.

IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STANDARD SHEET 60-41.

MAXIMUM PAYMENT LINES FOR ITEM 5 SHALL BE AS SHOWN ON THE PLANS. BITUMINOUS MATERIAL, ITEM 61, SHALL BE APPLIED TO THE BACKS OF ABUTMENTS AND WINGWALLS.
THE ANCHOR BOLTS FOR THE BRIDGE BEARINGS SHALL BE SET PRIOR TO THE POURING OF THE CONCRETE.
GIRDER BEARINGS SHALL BE PROVIDED WITH A 1/2" THICK BEARING PAD, ITEM 115. THE SIZE OF THE PAD SHALL BE EQUAL TO THE SIZE OF THE BEARING PLATE. HOLES 15/8" DIAM. SHALL BE PROVIDED IN THE BEARING PADS AT LOCATIONS TO MATCH THE HOLES IN THE BEARING PLATES.

ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED ONE INCH UNLESS OTHERWISE NOTED.
PAYMENT FOR FURNISHING AND PLACING SPONGE RUBBER AND CAULKING COMPOUND SHALL BE INCLUDED IN THE UNIT PRICE OF CONCRETE ITEMS. SPONGE RUBBER JOINTS SHALL BE FILLED WITH SPONGE RUBBER 1/4" WIDER THAN THE JOINT WIDTH SHOWN ON THE PLAN AND SHALL CONFORM TO A.S.T.M. SPECIFICATIONS D544.

SUPERSTRUCTURE NOTES

SUPERSTRUCTURE : RIVETED PLATE GIRDER CONSTRUCTION.
MAIN GIRDER STEEL : A.A.S.H.O. HIGH-STRENGTH LOW ALLOY STRUCTURAL STEEL A.S.T.M. A-440
FLOOR SYSTEM : STRUCTURAL STEEL, A.S.T.M. A7-56T.
RIVETS : STRUCTURAL RIVET STEEL, A.S.T.M. A141.
A.S.T.M. A-440 HIGH-STRENGTH STRUCTURAL STEEL WILL BE PAID FOR UNDER ITEM 29X. STRUCTURAL STEEL, STEEL CASTINGS, BRIDGE ANCHOR BOLTS, RIVETS, BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29-B. ITEM 29 OF THE NEW YORK STATE DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS SHALL APPLY EXCEPT THAT DESIGN AND FABRICATION SHALL BE IN ACCORDANCE WITH THE 1961 A.R.E.A. SPECIFICATIONS FOR STEEL RAILWAY BRIDGES.
RIVETS, 1"Ø, OPEN HOLES 1 1/8"Ø, EXCEPT AS OTHERWISE NOTED.
ERECTION SHALL BE IN ACCORDANCE WITH THE CURRENT A.R.E.A. SPECIFICATIONS FOR THE ERECTION OF STEEL RAILWAY BRIDGES.
WELDING SHALL COMPLY WITH THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR WELDED HIGHWAY AND RAILWAY BRIDGES OF THE AMERICAN WELDING SOCIETY.
FLOOR SYSTEM TO GIRDERS AND DIAPHRAGM CONNECTIONS SHALL BE REAMED IN THE SHOP WITH MEMBERS ASSEMBLED.
THE COST OF FURNISHING AND PLACING LEAD PLATES UNDER DECK PLATE ON BACKWALLS WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 18X.

FOOTINGS : ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE HEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING AND AMENDMENTS TO PAINT NOTES, SPECIFICATIONS SEE SHEET NO. B4.

ESTIMATE OF QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED	FINAL QUANTITY
2EF-B	SELECTED FILL	C. Y.	287	310	1236.0
5	TRENCH, CULVERT & BRIDGE EXC.	C. Y.	133	145	131.44
11H	PERF CORR. METAL PIPE UNDERDRAIN 6" DIAM.	L. F.	248	255	31.4
13B	CAST IRON PIPE, 6" DIAM.	L. F.	22	23	7.5
15-2A	PORTLAND CEMENT TYPE 2A	B B L.	1223	1285	1738.4
18X	CLASS 1A CONCRETE FOR STRUCTURES	C. Y.	293	300	311.71
20Y	CLASS 1 CONCRETE	C. Y.	462	480	533.0
24AX	BAGGED SCREENED GRAVEL OR SLAG	C. Y.	58	60	0
28	BAR REINFORCEMENT FOR STRUCTURES	L. B.	50,422	51,500	62,024
29B	STRUCTURAL STEEL	L. B.	920,599	948,200	1,161,637
29X	A.S.T.M. A-440 HIGH STRENGTH STRUCT. STEEL	L. B.	546,184	562,000	692,765
61	BITUMINOUS MATERIAL	GAL.	138	150	18.0
62	MEMBRANE WATERPROOFING	S. Y.	954	1,000	127.2
63	PROTECTIVE COURSE FOR MEMBRANE W.P.	S. Y.	787	820	101.8
83TX	TEMPORARY SHEET PILING	S. F.	366	380	0
115	BRIDGE BEARING PADS	S. F.	90	100	12.17
121A	TOPSOIL - FURNISHED & PLACED	C. Y.	12	13	0.0
123	SEEDING	ACRES	.022	.02	0.00
30S	MISCELLANEOUS METALS	L. B.	228	240	38.0
352XA	RUBBER JOINT MATERIAL	GAL.	1	1	.55
9VC	SEWER PIPE VITRIFIED, 8" DIAM.	L. F.	102	110	80.20
21	CLASS 2 CONCRETE	C. Y.	27	30	34.77
30W	W.I. PIPE, 6" DIAM.	L. B.	1176	1200	316
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	32	38	355.0
24AXX	SCREENED AGGREGATE - STRUCTURE BACKFILL	C. Y.			50.11

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

REVISION TO TABLE 77

N.Y.C.R.R. OVER EXPRESSWAY
GENERAL NOTES & ESTIMATE
OF QUANTITIES

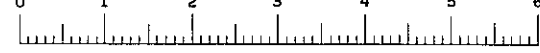
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN
CHECKED
TRACED

R.E.K.
C.E.

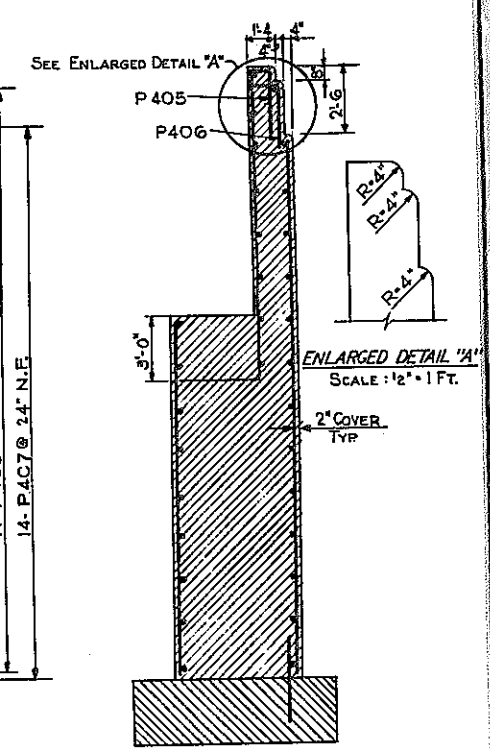
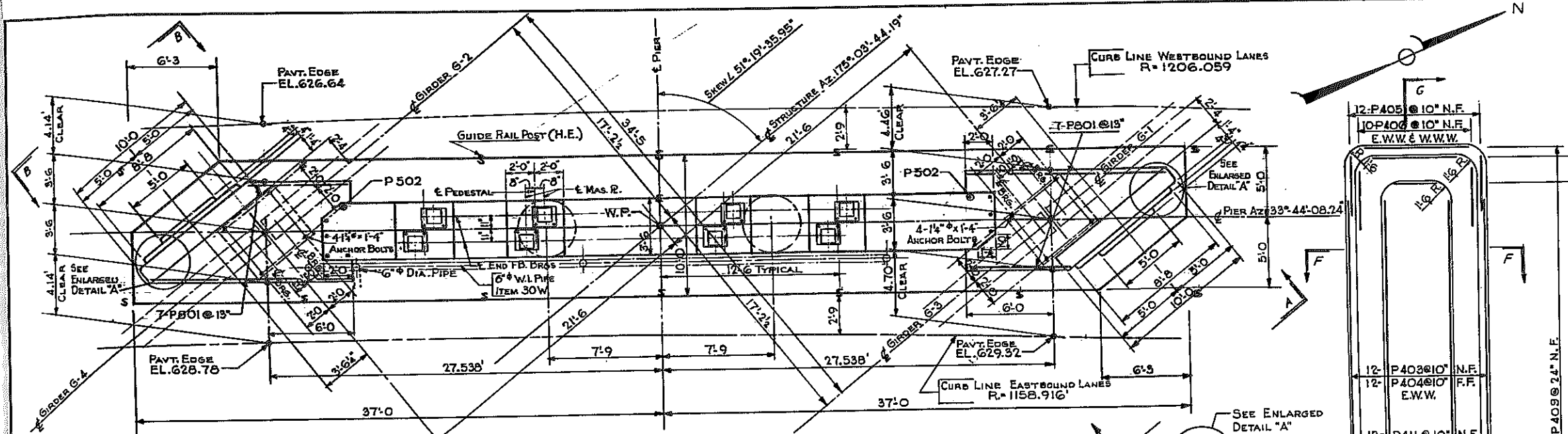
DRAWING NO 2 OF 16



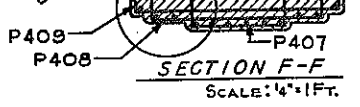
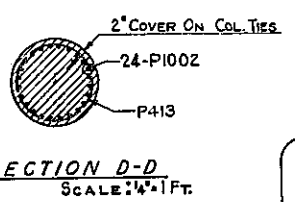
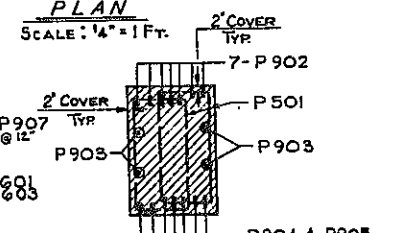
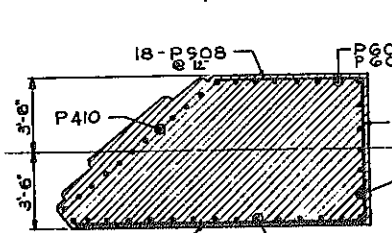
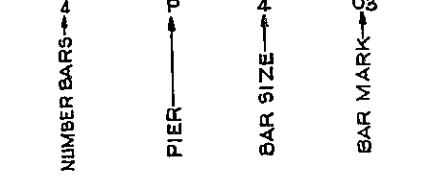
Dwg 60-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	134	173

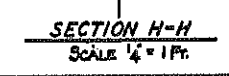
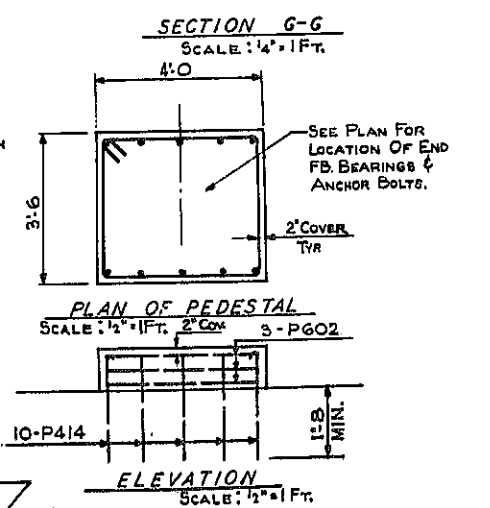
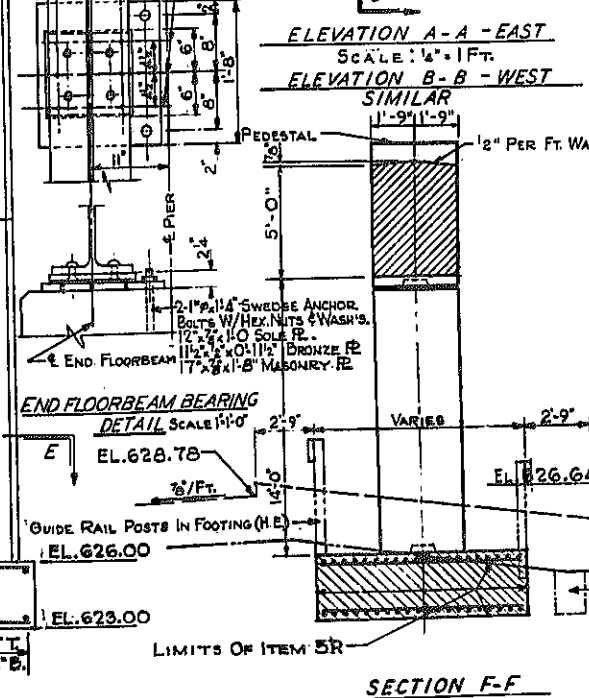
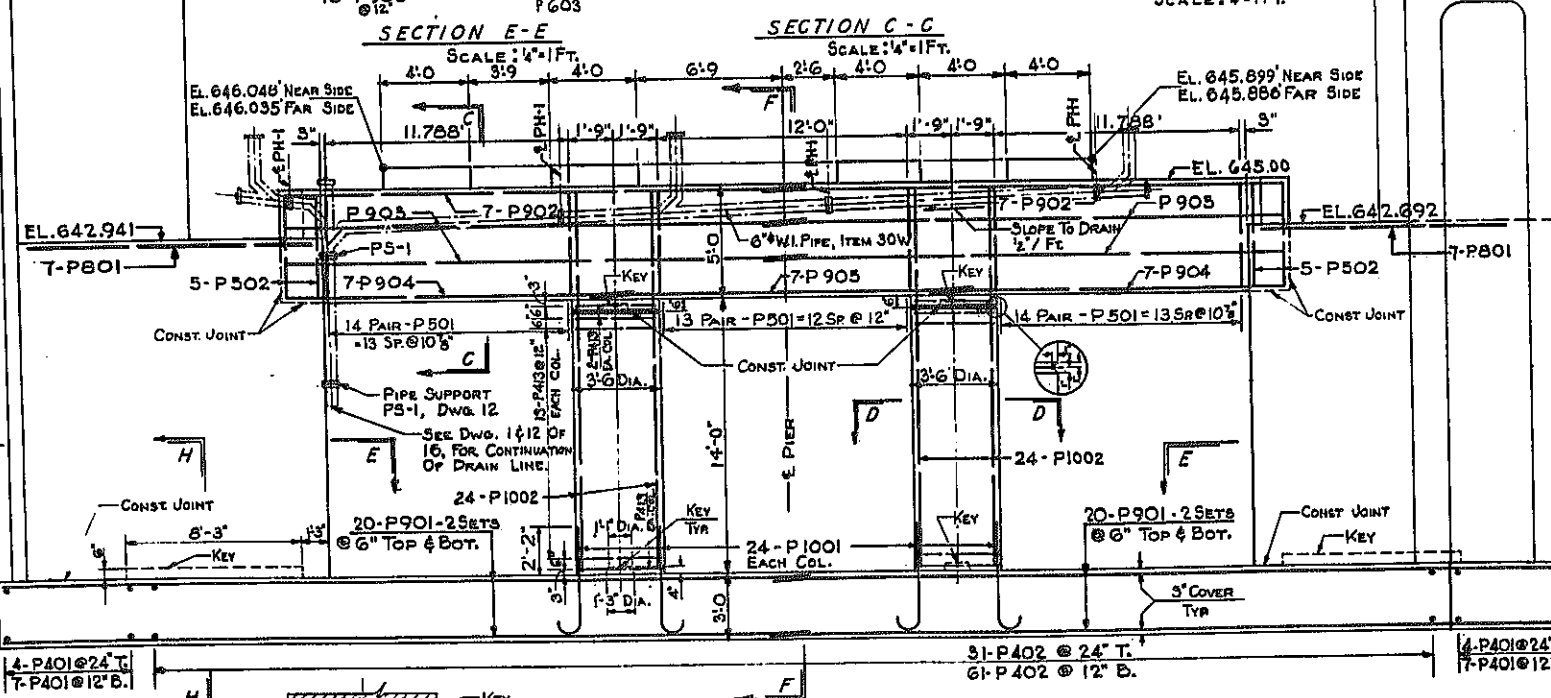
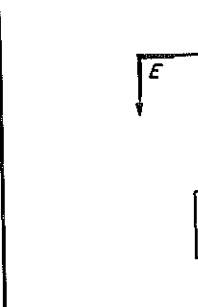
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



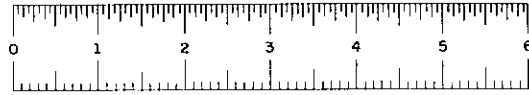
BAR REINFORCING LEGEND



PLAN OF RAILING POST (H.E.)
SCALE: 1" = 1 FT.

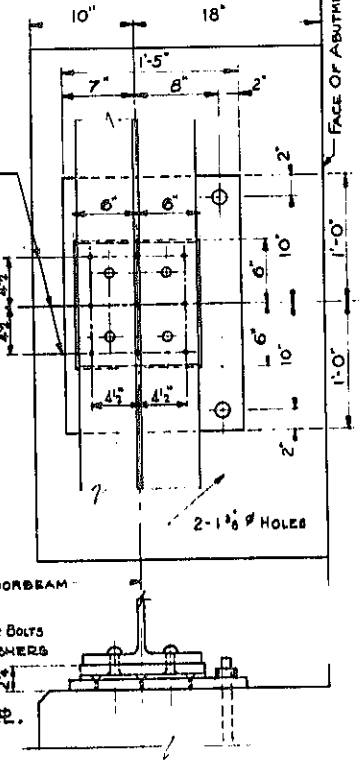
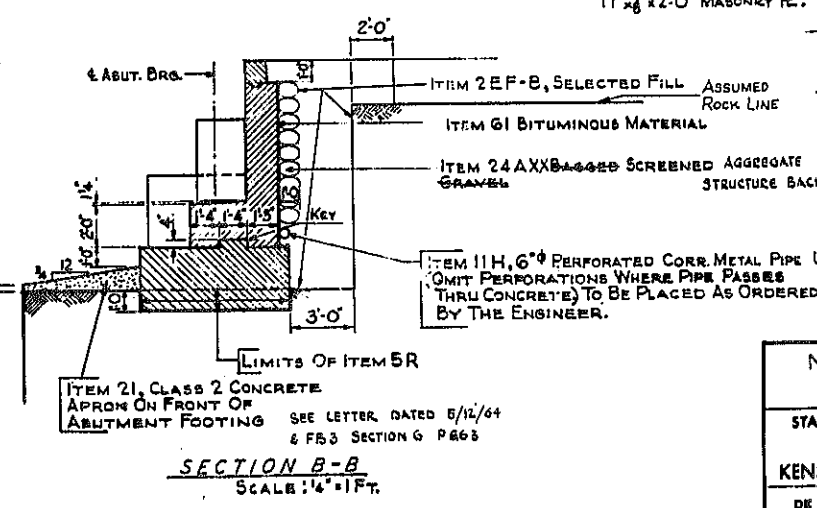
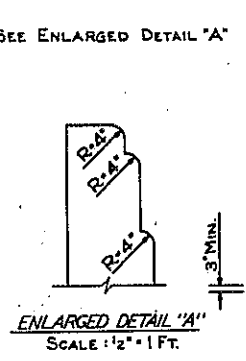
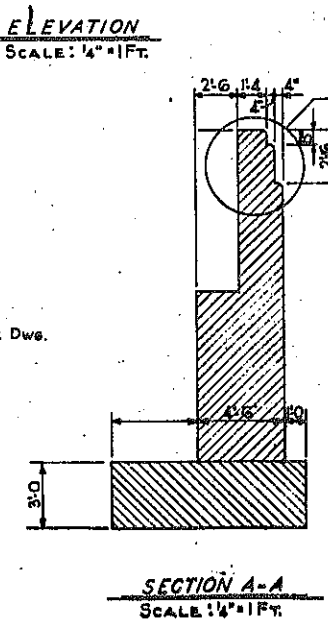
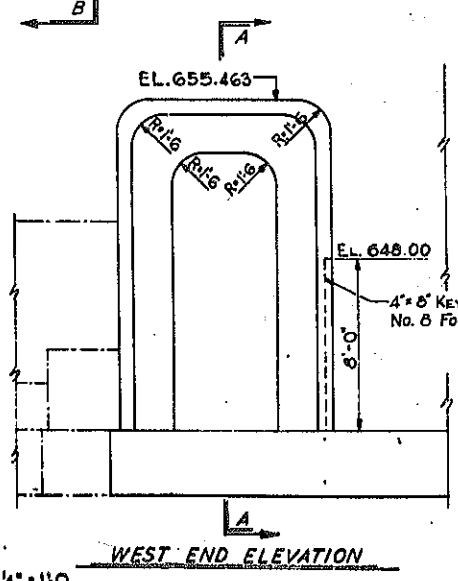
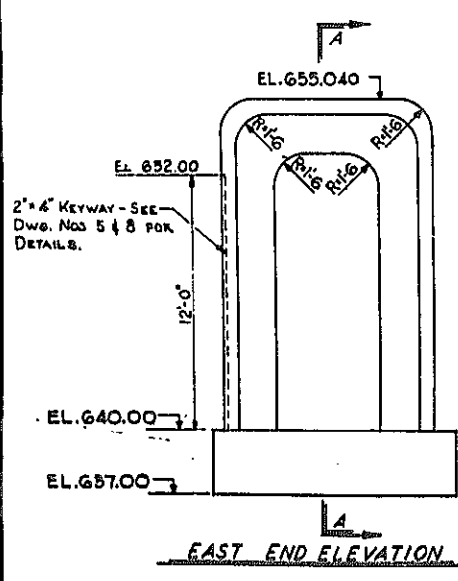
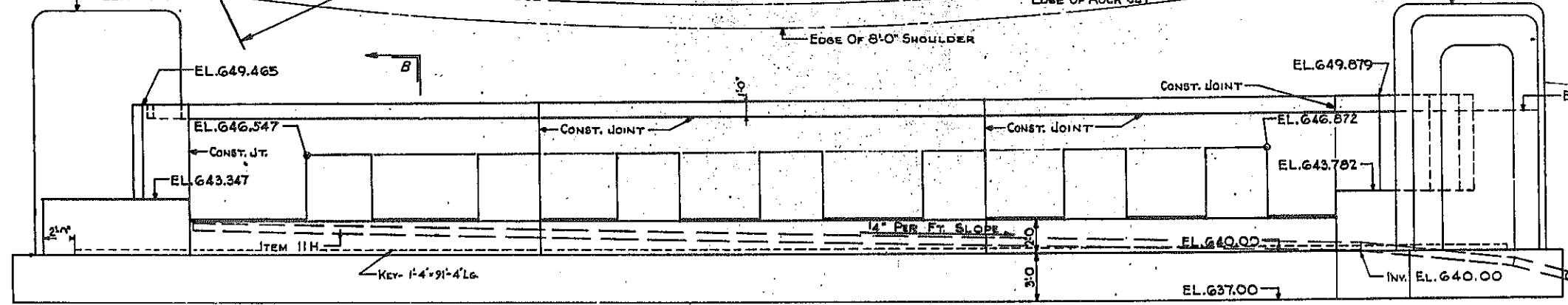
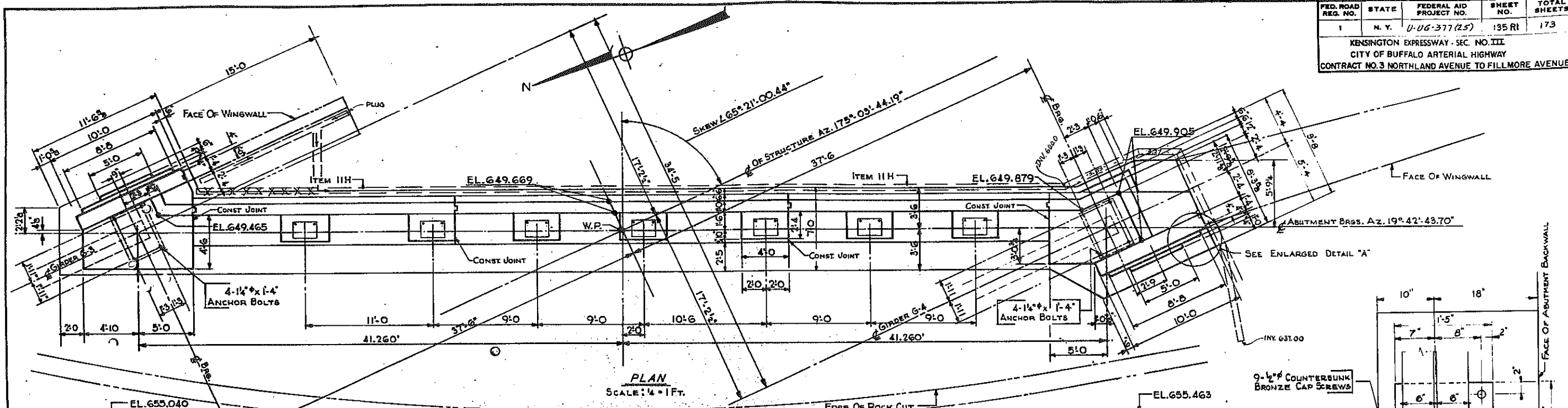


NYCRR. OVER EXPRESSWAY PIER
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK
DRAWN R.E.K.
CHECKED C.E.L.
TRACED



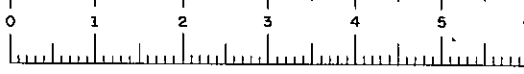
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	135 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



N.Y.C.R.R. OVER EXPRESSWAY
SOUTH ABUTMENT
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.
DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

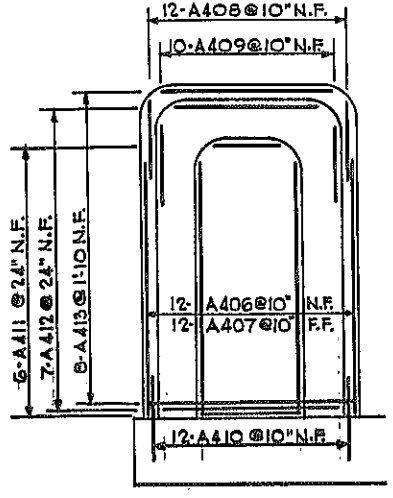
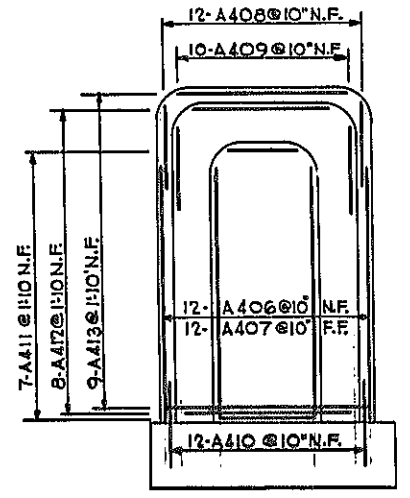
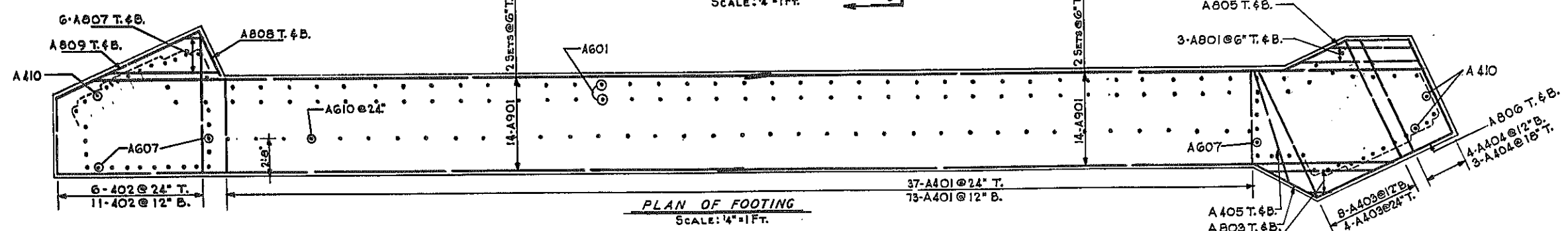
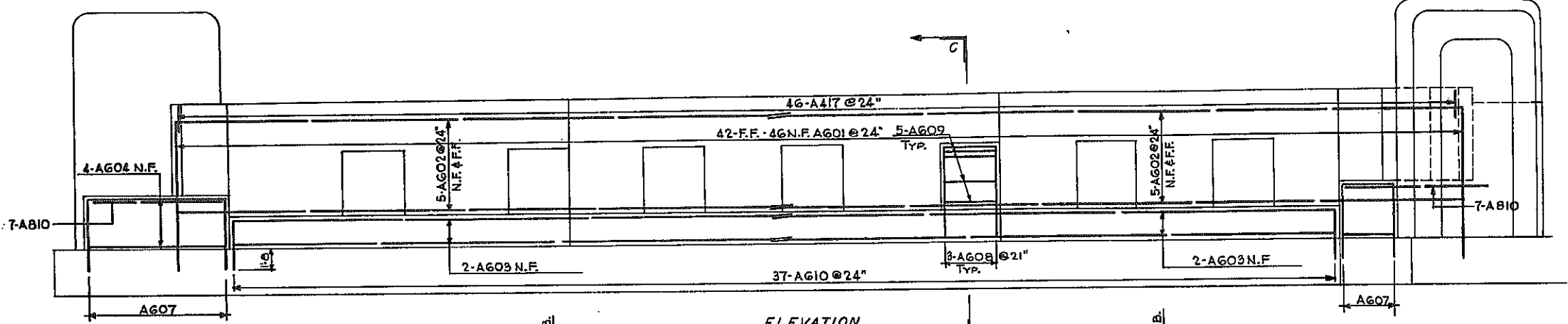
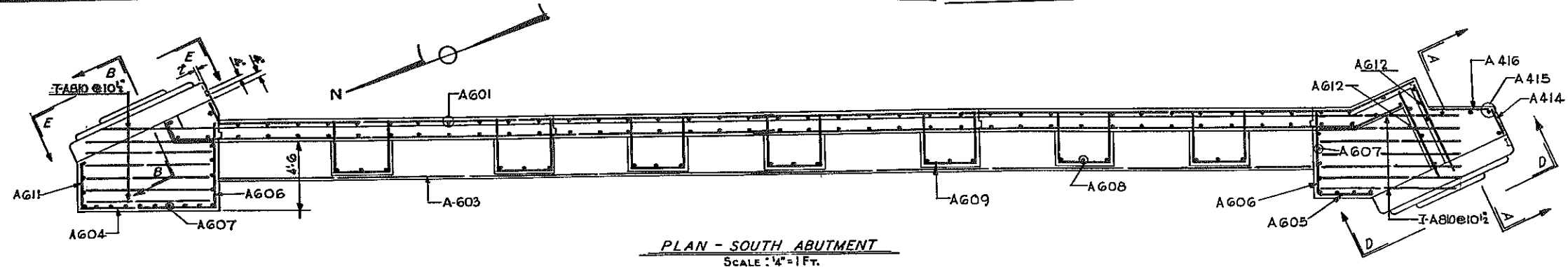
DRAWN	C.E.
CHECKED	
TRACED	



FAC 63-2

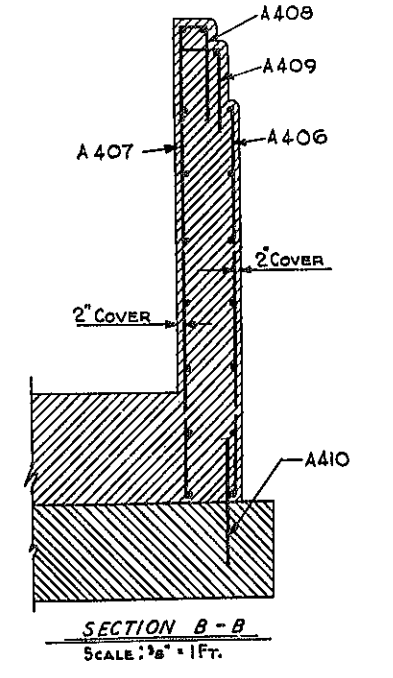
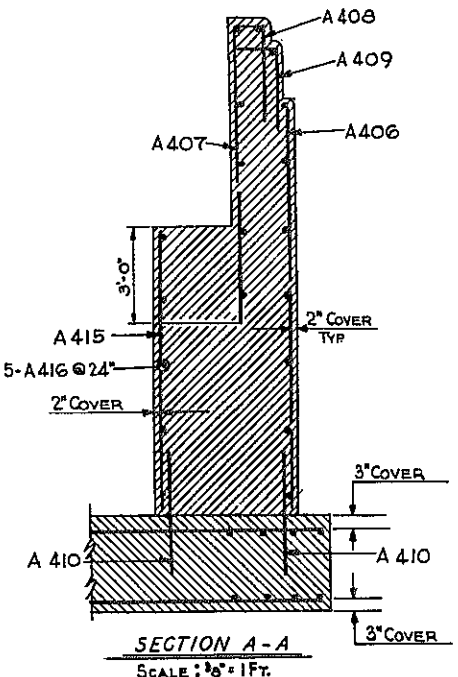
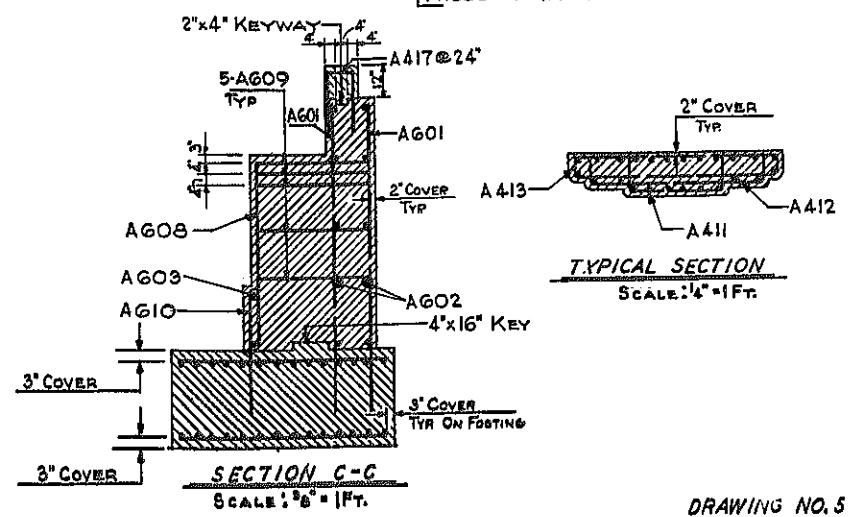
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377 (25)	136	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



BAR REINFORCING LEGEND

NUMBER BARS	ABUTMENT	BAR SIZE	BAR MARK
3	A	4	O4

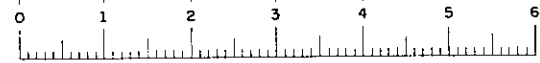


N.Y.C.R.R. OVER EXPRESSWAY
SOUTH ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

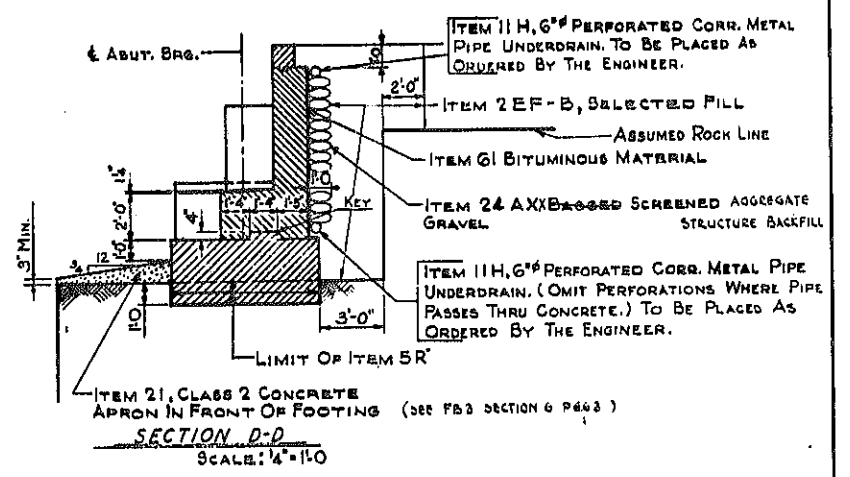
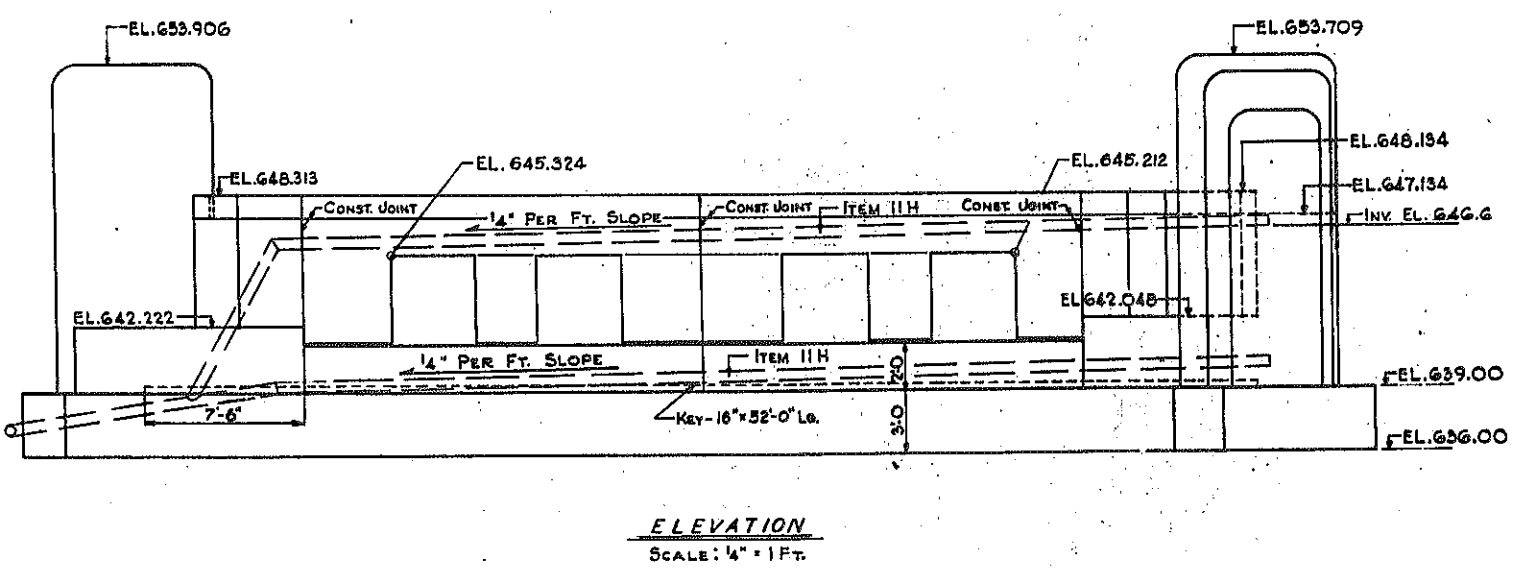
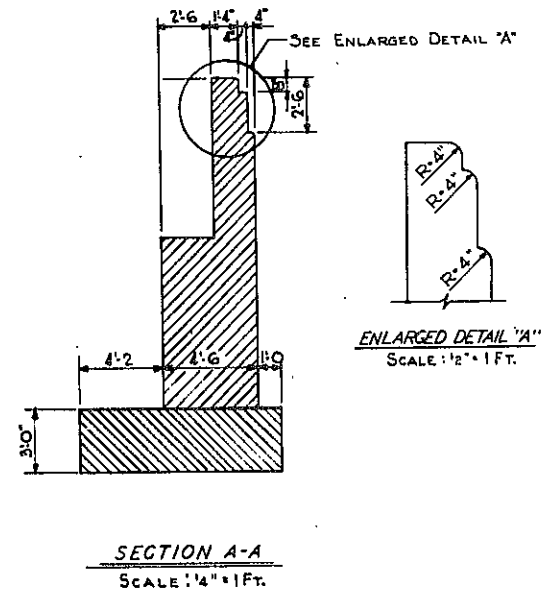
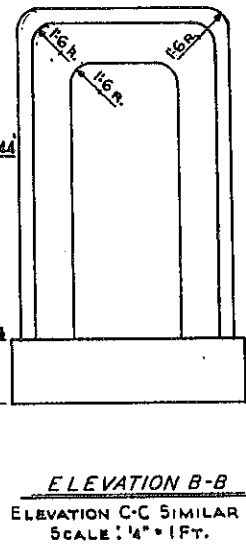
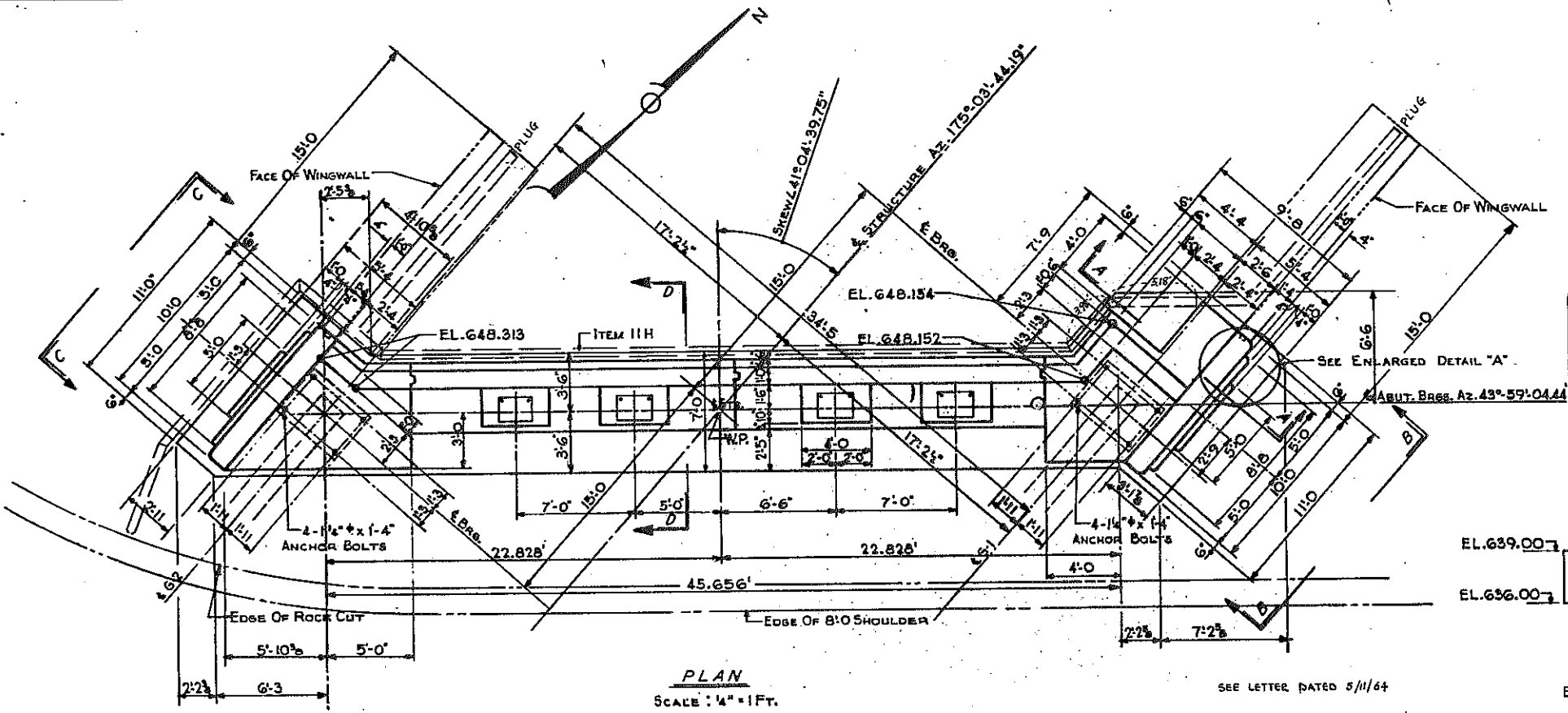
DR. LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REK DEL
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DRAWING NO. 5 OF 16



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	137R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: SEE DRAWING NO. 4 FOR DETAIL OF END FLOORBEAM BEARING.

REVISION TO UNDERDRAIN

N.Y.C.R.R. OVER EXPRESSWAY
NORTH ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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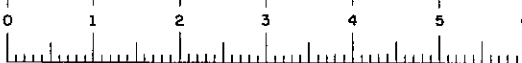
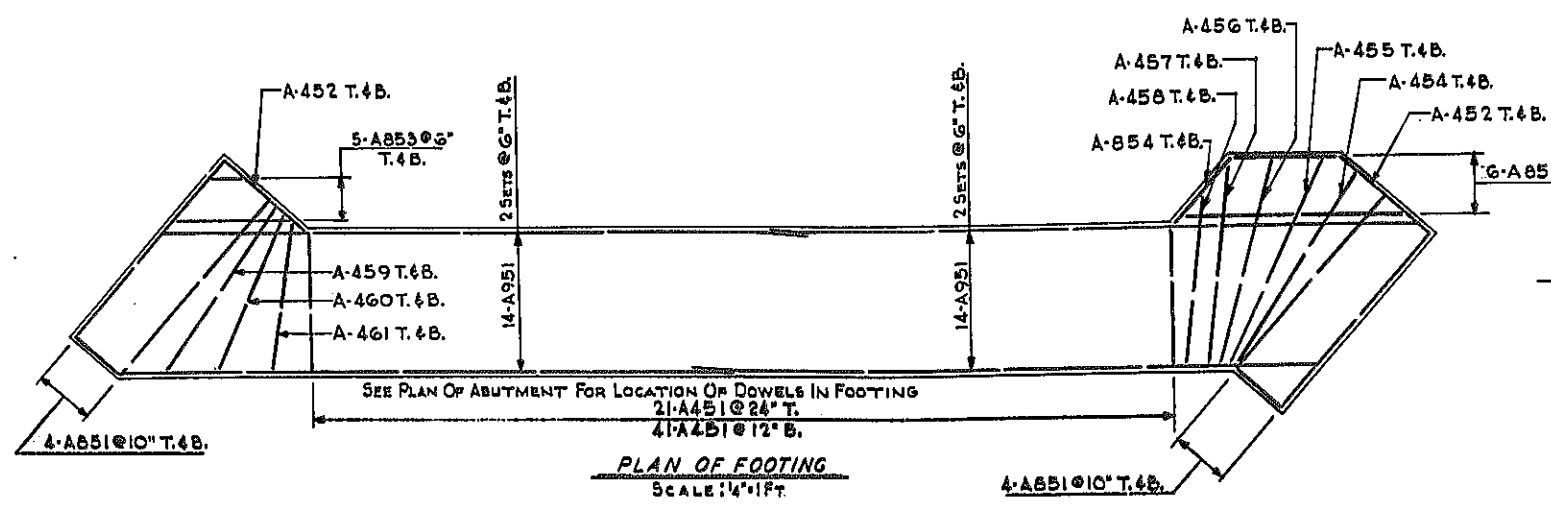
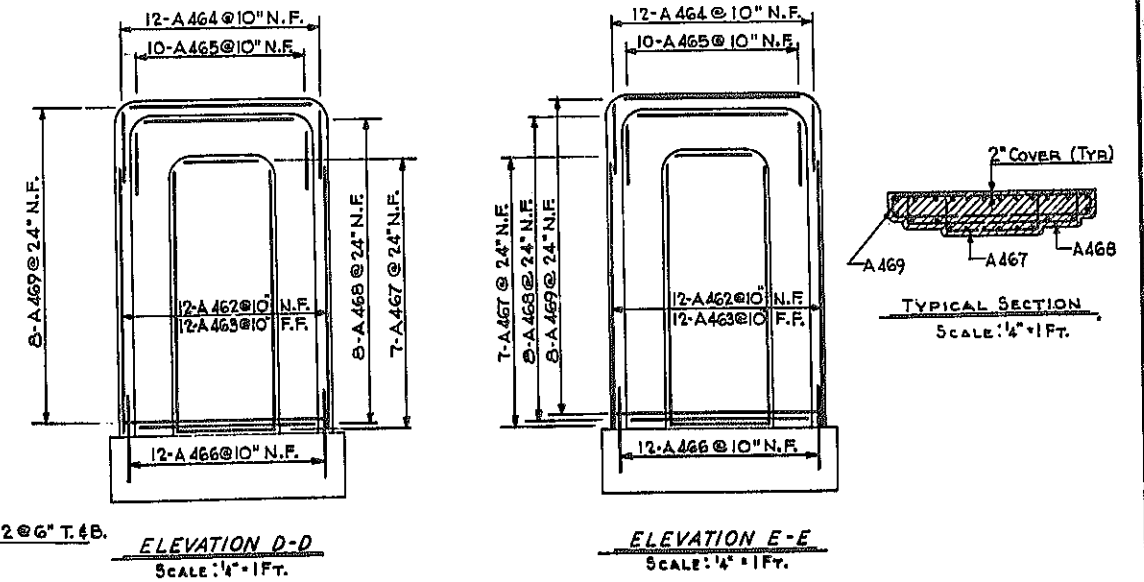
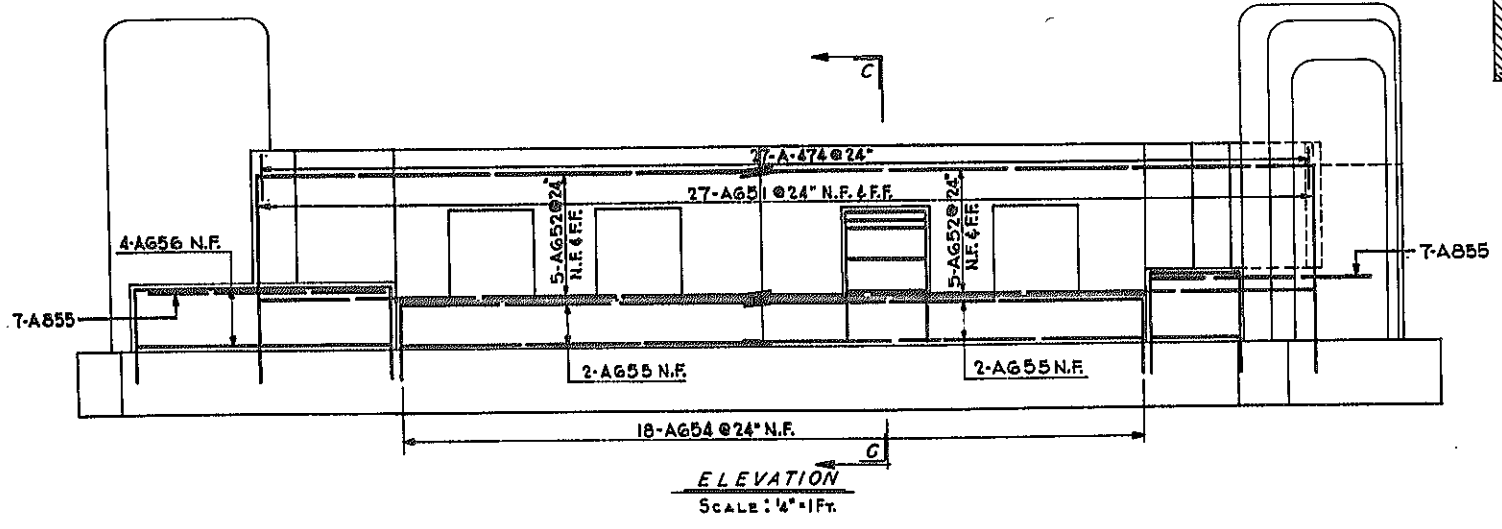
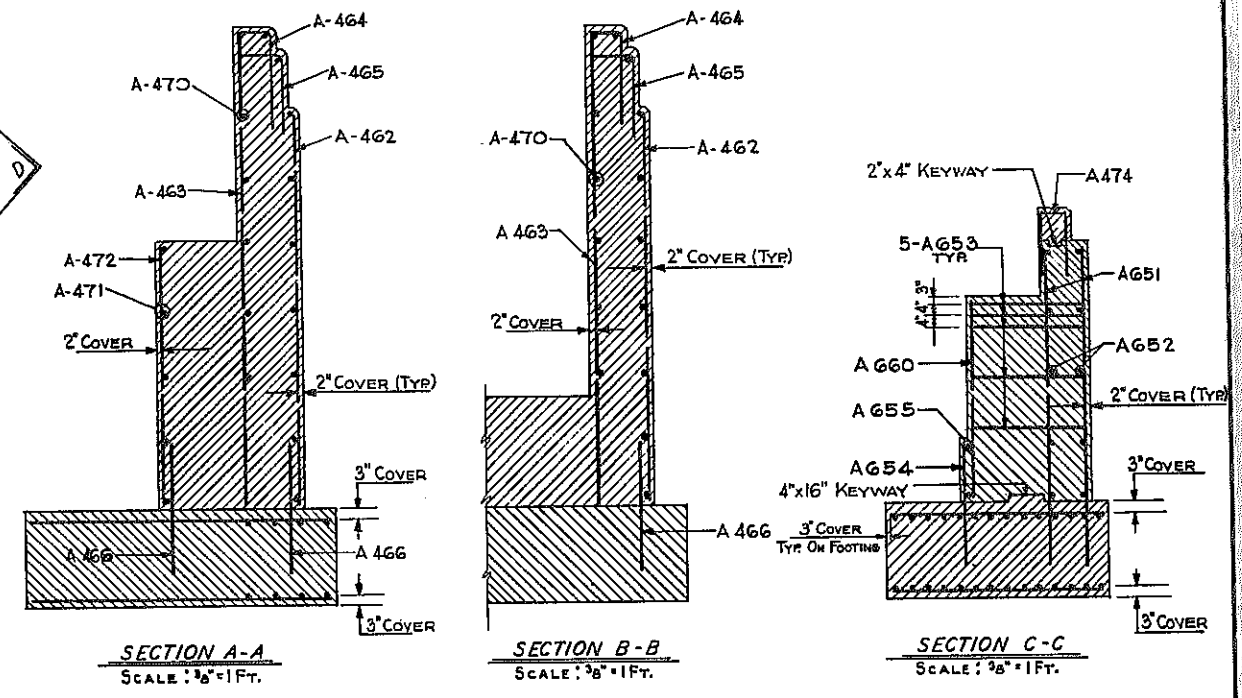
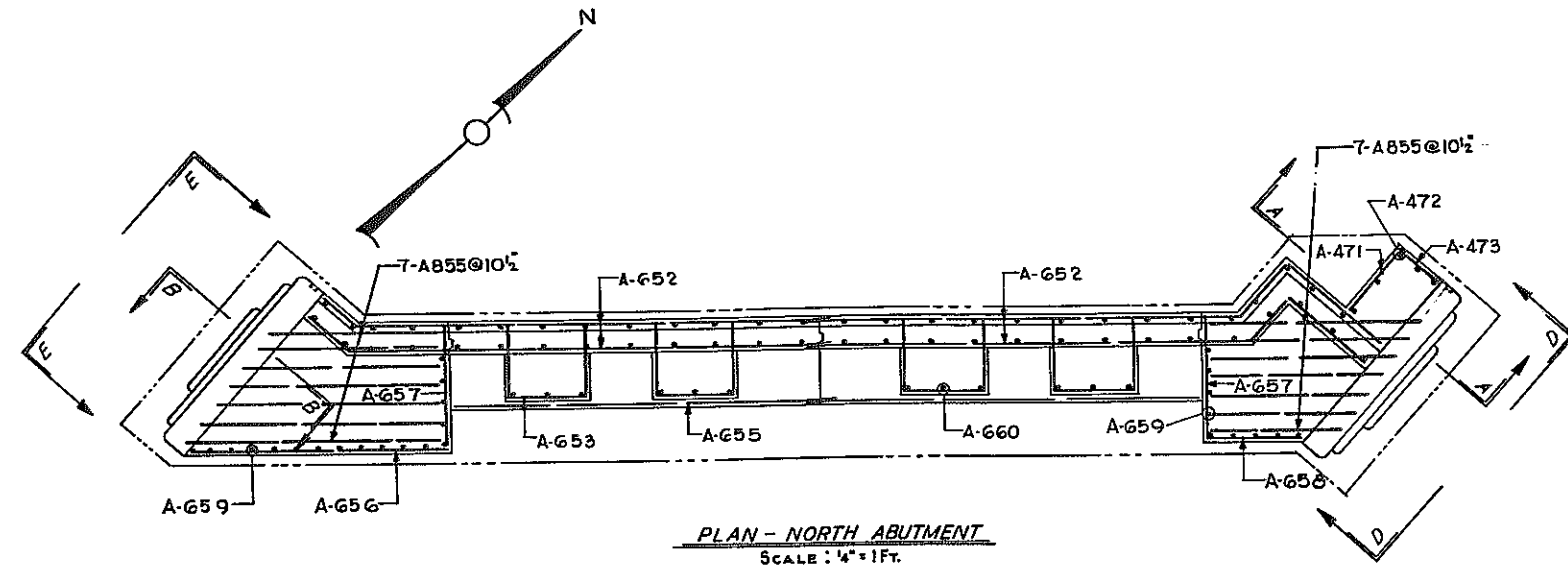


FIG. 03-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	138	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVE.



BAR REINFORCING LEGEND

NUMBER BARS	ABUTMENT	BAR SIZE	BAR MARK
3	A	4	OS

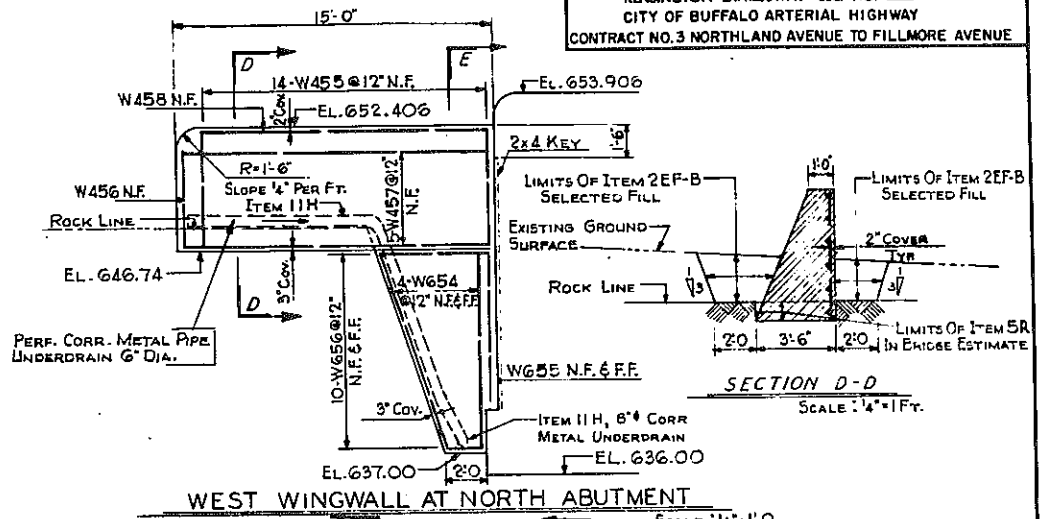
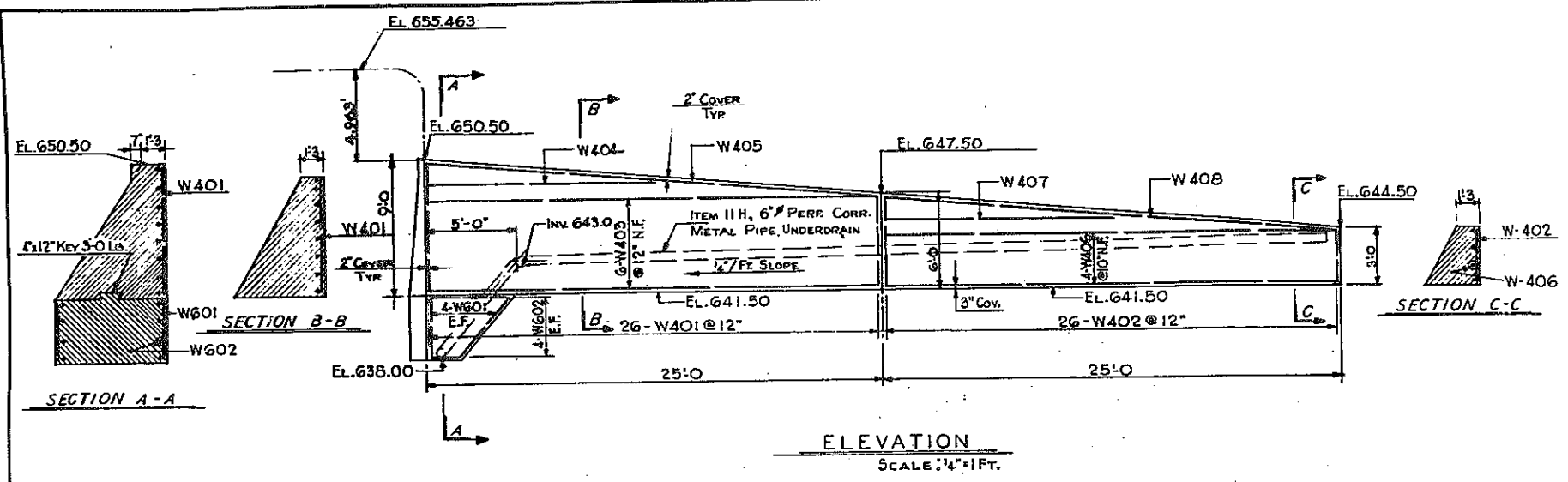
N.Y.C.R.R. OVER EXPRESSWAY
NORTH ABUTMENT DETAILS
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN <i>R.E.K.</i>
CHECKED <i>DEL</i>	TRACED

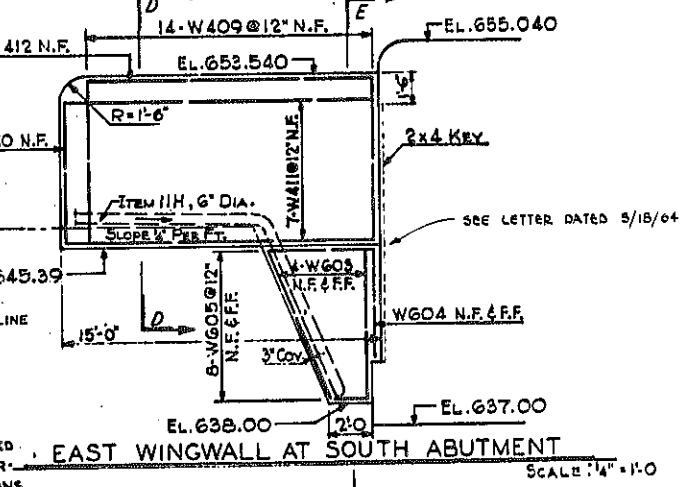
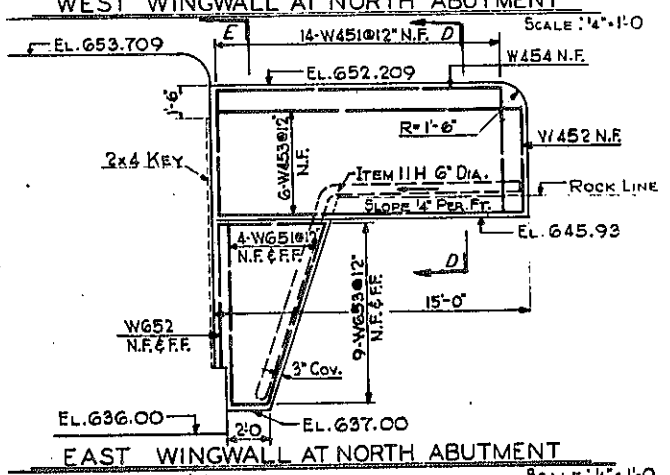
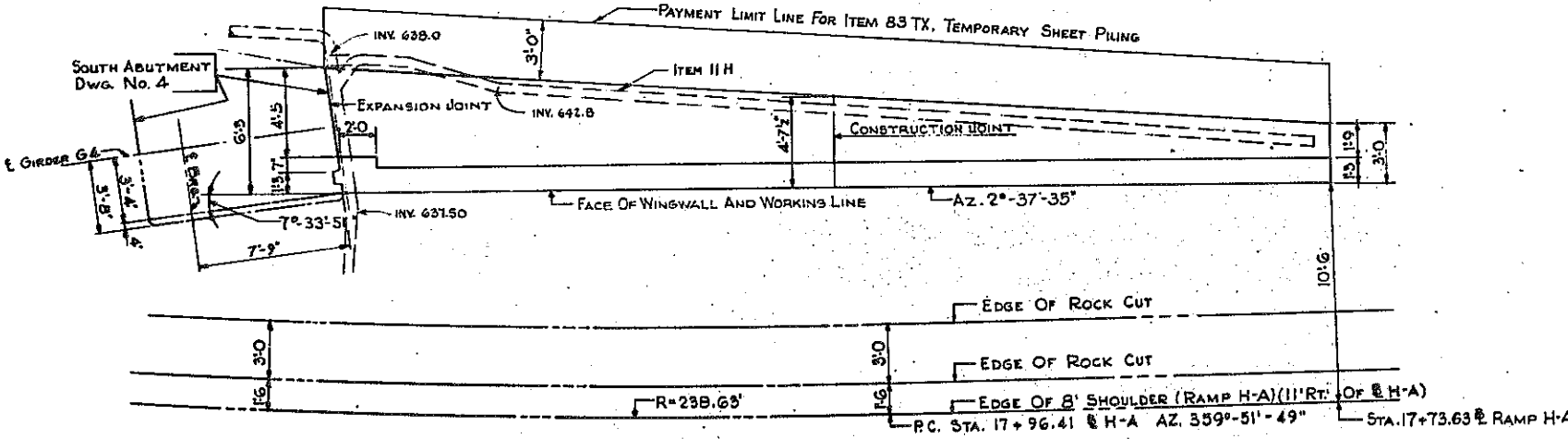
DRAWING NO 7 OF 16

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	139 R1	173

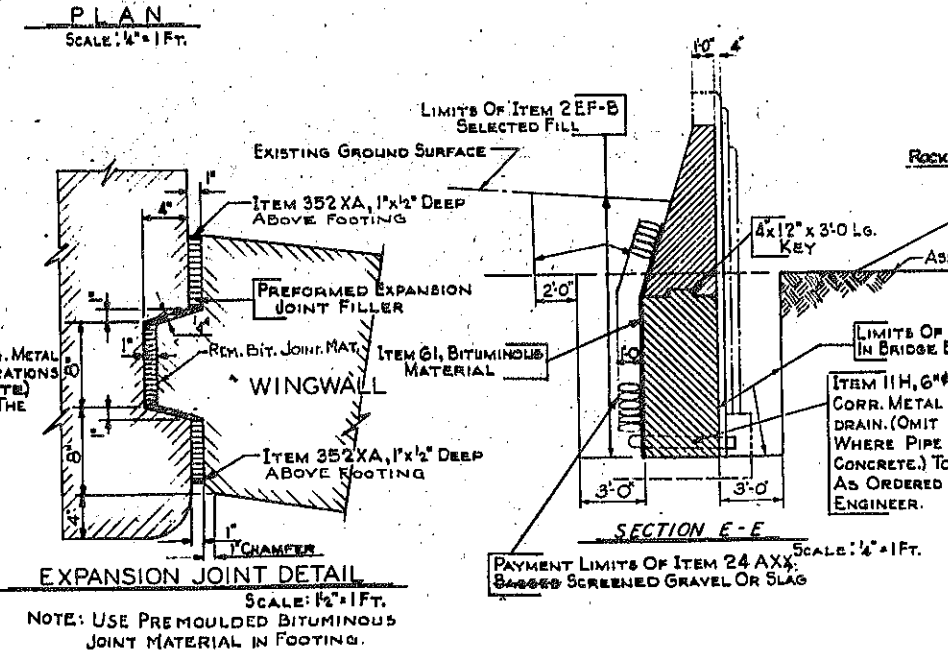
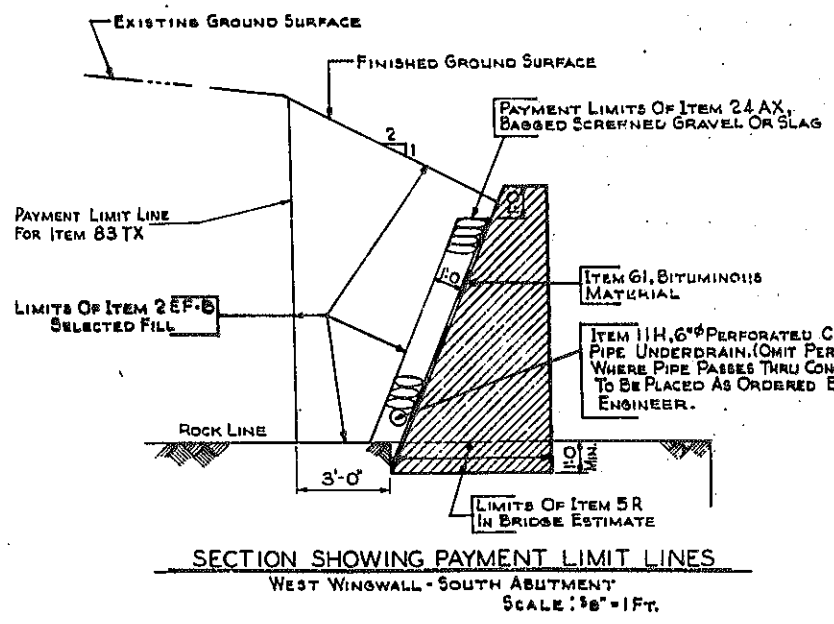
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: FOR A.O.B.E. REVISED PAY LINES AT WINGWALLS (N. ABUT.) SEE FB-3 SECT 6 UNDER APPROPRIATE ITEMS.



NOTE: WINGWALLS TO BE ITEM 20Y, CLASS I CONCRETE. REINFORCING STEEL COVERING SHALL BE 2\"/>

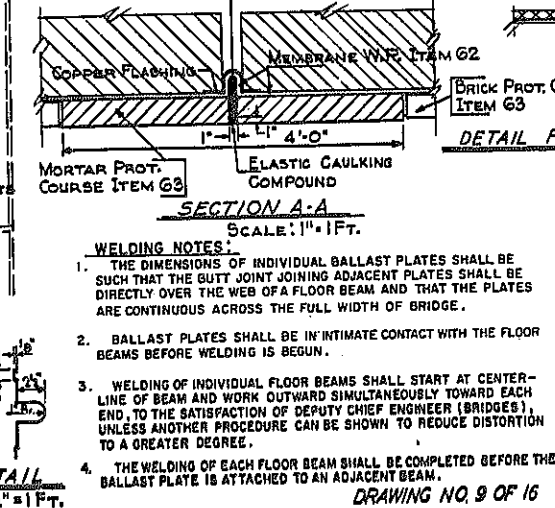
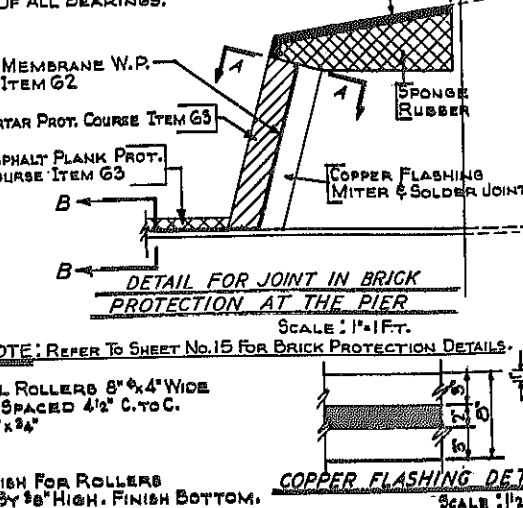
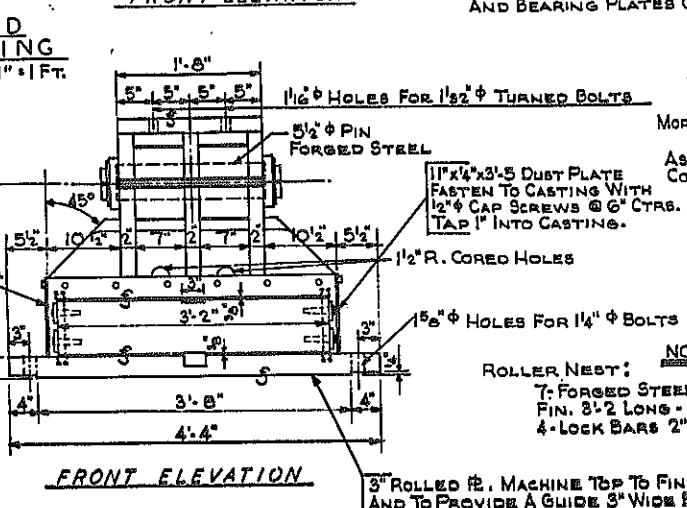
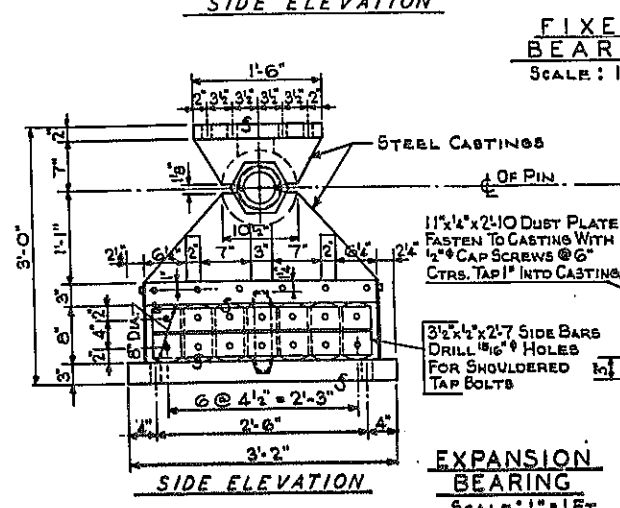
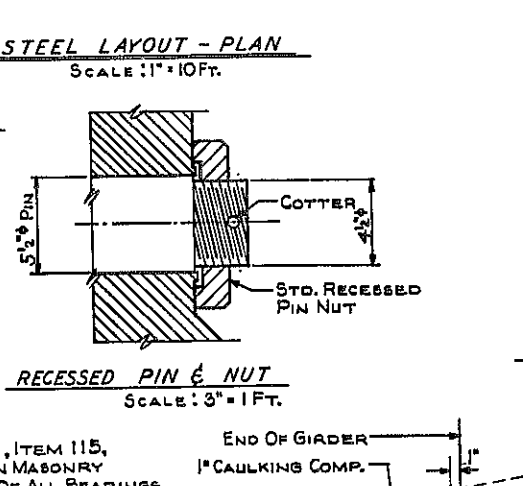
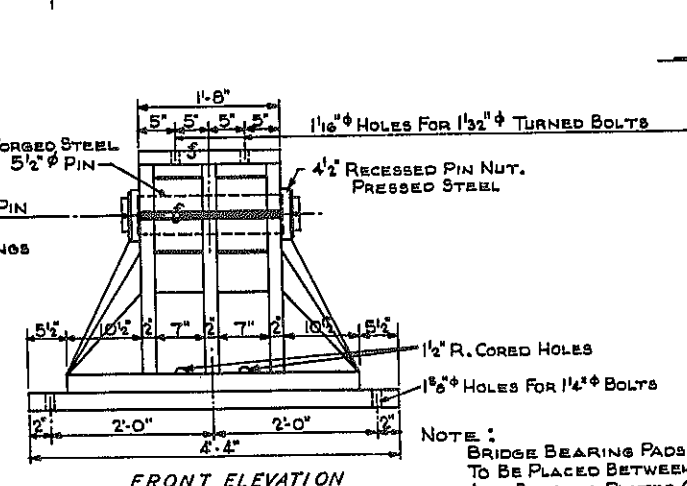
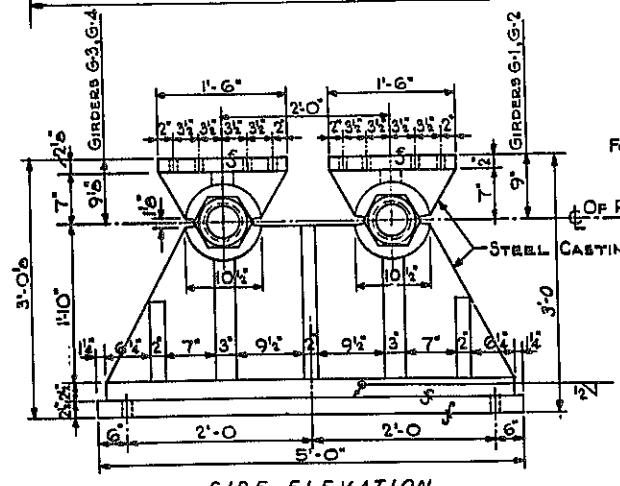
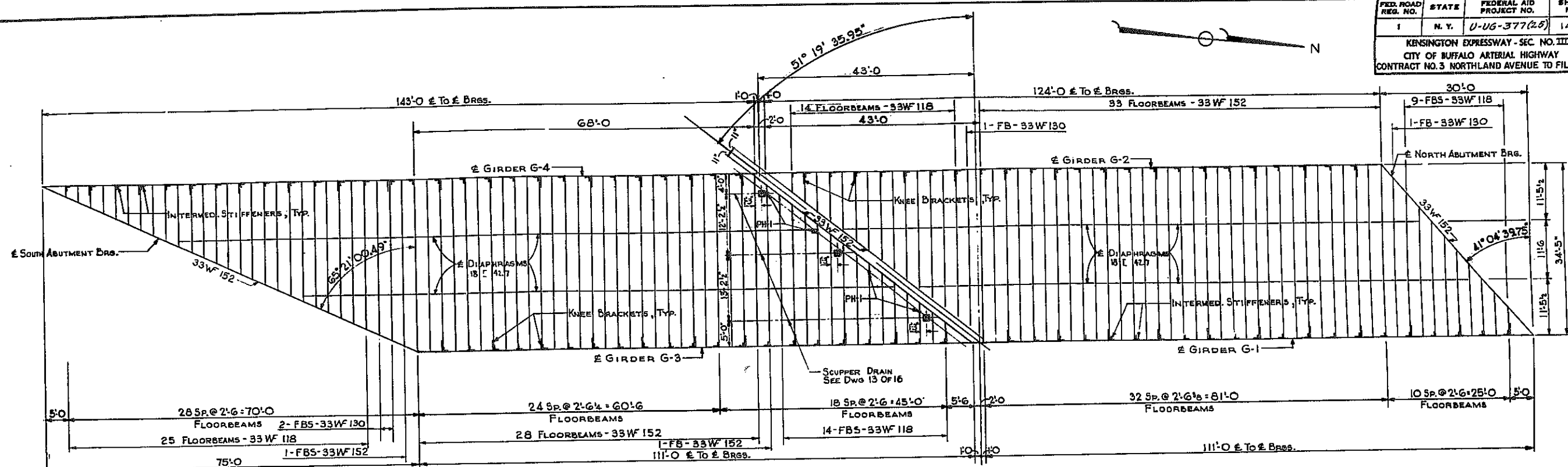


N.Y.C.R.R. OVER EXPRESSWAY WINGWALLS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL HIGHWAY KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED R.E.K. C.E.

DRAWING NO. 8 OF 16

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	140	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- WELDING NOTES:**
- THE DIMENSIONS OF INDIVIDUAL BALLAST PLATES SHALL BE SUCH THAT THE BUTT JOINT JOINING ADJACENT PLATES SHALL BE DIRECTLY OVER THE WEB OF A FLOOR BEAM AND THAT THE PLATES ARE CONTINUOUS ACROSS THE FULL WIDTH OF BRIDGE.
 - BALLAST PLATES SHALL BE IN INTIMATE CONTACT WITH THE FLOOR BEAMS BEFORE WELDING IS BEGUN.
 - WELDING OF INDIVIDUAL FLOOR BEAMS SHALL START AT CENTER-LINE OF BEAM AND WORK OUTWARD SIMULTANEOUSLY TOWARD EACH END TO THE SATISFACTION OF DEPUTY CHIEF ENGINEER (BRIDGES), UNLESS ANOTHER PROCEDURE CAN BE SHOWN TO REDUCE DISTORTION TO A GREATER DEGREE.
 - THE WELDING OF EACH FLOOR BEAM SHALL BE COMPLETED BEFORE THE BALLAST PLATE IS ATTACHED TO AN ADJACENT BEAM.

**N.Y.C.R.R. OVER EXPRESSWAY
STEEL LAYOUT PLAN**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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DRAWING NO. 9 OF 16

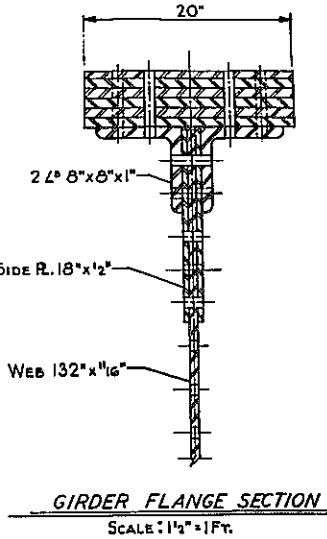


FIG 63-2

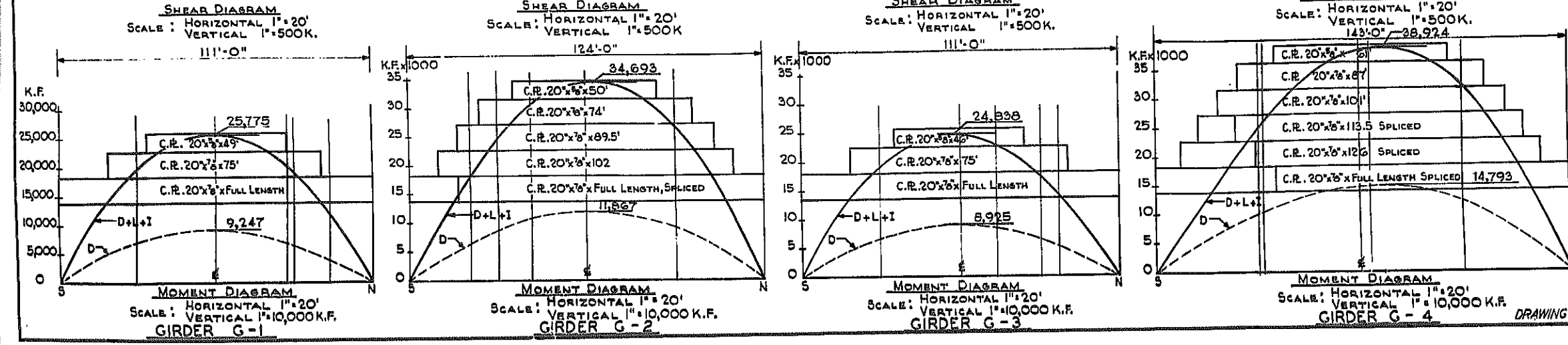
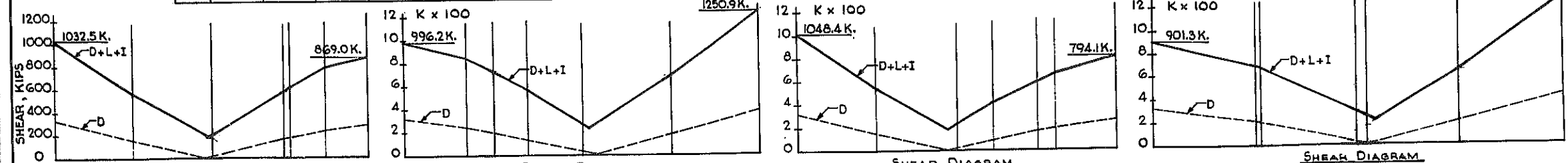
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	141	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

MARK	SPAN C. TO C. BEARINGS L. FT.	MAXIMUM MOMENTS				SECT. MOD. REQUIRED		MAXIMUM SHEARS				WEB REQ'D. IN ²	SECTION	L TO B Ls FT. IN.	SECT. MOD. OBTAINED		UNIT STRESS OBTAINED		WEB OBT.	BEARING AREA		REMARKS		
		D.L.	L.L.	I	TOTAL	COMP.	TENS.	D.L.	L.L.	I	TOTAL				COMP.	TENS.	COMP.	TENS.		COMP.	TENS.		REQ'D.	OBT.
		KIP - FT.				IN ³		KIPS							IN ³		LBS. PER SQ. IN.			IN ²				
G-1	111.0	9,247	13,086	3,442	25,775	11,571	11,456	HEAVY END (SOUTH) 338.7 549.3	144.5	1,032.5	64.53	11'-0"	13,509	11,518	22,895	26,855	90.75	HEAVY END 1570 LIGHT END 1358	1" RIVETS 1 1/2" OPEN HOLES					
G-2	124.0	11,867	18,217	4,609	34,693	15,575	15,419	HEAVY END (NORTH) 393.0 684.7	173.2	1,250.9	78.18	11'-0"	17,977	15,495	23,158	26,868	90.75	HEAVY END 2052.8 LIGHT END 1968	1" RIVETS 1 1/2" OPEN HOLES					
G-3	111.0	8,925	12,599	3,314	24,838	11,151	11,039	HEAVY END (SOUTH) 333.0 566.4	149.0	1,048.4	65.52	11'-0"	13,509	11,518	22,064	25,877	90.75	HEAVY END 1793 LIGHT END 1609	1" RIVETS 1 1/2" OPEN HOLES					
G-4	143.0	14,793	19,709	4,769	38,924	17,474	17,300	HEAVY END (NORTH) 438.6 676.3	163.7	1,278.6	79.91	11'-0"	20,219	17,491	23,101	26,704	90.75	HEAVY END 2105.9 LIGHT END 1982.4	1" RIVETS 1 1/2" OPEN HOLES					



MARK	SPAN C TO C GIRDERS	UNIFORM DEAD LOAD LBS. PER LIN. FOOT		MAXIMUM MOMENTS THOUSAND FOOT POUNDS			SECT. MOD. REQ'D	MAXIMUM SHEARS THOUSAND POUNDS				SECTION	SECT. MOD. OBTAINED		UNIT STRESS OBTAINED #/S		REMARKS		
		FLOOR	STEEL	D.L.	L.L.	I		TOTAL	D.L.	L.L.	I		TOTAL	COMP.	TENS.	COMP.		TENS.	
		KIP - FT.		KIP - FT.				KIPS					IN ³		LBS. PER SQ. IN.				
	34.5'	388	217	605	93.6	489.5	207.5	790.6	526.9	11.50	49.5	20.1	81.1	R. 30 3/8" x 5/8" BEAM 55W 152	97.8.6	55.0.8	9,695	17,220	32.12 MAX. SPAN IN SKEW ENDS
		388	194	577	78.3	433.2	186.1	697.6	465.1	10.81	64.2	25.7	100.7	R. 30" x 5/8" BEAM 55WF 150	883.7	465.1	9,470	18,000	30.04 MAX. SPAN IN SKEW ENDS
		388	182	565	68.1	387.2	168.8	624.1	416.1	10.20	62.4	25.1	97.7	R. 30" x 5/8" BEAM 55WF 118	889.8	417.0	8,500	17,960	1278.6K.



NOTE: SEE DWG. NO. 2 OF 16 FOR NOTES ON GIRDER MATERIAL.

N.Y.C.R.R. OVER EXPRESSWAY
STRESS SHEET

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

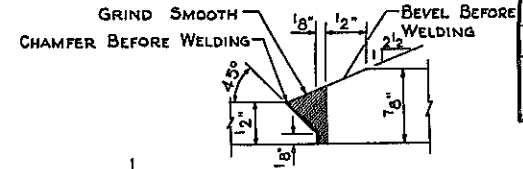
DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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DRAWING NO. 10 OF 16

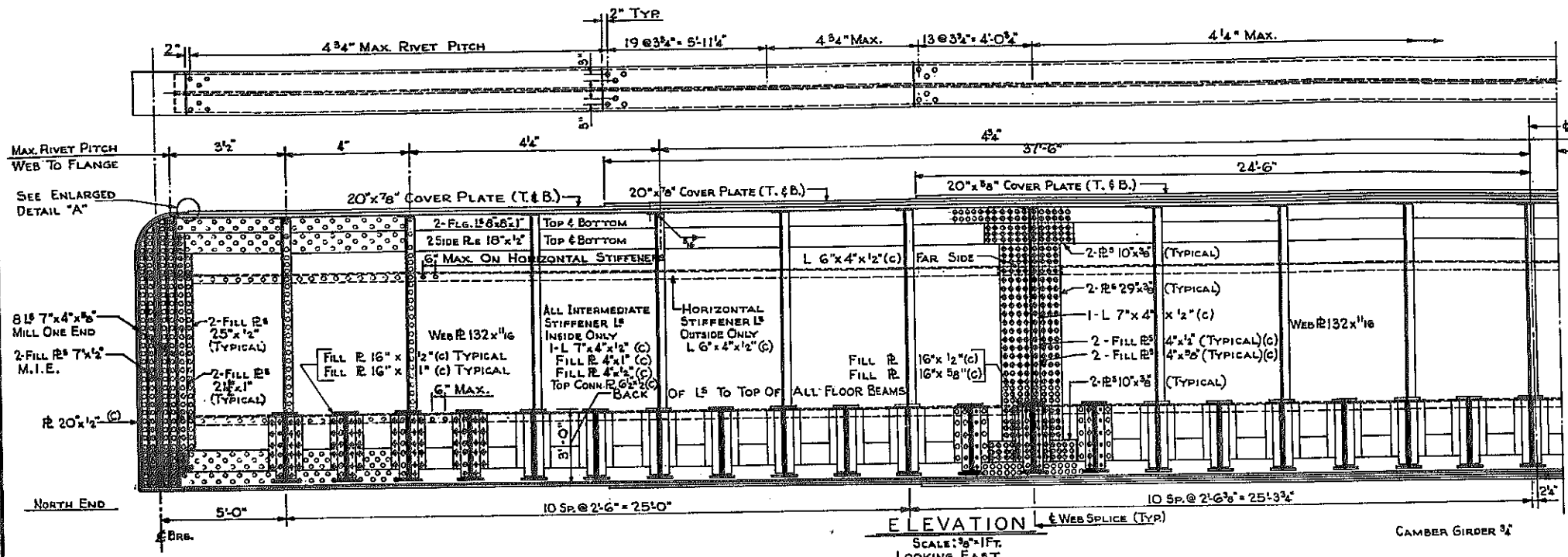


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(26)	142	173

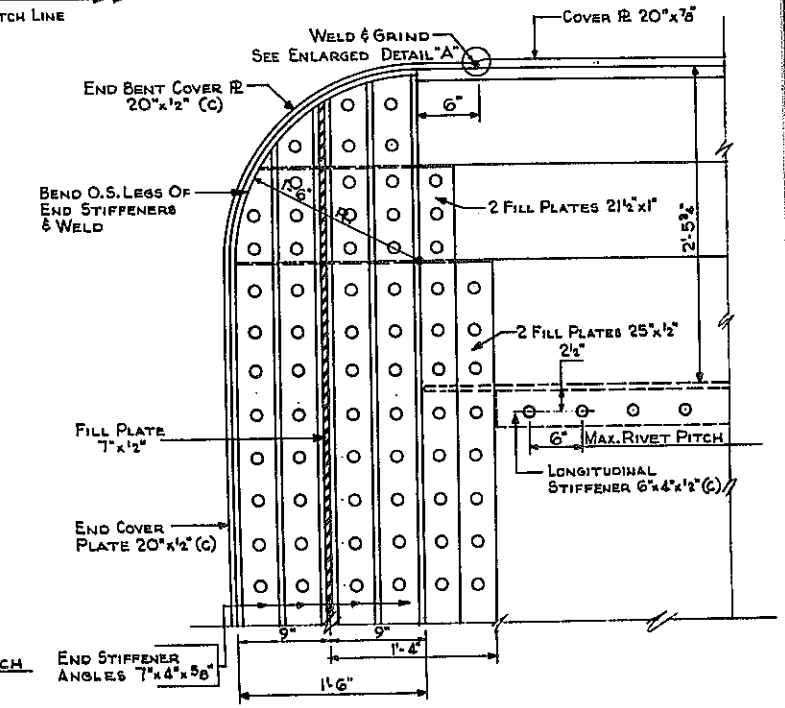
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



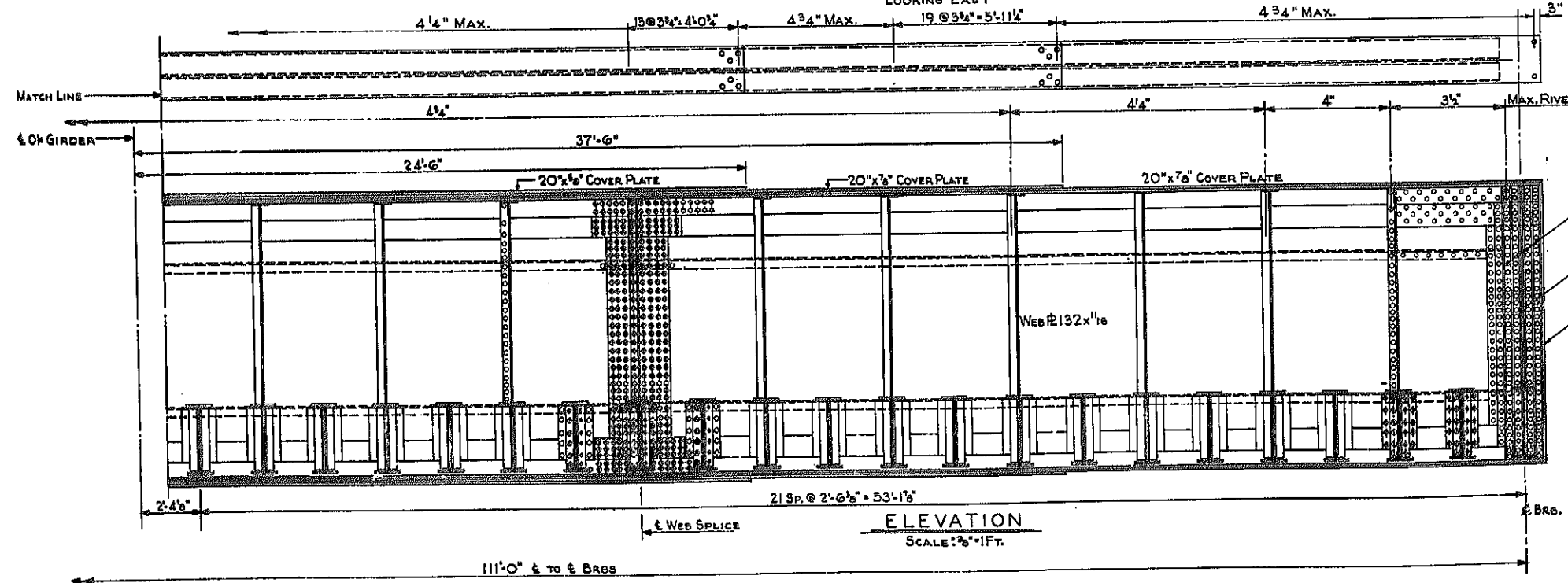
ENLARGED DETAIL "A"
SCALE: FULL SIZE



ELEVATION - LOOKING EAST
SCALE: 3/8\"/>



DETAIL AT END OF GIRDER
SCALE: 1/2\"/>



ELEVATION - LOOKING WEST
SCALE: 3/8\"/>

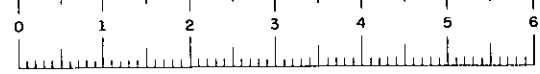
- NOTES:
- SEE DWG. NO. 2 FOR NOTES ON GIRDER MATERIAL.
 - PARTS MARKED (C) ARE STRUCTURAL STEEL, A.S.T.M.-A7-56T.
 - RIVETS 1\"/>

NOTE: BRACKETS NOT SHOWN ON GIRDER G-1, FOR LOCATION SEE DWG. NO. 9, AND FOR DETAIL SEE DWG. NO. 1.

N.Y.C.R.R. OVER EXPRESSWAY
GIRDER G-1

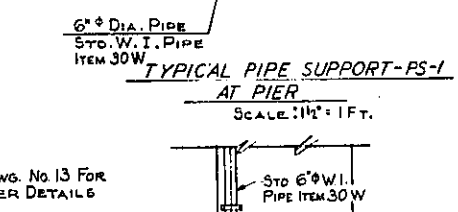
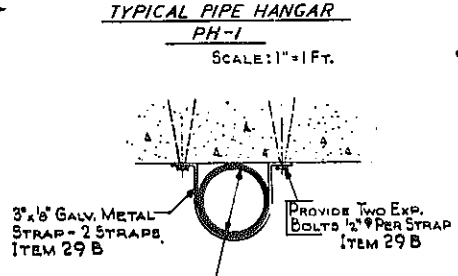
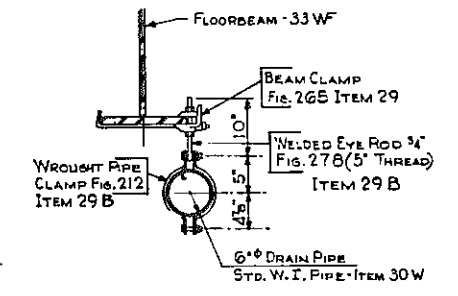
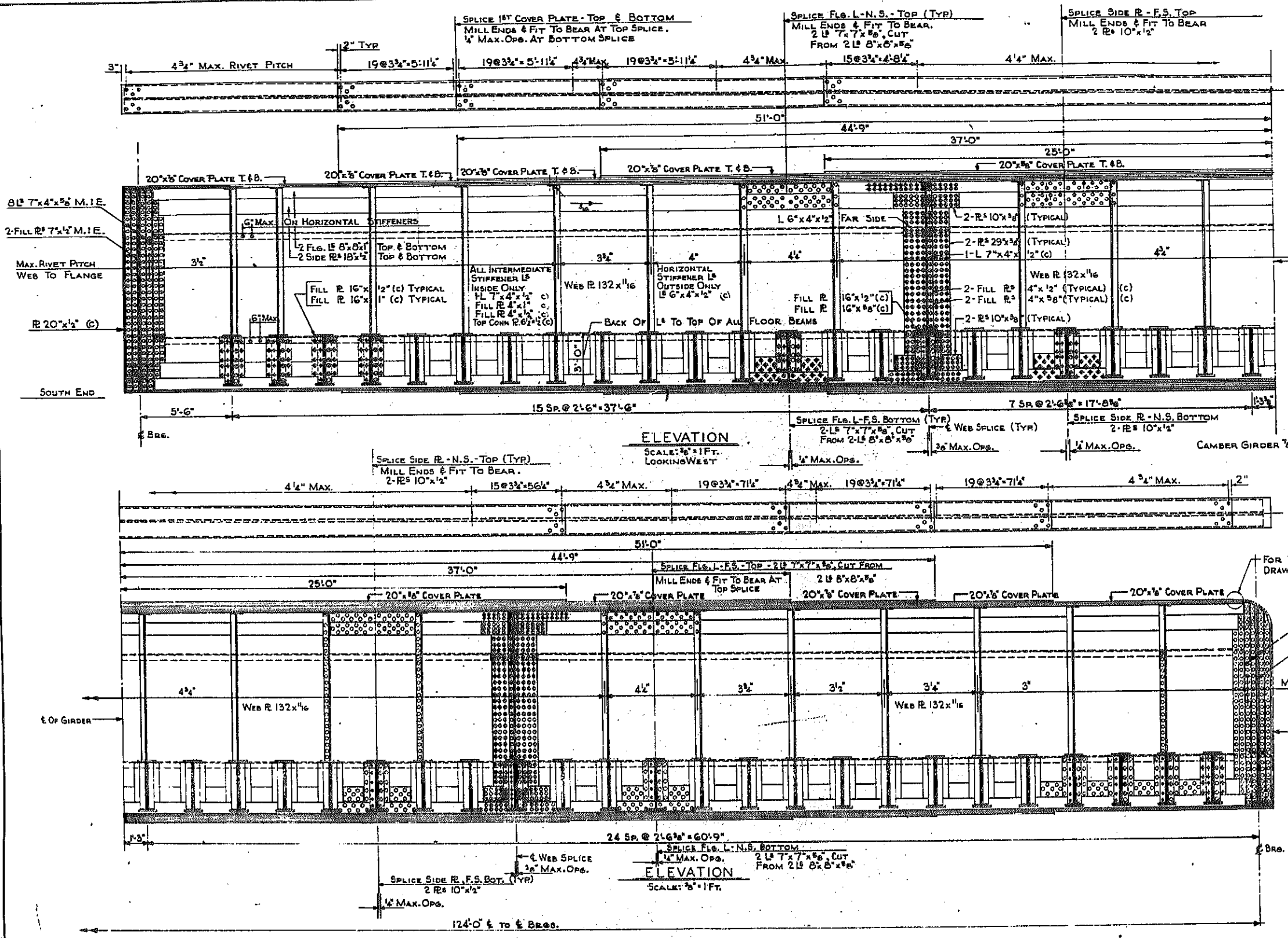
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEWY, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	143RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SEE DWG. NO. 13 FOR SCUPPER DETAILS
FOR DETAILS SEE FB-3 SEC. 6 PG 125

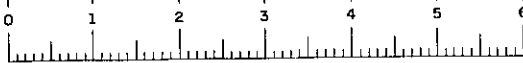
FOR WELD SEE DETAIL 'A', DRAWING NO. 11.

NOTES:
SEE SHEET NO. 2 FOR NOTES ON GIRDER MATERIAL.
PARTS MARKED (C) ARE STRUCTURAL STEEL, A.S.T.M.-A7-56T.
RIVETS 1" Ø, OPEN HOLES 1 1/2" Ø EXCEPT AS OTHERWISE NOTED.

REVISION NOTE Added 11

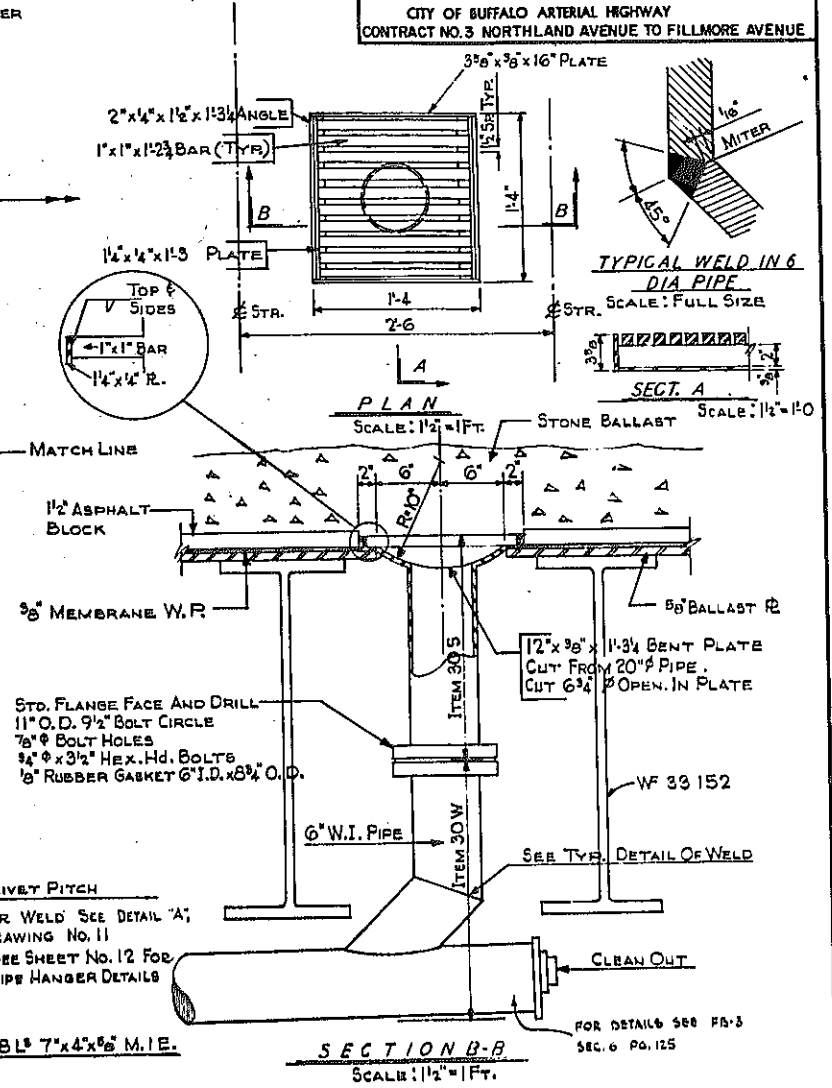
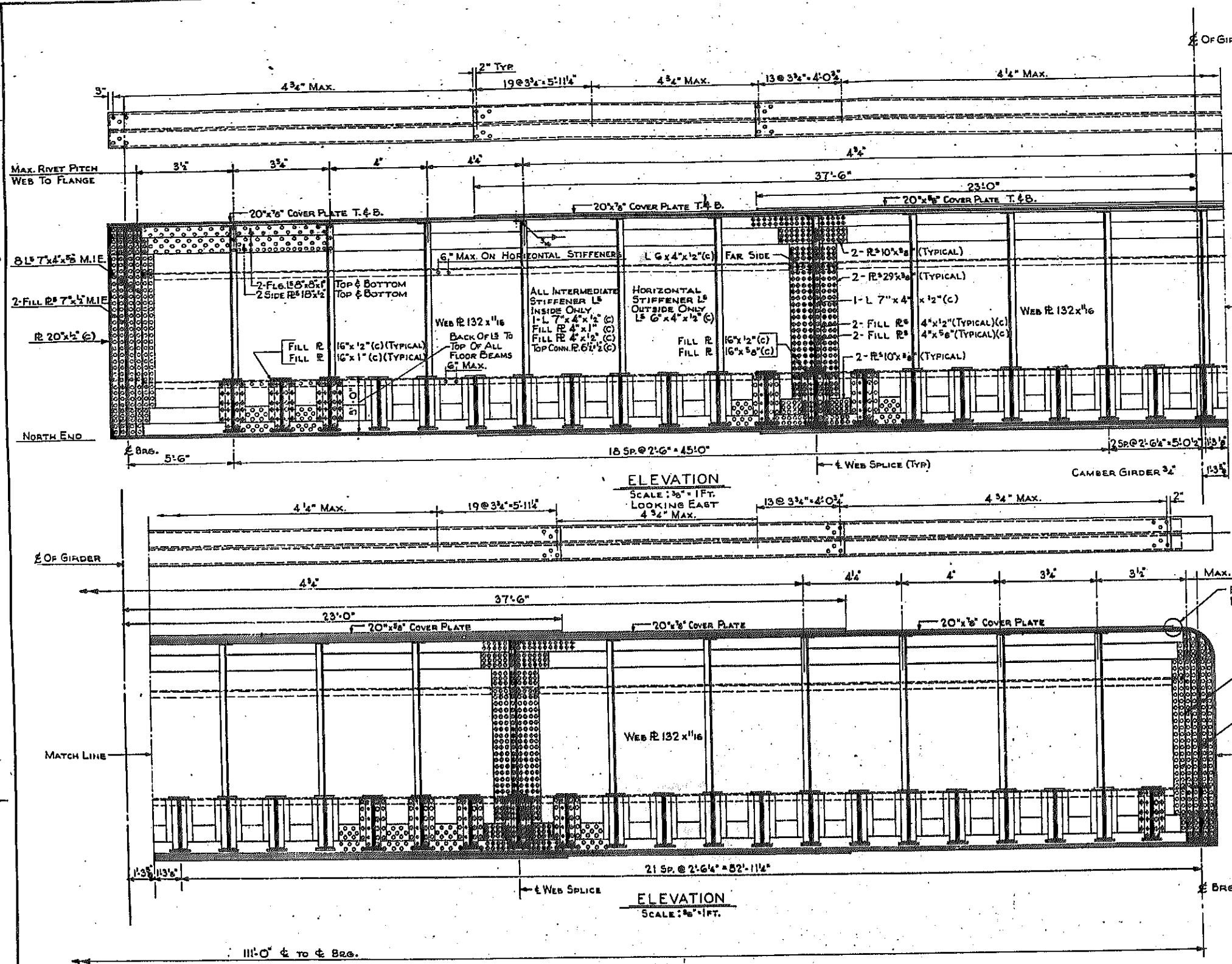
N.Y.C.R.R. OVER EXPRESSWAY GIRDER G-2	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED
	R.E.K. C.E.

DRAWING NO 12 OF 16



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	144R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



DETAILS OF SCUPPER DRAINS

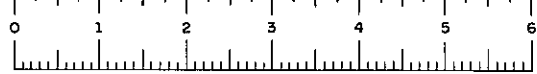
NOTE:
MATERIAL IN GRATING AND FRAME SHALL MEET THE A.S.T.M. SPECIFICATION A 242, EXCEPT THAT MIN. TENSILE STRENGTH MAY BE 65,000 P.S.I. AND THE MIN. YIELD POINT MAY BE 45,000 P.S.I.
ALL WELDS TO BE $\frac{5}{16}$ " FILLET WELDS.
PAYMENT FOR FURNISHING AND PLACING THE SCUPPER DRAINS WILL BE MADE UNDER ITEM 305, SEE DRAWING NO. 3 FOR PLAN ARRANGEMENT.

NOTES:
SEE SHEET NO. 2 FOR NOTES ON GIRDER MATERIAL.
PARTS MARKED (C) ARE STRUCTURAL STEEL A.S.T.M.-A7-56T.
RIVETS 1" ϕ , OPEN HOLES 1 1/8" ϕ EXCEPT AS OTHERWISE NOTED.

REVISION NOTE ADDED 87

NY.C.R.R. OVER EXPRESSWAY GIRDER G-3		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.

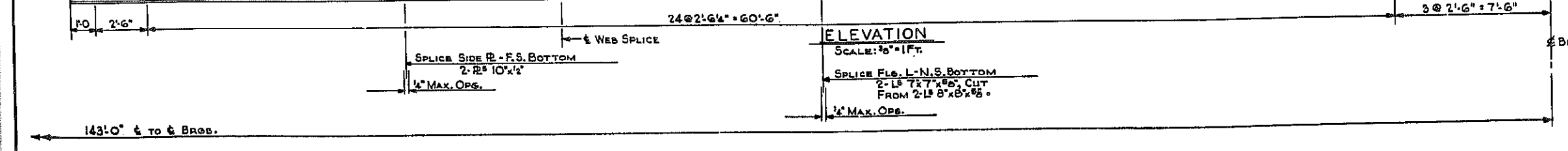
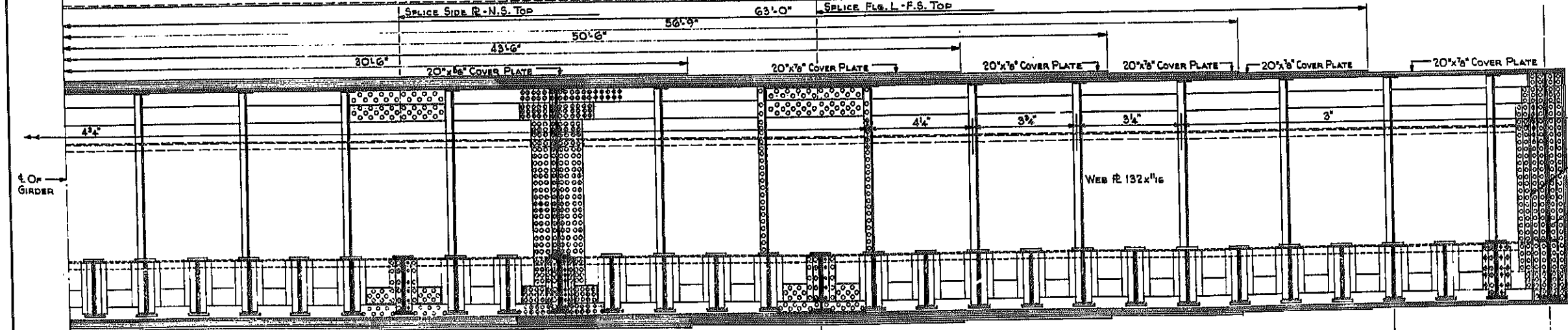
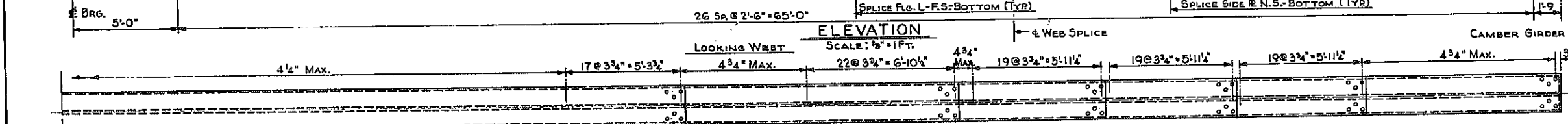
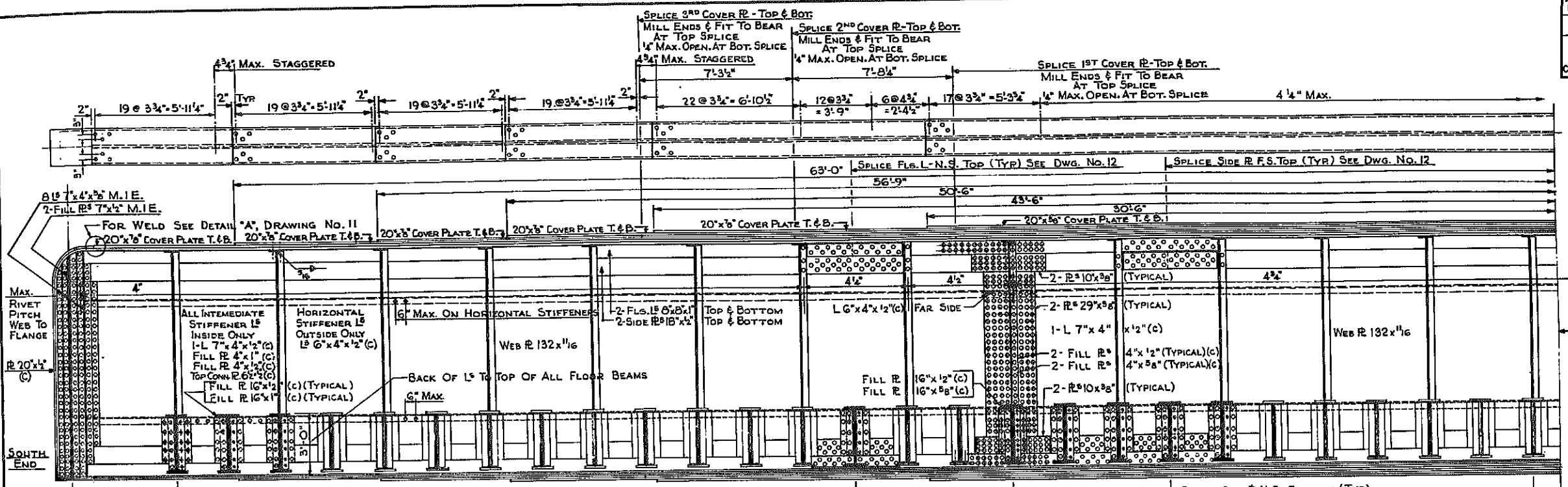
DRAWING NO. 13 OF 16



040 00-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	145	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
SEE SHEET NO. 2 FOR NOTES ON GIRDER MATERIAL.
PARTS MARKED (C) ARE STRUCTURAL STEEL, A.S.T.M.-A7-56 T.
RIVETS 1" Ø, OPEN HOLES 1 1/8" Ø EXCEPT AS OTHERWISE NOTED.

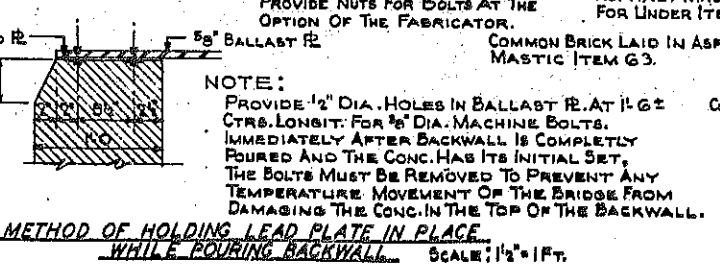
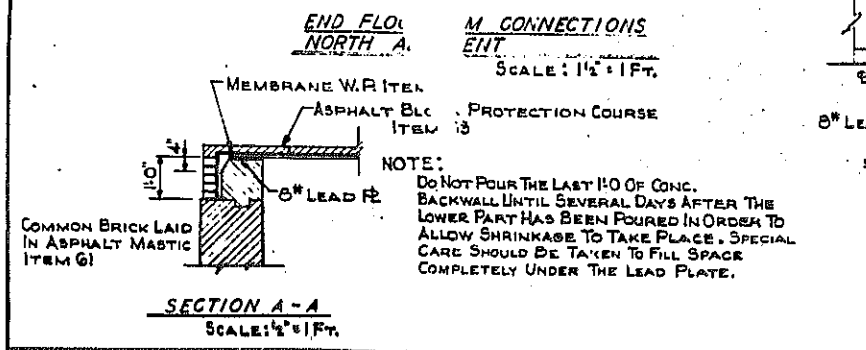
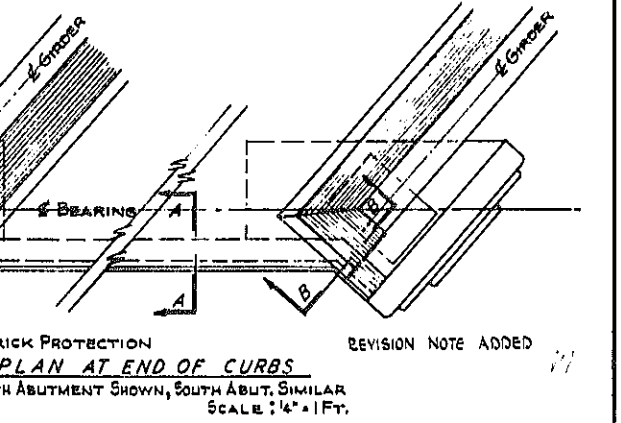
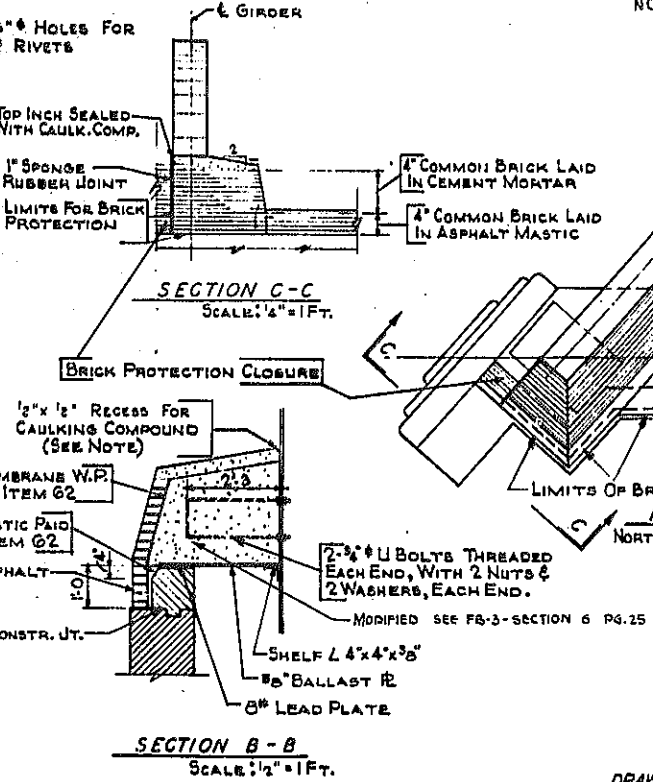
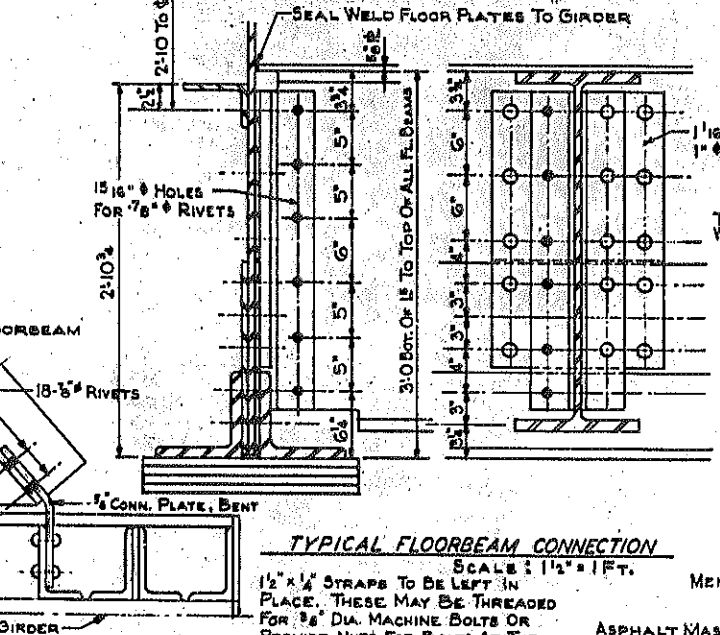
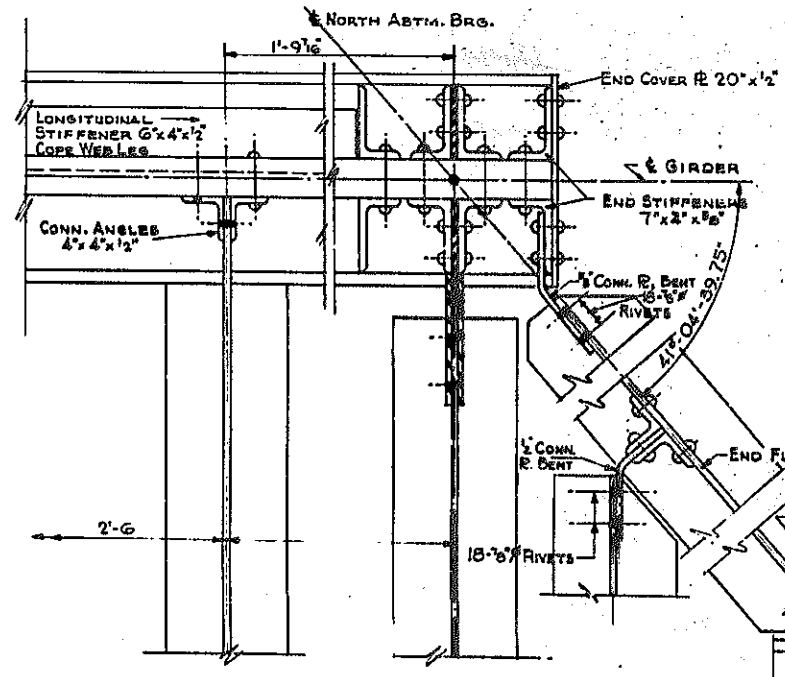
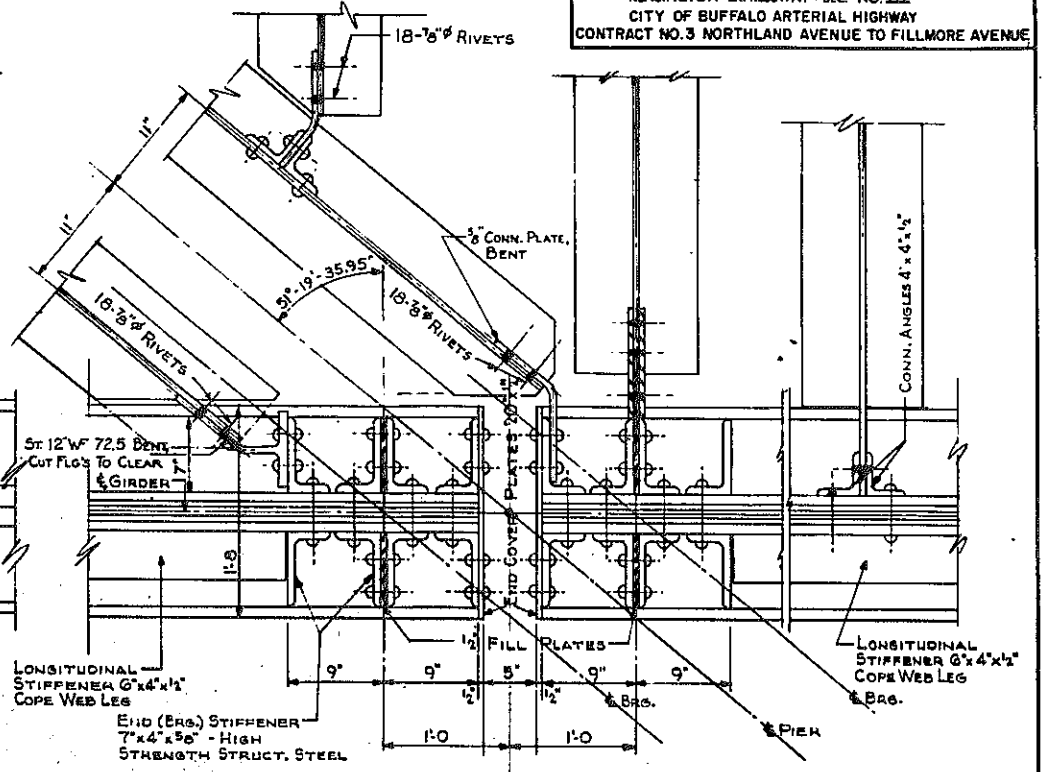
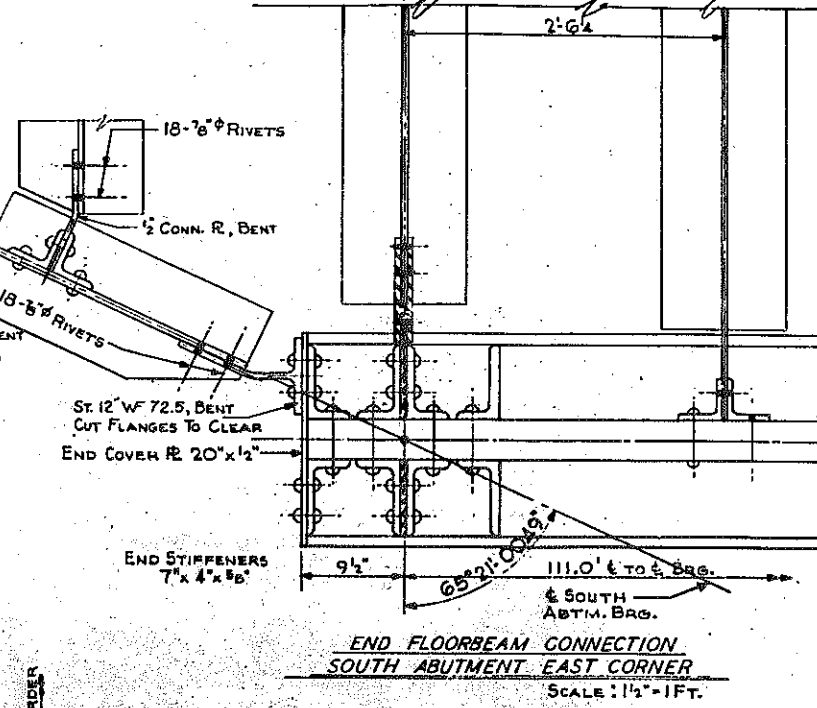
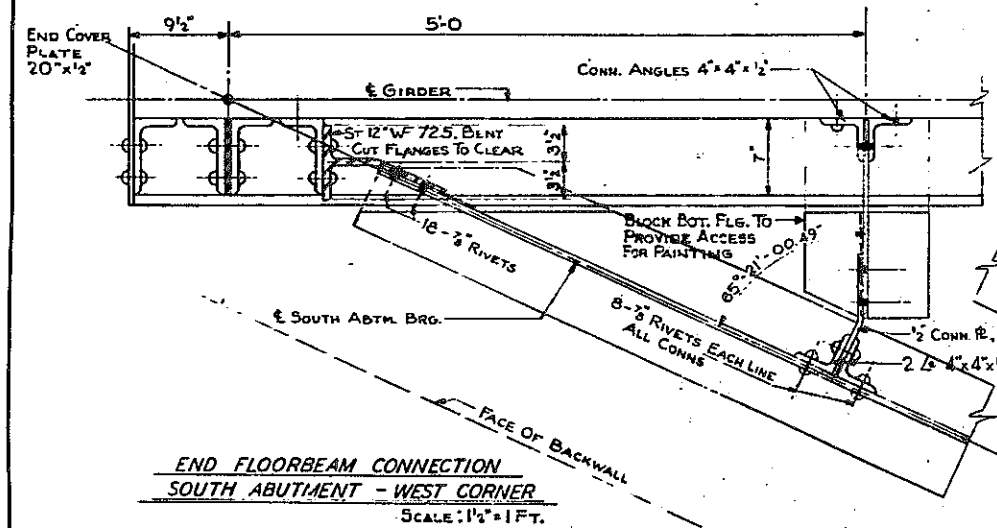
MAX. RIVET PITCH
8L 7"x4"x5/8" M.I.E.
2-FILL R 7"x1/2" M.I.E.
R 20"x1/2" (C)
NORTH END

N.Y.C.R.R. OVER EXPRESSWAY GIRDER G-4		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DR. LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.

DRAWING NO 14 OF 16

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	146R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



END FLOORBEAM CONNECTION TO EAST GIRDERS AT PIER
SCALE: 1/2" = 1 FT.
CONNECTIONS TO WEST GIRDERS SIMILAR BY ROTATION

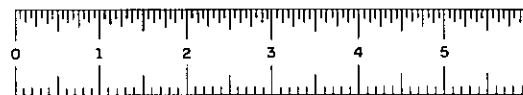
NOTE:
A 3/8" BEAD OF CAULKING COMPOUND SHALL BE APPLIED TO THE TOP OF CURBS AND CURB CONSTRUCTION JOINTS ALONG GIRDER WEBS AND AROUND BRACKETS AND STIFFENER ANGLES WHERE SHOWN ON THE PLANS. COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 18 X.

NOTE:
DO NOT POUR THE LAST 1' 0" OF CONC. BACKWALL UNTIL SEVERAL DAYS AFTER THE LOWER PART HAS BEEN POURED IN ORDER TO ALLOW SHRINKAGE TO TAKE PLACE. SPECIAL CARE SHOULD BE TAKEN TO FILL SPACE COMPLETELY UNDER THE LEAD PLATE.

NOTE:
PROVIDE 1/2" DIA. HOLES IN BALLAST R. AT 11" G.C. CTRS. LONGIT. FOR 3/8" DIA. MACHINE BOLTS. IMMEDIATELY AFTER BACKWALL IS COMPLETELY POURED AND THE CONC. HAS ITS INITIAL SET, THE BOLTS MUST BE REMOVED TO PREVENT ANY TEMPERATURE MOVEMENT OF THE BRIDGE FROM DAMAGING THE CONC. IN THE TOP OF THE BACKWALL.

N.Y.C.R.R. OVER EXPRESSWAY SUPERSTRUCTURE DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.

DRAWING NO. 15 OF 16



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	147R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR LIST

ABUTMENTS

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
SOUTH ABUTMENT - FOOTING								
A-401	#4	6'-6"	STR	110				TRANS. BARS
A-402	#4	7'-5"	STR	17				TRANS. BARS (VARIES 5'-6" TO 10'-0")
A-403	#4	9'-2"	STR	12				TRANS. BARS
A-404	#4	8'-5"	STR	7				TRANS. BARS (VARIES 7'-6" TO 9'-2")
A-405	#4	8'-0"	STR	2				TRANS. BARS
A-801	#8	4'-5"	STR	6				LONG BARS (VARIES 4'-6" TO 8'-6")
A-802	#8	4'-8"	STR	8				LONG BARS (VARIES 1'-8" TO 7'-8")
A-803	#8	5'-0"	STR	2				LONG BARS
A-804	#8	5'-0"	STR	2				LONG BARS
A-805	#8	5'-0"	STR	2				LONG BARS
A-806	#8	10'-0"	STR	2				LONG BARS
A-807	#8	2'-5"	STR	12				LONG BARS (VARIES 1'-6" TO 8'-0")
A-808	#8	4'-0"	STR	2				LONG BARS
A-809	#8	4'-0"	STR	2				LONG BARS
A-810	#8	11'-0"	STR	56				LONG BARS
SOUTH ABUTMENT WALLS								
A-810	#8	9'-6"	STR	14				HORIZ. BARS ABUT.
A-406	#4	11'-0"	STR	24				VERT. BARS - CURTAIN WALLS
A-407	#4	13'-6"	STR	24				VERT. BARS - CURTAIN WALLS
A-408	#4	4'-0"	I	24	1'-0"	3'-0"		BENT BARS - CURTAIN WALLS
A-409	#4	3'-10"	I	20	1'-4"	2'-6"		BENT BARS - CURTAIN WALLS
A-410	#4	4'-0"	STR	28				DOWELS - CURTAIN WALLS
A-411	#4	7'-10"	II	13	4'-8"	1'-8"		HORIZ. BARS - CURTAIN WALLS
A-412	#4	10'-10"	II	15	8'-4"	1'-4"		HORIZ. BARS - CURTAIN WALLS
A-413	#4	11'-6"	II	17	9'-8"	1'-0"		HORIZ. BARS - CURTAIN WALLS
A-414	#4	2'-4"	STR	5				HORIZ. BARS - ABUT.
A-415	#4	8'-6"	STR	4				VERT. BARS - ABUT.
A-416	#4	4'-0"	STR	5				HORIZ. BARS - ABUT.
A-417	#4	4'-4"	II	46	8"	1'-10"		BENT BARS - ABUT.
A-601	#6	10'-9"	STR	88				VERT. BARS - ABUT.
A-602	#6	42'-0"	STR	20				HORIZ. BARS - ABUT.
A-603	#6	36'-10"	STR	4				HORIZ. BARS - ABUT.
A-604	#6	8'-6"	STR	4				HORIZ. BARS - ABUT.
A-605	#6	3'-8"	STR	4				HORIZ. BARS - ABUT.
A-606	#6	5'-4"	STR	8				HORIZ. BARS - ABUT.
A-607	#6	8'-0"	STR	25				VERT. BARS - ABUT.
A-608	#6	6'-8"	STR	21				VERT. BARS - ABUT.
A-609	#6	10'-6"	II	25	3'-6"	3'-6"		BENT HORIZ. BARS - PEDESTALS
A-610	#6	3'-6"	STR	87				DOWELS - ABUT. - FTG.
A-611	#6	3'-0"	STR	4				HORIZ. BARS - ABUT.
A-612	#6	7'-10"	STR	8				HORIZ. BARS - ABUT.
NORTH ABUTMENT - FOOTING								
A-451	#4	6'-6"	STR	62				TRANS. BARS
A-452	#4	5'-4"	STR	4				TRANS. BARS
A-453	#4	10'-6"	STR	2				TRANS. BARS
A-454	#4	10'-9"	STR	2				TRANS. BARS
A-455	#4	10'-0"	STR	2				TRANS. BARS
A-456	#4	9'-5"	STR	2				TRANS. BARS
A-457	#4	7'-8"	STR	2				TRANS. BARS
A-458	#4	9'-8"	STR	2				TRANS. BARS
A-459	#4	8'-5"	STR	2				TRANS. BARS
A-460	#4	8'-5"	STR	2				TRANS. BARS
A-461	#4	7'-5"	STR	2				TRANS. BARS
A-851	#8	10'-6"	STR	16				LONG BARS
A-852	#8	9'-11"	STR	12				LONG BARS (VARIES 5'-4" TO 10'-6")
A-853	#8	5'-8"	STR	10				LONG BARS (VARIES 1'-8" TO 5'-8")
A-854	#8	4'-0"	STR	2				HORIZ. BARS
A-855	#8	30'-6"	STR	56				LONG BARS
NORTH ABUTMENT - EAST WINGWALL								
P	#10	8'-0"		7				HORIZ. BARS - FTG.
O	#8	8'-2"		7				HOOK BARS - FTG.
Q	#6	9'-6"		7				VERT. BARS - FTG.
L	#4	6'-2"		9				HORIZ. BARS - TOP FTG.
L	#4	6'-2"		3				HORIZ. BARS - BOT. FTG.
F	#6	12'-10"		7				VERT. BARS - WALL
H	#4	6'-2"		3				HORIZ. BARS - WALL (NF)
H	#4	6'-2"		3				HORIZ. BARS - WALL (RF)
H	#4	11'-8"		10				HORIZ. BARS - WALL (NF)
H	#4	11'-8"		4				HORIZ. BARS - WALL (RF)
D	#4	8'-0"		4				HORIZ. BARS - FTG.
O	#7	7'-8"		1				HOOKS - FTG.
Q	#6	8'-2"		1				VERT. BARS - FTG.
L	#4	8'-2"		1				HORIZ. BARS - TOP
L	#4	8'-2"		1				HORIZ. BARS - BOT.
F	#6	10'-0"		1				VERT. BARS - WALL
DOWEL	#8	3'-0"		1				DOWELS - FTG.

ABUTMENTS

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
NORTH ABUTMENT - WALLS								
A-462	#4	12'-0"	STR	24				VERT. BARS - CURTAIN WALLS
A-463	#4	14'-6"	STR	24				VERT. BARS - CURTAIN WALLS
A-464	#4	4'-0"	I	24	1'-0"	3'-0"		BENT BARS - CURTAIN WALLS
A-465	#4	3'-10"	I	20	1'-4"	2'-6"		BENT BARS - CURTAIN WALLS
A-466	#4	4'-0"	STR	29				DOWELS - CURTAIN WALLS
A-467	#4	7'-10"	II	14	4'-8"	1'-8"		HORIZ. BARS - CURTAIN WALLS
A-468	#4	10'-10"	II	16	8'-4"	1'-4"		HORIZ. BARS - CURTAIN WALLS
A-469	#4	11'-6"	II	16	9'-8"	1'-0"		HORIZ. BARS - CURTAIN WALLS
A-470	#4	9'-6"	STR	12				HORIZ. BARS - CURTAIN WALLS
A-471	#4	4'-0"	STR	5				HORIZ. BARS - CURTAIN WALLS
A-472	#4	8'-0"	STR	5				VERT. BARS - CURTAIN WALLS
A-473	#4	3'-4"	STR	5				HORIZ. BARS - CURTAIN WALLS
A-474	#4	4'-4"	II	27	8"	1'-10"		BENT BARS - ABUT.
A-651	#6	8'-0"	STR	54				VERT. BARS - ABUT.
A-652	#6	26'-8"	STR	20				HORIZ. BARS - ABUT.
A-653	#6	10'-6"	II	20	3'-6"	3'-6"		HORIZ. BENT BARS - PEDESTALS
A-654	#6	3'-9"	STR	18				DOWELS - ABUT.
A-655	#6	18'-0"	STR	4				HORIZ. BARS - ABUT.
A-656	#6	10'-6"	STR	4				HORIZ. BARS - ABUT.
A-657	#6	5'-8"	STR	8				HORIZ. BARS - ABUT.
A-658	#6	4'-0"	STR	4				HORIZ. BARS - ABUT.
A-659	#6	5'-0"	STR	26				VERT. BARS - ABUT.
A-660	#6	6'-0"	STR	12				VERT. BARS - ABUT.
A-855	#8	8'-0"	STR	14				HORIZ. BARS ABUT. (VARIES 10'-0" TO 6'-0")
SOUTH ABUTMENT - WEST WINGWALL								
W-401	#4	7'-4"	STR	26				VERT. BARS (VARIES 8'-10" TO 5'-10")
W-402	#4	4'-2"	STR	26				VERT. BARS (VARIES 5'-10" TO 2'-10")
W-403	#4	24'-8"	STR	6				HORIZ. BARS
W-404	#4	12'-0"	STR	1				HORIZ. BARS
W-405	#4	24'-8"	STR	1				HORIZ. BARS
W-406	#4	24'-8"	STR	4				HORIZ. BARS
W-407	#4	12'-0"	STR	1				HORIZ. BARS
W-408	#4	24'-8"	STR	1				HORIZ. BARS
W-601	#6	3'-0"	STR	8				VERT. BARS
W-602	#6	3'-0"	STR	8				HORIZ. BARS (VARIES 1'-6" TO 4'-6")
SOUTH ABUTMENT - EAST WINGWALL								
W-409	#4	7'-10"	STR	14				VERT. BARS
W-410	#4	6'-4"	STR	1				VERT. BARS
W-411	#4	14'-8"	STR	8				HORIZ. BARS
W-412	#4	13'-2"	STR	1				HORIZ. BARS
W-603	#6	6'-11"	STR	8				VERT. BARS
W-604	#6	4'-11"	STR	2				VERT. BARS
W-605	#6	12'-8"	STR	16				HORIZ. BARS (VARIES 1'-6" TO 5'-0")
NORTH ABUTMENT - EAST WINGWALL								
W-413	#4	5'-11"	STR	14				VERT. BARS
W-414	#4	4'-8"	STR	1				VERT. BARS
W-415	#4	14'-8"	STR	8				HORIZ. BARS
W-416	#4	13'-2"	STR	1				HORIZ. BARS
W-606	#6	8'-5"	STR	8				VERT. BARS
W-607	#6	6'-8"	STR	2				VERT. BARS
W-608	#6	12'-8"	STR	18				HORIZ. BARS (VARIES 1'-6" TO 5'-0")
NORTH ABUTMENT - WEST WINGWALL								
PW-409	#4	8'-4"	STR	316				VERT. BARS HORIZ. BARS
PW-410	#4	7'-10"	STR	1				VERT. BARS HORIZ. BARS
PW-411	#4	9'-4"	STR	1-5				HORIZ. BARS
PW-412	#4	8'-4"	STR	1				HORIZ. BARS
QW-413	#4	8'-4"	STR	6-8				VERT. BARS HOOK BARS
QW-414	#4	0'-11"	STR	5-2				VERT. BARS
LW-415	#4	6'-8"	STR	620				HORIZ. BARS (VARIES 1'-6" TO 5'-0")
L	#4	5'-0"		1				HORIZ. BARS
L	#4	4'-0"		1				HORIZ. BARS
L	#4	3'-0"		1				HORIZ. BARS
L	#4	6'-2"		3				HORIZ. BARS
H	#4	7'-6"		8				HORIZ. BARS (2'-9" TO 7'-9" NR)
H	#4	7'-6"		8				HORIZ. BARS (RF)
H	#4	10'-6"		68				HORIZ. BARS (NF)
F	#6	11'-0"		78				VERT. BARS - WALL
F	#6	7'-6"		18				VERT. BARS
F	#6	6'-6"		37				VERT. BARS
F	#6	4'-0"		37				VERT. BARS
F	#6	4'-0"		1				VERT. BARS
DOWEL	#8	3'-0"		1				DOWELS - FTG.

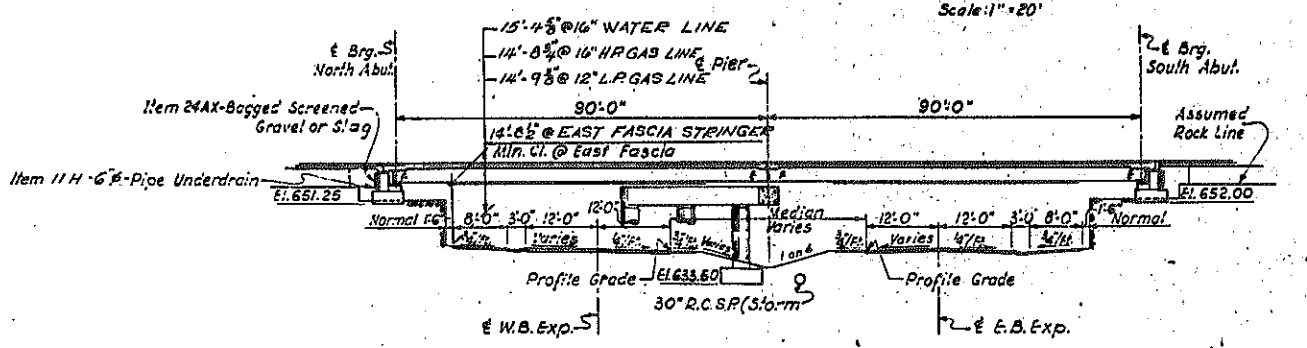
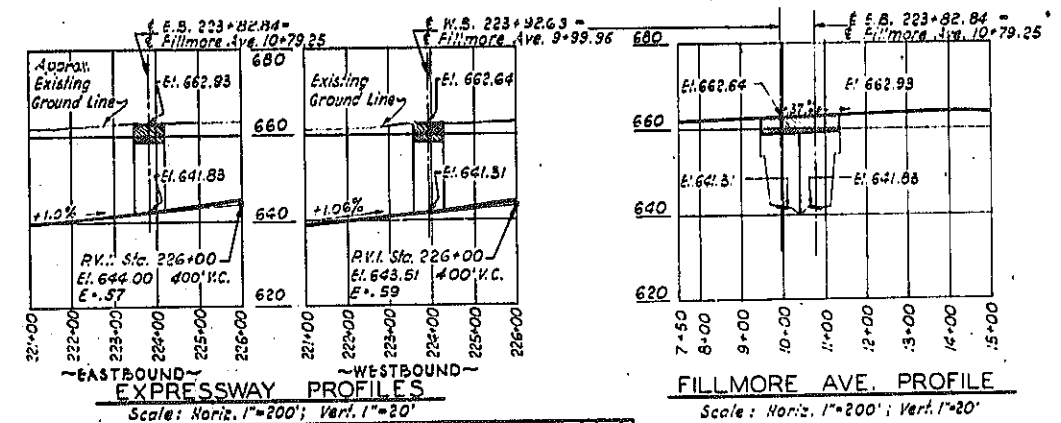
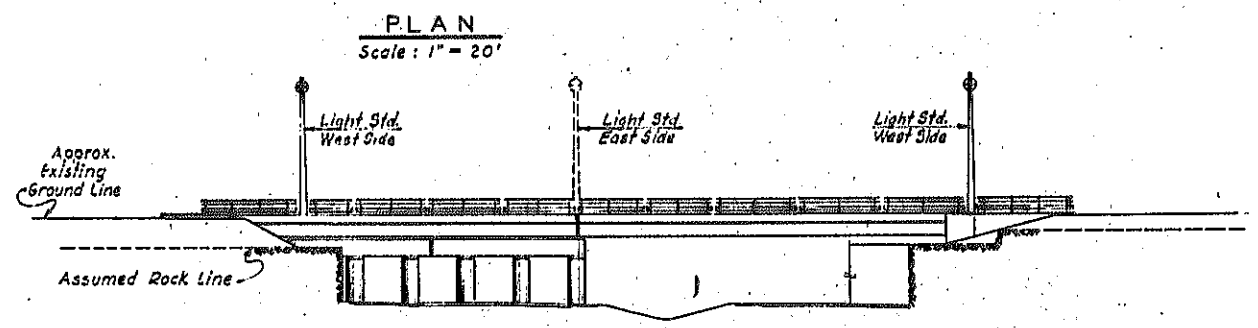
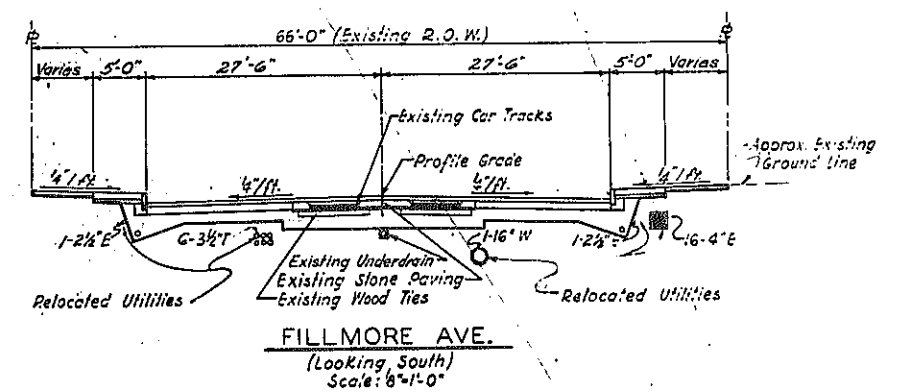
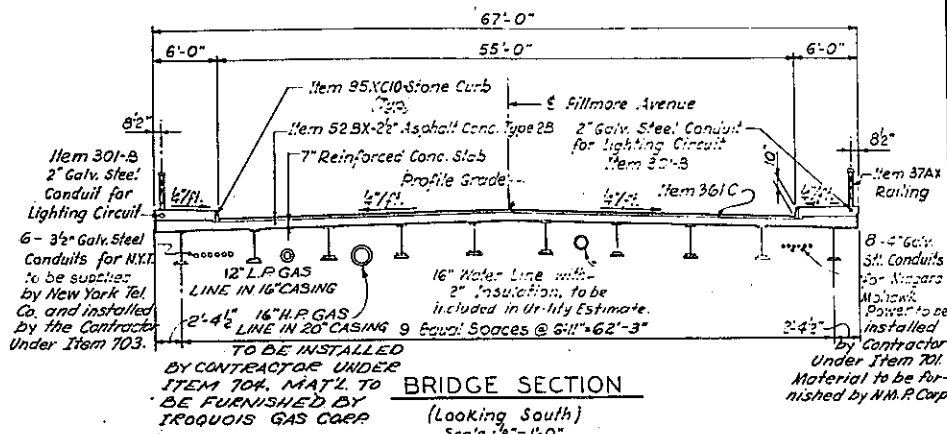
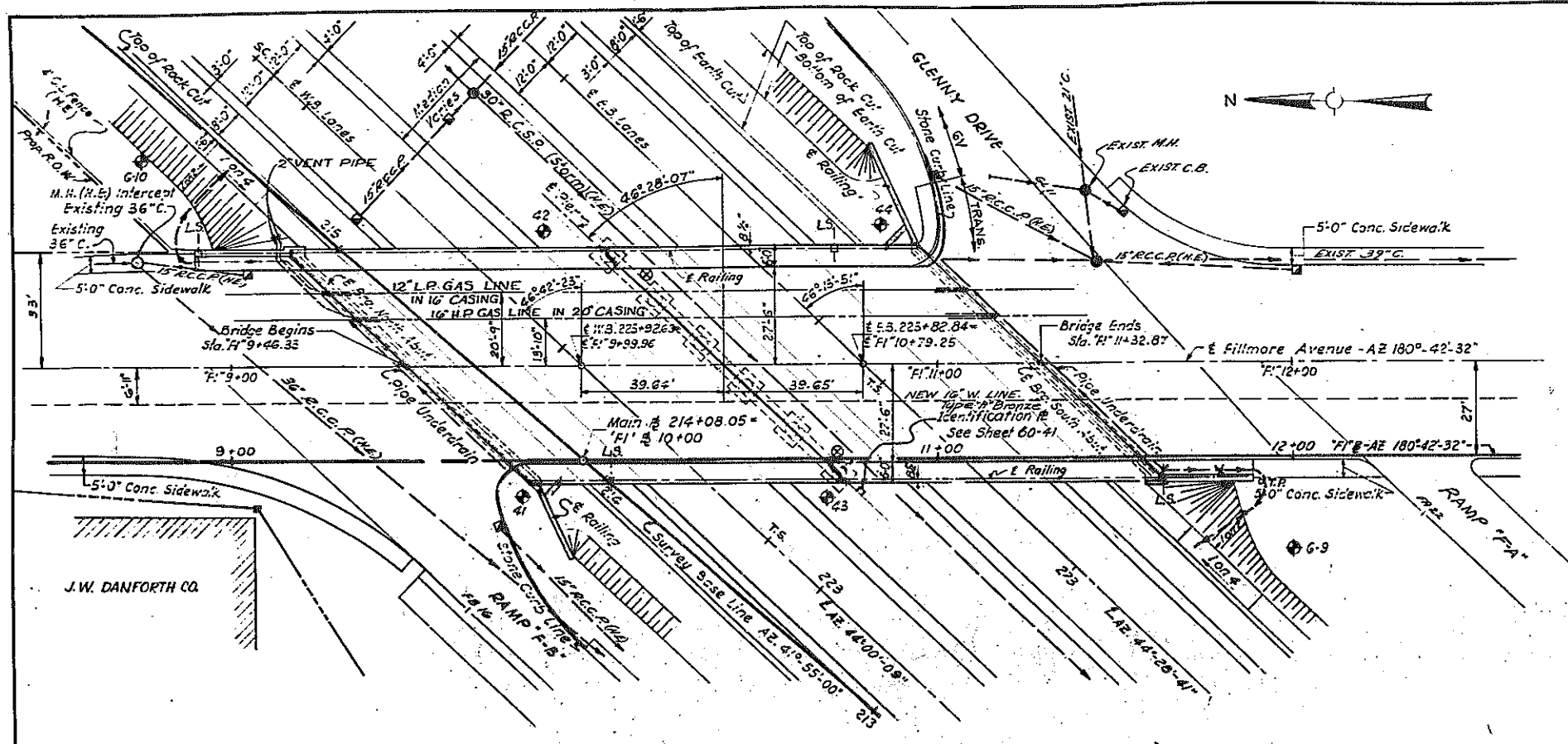
PIER

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
FOOTING								
P401	#4	6'-0"	STR	22				TRANS. BARS T. & B. VARIES 4'-6" TO 9'-3"
P402	#4	9'-6"	STR	92				TRANSVERSE BARS TOP & BOT.
P901	#9	37'-9"	STR	80				HORIZONTAL BARS TOP & BOT.
P1001	#10	6'-0"	STR	48	4'-11"	1'-1"	9"	DOWELS - FTG.
COLUMNS - BEAM & WINGWALL								
P403	#4	24'-4"	STR	12				VERTICAL BARS - E. WW.
P404	#4	26'-10"	STR	12				VERTICAL BARS - E. WW.
P405	#4	4'-0"	I	24	1'-0"	3'-3"		BENT BARS E. & W. WW.
P406	#4	5'-1"	I	20	1'-4"	4'-0"		BENT BARS E. & W. WW.
P407	#4	7'-6"	II	28	4'-8"	1'-8"		HORIZONTAL BARS E. & W. WW.
P408	#4	10'-6"	II	28	8'-4"	1'-4"		HORIZONTAL BARS E. & W. WW.
P409	#4	11'-2"	II	30	9'-8"	1'-0"		HORIZONTAL BARS E. & W. WW.
P410	#4	2'-3"	STR	24				DOWELS - FTG.
P411	#4	24'-7"	STR	12				VERTICAL BARS - W. WW.
P412	#4	27'-1"	STR	12				VERTICAL BARS - W. WW.
P413	#4	10'-9"	III	32	3'-2"			COLUMN TIES
P414	#4	3'-0"	IV	40	2'-6"	6"		CAP PEDESTALS DOWELS
P501	#5	14'-2"	IV	82	4'-8"	2'-2"	4"	CAP STIRRUPS
P502	#5	4'-8"	STR	10				VERTICAL BARS - CAP BEAM
P601	#6	16'-6"	STR	54				VERTICAL BARS - EXT. COLS.
P602	#6	14'-6"	IV	12	3'-2"	3'-2"	7"	HORIZONTAL BARS PEDESTALS
P902	#9	24'-4"	STR	14				HORIZONTAL BARS - TOP OF CAP BEAM
P903	#9	24'-0"	STR	8				HORIZONTAL BARS - SIDES OF CAP BEAM
P904	#9	16'-7"	STR	14				HORIZONTAL BARS - BOT. OF CAP BEAM
P905	#9	17'-5"	STR	7				HORIZONTAL BARS - BOT. OF CAP BEAM
P906	#9	13'-1"	STR	36				HORIZONTAL BARS - EXT. COLS.
P907	#9	6'-8"	STR	36				HORIZONTAL BARS - EXT. COLS.
P908	#9	7'-0"	STR	36				HORIZONTAL BARS - EXT. COLS.
P1002	#10	18'-10"	STR	48				VERTICAL BARS - INT. COLS.
P603	#6	3'-2"	STR	54				DOWELS - FTG.
P801	#8	6'-2"	STR	14				HORIZ. BARS - E. & W. EXT. COLS. (VAR. 14'-0" TO 7'-0")
SUPERSTRUCTURE CONCRETE								
S601	#6	37'-6"	STR	16				HORIZONTAL BARS (AT GIRDER G-4)
S60								

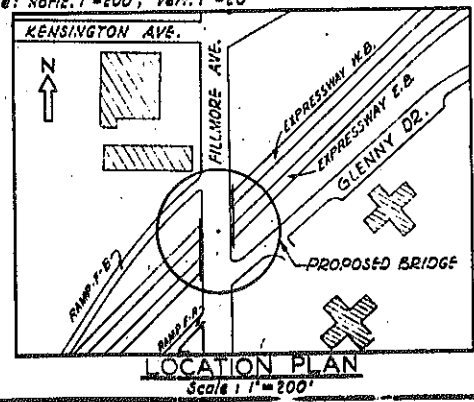


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	148R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- LEGEND**
- ⊙ - Indicates Boring Locations.
 - E - Expansion Bearings
 - F - Fixed Bearings
 - Items marked (H.E.) to be included in Highway Estimate.
 - ⊕ - Indicates Location of 1/2" Galvanized Pipe Drains



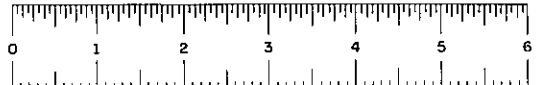
UNDERDRAIN & S. ABUT. REVISED BY LETTER DATED APRIL 15, 64.
UNDERDRAIN & N. ABUT. REVISED BY LETTER DATED APRIL 10, 64.

B2, SHEET NO. 1

FILLMORE AVENUE OVER EXPRESSWAY GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	A. C.
ENGINEERS-ARCHITECTS	CHECKED	D. H.
NEW YORK	TRACED	A. C.



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	149R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : H20-S16-44 LIVE LOAD

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.

THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, AND IN SIDEWALK POURS ON ABUTMENT WINGWALLS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS AND WINGWALLS (EXCEPT IN SIDEWALK POURS) SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

CEMENT : CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET 60-41.

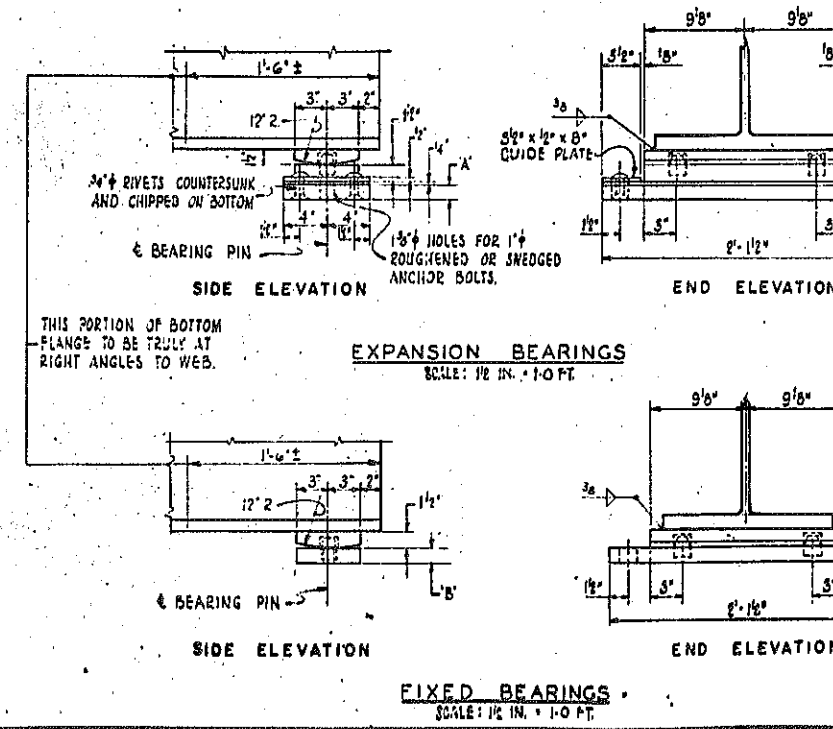
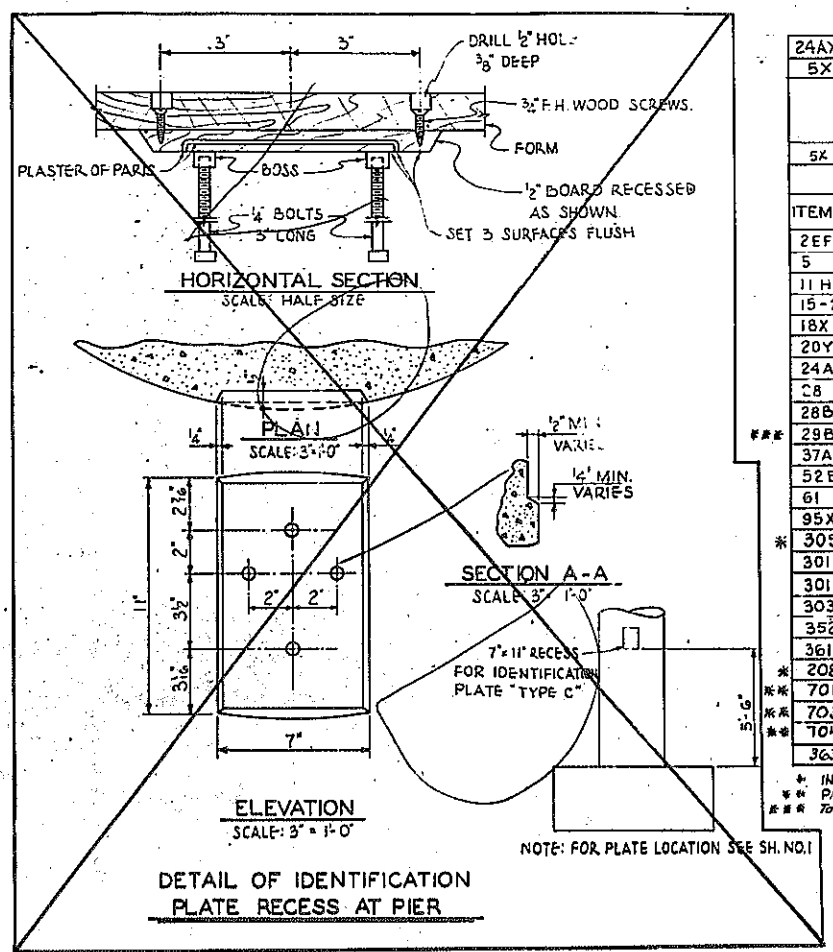
OTHER : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED.

SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.

THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 28 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER, COMPRESSED ASBESTOS SHEET PACKING, PREAM. BIT. JT MATERIAL AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

ABUTMENT FOOTINGS : THE ELEVATION OF SOUND ROCK FOR ABUTMENT FOOTINGS IS TENTATIVE AND ACTUAL FINDINGS IN THE FIELD MAY REQUIRE. IF SO ORDERED BY THE DEPUTY CHIEF ENGINEER, THAT THE ABUTMENT DETAILS BE MODIFIED, OR REVISED. WHERE NECESSARY TO EXCAVATE BELOW THE ASSUMED ELEVATION OF BOTTOM OF FOOTINGS OF ABUTMENTS TO OBTAIN A BASE ON SOUND ROCK, SUCH EXCAVATION SHALL BE MADE AND LOOSE ROCK REMOVED TO THE SATISFACTION OF THE ENGINEER. THE AREA BETWEEN THE TOP OF SOUND ROCK AND THE ASSUMED BOTTOM OF ABUTMENT FOOTINGS SHALL BE FILLED WITH ITEM 20Y, CLASS 1 CONCRETE. ROCK REMOVED BEYOND THE LINES AND LEVELS ORDERED BY THE ENGINEER WILL NOT BE PAID FOR AND THE AREA SHALL BE BACKFILLED WITH CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE. EXCAVATION AS ORDERED OR SHOWN ON THE PLANS SHALL BE ITEMS 5E AND 5R TRENCH, CULVERT AND BRIDGE EXCAVATION.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 81.



ESTIMATE OF QUANTITIES		IROQUOIS GAS		FINN. QUANTITY	
ITEM NO.	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED	TOTAL ROUNDED
24AXX	SCREENED AGGREGATE-STRUCT. BACKFILL	C.Y.			5522
5XX	SPECIAL CURB EXCAVATION				20000
*Note: Item 5 replaces Items 5E and 5R where they appear on these plans					
5Z					0.0
2EF-B	SELECTED FILL	C.Y.	310	340	428.0
5	TRENCH, CULVERT & BRIDGE EXC.	C.Y.	125	140	113.54
11 H	PERF CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	296	305	210
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl	1354	1420	1522.1
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	461	470	453.0
20Y	CLASS 1 CONCRETE	C.Y.	398	410	558.0
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	66	70	80
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	106,700	109,000	104,477
28B	SHEAR CONNECTORS	L.F.	2023	2,100	2,281.0
29B	STRUCTURAL STEEL	Lb.	599,430	617,200	4,400,450.0
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	463	480	472.1
52BX	ASPHALT CONCRETE, TYPE 2B	TON	161	170	124.25
61	BITUMINOUS MATERIAL	GAL.	170	180	88.0
95XC10	10" STONE CURB (BRIDGE)	L.F.	374	380	378.1
305	MISCELLANEOUS METALS	Lb.	380	390	372.0
301B	2" GALVANIZED STEEL CONDUIT	L.F.	415	430	412.7
301C	2 1/2" GALVANIZED STEEL CONDUIT	L.F.	20	20	18.8
303B	FURNISH & INSTALL LIGHT STD.	Ea.	4	4	4.0
352XA	RUBBER JOINT MATERIAL	GAL.	16	18	20.0
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	138	150	165.0
208-16	FURNISH & INSTALL STEEL WATER PIPE (6" DIAM)	L.F.	187	190	174.0
701	INSTALL N.M.P. CORR COND. ON FILLMORE AVE. STRUCT.	L.S.	NEC.	NEC.	100.00
703	INSTALL N.Y. TEL. CO. COND. ON FILLMORE AVE. STRUCT.	L.S.	NEC.	NEC.	100.00
704	INSTALLING GAS MAINS	L.S.	NEC.	NEC.	100.00
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	24	25	28.50

* INCLUDED IN PUBLICLY OWNED WATER WORKS ESTIMATE.
 ** PAID FOR BY UTILITIES INVOLVED.
 *** Total includes portion paid for by Iroquois Gas & Water Utility.

Note: The pieces of structural steel which make up the 4,500 pounds of Structural Steel chargeable to the Iroquois Gas Corp. are as follows:
 8 pieces of 21 WF 96 diaphragms (D1) plus the pipe supports.

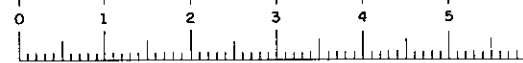
REVISION TO TABLE

FILLMORE AVENUE OVER EXPRESSWAY
 NOTES, BEARINGS AND ESTIMATE

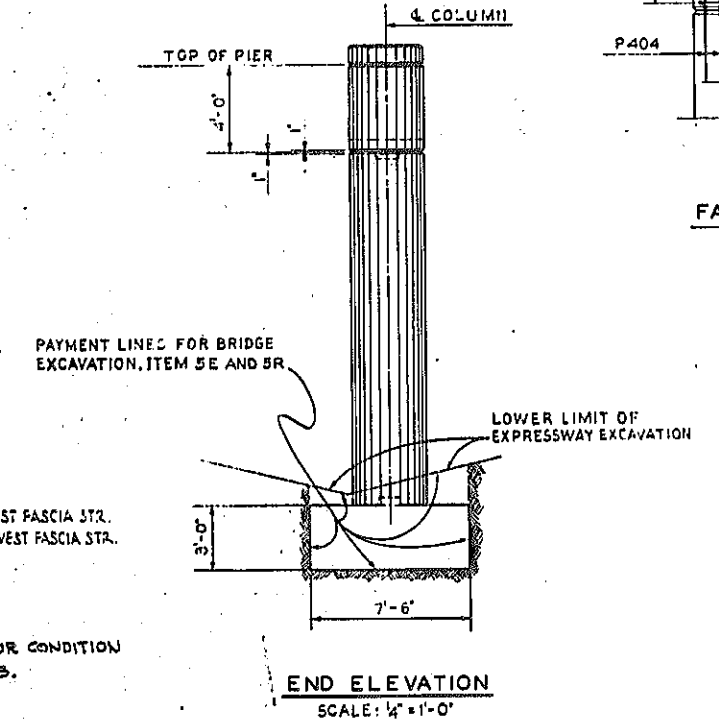
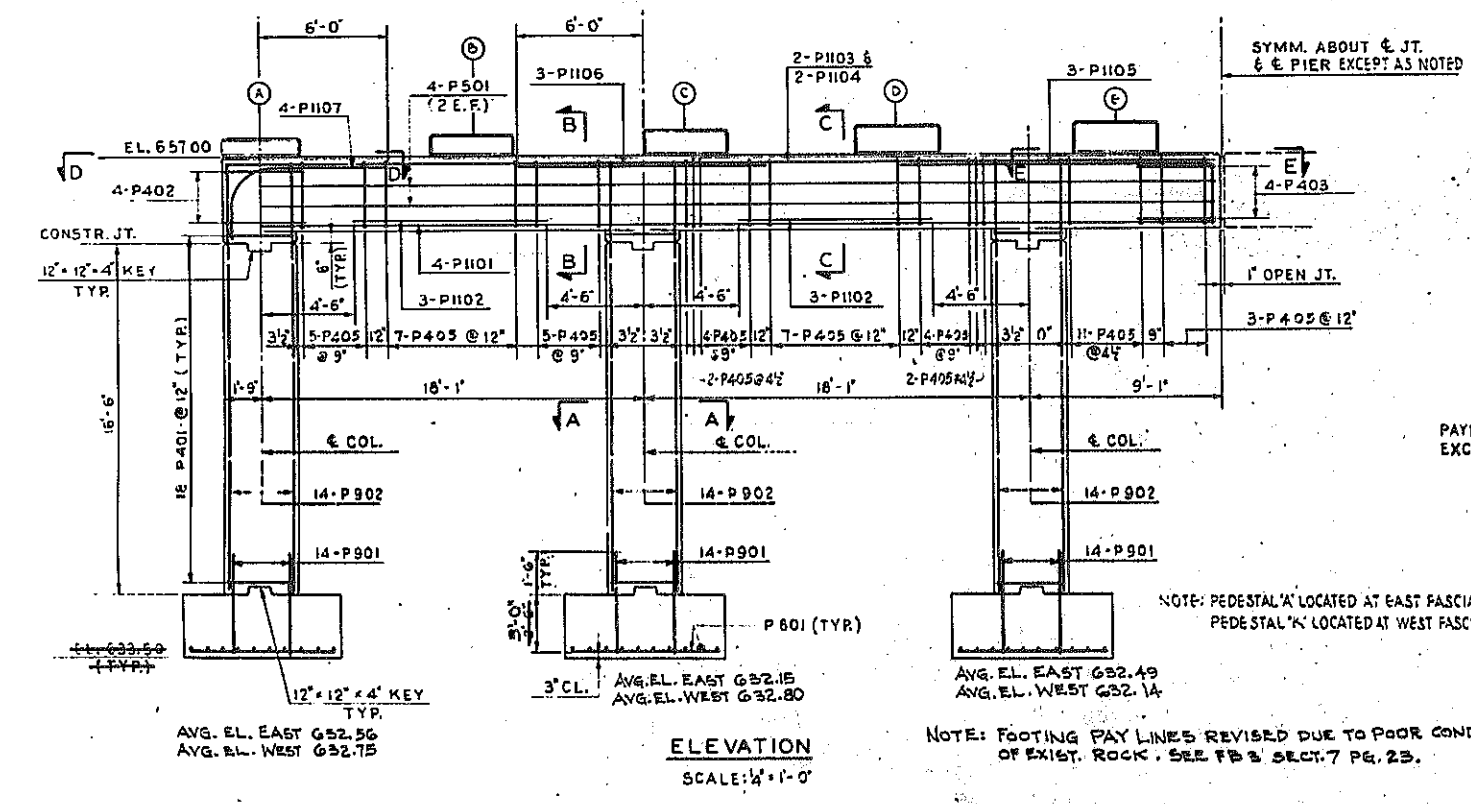
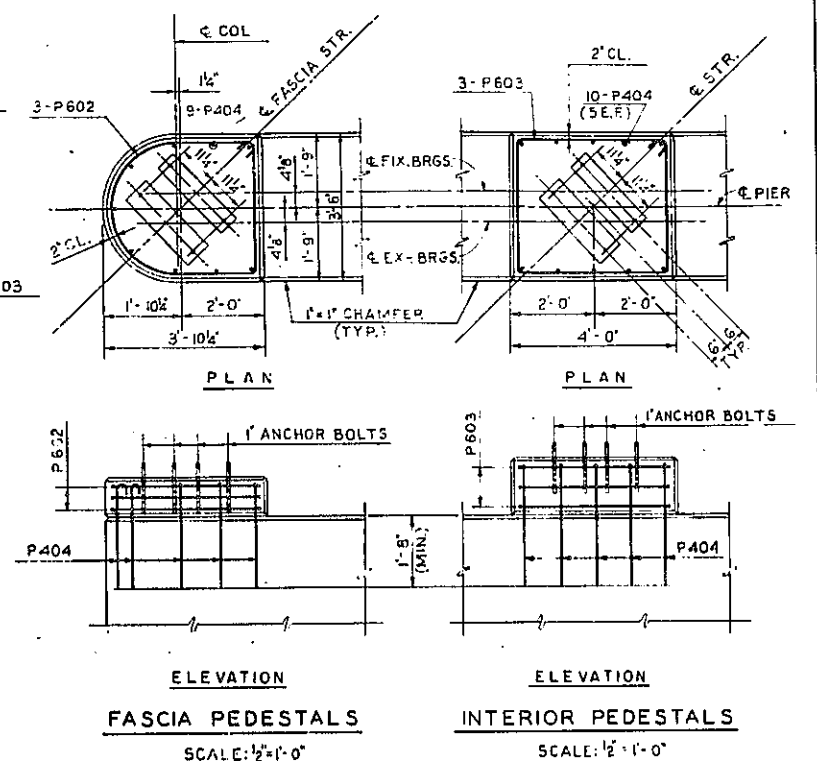
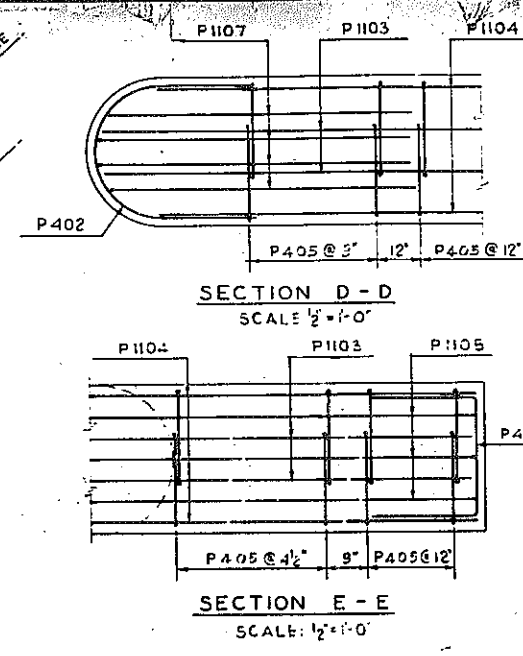
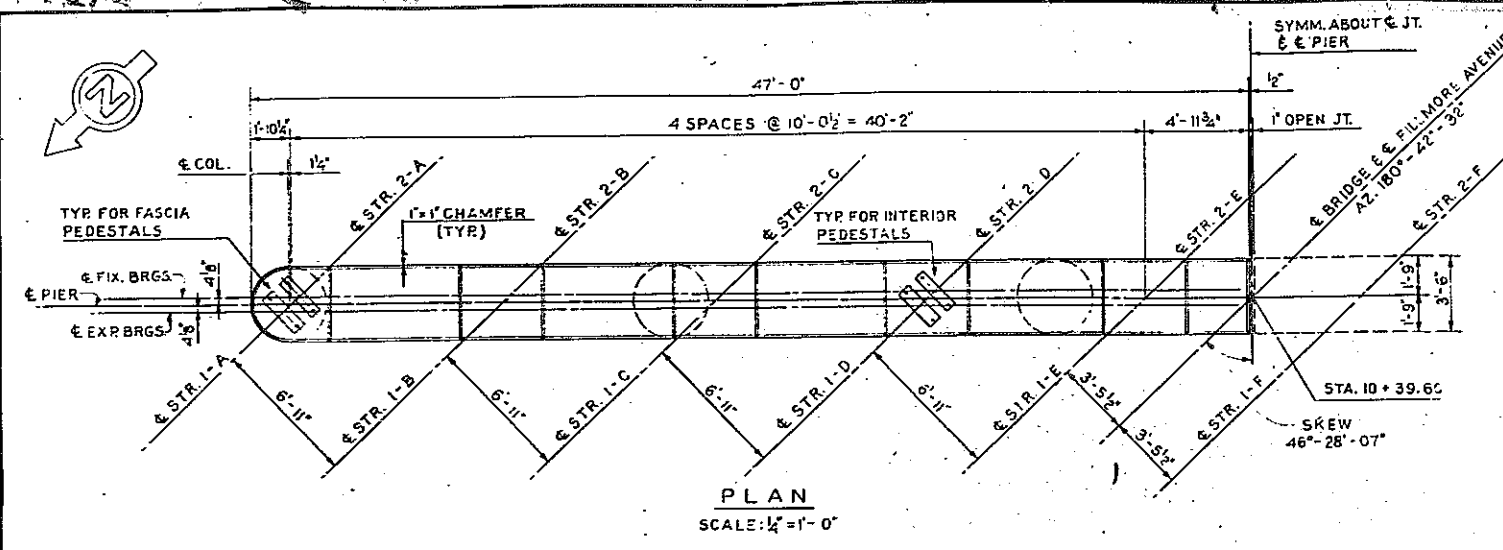
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK

DRAWN: J.S.
 CHECKED: D.H.
 TRACED: J.J.



FED. ROAD REG. NO. 1 STATE N. Y. FEDER. PROJECT NO. UUG-377(25) NO. 150R1 TOTAL SHEETS 173
 KENSINGTON EXPRESSWAY, SEC. NO. III.
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES**
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - CONCRETE IN THE PIER COLUMNS AND CAP SHALL BE ITEM 18A, CLASS 1A CONCRETE FOR STRUCTURES.
 - CONCRETE IN PIER FOOTINGS SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.

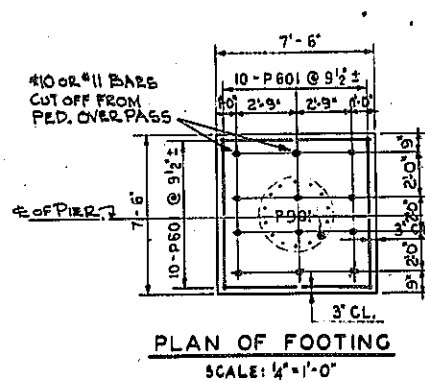
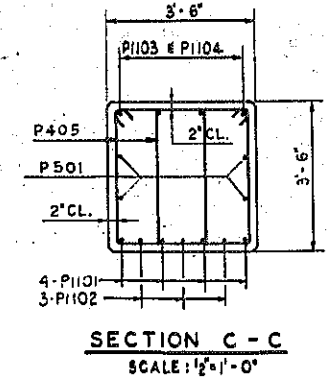
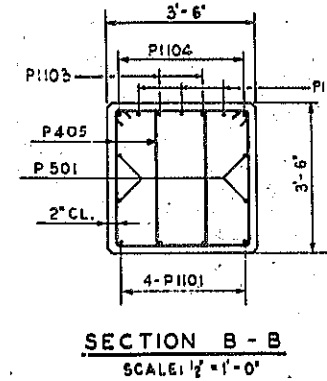
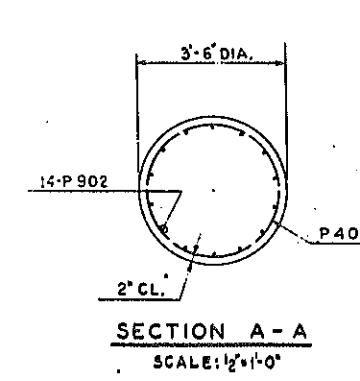


TABLE OF ELEVATIONS

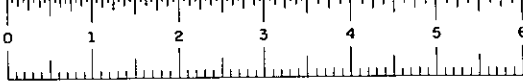
LOCATION	ELEVATION
TOP OF PEDestal A	657.85
" " " B	658.02
" " " C	658.19
" " " D	658.36
" " " E	658.53
" " " F	658.56
" " " G	658.44
" " " H	658.32
" " " J	658.20
" " " K	658.09

BAR REINFORCING LEGEND

NUMBER OF BARS	PIER	BAR SIZE	BAR MARK	REVISION TO FOOTING REINF. PAYLINES
4	P	11	01	

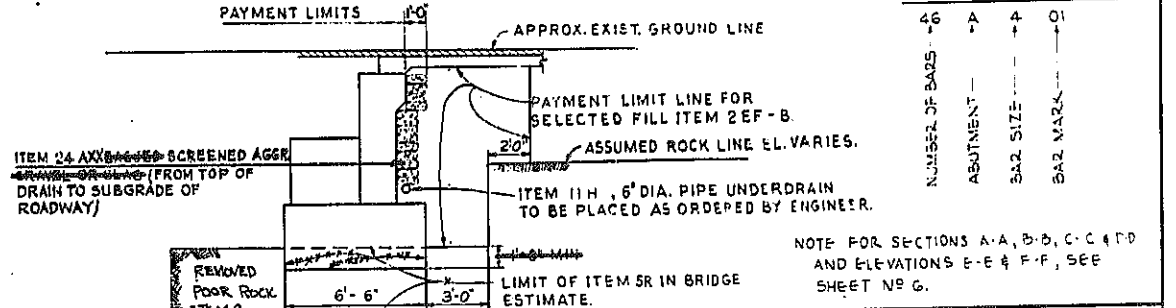
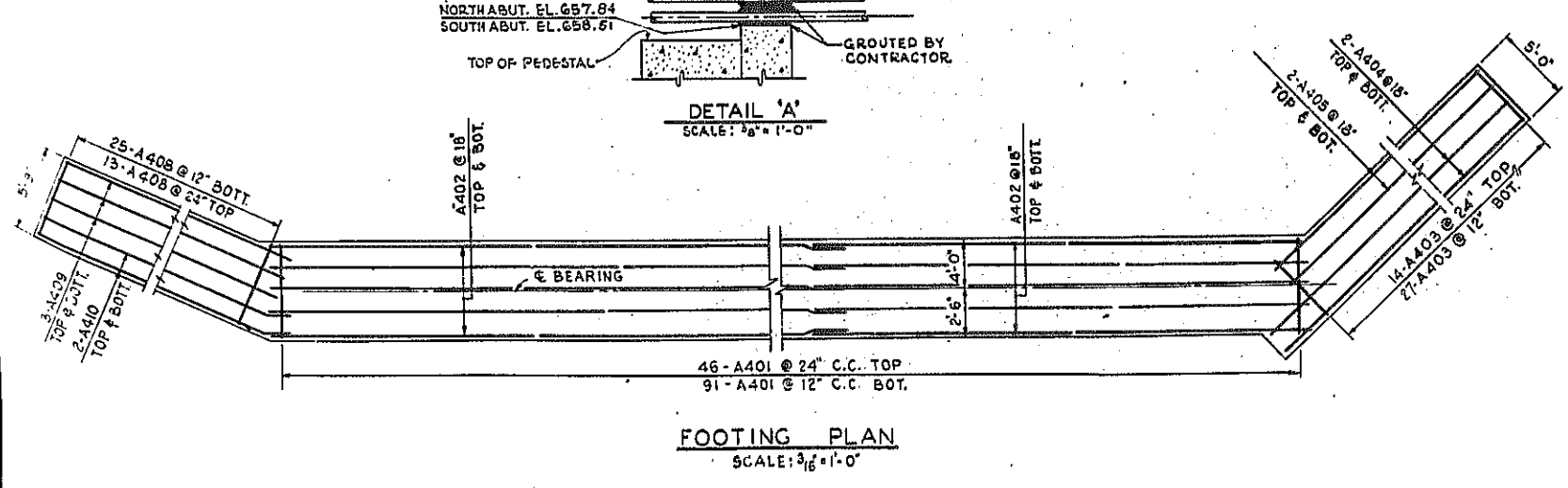
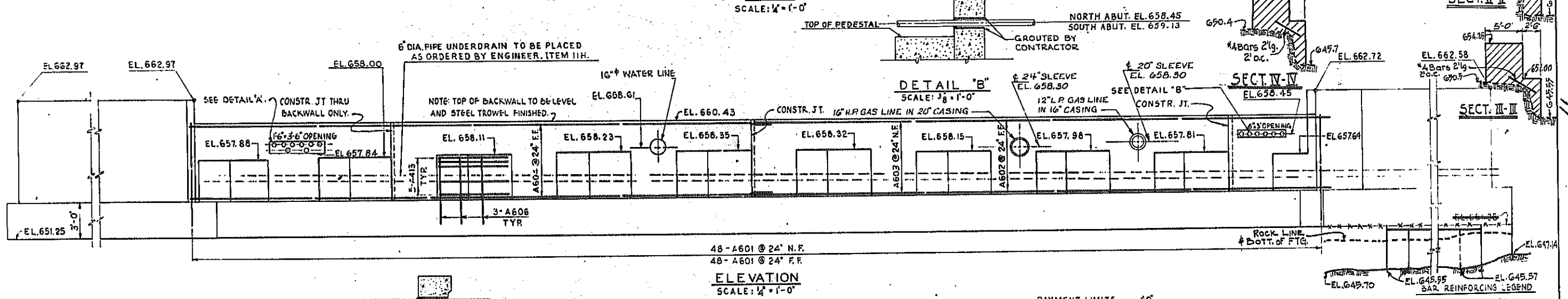
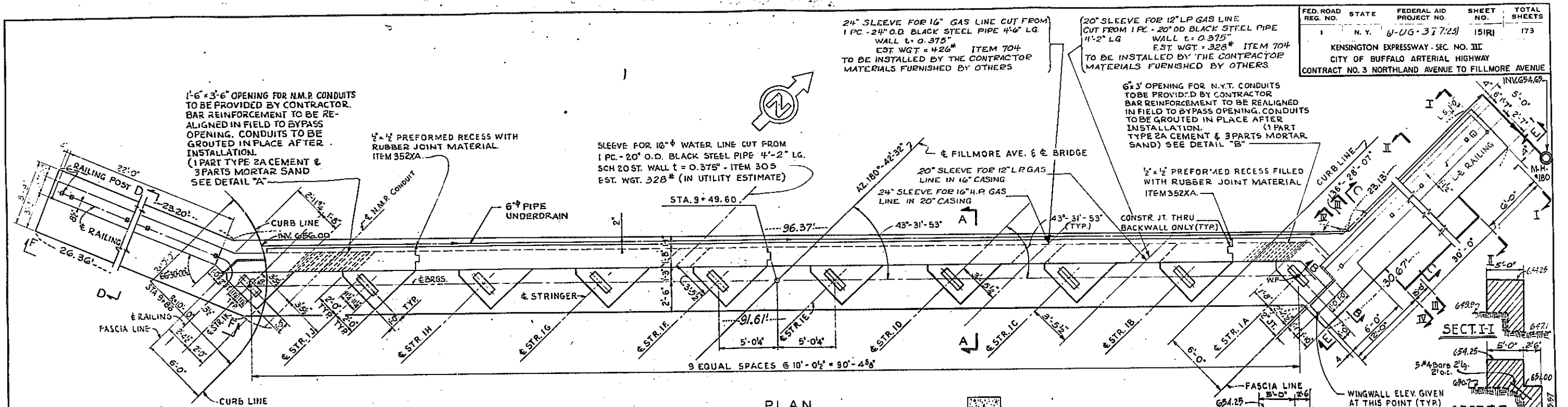
FILLMORE AVENUE OVER EXPRESSWAY PIER
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III.
 DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK
 DRAWN E.B.
 CHECKED D.H.
 TRACED E.B.

BR. SHEET NO. 3



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	6-UG-37728	151R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



FILLMORE AVENUE OVER EXPRESSWAY
NORTH ABUTMENT

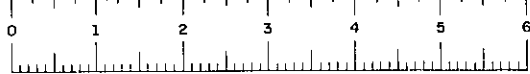
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK

DRAWN N.L.
CHECKED D.H.
TRACED F.B.

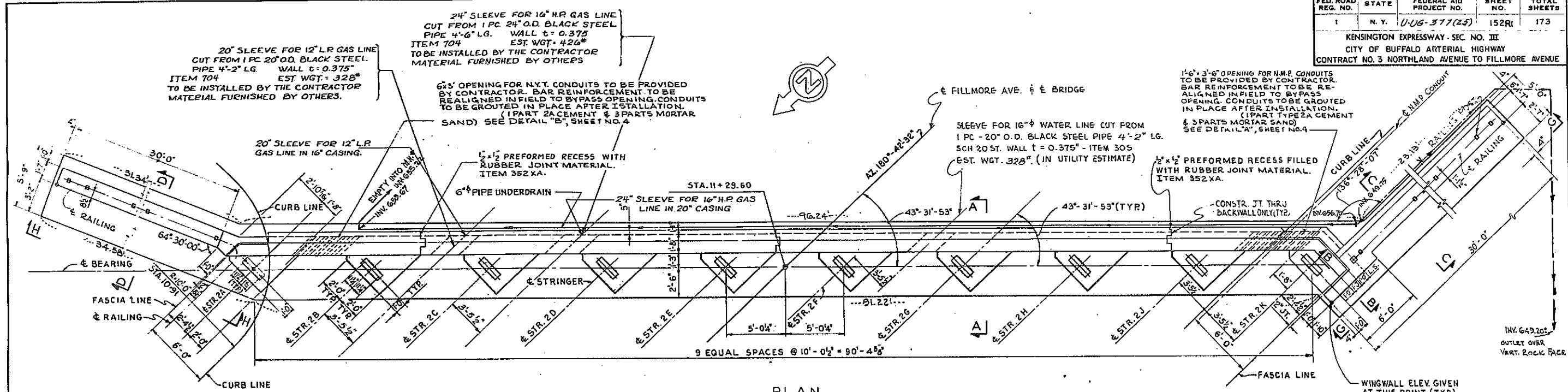
REVISION TO FOOTING DETAILS

B32 SHEET NO. 4

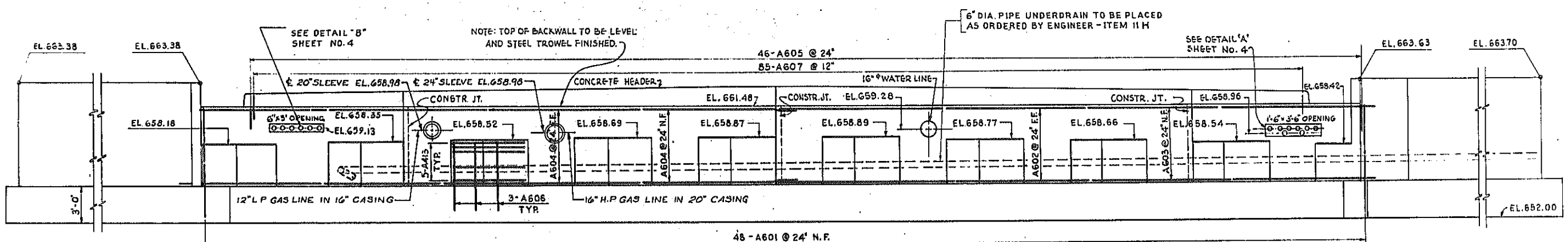


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-05-377(25)	152R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



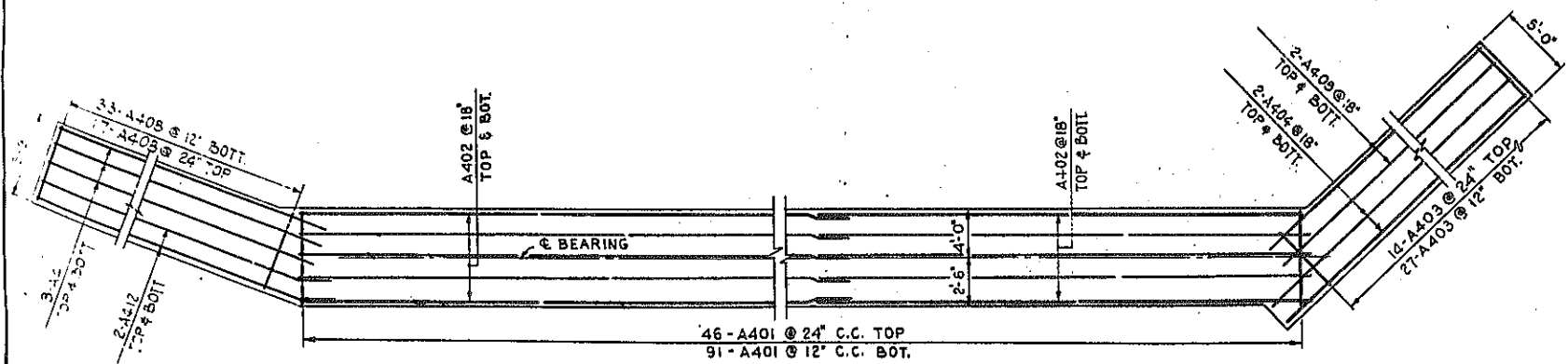
PLAN
SCALE: 1/4" = 1'-0"



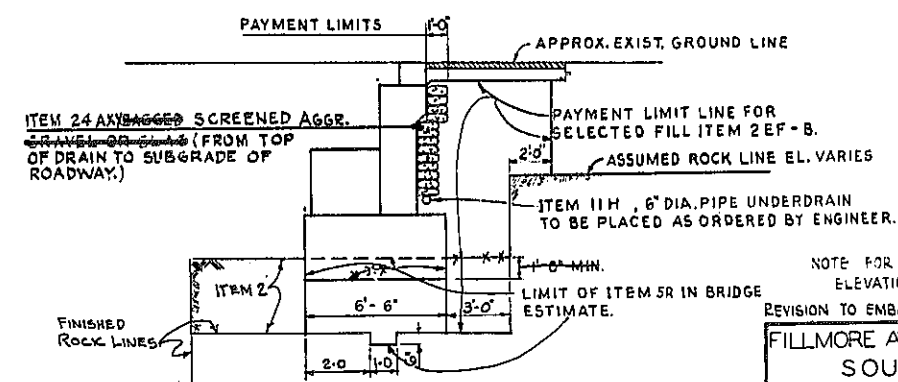
ELEVATION
SCALE: 1/4" = 1'-0"

BAR REINFORCING LEGEND

46	A	6	01
NUMBER OF BARS	ABUTMENT	BAR SIZE	BAR MARK



FOOTING PLAN
SCALE: 3/8" = 1'-0"



EMBANKMENT DETAILS AT ABUTMENTS
SCALE: 1/4" = 1'-0"

NOTE FOR SECTIONS A-A, B-B, C-C & D-D AND ELEVATIONS F-F & G-G SEE SHEET NO. 6.

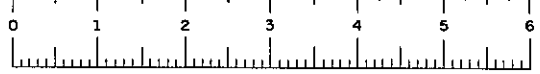
REVISION TO EMBANKMENT DETAILS; UNDERDRAIN

FILLMORE AVENUE OVER EXPRESSWAY SOUTH ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK

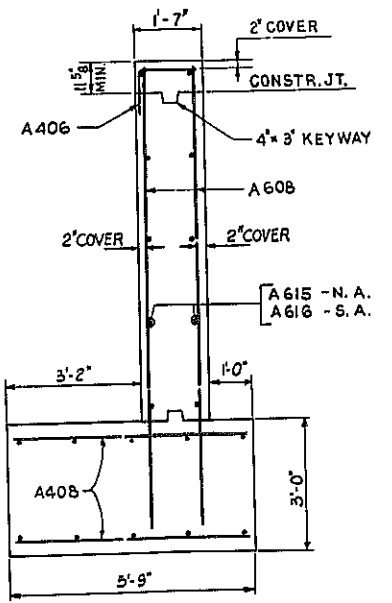
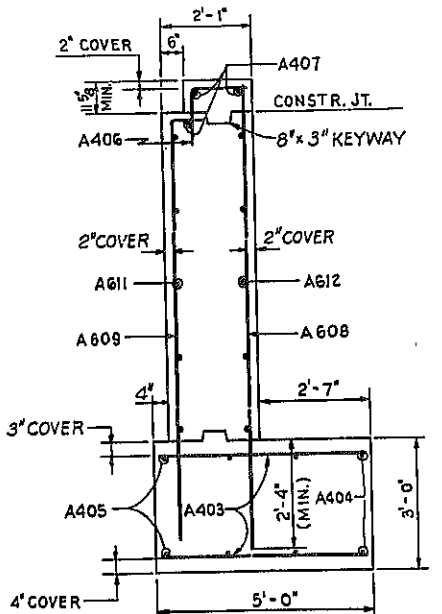
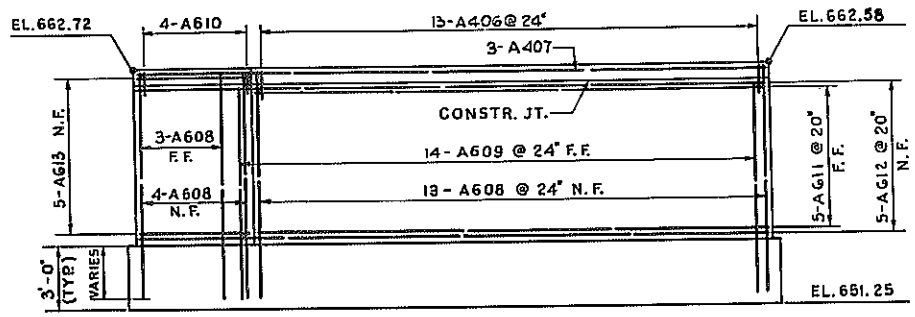
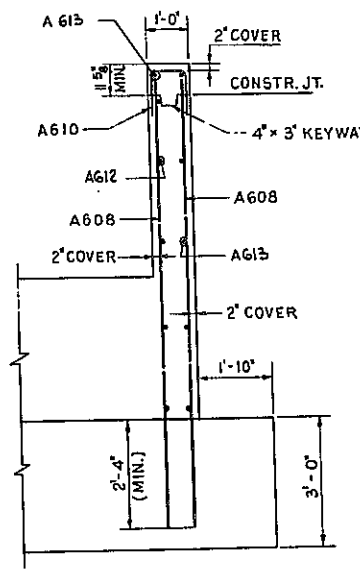
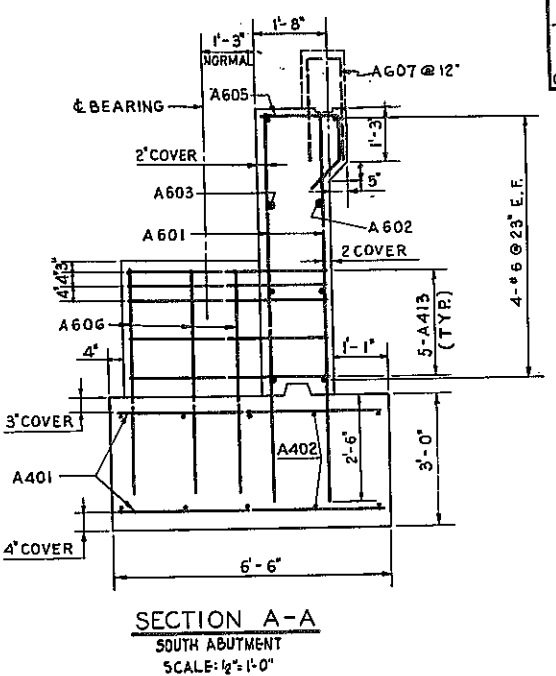
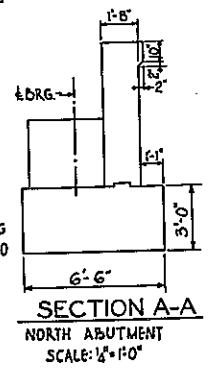
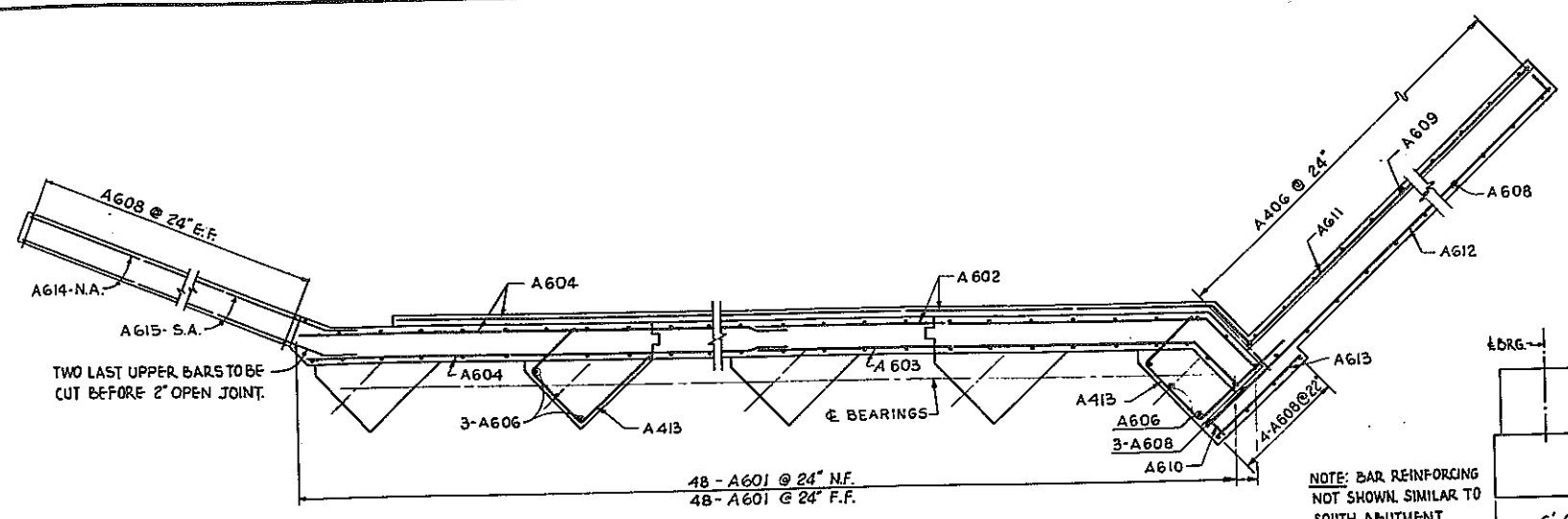
DRAWN BY M.L. CHECKED BY D.H. TRACED BY M.L./E.B.



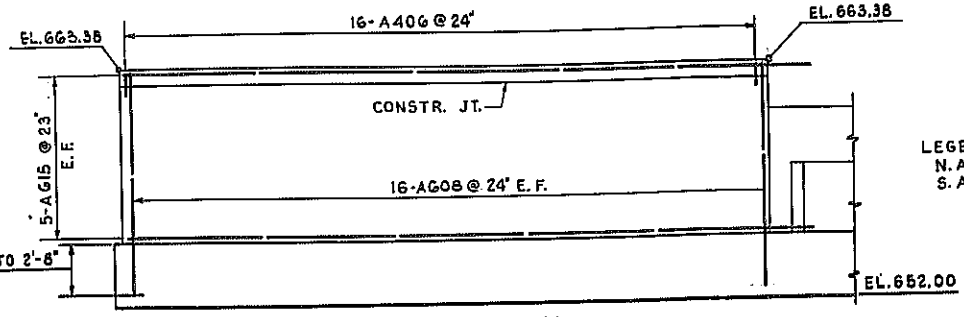
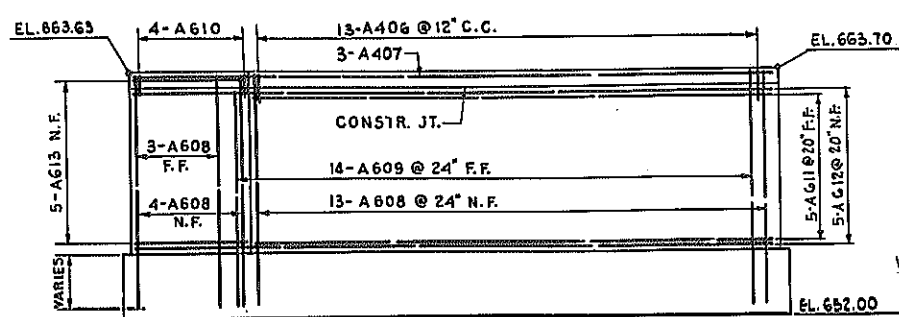
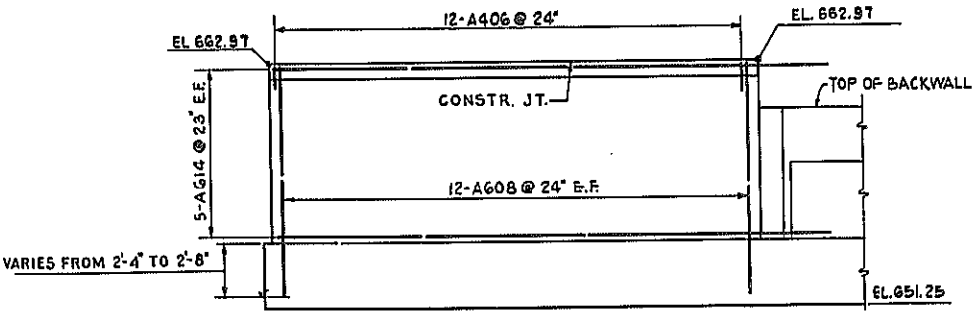
FAC 63-2

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	153	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
- ALL CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - CONCRETE IN SIDEWALKS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 - FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - BITUMINOUS MATERIAL ITEM 61 SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.
 - FOR RAILING DETAILS SEE SHEET NO. 10.
 - FOR LOCATION OF SECTIONS A-A, B-B, C-C & D-D AND ELEVATIONS G-G & H-H SEE SHEETS NO. 4 & 5.



BAR REINFORCING LEGEND

NUMBER OF BARS	ABUTMENT	BAR SIZE	BAR MARK
12	A	6	OB

LEGEND:
N.A.: NORTH ABUTMENT.
S.A.: SOUTH ABUTMENT.

BL SHEET NO. 6

FILLMORE AVENUE OVER EXPRESSWAY
ABUTMENT DETAILS

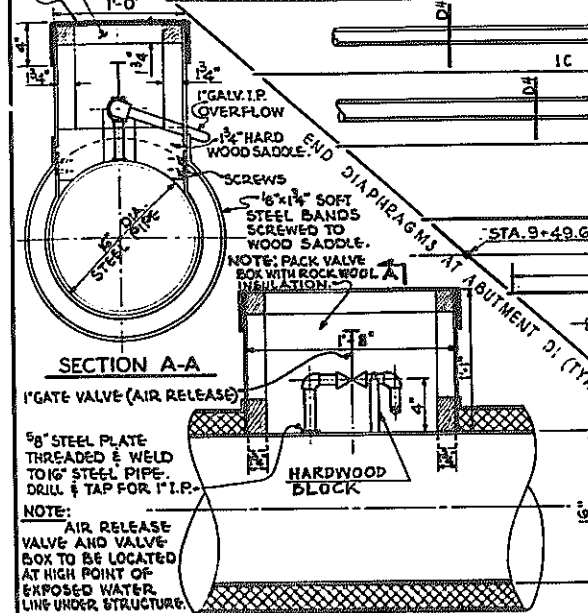
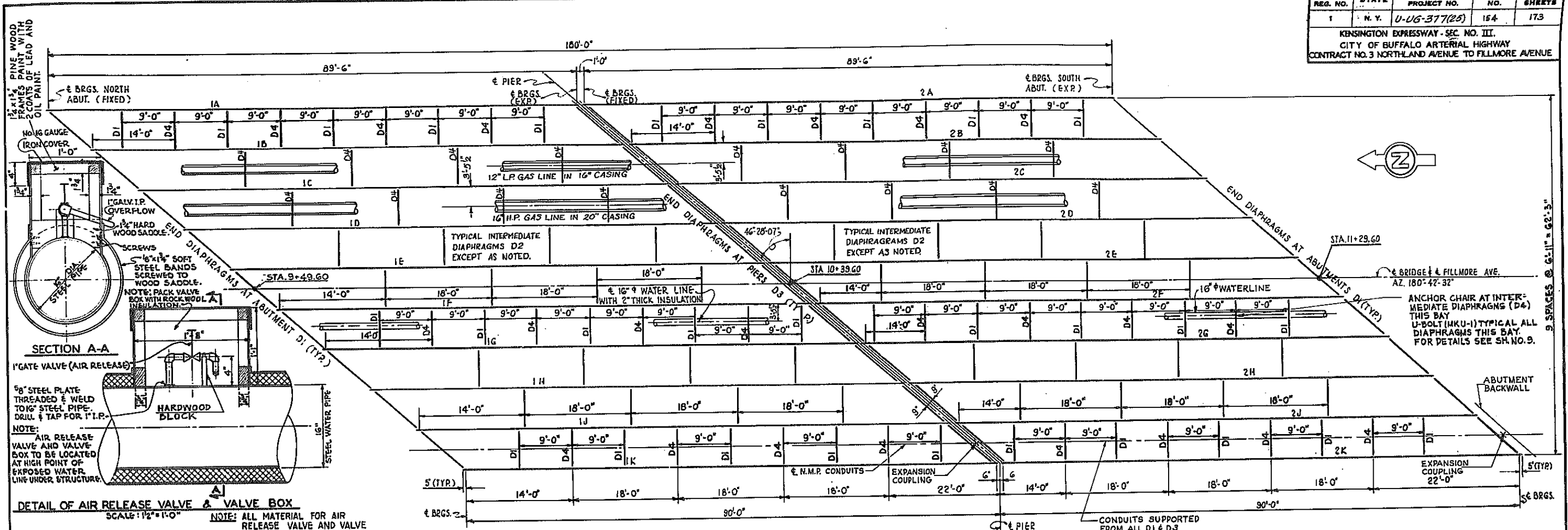
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.L./D.H. D.H. M.L./E.B.
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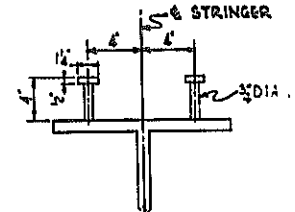
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	154	173

KENSINGTON EXPRESSWAY - SEC. NO. III.
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

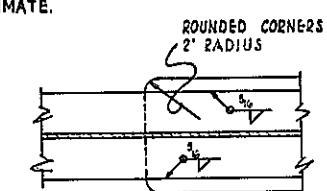


DETAIL OF AIR RELEASE VALVE & VALVE BOX
SCALE: 1/2" = 1'-0"

NOTE: ALL MATERIAL FOR AIR RELEASE VALVE AND VALVE BOX TO BE INCLUDED IN UTILITY ESTIMATE.



DETAIL OF STUD SHEAR CONNECTOR



COVER PLATE DETAIL
SCALE: 1" = 1'-0"

NOTE: SPECIAL PRECAUTIONS MUST EXERCISED WHERE WELDING CROSSES EDGE OF FLANGE TO AVOID ANY POSSIBILITY OF "UNDERCUT" OR "NICKS" IN EDGE OF FLANGE.

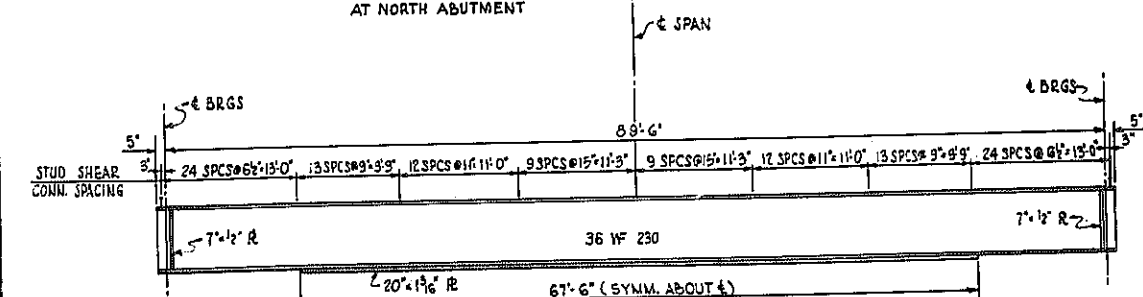
NOTE: FOR DETAILS OF INTERMEDIATE AND END DIAPHRAGMS D1, D2, D3 AND D4, SEE SHEET NO. 9.

DIAPHRAGM SCHEDULE

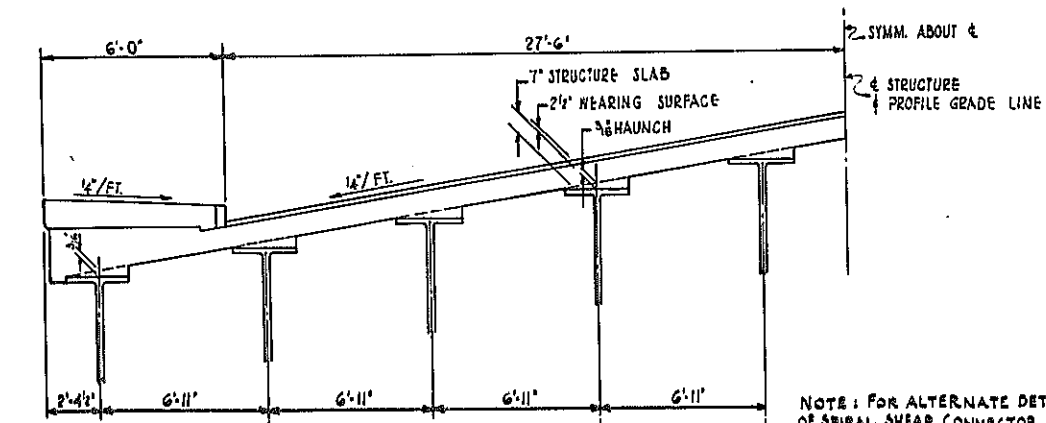
D1	15C 33.9
D2	BUILT UP SECTION (WEB 3/8" TOP & BOTTOM FLANGE 8x8")
D3	16 WF 36
D4	21 WF 96

FRAMING PLAN
SCALE: 1/8" = 1'-0"

BEARINGS				
FIXED BEARINGS				
LOCATION	STRINGER	N.O.	"B"	TOTAL HGT.
N. ABUT.	ALL	10	12"	3'
PIER	ALL	10	3 1/2"	4 3/8"
EXPANSION BEARINGS				
LOCATION	STRINGER	N.O.	"A"	TOTAL HGT.
PIER	ALL	10	14"	4 1/2"
S. ABUT.	ALL	10	14"	4 1/2"

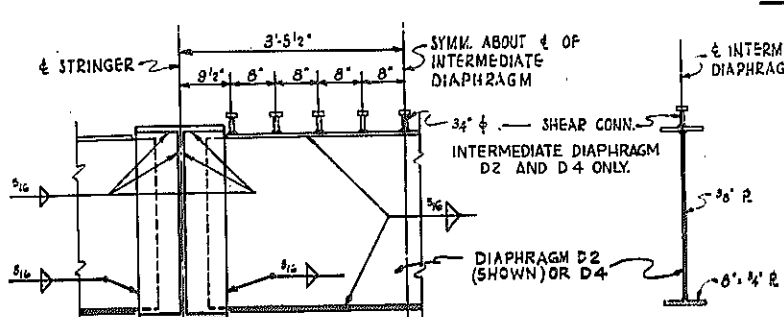


TYPICAL ROLLED BEAM STRINGER WITH COVER PLATE
NOT TO SCALE

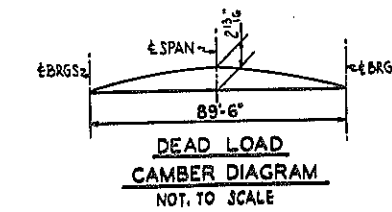


DIAGRAMMATIC SECTION
NOT TO SCALE

NOTE: FOR ALTERNATE DETAIL OF SPIRAL SHEAR CONNECTOR SEE SHEET NO. 105.



DETAIL OF STUD SHEAR CONNECTOR ALONG INTERMEDIATE DIAPHRAGM
SCALE: 3/4" = 1'-0"



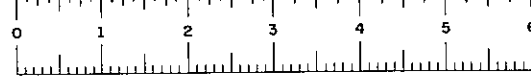
DEAD LOAD CAMBER DIAGRAM
NOT TO SCALE

NOTE: 1- FOR BEARING DETAILS SEE SH. NO. 2.

FILLMORE AVENUE OVER EXPRESSWAY
FRAMING PLAN

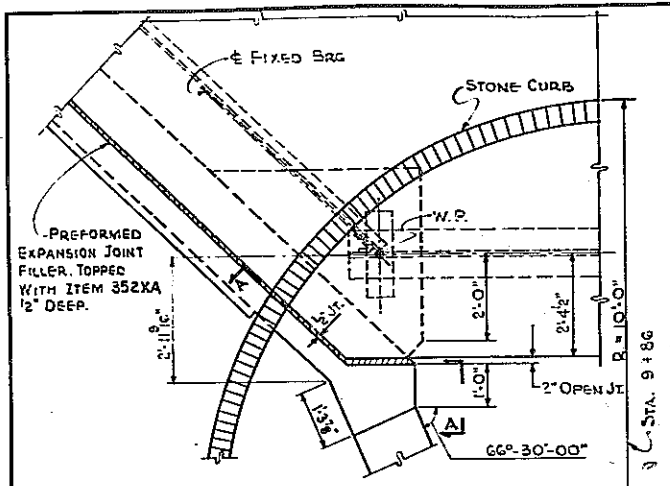
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III.

DE LEUW, CATHAR & BRILL	DRAWN	S.J.
ENGINEERS-ARCHITECTS	CHECKED	F.H.
NEW YORK	TRACKS	S.S.

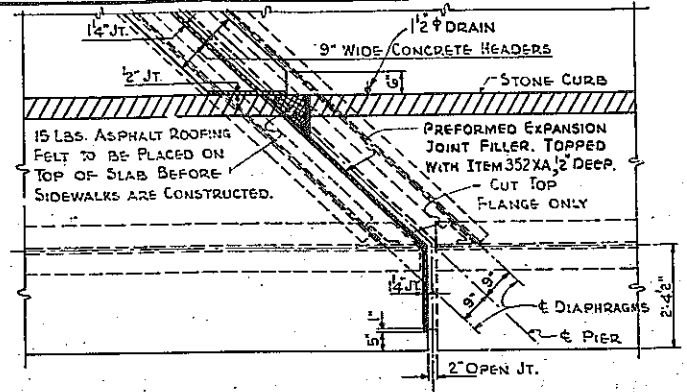


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	155R1	173

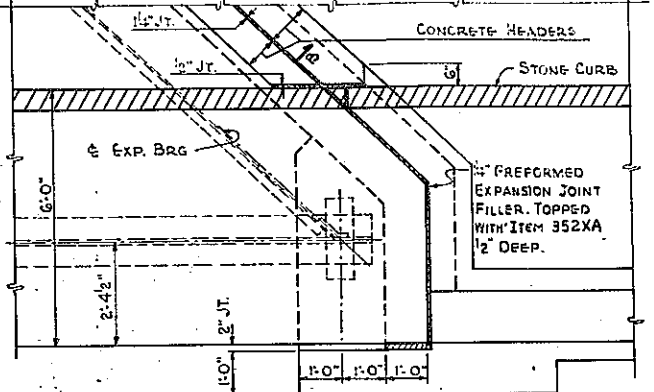
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



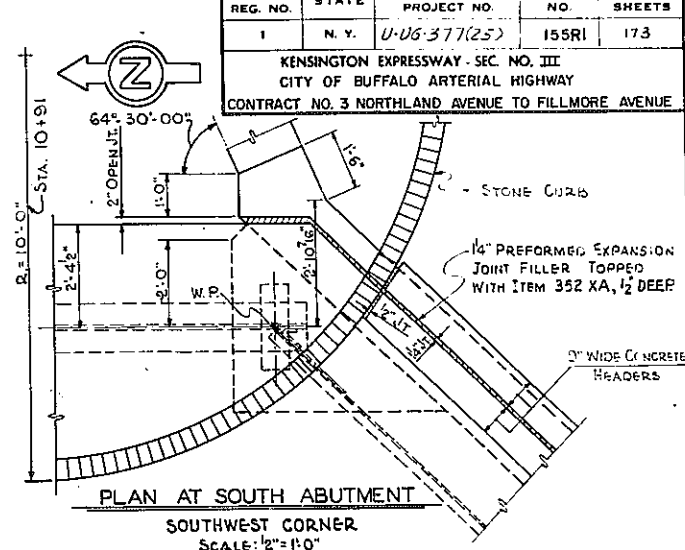
PLAN AT NORTH ABUTMENT
NORTHWEST CORNER
SCALE: 2"=1'-0"



PLAN AT PIER
SCALE: 2"=1'-0"

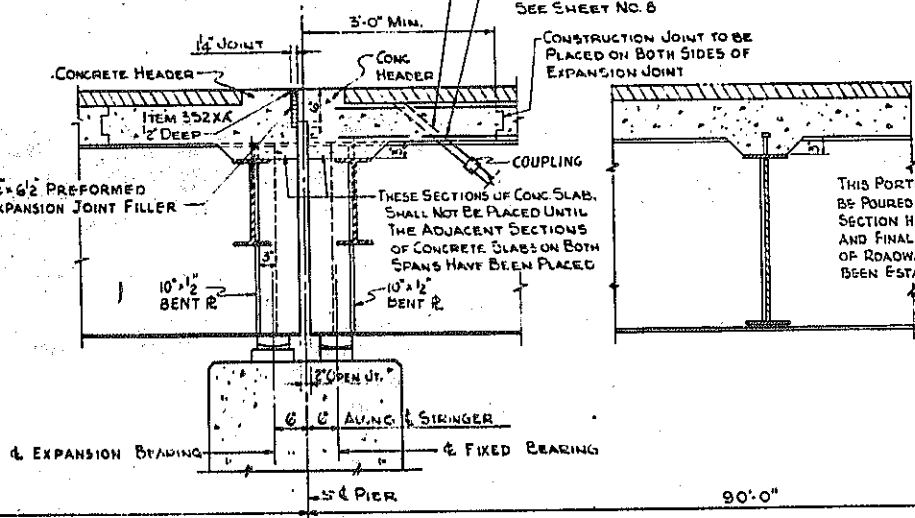
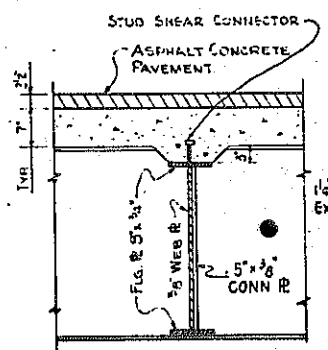
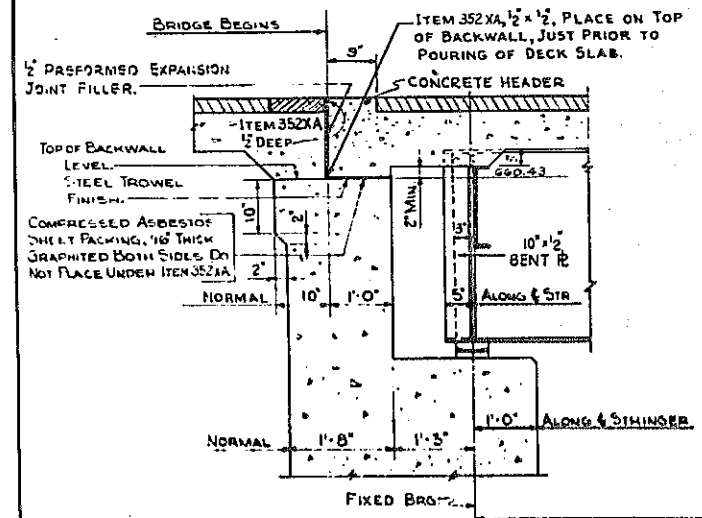


PLAN AT SOUTH ABUTMENT
SOUTHWEST CORNER SHOWN
NORTHEAST CORNER SIMILAR
SCALE: 2"=1'-0"

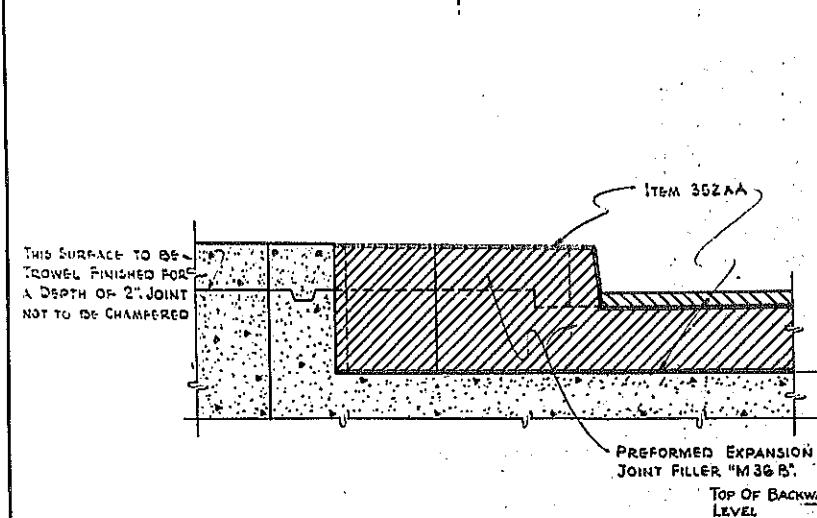
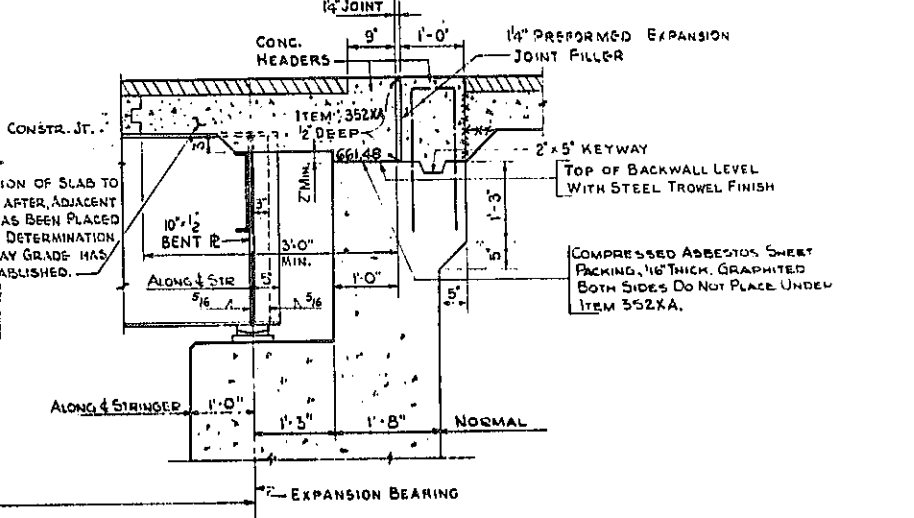


PLAN AT SOUTH ABUTMENT
SOUTHWEST CORNER
SCALE: 2"=1'-0"

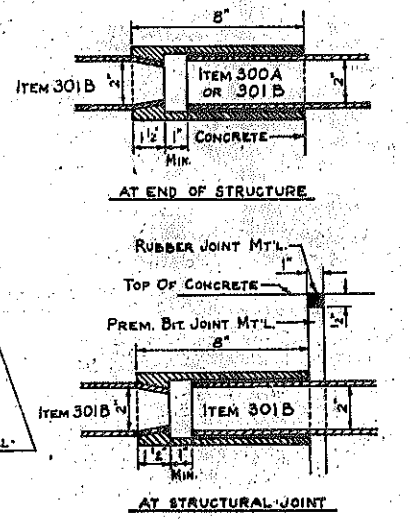
1/2" GALVANIZED PIPE DRAINS SHALL BE PLACED THRU SLAB ON BOTH SIDES OF STRUCTURE AT REAR OF HEADER ON LOW END OF SPAN. DRAINS SHALL BE EXTENDED BY THE USE OF BENT OR ANGLED FITTINGS SO THAT THE DRAINAGE WILL NOT FALL ON STEEL OR CONCRETE SURFACES. PAYMENT FOR FURNISHING AND INSTALLING DRAINS SHALL BE MADE UNDER ITEM 18 X.



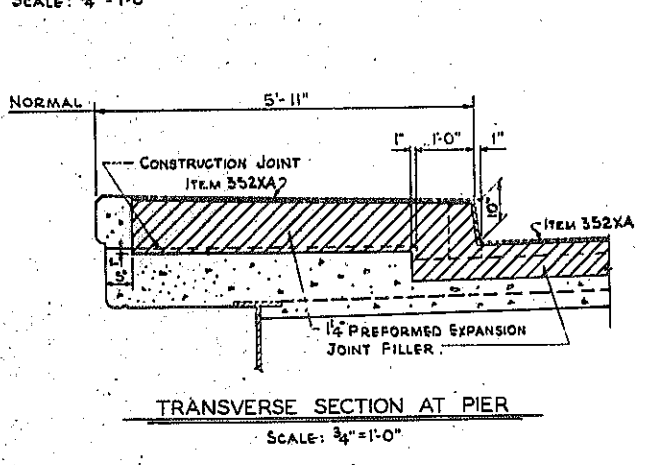
LONGITUDINAL SECTION ALONG PROFILE GRADE LINE
SCALE: 3/4"=1'-0"



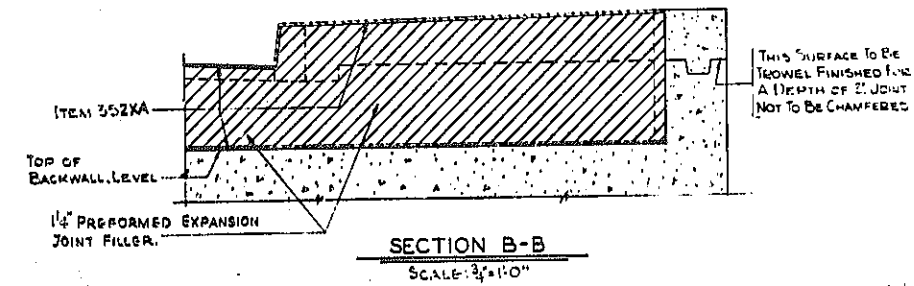
SECTION A-A
SCALE: 3/4"=1'-0"



CONDUIT EXPANSION COUPLING
NOT TO SCALE



TRANSVERSE SECTION AT PIER
SCALE: 3/4"=1'-0"

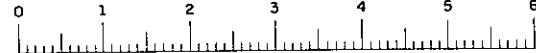


SECTION B-B
SCALE: 3/4"=1'-0"

REVISION TO LONGITUDINAL SECTION

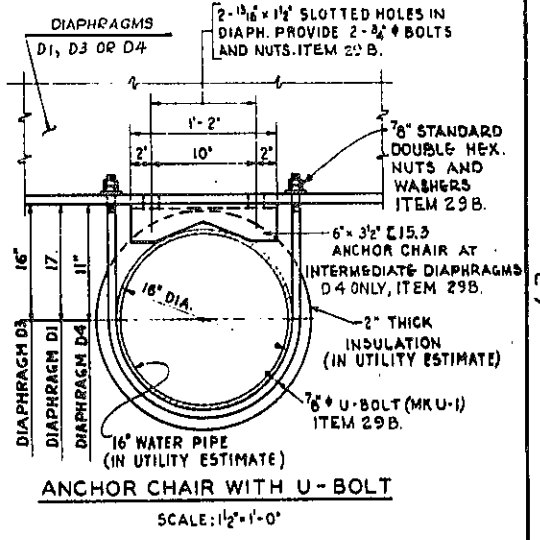
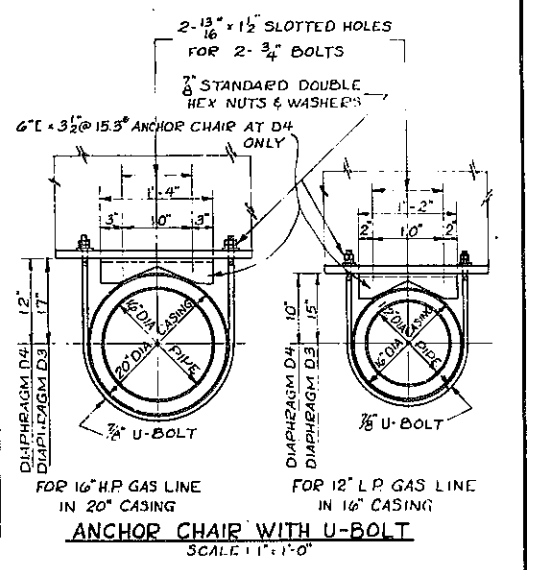
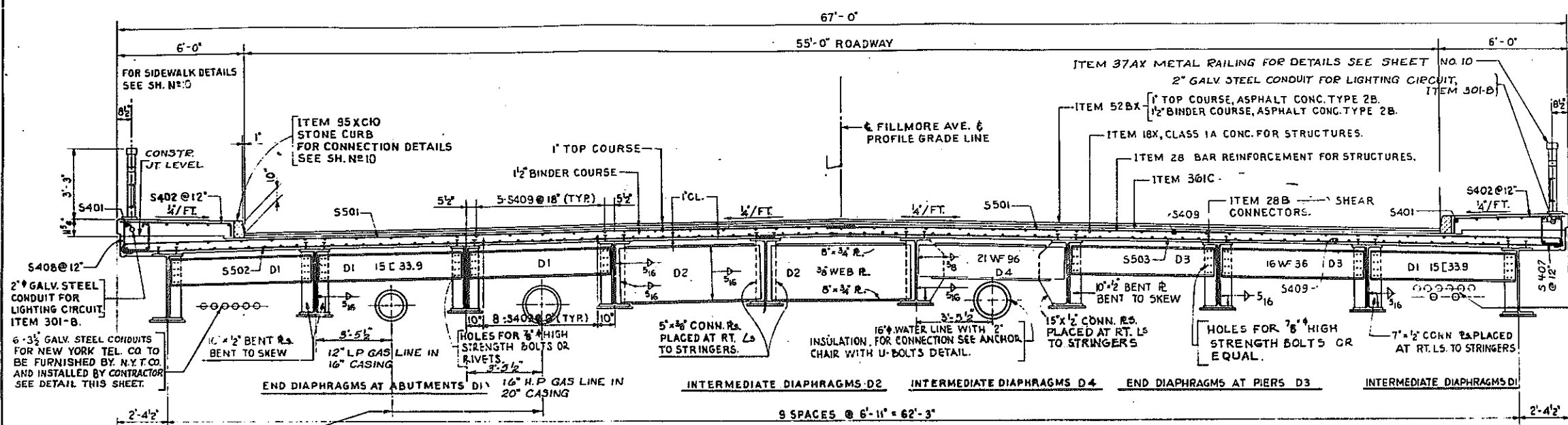
FILLMORE AVENUE OVER EXPRESSWAY LONGITUDINAL SECTION & DETAILS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN A. C. CHECKED D. H. TRACED A. C.

Be. SHEET NO. 8

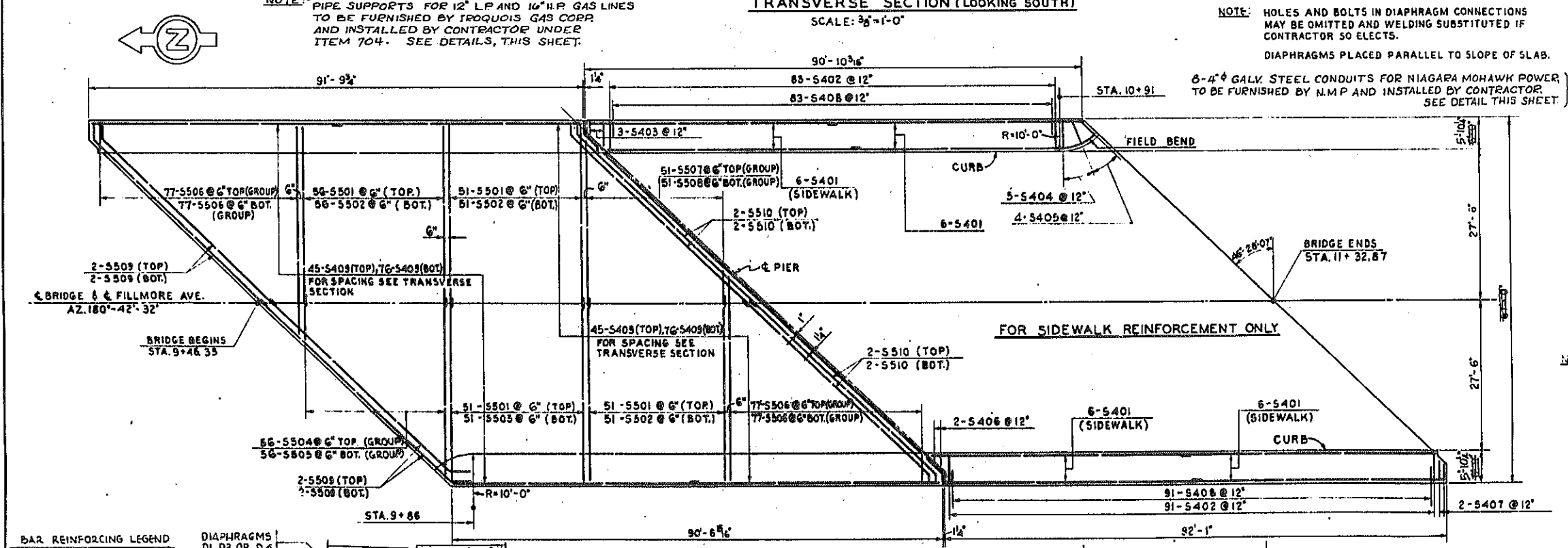


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	156 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
- PIPE SUPPORTS FOR WATER LINE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 29 B.
 - HOLES IN DIAPHRAGMS TO BE PROVIDED BY CONTRACTOR. COST TO BE INCLUDED IN PRICE BID FOR ITEM 29 B.
 - FOR FRAMING PLAN SEE SHEET NO. 7.
 - FOR LONGITUDINAL SECTIONS SEE SHEET NO. 8.
 - INSULATION FOR 16" WATER PIPE SHALL BE GLASS FIBER PIPE INSULATION IN ONE PIECE MOLDED SECTIONS 2" THICK, AS MFGD. BY GUSTIN-BACON MFG. CO., OR EQUAL. PIPE INSULATION TO BE FURNISHED WITH VAPOR BARRIER JACKET OF TOUGH KRAFT FOIL LAMINATE. JACKETED PIPE INSULATION SHALL BE COVERED WITH ALUMINUM WEATHER-PROOF JACKETING AS MFGD. BY CHILDERS MFG. CO., OR EQUAL. TO BE INCLUDED IN UTILITY ESTIMATE.



FILLMORE AVENUE OVER EXPRESSWAY
DECK PLAN AND SECTION

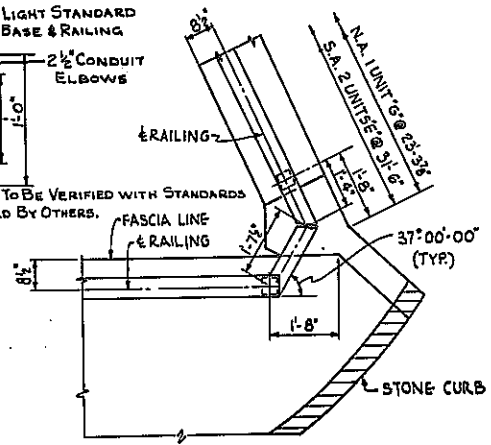
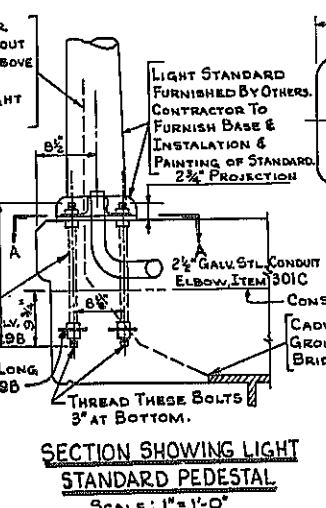
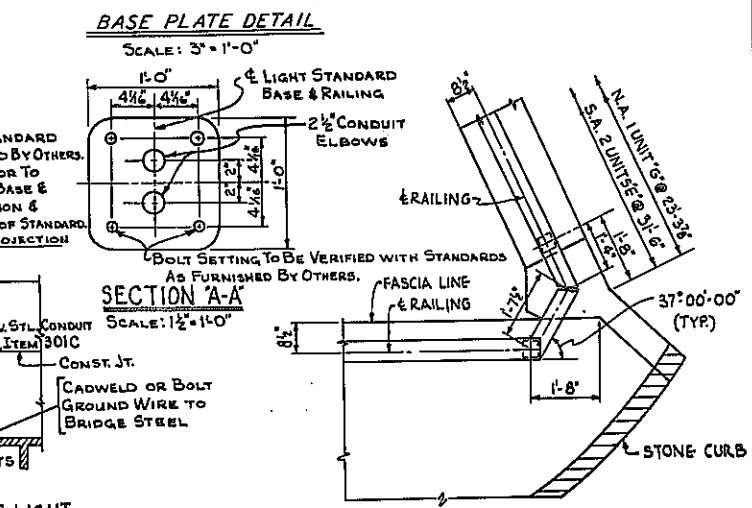
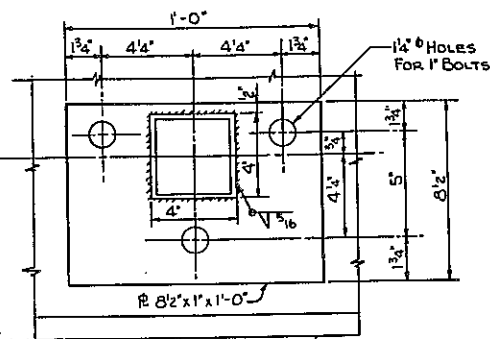
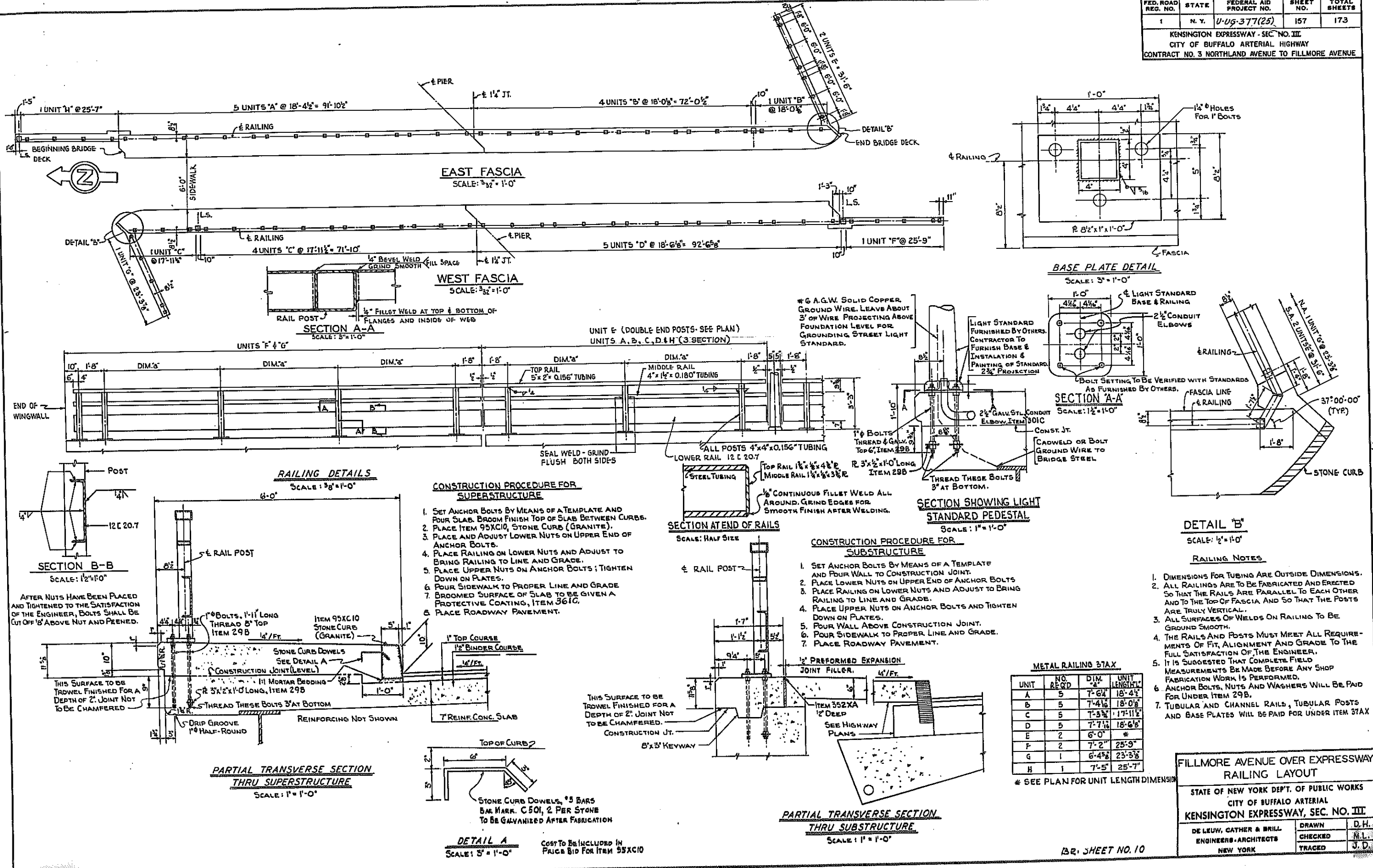
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN E. B.
CHECKED D. H.
TRACED E. B.

B2 SHEET NO. 9

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	157	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



- CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND FOUR SLAB. BROOM FINISH TOP OF SLAB BETWEEN CURBS.
 2. PLACE ITEM 95XC10, STONE CURB (GRANITE).
 3. PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 4. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 5. PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON PLATES.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. BROOMED SURFACE OF SLAB TO BE GIVEN A PROTECTIVE COATING, ITEM 361C.
 8. PLACE ROADWAY PAVEMENT.

- CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND FOUR WALL TO CONSTRUCTION JOINT.
 2. PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 3. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 4. PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON PLATES.
 5. POUR WALL ABOVE CONSTRUCTION JOINT.
 6. POUR SIDEWALK TO PROPER LINE AND GRADE.
 7. PLACE ROADWAY PAVEMENT.

METAL RAILING 3TAX

UNIT	NO. REQ'D	DIM. - ft.	UNIT LENGTH - ft.
A	5	7'-6 1/2"	18'-4 1/2"
B	5	7'-4 1/2"	18'-0 1/2"
C	5	7'-3 3/4"	17'-11 1/2"
D	5	7'-7 1/4"	18'-6 1/8"
E	2	6'-0"	#
F	2	7'-2"	25'-9"
G	1	6'-4 5/8"	23'-3 3/8"
H	1	7'-5"	25'-7"

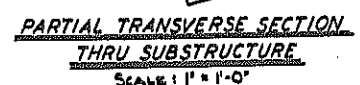
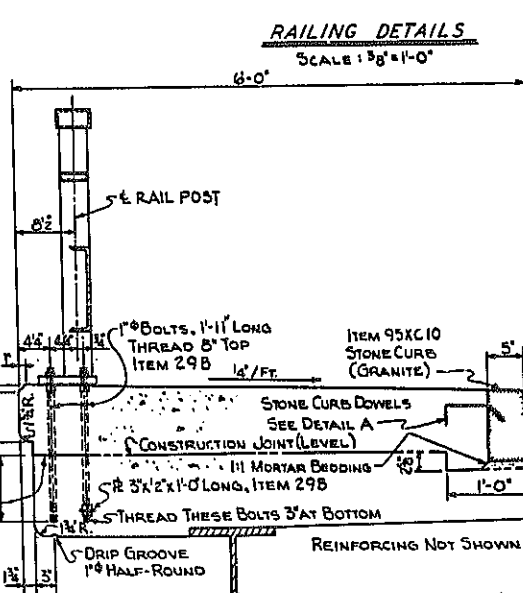
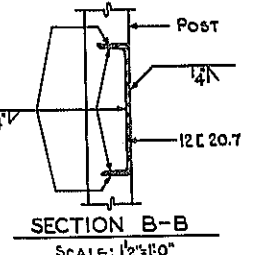
* SEE PLAN FOR UNIT LENGTH DIMENSIONS

- RAILING NOTES**
1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
 2. ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF FASCIA AND SO THAT THE POSTS ARE TRULY VERTICAL.
 3. ALL SURFACES OF WELDS ON RAILING TO BE GROUND SMOOTH.
 4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
 5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
 6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
 7. TUBULAR AND CHANNEL RAILS, TUBULAR POSTS AND BASE PLATES WILL BE PAID FOR UNDER ITEM 37AX.

FILLMORE AVENUE OVER EXPRESSWAY RAILING LAYOUT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	D.H.
ENGINEERS-ARCHITECTS	CHECKED	M.L.
NEW YORK	TRACED	J.D.



DETAIL A
SCALE: 3" = 1'-0"

COST TO BE INCLUDED IN PAICB BID FOR ITEM 95XC10

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (22)	158	173

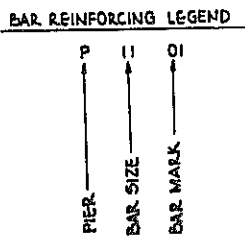
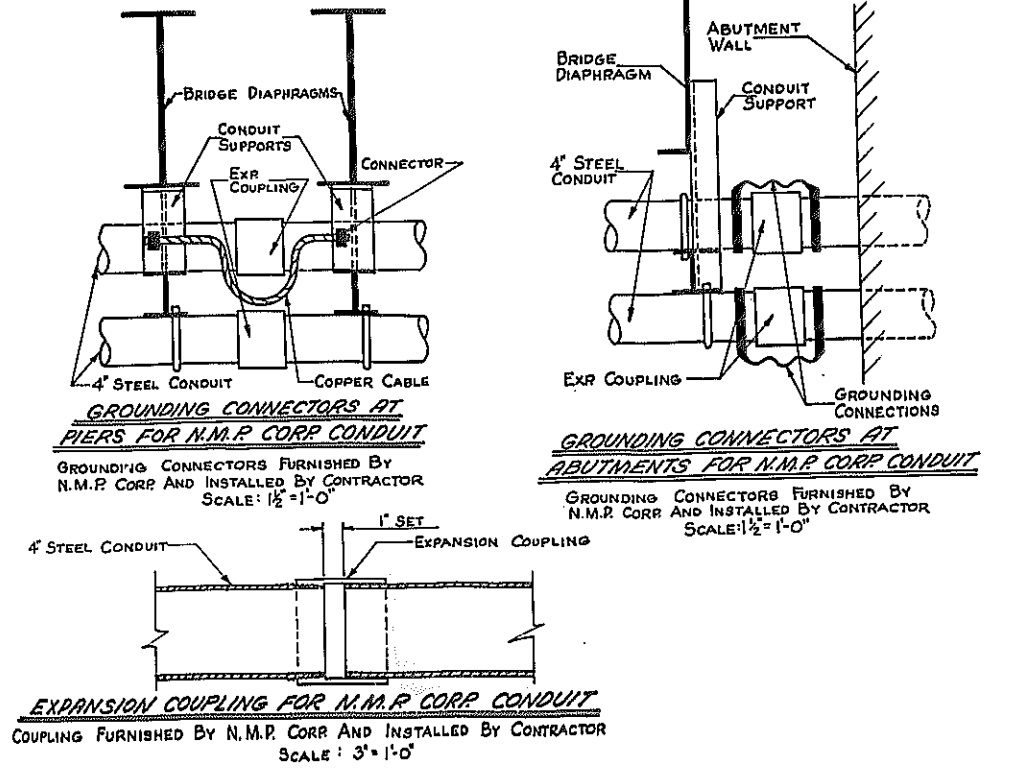
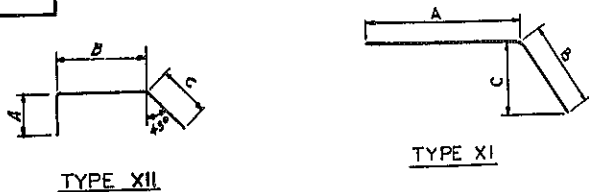
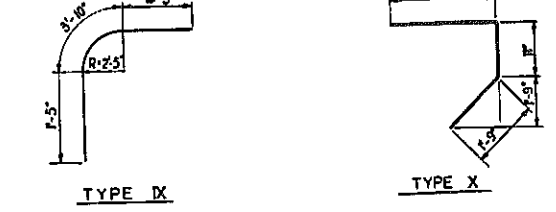
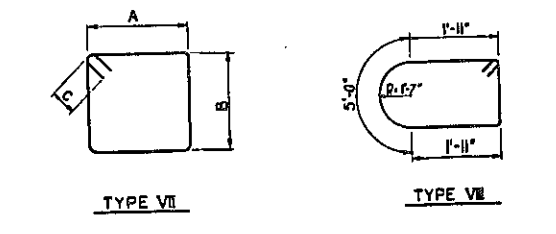
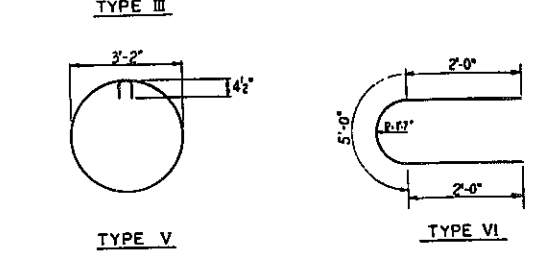
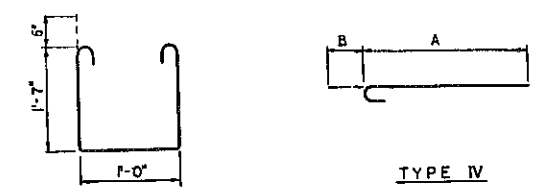
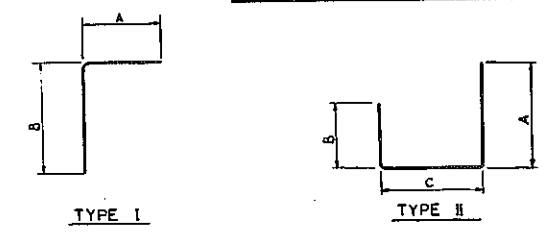
KENSINGTON EXPRESSWAY - SEC. NO. III.
CITY OF BUFFALO ARTERIAL-HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

ABUTMENTS												
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	DESCRIPTION		
				NORTH	SOUTH	TOTAL						
A401	4	6'-0"	STR.	137	137	274				TRANSVERSE FOOTING		
A402	4	48'-0"	STR.	20	20	40				LONGITUDINAL FOOTING		
A403	4	4'-6"	STR.	41	41	82				TRANSVERSE WINGWALL FOOTING		
A404	4	30'-2"	STR.	4	4	8				LONGITUDINAL WINGWALL FOOTING		
A405	4	27'-2"	STR.	4	4	8						
A406	4	3'-0"	I	25	29	54	1'-3"	1'-9"		BENT BAR TRANSVERSE SIDEWALK		
A407	4	29'-8"	STR.	3	3	6				LONGITUDINAL WINGWALL SIDEWALK		
A408	4	5'-3"	STR.	38	50	88				TRANSVERSE WINGWALL FOOTING		
A409	4	26'-0"	STR.	6	-	6				LONGITUDINAL WINGWALL FOOTING		
A410	4	27'-0"	STR.	-	-	-						
A411	4	34'-0"	STR.	-	6	6						
A412	4	35'-6"	STR.	-	4	4						
A413	4	12'-8"	II	50	50	100	6'-0"	3'-0"	3'-8"	BENT BAR, PEDESTAL		
A601	6	8'-7"	STR.	96	96	192				VERTICAL BACKWALL		
A602	6	51'-0"	XI	4	4	8	47'-6"	3'-6"	2'-6"	HORIZONTAL BACKWALL		
A603	6	43'-8"	XI	4	4	8	46'-5"	3'-3"	2'-4"			
A604	6	48'-8"	STR.	8	8	16				BENT BAR BACKWALL		
A605	6	4'-5"	X	-	46	46				VERTICAL PEDESTAL		
A606	6	6'-4"	STR.	30	30	60				VERTICAL HEADER		
A607	6	6'-8"	II	-	85	85	3'-0"	3'-0"	8'	VERTICAL WINGWALL		
A608	6	10'-8"	STR.	37	45	82				VERTICAL WINGWALL		
A609	6	10'-5"	I	14	14	28	9'-9"	8'		BENT BAR, CURTAIN WALL		
A610	6	3'-8"	II	4	4	8	1'-6"	1'-6"	8'	HORIZONTAL WINGWALL		
A611	6	26'-8"	STR.	5	5	10						
A612	6	29'-8"	STR.	5	5	10						
A613	6	5'-8"	STR.	5	5	10						
A614	6	25'-0"	STR.	10	-	10						
A615	6	33'-0"	STR.	10	-	10						

CURB												
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	DESCRIPTION		
				NORTH	SOUTH	TOTAL						
C501	5	1'-0"	XII	-	-	-	0'-3"	0'-6"	0'-3"	DOWELS		

PIER											
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION			
								P401	4	10'-9"	V
P402	4	9'-0"	W	8				CAP CORNERS			
P403	4	8'-4"	II	8	2'-6"	2'-6"	3'-4"	TIES, CAP CENTER			
P404	4	3'-6"	IV	98	3'-0"	6'		BENT BAR PEDESTAL			
P405	4	11'-4"	VII	200	2'-2"	3'-2"	0'-4"	CAP STIRRUPS			
P501	5	45'-1"	STR.	8				HORIZONTAL CAP			
P601	6	7'-0"	STR.	120				LONGITUDINAL & TRANSVERSE FOOTING			
P602	6	12'-10"	VR	6				DOWELS, FASCIA PEDESTALS			
P603	6	14'-4"	VR	24	3'-5"	3'-2"	4'	" INTERIOR "			
P901	9	4'-9"	STR.	84				DOWELS COLUMN			
P902	9	20'-2"	STR.	84				VERTICAL, COLUMN			
PI101	11	45'-0"	STR.	8				LONGITUDINAL BOTTOM CAP			
PI102	11	9'-1"	STR.	15				" TOP "			
PI103	11	46'-8"	STR.	4				" TOP "			
PI104	11	45'-1"	STR.	4				" TOP "			
PI105	11	18'-1"	I	6	14'-11"	3'-2"		BENT BAR, CAP CORNERS			
PI106	11	12'-0"	STR.	6				LONGITUDINAL TOP CAP			
PI107	11	9'-8"	IX	8				BENT BAR, CAP CORNERS			

SLAB											
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION			
								S401	4	46'-4"	STR.
S402	4	5'-9"	I	348	5'-2"	7'		TRANSVERSE SIDEWALK			
S403	4	AV. 3'-7"	STR.	6				TRANSVERSE SIDEWALK, 2 EACH, FROM 2'-1" TO 5'-1" INCREMENT 1'-6"			
S404	4	AV. 5'-2"	STR.	10				TRANS. SIDEWALK, 2 EACH, FROM 4'-8" TO 5'-8" INCREMENT 3"			
S405	4	AV. 3'-3"	STR.	8				TRANS. SIDEWALK, 2 EACH, FROM 2'-2" TO 4'-4" INCREMENT 8 1/2"			
S406	4	AV. 2'-1"	STR.	4				TRANSVERSE SIDEWALK, 2 EACH, 1'-0" & 3'-2"			
S407	4	AV. 3'-2"	STR.	4				" " 2 EACH, 2'-0" & 4'-4"			
S408	4	5'-2"	II	348				BENT BAR, SIDEWALK			
S409	4	46'-4"	STR.	484				LONGITUDINAL TOP & BOT.			
S501	5	34'-4"	IV	418	33'-9"	7'		TRANSVERSE TOP			
S502	5	30'-6"	STR.	316				" BOTTOM			
S503	5	37'-0"	STR.	102				" "			
S504	5	AV. 19'-10"	STR.	112				TRANS. TOP, 2 EACH, FROM 5'-10" TO 33'-10" INCREMENT 6"			
S505	5	AV. 23'-3"	STR.	112				TRANS. BOT., 2 EACH, FROM 9'-5" TO 37'-5" INCREMENT 6"			
S506	5	AV. 21'-10"	STR.	616				TRANSVERSE TOP & BOT. 2 EACH FROM 5'-8" TO 38'-0" INCREMENT 5'-2"			
S507	5	AV. 22'-10"	STR.	102				TRANS. TOP, 2 EACH, FROM 7'-8" TO 38'-0" INCREMENT 7'-6"			
S508	5	AV. 22'-9"	STR.	102				TRANS. BOT., 2 EACH, FROM 11'-1" TO 34'-5" INCREMENT 5'-2"			
S509	5	49'-5"	STR.	16				TRANSVERSE TOP & BOT.			
S510	5	48'-6"	STR.	16				" "			



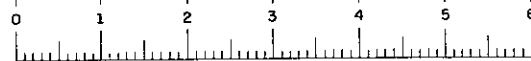
NUMBER	SIZE
4	1/2"
5	3/8"
6	3/4"
9	1"
11	1 1/4"

FILLMORE AVE. OVER EXPRESSWAY
BAR LIST

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III.

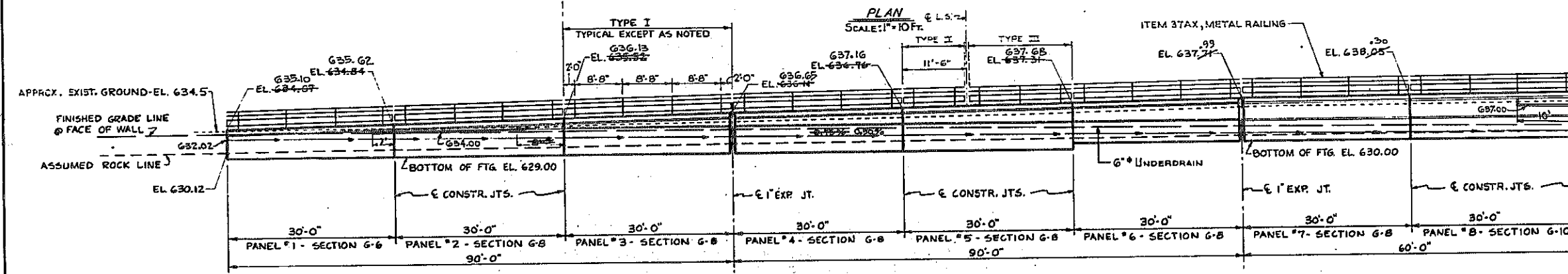
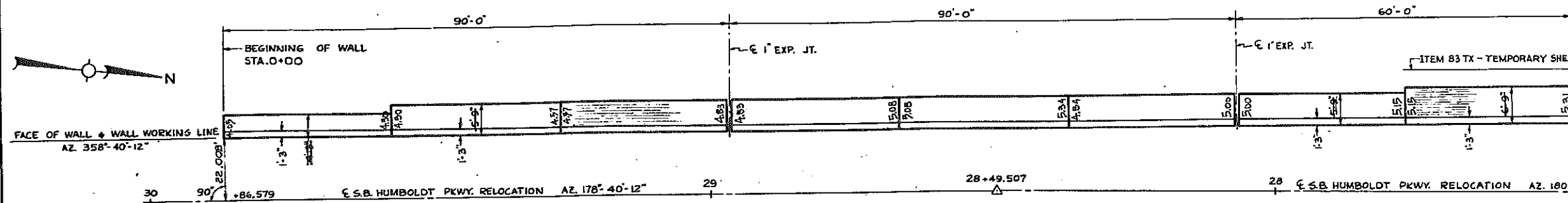
DE LEUW, CATHER & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN P. C.
CHECKED D. H.
TRACED P. C.



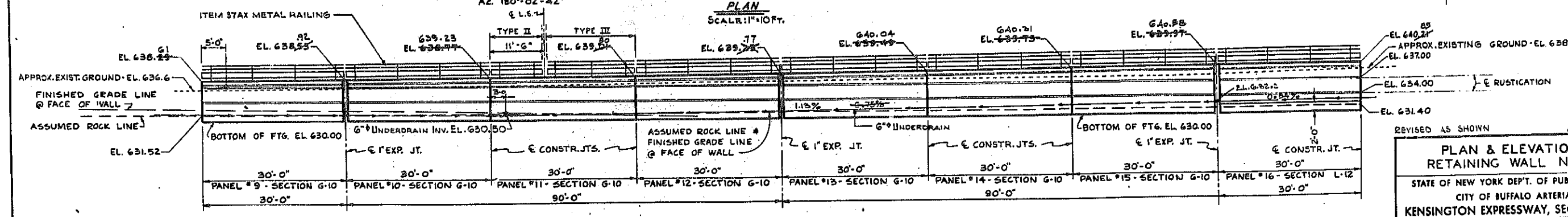
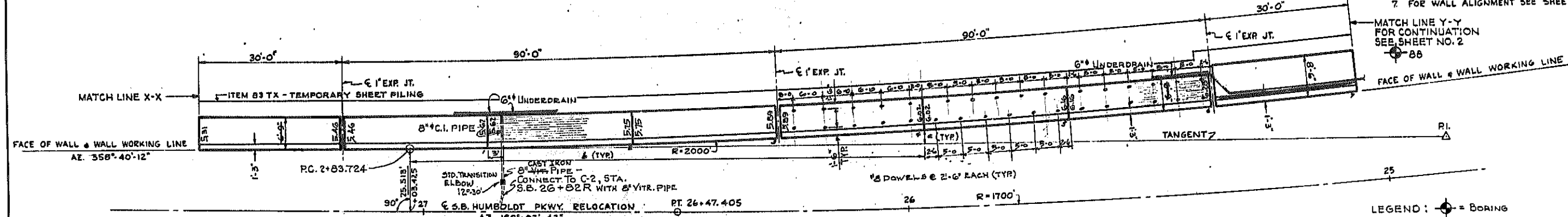
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	159 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT 3 - NORTHLAND AVE. TO FILLMORE AVE.
 RUSTICATION CHANGED BY LETTER DATED APR. 13, 64



DEVELOPED ELEVATION
 SCALE: 1" = 10 FT.

- NOTES:**
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILING SEE SHEET NO. 12
 6. FOR GENERAL WALL NOTES SEE SHEET NO. 11
 7. FOR WALL ALIGNMENT SEE SHEET NO. 22



DEVELOPED ELEVATION
 SCALE: 1" = 10 FT.

LEGEND: = BORING

EL. 636.29
 APPROX. EXISTING GROUND - EL. 636.6
 EL. 634.00
 EL. 631.40

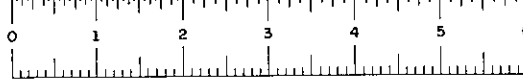
REVISED AS SHOWN

**PLAN & ELEVATION
 RETAINING WALL NO. 1**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

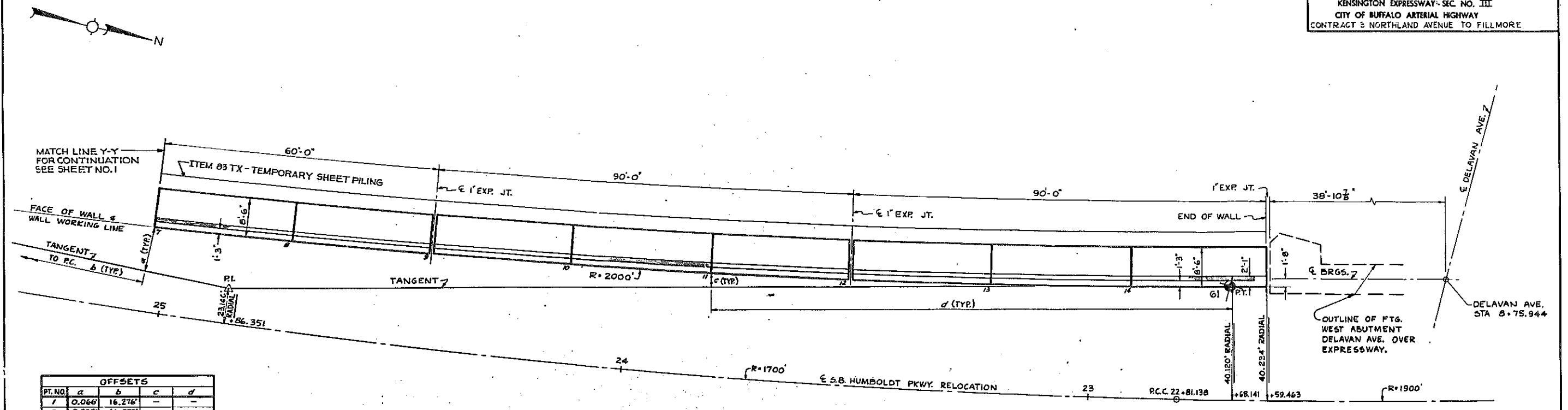
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D. T.E.K.
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BR. SHEET NO. 11



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	180 R1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT 3 NORTHLAND AVENUE TO FILLMORE

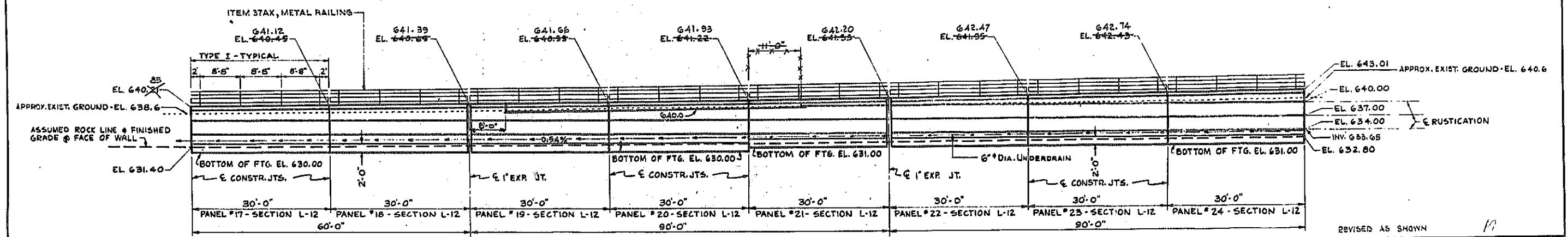


PT. NO.	a	b	c	d
1	0.066'	16.276'	-	-
2	0.535'	46.272'	-	-
3	1.454'	76.257'	-	-
4	2.823'	106.226'	-	-
5	4.641'	136.170'	-	-
6	6.908'	166.084'	-	-
7	9.623'	195.961'	-	-
8	-	-	10.141'	201.154'
9	-	-	7.348'	171.285'
10	-	-	5.003'	141.377'
11	-	-	3.107'	111.437'
12	-	-	1.660'	81.472'
13	-	-	0.663'	51.489'
14	-	-	0.116'	21.495'

WALL CURVE DATA	
R = 2000'	
Δ = 12° 15' 17.1"	
T = 214.705'	
L = 427.771'	
P.C. = 2+83.724	
P.T. = 4+29.429	A+98.429
RT = 7+11.429	7+11.429

PLAN
SCALE: 1" = 10 FT.

- NOTES:
- ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 - FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 - FOR WALL SECTIONS SEE SHEET NO. 9
 - FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 - FOR DETAILS OF RAILING SEE SHEET NO. 12
 - FOR GENERAL WALL NOTES SEE SHEET NO. 11



DEVELOPED ELEVATION
SCALE: 1" = 10 FT.

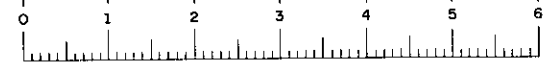
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PLAN & ELEVATION
RETAINING WALL NO. 1 [CONT'D]

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

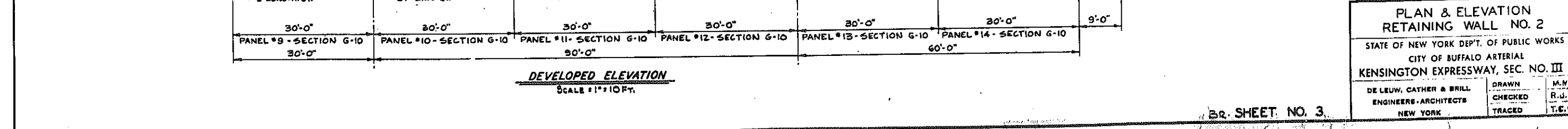
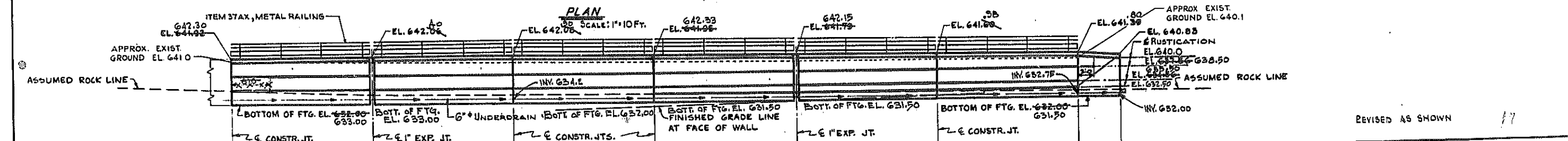
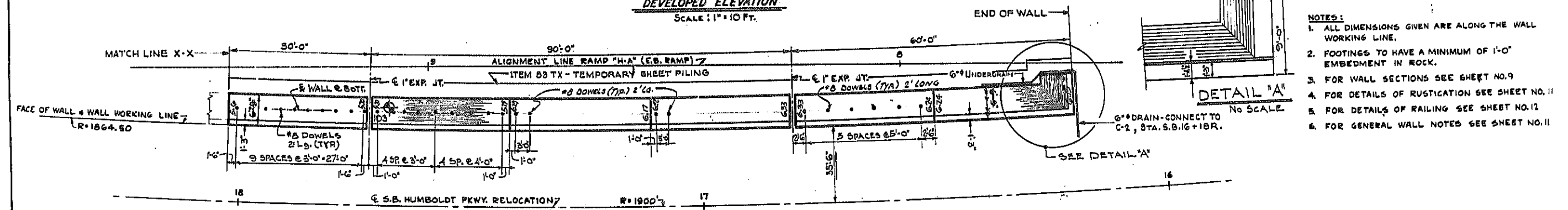
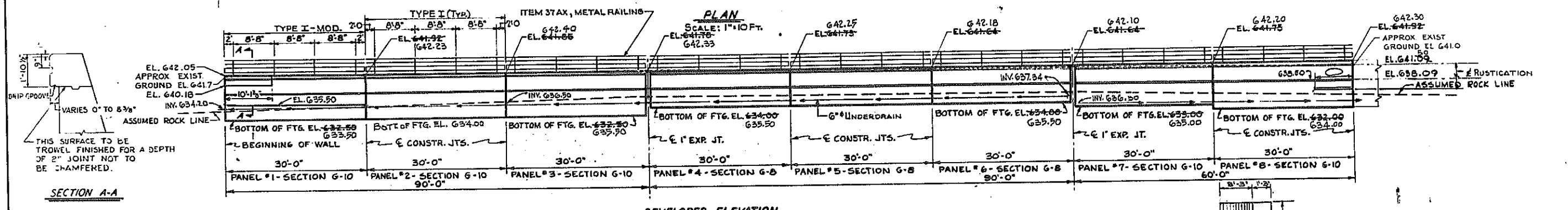
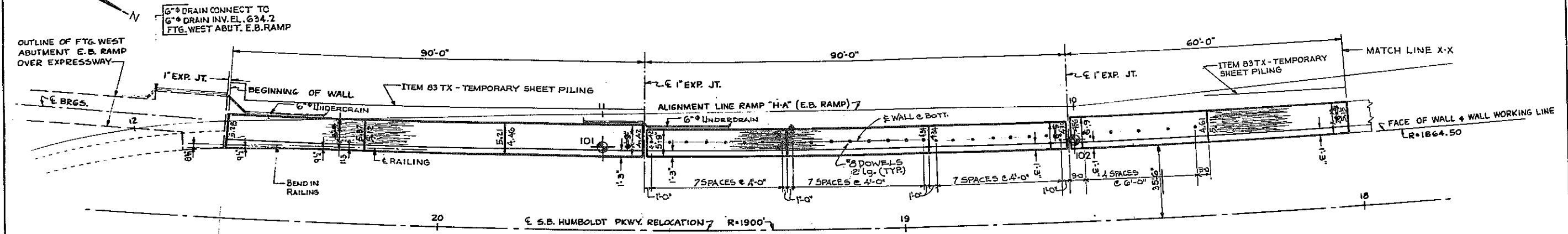
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D. T.E.K.
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BR. SHEET NO. 2



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	161 RI	173

KENSINGTON EXPRESSWAY - SEC. NO III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3, NORTHLAND AVE. TO FILLMORE AVE.



- NOTES:
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILING SEE SHEET NO. 12
 6. FOR GENERAL WALL NOTES SEE SHEET NO. 11

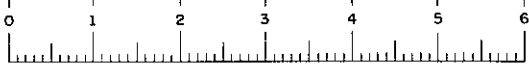
REVISED AS SHOWN

**PLAN & ELEVATION
RETAINING WALL NO. 2**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

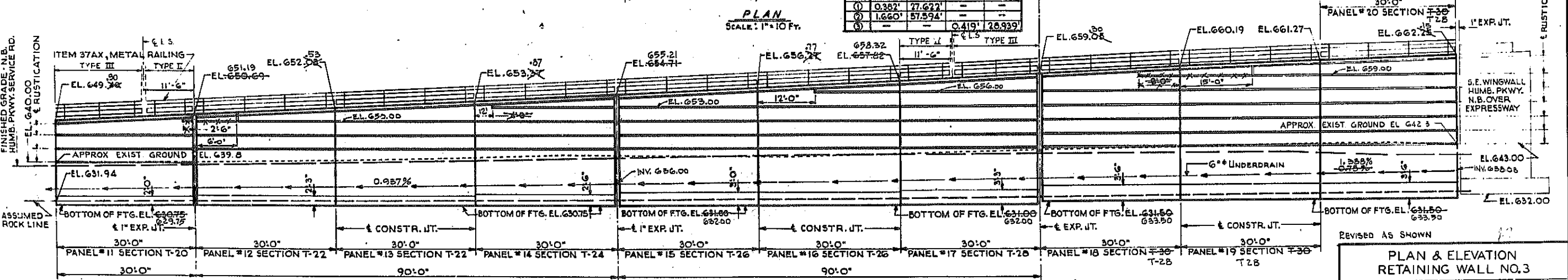
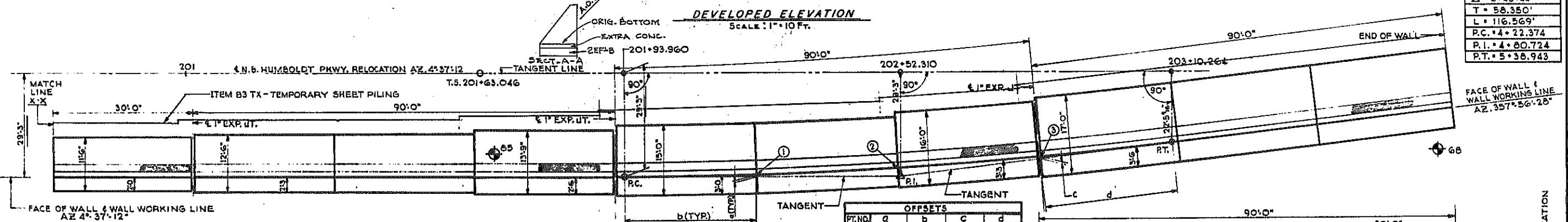
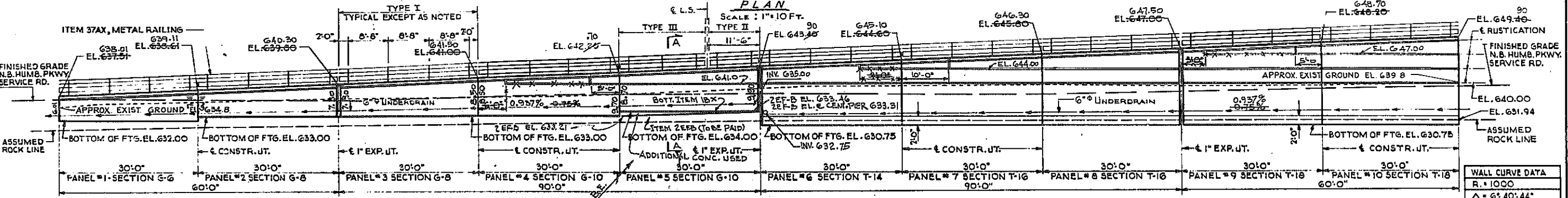
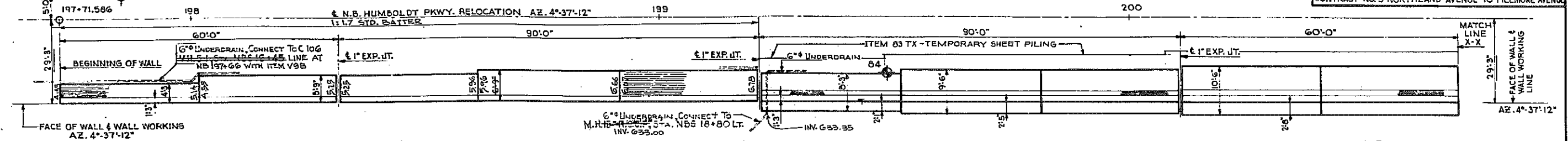
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.V. R.J.D. T.E.K.
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BR. SHEET NO. 3



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	162 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
- ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 - FOOTINGS TO HAVE A MINIMUM OF 18" EMBEDMENT IN ROCK.
 - FOR WALL SECTIONS SEE SHEET NO. 9 & 10
 - FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 - FOR DETAILS OF RAILING SEE SHEET NO. 12
 - FOR GENERAL NOTES SEE SHEET NO. 11

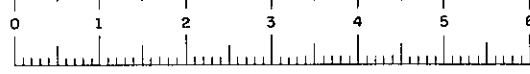
NOTES: PANELS 18, 19 & 20 WERE REDESIGNED AND WERE TO HAVE BEEN BUILT AS T-28'S. CONTRACTOR BUILT T-30 FOOTINGS WITH T-28 WALLS.

REVISED AS SHOWN

**PLAN & ELEVATION
RETAINING WALL NO. 3**

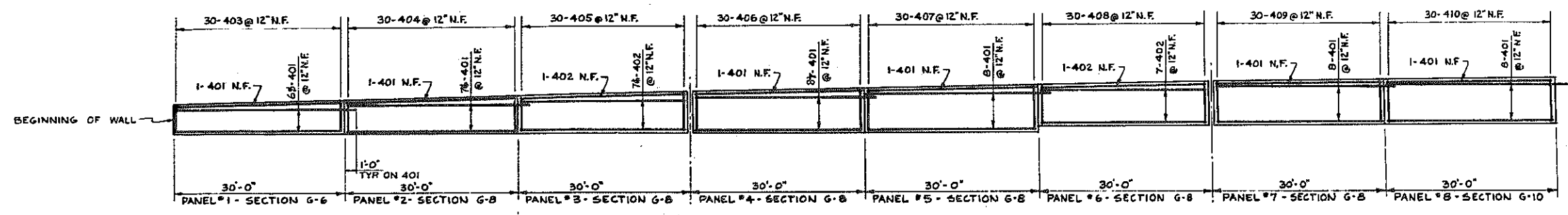
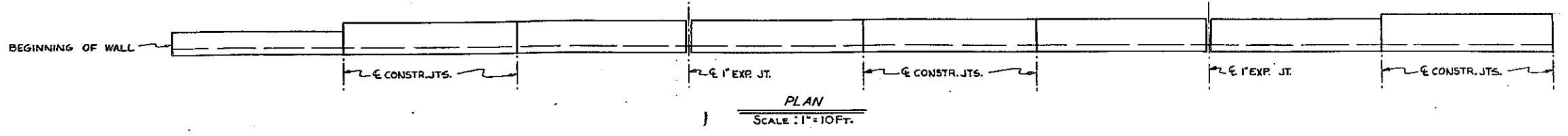
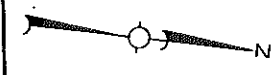
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REK R.J.D.
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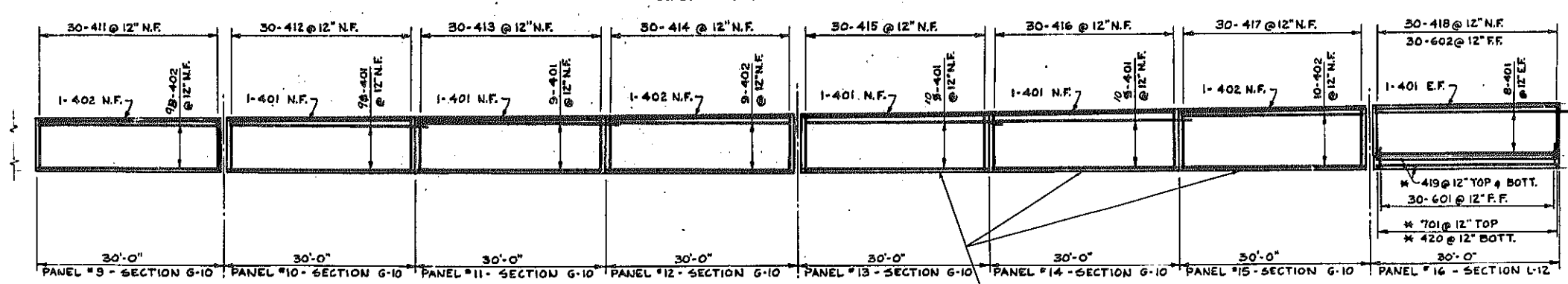
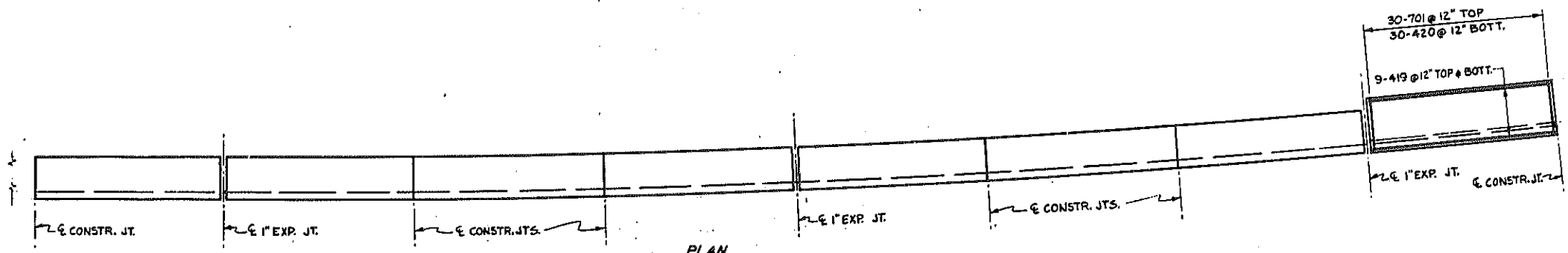


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-66-377 (25)	53 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



ELEVATION
SCALE: 1" = 10 FT.



ELEVATION
SCALE: 1" = 10 FT.

#8 Dowels, 2'-6" Long, were incorporated to key Panels 13 thru 15 to rock. For spacing see sheet 157 - Expense incurred in placing these dowels to be borne by the contractor.

BAR REINFORCEMENT LEGEND

7	4	3
↑	↑	↑
NUMBER OF BARS	BAR SIZE	BAR MARK

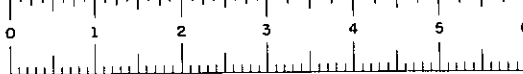
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BAR REINFORCEMENT RETAINING WALL NO. 1

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

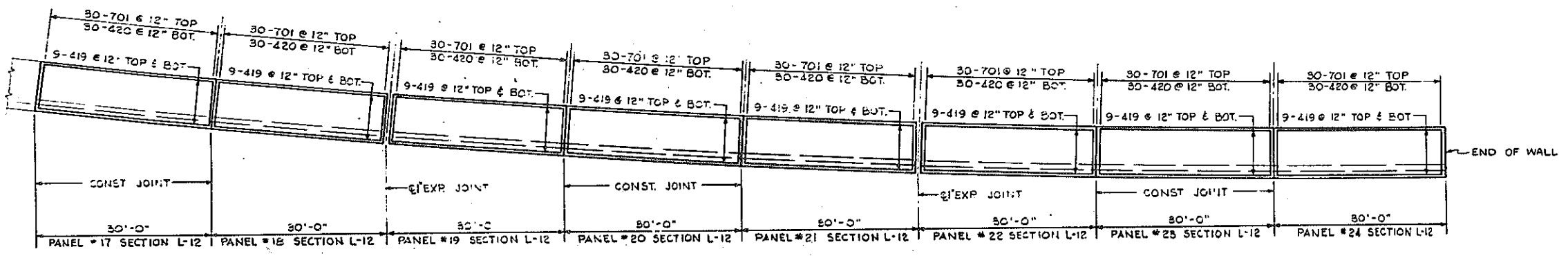
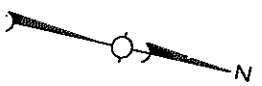
DE LIEW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D.
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BR. SHEET NO. 5

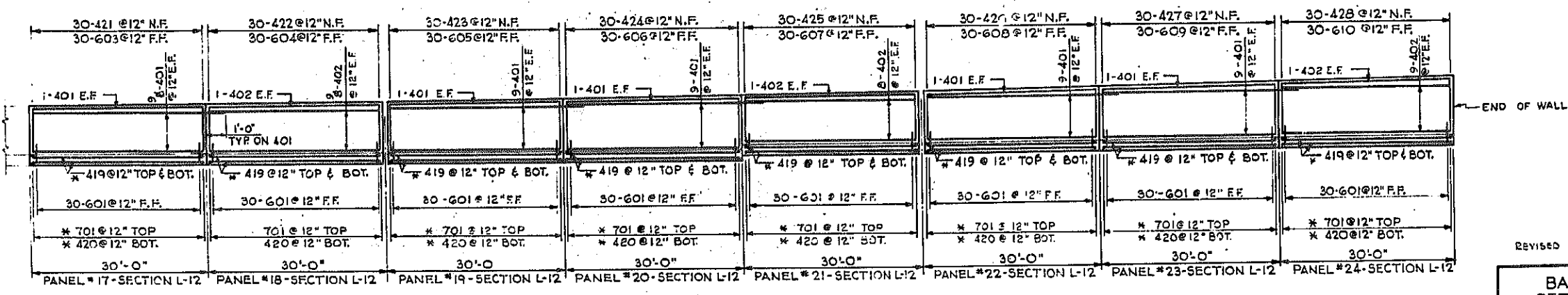


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	164-R	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN
 SCALE: 1" = 10 FT.



ELEVATION
 SCALE: 1" = 10 FT.

BAR REINFORCEMENT LEGEND

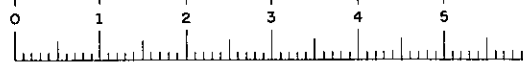
NUMBER OF BARS	BAR SIZE	BAR MARK
7	4	01

REVISED AS SHOWN

BAR REINFORCEMENT RETAINING WALL NO. I (CONT.)

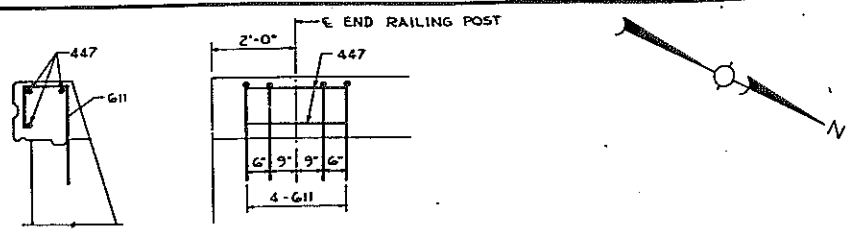
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL HIGHWAY
 KENSINGTON EXPRESSWAY, SEC. NO. III

DR. LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN	M.M.
	CHECKED	R.L.D.
	TRACED	

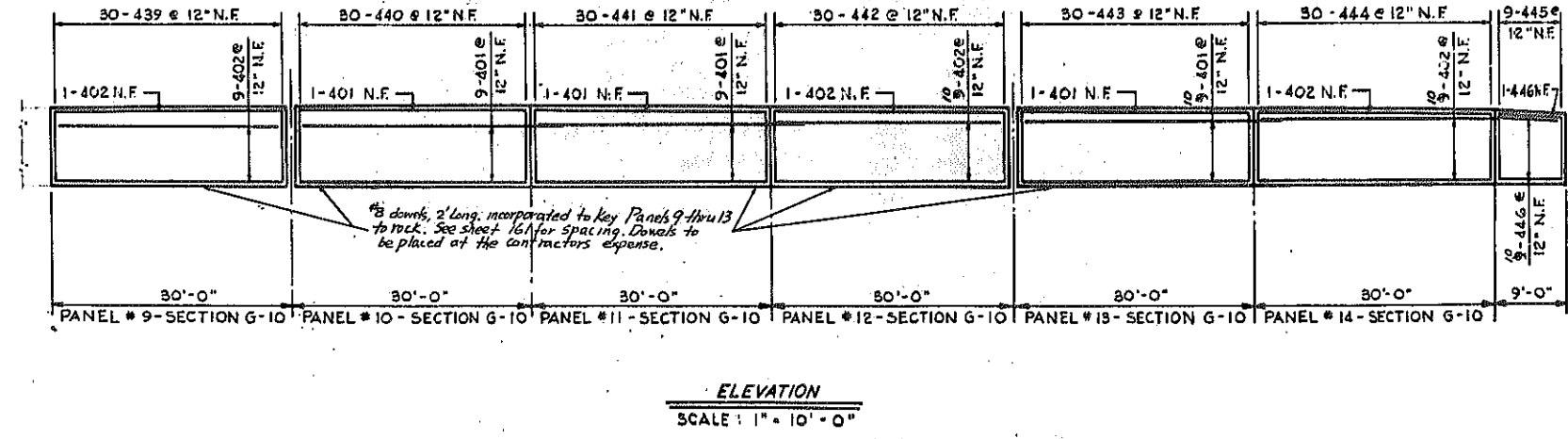
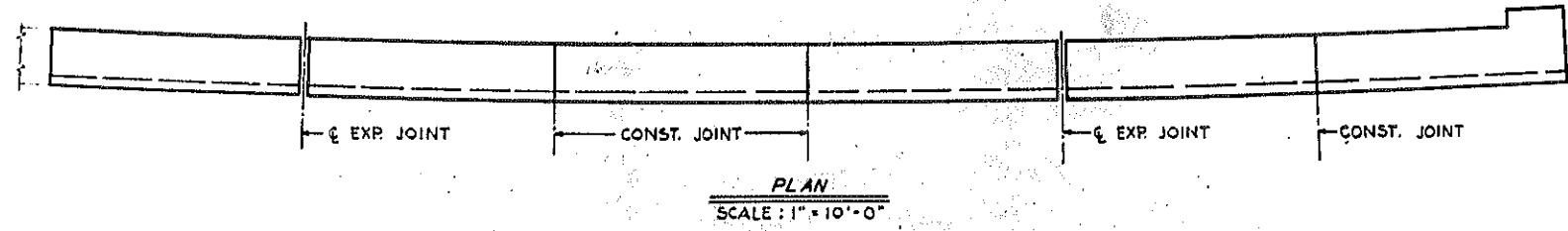
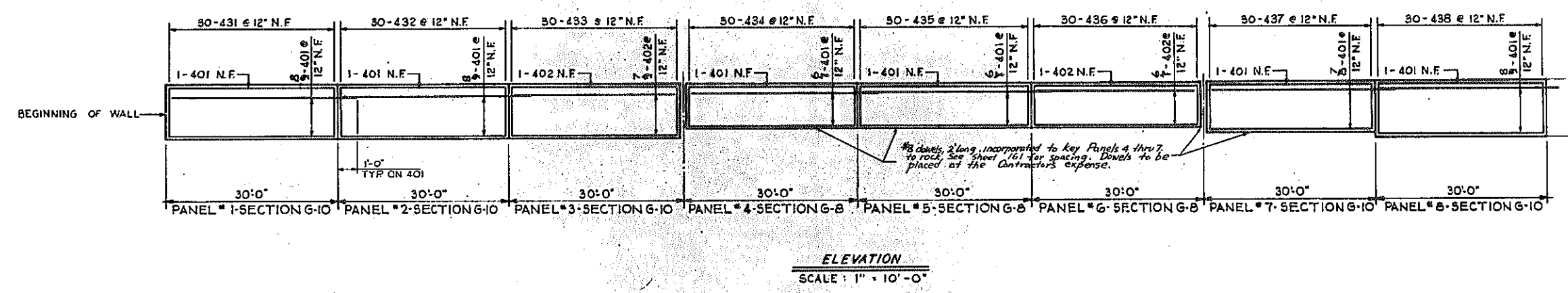
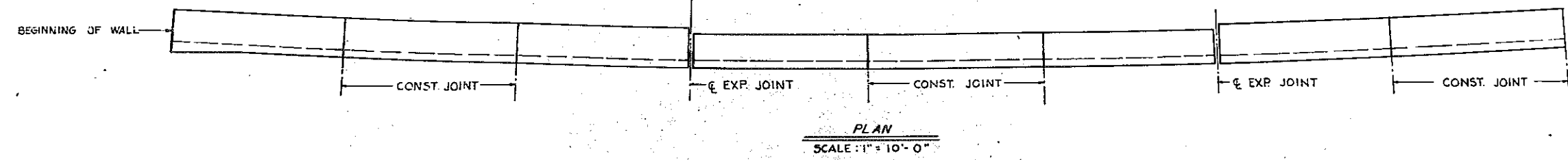


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	165 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



ADDED REINFORCEMENT AT BEGINNING OF WALL



BAR REINFORCEMENT LEGEND

7	4	01
↑	↑	↑
NUMBER OF BARS	BAR SIZE	BAR MARK

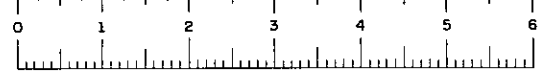
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BAR REINFORCEMENT RETAINING WALL NO. 2

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	BEK R.J.D.
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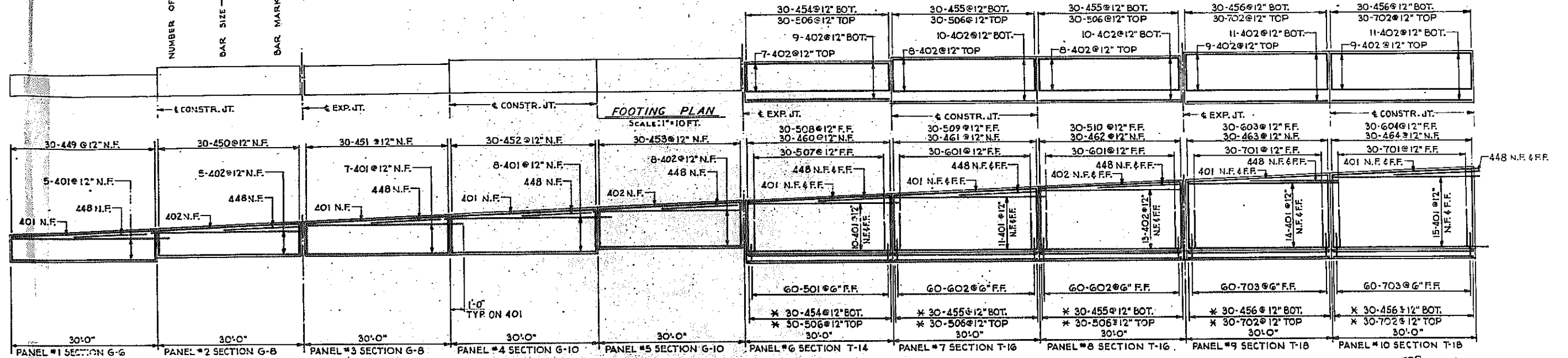
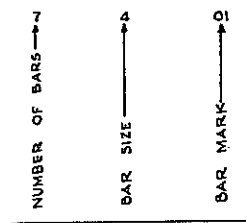
B2 SHEET NO. 7



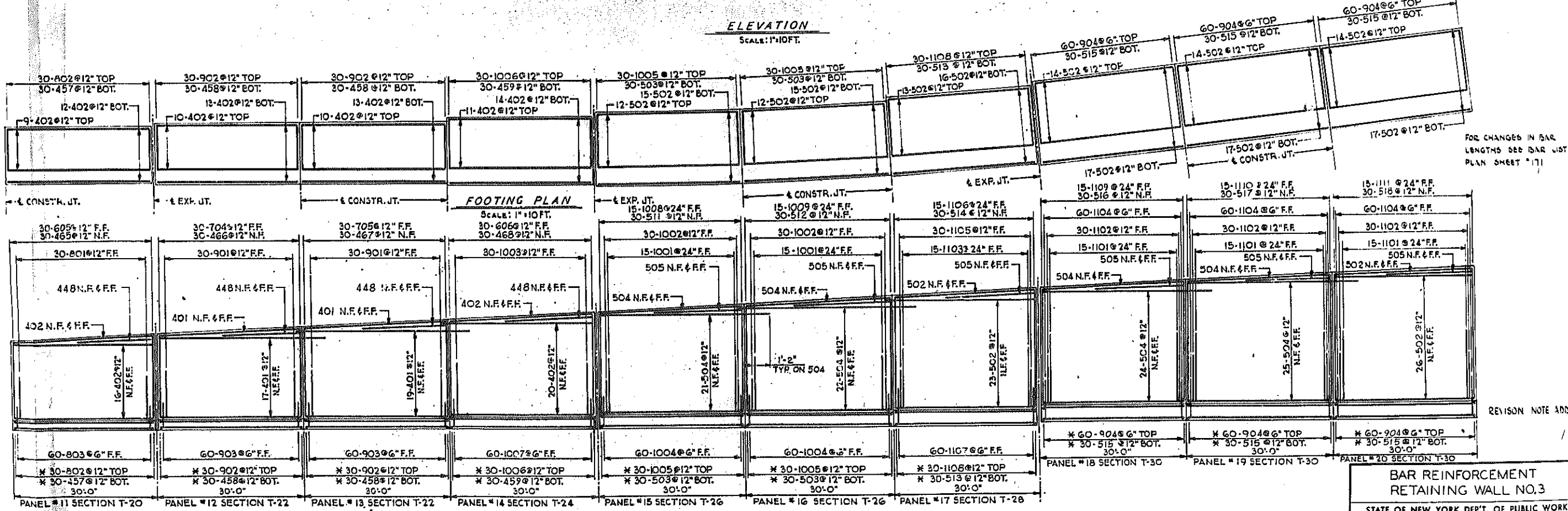
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-26-377 (25)	128 RI	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO F. W. WIDE AVENUE

BAR REINFORCEMENT LEGEND



ELEVATION
SCALE: 1"=10 FT.



FOR CHANGES IN BAR LENGTHS SEE BAR LIST PLAN SHEET #11

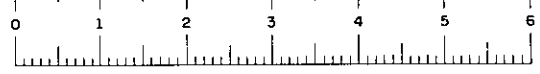
REVISION NOTE ADDED

ELEVATION
SCALE: 1"=10 FT.

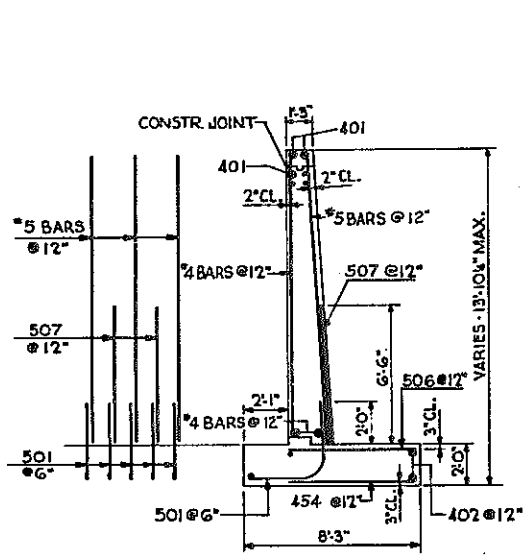
BAR REINFORCEMENT RETAINING WALL NO. 3

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

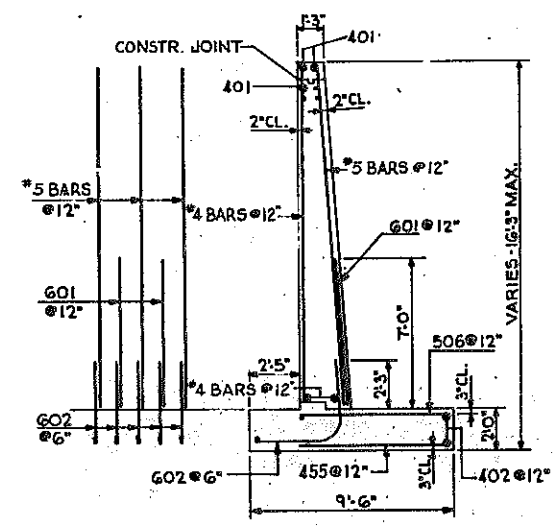
DE LEUW, CATHER & BRILL	DRAWN	ZBK
ENGINEERS-ARCHITECTS	CHECKED	R.J.D.
NEW YORK	TRACED	



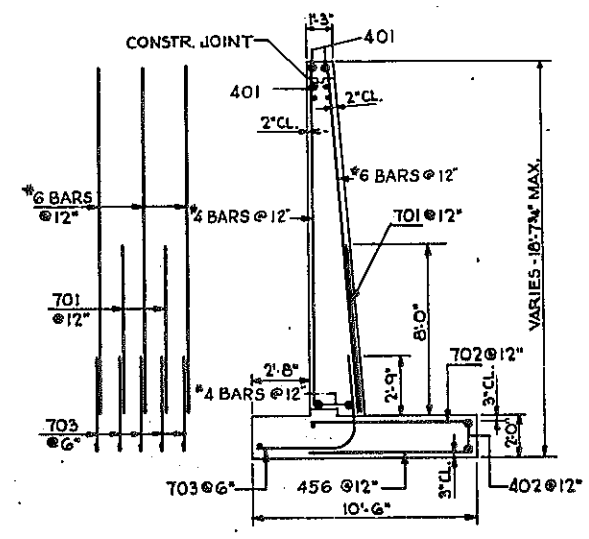
FED. ROAD RES. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	167R	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



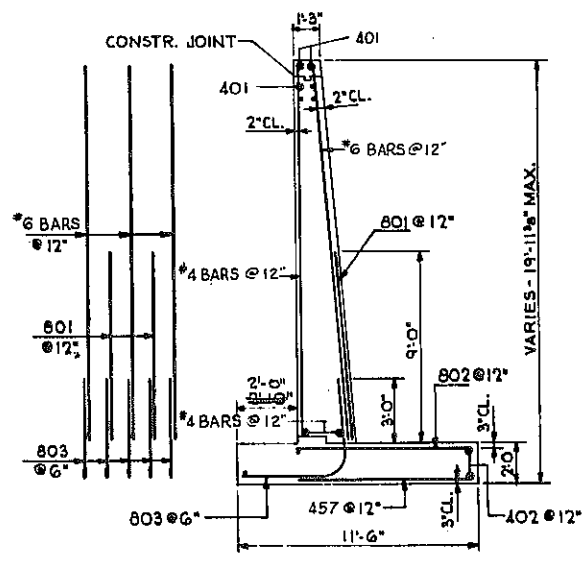
WALL SECTION T-14



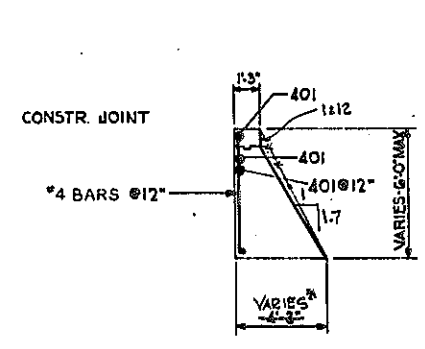
WALL SECTION T-16



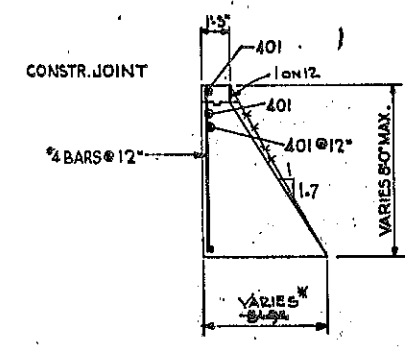
WALL SECTION T-18



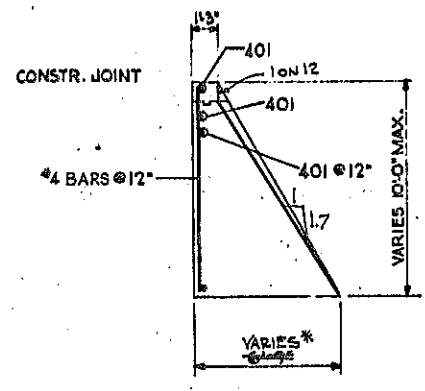
WALL SECTION T-20



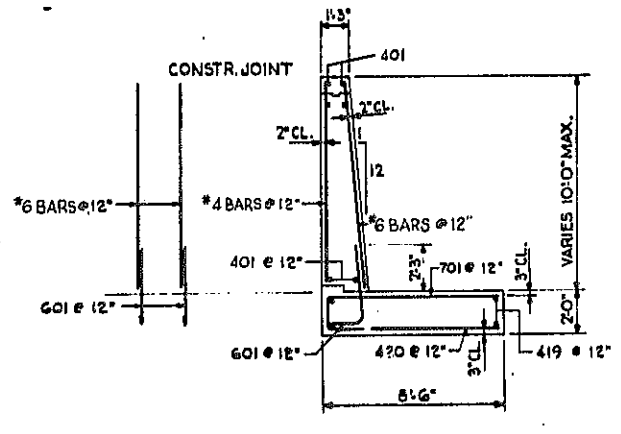
WALL SECTION G-6



WALL SECTION G-8



WALL SECTION G-10

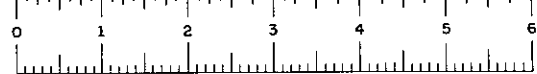


WALL SECTION L-12

NOTE: FOR WIDTH OF BOTTOM FOR THESE PANELS SEE SHEET NO'S 159, 161 & 163
1 ON 1.7 SLOPE ON SECT G-6, G-8 & G-10 APPLIES ONLY TO WALL B.

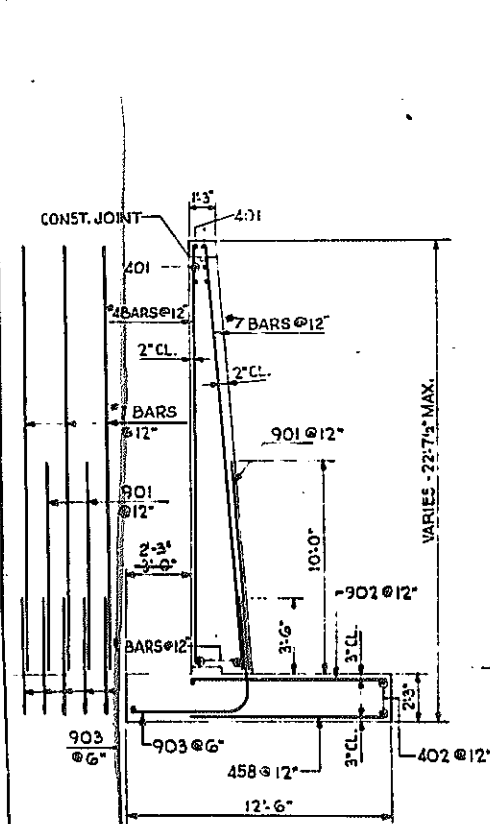
REVISION TO WALL BATTER

WALL SECTIONS G-6, G-8, G-10, L-12, T-14, T-16, T-18, T-20		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	EEK. R.J.D.

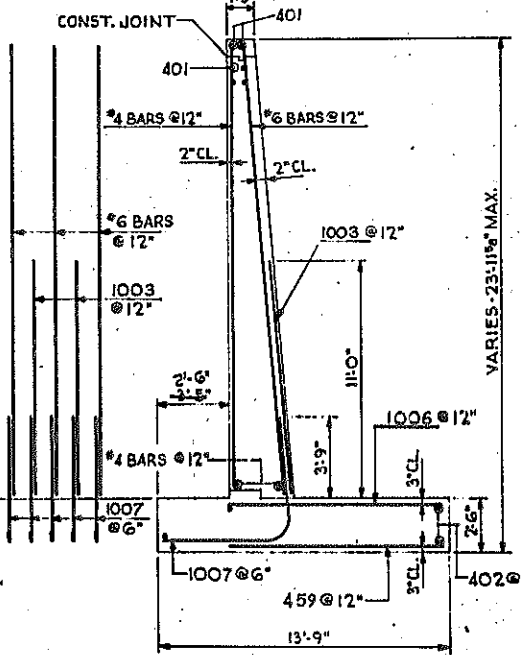


FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	168 RI	173

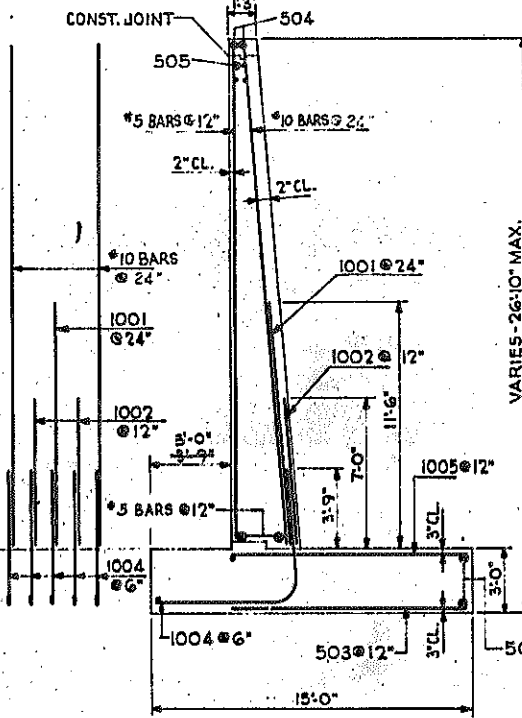
KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



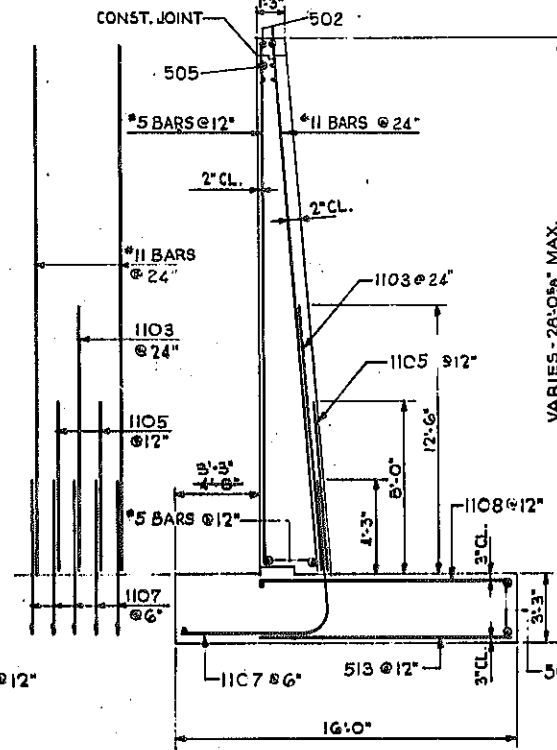
WALL SECTION T-22
SCALE: 1/4" = 1 FT.



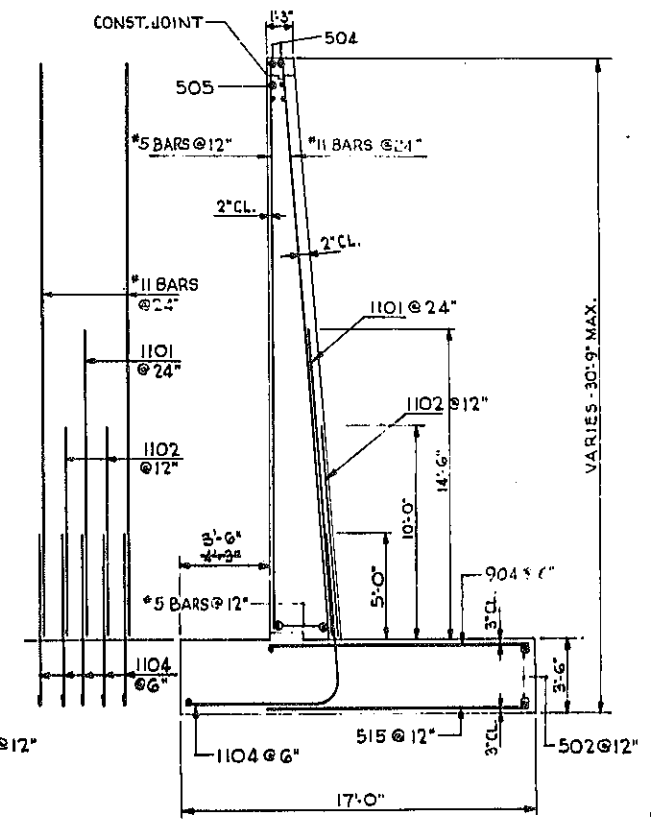
WALL SECTION T-24
SCALE: 1/4" = 1 FT.



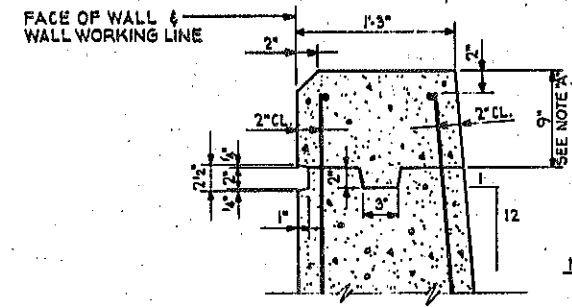
WALL SECTION T-26
SCALE: 1/4" = 1 FT.



WALL SECTION T-28
SCALE: 1/4" = 1 FT.

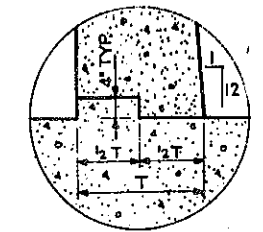


WALL SECTION T-30
SCALE: 1/4" = 1 FT.



TOP OF WALL DETAIL
SCALE: 1/2" = 1 FT.

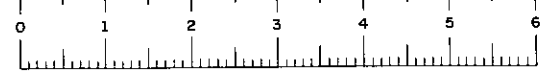
NOTE "A"
SEE BEGINNING OF WALL #2 FOR VARIATION.



TYPICAL KEY DETAIL
SCALE: 3/8" = 1 FT.

REVISION TO FOOTING - (TOE)

WALL SECTIONS T-22, T-24, T-26, T-28, T-30	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN: B&K CHECKED: R.J.D. TRACED



FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	169R	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3, NORTHLAND AVE. TO FILLMORE AVE.

ESTIMATE OF QUANTITIES - WALL NO. 1

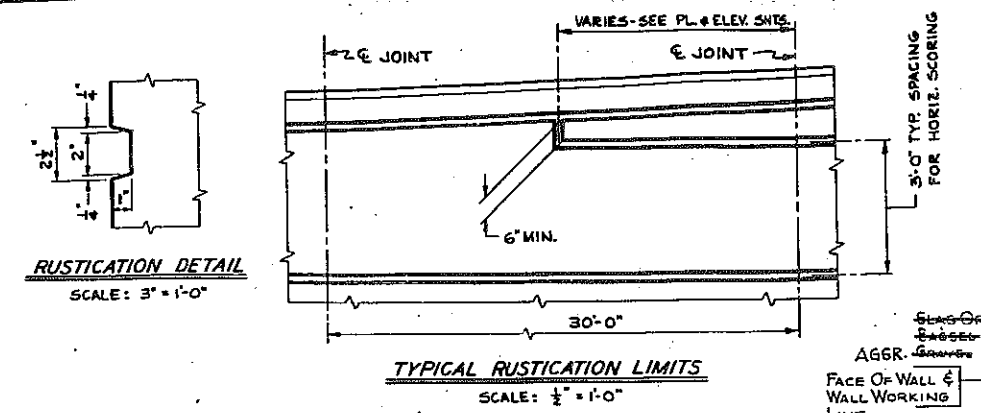
NO.	ITEM	UNIT	NEAT	FINAL QUANTITIES
2EF-B	SELECTED FILL	C.Y.	1,249	988.63
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	785	271.11
11H	PERF CORR METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	720	720.0
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	1,287	155.73
18-X	CLASS IA CONC. FOR STRUCTURES	C.Y.	312	312.84
20Y	CLASS I CONCRETE	C.Y.	508	450.77
24AXX	BAGGED-SCREENED GRAVEL OR SLAG	C.Y.	185	185.1
2E	BAR REINFORCEMENT FOR STRUCTURES	L.B.	26,315	2626.4
29B	STRUCTURAL STEEL	L.B.	2,369	226.9
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	717	720.1
61	BITUMINOUS MATERIAL	Gal.	502	320.6
301B	2" GALV. STEEL CONDUIT	L.F.	335	16.0
301C	2 1/2" GALV. STEEL CONDUIT	L.F.	8	3.6
303C9A	FURNISH & INSTALL LIGHT STDS. 30" MTG. HGT.	Eq.	1	1
305C9B	FURNISH & INSTALL LIGHT STDS. 30" MTG. HGT.	Eq.	1	1
352XA	RUBBER JOINT MATERIAL	Gal.	16	6.75
9VC	SEWER PIPE (VITRIFIED) 8"	L.F.	7	7
13C	CAST IRON PIPE (8")	L.F.	8	8
83TX	TEMPORARY SHEET PILING	S.F.	3,468	0

ESTIMATE OF QUANTITIES - WALL NO. 2

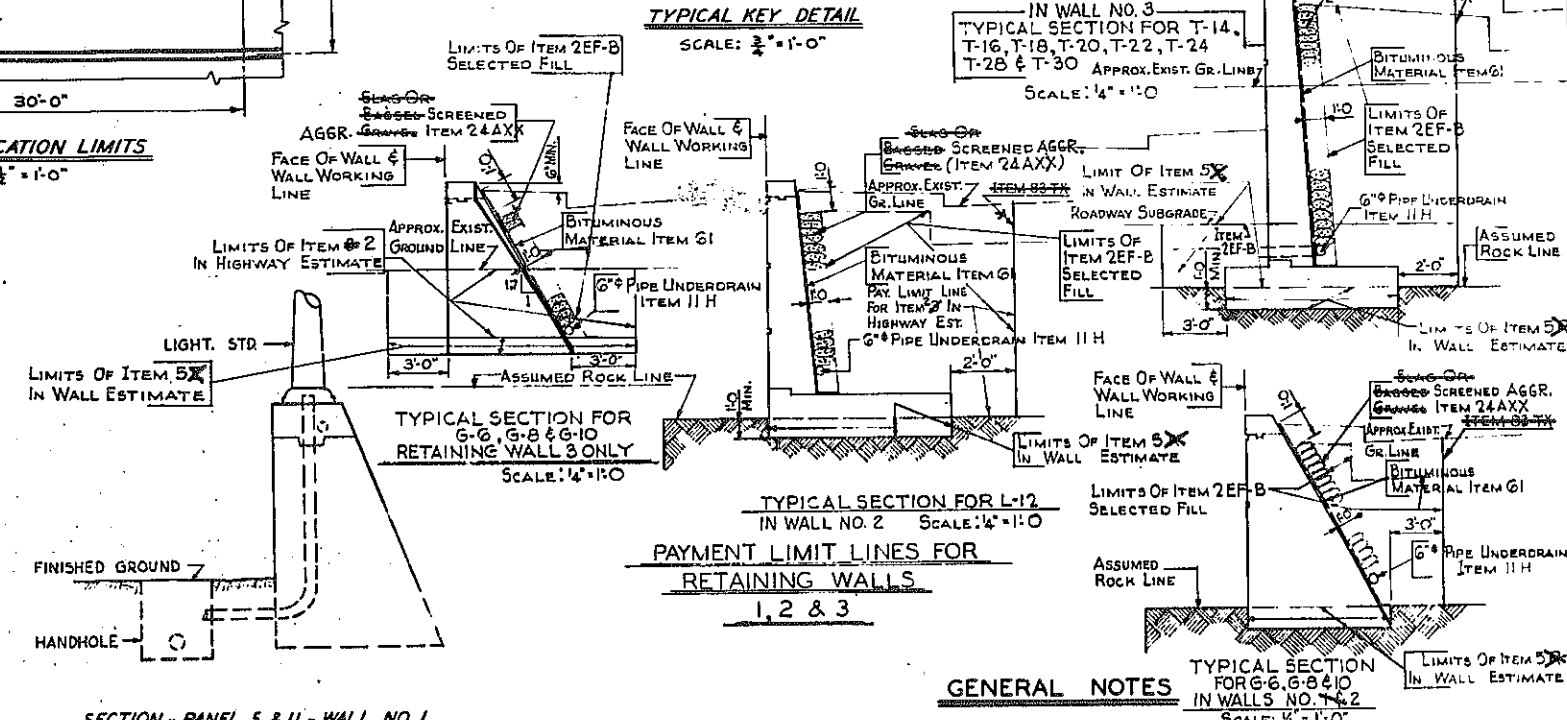
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2EF-B	SELECTED FILL	C.Y.	515	371.46
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	239	151.61
11H	PERF CORR METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	460	455.0
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	871	601.85
20Y	CLASS I CONCRETE	C.Y.	562	425.57
24AXX	BAGGED-SCREENED GRAVEL OR SLAG	C.Y.	99	86.54
2E	BAR REINFORCEMENT FOR STRUCTURES	L.B.	5,330	448.72
29B	STRUCTURAL STEEL	L.B.	1,310	125.6
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	420	411.1
61	BITUMINOUS MATERIAL	Gal.	309	111
352XA	RUBBER JOINT MATERIAL	Gal.	9	5.0
83TX	TEMPORARY SHEET PILING	S.F.	1,935	0

ESTIMATE OF QUANTITIES - WALL NO. 3

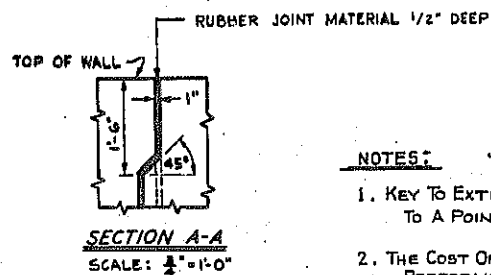
NO.	ITEM	UNIT	NEAT	FINAL QUANTITIES
2EF-B	SELECTED FILL	C.Y.	3,581	327.17
11H	PERF CORR METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	600	511.0
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	2,273	2214.4
18X	CLASS IA CONC. FOR STRUCTURES	C.Y.	1,280	1211.87
20Y	CLASS I CONCRETE	C.Y.	149	176.88
24AXX	BAGGED-SCREENED GRAVEL OR SLAG	C.Y.	351	242.20
2E	BAR REINFORCEMENT FOR STRUCTURES	L.B.	120,901	10,218
29B	STRUCTURAL STEEL	L.B.	2,037	176.2
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	598	600.4
61	BITUMINOUS MATERIAL	Gal.	750	281.7
83TX	TEMPORARY SHEET PILING	S.F.	3,673	0
301B	2" GALV. STEEL CONDUIT	L.F.	615	544.4
301C	2 1/2" GALV. STEEL CONDUIT	L.F.	15	14.8
303C15	FURNISH & INSTALL LIGHT STDS. 30" MTG. HGT.	Eq.	3	3
352XA	RUBBER JOINT MATERIAL	Gal.	18	13.1
9VC	SEWER PIPE (VITRIFIED) 8"	L.F.	35	34
13C	CAST IRON PIPE (8")	L.F.	4	4.00
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	1,673	1208.70



Note: Item 5 replaces Item 5E and 5R where they appear on these plans.

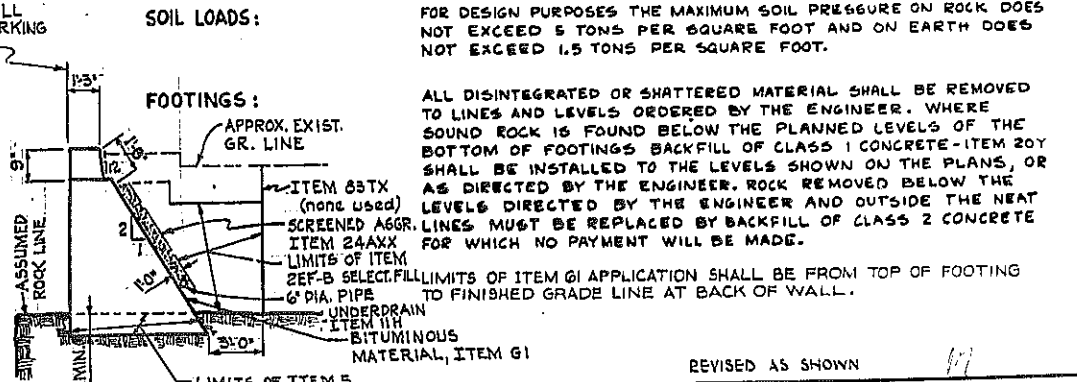
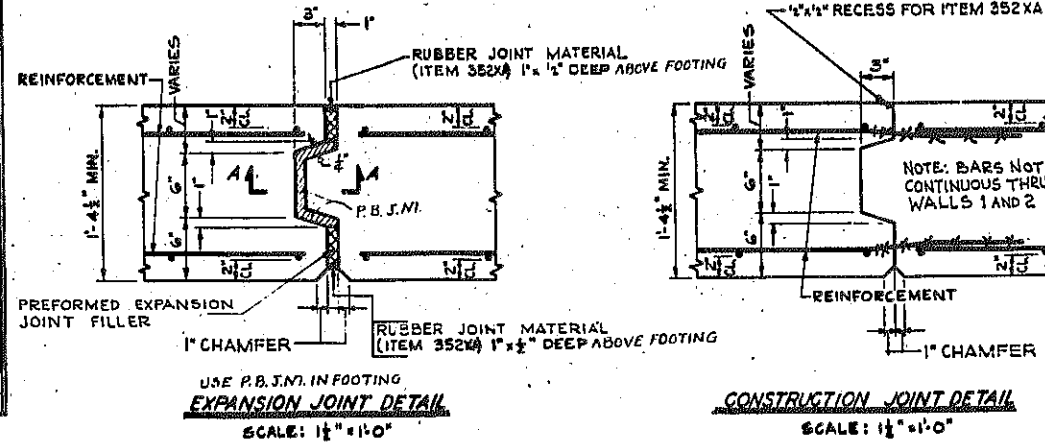


SECTION - PANEL 5 & 11 - WALL NO. 1 ELECTRICAL CONDUIT NO SCALE



NOTES:

1. KEY TO EXTEND FROM TOP OF FOOTING TO A POINT 1/8 BELOW TOP OF WALL.
2. THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER AND P.B.J.M. SHALL BE INCLUDED IN THE PRICES BID FOR ITEMS 18X AND 20Y.



TYPICAL SECTION FOR G-6, G-8 & G-10 WALL No. 1. NO SCALE

DESIGN SPECIFICATIONS: A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

CONCRETE: CONCRETE IN GRAVITY WALL SECTIONS SHALL BE ITEM 20Y CLASS I CONCRETE. CONCRETE IN ALL OTHER SECTIONS AND WALL CONSTRUCTION SHALL BE ITEM 18X CLASS IA CONCRETE FOR STRUCTURES.

CEMENT: ALL CEMENT USED IN THE CONCRETE ITEMS FOR WALLS SHALL BE PORTLAND CEMENT TYPE 2A ITEM 15-2A, ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

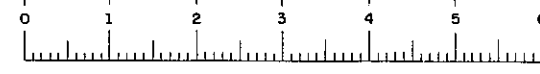
SOIL LOADS: FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK DOES NOT EXCEED 5 TONS PER SQUARE FOOT AND ON EARTH DOES NOT EXCEED 1.5 TONS PER SQUARE FOOT.

FOOTINGS: ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

LIMITS OF ITEM G1 APPLICATION SHALL BE FROM TOP OF FOOTING TO FINISHED GRADE LINE AT BACK OF WALL.

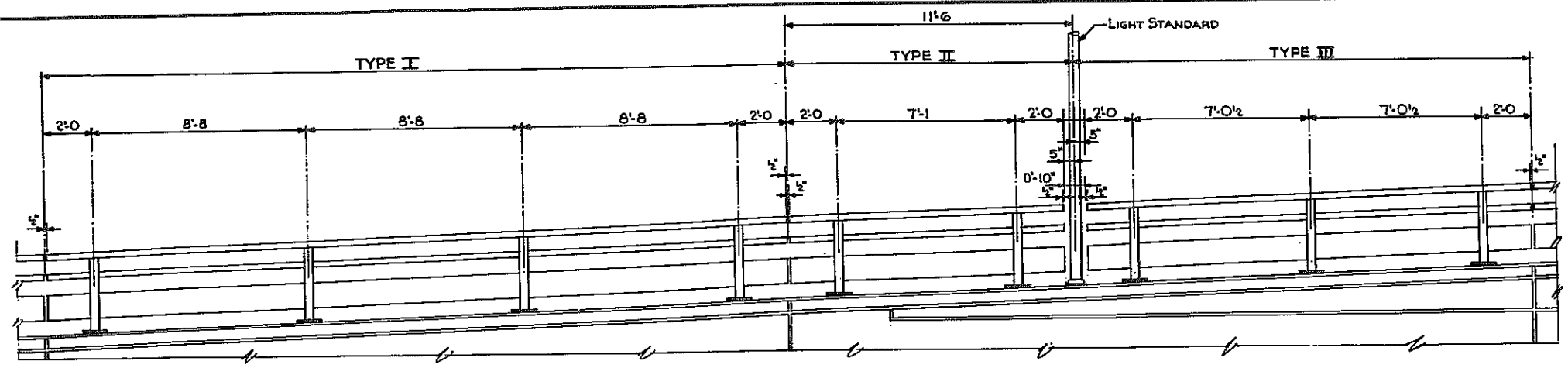
REVISED AS SHOWN

DETAILS RETAINING WALLS 1, 2 & 3			
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. R.J.D. T.E.K.	



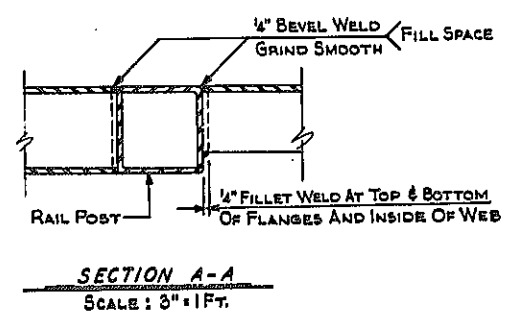
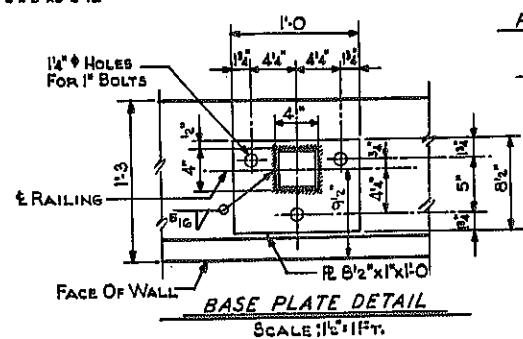
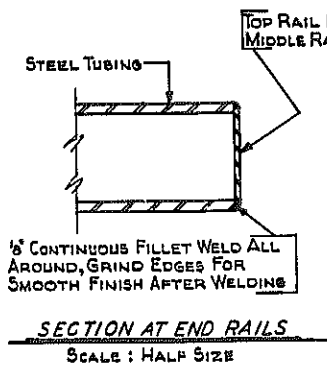
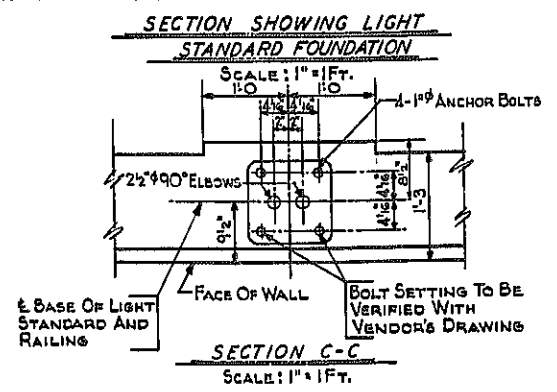
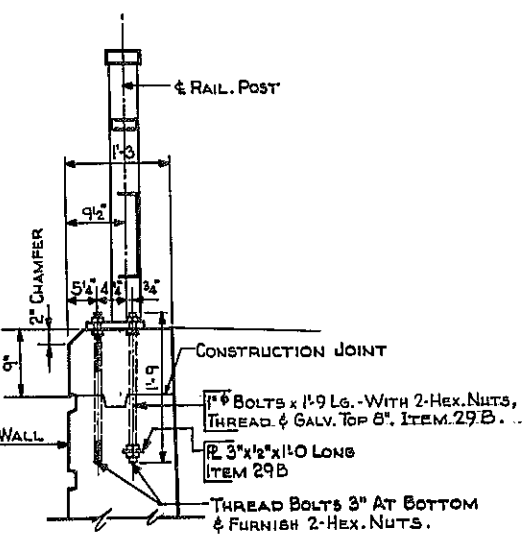
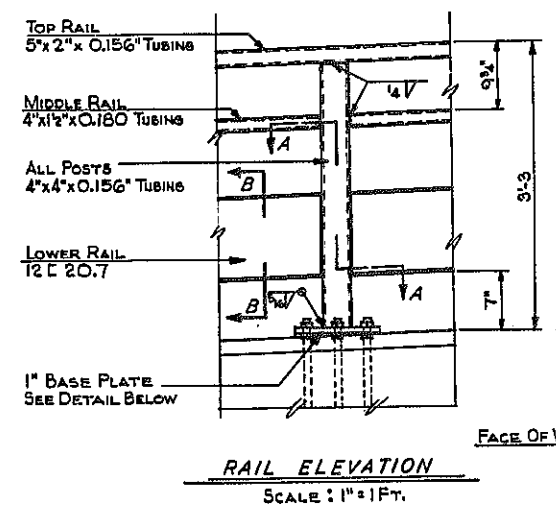
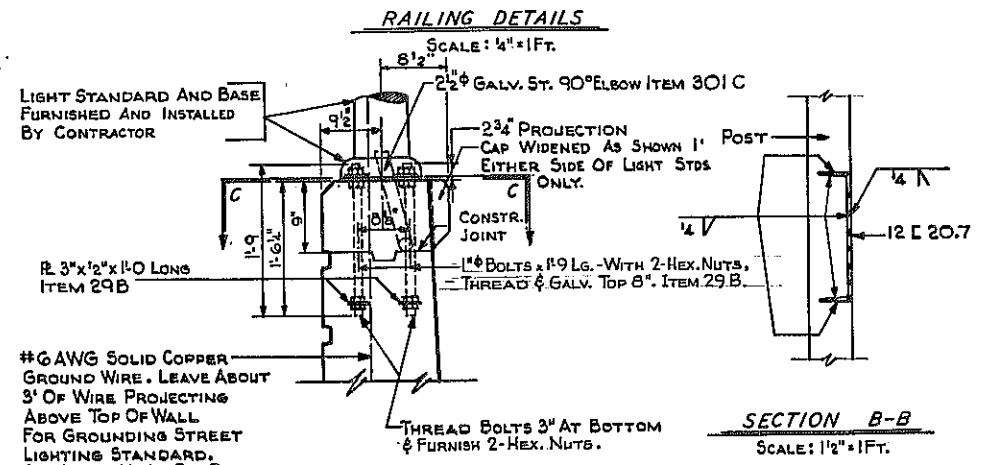
FED. ROAD PROJ. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (29)	170	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCHEDULE FOR RAILING			
UNITS REQUIRED			
WALL NO.	TYPE I	TYPE II	TYPE III
1	22	2	2
2	14	0	0
3	17	3	3

* REFER TO SHEET NO. 3 FOR VARIATION OF RAILING IN WALL NO. 2 PANEL NO. 1



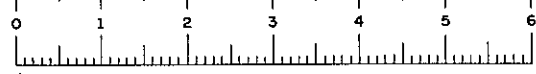
- RAILING NOTES**
1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
 2. ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF FASCIA AND SO THAT THE POSTS ARE TRULY VERTICAL.
 3. ALL SURFACES OF WELDS ON RAILING TO BE GRIND SMOOTH.
 4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
 5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
 6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29 B.
 7. TUBULAR AND CHANNEL RAILS, TUBULAR POSTS AND BASE PLATES WILL BE PAID FOR UNDER ITEM 37 AX.

- CONSTRUCTION PROCEDURE FOR RAILING**
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR WALL TO CONSTRUCTION JOINT.
 2. PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS
 3. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 4. PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON PLATES.
 5. POUR WALL ABOVE CONSTRUCTION JOINT.
 6. AFTER NUTS HAVE BEEN PLACED AND TIGHTENED TO THE SATISFACTION OF THE ENGINEER, BOLTS SHALL BE CUT OFF 1/8" ABOVE NUT AND PEENED.

RAILING DETAILS
RETAINING WALLS 1, 2 & 3

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E.E.K. C.E.
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BAR LIST

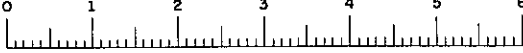
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	171R1	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

RETAINING WALL NO. I

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
PANEL #1 SECTION G-6								
401	4	30'-10"	STR	6				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 5'-9" TO 6'-3"
403	4	29'-8"	STR	30				VERT. BARS (VAR. 4'-6" TO 5'-4", 1/2" INC.)
PANEL #2 SECTION G-8								
401	4	30'-10"	STR	7				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 6'-3" TO 6'-9"
404	4	29'-8"	STR	30				VERT. BARS (VAR. 5'-3" TO 6'-0", 1/2" INC.)
PANEL #3 SECTION G-8								
402	4	29'-8"	STR	7				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR 6'-9" TO 7'-4"
405	4	29'-8"	STR	30				VERT. BARS (VAR. 5'-4" TO 6'-2", 1/2" INC.)
PANEL #4 SECTION G-8								
401	4	30'-10"	STR	8				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 7'-4" TO 7'-10"
406	4	29'-8"	STR	30				VERT. BARS (VAR. 6'-9" TO 7'-4", 1/2" INC.)
PANEL #5 SECTION G-8								
401	4	30'-10"	STR	8				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 7'-4" TO 7'-10"
407	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-4" TO 7'-10", 1/2" INC.)
PANEL #6 SECTION G-8								
402	4	29'-8"	STR	7				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR 7'-4" TO 7'-8"
408	4	29'-8"	STR	30				VERT. BARS (VAR. 6'-10" TO 7'-5", 1/2" INC.)
PANEL #7 SECTION G-8								
401	4	30'-10"	STR	8				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 7'-4" TO 7'-10"
409	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-4" TO 7'-10", 1/2" INC.)
PANEL #8 SECTION G-10								
401	4	30'-10"	STR	8				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 7'-10" TO 8'-2"
410	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-10" TO 7'-10", 1/2" INC.)
PANEL #9 SECTION G-10								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR 8'-3" TO 8'-7"
411	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-10" TO 8'-4", 1/2" INC.)
PANEL #10 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 8'-7" TO 8'-10"
412	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-10", 1/2" INC.)
PANEL #11 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 8'-10" TO 9'-2"
413	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-10", 1/2" INC.)
PANEL #12 SECTION G-10								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR 9'-2" TO 9'-5"
414	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-7" TO 8'-10", 1/2" INC.)
PANEL #13 SECTION G-10								
401	4	30'-10"	STR	10				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 9'-5" TO 9'-8"
415	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-10" TO 9'-4", 1/2" INC.)
PANEL #14 SECTION G-10								
401	4	30'-10"	STR	10				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR 9'-8" TO 9'-11"
416	4	29'-8"	STR	30				VERT. BARS (VAR. 9'-0" TO 9'-4", 1/2" INC.)
PANEL #15 SECTION G-10								
402	4	29'-8"	STR	10				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR 9'-11" TO 10'-3"
417	4	29'-8"	STR	30				VERT. BARS (VAR. 9'-4" TO 9'-7", 1/2" INC.)
PANEL #16 SECTION L-12								
401	4	30'-10"	STR	16				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 7'-11" TO 8'-2"
418	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-11" TO 7'-11", 1/2" INC.)
413	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
602	G	6'-6"	STR	30				VERT. BARS (VAR. 8'-3" TO 8'-6", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #17 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-2" TO 8'-5"
421	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-6" TO 7'-9", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
603	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-0" TO 8'-9", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #18 SECTION L-12								
402	4	29'-8"	STR	18				HORIZ. BARS
402	4	29'-8"	STR	2				HORIZ. CAP BARS 8'-1" TO 8'-4"
422	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-7" TO 8'-0", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
604	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-1" TO 8'-4", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #19 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-3" TO 8'-6"
423	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-10" TO 8'-10", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
605	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-7", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #20 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 9'-0" TO 9'-3"
424	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-3" TO 8'-6", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
606	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-7" TO 8'-10", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #21 SECTION L-12								
402	4	29'-8"	STR	16				HORIZ. BARS
402	4	29'-8"	STR	2				HORIZ. CAP BARS 8'-3" TO 8'-6"
425	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-7" TO 7'-10", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
607	G	6'-7-9"	STR	30				VERT. BARS (VAR. 7'-11" TO 8'-2", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #22 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-6" TO 8'-9"
426	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-10" TO 8'-5", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
608	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-2" TO 8'-9", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #23 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-9" TO 9'-1"
427	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-5" TO 8'-10", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
609	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-7" TO 9'-2", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #24 SECTION L-12								
402	4	29'-8"	STR	18				HORIZ. BARS
402	4	29'-8"	STR	2				HORIZ. CAP BARS 9'-1" TO 9'-4"
428	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-9" TO 9'-4", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
610	G	6'-7-9"	STR	30				VERT. BARS (VAR. 9'-1 1/4" TO 9'-8 1/4", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP

RETAINING WALL NO. I

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
PANEL #17 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-2" TO 8'-5"
421	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-6" TO 7'-9", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
603	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-0" TO 8'-9", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #18 SECTION L-12								
402	4	29'-8"	STR	18				HORIZ. BARS
402	4	29'-8"	STR	2				HORIZ. CAP BARS 8'-1" TO 8'-4"
422	4	29'-8"	STR	30				VERT. BARS (VAR. 7'-7" TO 8'-0", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
604	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-1" TO 8'-4", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #19 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 8'-3" TO 8'-6"
423	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-10" TO 8'-10", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
605	G	6'-7-9"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-7", 1/2" INC.)
701	7	11'-2 1/2"	III	30	8'-0"	1'-6"	1'-8 1/2"	TRANS. FTG. BARS, TOP
PANEL #20 SECTION L-12								
401	4	30'-10"	STR	18				HORIZ. BARS
401	4	30'-10"	STR	2				HORIZ. CAP BARS 9'-0" TO 9'-3"
424	4	29'-8"	STR	30				VERT. BARS (VAR. 8'-3" TO 8'-6", 1/2" INC.)
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.
420	4	7'-1 1/2"	STR	30				TRANS. FTG. BARS, BOT.
601	G	5'-7 1/4"	II	30	1'-5 1/2"	0'-4 1/4"	3'-9"	DOWELS
606	G	6'-7-9"	STR	30	</			



BAR LIST

RETAINING WALL NO. 2

Bar List Revised
Date 1/16 - Change #16

Table with columns: MARK, SIZE, LENGTH, TYPE, NUMBER, A, B, C, DESCRIPTION. Contains 14 panels of reinforcement data for Retaining Wall No. 2.

RETAINING WALL NO. 3

Table with columns: MARK, SIZE, LENGTH, TYPE, NUMBER, A, B, C, DESCRIPTION. Contains 9 panels of reinforcement data for Retaining Wall No. 3.

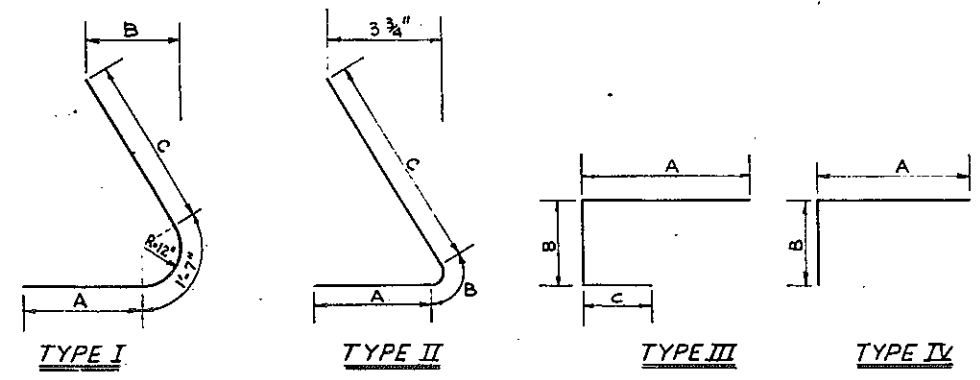
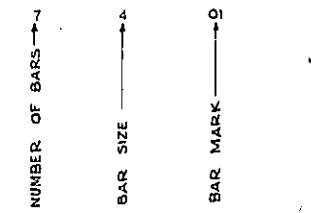


Table with columns: FED. ROAD REG. NO., STATE, FEDERAL AID PROJECT NO., SHEET NO., TOTAL SHEETS. Values: 1, N.Y., U-UG-377(25), 1281, 173.

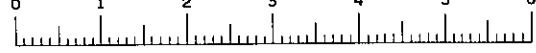
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR REINFORCEMENT LEGEND



REVISION TO BAR LIST

RETAINING WALLS 1,2,&3
BAR LIST
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK
DRAWN: R.J.R.
CHECKED: TRACY



FED. ROAD RES. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	173R1	173

KENSINGTON EXPRESSWAY, SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR LIST

RETAINING WALL NO. 3									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #10 SECTION T-18									
456	4	7'-7"	STR	30				TRANS. FTG. BARS, BOT.	
402	4	29'-8"	STR	20				LONG. FTG. BARS, TOP & BOT.	
464	4	15'-0"	STR	30				VERT. BARS (VAR. 14'-0" TO 16'-0", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	30				HORIZ. BARS	
604	6	15'-0"	STR	30				VERT. BARS (VAR. 15'-11 1/2" TO 16'-4", 1/2" INC.)	
701	7	8'-0"	STR	30				VERT. BARS	
702	7	7'-7"	STR	30				TRANS. FTG. BARS, TOP	
703	T	9'-1"	I	60	4'-0"	0'-3 1/2"	5'-6"	DOWELS	
PANEL #11 SECTION T-20									
457	4	8'-6"	STR	30				TRANS. FTG. BARS, BOT.	
402	4	29'-8"	STR	21				LONG. FTG. BARS, TOP & BOT.	
465	4	15'-0"	STR	30				VERT. BARS (VAR. 16'-0" TO 17'-3", 1/2" INC.)	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
402	4	29'-8"	STR	34 3/2				HORIZ. BARS	
605	6	15'-0"	STR	30				VERT. BARS (VAR. 16'-0" TO 17'-7", 1/2" INC.)	
801	8	9'-0"	STR	30				VERT. BARS	
802	8	8'-6"	STR	30				TRANS. FTG. BARS, TOP	
803	8	9'-7"	I	60	4'-5"	0'-3 1/2"	3'-9"	DOWELS	
PANEL #12 SECTION T-22									
458	4	9'-4"	STR	30				TRANS. FTG. BARS, BOT.	
402	4	29'-8"	STR	23				LONG. FTG. BARS, TOP & BOT.	
466	4	15'-0"	STR	30				VERT. BARS (VAR. 17'-1 1/2" TO 18'-4", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	34				HORIZ. BARS	
704	7	16'-7"	STR	30				VERT. BARS (VAR. 17'-5 1/2" TO 18'-8", 1/2" INC.)	
901	9	10'-0"	STR	30				VERT. BARS	
902	9	9'-4"	STR	30				TRANS. FTG. BARS, TOP	
903	9	10'-9"	I	60	4'-8"	0'-4 1/2"	4'-6"	DOWELS	
PANEL #13 SECTION T-22									
458	4	9'-4"	STR	30				TRANS. FTG. BARS, BOT.	
402	4	29'-8"	STR	23				LONG. FTG. BARS, TOP & BOT.	
467	4	15'-0"	STR	30				VERT. BARS (VAR. 18'-5" TO 19'-7 1/2", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	38				HORIZ. BARS	
705	7	16'-7"	STR	30				VERT. BARS (VAR. 18'-9" TO 19'-11 1/2", 1/2" INC.)	
901	9	10'-0"	STR	30				VERT. BARS	
902	9	9'-4"	STR	30				TRANS. FTG. BARS, TOP	
903	9	10'-9"	I	60	4'-8"	0'-4 1/2"	4'-6"	DOWELS	
PANEL #14 SECTION T-24									
459	4	10'-3"	STR	30				TRANS. FTG. BARS, BOT.	
402	4	29'-8"	STR	25				LONG. FTG. BARS, TOP & BOT.	
468	4	15'-0"	STR	30				VERT. BARS (VAR. 19'-6" TO 20'-9", 1/2" INC.)	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
402	4	29'-8"	STR	40				HORIZ. BARS	
606	6	15'-0"	STR	30				VERT. BARS (VAR. 19'-10" TO 21'-1", 1/2" INC.)	
1003	10	11'-0"	STR	30				VERT. BARS	
1006	10	10'-3"	STR	30				TRANS. FTG. BARS, TOP	
1007	10	11'-9"	I	60	5'-2"	0'-5"	5'-0"	DOWELS	
PANEL #15 SECTION T-26									
503	5	11'-2"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	27				LONG. FTG. BARS, TOP & BOT.	
511	5	15'-0"	STR	30				VERT. BARS (VAR. 20'-4" TO 21'-7", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	44 4/2				HORIZ. BARS	
1001	10	11'-6"	STR	15				VERT. BARS	
1002	10	7'-0"	STR	30				VERT. BARS	
1008	10	12'-10"	STR	15				VERT. BARS (VAR. 20'-4" TO 21'-7", 1/2" INC.)	
104	10	12'-10"	I	60	5'-9"	0'-5 1/2"	5'-6"	DOWELS	
105	10	11'-2"	STR	30				TRANS. FTG. BARS, TOP	

Top of wall raised .5'

RETAINING WALL NO. 3									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #16 SECTION T-26									
503	5	11'-2"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	27				LONG. FTG. BARS, TOP & BOT.	
512	5	15'-0"	STR	30				VERT. BARS (VAR. 21'-7" TO 23'-0", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	42				HORIZ. BARS	
1001	10	11'-6"	STR	15				VERT. BARS	
1002	10	7'-0"	STR	30				VERT. BARS	
1009	10	12'-10"	STR	15				VERT. BARS (VAR. 21'-11" TO 23'-0", 1/2" INC.)	
1004	10	12'-10"	I	60	5'-9"	0'-5 1/2"	5'-6"	DOWELS	
1005	10	11'-2"	STR	30				TRANS. FTG. BARS, TOP	
PANEL #17 SECTION T-28									
513	5	11'-10"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	29				LONG. FTG. BARS, TOP & BOT.	
514	5	15'-0"	STR	30				VERT. BARS (VAR. 22'-11" TO 24'-4", 1/2" INC.)	
502	5	29'-8"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
502	5	29'-8"	STR	46				HORIZ. BARS	
1103	11	12'-6"	STR	15				VERT. BARS	
1105	11	8'-0"	STR	30				VERT. BARS	
1106	11	13'-10"	STR	15				VERT. BARS (VAR. 23'-3" TO 24'-5", 1/2" INC.)	
1107	11	13'-11"	I	60	6'-1"	0'-6"	6'-3"	DOWELS	
1108	11	11'-0"	STR	30				TRANS. FTG. BARS, TOP	
PANEL #18 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT.	
516	5	15'-0"	STR	30				VERT. BARS (VAR. 23'-7" TO 24'-7", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	46 4/2				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1109	11	13'-10"	STR	15				VERT. BARS (VAR. 23'-0" TO 24'-0", 1/2" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-5"	DOWELS	
PANEL #19 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT.	
517	5	15'-0"	STR	30				VERT. BARS (VAR. 24'-6" TO 25'-8", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	46 5/2				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1110	11	13'-10"	STR	15				VERT. BARS (VAR. 24'-10" TO 25'-10", 1" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-5"	DOWELS	
PANEL #20 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT.	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT.	
518	5	15'-0"	STR	30				VERT. BARS (VAR. 25'-3" TO 26'-6", 1/2" INC.)	
502	5	29'-8"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
502	5	29'-8"	STR	48 5/2				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1111	11	13'-10"	STR	15				VERT. BARS (VAR. 25'-11" TO 26'-11", 1/2" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-5"	DOWELS	

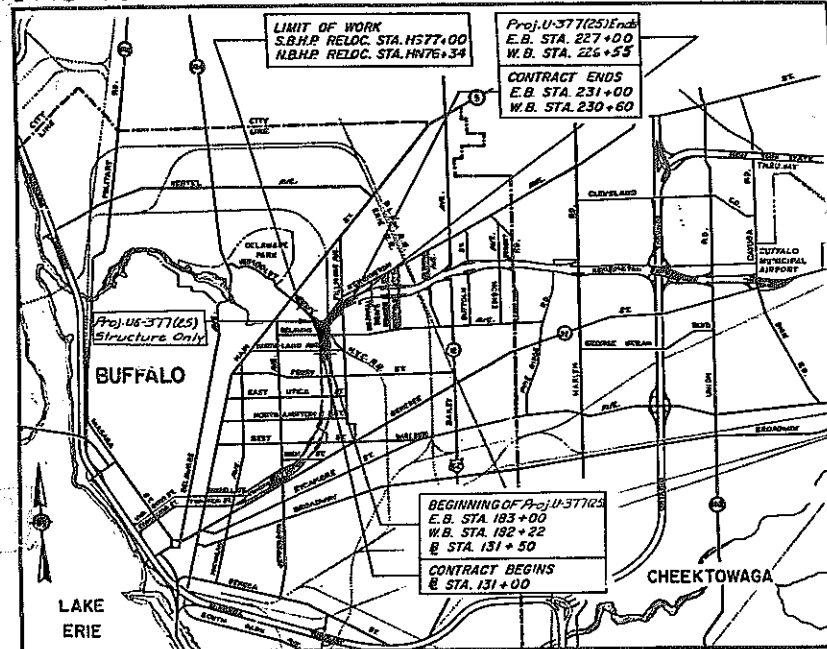
REVISION TO BAR LIST 17

RETAINING WALLS 1,2,&3
BAR LIST

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III.

DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	D.E.L. R.D.O.
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34 SHEET NO. 15



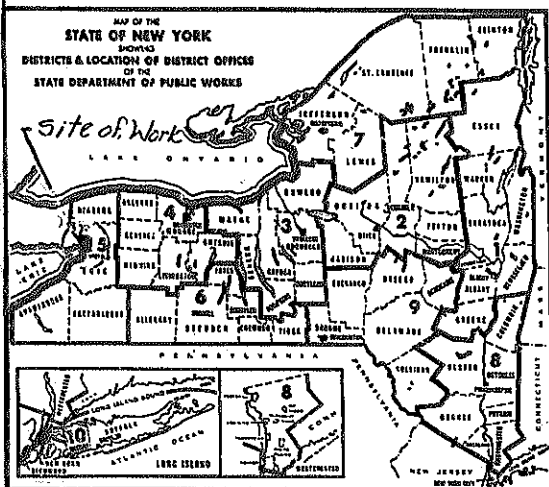
LOCATION MAP
1 MILE

Approx. Location: On Kensington Expressway Arterial Highway in the City of Buffalo between Butler Ave. and Fillmore Ave.

DESIGN TRAFFIC DATA

Design Class U
Estimated 1985 Traffic D.H. 2300
A.A.D.T. 33000

- MALL WIDTH - 10.5' TO 33.0'
- DESIGN SPEED INTERCHANGE AREA - 50 MPH
- DESIGN SPEED EXPRESSWAY - 60 MPH
- MAXIMUM EXPRESSWAY GRADE - 2.26 %
- MINIMUM EXPRESSWAY GRADE - 0.70 %
- MAXIMUM EXPRESSWAY CURVATURE - 5°-00'
- MINIMUM EXPRESSWAY S.S.D. - 475'



**STATE OF NEW YORK
DEPARTMENT OF PUBLIC WORKS
DIVISION OF CONSTRUCTION**

**PLANS FOR CONSTRUCTING WITH FEDERAL AID, THE
CITY OF BUFFALO: KENSINGTON EXPRESSWAY ARTERIAL HIGHWAY
SECTIONS 2 AND 3
(Northland Avenue to Fillmore Avenue)
CONTRACT No. 3**

From Station 131+00 E.L. to Station W.B. 230+60, a length of 0.92 mile (Plus 3.58 miles of Access) in the City of Buffalo

173 SHEETS

F.A. PROJECT No. U-UG-377 (25)

CONTRACT No. F.A.C. 63-2

ERIE COUNTY

TYPE OF CONSTRUCTION		
Asphalt Concrete, Type 2A		0.83 Mile
Asphalt Concrete, Type 2A (Access)		3.58 Miles
Miscellaneous Work		0.09 Mile
Including		
H.G.S., Delavan Avenue over Expressway, 3 Span, Comp. W. F. Beam, 1 @ 69' 6", 1 @ 63' 0" and 1 @ 53' 6" c. to c. bearings		
H.G.S., E.S. Ramp HA over Expressway, 3 Span, Comp. W. F. Beam, 1 @ 107' 12", 1 @ 83.53' & 1 @ 58.20' c. to c. bearings		
H.G.S., N.B. Humboldt Parkway over Expressway, 6 Span, Comp. W. F. Beam, 1 @ 93.35', 1 @ 37.12', 1 @ 63.02', 1 @ 110.85', 1 @ 94.48' & 1 @ 52.01' c. to c. bearings		
H.G.S., Fillmore Avenue over Expressway, 2 Span, Comp. W. F. Beam, 2 @ 89' 6" Each c. to c. brgs.		
R.R.G.S., New York Central Railroad over Expressway, 2 Span through Girder, 1 @ 117' 6" & 1 @ 127' 0" c. to c. bearings		

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	1	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

Pedestrian Overpass E.B. Station 191+09, 6 Span, W. F. Beam and Slab Construction, 2 @ 30'-0", 2 @ 31' 3", 1 @ 101' 0" and 1 @ 102' 0" c. to c. bearings

STANDARD STRUCTURE SHEETS

- 50-34, 51-21, 52-2C, 52-2W, 54-9, 54-18P, 55-51, 56-1, 56-52, 58-1, 58-60, 59-11WR1, 59-48A, 59-48B, 59-40D1, 59-48D2, 60-19A, 60-41, 60-48E, 60-48FR1, 60-53, 61-9HB, 61-11, 61-11XR1, 61-19, 61-45, 61-45A, 61-48C, 61-48G, 61-48H, 61-61R1, 61-62C, 61-106, 61-107, 61-109A, 61-109B, 62-4, 62-6, 62-7, 62-11C, 62-17, 62-43A, 62-43B, 62-45B, 62-48Ca, 62-48J, 62-48K, 62-49

All work contemplated under this contract to be covered by and in conformity with the specifications adopted January 2, 1957, except as modified on these plans and in the Itemized Proposal.

PREPARED AND RECOMMENDED BY:
DE LEUW, CATHER AND BRILL, ENGINEERS-ARCHITECTS

C. A. Brill DATE 7-22-1967
NEW YORK STATE PROFESSIONAL ENGINEER LICENSE No. 38438

NEW YORK STATE
DEPARTMENT OF PUBLIC WORKS

APPROVED:
N. W. Krapp DATE 7/5/67
N. W. KRAPP, DISTRICT ENGINEER, DISTRICT No. 6

John Raymond DATE 7/12/67
JOHN J. RAYMOND, Asst. Deputy Chief Engineer

V. J. Busone DATE 12/12/67
V. J. BUSONE, ASST. DEPUTY CHIEF ENGINEER

R. W. Sweet DATE 12/19/67
R. W. SWEET, CHIEF ENGINEER

CITY OF BUFFALO

APPROVED:
Chester A. Kowal DATE 1/1/68
CHESTER A. KOWAL, MAYOR, CITY OF BUFFALO

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS

APPROVED: _____ DATE _____
DIVISION ENGINEER

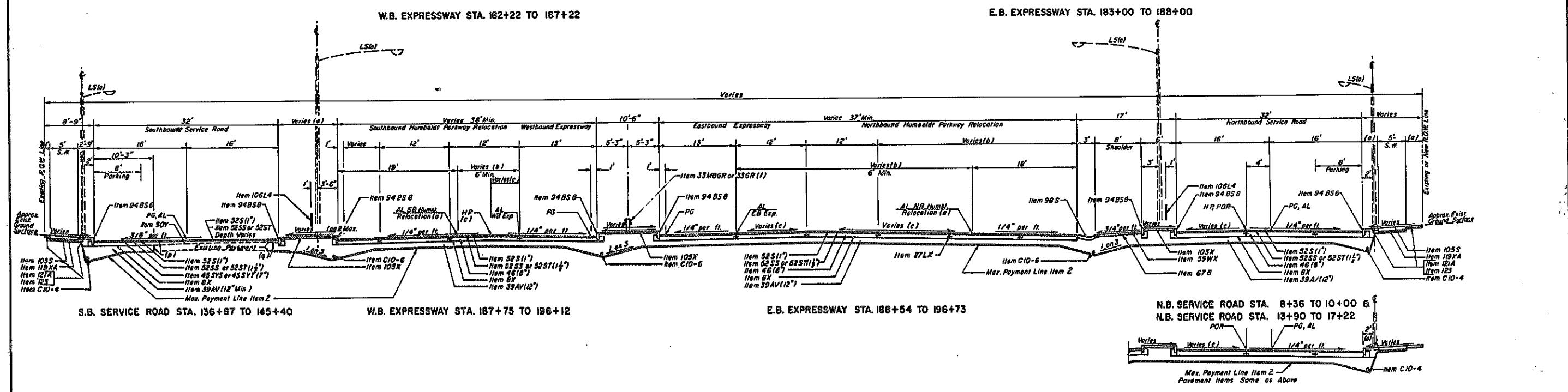
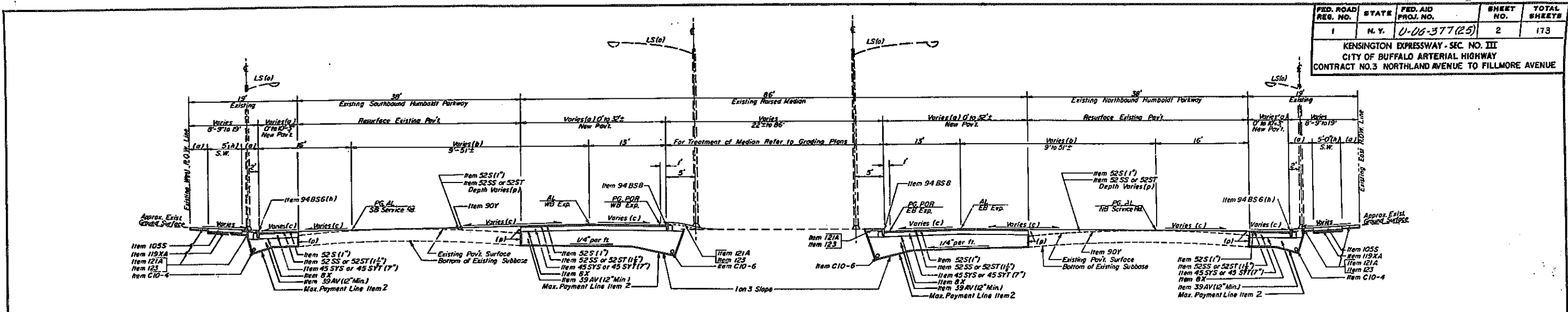
TITLE SHEET

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEUW, CATHER & BRILL, ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	S.C.P. B.L.B. D.L.B.
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PART 1 TO 80

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	2	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE				



DESCRIPTION OF ITEMS

Item 2	Unclassified Excavation	Item 94AS	Stone Curb (12") Mountable
Item 7A	Trimming Road Section	Item 94DS	Stone Curb (6") Sloped
Item 8X	Preparing Fine Grade	Item 94SS6	Stone Curb (6") Vertical
Item C10-4	Force Concrete Pipe Underdrain - 4"	Item 94SS8	Stone Curb (8") Vertical
Item C10-6	Rough Concrete Pipe Underdrain - 6"	Item 94SS10	Stone Curb (10") Vertical
Item 27LX	Longitudinal Joint Supports - Optimal	Item 98B	Resetting Old Curb
Item 32NS	Resetting Guide Railing	Item 98S	Concrete Outer
Item 33OR	Galvanized Beam Type Guide Railing (GL Posts)	Item 105B	Concrete Sidewalk (6")
Item 33ORR	Galvanized Beam Type Rail Barrier (SR Posts)	Item 105X	Concrete Sidewalk (8")
Item 37AX	Metal Railing	Item 106L4	Chain Link Fence (4")
Item 39AV	Foundation Course Granular Material Type A	Item 106L6	Chain Link Fence (6")
Item 45SS	Bottom Course Asphalt Concrete Type 1A (Slag Aggregate)	Item 119XA	Gravel, Slag or Stone Fill (3")
Item 45ST	Bottom Course Asphalt Concrete Type 1A (Stone Aggregate)	Item 121A	Topsoil Fertilized and Placed (4")
Item 46	Concrete Foundation for Pavement (8")	Item 123	Sodding
Item 52S	Asphalt Concrete - Type 2A Top Course	Item 124	Seedling (1 1/2")
Item 52NS	Asphalt Concrete - Type 2A Binder Course - Slag Aggregate	Item 155	Broken Rock Trench, Drilling and Blasting
Item 52ST	Asphalt Concrete - Type 2A Binder Course - Stone Aggregate	Item 200	Color Contrast for Pavement
Item 59X	Bituminous Stabilized Course (Including Shoulders)	Item 601	Temporary Wire Fence 5' High
Item 59C	Stabilized Gravel Surface Course, Calcium Chloride Treatment (incl. Shoulders)	Item 16A	Furnishing and Applying Calcium Chloride
Item 67B	Medium Curing Liquid Asphalt (1/2 Gal per S.Y.)		
Item 70B	Bituminous Material - Grade C		
Item 90Y	Cleaning Existing Pavement		

REFERENCES

- (a) For details refer to Alignment and Paving Plans.
- (b) For details refer to Alignment Data, Sheet No. 20.
- (c) For details refer to Profiles and Grading Plans.
- (d) For details refer to Grading Plans.
- (e) For details and limits of underdrain refer to Table, Sheet No. 16.
- (f) For limits of Guide Railing and Mail Box refer to Alignment and Paving Plans, Detail on Sheet No. 15 and Table on Sheet No. 13.
- (g) For limits of Median Paving refer to Alignment and Paving Plans.
- (h) For limits of New Sidewalk and Curb refer to Alignment and Paving Plans.
- (i) Height of curb varies as shown on Alignment and Paving Plans.
- (j) For location of Fencing, refer to Grading and Fencing Plans.
- (k) For limits of Color Contrast for Pavement refer to Alignment and Paving Plans. (Item 261).
- (l) For locations refer to Detour Sheets.
- (m) For location and details of Electrical Distribution refer to Sheets 53 through 59.
- (n) For details refer to Sheet No. 16.
- (o) For details refer to Sheet No. 14.

- GENERAL NOTES**
- Limiting Stations designating Typical Sections are only approximate. Alignment and Paving Plans, Drainage and Utility Plans, and Grading and Fencing Plans shall be the final guide for all Roadway Sections. All Typical Sections are drawn looking North or East unless otherwise noted.
 - For Curb, Outer and Pavement Details refer to Sheet Nos. 14 and 15.
 - Reusable stone curbs affected by the new construction shall be reset under Item 96S as ordered by the Engineer.
 - For variations in the grading of side slopes and the location of Final Rock Lines refer to Grading and Fencing Plans.
 - For details of roadside development refer to Sheet No. 17.
 - Gravel surfaced areas not otherwise treated shall be fertilized and seeded (in accordance with Item 123) above the Maximum Payment Line for Item 2.
 - Any roadway excavation below the Maximum Payment Line for Item 39AV shall be backfilled with Item 39AV at the Contractor's expense.
 - The Maximum Payment Line for Item 39AV, in rock, will be the line shown 3" above the Maximum Payment Line for Item 2.
 - Concrete foundation for Pavement (Item 46) may be placed to a maximum width of 25' at one time at locations where this would not interfere with maintenance of traffic. Refer to standard Sheet 62-48 X and Special Notes in Proposal.
 - Locate and outlet pipe underdrain and rock trench underdrain to drainage structures as directed in table of underdrain on Sheet No. 18.
 - Where Item 39AV acts as backfill around pipe, including pipe underdrain, all material to be placed within 6" of pipe shall pass a 2" sieve.

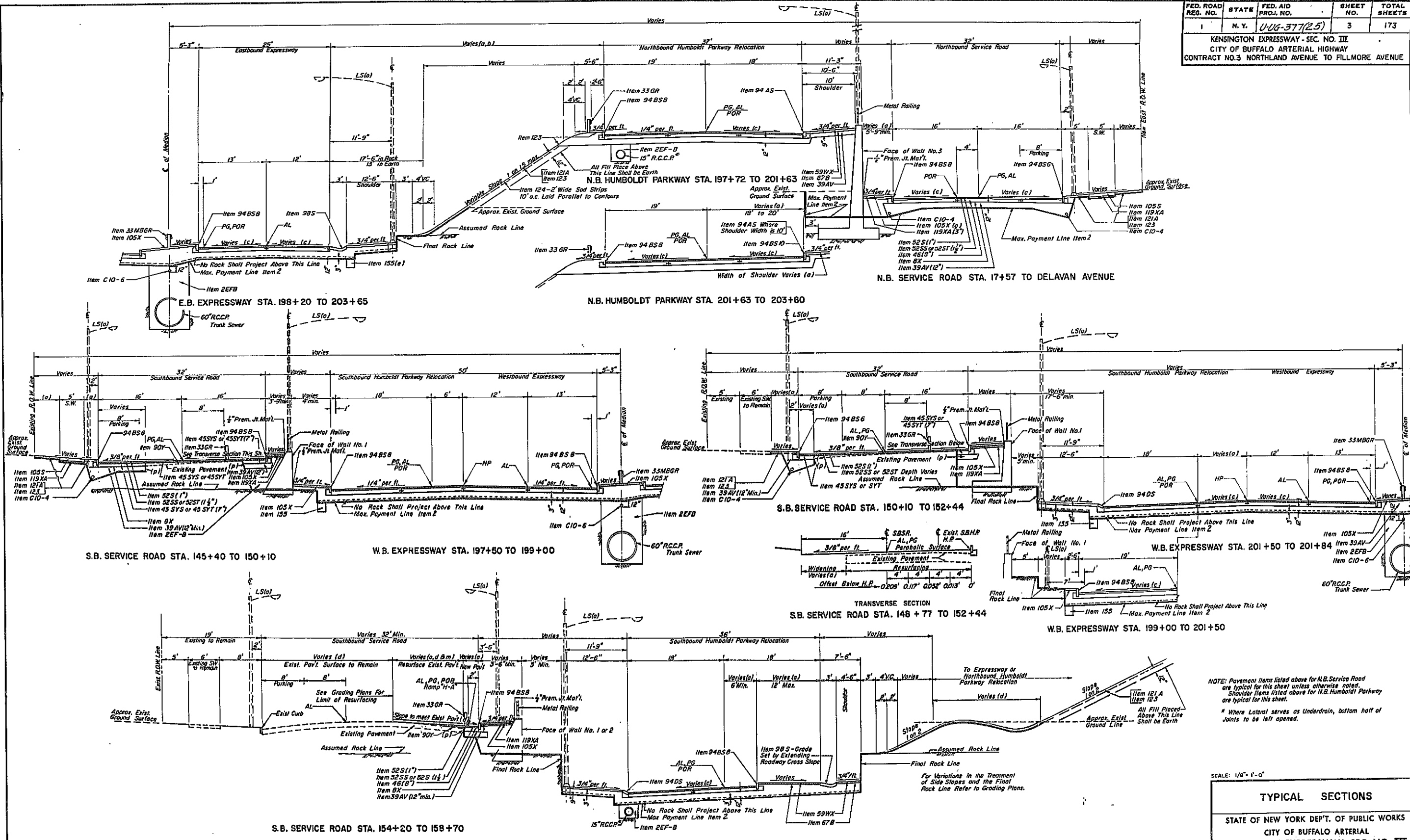
LEGEND

- AL - Alignment Line
- PG - Profile Grade Line
- POR - Point of Rotation
- HP - High Point
- SE - Sidewalk
- VC - Vertical Curve
- LS - Light Standard
- EB - Eastbound
- WB - Westbound
- NB - Northbound
- SB - Southbound

TYPICAL SECTIONS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL HIGHWAY		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	J.R.H.
NEW YORK	TRACED	G.H.E.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	UUG-377(25)	3	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



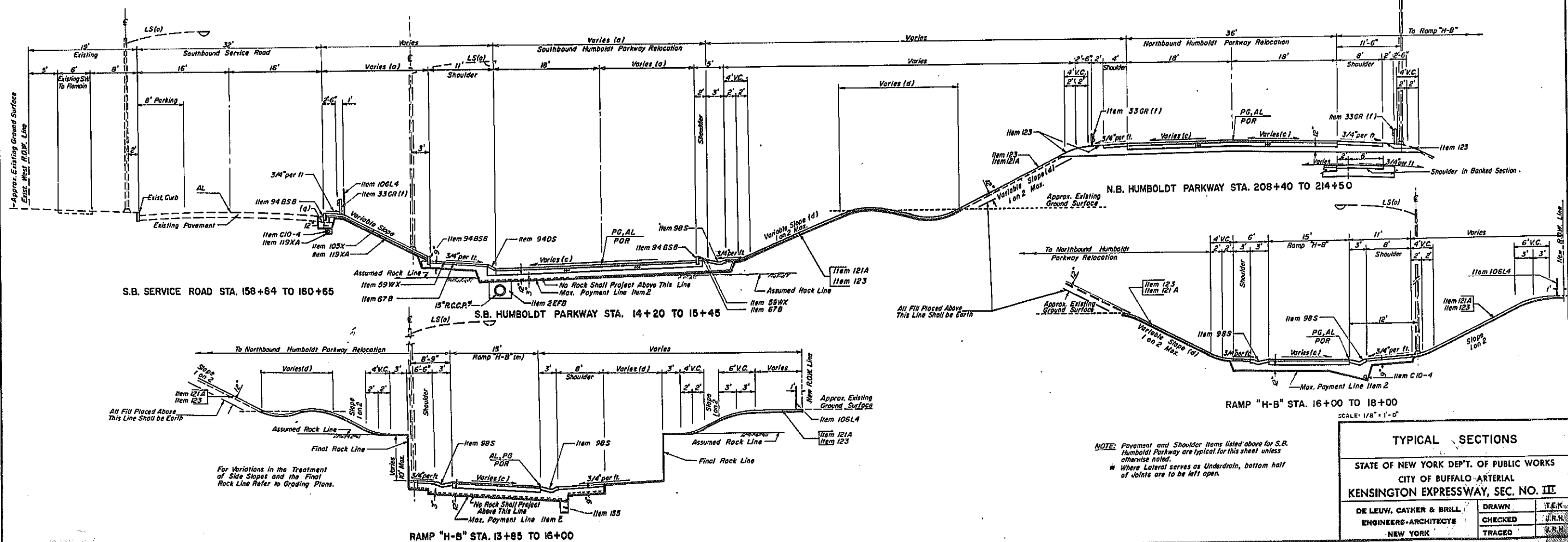
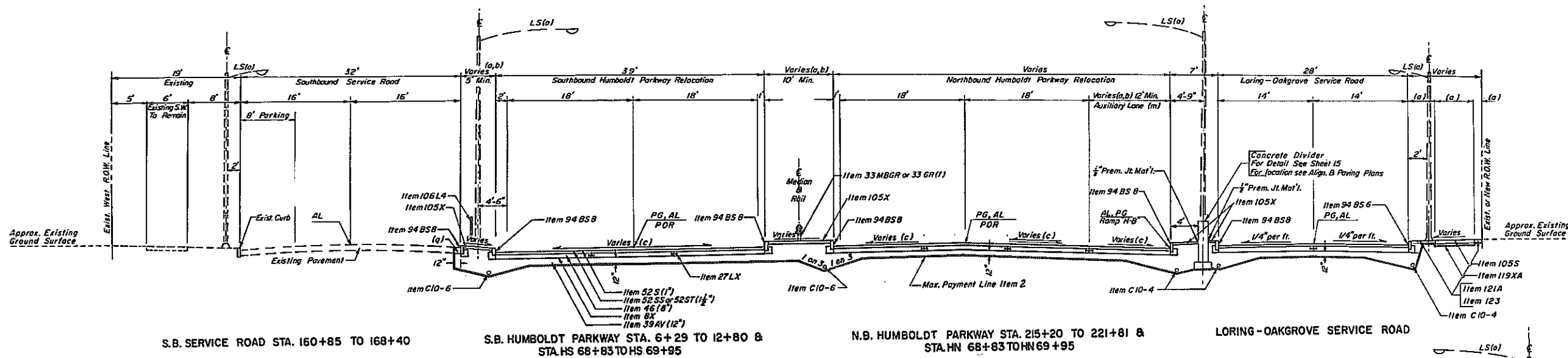
NOTE: Pavement items listed above for N.B. Service Road are typical for this sheet unless otherwise noted. Shoulder items listed above for N.B. Humboldt Parkway are typical for this sheet. * Where Latent serves as Underdrain, bottom half of Joints to be left opened.

SCALE: 1/8" = 1'-0"

TYPICAL SECTIONS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL HIGHWAY		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.R.H. T.E.K. C.L.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	4	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



For Variations in the Treatment of Side Slopes and the Final Rock Line Refer to Grading Plans.

NOTE: Pavement and Shoulder Items listed above for S.B. Humboldt Parkway are typical for this sheet unless otherwise noted.
Where Lateral serves as Underdrain, bottom half of joints are to be left open.

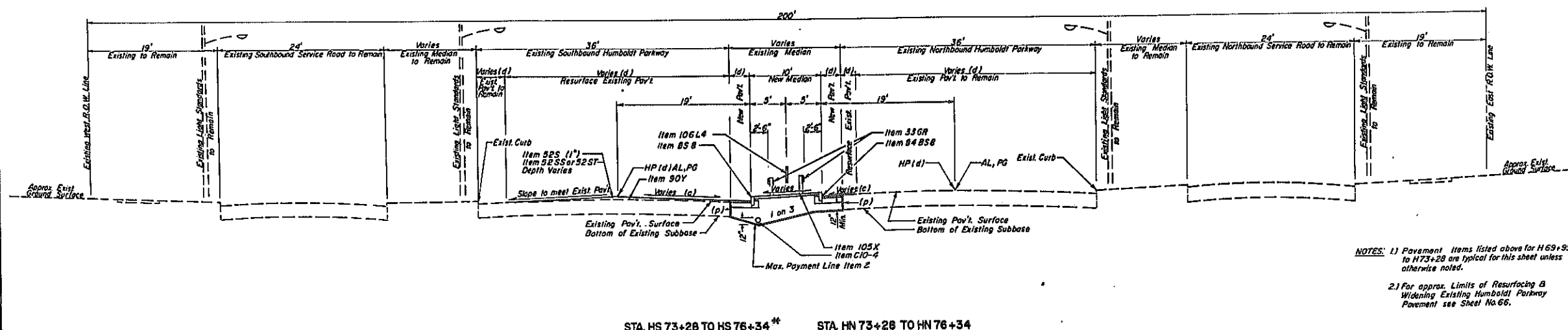
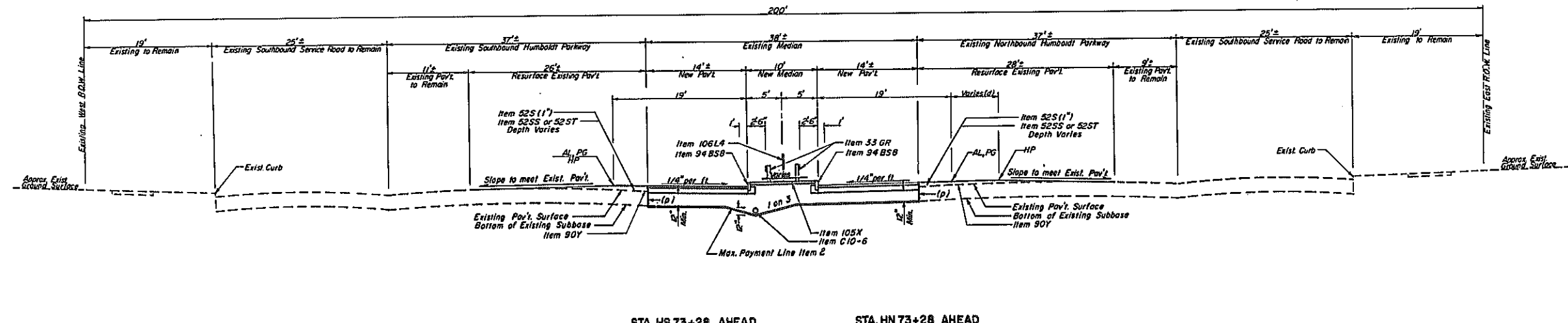
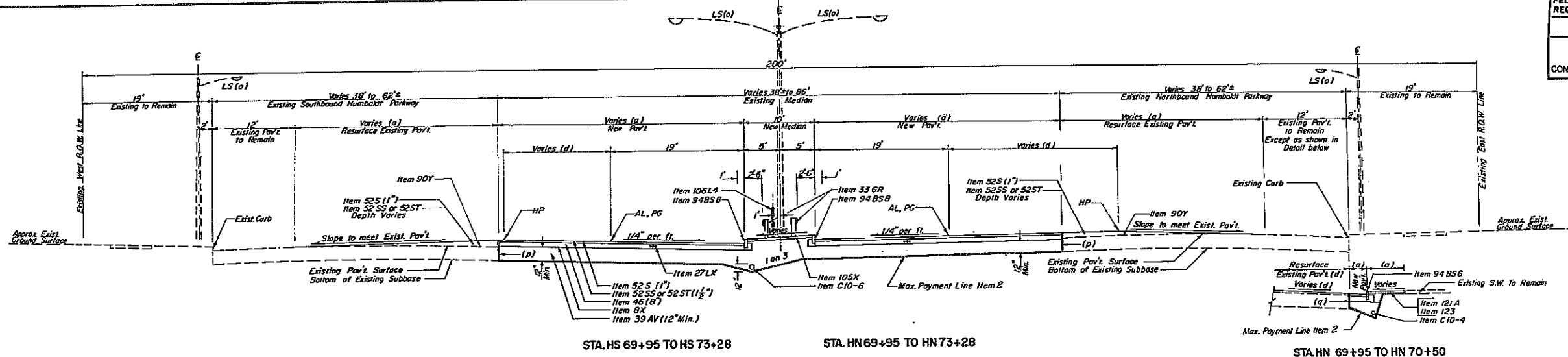
TYPICAL SECTIONS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	J.R.H.
NEW YORK	TRACED	J.R.H.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	5	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



* Clean & Resurface Exist. Pav't between Sta. HS 76+34 & HS 77+00 with Item 52S within the Limits shown on Sheet No. 66. Existing Curb to Remain.

NOTES: 1) Pavement Items listed above for H69+95 to H73+28 are typical for this sheet unless otherwise noted.
 2) For approx. Limits of Resurfacing & Widening Existing Humboldt Parkway Pavement see Sheet No. 66.

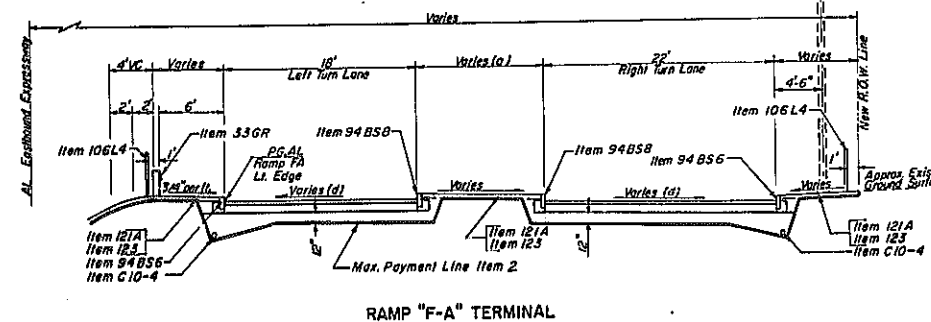
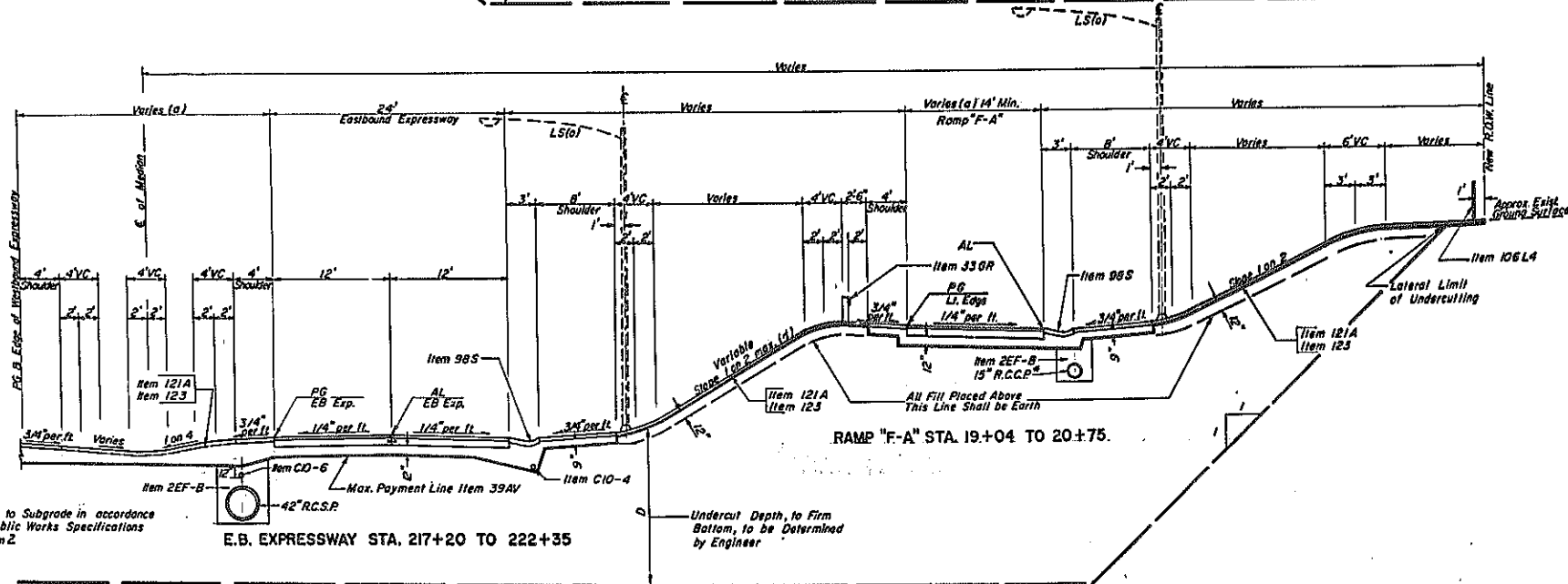
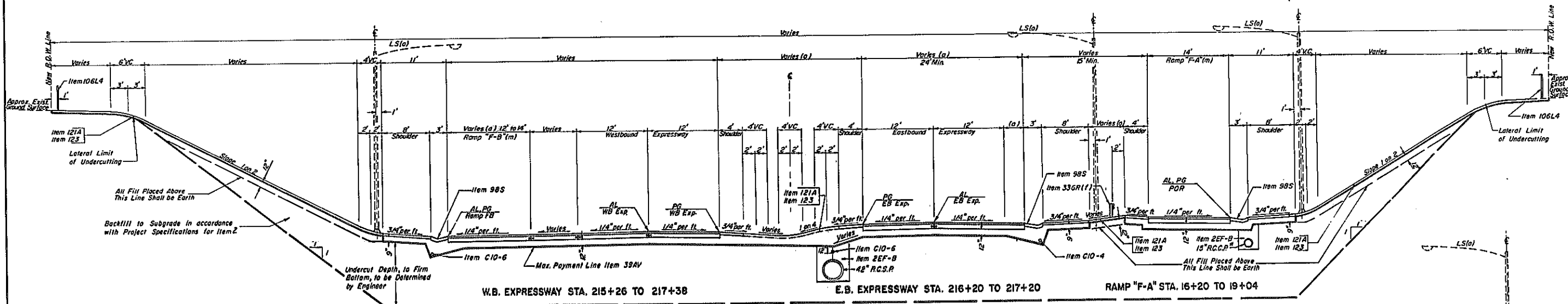
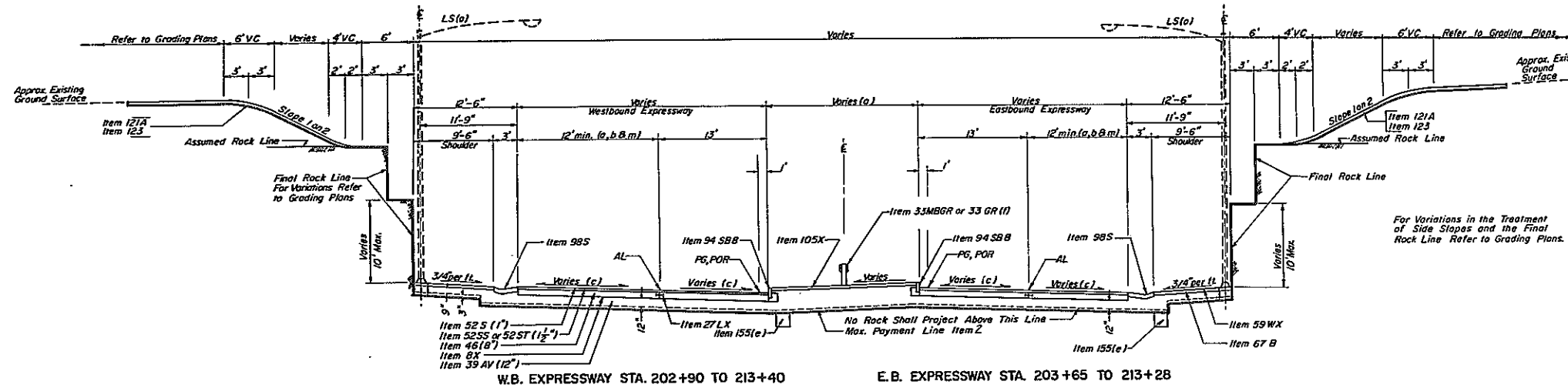
SCALE: 1/8" = 1'-0"

TYPICAL SECTIONS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN J.R.H. CHECKED T.E.K. TRACKED J.R.H.

16

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377 (25)	6	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO 3 NORTHLAND AVENUE TO FILLMORE AVENUE



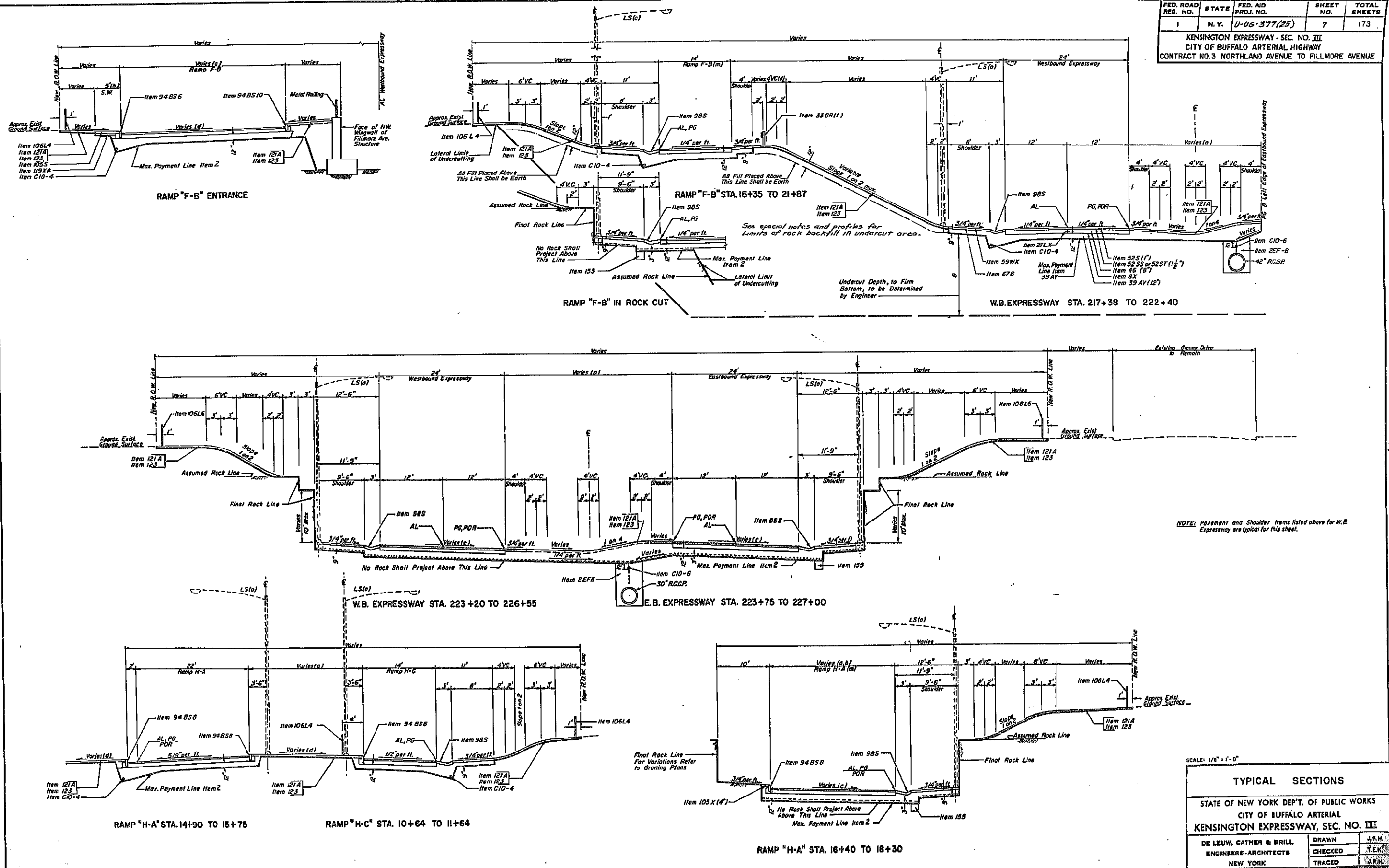
NOTE: Pavement and Shoulder Items listed for top section are typical for this sheet.
* Where Lateral serves as Underdrain, bottom half of Joints are to be left opened.
See special notes of profile for limits of rock backfill in undercut areas.

SCALE: 1/8" = 1'-0"

TYPICAL SECTIONS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHER & BRILL	DRAWN	J.R.H.	
ENGINEERS-ARCHITECTS	CHECKED	T.E.K.	
NEW YORK	TRACED	J.R.H.	

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	7	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



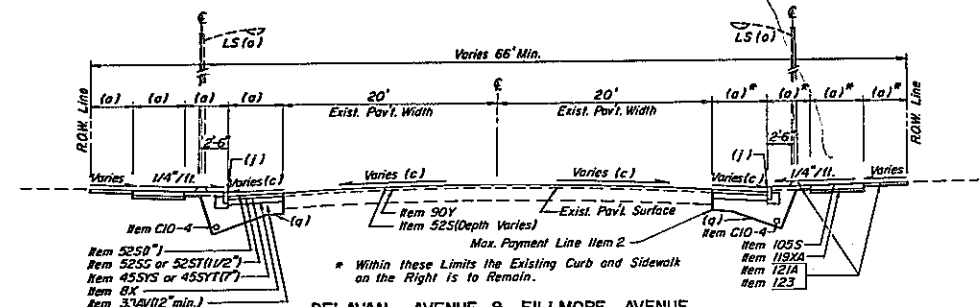
NOTE: Pavement and Shoulder Items listed above for W.B. Expressway are typical for this sheet.

SCALE: 1/8" = 1'-0"

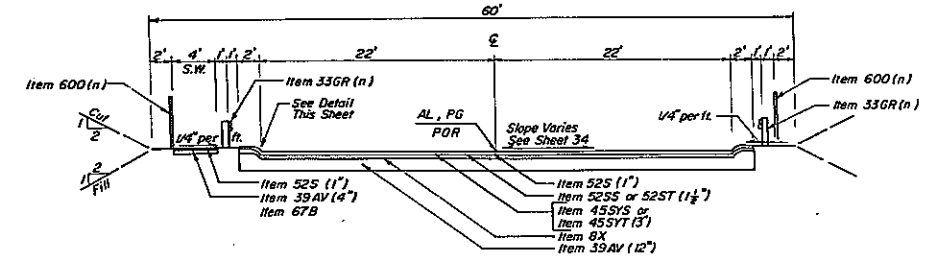
TYPICAL SECTIONS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.R.H. Y.E.N. J.R.H.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	8	173

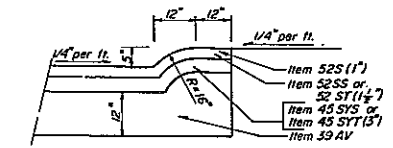
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CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE



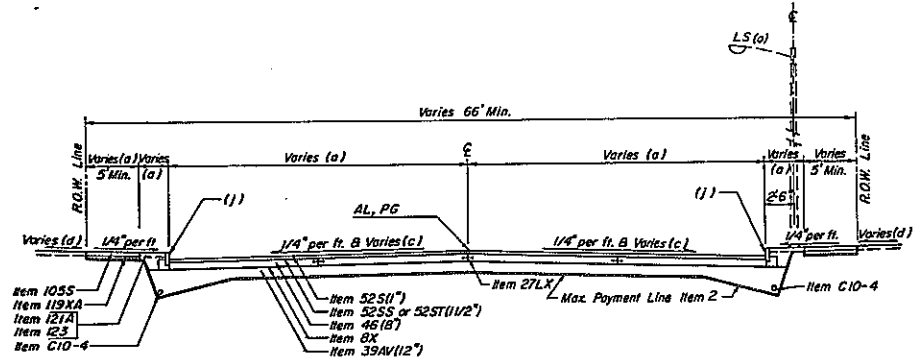
DELAVAN AVENUE & FILLMORE AVENUE
 D 7 + 37* TO D 8 + 00*
 D 11 + 50 TO D 12 + 90
 FI 7 + 75 TO FI 9 + 00
 FI 12 + 00 TO FI 15 + 00



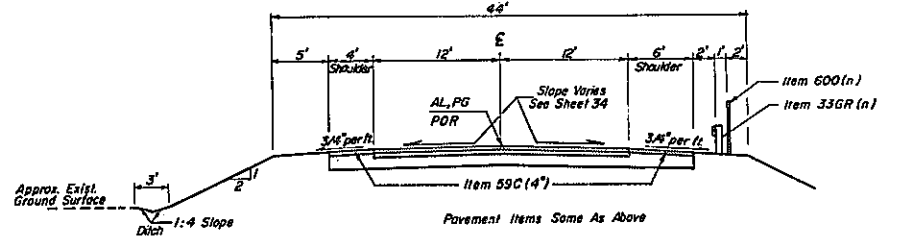
DELAVAN AVENUE DETOUR
(Looking East)



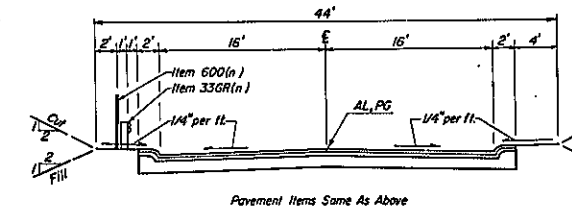
ASPH. CONC. CURB FOR DETOURS
Scale: 1/2" = 1'-0"



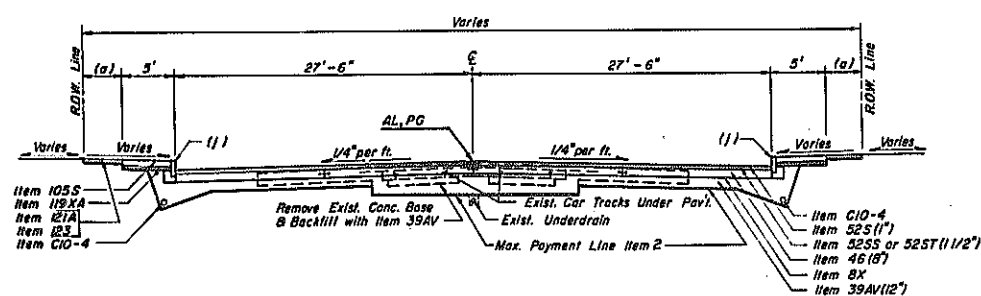
DELAVAN AVENUE
D 8 + 00 TO S.B. Humboldt Pkwy. Service Road
D 10 + 65 TO D 11 + 50



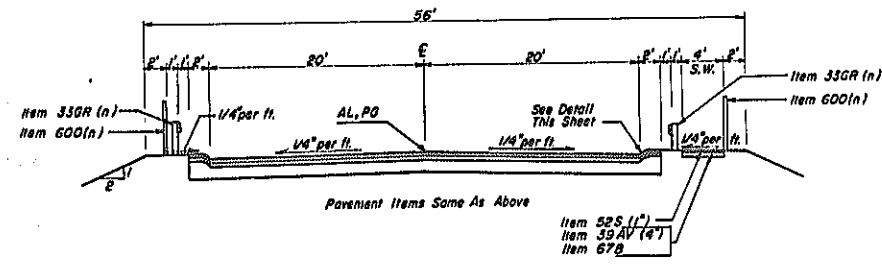
NORTHBOUND HUMBOLDT DETOUR
Delavan Ave. Detour to Loring-Oakgrove Service Rd.
(Looking North)



NORTHBOUND HUMBOLDT DETOUR
Delavan Ave. to Delavan Ave. Detour
(Looking North)



FILLMORE AVENUE
FI 9 + 00 TO FI 9 + 44
FI 11 + 35 TO FI 12 + 00



FILLMORE AVENUE DETOUR
(Looking North)

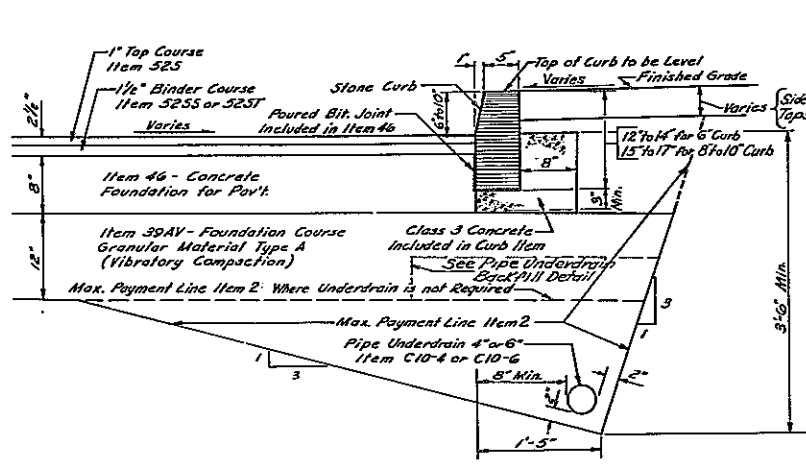
NOTE: Guide Railing (Item 33GR) will be recovered from the detours and reset as directed by the Engineer.

SCALE: 1/8" = 1'-0"

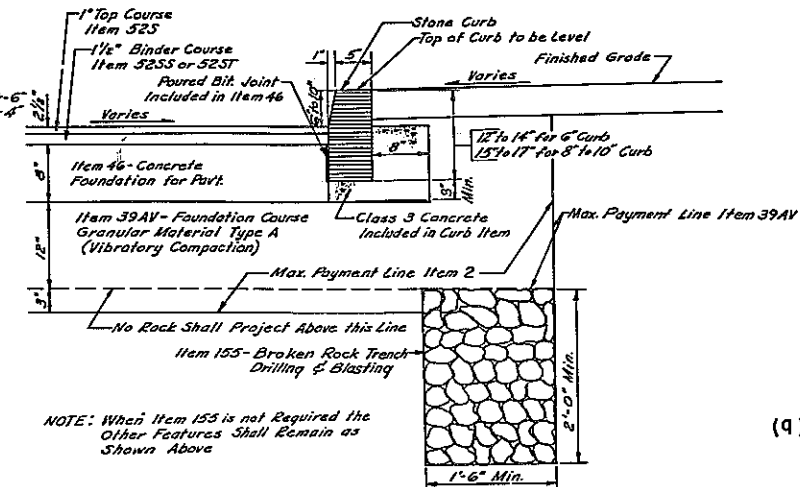
TYPICAL SECTIONS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.R.H. S.E.K. J.R.H.	

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	14	173

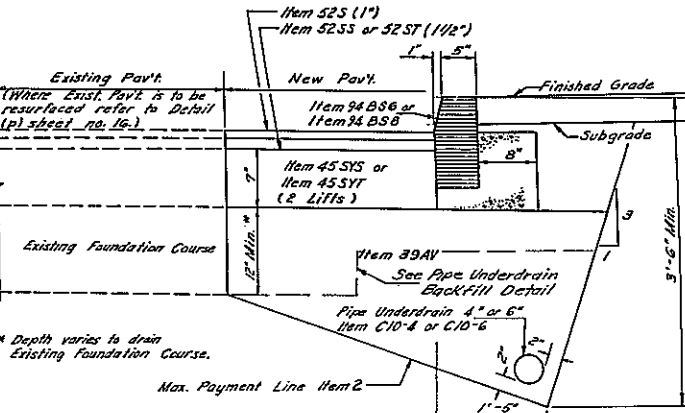
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



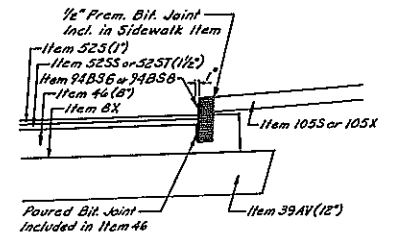
VERTICAL STONE CURB IN EARTH
ITEM 94 BS6, 94 BS8 or 94 BS10



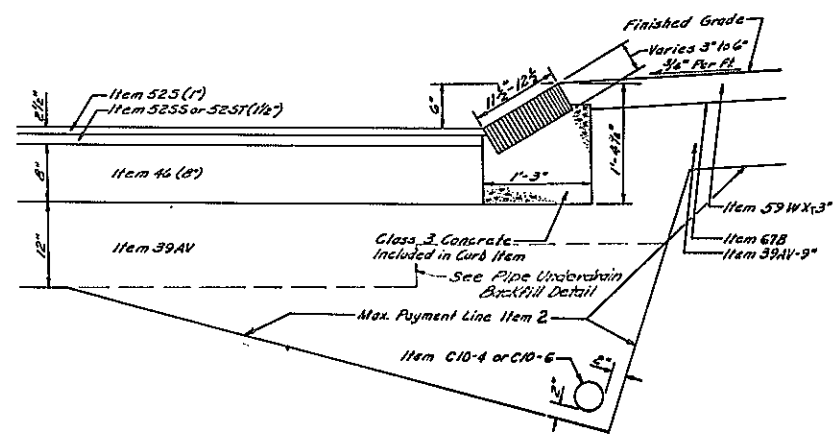
VERTICAL STONE CURB IN ROCK
ITEM 94 BS6, 94 BS8 or 94 BS10



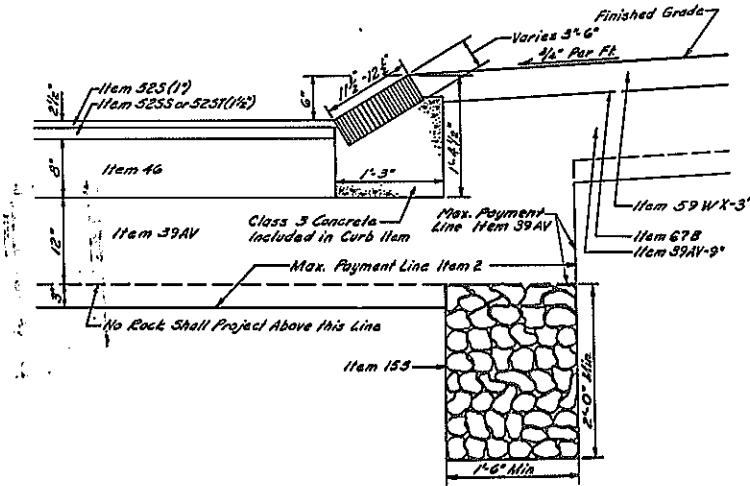
(4) CURB & UNDERDRAIN DETAIL FOR ROAD WIDENING



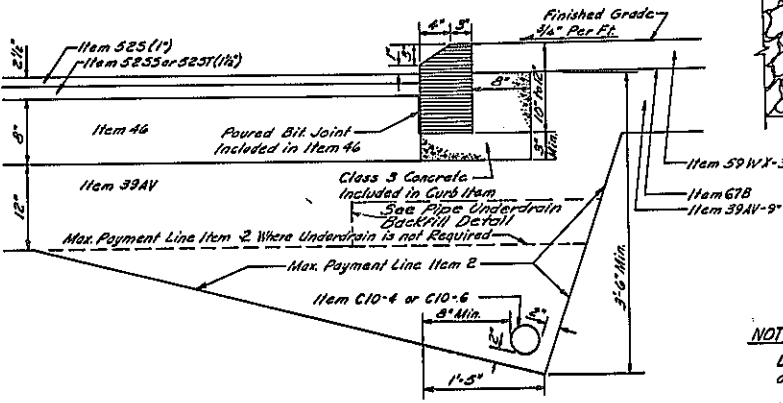
JOINT DETAIL WHERE CONCRETE SIDEWALK PAVEMENT ABUTS CURB
Scale 1/2" = 1'-0"



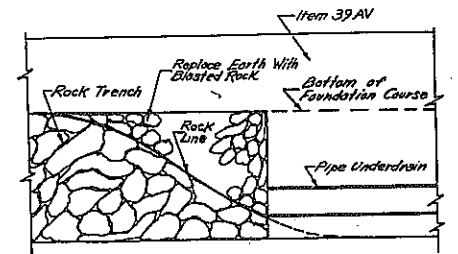
6" SLOPED STONE CURB IN EARTH
ITEM 94 DS



6" SLOPED STONE CURB IN ROCK
ITEM 94 DS

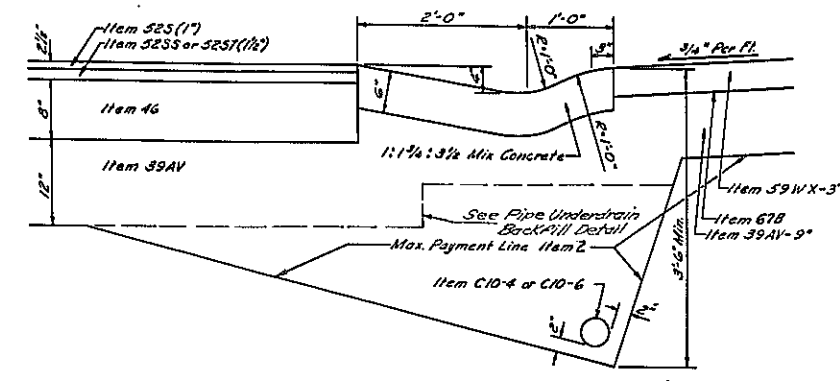


4" MOUNTABLE STONE CURB IN EARTH
ITEM 94 AS

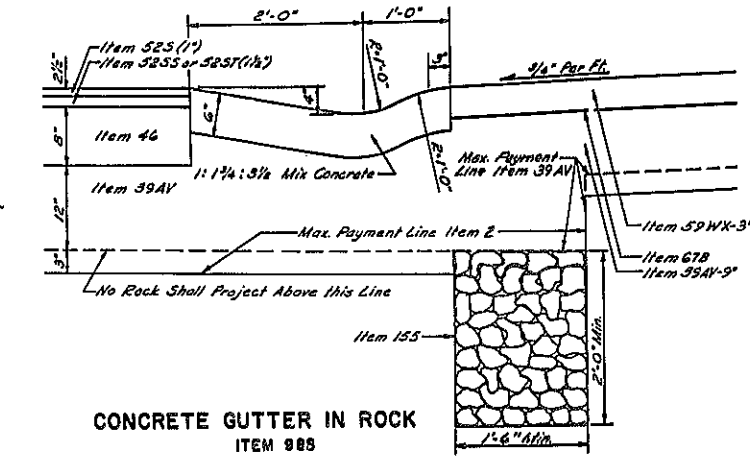


UNDERDRAIN TRANSITION ROCK TRENCH TO PIPE UNDERDRAIN

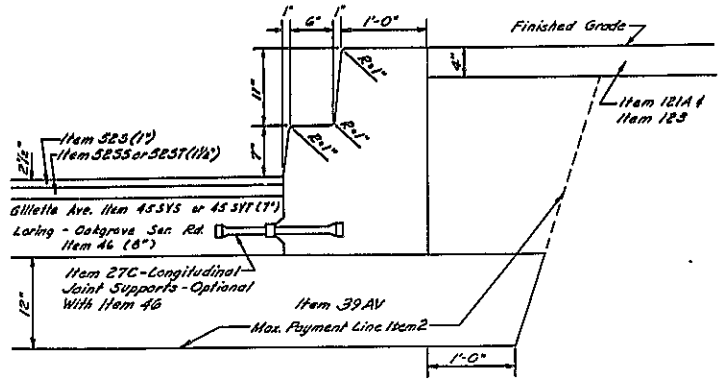
NOTES:
Detail for Item 965, Resetting Old Curb, similar to above curb details.
Where Item 39AV, Foundation Course - Granular Material Type A, also acts as backfill around pipe, including pipe underdrains, all material to be placed within 6 inches of pipe shall pass a 2 inch sieve.
For the locations of features detailed above refer to Typical Sections and Alignment and Paving Plans.
All pipe underdrains and broken rock trench shall outlet to new Drop Inlets, Catch Basins or Manholes as outlined in Summary of Underdrain, See Sheet No. 12. Actual Exc. Limits 2' or 2' Min.



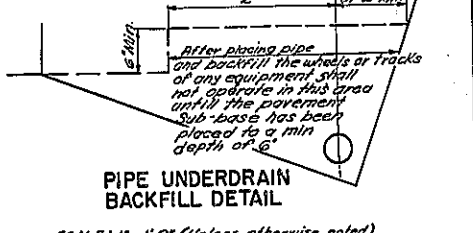
CONCRETE GUTTER IN EARTH
ITEM 985



CONCRETE GUTTER IN ROCK
ITEM 985



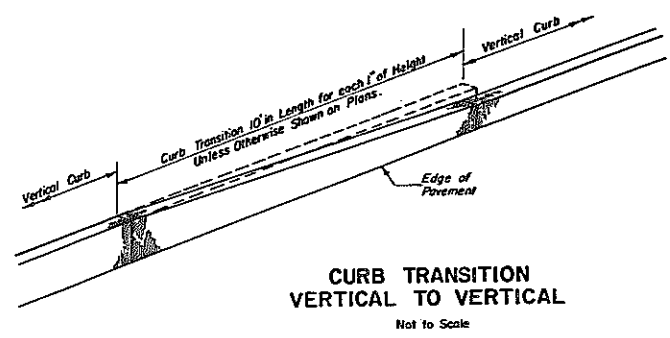
16" CONCRETE CURB
ITEM 97
(TYPICAL SECTION AT GILLET AV. CUL-DE-SAC & LORING - OAKGROVE SERVICE RD. TURN-AROUND)



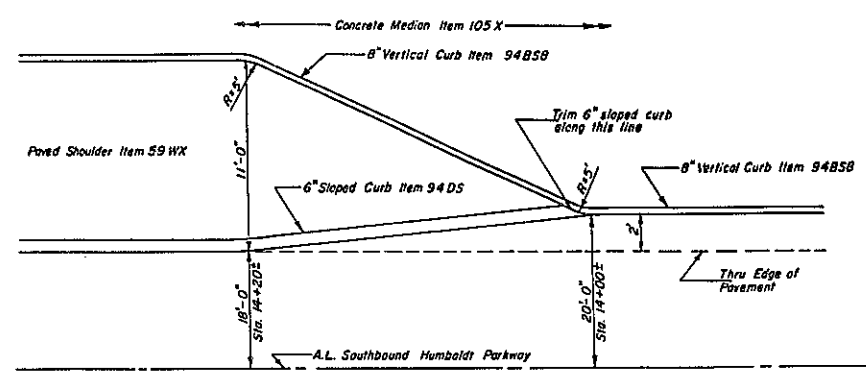
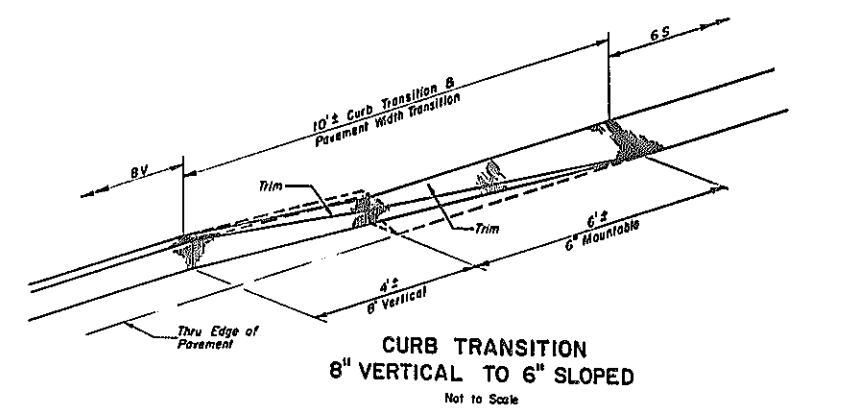
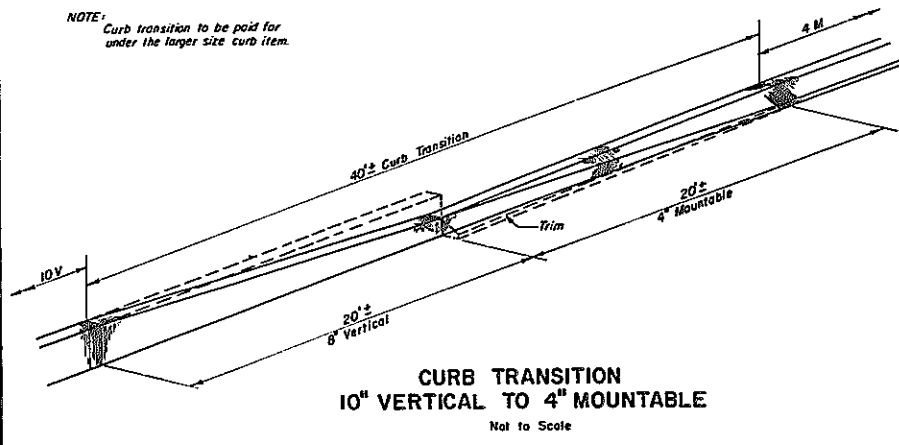
PIPE UNDERDRAIN BACKFILL DETAIL
SCALE: 1" = 1'-0" (Unless otherwise noted)

CURB & GUTTER DETAILS			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.M.B. F.P.P. T.E.K.	

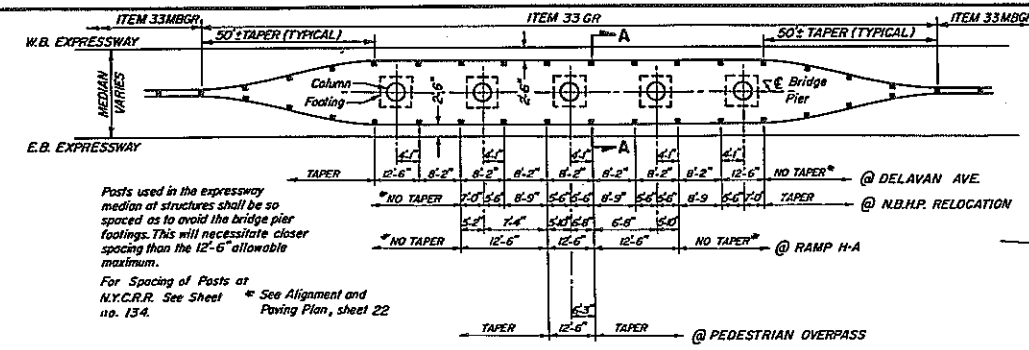
FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	15	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



NOTE: Curb transition to be paid for under the larger size curb item.

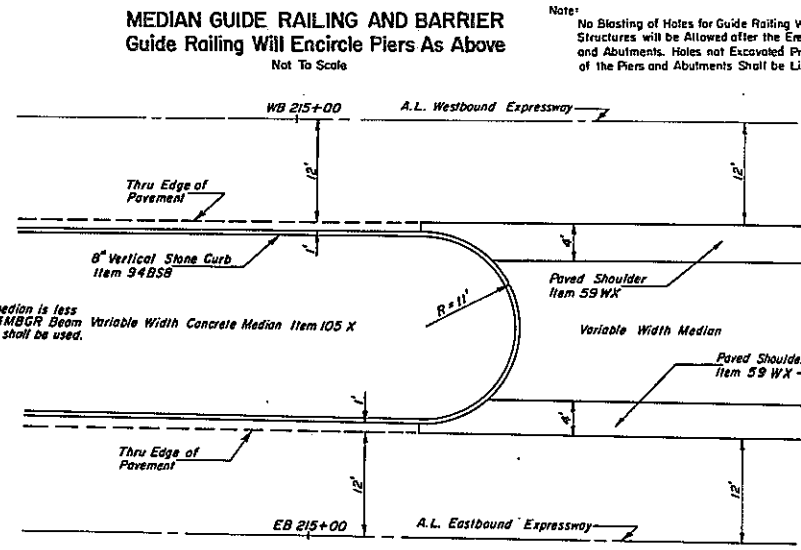
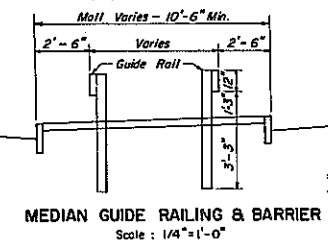


CURB PLAN AT SOUTHBOUND HUMBOLDT PARKWAY RELOCATION STA. 14+00
Scale: 1" = 5'-0"

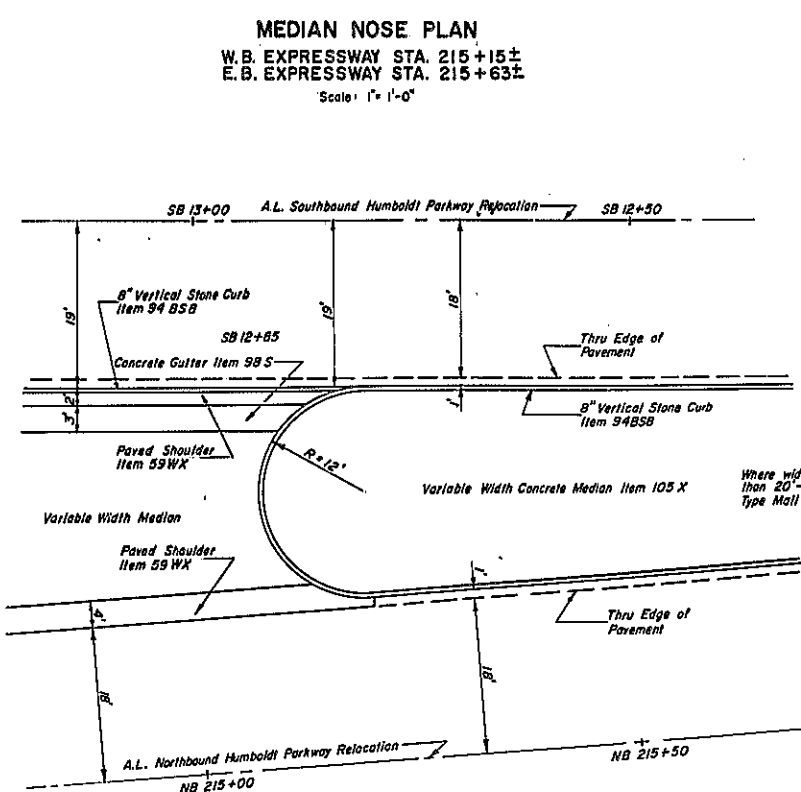


Posts used in the expressway median at structures shall be so spaced as to avoid the bridge pier footings. This will necessitate closer spacing than the 12'-6" allowable maximum.
For Spacing of Posts at N.Y.C.R.R. See Sheet no. 134. * See Alignment and Paving Plan, sheet 22

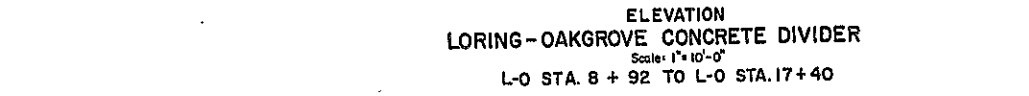
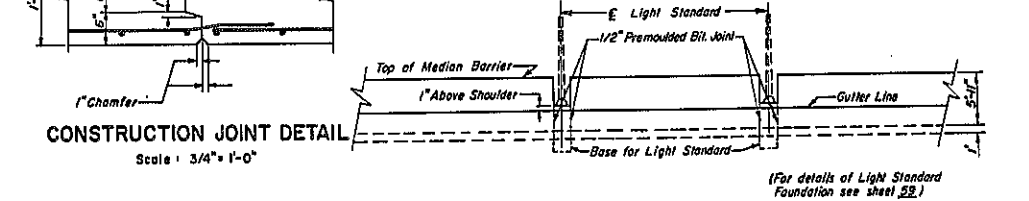
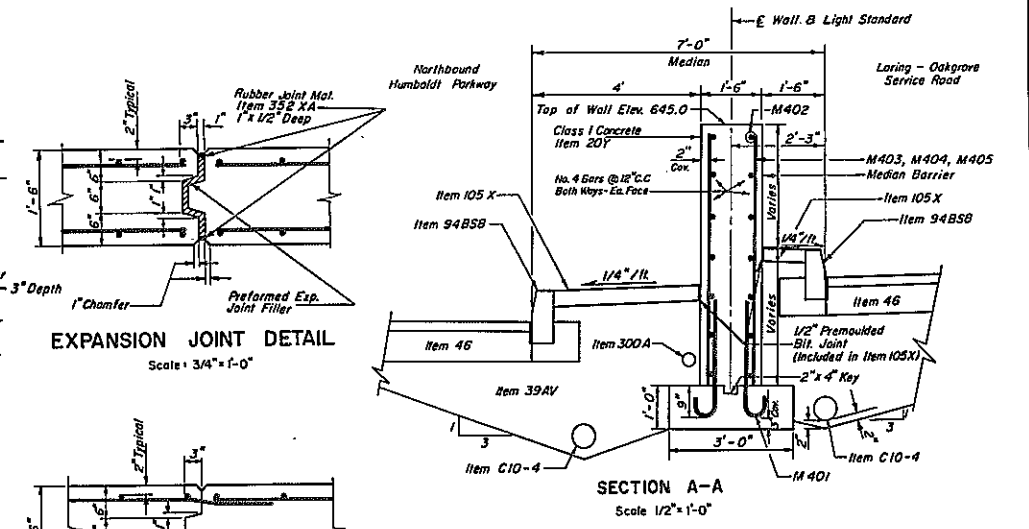
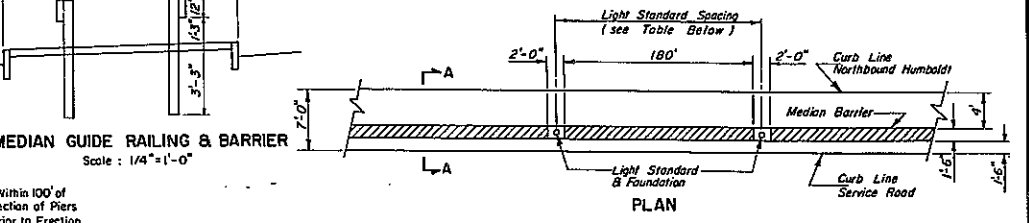
For details of Guide Rail or Wall Barrier refer to Std. sheet no. GL-17.
For location refer to Alignment and Paving Plans & Table sheet No. 13.



Where width of median is less than 20'- Item 33MBGR Beam Type Wall Barrier shall be used.



MEDIAN NOSE PLAN
N.B. HUMBOLDT PARKWAY STA. 215+20±
S.B. HUMBOLDT PARKWAY STA. 12+60±
Scale: 1" = 10'-0"



STATION	FROM	TO	LENGTH	NO. PANELS	TYPE	FOOTING ELEV.	HEIGHT STEM
8+92	9+22	30'	1	A	638.0	6'	
9+22	10+12	90'	3	A	638.0	6'	
10+12	11+02	90'	3	A	638.0	6'	
11+04	11+94	90'	3	A	638.0	6'	
11+94	12+84	90'	3	A	638.0	6'	
12+86	13+76	90'	3	B	637.0	7'	
13+76	14+66	90'	3	C	636.0	8'	
14+68	15+58	90'	3	C	636.0	8'	
15+58	16+48	90'	3	C	636.0	8'	
16+50	17+40	90'	3	B	637.0	7'	

PANEL No. OF MARK	NO. OF PANELS	MARK	SIZE	LENGTH	NO. BARS	A	B	DESCRIPTION
A	13	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	12			HORIZ. BARS
		M403	No. 4	5'-9"	60			VERT. BARS
B	6	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	14			HORIZ. BARS
		M404	No. 4	6'-9"	60			VERT. BARS
C	9	M401	No. 4	3'-3"	60	2'-9"	6"	DOWELS
		M402	No. 4	3'-0"	16			HORIZ. BARS
		M405	No. 4	7'-9"	60			VERT. BARS

- NOTES:
- Expansion Joints to be constructed at the following Stations: 9+22, 10+12, 11+94, 13+76 & 15+58.
 - Construction Joints at 30' Intervals.
 - Location of Light Standards in Median Barrier
 - *L-0" 11+03
 - *L-0" 12+85
 - *L-0" 14+67
 - *L-0" 16+49

SCALE: AS SHOWN

CURB & MEDIAN DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL	DRAWN	T.G.K.
ENGINEERS-ARCHITECTS	CHECKED	J.R.H.
NEW YORK	TRACED	G.C.F.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377 (25)	16	173

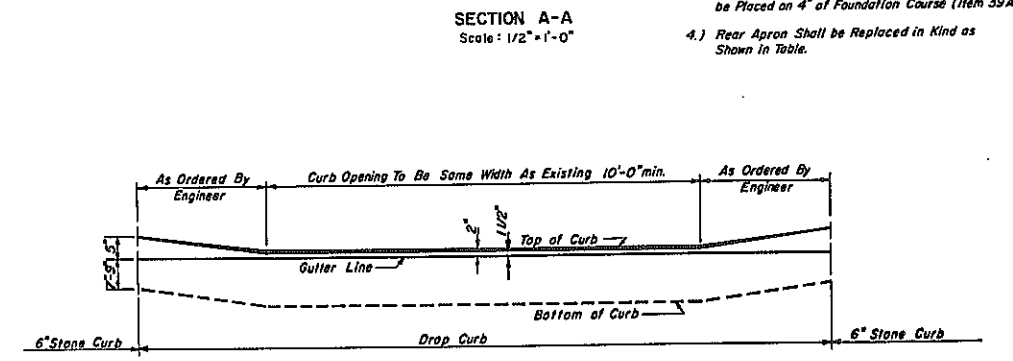
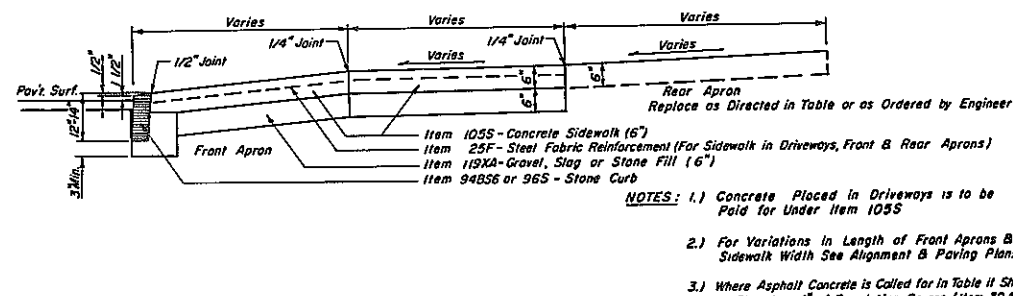
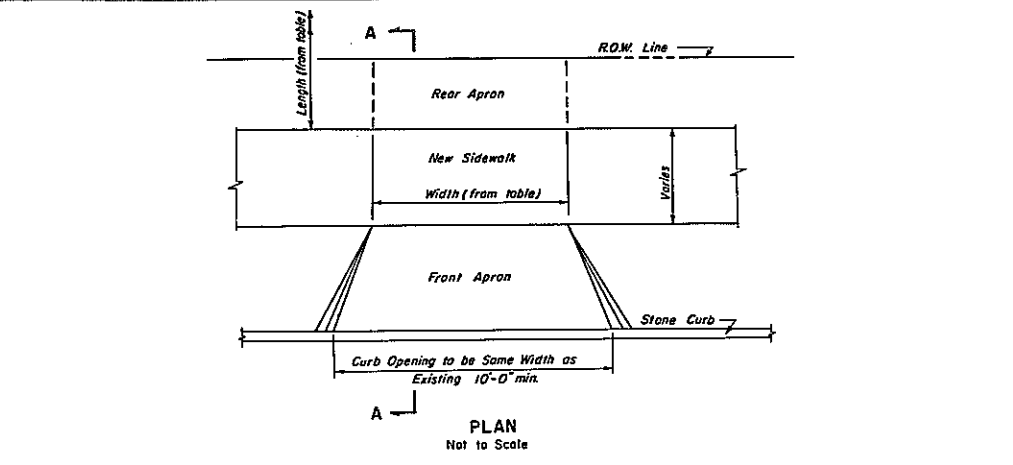
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

TABLE OF DRIVEWAYS TO BE RE-ESTABLISHED

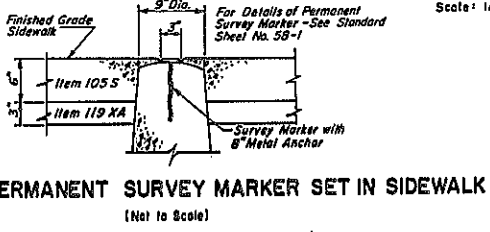
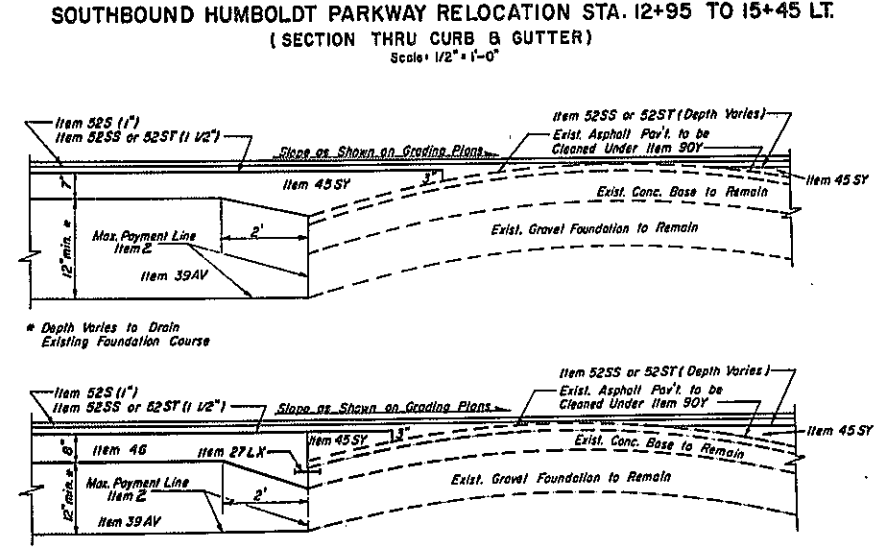
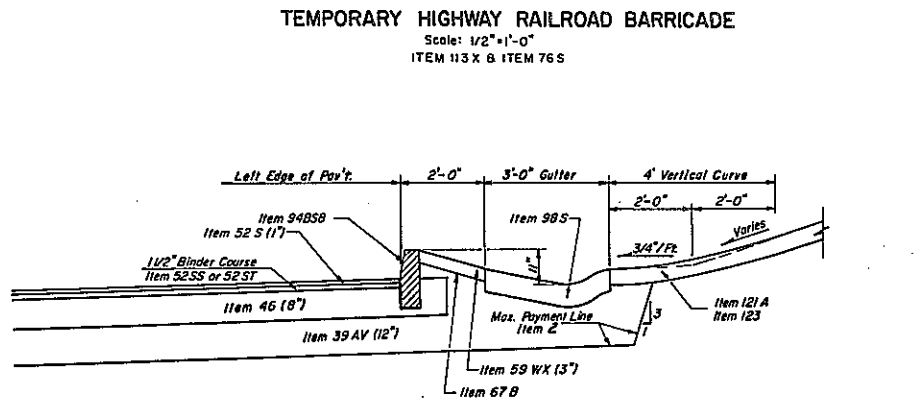
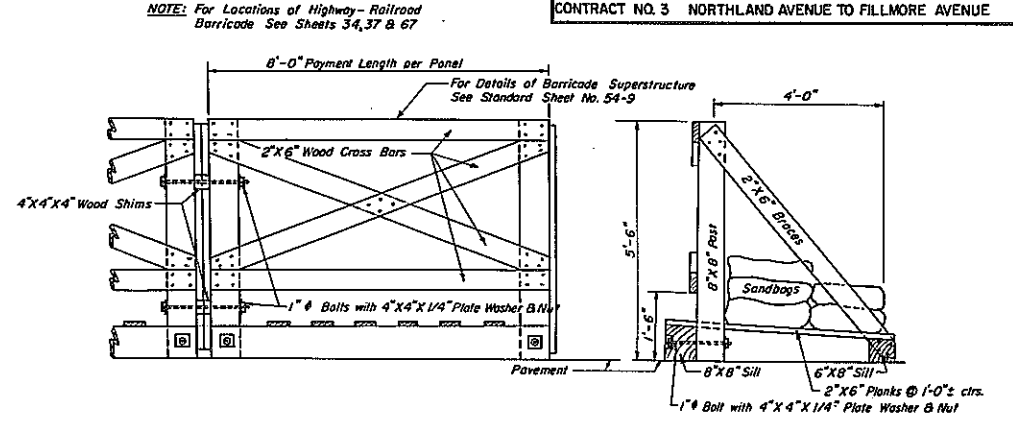
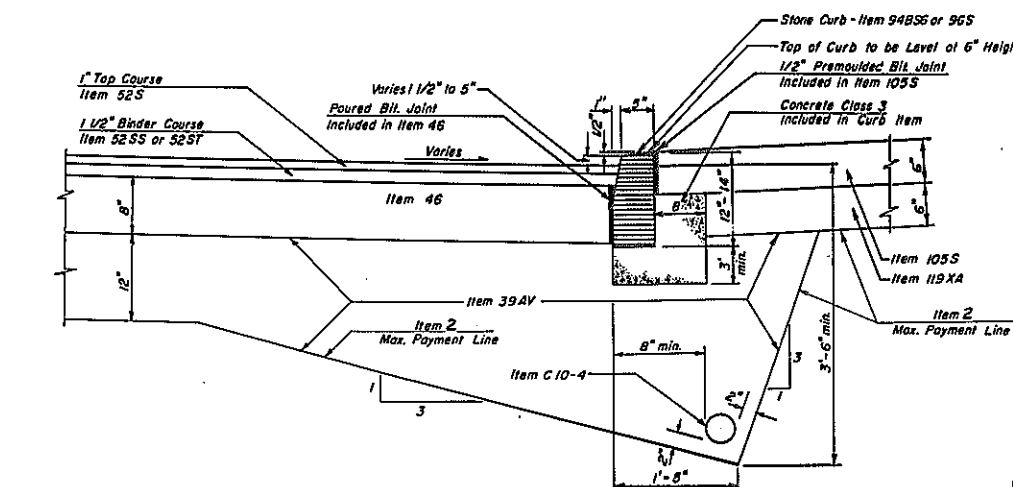
FOR DRIVEWAY REAR APRONS					
LOCATION	SIDE	LENGTH	WIDTH	DEPTH	TYPE
S.B.H.P. SERVICE RD.					
€ STA. 135 + 52	LT.	4'	9'	2 1/2"	ASPHALT CONCRETE
135 + 94	LT.	3'	8'	6"	CONCRETE
136 + 39	LT.	1'	8'	6"	"
138 + 24	LT.	1'	10'	6"	"
138 + 66	LT.	1'	10'	6"	"
141 + 15	LT.	1'	8'	6"	"
143 + 85	LT.	1'	7'	6"	"
144 + 16	LT.	1'	3.5'@2.0'	6"	"
144 + 51	LT.	1'	7'	6"	"
144 + 84	LT.	1'	7.5'	6"	"
145 + 18	LT.	1'	7.5'	6"	"
145 + 57	LT.	1'	10'	6"	"
N.B.H.P. SERVICE RD.					
€ STA. 7 + 18	RT.	2'	7'	6"	CONCRETE
7 + 79	RT.	1'	8'	2 1/2"	ASPHALT CONCRETE
8 + 14	RT.	1'	8'	2 1/2"	CONCRETE
8 + 46	RT.	1'	8'	6"	"
8 + 86	RT.	1'	1.5'@2.5'	6"	"
9 + 30	RT.	1'	2.0'@2.5'	6"	"
9 + 71	RT.	1'	2.0'@2.5'	6"	"
11 + 43	RT.	4'	8'	6"	"
11 + 75	RT.	4'	7'	6"	"
12 + 34	RT.	4'	6'	6"	GRAVEL
12 + 78	RT.	4'	29'	2 1/2"	ASPHALT CONCRETE
14 + 84	RT.	5'	8'	6"	CONCRETE
15 + 54	RT.	5'	8'	6"	"
15 + 87	RT.	5'	7.5'	6"	"
16 + 59	RT.	5'	8'	2 1/2"	ASPHALT CONCRETE
16 + 99	RT.	5'	9'	2 1/2"	"
17 + 36	RT.	5'	7'	6"	CONCRETE
19 + 59	RT.	5'	8.5'	6"	"
19 + 90	RT.	5'	7.5'	6"	"
20 + 20	RT.	5'	2.0'@2.0'	6"	"
21 + 56	RT.	5'	8'	6"	GRAVEL
LORING-DANBROVE SERV. RD.					
€ STA. 10 + 42	LT.	1'	2.0'@3.0'	6"	CONCRETE
10 + 84	LT.	1'	3.0'@2.5'	6"	"
11 + 26	LT.	1'	8'	6"	"
11 + 64	LT.	1'	8'	6"	"
12 + 44	LT.	1'	16'	6"	"
12 + 68	LT.	1'	8'	6"	"
13 + 25	LT.	1'	7'	6"	"
15 + 21	LT.	7'	9'	6"	"
15 + 54	LT.	5'	2.5'@2.5'	6"	"
16 + 26	LT.	2'	7'	6"	"
16 + 64	LT.	2'	9'	6"	"
17 + 05	LT.	2'	6'	6"	"
FILLMORE AVENUE					
€ STA. 7 + 93	RT.	1'	21'	6"	CONCRETE
8 + 31	RT.	1'	31'	6"	"
15 + 10	RT.	4'	20'	6"	GRAVEL

ESTIMATED NEAT QUANTITIES

	REAR APRON	FRONT APRON	TOTAL
ITEM 25F	95 S.Y.	170 S.Y.	265 S.Y.
ITEM 39AV	4 C.Y.	—	4 C.Y.
ITEM 52S	5 TONS	—	5 TONS
ITEM 105S	95 S.Y.	170 S.Y.	265 S.Y.
ITEM 119XA	17 C.Y.	28 C.Y.	45 C.Y.



RE-ESTABLISHMENT OF DRIVEWAYS



DRIVEWAY DETAILS & TABLE		
ROADWAY DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO.		
DE LEUW, CATHER & BRILL	DRAWN	J.R.H.
ENGINEERS-ARCHITECTS	CHECKED	R.J.D.
NEW YORK	TRACED	J.R.H.

SUMMARY OF DRAINAGE STRUCTURES

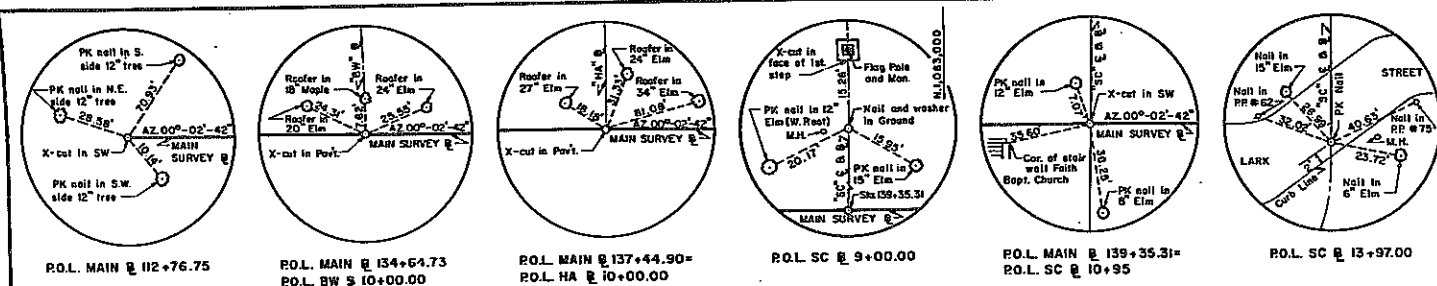
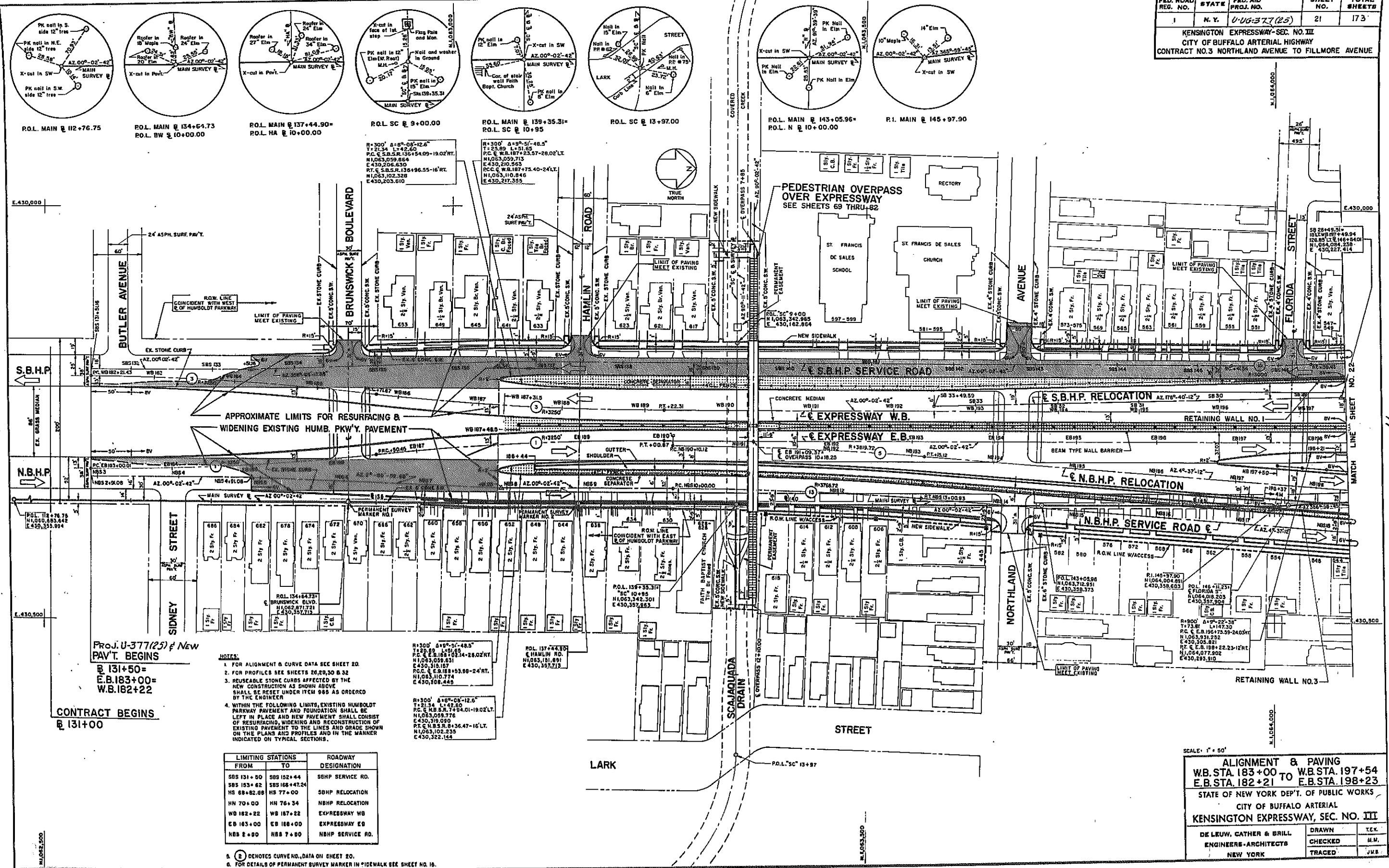
LOCATION	DESCRIPTION	LOCATION	DESCRIPTION	LOCATION	DESCRIPTION	LOCATION	DESCRIPTION
1 SBS 134+33 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 134+33 Lt. with 15' of 15" RCCP.	64 SB 19400 Rt.	Build C-2 C.B. Conn. to New C.B. at SB 19400 Rt. with 25' of 15" RCCP.	125 SB 7468 Lt.	Alter Exist. M.H.	191 RC 10+60	to EA 19+34.5 Const. Outter on Rt. 1671.
2 SBS 134+53 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 134+53 Lt. with 15' of 15" RCCP.	65 SB 16418 Rt.	Build C-2 C.B. Conn. to New C.B. at SB 16418 Rt. with 20' of 15" RCCP.	126 HS 69420 Rt.	Alter Exist. M.H.	192 EB 210+00	to EB 21+58 Const. Outter on Rt. 1581.
3 SBS 134+80 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 134+80 Lt. with 15' of 15" RCCP.	66 SB 16418 Lt.	Build C-2 C.B. Conn. to New C.B. at SB 16418 Lt. with 20' of 15" RCCP.	127 HS 72+27 Rt.	Alter Exist. M.H.	193 FA 13+58	to FA 20+75 Const. Outter on Rt. 7171.
4 SBS 137+15 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 137+15 Lt. with 15' of 15" RCCP.	67 SB 16418 Lt.(90)	Build C-2 C.B. Conn. to New C.B. at SB 16418 Lt. with 20' of 15" RCCP.	128 NB 216+55 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 16418 Lt. with 20' of 15" RCCP.	194 FB 15+45	to FB 24+03 Const. Outter on Rt. 10181.
5 SBS 137+35 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 137+35 Lt. with 15' of 15" RCCP.	68 WB 199418 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #3 with 15' of 15" RCCP.	129 NB 220+37 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 7468 Lt. with 47' of 15" RCCP.	195 WB 217+40	to WB 22+03 Const. Outter on Rt. 8721.
6 SBS 137+55 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 137+55 Lt. with 15' of 15" RCCP.	69 WB 202+00 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #4 with 15' of 15" RCCP.	130 HN 69406 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 7468 Lt. with 47' of 15" RCCP.	196 EB 215+77	to EB 227+00 Const. Outter on Rt. 10801.
7 SBS 139+13 Lt.	Build C-2 C.B. w/Hd. Conn. to exist. M.H. at SBS 139+13 Lt. with 15' of 15" RCCP.	70 WB 204+45 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #5 with 15' of 15" RCCP.	131 HN 72+27 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 7468 Lt. with 47' of 15" RCCP.	197 NY 5+18	to NY 6+92 Lt.-Install 15" Corr. Metal Pipe Drain 174' (at N.T.C.R.R. Detour & Sidetrack) with Galv. Metal End Sections.
8 SBS 142+50 Lt.	Build E-2 D.I. Conn. to New C.B. at SBS 142+50 Lt. with 15' of 15" RCCP.	71 WB 204+66 Rt.	Build E-2 D.I. Conn. to New C.B. at SB 204+66 Rt. with 11' of 15" RCCP.	132 FI 114+5 Lt.	Alter Exist. Receiver	200 HB 18+85 Rt(135)	Build S-1 M.H. Conn. to New M.H. at LO 16+12 with 180' of 15" RCCP.
9 SBS 142+70 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SBS 142+70 Lt. with 15' of 15" RCCP.	72 WB 204+78 Lt.	Build C-2 C.B. Conn. to New C.B. at SB 204+78 Lt. with 21' of 15" RCCP.	199 HN 74+90 Lt.	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	201 WB 204+45 Lt.	Conn. Overflow to Future Pumping Sta. w/36"-36" RCCP
10 SBS 142+90 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SBS 142+90 Lt. with 15' of 15" RCCP.	73 WB 204+78 Lt.	Build C-2 C.B. Conn. to New C.B. at SB 204+78 Lt. with 21' of 15" RCCP.	200 HB 18+85 Rt(135)	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	202 L-O 14+15 Lt.	Build C-2 C.B. w/Hd. Conn. to New C.B. at L-O 14+15 Lt. with 24'-15" RCCP.
11 SBS 143+12 Lt.	Build C-2 C.B. w/Hd. Conn. to New C.B. at SB 143+12 Lt. with 11' of 15" RCCP.	74 WB 204+78 Lt.	Build C-2 C.B. Conn. to New C.B. at SB 204+78 Lt. with 21' of 15" RCCP.	203 L-O 14+43 Lt.	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	400 WFD 10+42 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at L-O 14+35 Lt. with 15" RCCP.
12 SBS 143+50 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. Drain in Florida St. with 20' of 15" RCCP.	75 WB 208+95 Rt.	Build C-2 C.B. Conn. to New C.B. at WB 208+95 Rt. with 190' of 18" RCCP.	401 NED 12+45 Lt.(22)	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	402 FID 13+50 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at FI 13+36 Lt. with 26' of 15" RCCP.
13 WB 184+34 Rt.	Build E-2 D.I. Conn. to New C.B. at EB 184+34 Rt. with 184' of 15" RCCP.	76 WB 210+98 Rt.	Build C-2 C.B. Conn. to New M.H. #7 with 8' of 15" RCCP.	403 FI 104+7 Lt.	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	404 FID 7470 Rt.	Build Temp. D-2 R.H. Conn. to New M.H. at FI 11+45 Lt. with 85' of 36" RCCP.
14 WB 184+58 Lt.	Build C-2 C.B. Conn. to New C.B. at WB 184+58 Lt. with 31' of 15" RCCP.	77 HB 134+86 Lt.	Build E-1 D.I. Conn. to New C.B. at HB 134+86 Lt. with 19' of 15" RCCP.	405 FI 8481 Lt.	Build E-2 D.I. Conn. to New C.B. at SB 216+55 Lt. with 17' of 15" RCCP.	406 FID 7470 Lt.	Build E-2 D.I. on left Conn. to New C-2 at FI D 7470 Rt. with 36' of 15" RCCP.
15 WB 184+58 Rt.	Build C-2 C.B. Conn. to New C.B. at WB 184+58 Rt. with 31' of 15" RCCP.	78 HB 134+86 Lt.	Build E-1 D.I. Conn. to New C.B. at HB 134+86 Lt. with 19' of 15" RCCP.	204 NBS 19+20 Rt(19)	Alter Exist. M.H.	209 NBS 21+20 Rt(19)	Build Type S-1 M.H. on Rt. Conn. to Exist. M.H. at NBS 18+80 Lt. with 42' of 15" RCCP.
16 WB 191+42 Lt.	Build E-2 D.I. Conn. to New C.B. at WB 191+42 Lt. with 31' of 15" RCCP.	79 HB 17407 Lt.	Build C-1 C.B. Conn. to New C.B. at HB 17407 Lt. with 19' of 15" RCCP.		Alter Exist. Receiver.		Build Type S-1 M.H. on Rt. Conn. to New M.H. at NBS 19+20 Rt. with 196' of 15" RCCP.
17 WB 191+42 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #1 with 15' of 15" RCCP.	80 HB 17407 Lt.	Build C-1 C.B. Conn. to New C.B. at HB 17407 Lt. with 19' of 15" RCCP.		Alter Exist. Receiver.		
18 WB 193+96 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #2 with 3' of 15" RCCP.	81 EB 199488	Build D-4 M.H. Conn. to New M.H. #2 with 510' of 60" RCCP.		Alter Exist. Receiver.		
19 SB 2403 Rt.	Build C-2 C.B. Conn. to New C.B. at SB 2403 Rt. with 38' of 15" RCCP.	82 EB 199495 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #3 with 6' of 15" RCCP.		Alter Exist. Receiver.		
20 EB 191474 Lt.	Build Spec. M.H. #1 Conn. to Scasquada Drain with 78' of 60" RCCP.	83 EB 199495 Rt.	Build E-1 D.I. Conn. to New C.B. at EB 199495 Rt. with 25' of 15" RCCP.		Alter Exist. Receiver.		
21 EB 191474 Lt.	Build D-4 M.H. #2 Conn. to New M.H. #1 with 296' of 60" RCCP.	84 EB 202467 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #4 with 9' of 15" RCCP.		Alter Exist. Receiver.		
22 EB 185+46 Lt.	Build C-2 C.B. Conn. to New C.B. at NBS 6+28 Rt. with 58' of 15" RCCP.	85 EB 202467 Rt.	Build E-1 D.I. Conn. to New C.B. at EB 202467 Rt. with 25' of 15" RCCP.		Alter Exist. Receiver.		
23 EB 189434 Lt.	Build C-2 C.B. Conn. to New C.B. at EB 189434 Rt. with 31' of 15" RCCP.	86 EB 202480 Lt.	Build Spec. M.H. Conn. to New M.H. #3 with 288' of 60" RCCP.		Alter Exist. Receiver.		
24 EB 189434 Rt.	Build C-2 C.B. Conn. to New C.B. at EB 189434 Rt. with 31' of 15" RCCP.	87 EB 203478 Rt.	Build Spec. M.H. Conn. to New M.H. #4 with 90' of 50" RCCP.		Alter Exist. Receiver.		
25 EB 191485 Lt.	Build C-2 C.B. w/Hd. Conn. to New C.B. at NBS 10+12 Rt. with 97' of 15" RCCP.	88 EB 205424 Rt.	Build E-1 D.I. Conn. to New C.B. at EB 205424 Rt. with 10' of 15" RCCP.		Alter Exist. Receiver.		
26 EB 191490 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #1 with 9' of 15" RCCP.	89 EB 205426 Rt.	Build C-1 C.B. Conn. to New C.B. at WB 204+78 Rt. with 16' of 15" RCCP.		Alter Exist. Receiver.		
27 NB 191485 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #2 with 11' of 15" RCCP.	90 EB 208486 Rt.	Build E-3 M.H. Conn. to New M.H. #5 with 510' of 48" RCCP.		Alter Exist. Receiver.		
28 NB 191493 Rt.	Build C-1 C.B. Conn. to New C.B. at EB 191485 Lt. with 62' of 15" RCCP.	91 EB 211450 Lt.	Build Spec. M.H. Conn. to New M.H. #6 with 262' of 48" RCCP.		Alter Exist. Receiver.		
29 NB 196482 Lt.	Build C-2 C.B. Conn. to New C.B. at NB 196480 Rt. with 35' of 15" RCCP.	92 EB 211450 Rt.	Build C-1 C.B. w/Hd. Conn. to New M.H. #7 with 45' of 15" RCCP.		Alter Exist. Receiver.		
30 NB 196480 Rt.	Build C-1 C.B. Conn. to New C.B. at NBS 16+50 Lt. with 20' of 15" RCCP.	93 NB 198499 Rt.	Build C-2 C.B. Conn. to New C.B. at NB 196482 Lt. with 25' of 15" RCCP.		Alter Exist. Receiver.		
31 NBS 6428 Rt.	Build C-2 w/Hd. Conn. to Exist. M.H. at NBS 5+80 Rt. with 15' of 15" RCCP.	94 NB 199431 Rt.	Build E-2 D.I. Conn. to New C.B. at NB 198499 Lt. with 45' of 15" RCCP.		Alter Exist. Receiver.		
32 NBS 10412 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at NBS 10450 Rt. with 37' of 18" RCCP.	95 NB 201433 Lt.	Build C-2 C.B. Conn. to New C.B. at NB 198499 Lt. with 231' of 15" RCCP.		Alter Exist. Receiver.		
33 NBS 13491 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at NBS 10450 Rt. with 37' of 18" RCCP.	96 NB 203469 Lt.	Build E-2 D.I. Conn. to New C.B. at NB 201433 Lt. with 18' of 15" RCCP.		Alter Exist. Receiver.		
34 NBS 13485 Rt.	Build C-2 C.B. w/Hd. Conn. to New C.B. at NBS 13491 Lt. with 28' of 15" RCCP.	97 NBS 18473 Rt.	Build E-2 D.I. Conn. to New C.B. at NBS 18472 Rt. with 20' of 15" RCCP.		Alter Exist. Receiver.		
35 NBS 14402 Rt.	Build C-2 C.B. w/Hd. Conn. to New C.B. at NBS 13485 Rt. with 25' of 15" RCCP.	98 NBS 18472 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #8 with 19' of 15" RCCP.		Alter Exist. Receiver.		
36 NBS 14426 Rt.	Build E-2 D.I. Conn. to New C.B. at NBS 14402 Rt. with 26' of 15" RCCP.	99 NBS 193430 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #9 with 8' of 15" RCCP.		Alter Exist. Receiver.		
37 NBS 16445	Build S-1 M.H. Conn. to Exist. M.H. at NBS 14426 Rt. with 126' of 18" RCCP.	100 NBS 19480 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #10 with 10' of 15" RCCP.		Alter Exist. Receiver.		
38 NBS 16450 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. at NBS 16445 with 13' of 15" RCCP.	101 NBS 18480 Lt.	Build E-2 D.I. Conn. to New C.B. at D 11+41 Rt. with 50' of 15" RCCP.		Alter Exist. Receiver.		
39 NBS 16450 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. at NBS 16445 with 12' of 15" RCCP.	102 NBS 23473 Rt.	Build E-2 D.I. Conn. to New C.B. at D 11+41 Rt. with 50' of 15" RCCP.		Alter Exist. Receiver.		
40 SBS 137+15 Lt.	Alter Exist. M.H.	103 HC 10460 Lt.(90)	Build C-3 C.B. Conn. to New C.B. at D 11+40 Lt. with 31' of 15" RCCP.		Alter Exist. Receiver.		
41 SBS 137+76 Lt.	Alter Exist. M.H.	104 D 11411 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at D 11+40 Lt. with 22' of 15" RCCP.		Alter Exist. Receiver.		
42 SBS 138+31 Lt.	Alter Exist. M.H.	105 D 11419 Rt.	Alter Exist. M.H. Conn. to Spec. M.H. #5 with 117' of 24" RCCP.		Alter Exist. Receiver.		
43 SBS 142+80 Lt.	Alter Exist. M.H.	106 D 12+20 Rt.	Alter Exist. M.H.		Alter Exist. Receiver.		
44 SBS 142+85 Lt.	Alter Exist. M.H.	107 D 11410 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at D 11+40 Rt. with 27' of 15" RCCP.		Alter Exist. Receiver.		
45 NBS 10450 Rt.	Alter Exist. M.H.	108 HC 12414 Rt.	Build E-1 D.I. Conn. to New C.B. at HA 16490 Lt. with 71' of 15" RCCP.		Alter Exist. Receiver.		
46 NBS 14145 Rt.	Alter Exist. M.H.	109 HA 16490 Lt.	Build C-2 C.B. w/Hd. Conn. to New M.H. #11 at EB 208486 Rt. with 110' of 15" RCCP.		Alter Exist. Receiver.		
47 NBS 193490 Rt.	Alter Exist. M.H.	110 HA 18428 Rt.	Build C-1 C.B. w/Hd. Conn. to New M.H. #12 with 208486 Rt. with 21' of 15" RCCP.		Alter Exist. Receiver.		
48 NBS 19418 Lt.	Alter Exist. M.H.	111 HS 161400 Rt.	Build E-2 D.I. Conn. to New C.B. at EB 208486 Rt. with 21' of 15" RCCP.		Alter Exist. Receiver.		
49 SBS 118+48 Lt.	Build C-2 C.B. Conn. to New C.B. at SBS 116+50 Lt. with 195' of 15" RCCP.	112 SBS 164417 Rt.	Build E-2 D.I. Conn. to New C.B. at EB 208486 Rt. with 21' of 15" RCCP.		Alter Exist. Receiver.		
50 SBS 150454 Lt.	Build E-2 D.I. Conn. to New C.B. at SBS 116+50 Lt. with 201' of 15" RCCP.	113 SBS 165485 Rt.	Build E-2 D.I. Conn. to Exist. Drain in Road		Alter Exist. Receiver.		
51 SBS 152+57 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. at SBS 153400 (E of Delavan) with 35' of 15" RCCP.	114 LO 8+80 Lt.	Build E-2 D.I. Conn. to Exist. Drain in Road		Alter Exist. Receiver.		
52 SBS 153400 Rt.(14)	Build D-3 M.H. on Exist. 16" Brick Sewer.	115 SBS 169+87 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at HS 69+20 Rt. with 18' of 15" RCCP.		Alter Exist. Receiver.		
53 SBS 153422 Lt.	Alter Exist. M.H. in Delavan.	116 SB 13455 Rt.	Build E-2 D.I. Conn. to New C.B. at SB 16418 Rt. with 259' of 15" RCCP.		Alter Exist. Receiver.		
54 SBS 153422 Rt.	Build C-2 C.B. w/Hd. Conn. to New M.H. at SBS 153400 (E of Delavan) with 20' of 15" RCCP.	117 SB 9+75 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 10428 with 50' of 15" RCCP.		Alter Exist. Receiver.		
55 SBS 153444 Lt.	Alter Exist. M.H.	118 SB 10+28 Rt.	Alter Exist. M.H.		Alter Exist. Receiver.		
56 HA 9+93 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SBS 155493 Lt. with 72' of 15" RCCP.	119 SB 8400 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 10428 with 50' of 15" RCCP.		Alter Exist. Receiver.		
57 SBS 155493 Rt.	Build E-2 D.I. Conn. to Exist. Drain in Road.	120 HS 70404 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at SB 7468 Lt. with 50' of 15" RCCP.		Alter Exist. Receiver.		
58 SB 26482 Rt.	Build C-2 C.B. Conn. to New C.B. at WB 199418 Rt. with 144' of 15" RCCP.	121 HS 72+27 Lt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at HS 69420 Rt. with 125' of 15" RCCP.		Alter Exist. Receiver.		
59 SB 24+31 Rt.	Build E-2 D.I. Conn. to New C.B. at WB 202+00 Rt. with 65' of 15" RCCP.	122 HS 72416 Rt.	Build C-2 C.B. w/Hd. Conn. to Exist. M.H. at HS 72427 Rt. with 5' of 15" RCCP.		Alter Exist. Receiver.		
60 SB 21481 Rt.	Build E-2 D.I. Conn. to New C.B. at SB 21460 Rt. with 16' of 15" RCCP.	123 NB 210465 Rt.(40)	Build C-3 C.B. Conn. to New C.B. at HB 17407 Lt. with 136' of 15" RCCP.		Alter Exist. Receiver.		
61 SB 21450 Lt.	Build C-1 C.B. Conn. to Overflow M.H. at WB 20445 Lt. with 38' of 18" RCCP.	124 SB 19400 Lt.	Alter Exist. M.H.		Alter Exist. Receiver.		
62 SB 21455 Lt.	Build E-1 D.I. Conn. to New C.B. at SB 19400 Rt. with 36' of 15" RCCP.				Alter Exist. Receiver.		

SUMMARY OF UNDERDRAIN (CONT'D.)

BEGIN AT STA.	OUTLET AT STA.	DRAIN NO.	SIDE	ITEM C10-4	ITEM C10-6	ITEM 155
SBS 134+95	SBS 137415	5	Lt	218'		
SBS 137415	SBS 137435	6	Lt	210'		
SBS 137470	SBS 137455	6	Lt	201'		
SBS 137470	SBS 139413	7	Lt	139'		
SBS 139413	SBS 142426	6	Lt	309'		
SBS 142426	SBS 142470	9	Lt	59'		
SBS 142470	SBS 142490	10	Lt	38'		
SBS 146404	SBS 143412	(c)	Lt	295'		
SBS 146404	SBS 146430	(1)	Lt	25'		
SBS 152486	SBS 150434	50	Lt	150'		
SBS 152486	D 7437	(c)	Lt	200'		
SBS 158470	SBS 161400	111	Rt	228'		
FA 16450	FA 13465	170	Rt	38'		
FB 18450	FB 18450	150	Rt	322'		281'
FB 21400	FB 25450	(b)	Rt			
FB 25450	HB 10432	(4)	Rt			83'
HA 11490	HA 15410	(b)	Lt	20'		
HA 15410	HA 16490	109	Lt			178'
HB 10432	HB 13486	78	Rt			350'
HB 13486	HB 13475	78	Rt			287'
HB 17407	HB 17407	(b)	Rt			
HB 19411	HB 17407	80	Rt	203'		
HB 19411	HB 21452	133	Rt	238'		
HC 10456	HC 11425	(b)	Rt	69'		
HC 11425	HC 12418	108	Rt			87'
HC 12418	HA 18418	(c)	Rt			218'
D 8430	D 8430	(c)	Rt			
D 10488	D 11411	110	Rt	30'		
D 10481	D 11400	107	Lt	57'		
D 11480</						

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	21	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE



R.O.L. MAIN @ 112+76.75
R.O.L. MAIN @ 134+64.73
R.O.L. MAIN @ 137+44.90
R.O.L. SC @ 9+00.00
R.O.L. MAIN @ 139+35.31
R.O.L. SC @ 13+97.00
R.O.L. MAIN @ 143+05.96
R.O.L. N @ 10+00.00
R.O.L. MAIN @ 145+97.90

BRUNSWICK BOULEVARD
HAMLIN ROAD
PEDESTRIAN OVERPASS OVER EXPRESSWAY
SEE SHEETS 69 THRU-82
RECTORY
ST. FRANCIS DE SALES SCHOOL
ST. FRANCIS DE SALES CHURCH

APPROXIMATE LIMITS FOR RESURFACING & WIDENING EXISTING HUMB. PKW'Y. PAVEMENT
S.B.H.P. SERVICE ROAD
EXPRESSWAY W.B.
EXPRESSWAY E.B.
N.B.H.P. SERVICE ROAD
N.B.H.P. RELOCATION

SIDNEY STREET
NORTHLAND AVENUE
FLORIDA STREET
LARK STREET
SCALAQUDA DRAIN

PROJ. U-377(25) New PAV'T. BEGINS @ 131+50 = E.B. 183+00 = W.B. 182+22

- NOTES:
- FOR ALIGNMENT & CURVE DATA SEE SHEET 20.
 - FOR PROFILES SEE SHEETS 20, 29, 30 & 32.
 - REUSABLE STONE CURBS AFFECTED BY THE NEW CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 985 AS ORDERED BY THE ENGINEER.
 - WITHIN THE FOLLOWING LIMITS, EXISTING HUMBOLDT PARKWAY PAVEMENT AND FOUNDATION SHALL BE LEFT IN PLACE AND NEW PAVEMENT SHALL CONSIST OF RESURFACING, WIDENING AND RECONSTRUCTION OF EXISTING PAVEMENT TO THE LINES AND GRADE SHOWN ON THE PLANS AND PROFILES AND IN THE MANNER INDICATED ON TYPICAL SECTIONS.

LIMITING STATIONS	ROADWAY DESIGNATION
505 131+00 TO 505 152+44	SBHP SERVICE RD.
505 153+62 TO 505 166+47.24	SBHP RELOCATION
505 167+00 TO 505 177+00	SBHP RELOCATION
505 177+00 TO 505 176+34	NBHP RELOCATION
505 176+34 TO 505 182+22	EXPRESSWAY WB
505 182+22 TO 505 187+22	EXPRESSWAY EB
505 187+22 TO 505 188+00	EXPRESSWAY EB
505 188+00 TO 505 197+54	NBHP SERVICE RD.

5. (C) DENOTES CURVE NO., DATA ON SHEET 20.
6. FOR DETAILS OF PERMANENT SURVEY MARKER IN SIDEWALK SEE SHEET NO. 16.

SCALE: 1" = 50'

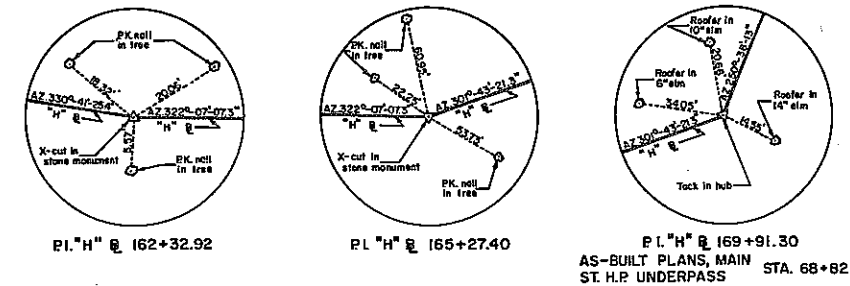
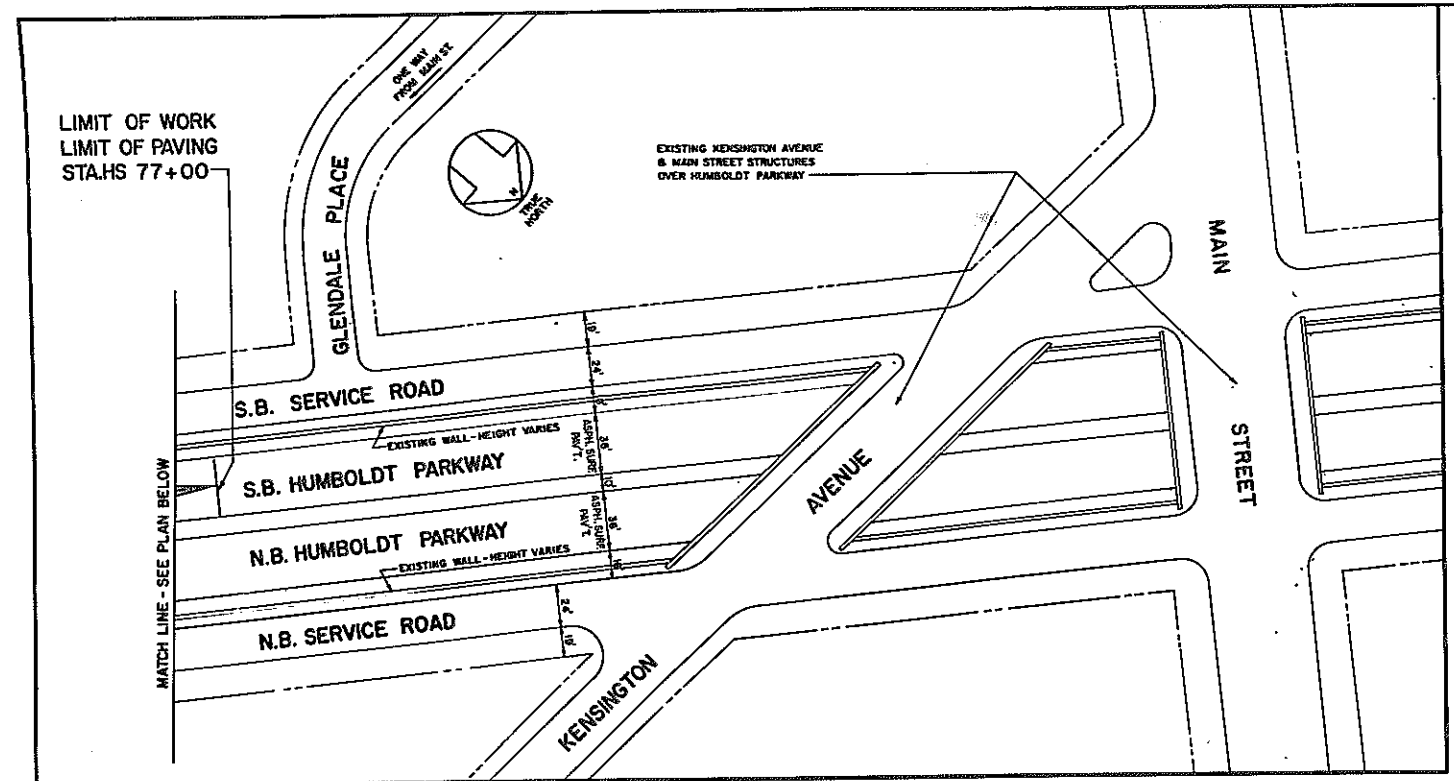
ALIGNMENT & PAVING
WB. STA. 183+00 TO WB. STA. 197+54
E.B. STA. 182+21 E.B. STA. 198+23
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	H.M.
NEW YORK	TRACED	J.W.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	23	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

LIMIT OF WORK
LIMIT OF PAVING
STA. HS 77+00



20 N.B.H.P. RELOCATION CURVE DATA

Δ = 7°-39'-56"
R = 2623.94'
D = 2°-11'-00.96"
L = 351.05'
T = 173.79'
E = 5.89'
C.C. = N1,064,007.007 E 427,663.663
PC = NB 218+05.29 N1,065,800.880 E 429,578.625
PT = NB 220+05.29 N1,065,923.172 E 429,454.447
PT = NB 221+05.55 N1,066,040.287 E 429,322.228
MAX. SUPEREL. 1/2" / FT.

21 S.B.H.P. RELOCATION CURVE DATA

Δ = 6°-15'-30.6"
R = 2575.94'
D = 2°-13'-27.37"
L = 371.29'
T = 185.97'
E = 6.70'
C.C. = N1,064,007.007 E 427,663.663
PC = SB 6+29.39 N1,065,532.488 E 429,354.464
PT = SB 8+15.35 N1,065,825.542 E 429,435.994
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
MAX. SUPEREL. 1/2" / FT.

7 S.B.H.P. RELOCATION CURVE DATA

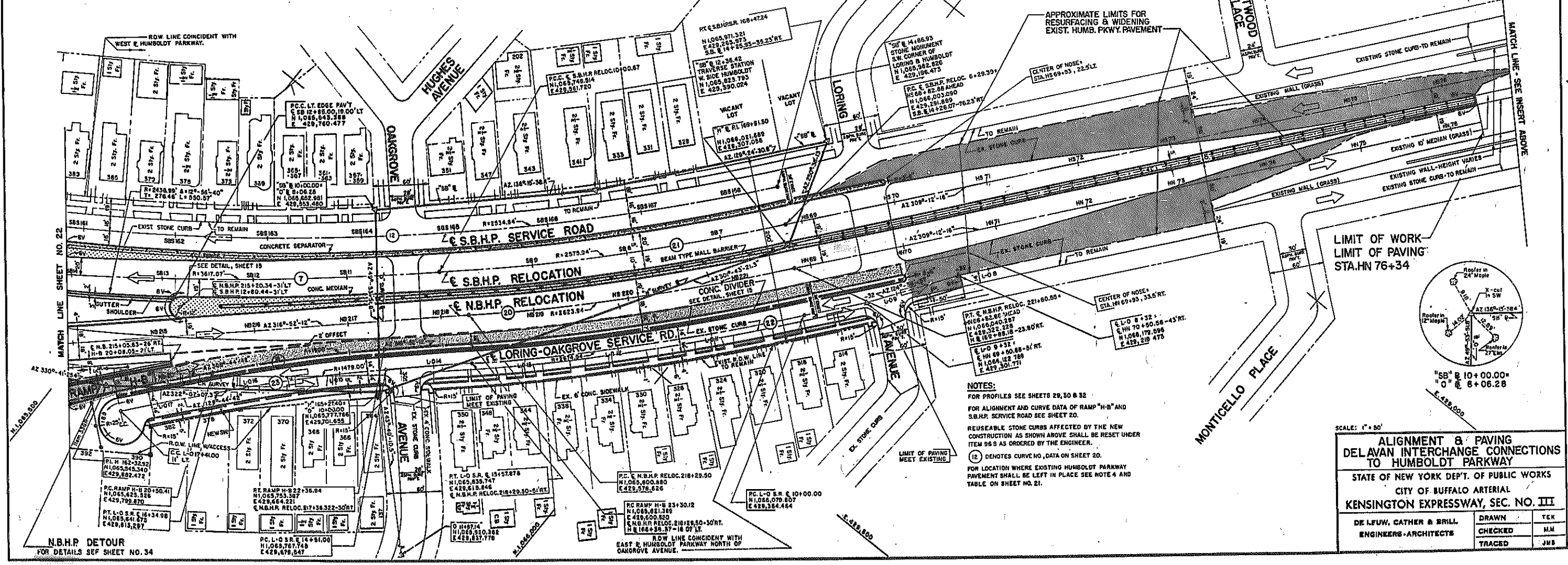
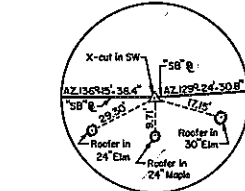
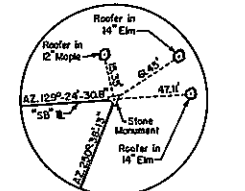
Δ = 9°-16'-04.4"
R = 2617.07'
D = 1°-35'-02.54"
L = 585.09'
T = 293.18'
E = 11.85'
C.C. = N1,063,303.134 E 426,896.319
PC = SB 6+29.39 N1,065,532.488 E 429,354.464
PT = SB 8+15.35 N1,065,825.542 E 429,435.994
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
PT = SB 10+00.00 N1,066,003.090 E 429,391.689
MAX. SUPEREL. 1/2" / FT.

22 L-O SERVICE RD. CURVE DATA

Δ = 7°-39'-56"
R = 2674.94'
D = 2°-08'-31.02"
L = 357.89'
T = 179.21'
E = 6.05'
C.C. = N1,064,007.007 E 427,663.663
PC = L-O 10+00.00 N1,065,825.542 E 429,435.994
PT = L-O 11+79.21 N1,066,003.090 E 429,391.689
PT = L-O 13+37.89 N1,066,003.090 E 429,391.689
PT = L-O 16+34.98 N1,066,003.090 E 429,391.689

23 L-O SERVICE RD. CURVE DATA

Δ = 7°-07'-39"
R = 1479.00'
D = 3°-52'-26.22"
L = 183.92'
T = 92.00'
E = 2.85'
C.C. = N1,066,778.075 E 430,758.923
PC = L-O 14+51.06 N1,066,778.075 E 430,758.923
PT = L-O 15+43.14 N1,066,778.075 E 430,758.923
PT = L-O 16+34.98 N1,066,778.075 E 430,758.923
PT = L-O 18+34.98 N1,066,778.075 E 430,758.923



NOTES:
FOR PROFILES SEE SHEETS 29, 30 & 32
FOR ALIGNMENT AND CURVE DATA OF RAMP "H-B" AND S.B.H.P. SERVICE ROAD SEE SHEET 20.
REUSABLE STONE CURBS AFFECTED BY THE NEW CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 95.3 AS ORDERED BY THE ENGINEER.
(12) DENOTES CURVE NO. DATA ON SHEET 20.
FOR LOCATION WHERE EXISTING HUMBOLDT PARKWAY PAVEMENT SHALL BE LEFT IN PLACE SEE NOTE 4 AND TABLE ON SHEET NO. 21.

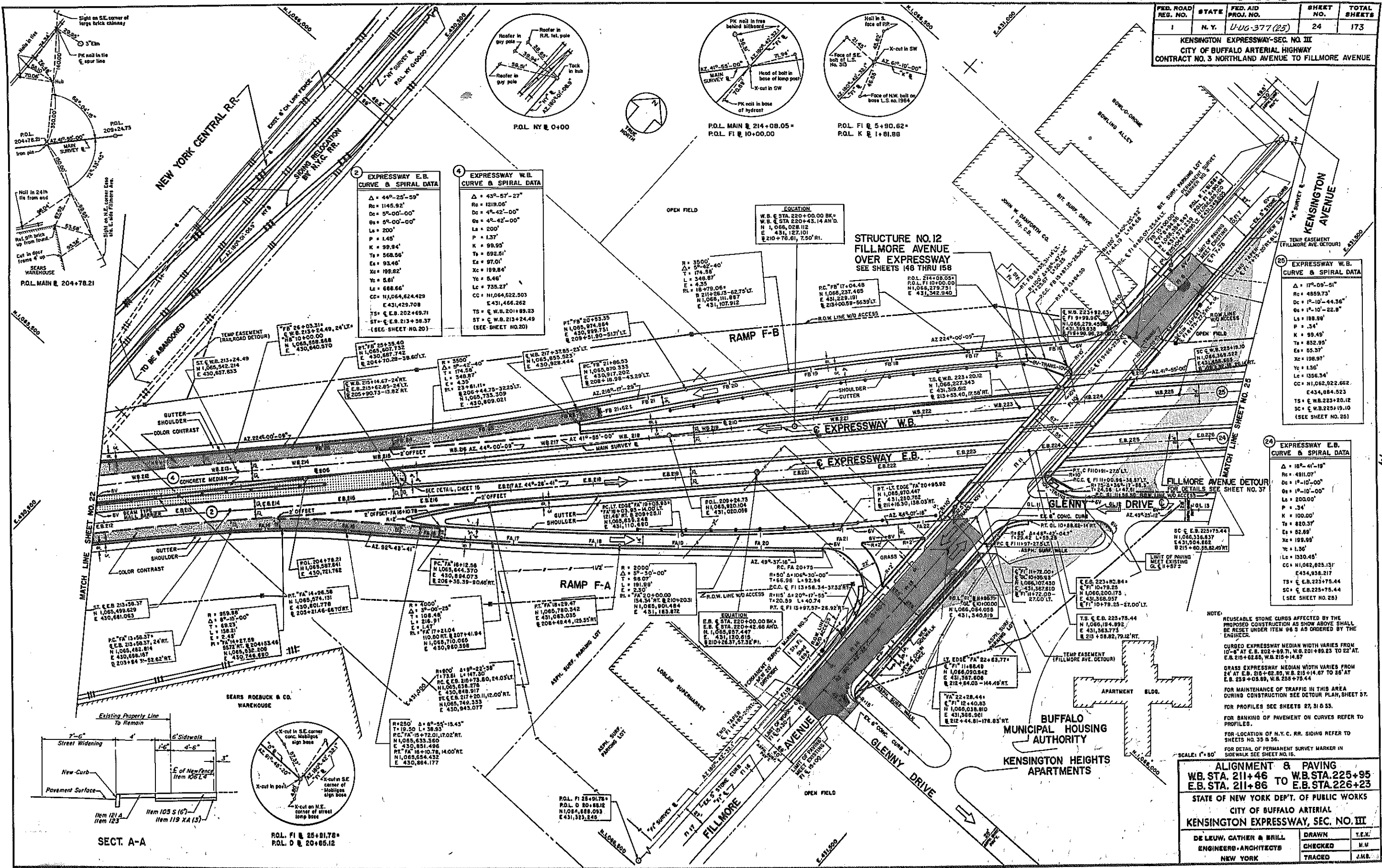
SCALE: 1" = 50'

ALIGNMENT & PAVING
DELAVAN INTERCHANGE CONNECTIONS
TO HUMBOLDT PARKWAY
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	TEK
ENGINEERS-ARCHITECTS	CHECKED	MM
	TRACED	JWB

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	24	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



EXPRESSWAY E.B. CURVE & SPIRAL DATA

$\Delta = 44^\circ-25'-59''$
 $R_c = 1145.92'$
 $D_c = 9^\circ-00'-00''$
 $O_c = 9^\circ-00'-00''$
 $L_c = 200'$
 $P = 1.45'$
 $K = 99.94'$
 $T_s = 368.96'$
 $E_s = 93.46'$
 $X_c = 199.82'$
 $Y_c = 5.61'$
 $L_c = 668.66'$
 $CC = N 1,064,624.429$
 $E = 431,466.262$
 $TS = E. B. 202+69.71$
 $ST = E. B. 213+58.37$
 (SEE SHEET NO. 20)

EXPRESSWAY W.B. CURVE & SPIRAL DATA

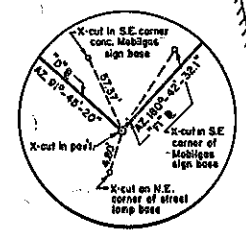
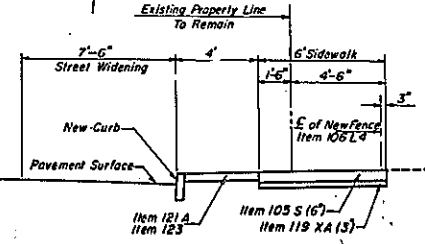
$\Delta = 43^\circ-57'-27''$
 $R_c = 1219.06'$
 $D_c = 4^\circ-42'-00''$
 $O_c = 4^\circ-42'-00''$
 $L_c = 200'$
 $P = 1.37'$
 $K = 99.95'$
 $T_s = 692.61'$
 $E_s = 97.01'$
 $X_c = 199.84'$
 $Y_c = 5.46'$
 $L_c = 735.27'$
 $CC = N 1,064,622.503$
 $E = 431,466.262$
 $TS = E. W. B. 201+89.23$
 $ST = E. W. B. 213+24.49$
 (SEE SHEET NO. 20)

EXPRESSWAY W.B. CURVE & SPIRAL DATA

$\Delta = 17^\circ-09'-51''$
 $R_c = 4659.73'$
 $D_c = 1^\circ-10'-44.36''$
 $O_c = 1^\circ-10'-22.6''$
 $L_c = 189.90'$
 $P = .34'$
 $K = 99.49'$
 $T_s = 852.95'$
 $E_s = 55.37'$
 $X_c = 198.97'$
 $Y_c = 1.56'$
 $L_c = 1356.34'$
 $CC = N 1,062,922.662$
 $E = 434,084.523$
 $TS = E. W. B. 223+20.12$
 $SC = E. W. B. 225+19.10$
 (SEE SHEET NO. 20)

EXPRESSWAY E.B. CURVE & SPIRAL DATA

$\Delta = 18^\circ-41'-19''$
 $R_c = 4811.07'$
 $D_c = 1^\circ-10'-00''$
 $O_c = 1^\circ-10'-00''$
 $L_c = 200.00'$
 $P = .34'$
 $K = 100.00'$
 $T_s = 820.37'$
 $E_s = 82.68'$
 $X_c = 199.99'$
 $Y_c = 1.50'$
 $L_c = 1330.45'$
 $CC = N 1,062,925.13'$
 $E = 434,085.217$
 $TS = E. B. 223+75.44$
 $SC = E. B. 225+19.44$
 (SEE SHEET NO. 20)



NOTE:
 REUSABLE STONE CURBS AFFECTED BY THE PROPOSED CONSTRUCTION AS SHOWN ABOVE SHALL BE RESET UNDER ITEM 98 AS ORDERED BY THE ENGINEER.
 CURVED EXPRESSWAY MEDIAN WIDTH VARIES FROM 10'-0" AT E. B. 202+89.71, W. B. 201+89.23 TO 22' AT E. B. 215+62.85, W. B. 215+14.87
 GRASS EXPRESSWAY MEDIAN WIDTH VARIES FROM 24' AT E. B. 215+62.85, W. B. 215+14.87 TO 26' AT E. B. 239+05.69, W. B. 239+75.44
 FOR MAINTENANCE OF TRAFFIC IN THIS AREA DURING CONSTRUCTION SEE DETOUR PLAN, SHEET 57.
 FOR PROFILES SEE SHEETS 27, 31 & 33.
 FOR BANKING OF PAVEMENT ON CURVES REFER TO PROFILES.
 FOR LOCATION OF N. Y. C. R.R. SIGNS REFER TO SHEETS NO. 35 & 36.
 FOR DETAIL OF PERMANENT SURVEY MARKER IN SIDEWALK SEE SHEET NO. 10.

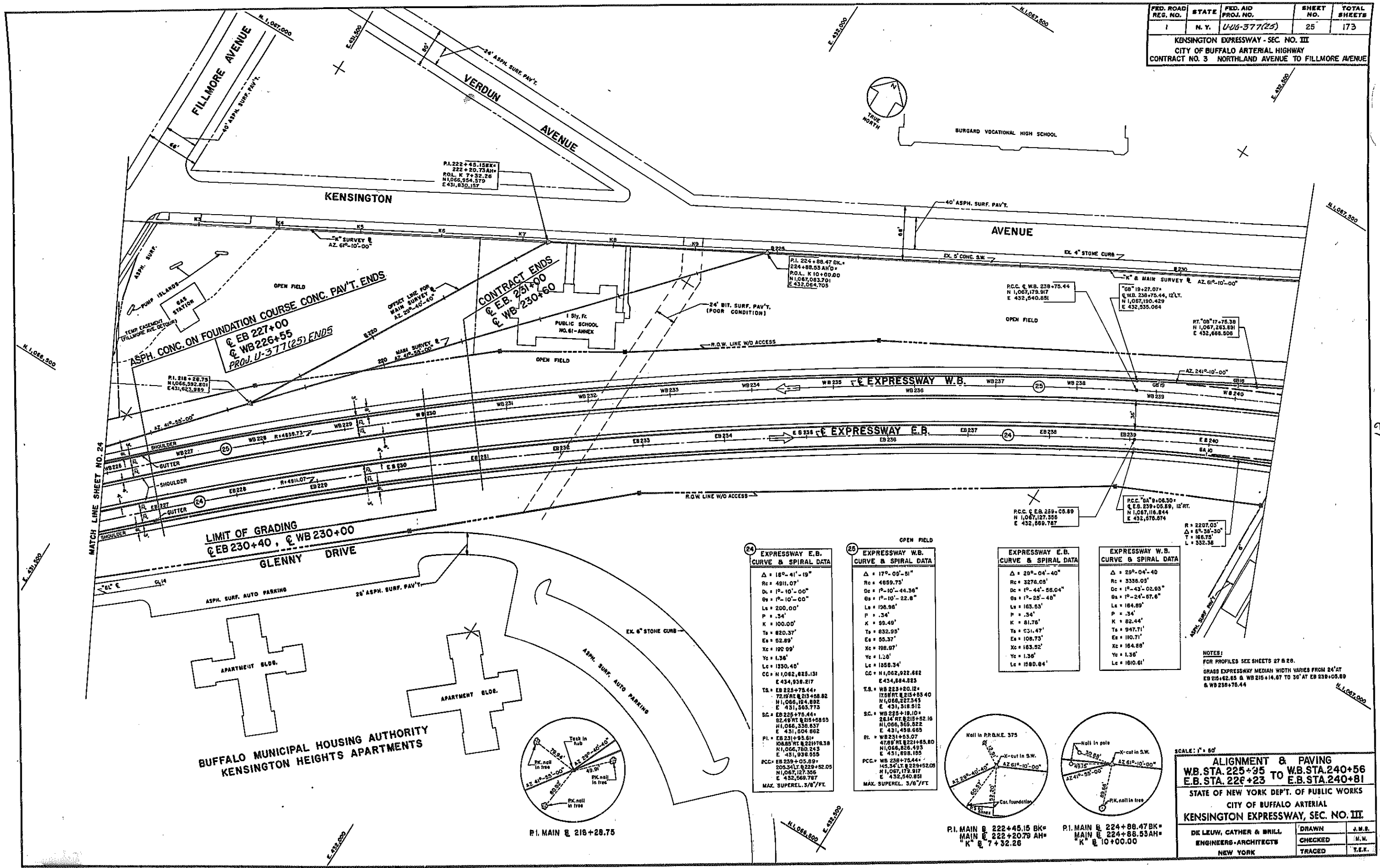
ALIGNMENT & PAVING
 W.B. STA. 211+46 TO W.B. STA. 225+95
 E.B. STA. 211+86 TO E.B. STA. 226+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	M.M.
NEW YORK	TRACED	J.M.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	25	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



CONTRACT ENDS @ E.B. 221+00 & W.B. 230+60

ASPH. CONC. ON FOUNDATION COURSE CONCR. PAV'T. ENDS @ EB 227+00 & WB 226+55 PROJ. U-377(25) ENDS

LIMIT OF GRADING @ EB 230+40, & WB 230+00

BUFFALO MUNICIPAL HOUSING AUTHORITY
KENSINGTON HEIGHTS APARTMENTS

24 EXPRESSWAY E.B. CURVE & SPIRAL DATA

Δ	18°-41'-19"
Rc	4911.07'
Dc	1°-10'-00"
Os	1°-10'-00"
Ls	200.00'
P	.34'
K	100.00'
Ts	820.37'
Es	82.89'
Xc	100.00'
Yc	1.36'
Lc	1350.40'
CC	N 1,062,685.131 E 434,938.817
Ts	EB 225+75.44
PC	N 1,066,184.892 E 431,505.773
BC	EB 225+76.44
PI	EB 231+95.61
PCC	EB 229+05.89
MAX. SUPERELEV.	3/8'/FT.

25 EXPRESSWAY W.B. CURVE & SPIRAL DATA

Δ	17°-00'-51"
Rc	4659.75'
Dc	1°-10'-44.36"
Os	1°-10'-22.8"
Ls	126.98'
P	.34'
K	63.49'
Ts	832.95'
Es	82.37'
Xc	108.07'
Yc	1.36'
Lc	1358.34'
CC	N 1,062,922.662 E 434,684.823
Ts	WB 223+20.12
PC	N 1,066,227.343 E 431,318.512
BC	WB 223+18.04
PI	WB 231+85.80
PCC	WB 229+05.89
MAX. SUPERELEV.	3/8'/FT.

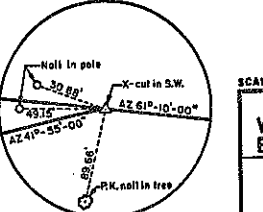
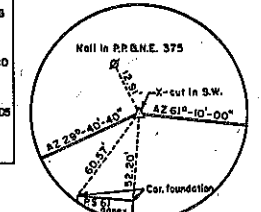
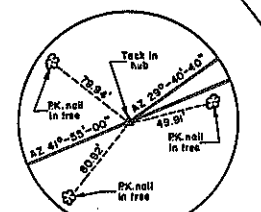
EXPRESSWAY E.B. CURVE & SPIRAL DATA

Δ	29°-04'-40"
Rc	3276.05'
Dc	1°-44'-56.04"
Os	1°-24'-48"
Ls	163.53'
P	.34'
K	81.76'
Ts	831.47'
Es	109.73'
Xc	163.92'
Yc	1.36'
Lc	1680.84'

EXPRESSWAY W.B. CURVE & SPIRAL DATA

Δ	29°-04'-40"
Rc	3336.05'
Dc	1°-43'-02.83"
Os	1°-24'-07.6"
Ls	163.53'
P	.34'
K	82.44'
Ts	947.71'
Es	110.71'
Xc	164.89'
Yc	1.36'
Lc	1610.61'

NOTES:
 FOR PROFILED SEE SHEETS 27 & 28.
 GRASS EXPRESSWAY MEDIAN WIDTH VARIES FROM 24' AT EB 215+62.65 & WB 215+14.07 TO 30' AT EB 239+05.89 & WB 230+75.44



SCALE: 1" = 50'

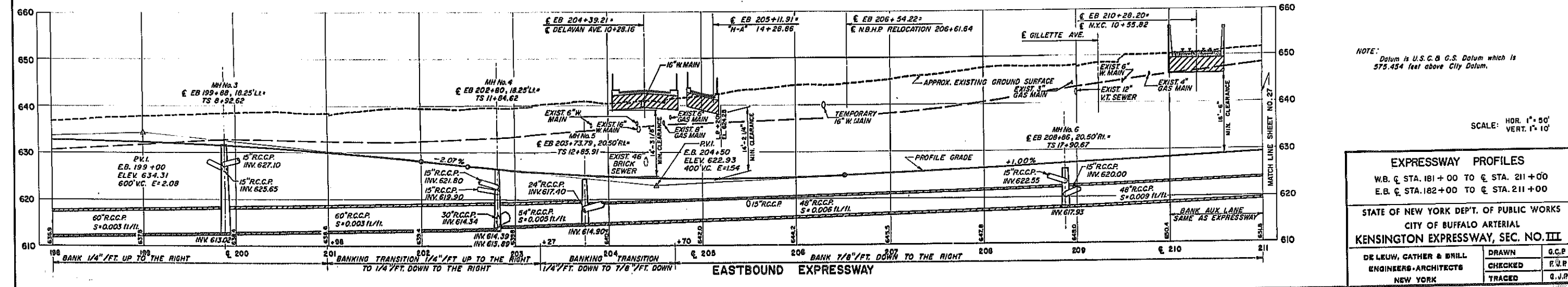
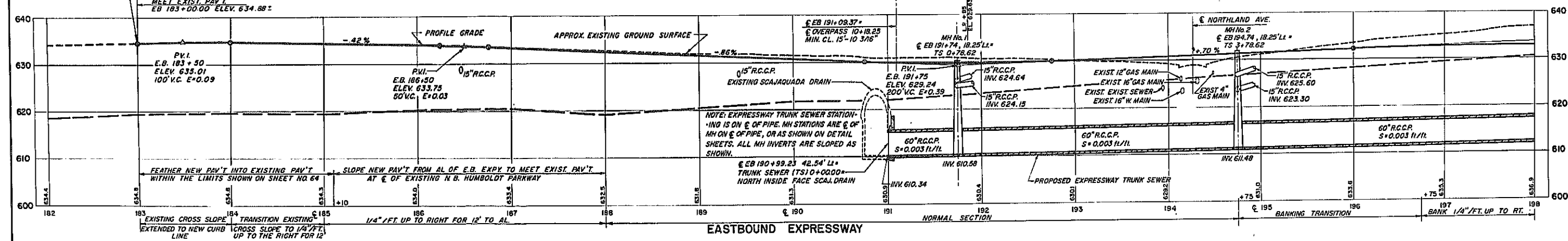
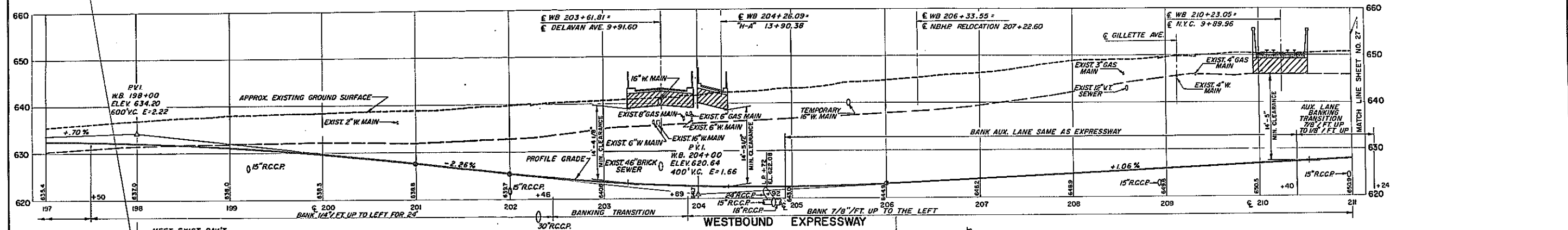
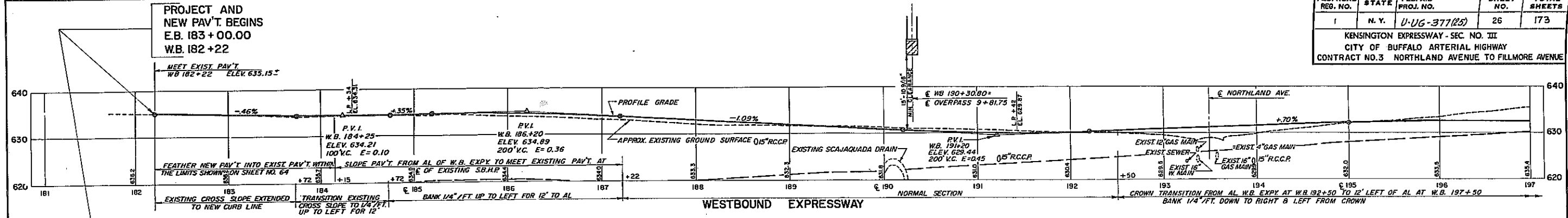
ALIGNMENT & PAVING
W.B. STA. 225+35 TO W.B. STA. 240+56
E.B. STA. 226+23 TO E.B. STA. 240+81

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CAYHER & BRILL	DRAWN	J.M.B.
ENGINEERS-ARCHITECTS	CHECKED	H.M.
NEW YORK	TRACED	T.E.K.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	26	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: Datum is U.S.C. & G.S. Datum which is 575.454 feet above City Datum.

SCALE: HOR. 1" = 50'
VERT. 1" = 10'

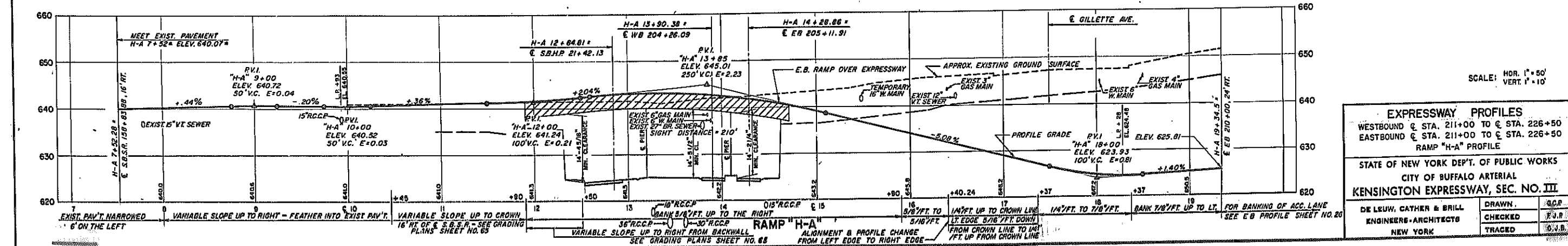
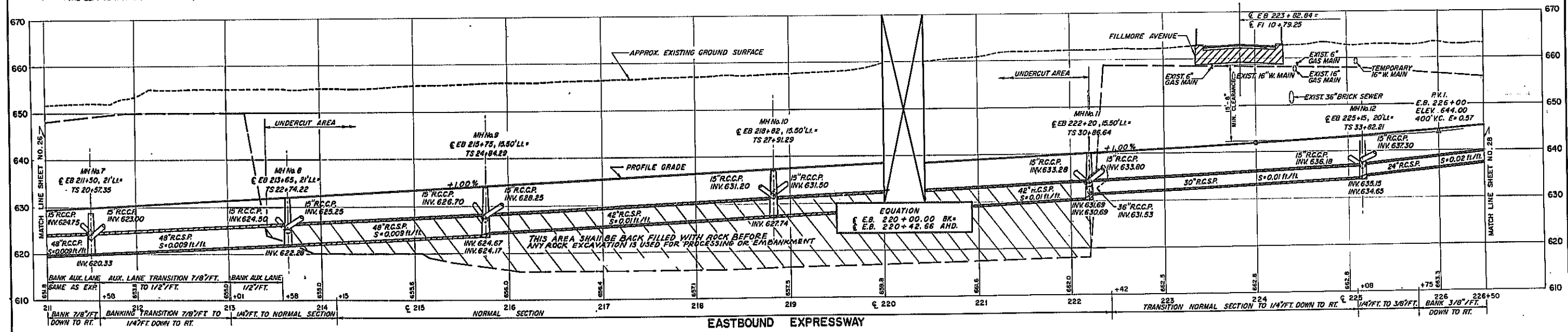
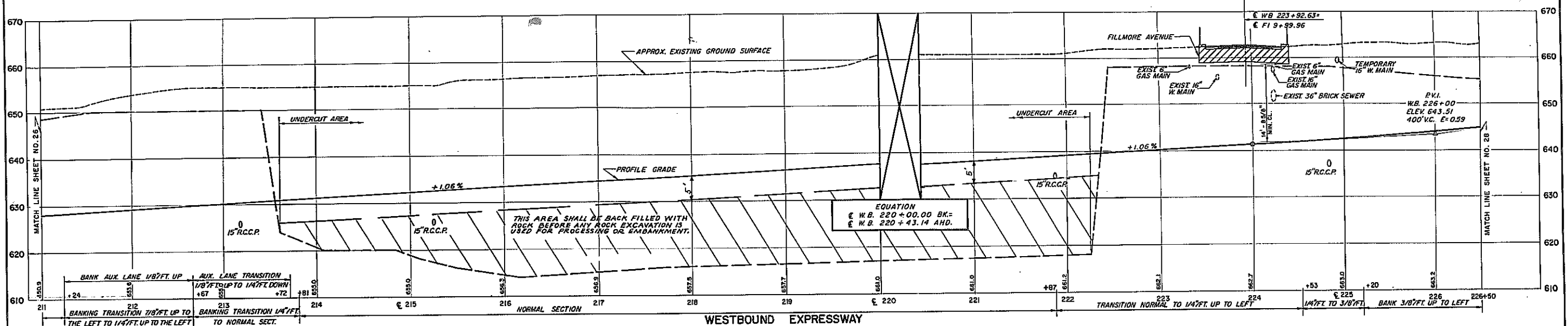
EXPRESSWAY PROFILES
W.B. STA. 181+00 TO STA. 211+00
E.B. STA. 182+00 TO STA. 211+00

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL	DRAWN	G.C.P.
ENGINEERS-ARCHITECTS	CHECKED	F.J.P.
NEW YORK	TRACED	G.J.P.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	27	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCALE: HOR. 1"=50'
VERT. 1"=10'

EXPRESSWAY PROFILES
WESTBOUND E. STA. 211+00 TO E. STA. 226+50
EASTBOUND E. STA. 211+00 TO E. STA. 226+50
RAMP "H-A" PROFILE

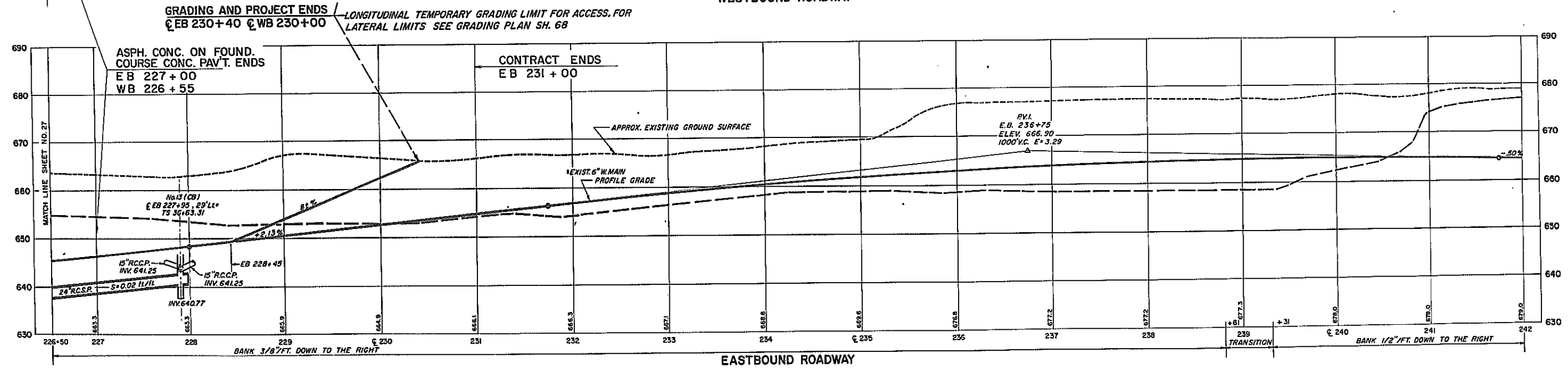
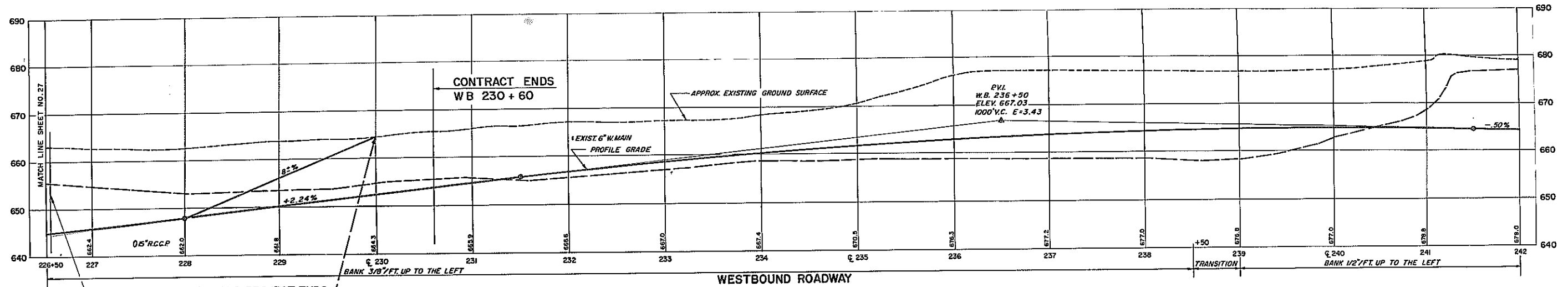
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK

DRAWN: G.C.P.
CHECKED: F.J.B.
TRACED: G.J.B.

FED. ROAD RES. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	28	173

KENSINGTON EXPRESSWAY - SEC. NO. III & IIIA
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCALE: HOR. 1" = 50'
VERT. 1" = 10'

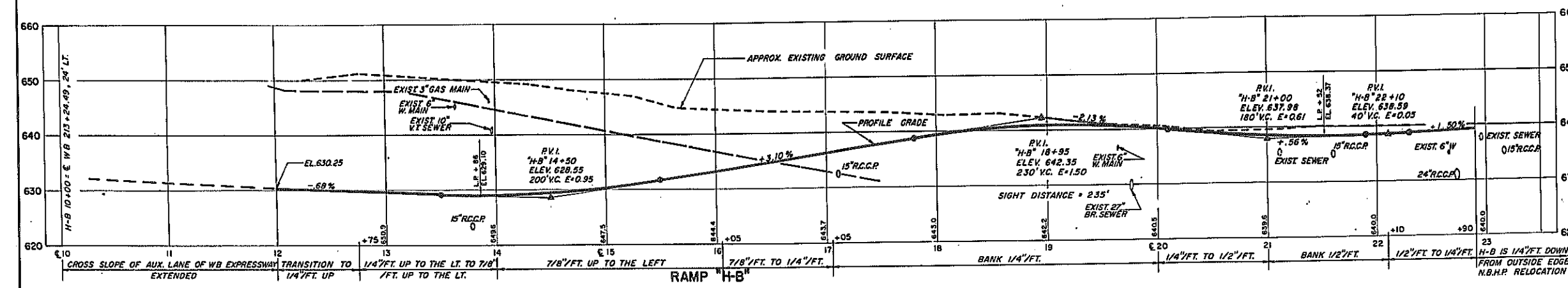
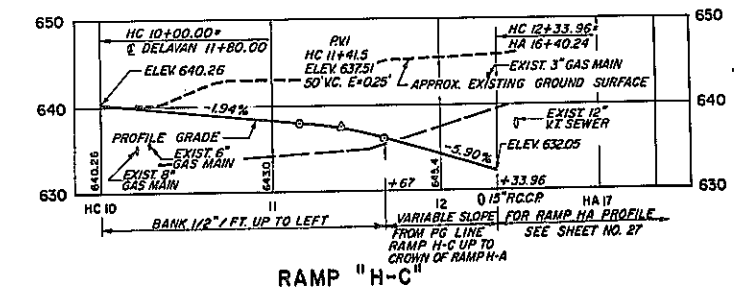
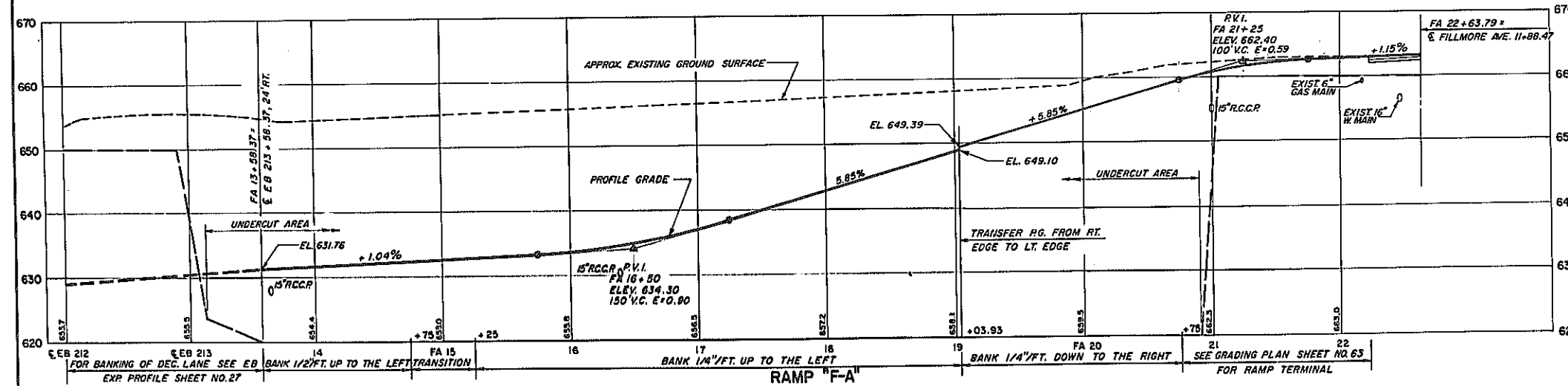
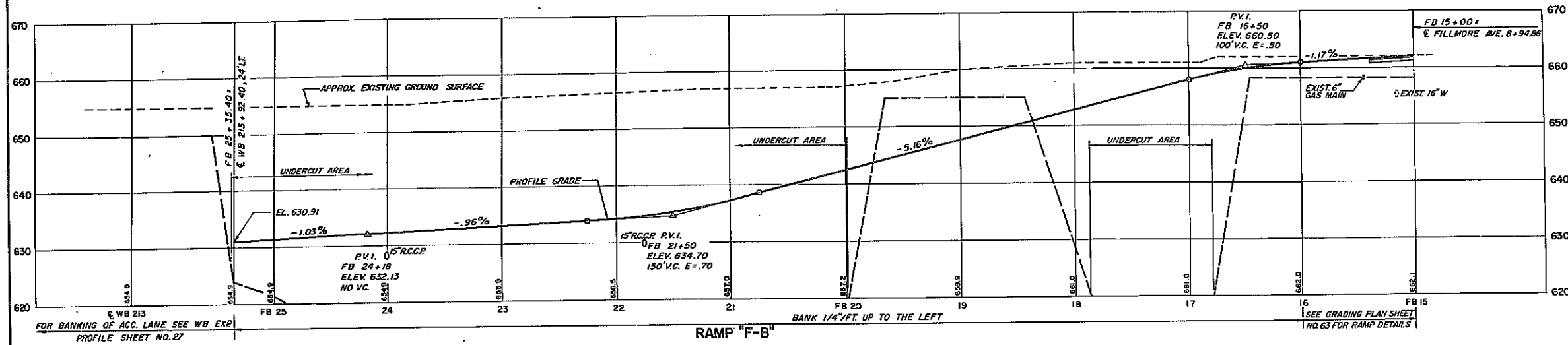
EXPRESSWAY PROFILES
WESTBOUND @ STA. 226+50 TO @ STA. 242+00
EASTBOUND @ STA. 226+50 TO @ STA. 242+00

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G.C.P. F.V.P. O.J.B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	31	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCALE: HOR. 1" = 50'
VERT. 1" = 10'

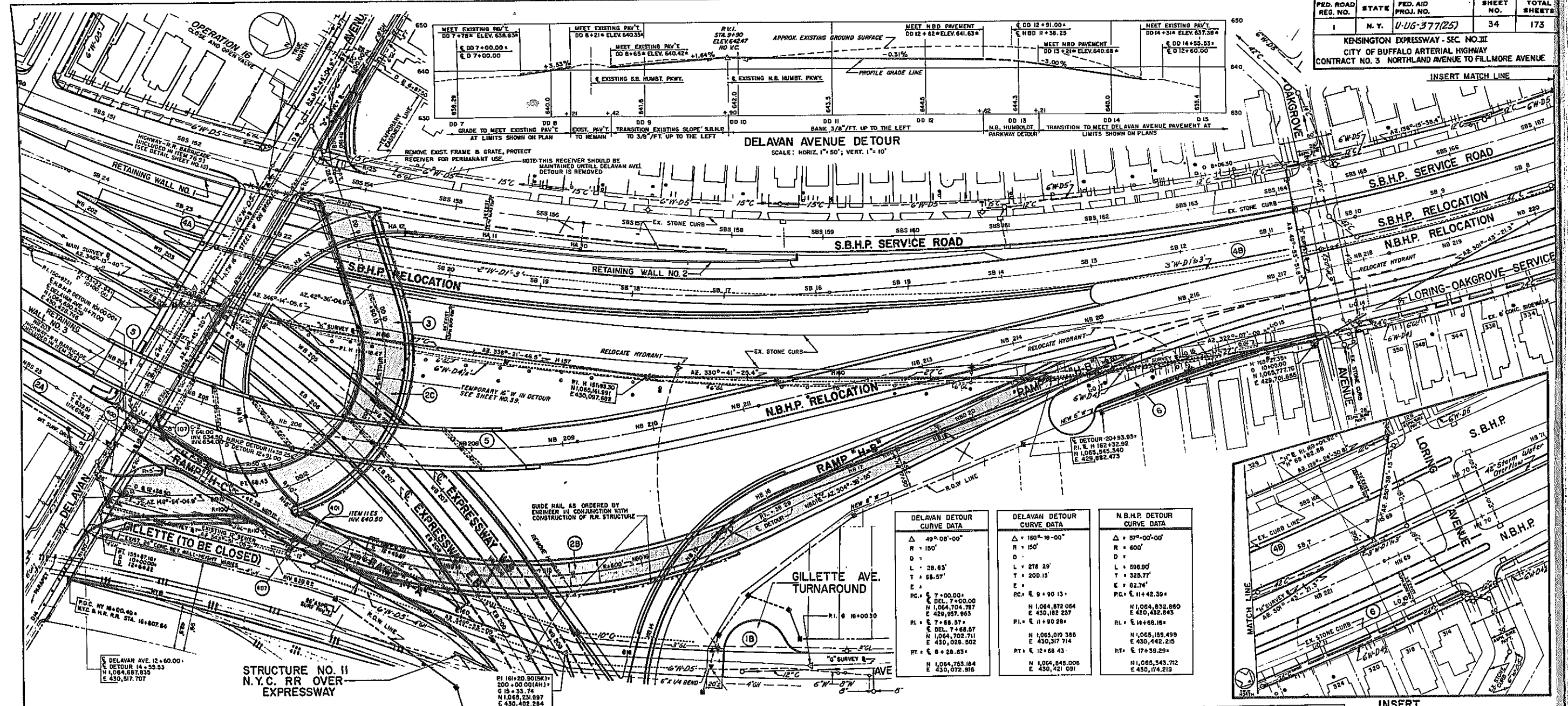
RAMP PROFILES
RAMP "F-A" & "F-B" PROFILE
RAMP "H-B" & "H-C" PROFILE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

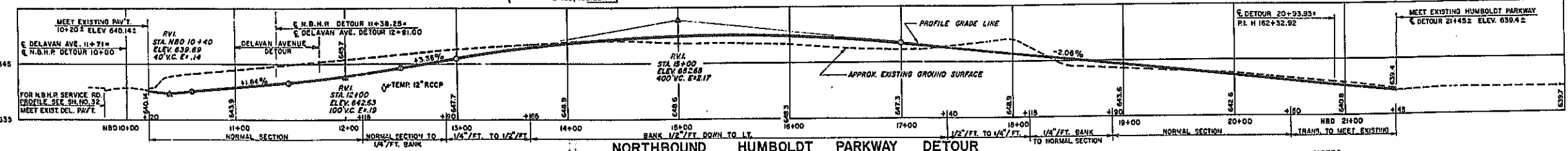
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	G. E. P. M. M. G. J. B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	34	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



DELAVAN DETOUR CURVE DATA	DELAVAN DETOUR CURVE DATA	N.B.H.P. DETOUR CURVE DATA
Δ = 49° 08' 00"	Δ = 160° 18' 00"	Δ = 57° 00' 00"
R = 150'	R = 150'	R = 600'
D = 150'	D = 150'	D = 600'
L = 28.63'	L = 278.25'	L = 296.90'
T = 66.57'	T = 200.15'	T = 325.77'
E = 7+00.00	E = 9+90.13	E = 62.74'
PC = 7+00.00	PC = 9+90.13	PC = 11+42.39'
DEL. 7+00.00	DEL. 9+90.13	DEL. 11+42.39'
N 1,064,704.787	N 1,064,872.064	N 1,064,832.860
E 429,937.053	E 430,182.237	E 430,432.545
PL = 7+68.37'	PL = 11+90.28'	PL = 14+68.18'
DEL. 7+68.37'	DEL. 11+90.28'	DEL. 14+68.18'
N 1,064,702.711	N 1,065,019.388	N 1,065,159.499
E 430,028.002	E 430,317.714	E 430,442.215
PT = 8+28.63'	PT = 12+68.43'	PT = 17+39.29'
N 1,064,753.184	N 1,064,848.006	N 1,065,343.712
E 430,072.916	E 430,421.091	E 430,174.219



- (1A) CONSTRUCT E.B. EXPRESSWAY PAVEMENT AND REQUIRED DRAINAGE AND PEDESTRIAN OVERPASS SUBSTRUCTURE WEST OF EXISTING N.B.H.P. IN EAST HALF OF EXISTING MEDIAN OF HUMBOLDT PARKWAY FROM SIDNEY ST. TO STA. E.B. 196+50 AS SHOWN ON SHEET NO. 21. CLOSE BRUNSWICK AND NORTHLAND AVENUES ACROSS HUMBOLDT PARKWAY.
- (1B) CONSTRUCT GILLETTE AVENUE TURNAROUND.
- (1C) DIVERT N.B.H.P. TRAFFIC TO 1A, THENCE BACK TO EXISTING N.B.H.P. AT STA. E.B. 196+50.
- (1D) CONSTRUCT N.B.H.P. SERVICE ROAD FROM SIDNEY ST. TO DELAVAN AVENUE INCLUDING NECESSARY PARTS OF RETAINING WALL NO. 3 AND PEDESTRIAN OVERPASS STRUCTURE.
- (1E) CONSTRUCT TEMPORARY PAVEMENT FOR N.B.H.P. DETOUR FROM DELAVAN AVENUE NORTH TO EXISTING N.B.H.P. AS SHOWN ON PLAN.

- SUGGESTED PROCEDURE**
- (2C) CONSTRUCT DELAVAN AVENUE DETOUR AS SHOWN ON PLAN, EXCEPT FOR THE PORTION CROSSING EXISTING N.B. HUMBOLDT PARKWAY.
 - (3) DIVERT N.B.H.P. TRAFFIC TO 2A AND 2B. TO EXISTING N.B.H.P. AT BASELINE STA. 1163+30 AND COMPLETE DELAVAN AVENUE DETOUR. DIVERT TRAFFIC TO DELAVAN AVENUE DETOUR.
 - (4A) CONSTRUCT DELAVAN AVENUE STRUCTURE AND PARTS OF EXPRESSWAY AND RAMP "H.A." AND N.B.H.P. STRUCTURES.
 - (4B) CONSTRUCT HUMBOLDT PARKWAY RELOCATIONS NORTH OF DELAVAN AVENUE DETOUR (NORTH OF BASELINE STA. 1163+50 ONLY THAT PORTION WEST OF PRESENT N.B.H.P. MAY BE BUILT). DIVERT DELAVAN AVENUE TRAFFIC FROM DETOUR TO COMPLETED DELAVAN AVENUE STRUCTURE.

- NORTHBOUND HUMBOLDT PARKWAY DETOUR**
SCALE: HORIZ. 1" = 50'; VERT. 1" = 10'
- (5) COMPLETE CONSTRUCTION OF N.B.H.P. STRUCTURE AND APPROACHES. DIVERT N.B.H.P. TRAFFIC FROM DETOUR TO COMPLETED N.B.H.P. RELOCATION.
 - (6) CONSTRUCT LORING-OAKGROVE SERVICE ROAD AND (EAST) 4TH LANE OF N.B.H.P. RELOCATION NORTH OF BASELINE STA. 1163+30.
- MAINTENANCE OF TRAFFIC SCHEDULE APPROVED**
Edward D. Baxter
COM. OF PUBLIC WORKS
CITY OF BUFFALO

- NOTES**
1. S.B.H.P. TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION. N.B.H.P., OAKGROVE AND LORING AVENUES SHALL BE MAINTAINED UNTIL N.B.H.P. RELOCATION AND LORING-OAKGROVE SERVICE ROAD (TWO WAY SERVICE) ARE OPERATIVE.
 2. DETAILS OF DETOUR ROADWAY SECTIONS ARE SHOWN ON SHEET NO. 8.
 3. CONTRACTOR SHALL PROVIDE LIGHTING TO THE AMOUNT OF 0.5 LUMENS PER SQUARE FOOT FOR SIDEWALKS AND ROADWAYS UNDER ITEM 765.
 4. CONTRACTOR SHALL MAINTAIN THE EXISTING DELAVAN AVE. SEWER UNTIL PERMANENT CONNECTION TO THE EXPRESSWAY TRUNK SEWER HAS BEEN MADE.
 5. FOR ALIGNMENT DATA FOR COMPLETE IMPROVEMENT OF DELAVAN, N.B.H.P., O.B.H.P. AND EXPRESSWAY, SEE SHEET NO. 20.
 6. FOR DETAILS OF TRAFFIC CONTROL SEE SHEET NO. 60.
 7. FOR WATER OPERATIONS IN CONNECTION WITH THE DELAVAN AVENUE DETOUR SEE SHEET NO. 39.

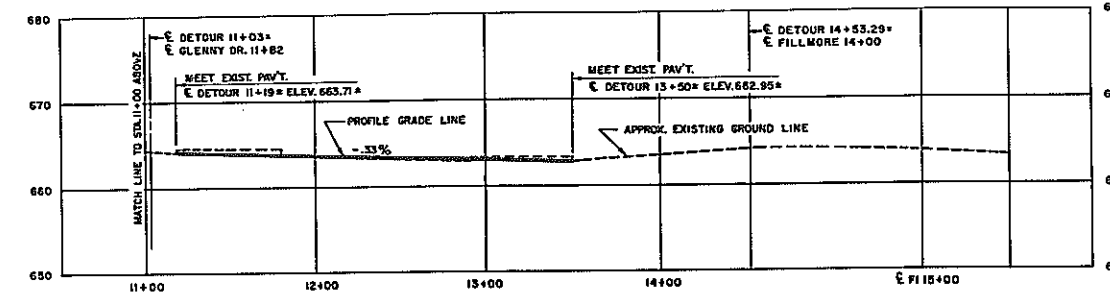
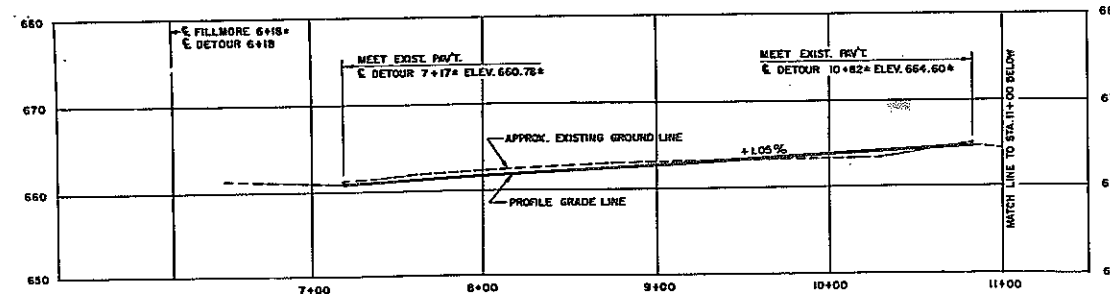
- INSERT**
SOUTHBOUND HUMBOLDT PARKWAY
1. S.B.H.P. TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
 2. THE WIDENING OF S.B.H.P. SERVICE RD. BETWEEN STA. S.B.S. 143+00 TO S.B.S. 150+00 SHALL BE PERFORMED PRIOR TO EXCAVATING THE BACK SLOPE FOR RETAINING WALL NO. 1, IN ORDER TO MAINTAIN 2-12' LANES DURING CONSTRUCTION.
 3. PAVING AT S.B.H.P. SERVICE RD. - DELAVAN AVE. INTERSECTION SHALL BE CONSTRUCTED IN TWO SECTIONS TO MAINTAIN TRAFFIC ON S.B.H.P. SERVICE RD.
 4. TEMPORARY GUIDE RAILING WILL BE ERCTED ON S.B.H.P. SERVICE RD. DURING CONSTRUCTION OF RETAINING WALLS NO. 1 & 2. (SEE SHEET NO. 13 FOR STATIONS)
- SCALE: 1" = 50'

DELAVAN AVENUE AND N.B.H.P. DETOURS
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

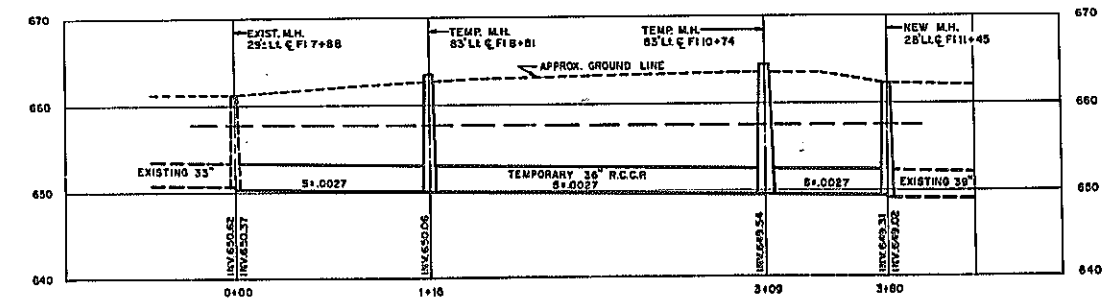
DE LEUW, CATHY & BRILL	DRAWN	T&K
ENGINEERS-ARCHITECTS	CHECKED	N.M.
NEW YORK	TRACED	J.H.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	37	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



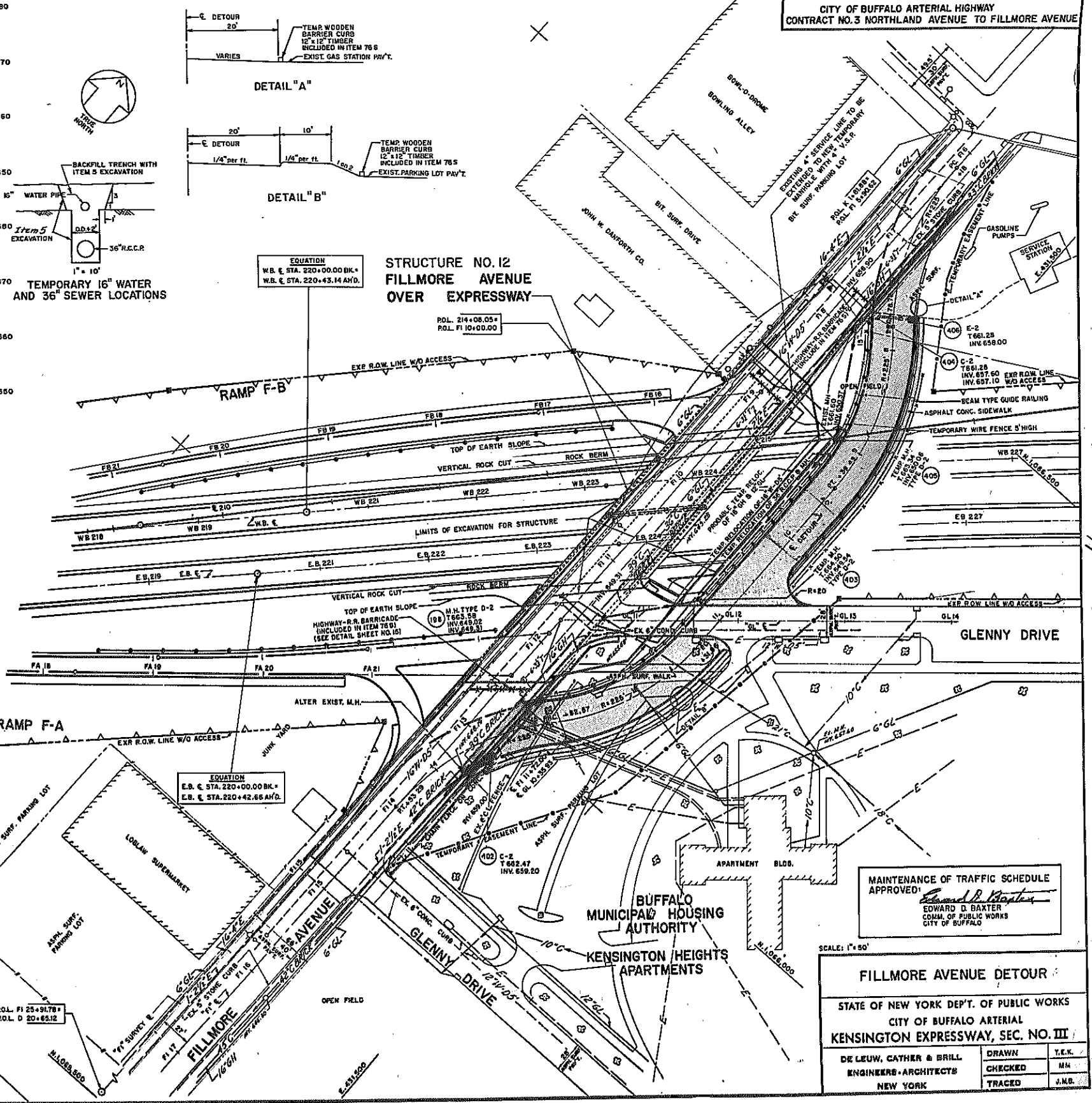
FILLMORE AVE. DETOUR PROFILE
HOR. 1" = 50'
VERT. 1" = 10'



TEMPORARY 36" SEWER
HOR. 1" = 50'
VERT. 1" = 10'

- NOTES:
- Details of typical section for detour is shown on sheet B.
 - Contractor shall provide lighting to the amount of 0.5 lumens per square foot for sidewalks and roadway of the detour under item 705.
 - Contractor shall maintain existing Fillmore Ave. sewer until temporary sewer and permanent connection to trunk sewer has been made.
 - For permanent location of Fillmore Ave. sewer see sheet 41.
 - All existing curb, sidewalk, fence, paved areas disturbed by detour construction are to be replaced under appropriate items.
 - Provide temporary sewer service for John W. Danforth Co. by extending the existing sewer service to temp. M.H. 83' Lt. & Fl 8-81.
 - Upon completion of temporary 36" sewer in detour, plug existing 36" sewer at the south level of existing M.H. at Fl 7+88, and the north level of new M.H. at Fl 11+45.
 - Upon completion of permanent Fillmore Ave. sewer to Expressway trunk sewer, unplug existing 36" sewer at M.H. at Fl 7+88 and plug temporary 36" sewer at M.H. at Fl 7+88 and Fl 11+45.
 - For legend see sheet 19.

FILLMORE AVE DETOUR CURVE DATA	
Δ	40°-05'-34"
R	225'
T	83.50'
L	160.72'
E	15.16'
PC	ETOUR 6+18'
	FILLMORE 6+18'
PT	ETOUR 7+78.72'
	FILLMORE 9+38.43'
RC	ETOUR 9+12.79-110'LL
	FILLMORE 11+31.06'
PT	FILLMORE 11+08.21-110'LL
	ETOUR 12+92.57'
RC	ETOUR 14+53.29'
	FILLMORE 14+00'



MAINTENANCE OF TRAFFIC SCHEDULE APPROVED:
Edward D. Baxter
EDWARD D. BAXTER
COMM. OF PUBLIC WORKS
CITY OF BUFFALO

SCALE: 1" = 50'

FILLMORE AVENUE DETOUR

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. M.H. J.N.B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	38	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

GENERAL NOTES - WATER

The City of Buffalo, Division of Water, will furnish at the site all pipe materials and labor to make connections to existing mains at "OPERATIONS" shown on the plans. The cost of materials and labor to be paid by the New York State Department of Public Works to the Division of Water. All water services that are to be discontinued shall be abandoned at the water main. All excavating, pumping, back-filling and re-surfacing in connection with this work and of "OPERATIONS" by the Division of Water shall be done by the contractor. No separate payment will be made for temporary resurfacing or pumping. Necessary excavation will be paid for under ITEM 5.

The following work to be done and materials required to be furnished by the Contractor.

Hydrants to be relocated are to be removed and installed in new locations.

Adjust all hydrants, valve boxes and covers and manhole frames and covers to new grades.

Abandoned valve boxes shall be pulled, or top section broken off and filled in with concrete and area resurfaced.

All abandoned pipes, valves and hydrants and other appurtenances, which are not used in the new work are to remain the property of the City of Buffalo unless otherwise specified, and will be recovered by the Division of Water.

Any exposed openings created in the abandoned pipe left in place shall be plugged with brick and cement to a depth of 12" in such a manner that no material can enter. Payment will be made under Item 102.08 & 15-2A.

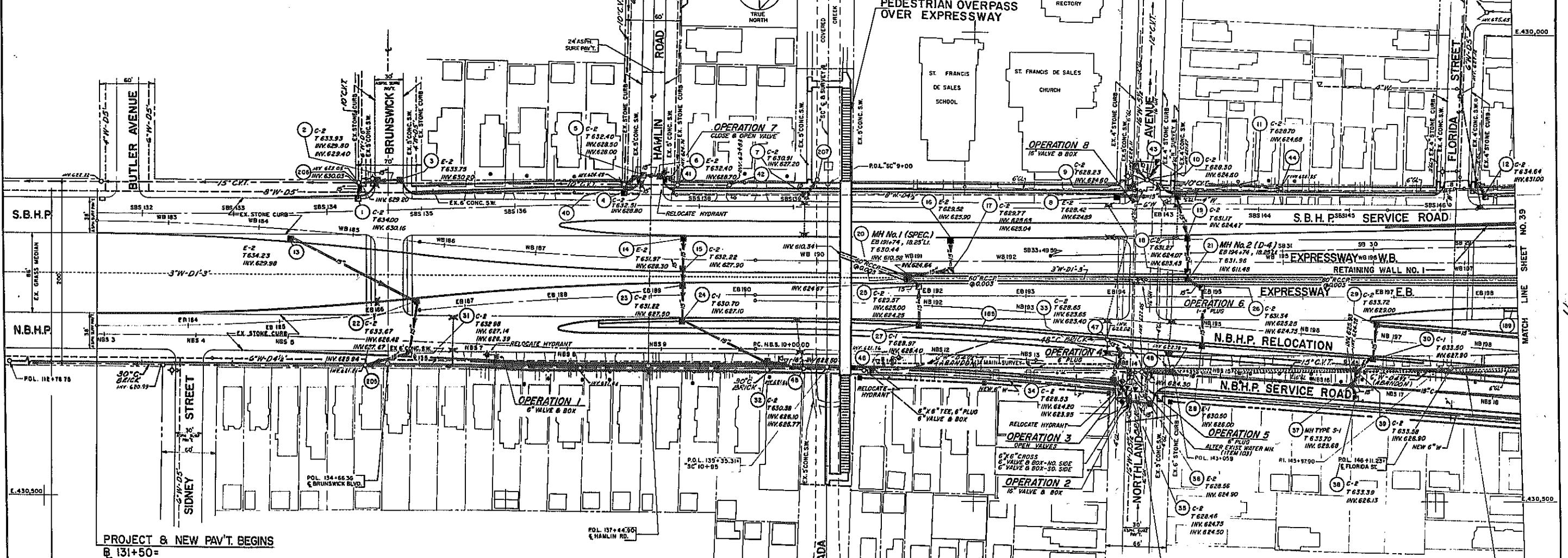
Contractor to excavate where ordered by the engineer to verify location of existing water lines.

Concrete Bracing and Anchor Straps shall be installed at all Bends and Plugs, including "OPERATIONS" performed by the City of Buffalo Division of Water.

The cost of such Bracing and Anchor Straps to be included in the bid price for the various pipes.

Payment for the above work will be made under the respective items described in the Specifications.

After acceptance of this contract by the State of New York, all new water mains and appurtenances shall be maintained by the Division of Water of the City of Buffalo.



PROJECT & NEW PAV'T. BEGINS
@ 131+50=
E.B. 183+00=
W.B. 182+22

GENERAL NOTES - SEWER

Frames & Covers from abandoned catch basins, manholes, etc. (not used at other locations) shall be removed and become property of Contractor.

Drainage Structures numbered 400 and over are Temporary Structures to provide acceptable Detours.

The contractor shall prove the locations of utilities at critical points in advance of major excavations for sewer construction.

Temporary Wire Fence, Item 500, shall be placed as shown on the plans or as ordered by the Engineer.

All new sewer pipes are R.C.C.P. (Item 14) unless otherwise noted.

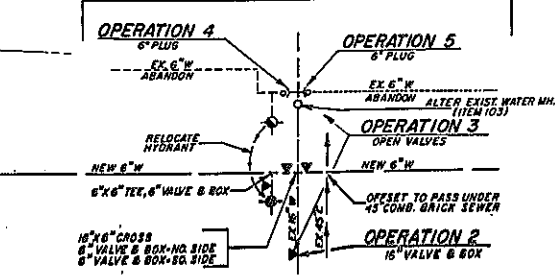
Where drainage pipes are to be placed in Fill Areas the fill shall be placed to two (2) feet above the pipe then excavation for the pipe shall be made as shown on Sheet No. 51.

SCALE: 1" = 50'

DRAINAGE & UTILITIES
W.B. STA. 183+00 TO W.B. STA. 197+54
E.B. STA. 182+21 TO E.B. STA. 198+23

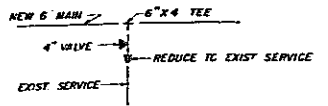
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL	DRAWN	T.E.L.
ENGINEERS-ARCHITECTS	CHECKED	J.K.
NEW YORK	TRACED	J.M.B.



FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	39	173

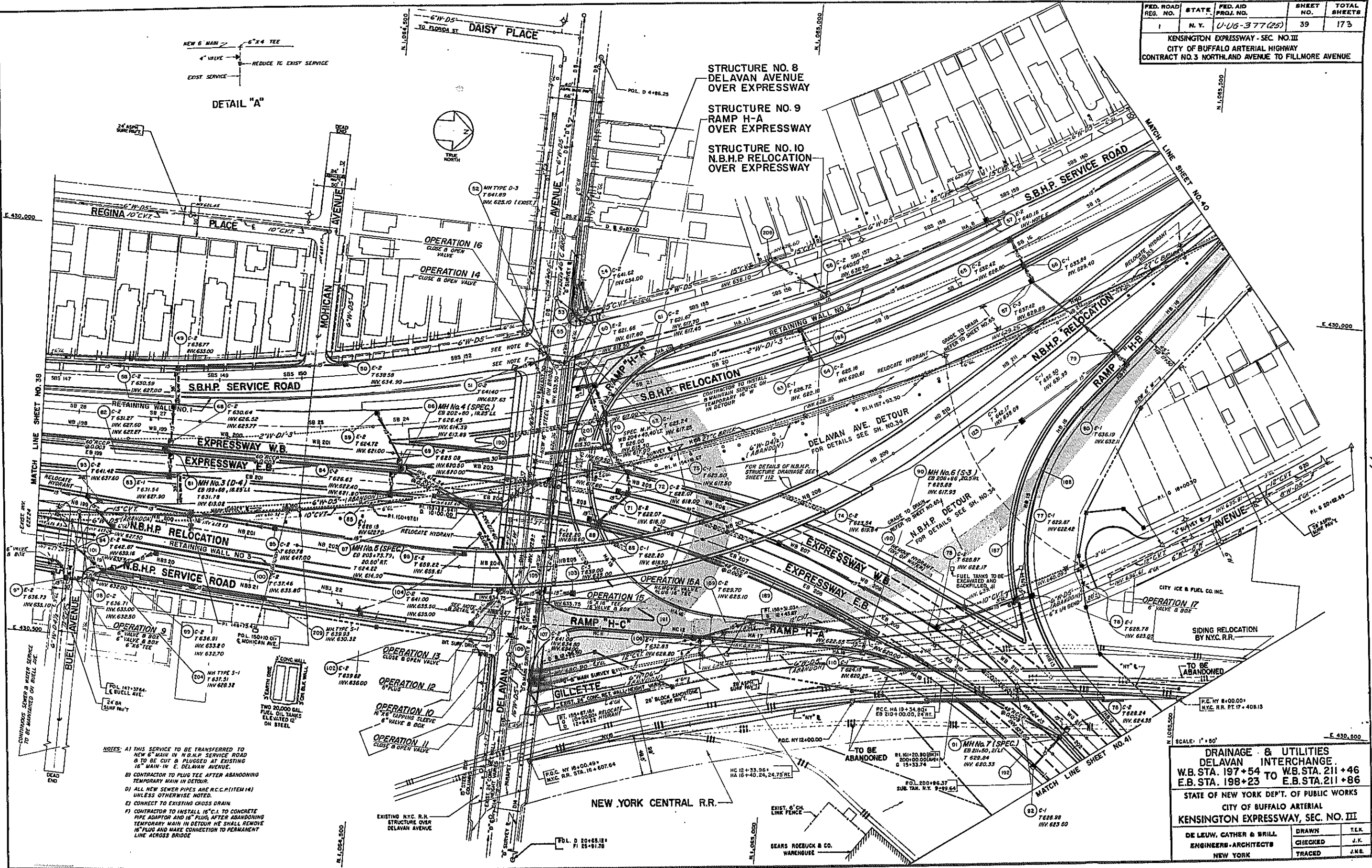
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



STRUCTURE NO. 8
DELANVAN AVENUE
OVER EXPRESSWAY

STRUCTURE NO. 9
RAMP H-A
OVER EXPRESSWAY

STRUCTURE NO. 10
N.B.H.P. RELOCATION
OVER EXPRESSWAY



NOTES: A) THIS SERVICE TO BE TRANSFERRED TO NEW 6" MAIN IN N.B.H.P. SERVICE ROAD & TO BE CUT & PLUGGED AT EXISTING 16" MAIN IN E. DELAVAN AVENUE.

B) CONTRACTOR TO PLUG TEE AFTER ABANDONING TEMPORARY MAIN IN DETOUR.

C) ALL NEW SEWER PIPES ARE R.C.C.P. (ITEM 14) UNLESS OTHERWISE NOTED.

D) CONNECT TO EXISTING CROSS DRAIN.

E) CONTRACTOR TO INSTALL 16" C.I. TO CONCRETE PIPE ADAPTOR AND 16" PLUG, AFTER ABANDONING TEMPORARY MAIN IN DETOUR HE SHALL REMOVE 16" PLUG AND MAKE CONNECTION TO PERMANENT LINE ACROSS BRIDGE.

SCALE: 1" = 50'

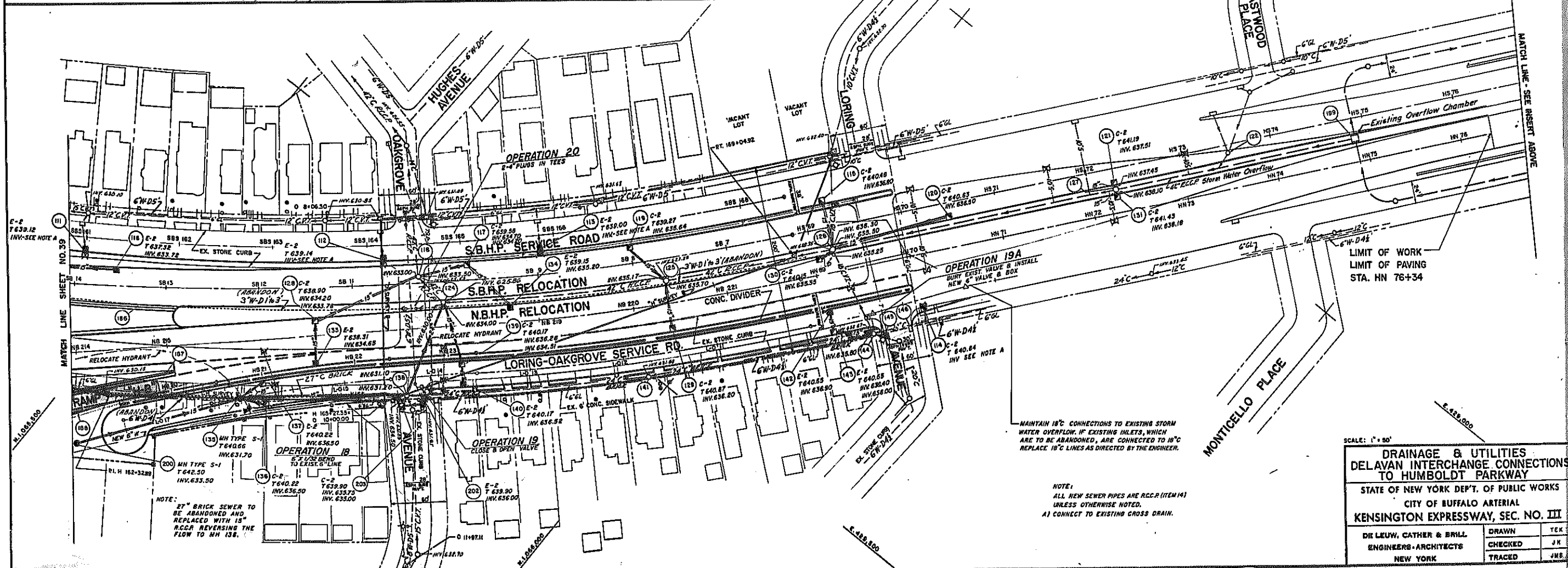
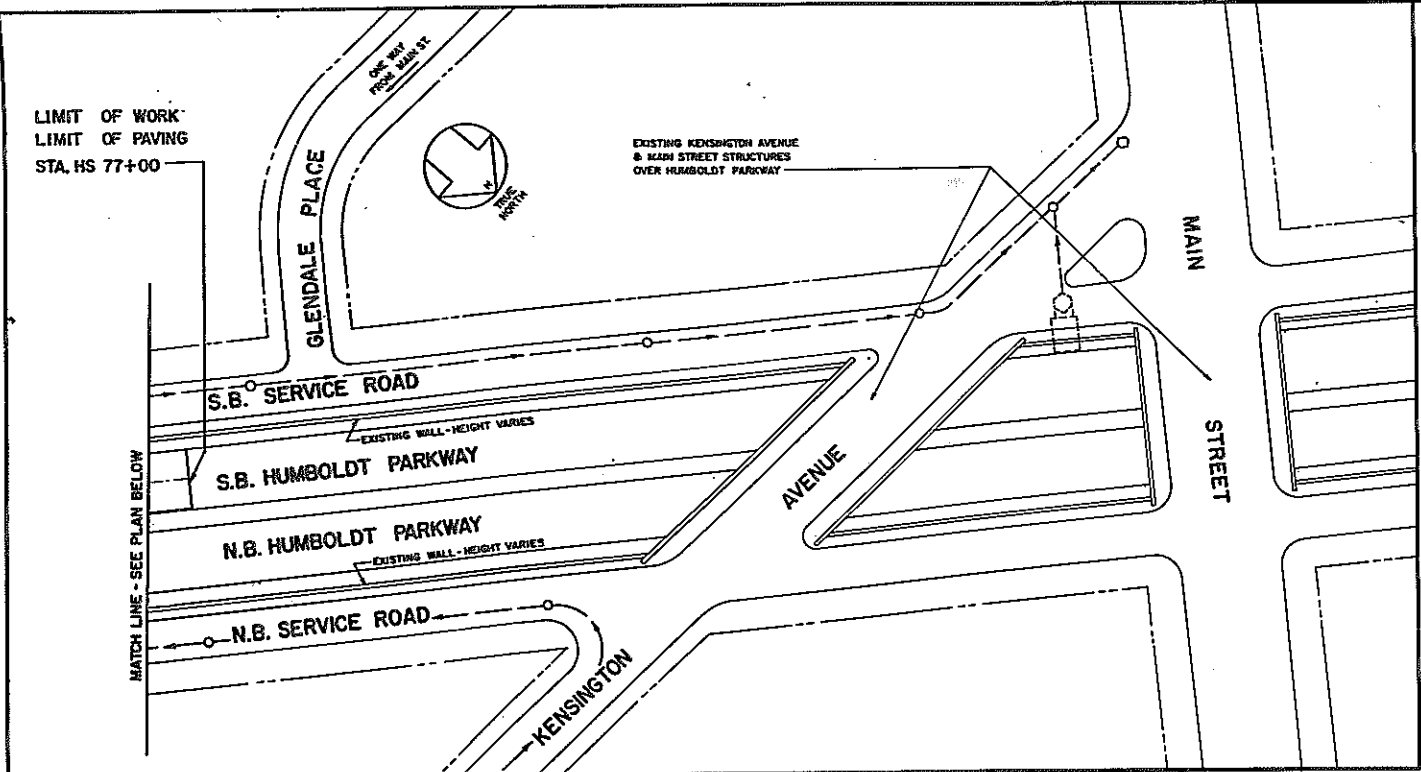
**DRAINAGE & UTILITIES
DELANVAN INTERCHANGE.**
W.B. STA. 197+54 TO W.B. STA. 211+46
E.B. STA. 198+23 TO E.B. STA. 211+86

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. J.K. J.K.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	40	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



MAINTAIN 18" CONNECTIONS TO EXISTING STORM WATER OVERFLOW. IF EXISTING INLETS, WHICH ARE TO BE ABANDONED, ARE CONNECTED TO 18" REPLACE 18" LINES AS DIRECTED BY THE ENGINEER.

NOTE:
ALL NEW SEWER PIPES ARE R.C.C.P. (ITEM 14) UNLESS OTHERWISE NOTED.
A) CONNECT TO EXISTING CROSS DRAIN.

SCALE: 1" = 50'

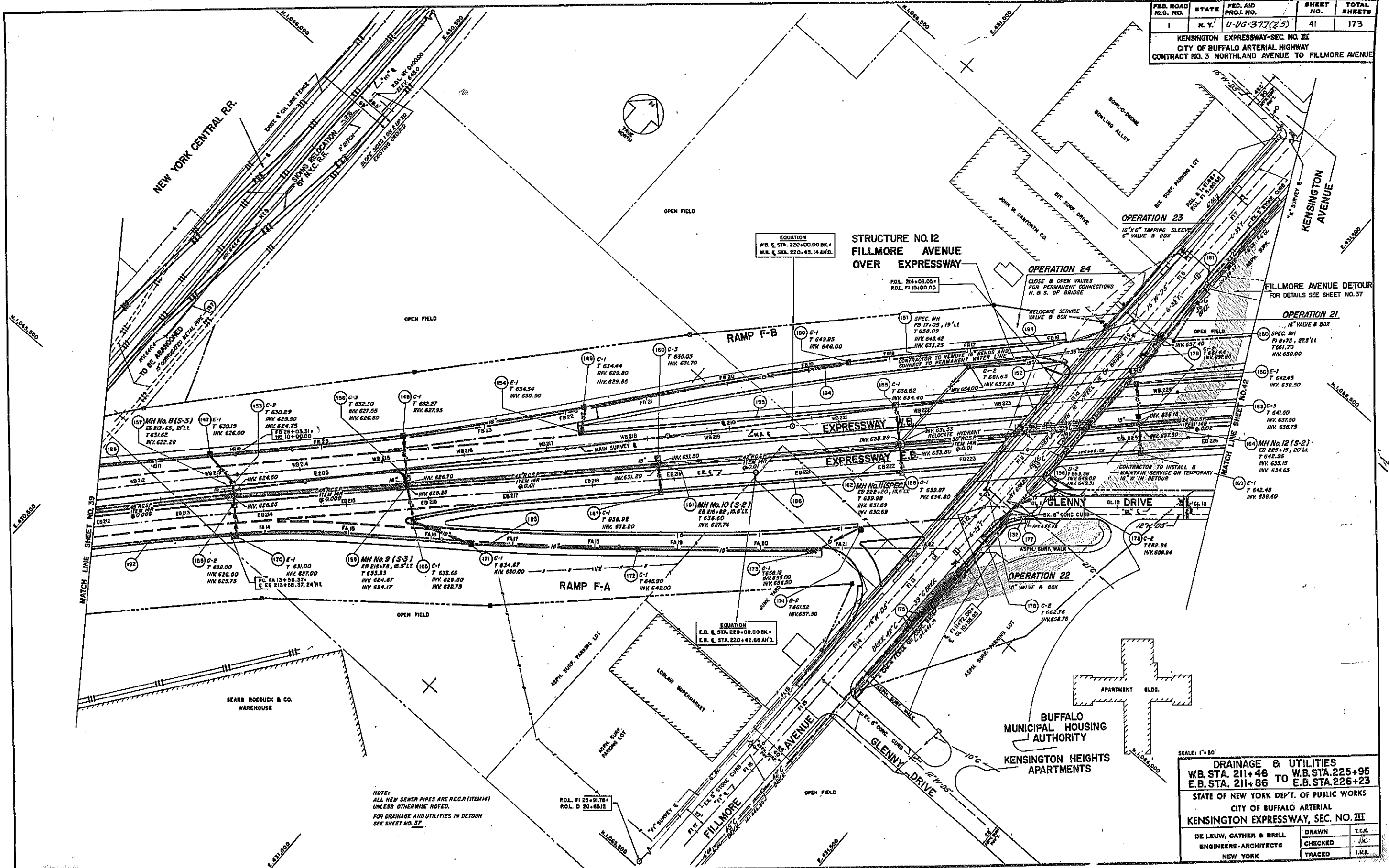
DRAINAGE & UTILITIES DELAVAN INTERCHANGE CONNECTIONS TO HUMBOLDT PARKWAY

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TEK JK JMS
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	41	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE:
ALL NEW SEWER PIPES ARE R.C.C. (ITEM H)
UNLESS OTHERWISE NOTED.
FOR DRAINAGE AND UTILITIES IN DETOUR
SEE SHEET NO. 37.

SCALE: 1" = 80'

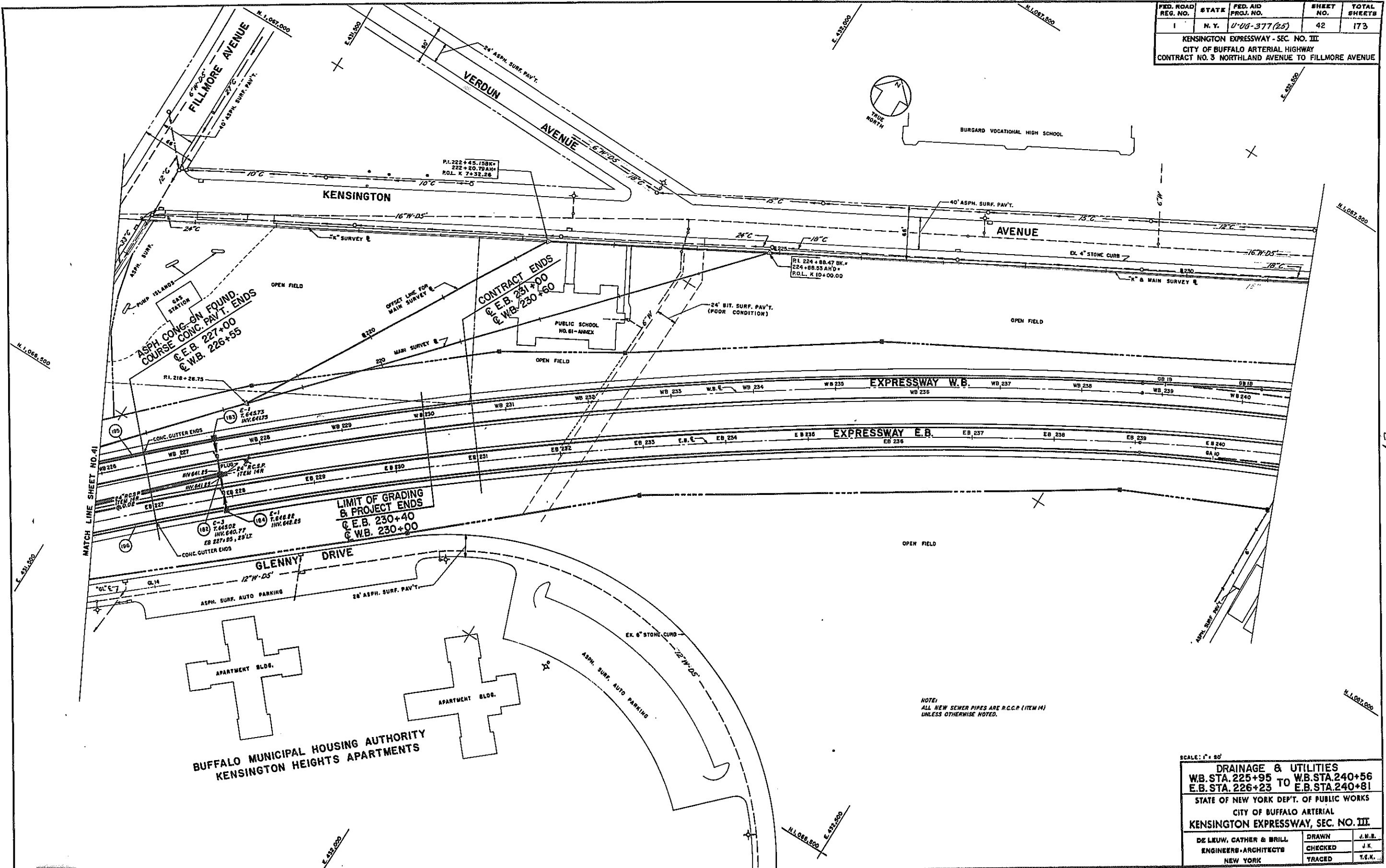
DRAINAGE & UTILITIES	
W.B. STA. 211+46	TO W.B. STA. 225+95
E.B. STA. 211+86	TO E.B. STA. 226+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.C.K. J.K. J.R.B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377 (25)	42	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



LIMIT OF GRADING & PROJECT ENDS
 C.E.B. 230+40
 C.W.B. 230+00

NOTE:
 ALL NEW SEWER PIPES ARE R.C.C.P. (ITEM 14)
 UNLESS OTHERWISE NOTED.

SCALE: 1" = 80'

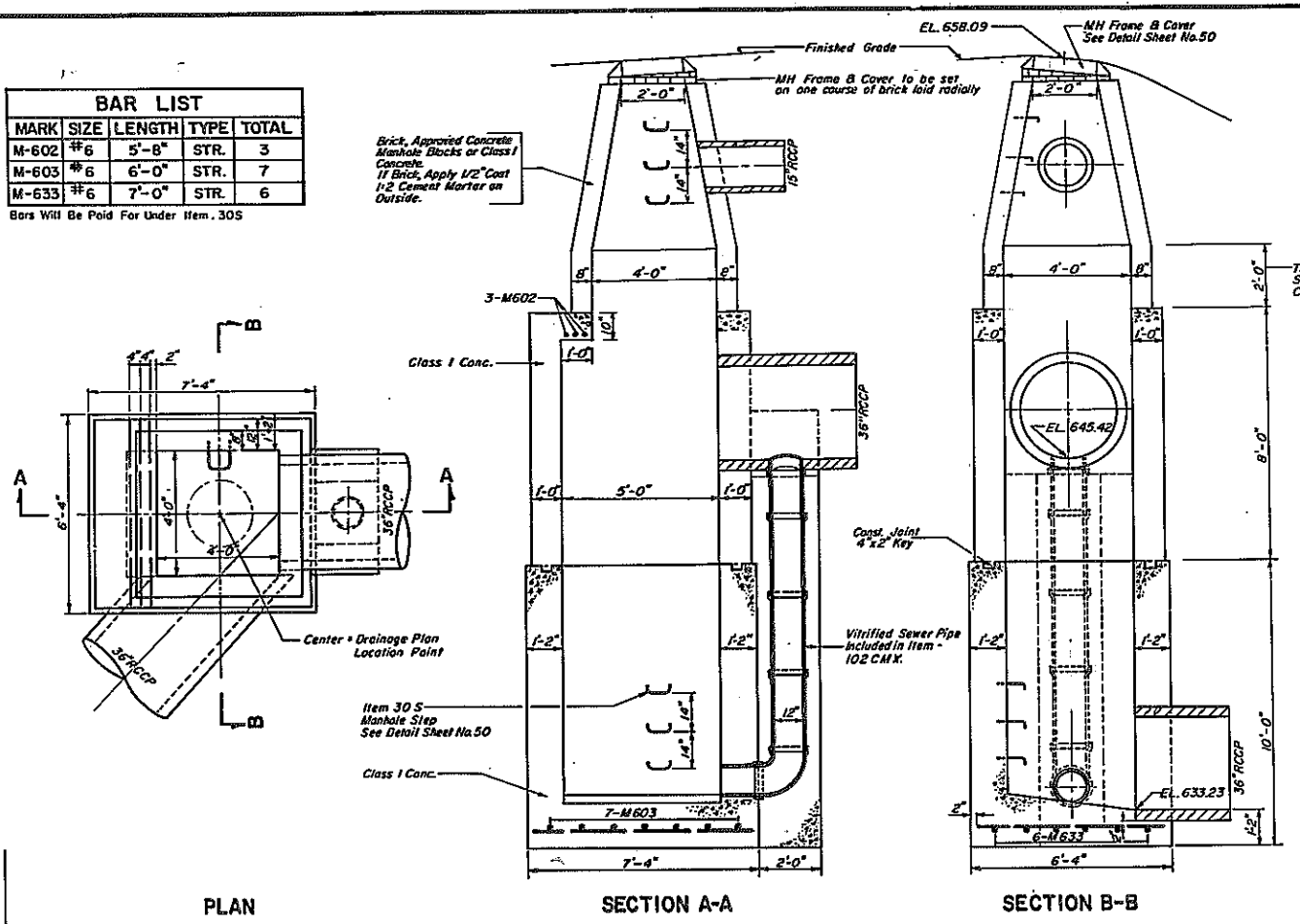
DRAINAGE & UTILITIES	
WB. STA. 225+95 TO WB. STA. 240+56	E.B. STA. 226+23 TO E.B. STA. 240+81
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DE LUKW, CATHER & BRILL	DRAWN J.M.B.
ENGINEERS-ARCHITECTS	CHECKED J.K.
NEW YORK	TRACED T.S.N.

FED. RD. REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	44	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

MARK	SIZE	LENGTH	TYPE	TOTAL
M-602	#6	5'-8"	STR.	3
M-603	#6	6'-0"	STR.	7
M-633	#6	7'-0"	STR.	6

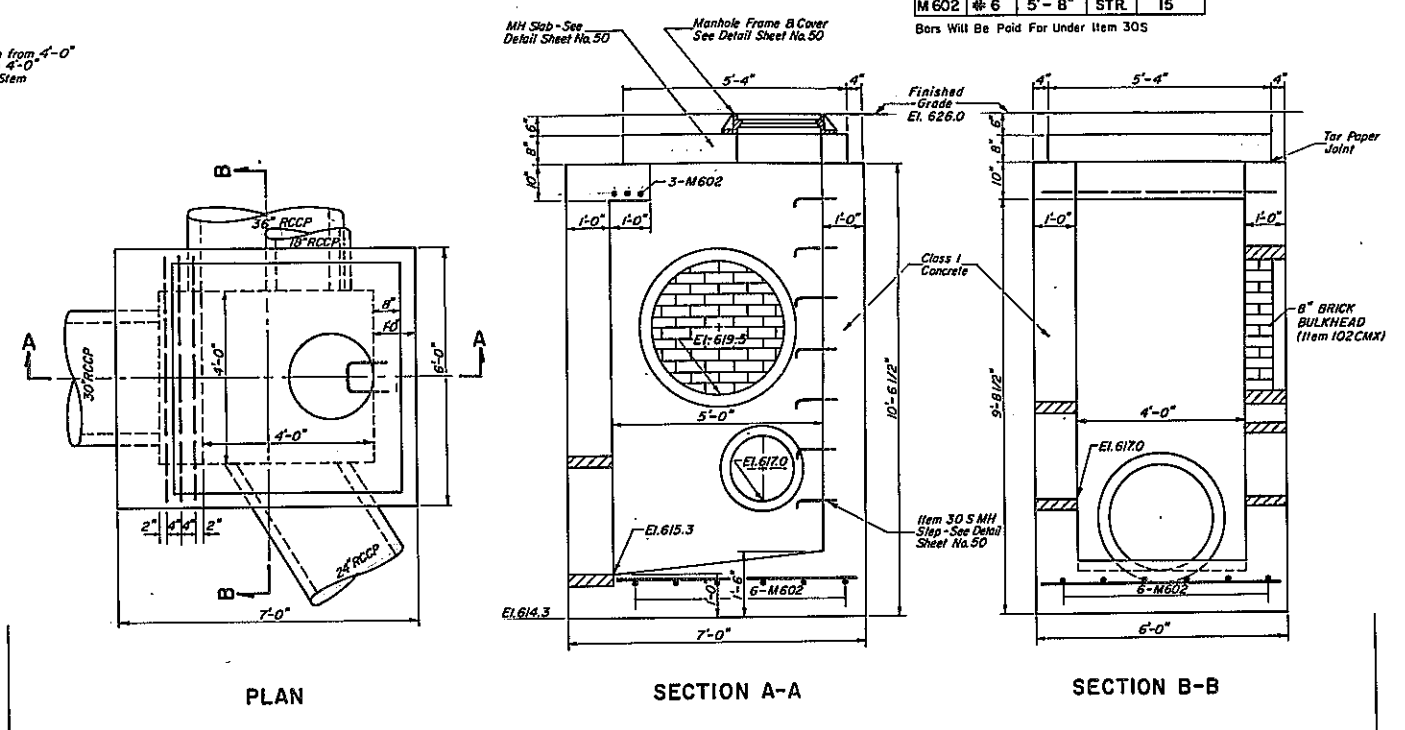
Bars Will Be Paid For Under Item 305



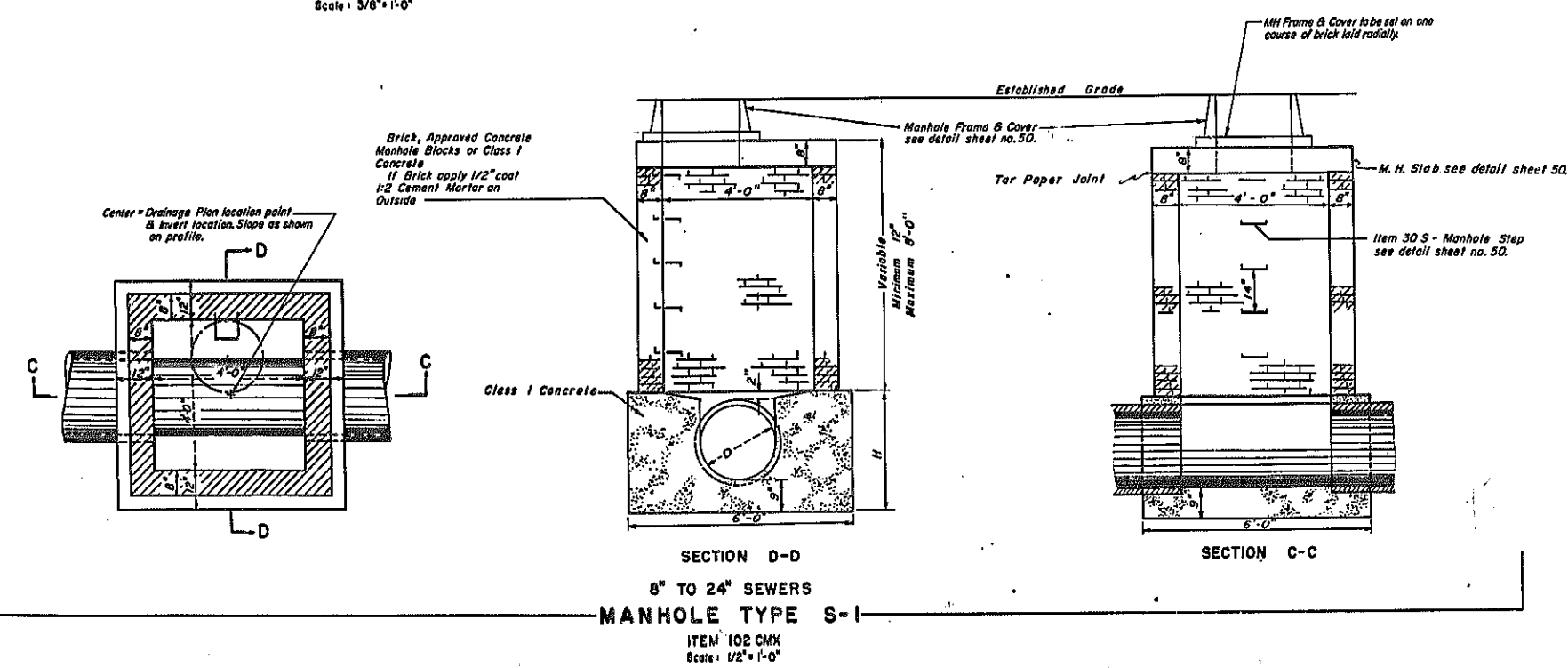
SPECIAL MANHOLE
 "FB" 17+05, 19' LT.
 ITEM 102 CMX
 Scale: 3/8" = 1'-0"

MARK	SIZE	LENGTH	TYPE	TOTAL
M602	#6	5'-8"	STR.	15

Bars Will Be Paid For Under Item 305



SPECIAL MANHOLE
 W.B. 204 + 45, 40' LT.
 ITEM 102 CMX
 Scale: 1/2" = 1'-0"



MANHOLE TYPE S-1
 8" TO 24" SEWERS
 ITEM 102 CMX
 Scale: 1/2" = 1'-0"

D	H
8"	20"
10"	22"
12"	24"
15"	27"
18"	31"
20"	33"
22"	35"
24"	37"

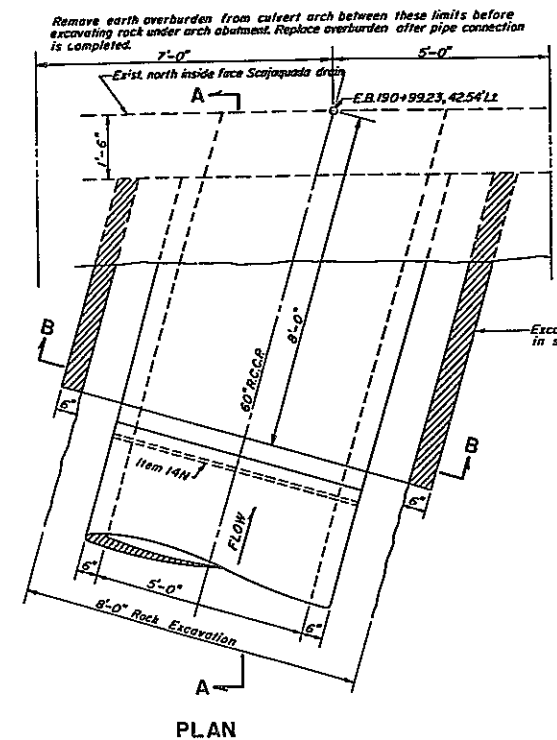
NOTE:
 For limits of excavation and backfill see sheet no. 05.

DRAINAGE STRUCTURES
 MANHOLE TYPE S-1

STATE OF NEW YORK - DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. III

DE LEUW, CATHY & BRILL	DRAWN	J. K.
ENGINEERS - ARCHITECTS	CHECKED	J. R. P.
NEW YORK	TRACED	G. C. P.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	47	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

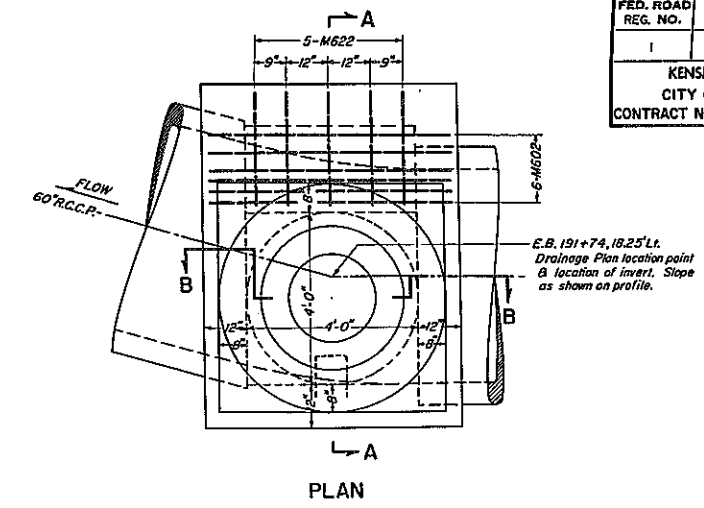


PLAN

BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M1001	No.10	8'-6"	Str.	13

Bars Will Be Paid For Under Item 30S

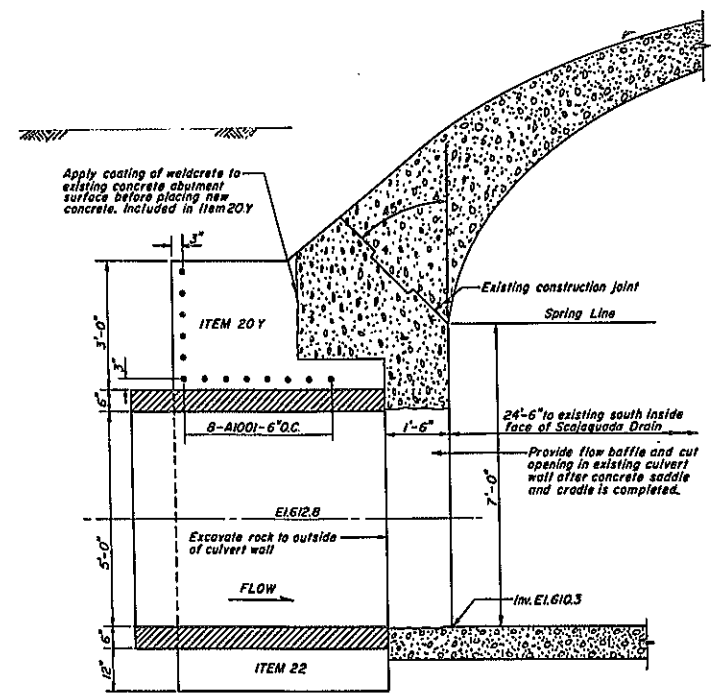
- CONSTRUCTION SEQUENCE**
1. REMOVE EARTH OVERBURDEN FROM CULVERT ARCH BEFORE EXCAVATING ROCK UNDER ARCH ABUTMENT.
 2. EXCAVATE ROCK UNDER ARCH ABUTMENT TO THE ELEVATION INDICATED. THE CONTRACTOR IS CAUTIONED THAT HE WILL BE REQUIRED TO REPAIR AT HIS OWN EXPENSE ANY DAMAGE OCCURRING TO EXISTING SCAJAGUADA DRAIN AS A RESULT OF HIS EXCAVATING OPERATION.
 3. THE FIRST SECTION OF 60" R.C.C.P. TO FIT FLUSH AGAINST FACE OF SCAJAGUADA DRAIN AT PROPER SKEW.
 4. PLACE CONCRETE CRADLE AROUND PIPE TO ELEVATION 612.8
 5. PLACE REINFORCED CONCRETE SADDLE AROUND PIPE TO ELEVATION 618.8
 6. PLACE REMAINING 60" R.C.C.P. TO MANHOLE (SPEC)
 7. PROVIDE TEMPORARY FLOW BAFFLE.
 8. CUT OPENING IN EXISTING CULVERT WALL, NO LESS THAN 28 DAYS AFTER CONCRETE SADDLE IS PLACED.
 9. REMOVE TEMPORARY FLOW BAFFLE.
 10. BACKFILL OVER EXPOSED PORTION OF CULVERT ARCH TO SUBGRADE OF WESTBOUND EXPRESSWAY.



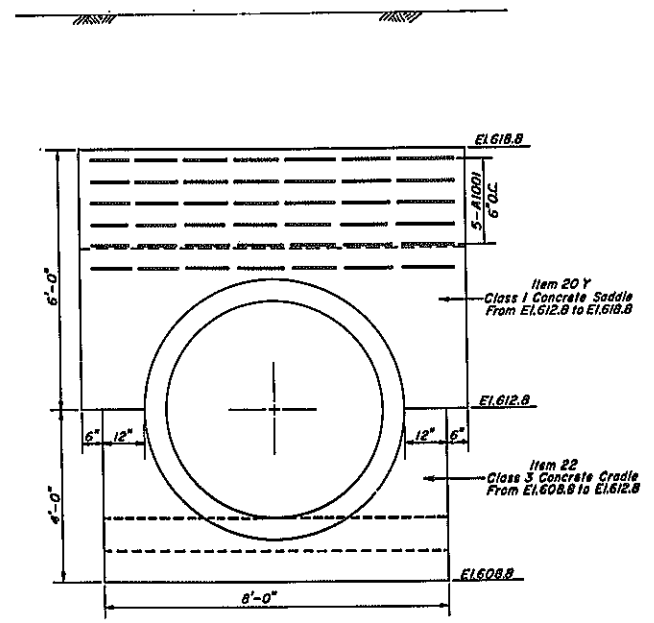
PLAN

BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	9
M602	No.6	5'-8"	Str.	6
M608	No.6	7'-6"	Str.	5
M622	No.6	2'-8"	Str.	5

Bars Will Be Paid For Under Item 30S

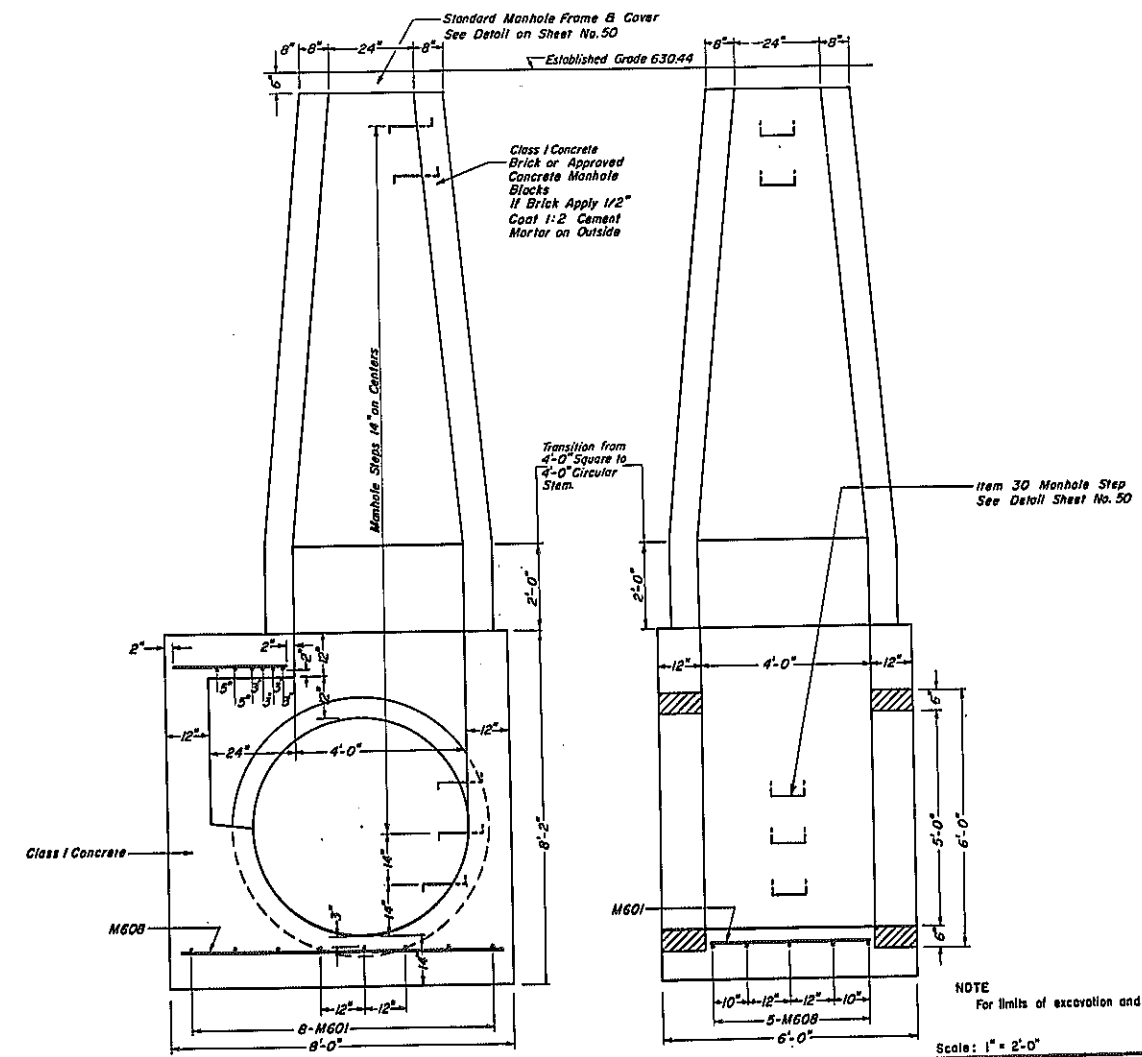


SECTION A-A



SECTION B-B

EXPRESSWAY TRUNK SEWER CONNECTION TO SCAJAGUADA DRAIN
E.B. 190+99.23, 42.54 Lt.



SECTION A-A

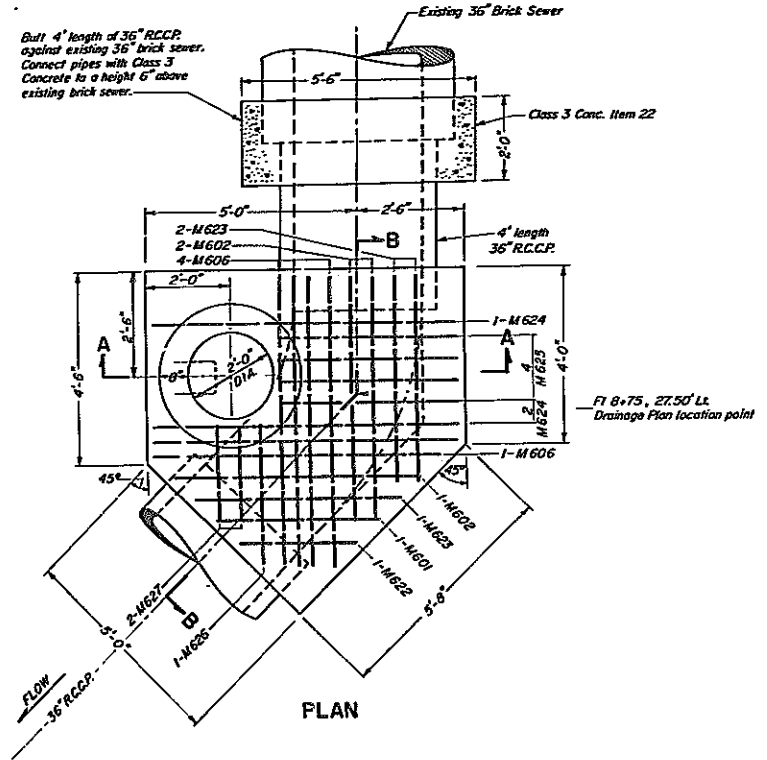
SECTION B-B

MH No. 1
SPECIAL MANHOLE
E.B. 191+74, 18.25 Lt.
ITEM 102 CMX

SPECIAL MANHOLES			
MH No. 1 & CONNECTION TO SCAJAGUADA DRAIN			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS			
CITY OF BUFFALO ARTERIAL			
KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.M.B. J.K. T.E.K.	

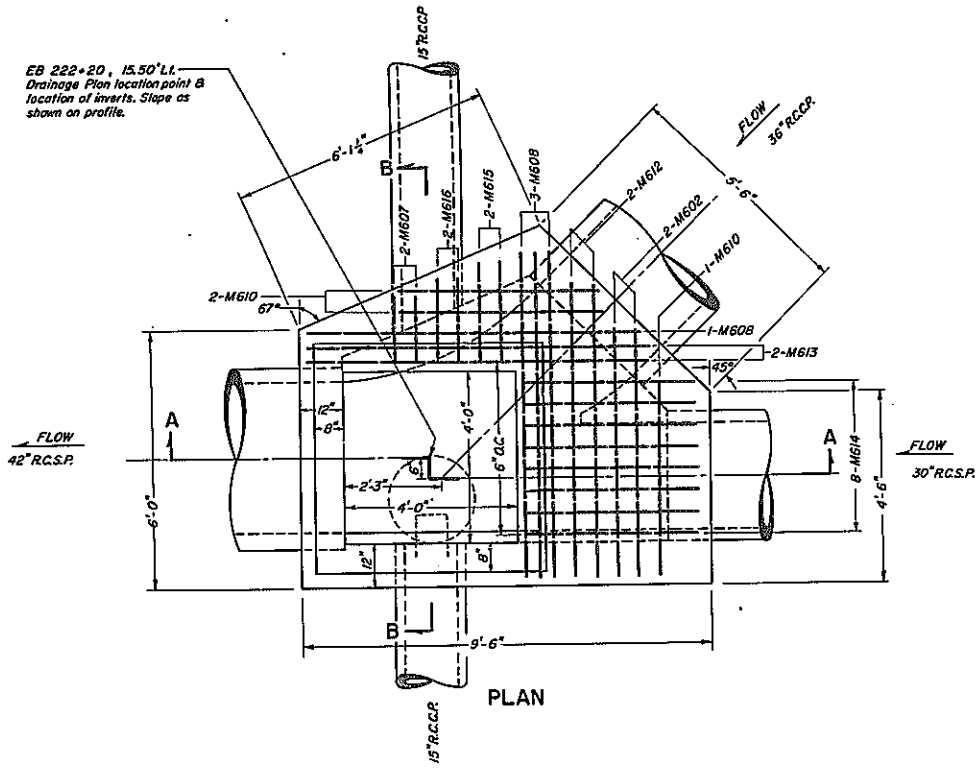
FED. ROAD RES. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
I	N. Y.	U-UG-377(23)	49	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



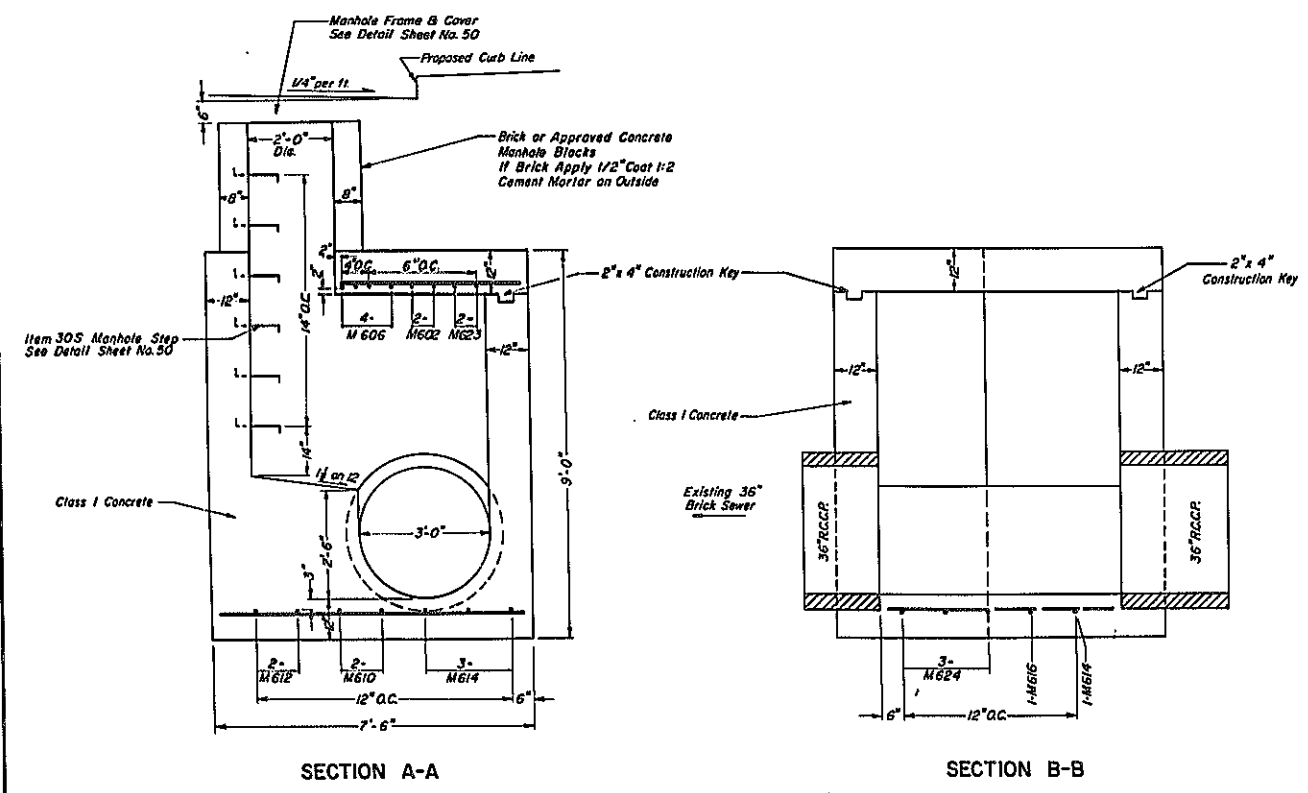
BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	1
M602	No.6	5'-8"	Str.	3
M606	No.6	6'-8"	Str.	5
M622	No.6	2'-8"	Str.	1
M623	No.6	4'-8"	Str.	3
M624	No.6	7'-2"	Str.	3
M625	No.6	4'-2"	Str.	4
M626	No.6	3'-2"	Str.	1
M627	No.6	2'-2"	Str.	2

Bars Will Be Paid For Under Item 305

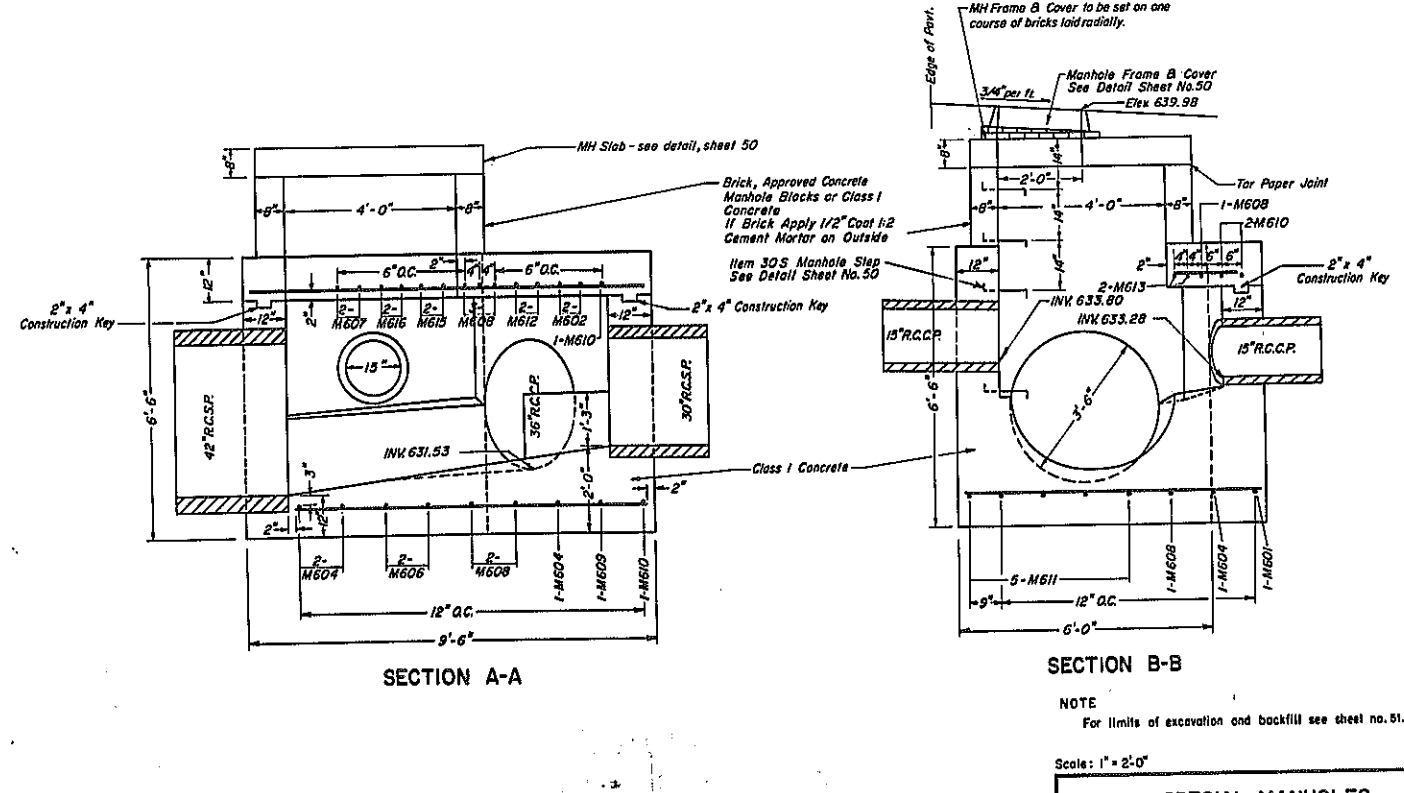


BAR LIST				
MARK	SIZE	LENGTH	TYPE	TOTAL
M601	No.6	3'-8"	Str.	1
M602	No.6	5'-8"	Str.	2
M604	No.6	6'-5"	Str.	4
M606	No.6	6'-8"	Str.	2
M607	No.6	1'-8"	Str.	2
M608	No.6	7'-6"	Str.	7
M609	No.6	5'-6"	Str.	1
M610	No.6	4'-6"	Str.	5
M611	No.6	8'-2"	Str.	5
M612	No.6	6'-9"	Str.	2
M613	No.6	8'-0"	Str.	2
M614	No.6	4'-0"	Str.	8
M615	No.6	2'-4"	Str.	2
M616	No.6	2'-0"	Str.	2

Bars Will Be Paid For Under Item 305



FILLMORE AVE. SEWER INTERCEPT
SPECIAL MANHOLE
FI 8+75, 27.50' Lt.
ITEM 102CMX



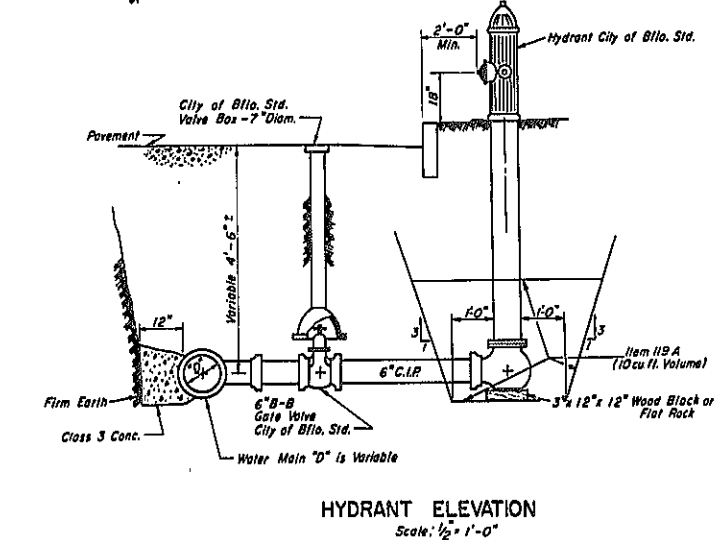
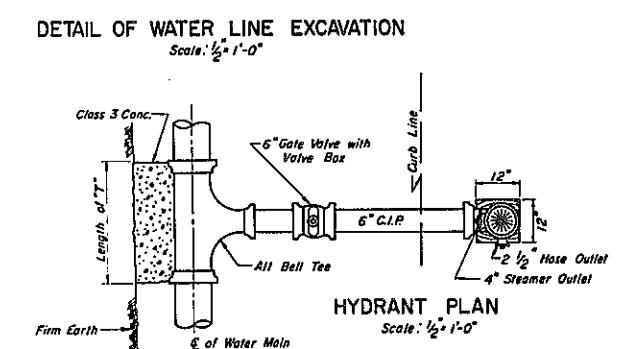
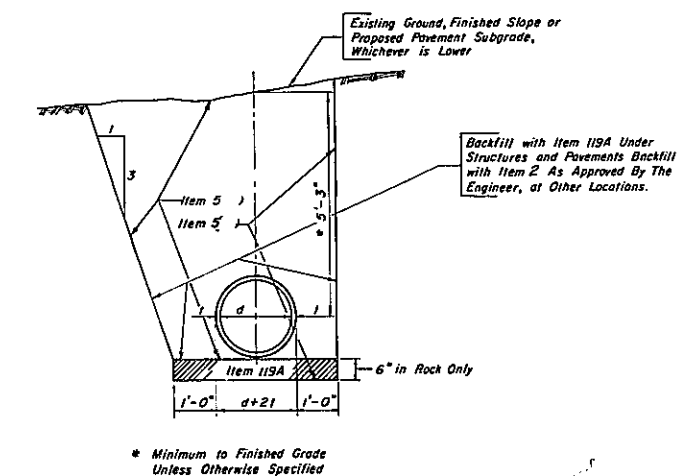
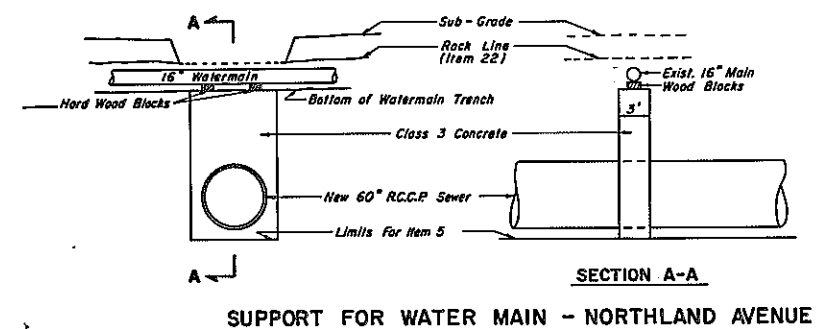
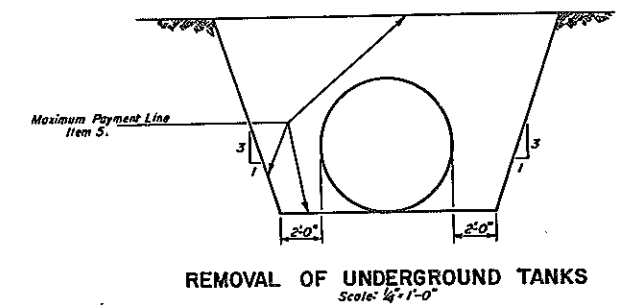
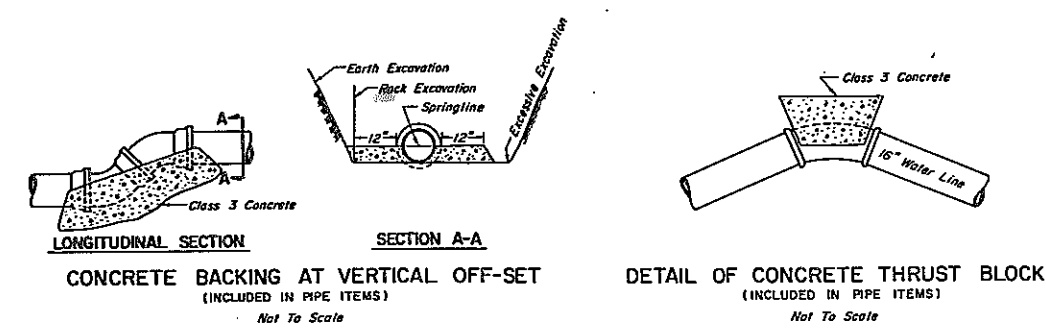
MH No. II
SPECIAL MANHOLE
EB 222+20, 15.50' Lt.
ITEM 102CMX

NOTE
For limits of excavation and backfill see sheet no. 51.

Scale: 1" = 2'-0"

SPECIAL MANHOLES		
MH No. II & FILLMORE AVE. SEWER INTERCEPT		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL	DRAWN	TEK
ENGINEERS-ARCHITECTS	CHECKED	F.J.P.
NEW YORK	TRACED	J.M.B.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	52	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



DESCRIPTION	SUMMARY OF ITEMS FOR WATER LINES											
	ITEM NO.											
	5	119A	200	202A	201B	201F	202B	205B	205C	209A	209B	5
SHEET NO. 38 - (new 6" main south of Northland Ave., new 6" main north of Northland Ave.)	809	802	-	-	705	-	4	4	-	13		
SHEET NO. 39 - (new 6" main continued to Delavan Ave., new 18" main across Delavan Ave. structure, new 6" main along Samp H-B.)	1974	768	560	1	1040	20	4	5	1	-	200 CONTINGENT	
SHEET NO. 40 - (new 6" main continued to connect with existing main in Loring-Oakgrove Service Rd.)	357	315	-	-	325	-	1	1	-	3		
SHEET NO. 41 - (new 16" main across Fillmore Ave. Structure.)	587	251	440	-	-	20	-	1	-	1		74
NEAT TOTAL	3727 C.Y.	2136 C.Y.	1000 L.F.	1 EA.	2070 L.F.	40 L.F.	9 EA.	11 EA.	1 EA.	17 EA.	200 L.F.	74 C.Y.

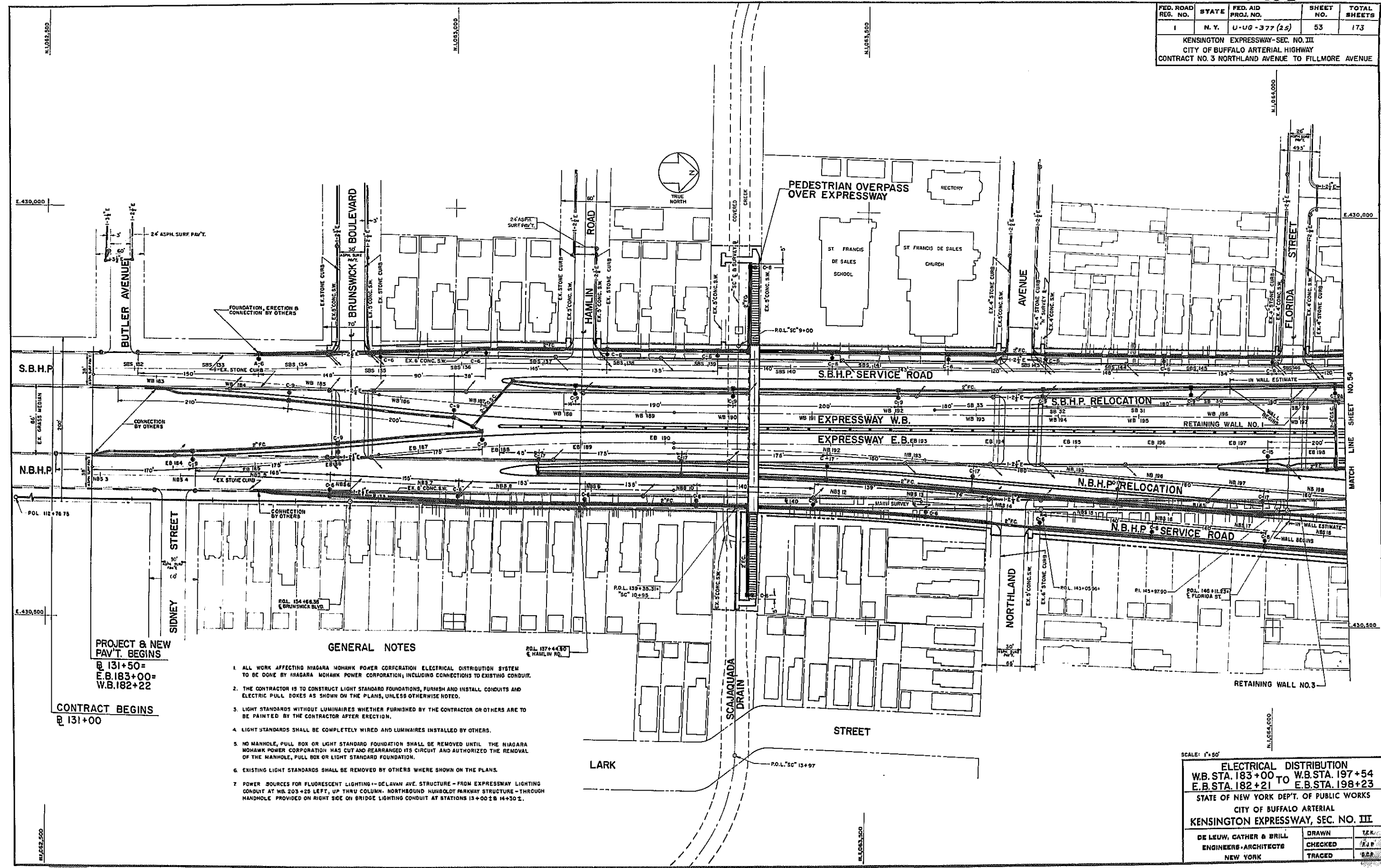
ITEM 209 A	
LOCATION	SIDE
SBS 135 + 20	L.T.
SBS 135 + 75	L.T.
SBS 136 + 20	L.T.
SBS 135 + 60	L.T.
SBS 137 + 00	L.T.
SBS 137 + 75	L.T.
SBS 138 + 90	L.T.
NBS 10 + 10	RT.
NBS 11 + 30	RT.
NBS 11 + 45	RT.
NBS 11 + 70	RT.
NBS 12 + 20	RT.
NBS 12 + 35	RT.
L-O 15 + 82	L.T.
L-O 16 + 15	L.T.
L-O 16 + 40	L.T.
FI 8 + 80	RT.
NEAT TOTAL	17

WATER DETAILS

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	G.J.B.
ENGINEERS-ARCHITECTS	CHECKED	R.J.D.
NEW YORK	TRACED	G.J.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	53	173
KENSINGTON EXPRESSWAY-SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



PROJECT & NEW PAV'T. BEGINS @ 131+50 = E.B. 183+00 = W.B. 182+22

CONTRACT BEGINS @ 131+00

GENERAL NOTES

1. ALL WORK AFFECTING NIAGARA MOHAWK POWER CORPORATION ELECTRICAL DISTRIBUTION SYSTEM TO BE DONE BY NIAGARA MOHAWK POWER CORPORATION, INCLUDING CONNECTIONS TO EXISTING CONDUIT.
2. THE CONTRACTOR IS TO CONSTRUCT LIGHT STANDARD FOUNDATIONS, FURNISH AND INSTALL CONDUITS AND ELECTRIC PULL BOXES AS SHOWN ON THE PLANS, UNLESS OTHERWISE NOTED.
3. LIGHT STANDARDS WITHOUT LUMINAIRES WHETHER FURNISHED BY THE CONTRACTOR OR OTHERS ARE TO BE PAINTED BY THE CONTRACTOR AFTER ERECTION.
4. LIGHT STANDARDS SHALL BE COMPLETELY WIRED AND LUMINAIRES INSTALLED BY OTHERS.
5. NO MANHOLE, PULL BOX OR LIGHT STANDARD FOUNDATION SHALL BE REMOVED UNTIL THE NIAGARA MOHAWK POWER CORPORATION HAS CUT AND REARRANGED ITS CIRCUIT AND AUTHORIZED THE REMOVAL OF THE MANHOLE, PULL BOX OR LIGHT STANDARD FOUNDATION.
6. EXISTING LIGHT STANDARDS SHALL BE REMOVED BY OTHERS WHERE SHOWN ON THE PLANS.
7. POWER SOURCES FOR FLUORESCENT LIGHTING IN DELAVAN AVE. STRUCTURE - FROM EXPRESSWAY LIGHTING CONDUIT AT WB 203+25 LEFT, UP THRU COLUMN. NORTHBOUND HUMBOLDT PARKWAY STRUCTURE - THROUGH HANDHOLE PROVIDED ON RIGHT SIDE ON BRIDGE LIGHTING CONDUIT AT STATIONS 13+00 TO 14+30.2.

SCALE: 1" = 50'

ELECTRICAL DISTRIBUTION
 W.B. STA. 183+00 TO W.B. STA. 197+54
 E.B. STA. 182+21 TO E.B. STA. 198+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

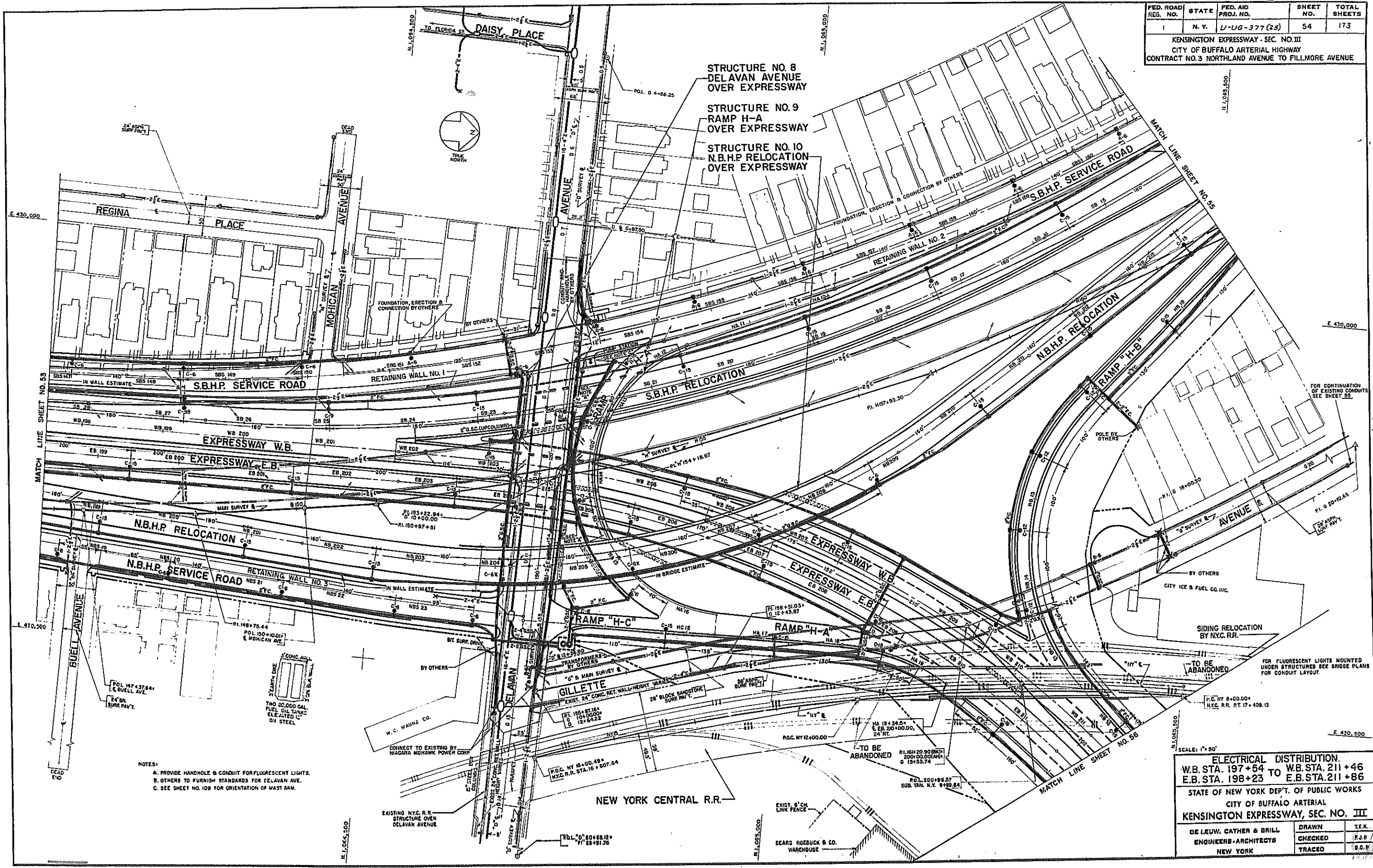
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.E.K. R.A.P. E.S.S.
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MATCH LINE SHEET NO. 54

15

FED. ROAD RES. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	54	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:
A. PROVIDE HANDHOLE & CONDUIT FOR FLUORESCENT LIGHTS.
B. OTHERS TO FURNISH STANDARDS FOR DELAVAN AVE.
C. SEE SHEET NO. 109 FOR ORIENTATION OF MAST ARM.

SCALE: 1" = 50'

ELECTRICAL DISTRIBUTION.
W.B. STA. 197+54 TO W.B. STA. 211+46
E.B. STA. 198+23 TO E.B. STA. 211+86

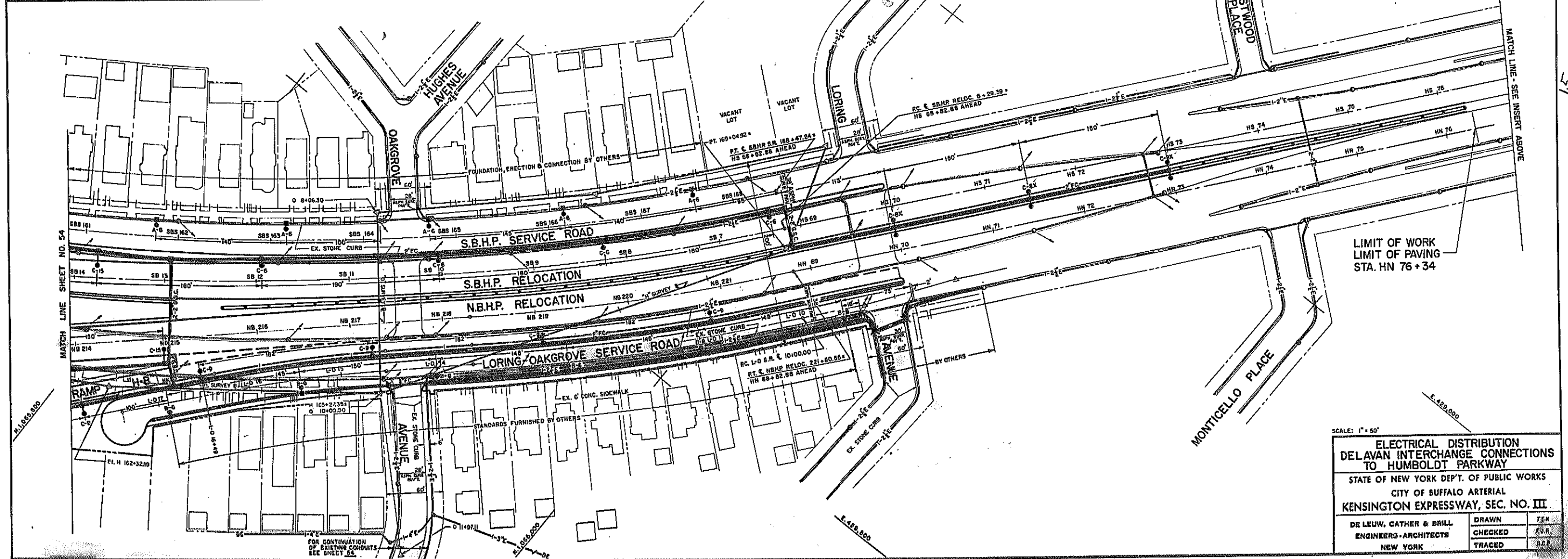
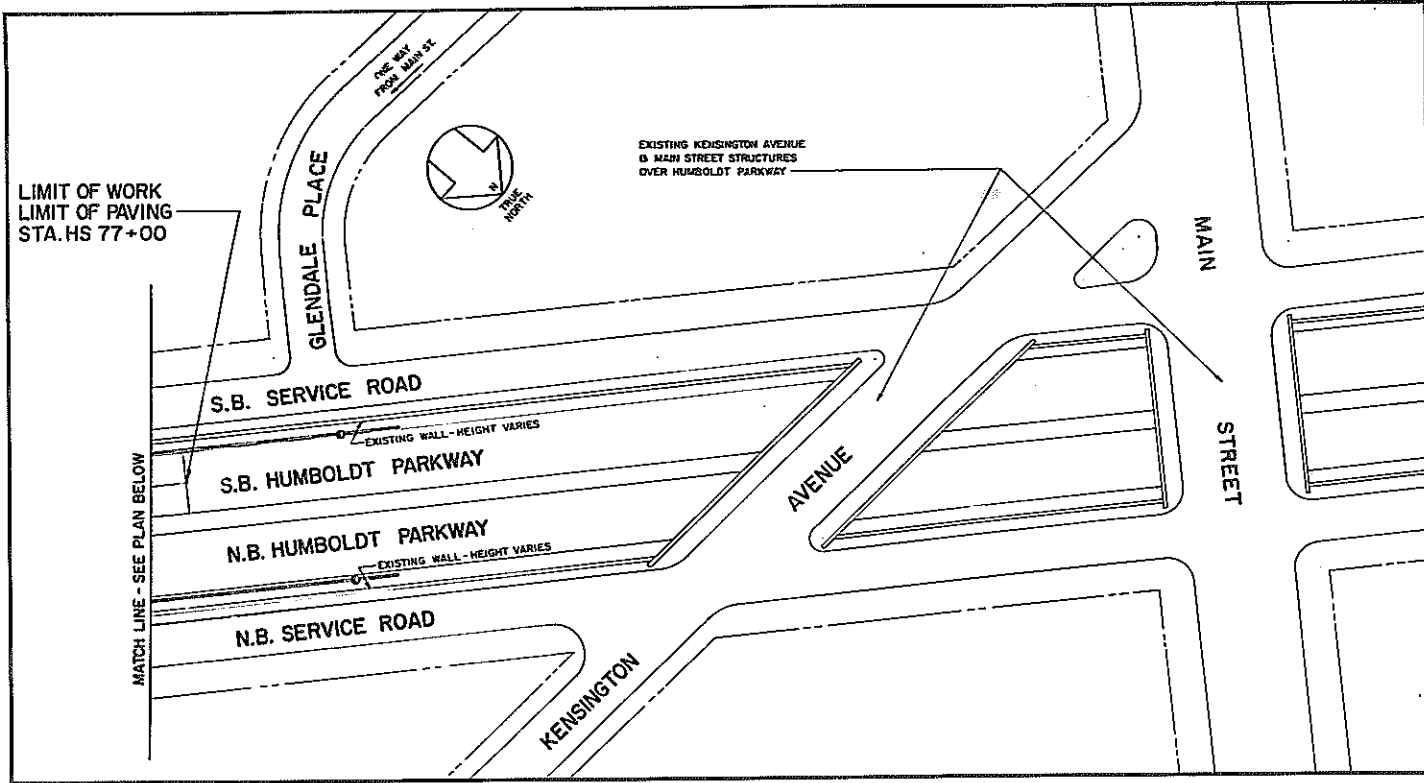
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	J.A.R.
NEW YORK	TRACED	G.O.P.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	55	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

LIMIT OF WORK
LIMIT OF PAVING
STA. HS 77+00



LIMIT OF WORK
LIMIT OF PAVING
STA. HN 76+34

SCALE: 1" = 50'

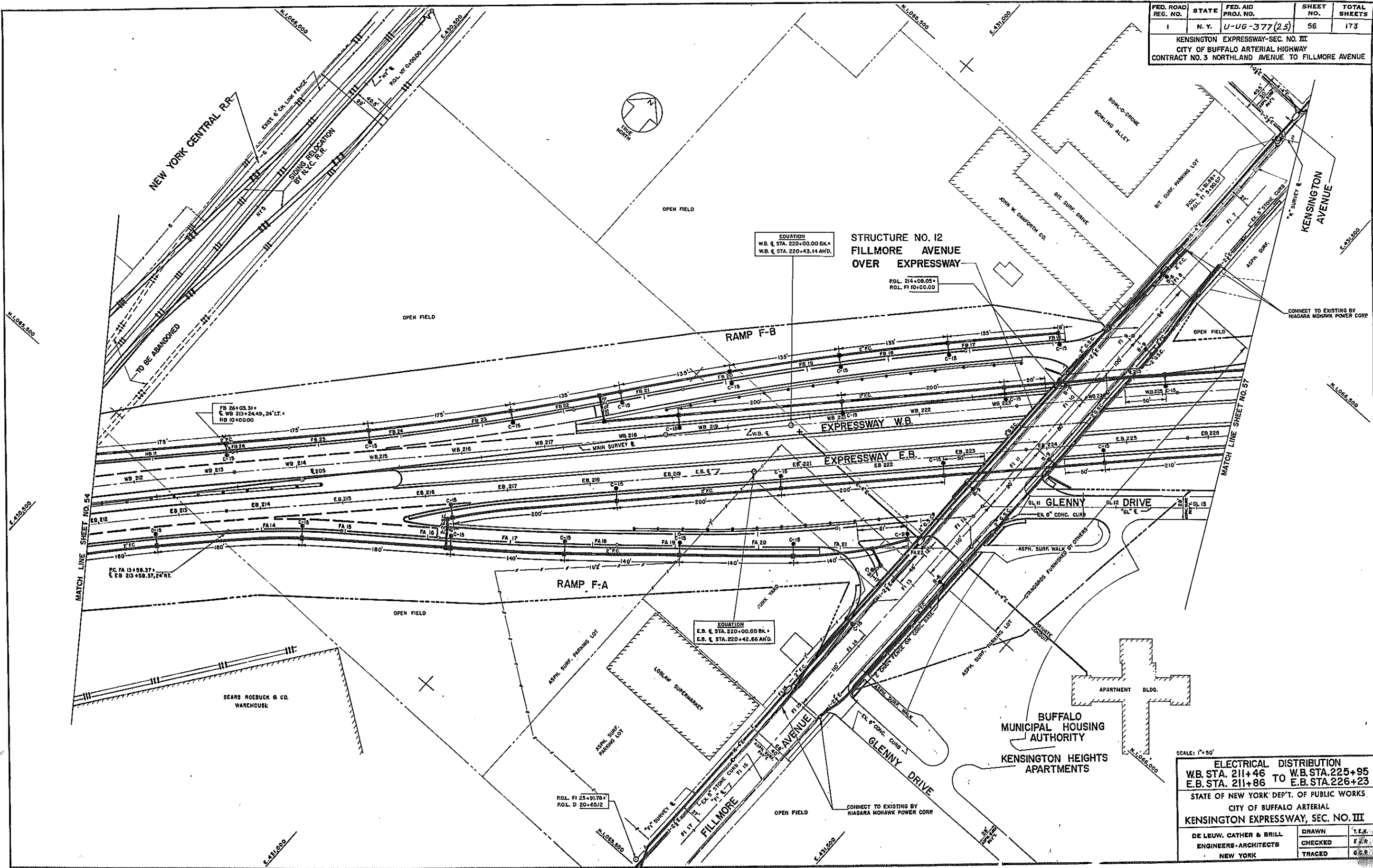
**ELECTRICAL DISTRIBUTION
DELANE INTERCHANGE CONNECTIONS
TO HUMBOLDT PARKWAY**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	TEK S.V.B. R.S.P.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	56	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SCALE: 1" = 50'

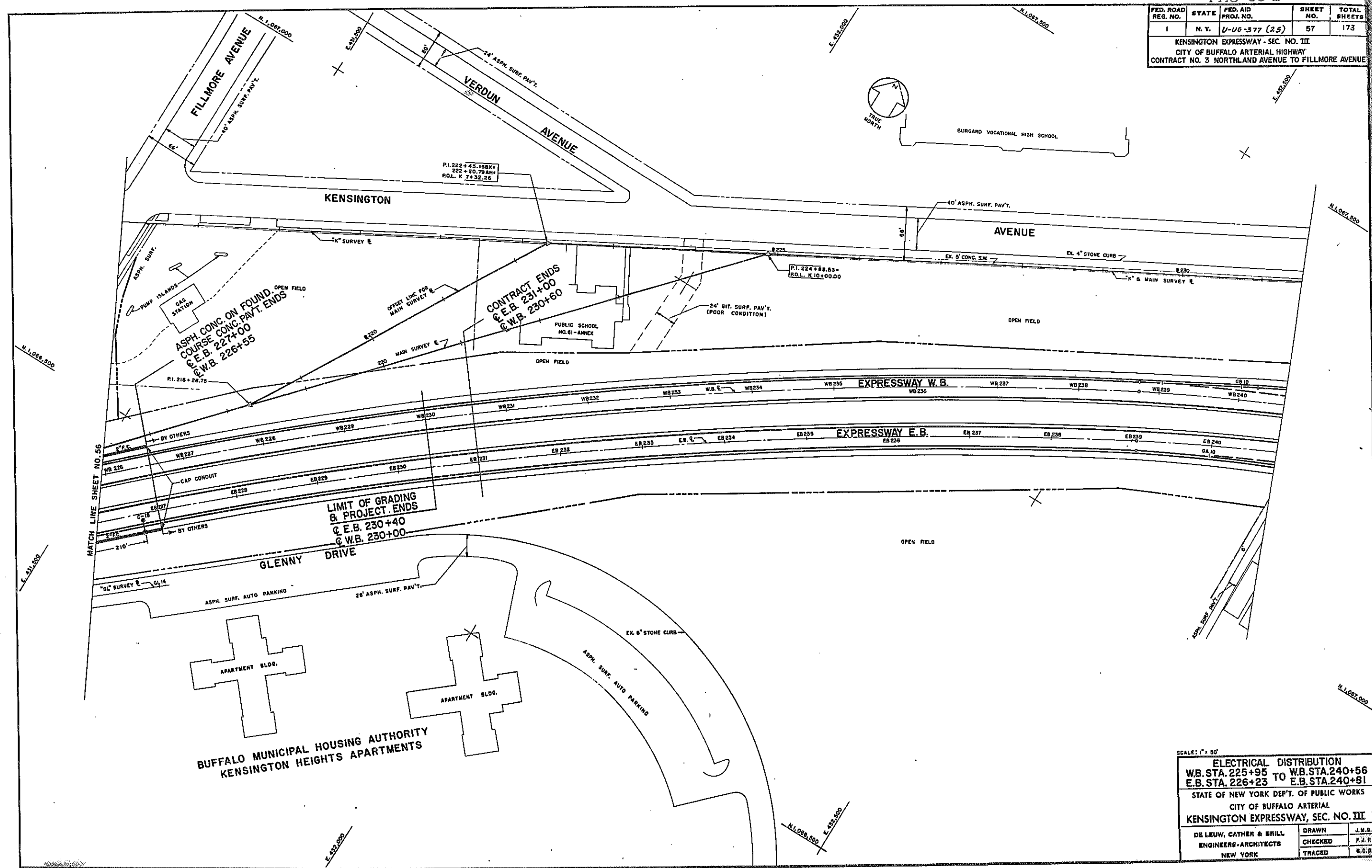
ELECTRICAL DISTRIBUTION	
WB. STA. 211+46 TO	WB. STA. 225+95
E.B. STA. 211+86 TO	E.B. STA. 226+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL	DRAWN	T.E.M.
ENGINEERS-ARCHITECTS	CHECKED	F.R.R.
NEW YORK	TRACED	C.S.F.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	57	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



ASPH. CONC. ON FOUND. COURSE CONC. PAV'T. ENDS
 C.E.B. 227+00
 C.W.B. 226+55

CONTRACT ENDS
 C.E.B. 231+00
 C.W.B. 230+60

LIMIT OF GRADING & PROJECT ENDS
 C.E.B. 230+40
 C.W.B. 230+00

SCALE: 1" = 50'

ELECTRICAL DISTRIBUTION
 W.B. STA. 225+95 TO W.B. STA. 240+56
 E.B. STA. 226+23 TO E.B. STA. 240+81

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	J.M.B.
ENGINEERS-ARCHITECTS	CHECKED	F.A.R.
NEW YORK	TRACED	G.C.R.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	60	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

TYPE OF SIGNAL HEAD & CONTROLLER			
SIGNAL NO.	TYPE	DESCRIPTION	CONTROLLER
A	CS	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
A1	B	2 WAY, 3-SECTION,	2 PHASE S.T.A.
A2	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
B	CS	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
B1	B	2 WAY, 3-SECTION,	2 PHASE S.T.A.
B2	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
C	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
C1	DS	4 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
D	B	2 WAY, 3-SECTION, ADJUSTABLE	2 PHASE S.T.A.
E	A	1 WAY, 3-SECTION,	2 PHASE S.T.A.
T1	C2	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE FIX. T.
T2	C2	3 WAY, 3-SECTION, ADJUSTABLE	2 PHASE FIX. T.

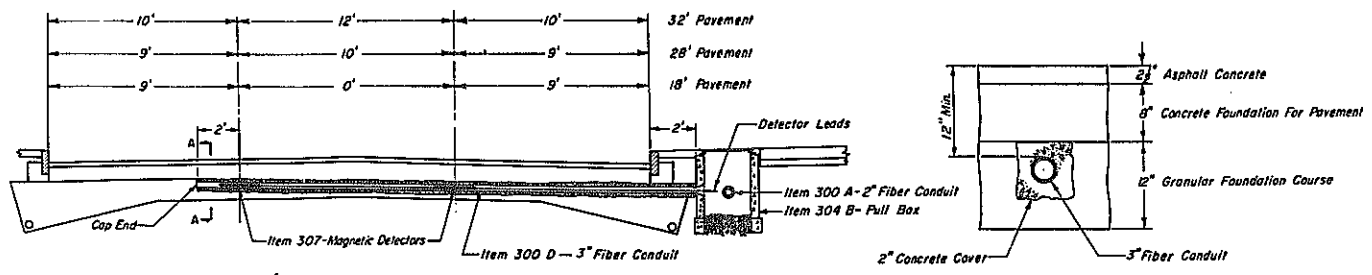
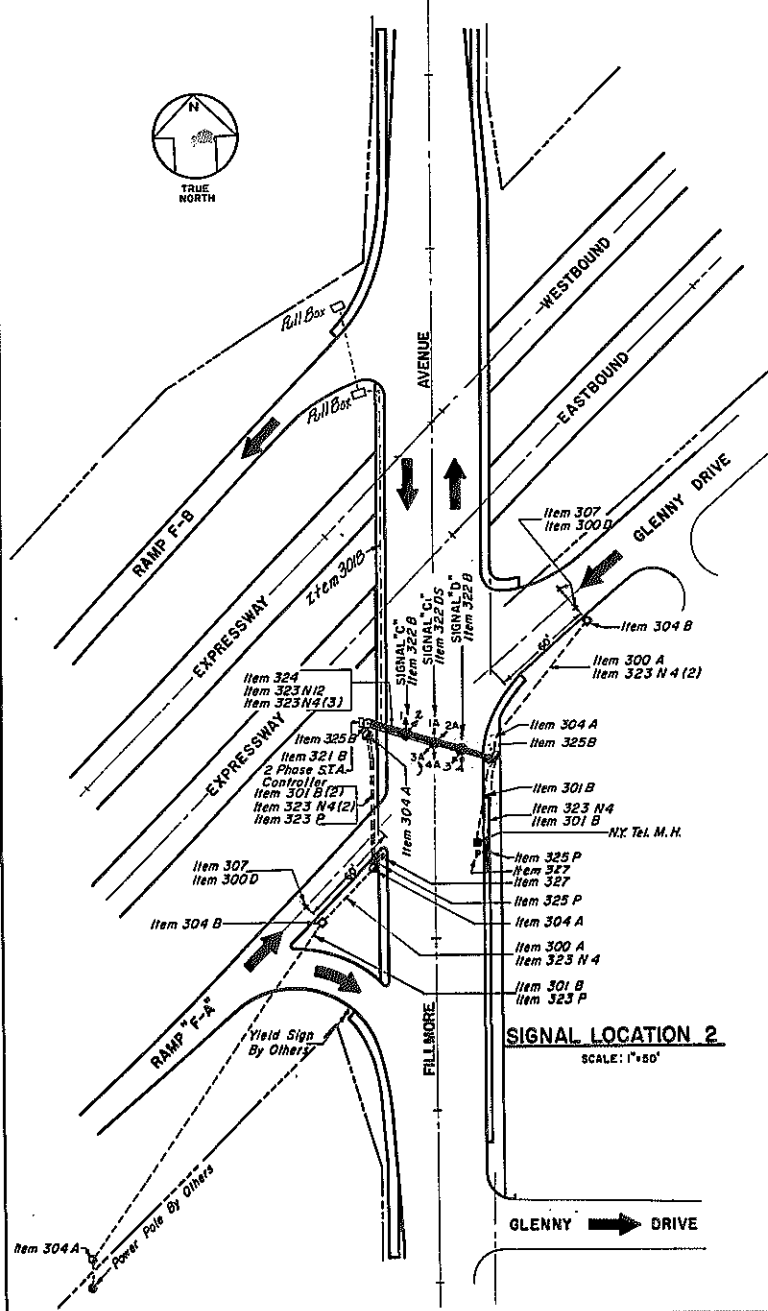
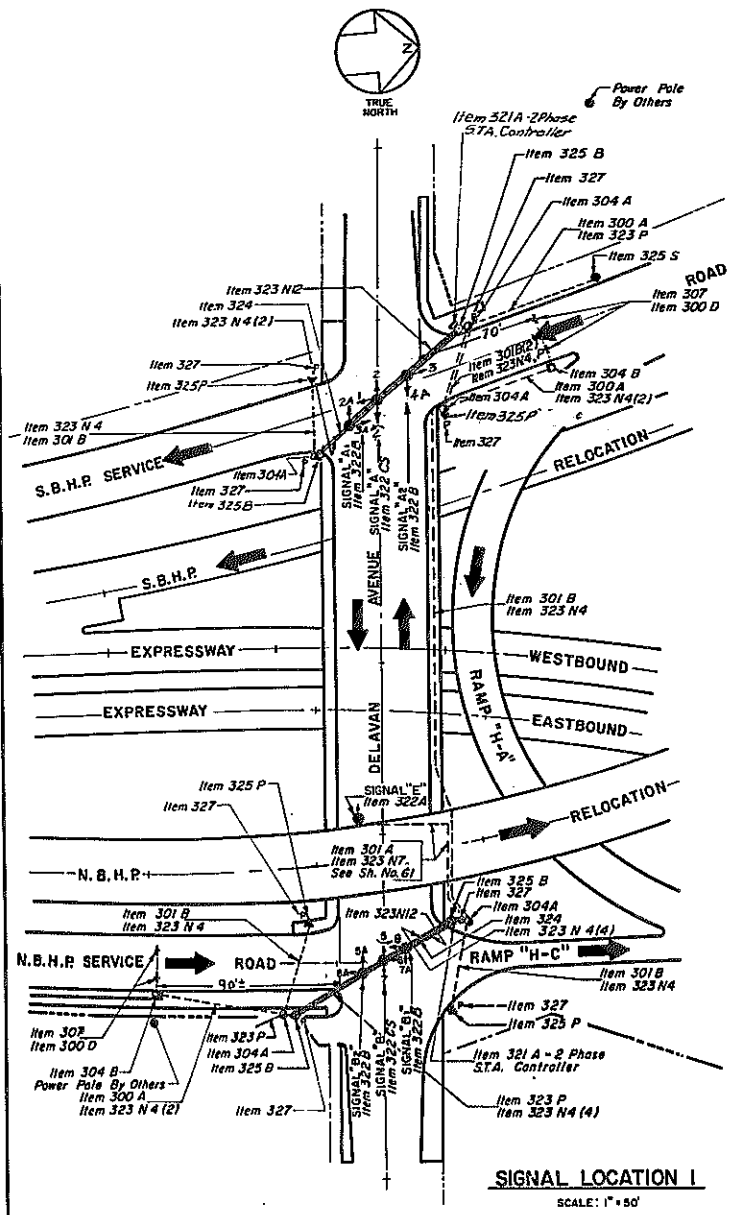
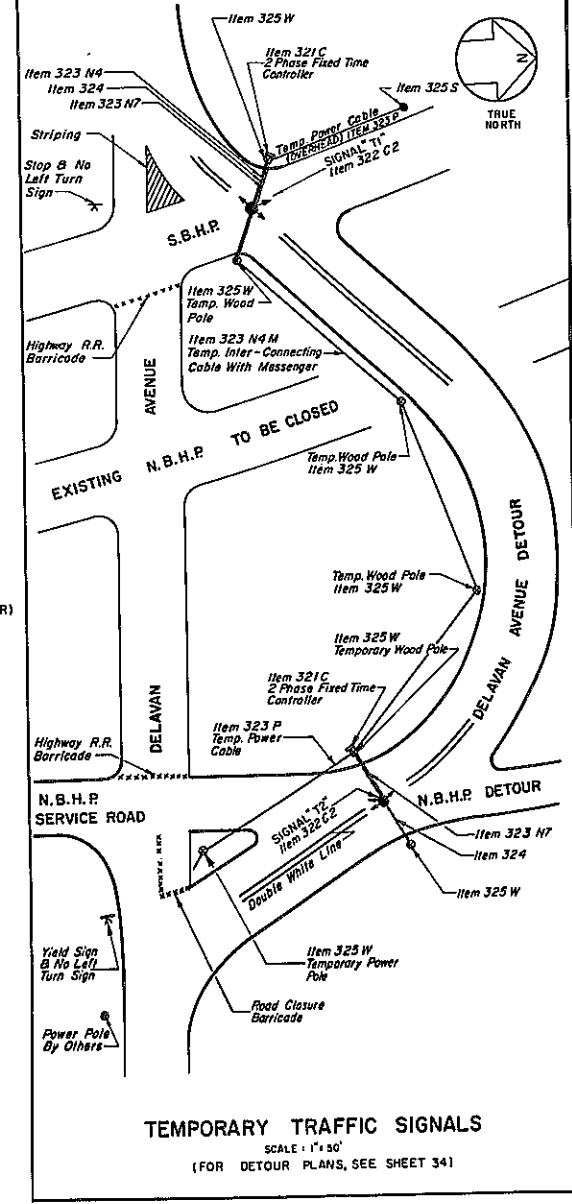
* FACES 4 & 5 TO HAVE EXTENDED GREEN WITH 12" ARROW IN BOTTOM
 † FACE 3 TO HAVE EXTENDED GREEN WITH 12" ARROW IN BOTTOM

LEGEND

SYMBOL	ITEM	DESCRIPTION
—	300 A	2" FIBER CONDUIT
—	300 D	3" FIBER CONDUIT
—	301 A	1 1/2" GALVANIZED STEEL CONDUIT
—	301 B	2" GALVANIZED STEEL CONDUIT
—	301 C	2 1/2" GALVANIZED STEEL CONDUIT
□	304 A	FURNISH & INSTALL PULL BOX
□	304 B	FURNISH & INSTALL PULL BOX (MAGNETIC DETECTOR)
+	307	MAGNETIC DETECTOR (NON DIRECTIONAL)
—	321 A, B, C	CONTROLLER
—	322	ADJUSTABLE SIGNALS
—	323	CABLES
—	324	SPAN WIRE
○	325 B	28' STEEL POLE (ANCHOR BASE TYPE)
△	325 P	PUSH BUTTON POLE
●	325 S	20' STEEL POLE (ANCHOR BASE TYPE)
⊙	325 W	35' TEMPORARY WOOD POLE
P	327	PEDESTRIAN PUSH BUTTON WITH SIGN

GENERAL NOTES - TRAFFIC SIGNALS

- NO DUCT SPLICES ARE PERMITTED. SPLICES SHALL BE MADE IN PULL BOXES. THE CONTRACTOR IS CAUTIONED TO ACCURATELY DETERMINE THE NECESSARY CABLE LENGTH BEFORE CUTTING CABLE.
- IF ELECTRIC POWER IS TO BE TAKEN FROM UTILITY COMPANY MANHOLE, THE CONTRACTOR SHALL EXTEND, AFTER APPROVAL FROM POWER COMPANY, THE CONDUIT AND POWER CABLE INTO MANHOLE WITH A 4" EXTENSION ON THE CABLE. THE UTILITY COMPANY SHALL MAKE ALL POWER CABLE CONNECTIONS TO LIVE WIRES.
- THE MAGNETIC DETECTORS SHALL BE LOCATED AS SHOWN ON THE PLANS. THE FINAL LOCATION OF THE DETECTOR SHALL BE DETERMINED BY PLACING THE DETECTOR AT THE LOCATION WHICH GIVES THE MAXIMUM EFFECT FOR VEHICLES TO BE DETECTED AND THE MINIMUM EFFECT FOR ALL OTHER VEHICLE MOVEMENTS.
- THE PROPOSED POLE LOCATIONS SHOWN ON PLAN ARE APPROXIMATE. THE EXACT LOCATIONS OF POLES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
- FORMING OF POLE FOUNDATIONS WILL NOT BE PERMITTED.
- ANCHOR BOLTS SHALL BE AS RECOMMENDED BY THE POLE MANUFACTURER.
- ELEVATION OF TOP OF CONCRETE FOUNDATION FOR ANCHOR BASE POLES SHALL ALLOW FOR ACCESS TO ADJUSTING NUTS UNDER BASE PLATE. AFTER POLE IS ADJUSTED, FOOTING SHALL BE BROUGHT TO FINISHED GRADE WITH MORTAR.
- CONTROLLER CABINETS SHALL BE RADIALY MOUNTED ON POLES A.G.B.E.
- SIGNALS ON FILLMORE AVENUE TO BE INTERCONNECTED WITH EXISTING KENSINGTON AVENUE AND GLENNY DRIVE SIGNALS, BY OTHERS.
- PAVEMENT MARKINGS, ROAD CLOSURE BARRICADES AND SIGNS REQUIRED IN CONJUNCTION WITH TEMPORARY DETOUR TRAFFIC SIGNALS SHALL BE FURNISHED AND ERECTED BY THE CONTRACTOR UNDER ITEM 76.5.
- THE CONTRACTOR WILL PROVIDE THE RIGHT OF ACCESS TO THE SIGN CONTRACTOR WORKING UNDER A SEPARATE STATE CONTRACT.
- CONDUITS SHALL BE DRAINED TO THE PULL BOXES.



ITEM NO. 307
MAGNETIC DETECTOR - NON-DIRECTIONAL
(LOOKING IN DIRECTION OF TRAVEL, GLENNY DRIVE & S.B.H.P. SERVICE ROAD OPPOSITE HAND)
SCALE: 1/4" = 1'-0"

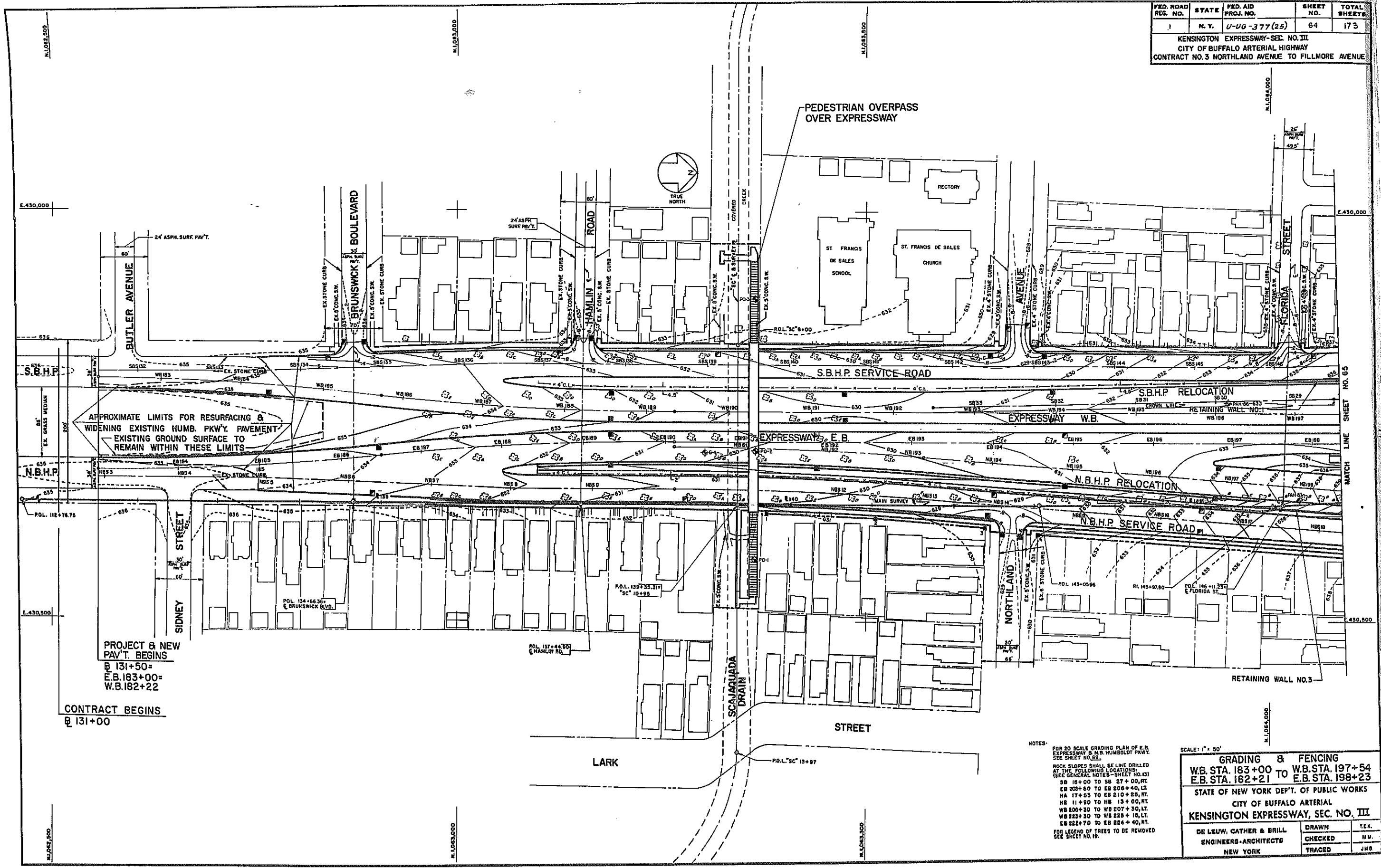
TRAFFIC SIGNAL PLANS

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DR	LEUW, CATHER & BRILL	ENGINEERS-ARCHITECTS	NEW YORK	DRAWN	C.C.R.
				CHECKED	R.J.D.
				TRACED	C.C.R.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	64	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



APPROXIMATE LIMITS FOR RESURFACING & WIDENING EXISTING HUMB. PKWY. PAVEMENT - EXISTING GROUND SURFACE TO REMAIN WITHIN THESE LIMITS

PROJECT & NEW PAV'T. BEGINS @ 131+50 = E.B. 183+00 = W.B. 182+22

CONTRACT BEGINS @ 131+00

NOTES:
FOR 20 SCALE GRADING PLAN OF E.B. EXPRESSWAY & N.B. HUMBOLDT PAV'T. SEE SHEET NO. 62.
ROCK SLOPES SHALL BE LINE DRILLED AT THE FOLLOWING LOCATIONS: (SEE GENERAL NOTES - SHEET NO. 12)
SB 16+00 TO SB 27+00, RT
EB 203+80 TO EB 206+40, LT
NB 174+55 TO EB 210+25, RT
NB 11+90 TO NB 13+00, RT
WB 204+30 TO WB 207+50, LT
WB 223+30 TO WB 228+15, LT
EB 222+70 TO EB 224+40, RT.
FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 19.

SCALE: 1" = 50'

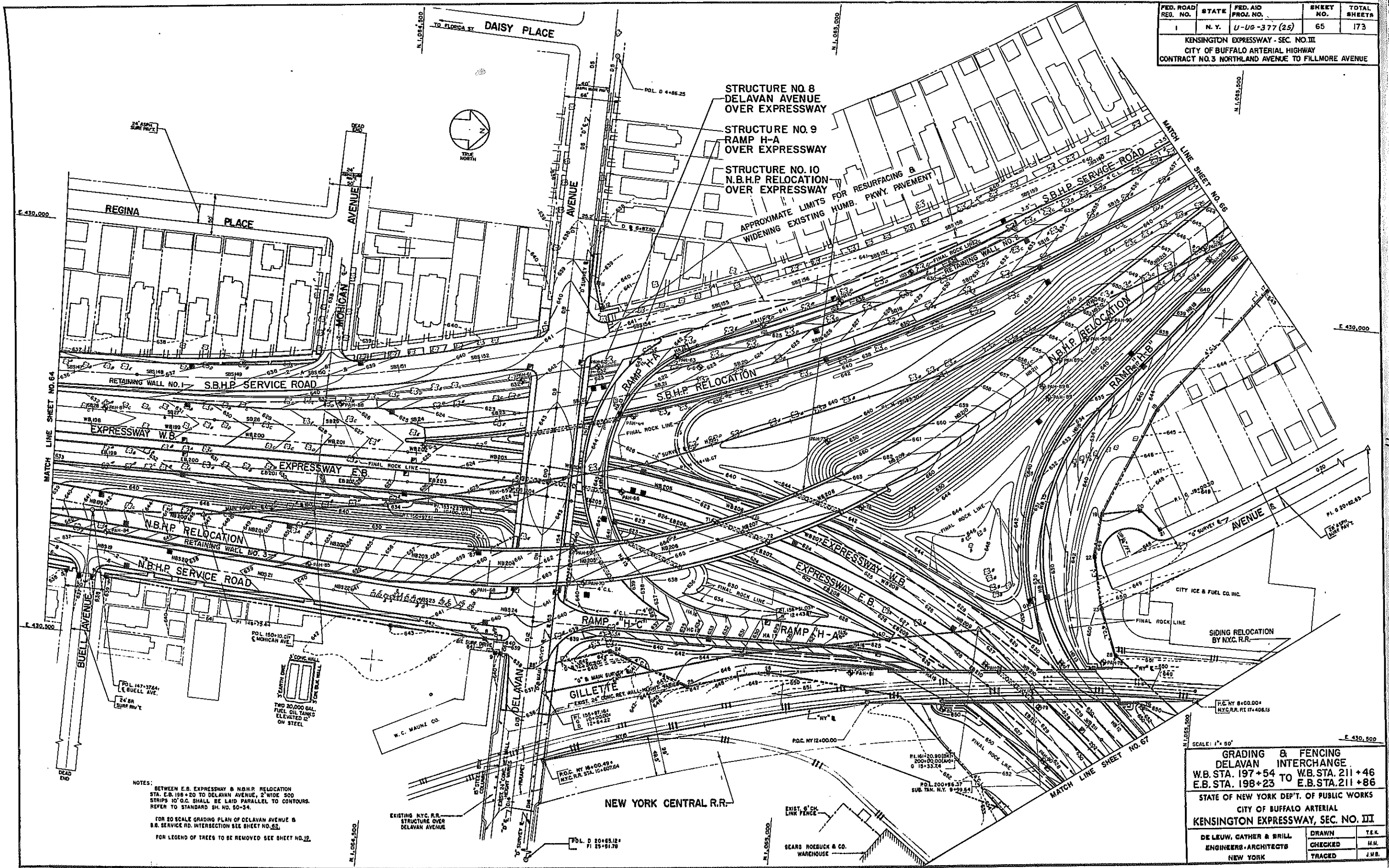
GRADING & FENCING	
W.B. STA. 183+00 TO	W.B. STA. 197+54
E.B. STA. 182+21	E.B. STA. 198+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	T.E.K.
ENGINEERS-ARCHITECTS	CHECKED	H.M.
NEW YORK	TRACED	J.H.B.

FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	65	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



STRUCTURE NO. 8
 DELAVAN AVENUE
 OVER EXPRESSWAY

STRUCTURE NO. 9
 RAMP H-A
 OVER EXPRESSWAY

STRUCTURE NO. 10
 N.B.H.P. RELOCATION
 OVER EXPRESSWAY

APPROXIMATE LIMITS FOR RESURFACING &
 WIDENING EXISTING HUMB. PKWY. PAVEMENT

NOTES:
 BETWEEN E.B. EXPRESSWAY & N.B.H.P. RELOCATION
 STA. E.B. 198+20 TO DELAVAN AVENUE, 2' WIDE 500
 STRIPS 10' O.C. SHALL BE LAID PARALLEL TO CONTOURS.
 REFER TO STANDARD SH. NO. 50-34.

FOR 20 SCALE GRADING PLAN OF DELAVAN AVENUE &
 S.B. SERVICE RD. INTERSECTION SEE SHEET NO. 63.

FOR LEGEND OF TREES TO BE REMOVED SEE SHEET NO. 12.

SCALE: 1" = 50'

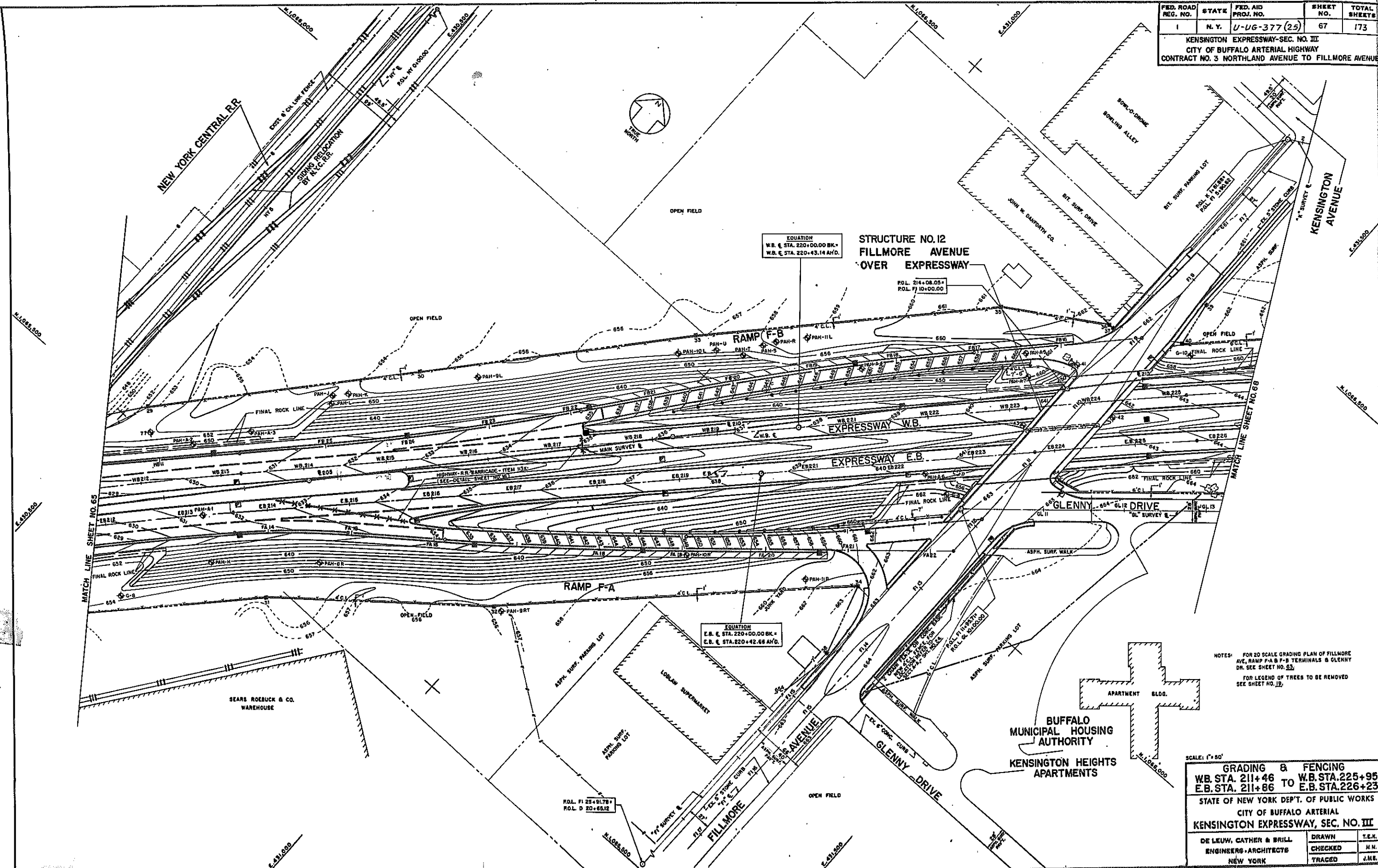
**GRADING & FENCING
 DELAVAN INTERCHANGE.**
 W.B. STA. 197+54 TO W.B. STA. 211+46
 E.B. STA. 198+23 TO E.B. STA. 211+86

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. M.H. J.M.B.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	67	173

KENSINGTON EXPRESSWAY-SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



EQUATION
W.B. & STA. 220+00.00 BK.
W.B. & STA. 220+43.14 AFD.

STRUCTURE NO. 12
FILLMORE AVENUE
OVER EXPRESSWAY

POL. 214+08.05 =
POL. FJ 10+00.00

EQUATION
E.B. & STA. 220+00.00 BK.
E.B. & STA. 220+42.66 AFD.

NOTES:
FOR 20 SCALE GRADING PLAN OF FILLMORE
AVE, RAMP F-A & F-B TERMINALS & GLENNY
DR. SEE SHEET NO. 65.
FOR LEGEND OF TREES TO BE REMOVED
SEE SHEET NO. 13.

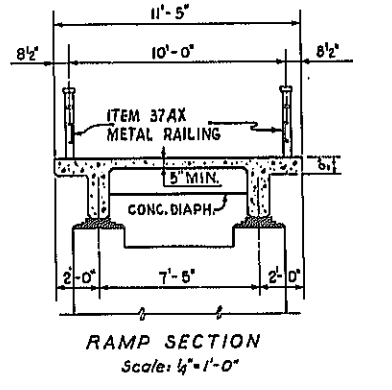
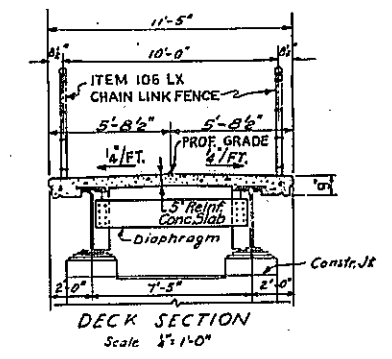
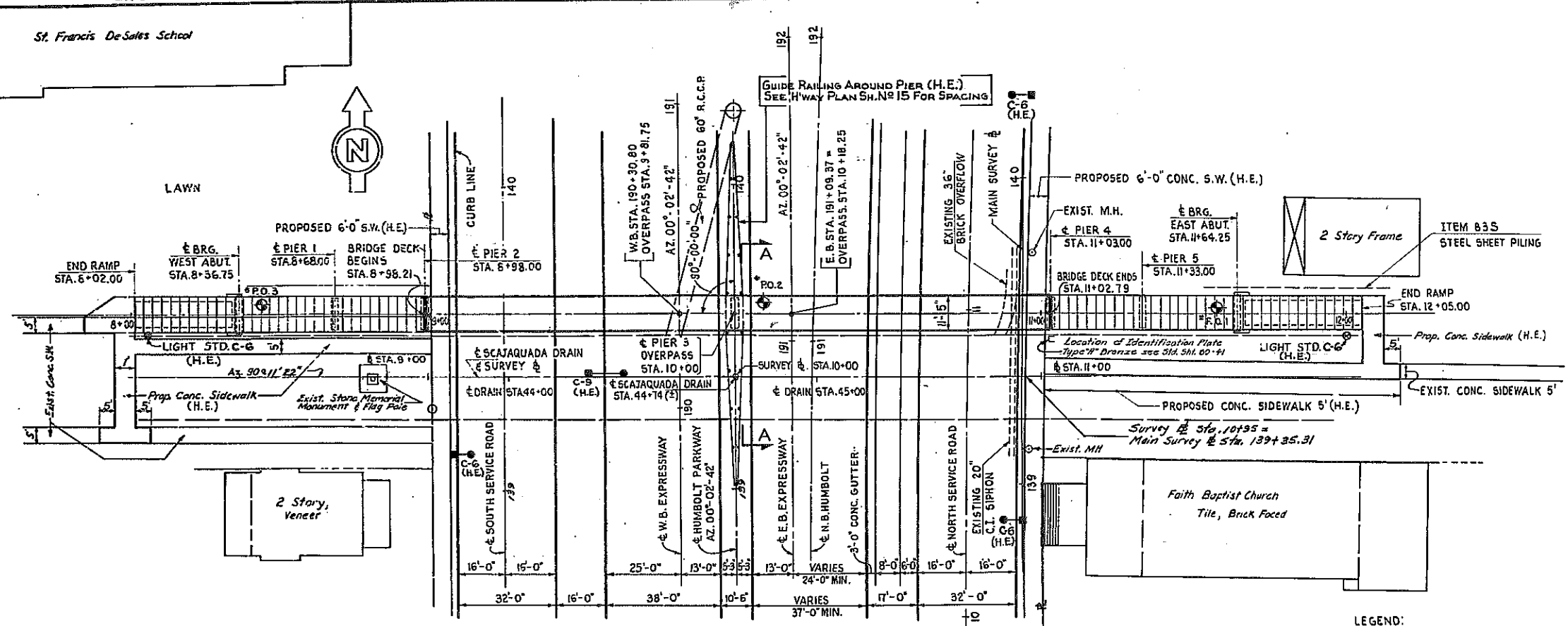
SCALE: 1" = 50'
GRADING & FENCING
W.B. STA. 211+46 TO W.B. STA. 225+95
E.B. STA. 211+86 TO E.B. STA. 226+23

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

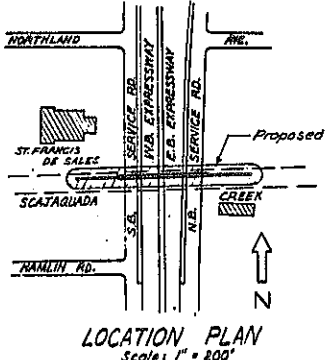
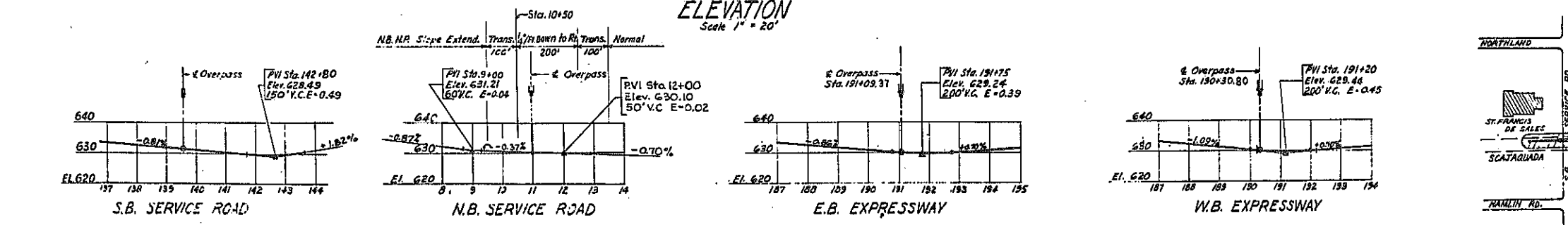
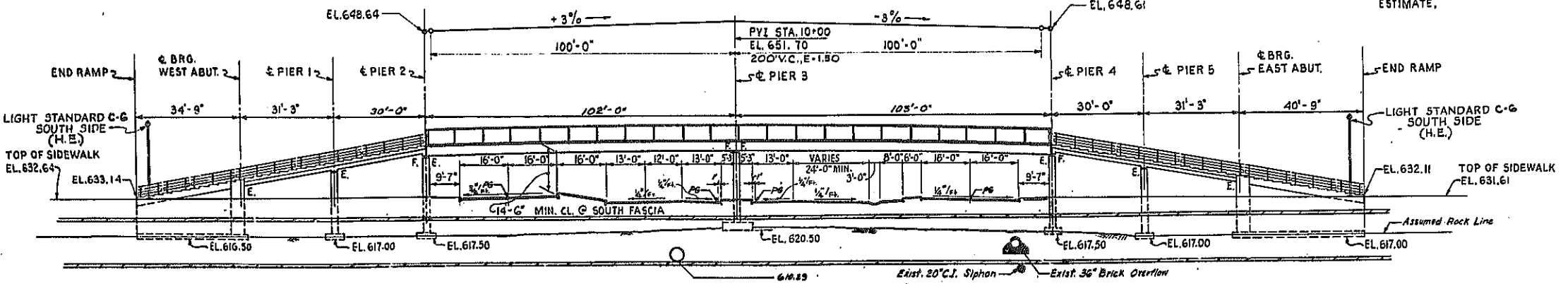
DE LEUW, CATHY & BRILL	DRAWN	T.E.N.
ENGINEERS-ARCHITECTS	CHECKED	H.N.
NEW YORK	TRACED	J.M.E.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	69	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE



LEGEND:
 ⊕ - INDICATES BORING LOCATION.
 E. - EXPANSION BEARING.
 F. - FIXED BEARING.
 H.E. - ITEM MARKED (H.E.) TO BE INCLUDED IN HIGHWAY ESTIMATE.



PEDESTRIAN OVERPASS - E.B. STA. 191+09.37
GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

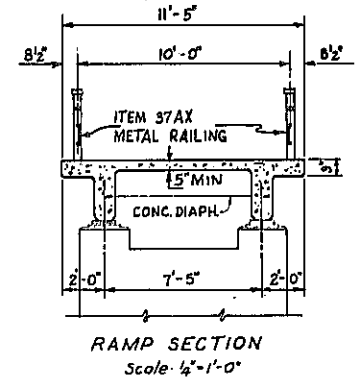
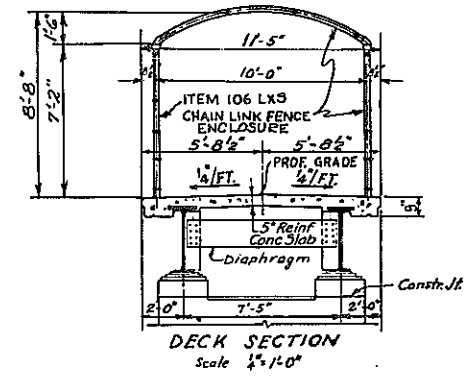
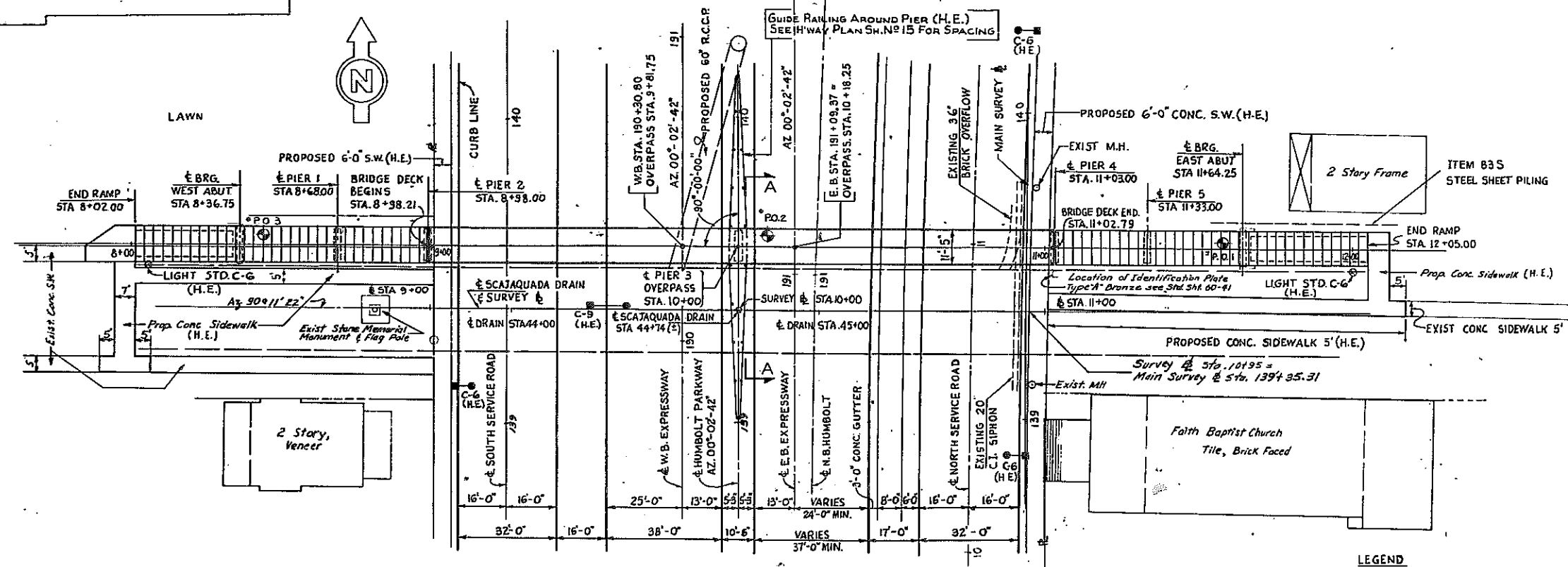
DE LEUW, CATHAR & GRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E.B. W.T. E.B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N Y	U-16-377(25)	695	173

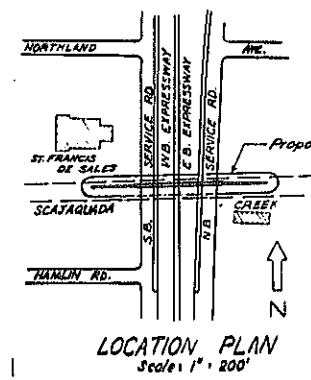
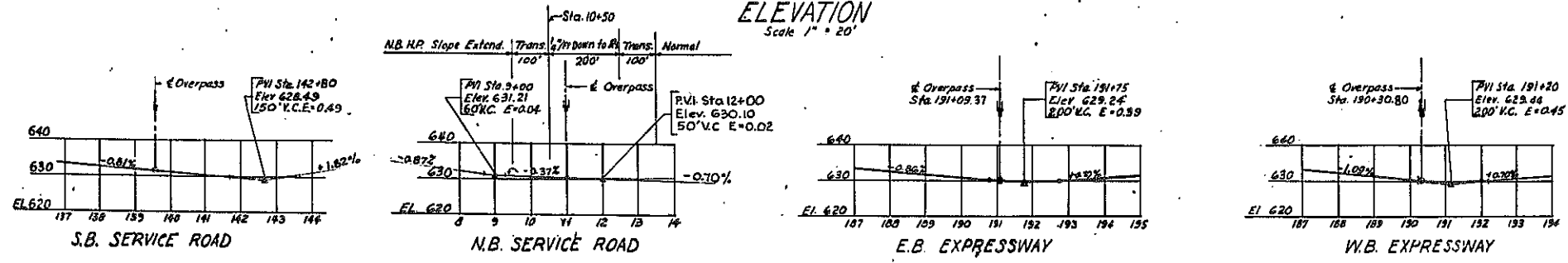
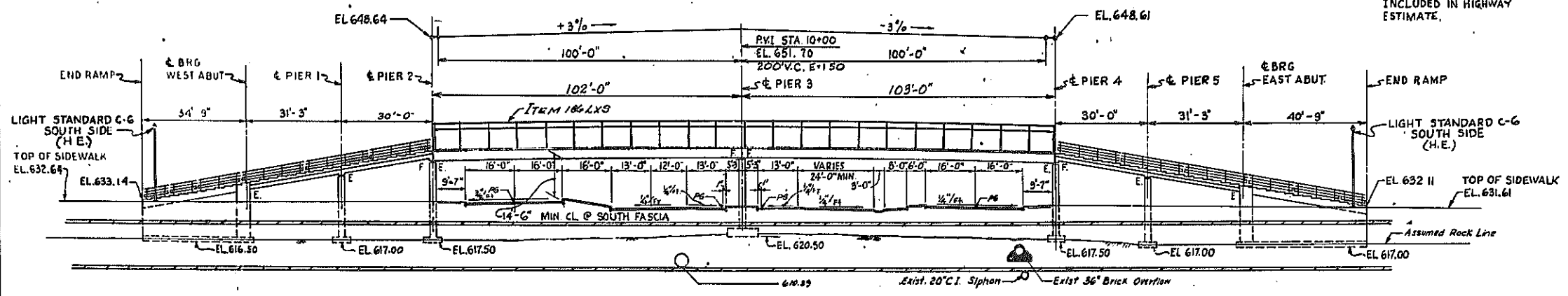
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

FIELD CHANGE

FIELD CHANGE
MF 69F1



LEGEND
 ● - INDICATES BORING LOCATION.
 E. - EXPANSION BEARING
 F. - FIXED BEARING
 H.E. - ITEM MARKED (H.E.) TO BE INCLUDED IN HIGHWAY ESTIMATE.



APPROVED
V.J. BURNS
Nov. 14, 1963
DEPUTY CHIEF ENGINEER

PREPARED PURSUANT TO THE HIGHWAY LAW
AND RECOMMENDED BY
Nov. 7, 1963
DIST. ENGINEER, DIST. NO. 5

FIELD CHANGE

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37
GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN E. B.
	CHECKED V. T.
	TRACED E. B.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	70	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.R.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : PEDESTRIAN SLAB LOADING 85 LBS. PER SQ. FT.

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.
THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE AND THE PIERS ABOVE FOOTINGS SHALL BE ITEM 18X CLASS 1A CONCRETE FOR STRUCTURES.
THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS AND WINGWALLS SHALL BE ITEM 20Y CLASS 1 CONCRETE.

CEMENT : CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

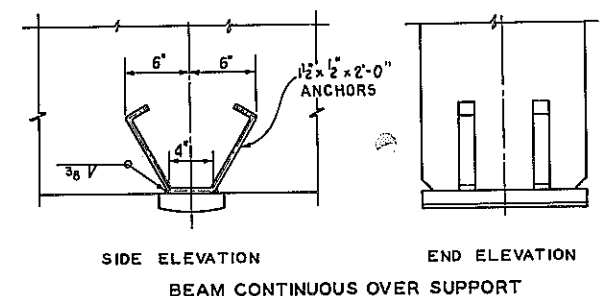
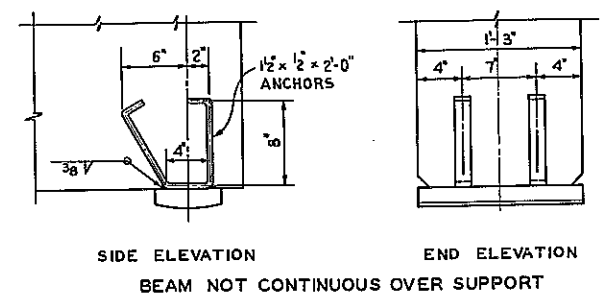
IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE BRONZE, TYPE C WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STANDARD SHEET 60-41.

OTHER : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. PEDESTRIAN SLAB SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER.

SURFACE OF PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALLOWED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.
THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER, COMPRESSED ASBESTOS SHEET PACKING AND ASPHALT HOOPING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

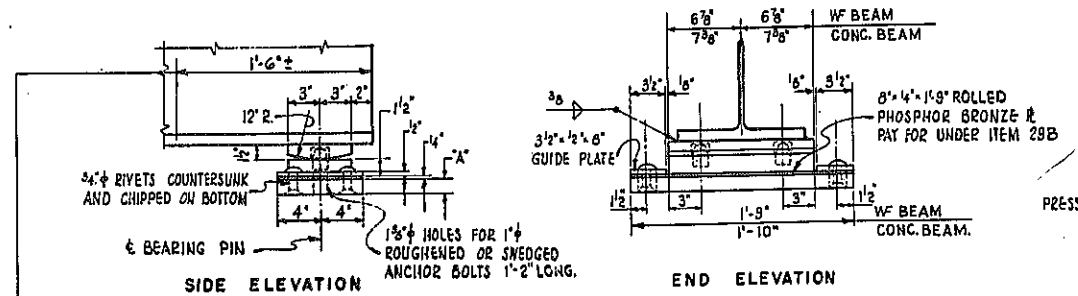
FOOTINGS : ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

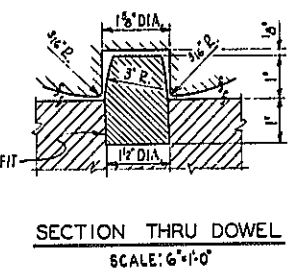
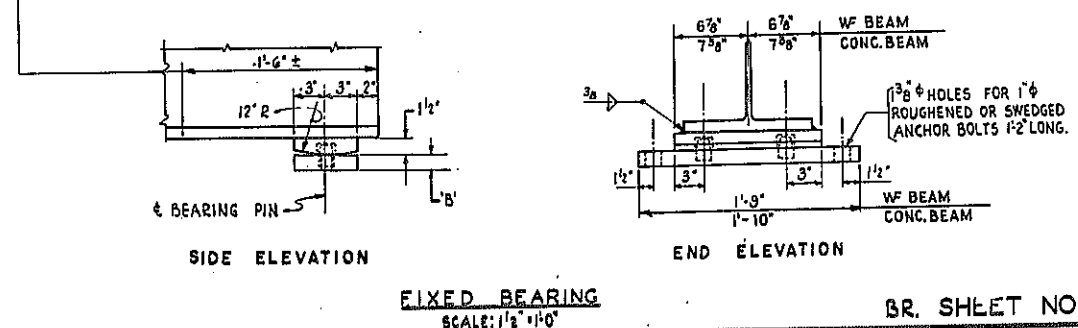


BEARING ANCHOR DETAILS FOR CONCRETE BEAMS

SCALE: 1/2" = 1'-0"
NOTE: PLACE ANCHORS SO THAT THEY WILL NOT INTERFERE WITH BEAM REINFORCEMENT.



THIS PORTION OF BOTTOM FLANGE TO BE TRULY AT RIGHT ANGLES TO WEB.
SCALE: 1/2" = 1'-0"



NOTE: FOR SHOES UNDER CONCRETE BEAMS SEE SH. NO. 8. FOR SHOES UNDER STEEL BEAMS, SEE FRAMING PLAN, SH. NO. 10.

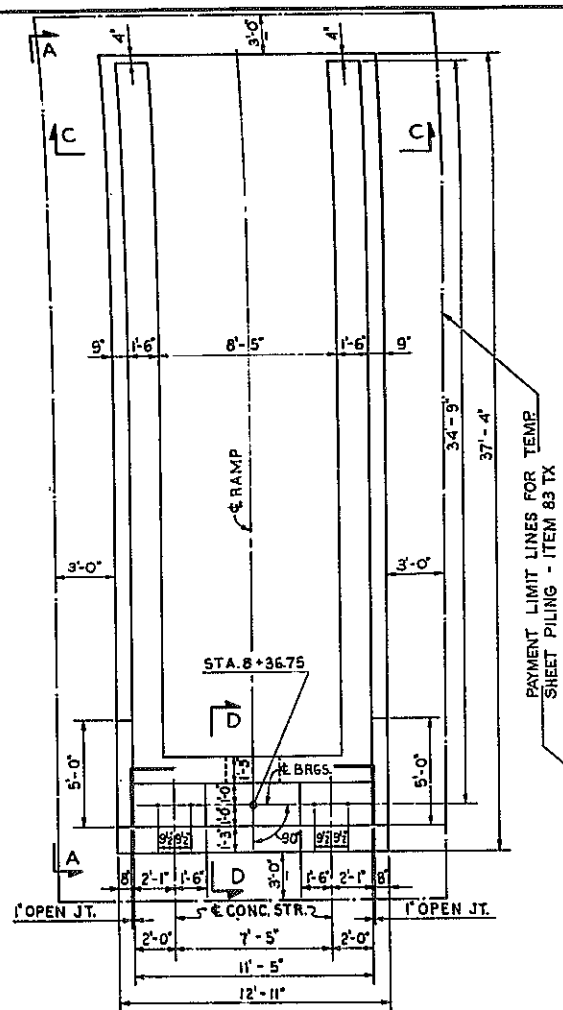
ESTIMATE OF QUANTITIES				
ITEM NO	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	921	955
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	252	260
20Y	CLASS 1 CONCRETE	C.Y.	334	340
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	53,000	54,000
29B	STRUCTURAL STEEL	Lb.	98,900	101,600
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	396	410
61	BITUMINOUS MATERIAL	GAL.	170	180
83S	STEEL SHEET PILING	S.F.	830	870
83TX	TEMPORARY SHEET PILING	S.F.	6887	7240
106 LX	CHAIN LINK FENCE	L.F.	407	420
352XA	RUBBER JOINT MATERIAL	GAL.	2	2
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	10	12
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	1605	1810

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

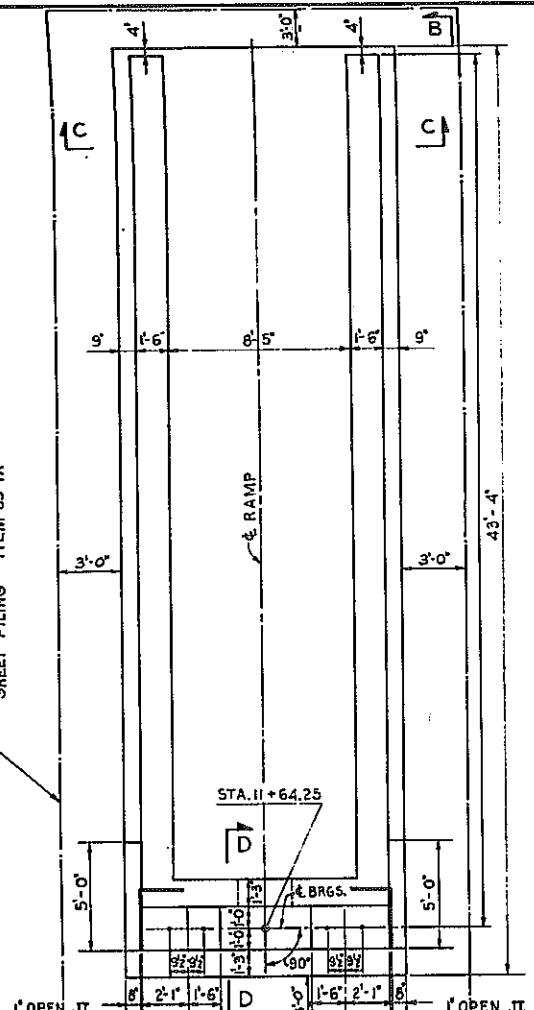
PEDESTRIAN OVERPASS - E.B. STA. 191+09.37
NOTES, BEARINGS AND ESTIMATE
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK
DRAWN S.S.
CHECKED W.T.
TRACED S.S.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	71	173

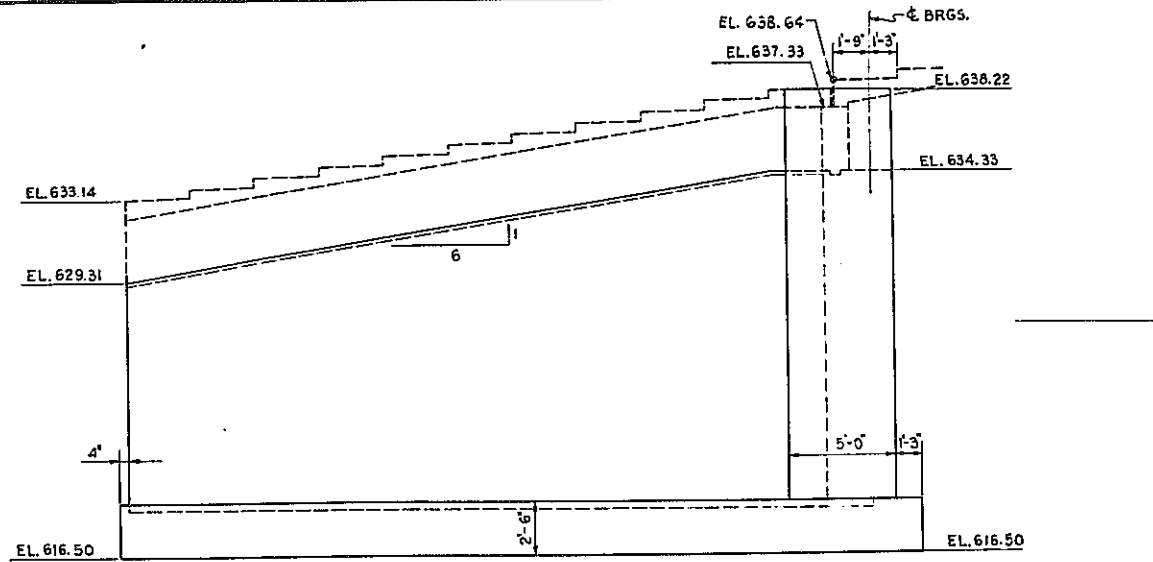
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO.3 NORTHLAND AVENUE TO FILLMORE AVENUE



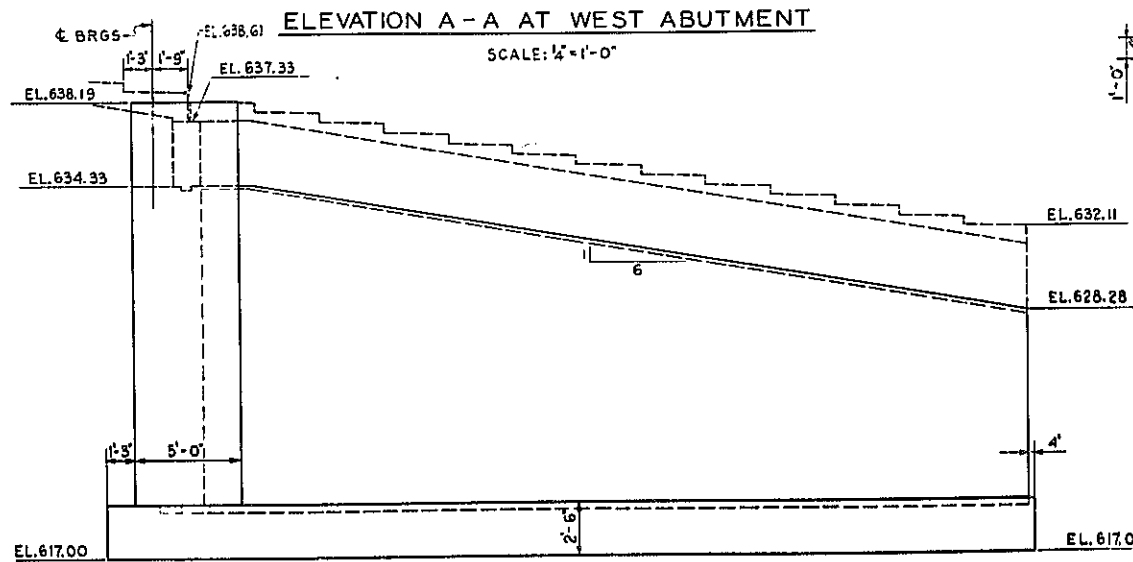
PLAN - WEST ABUTMENT
SCALE: 1/4" = 1'-0"



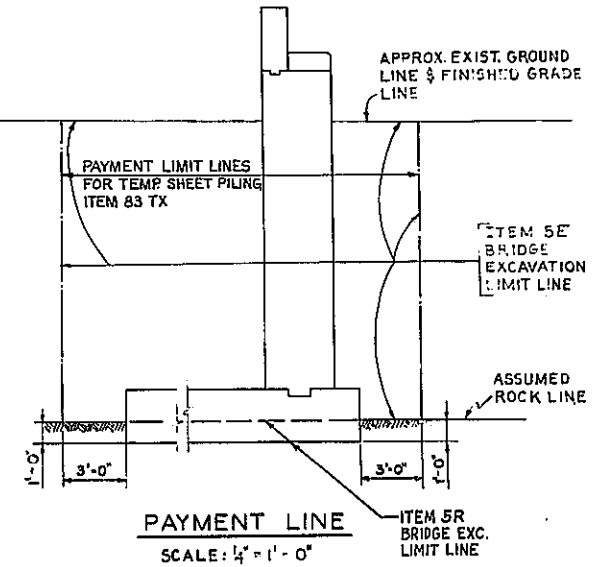
PLAN - EAST ABUTMENT
SCALE: 1/4" = 1'-0"



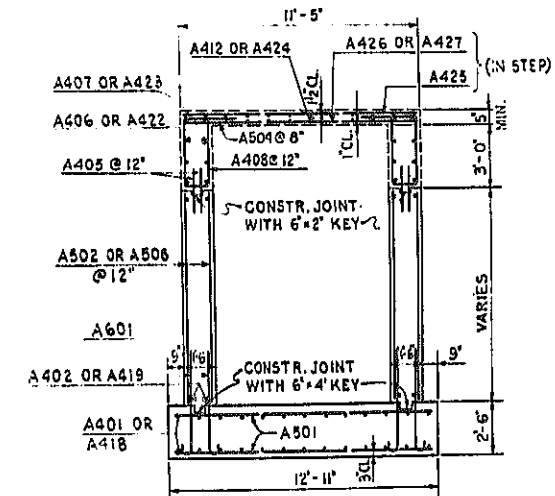
ELEVATION A-A AT WEST ABUTMENT
SCALE: 1/4" = 1'-0"



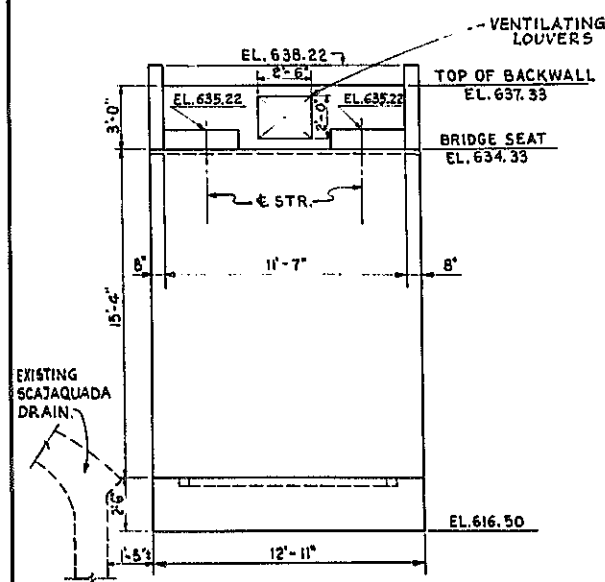
ELEVATION B-B AT EAST ABUTMENT
SCALE: 1/4" = 1'-0"



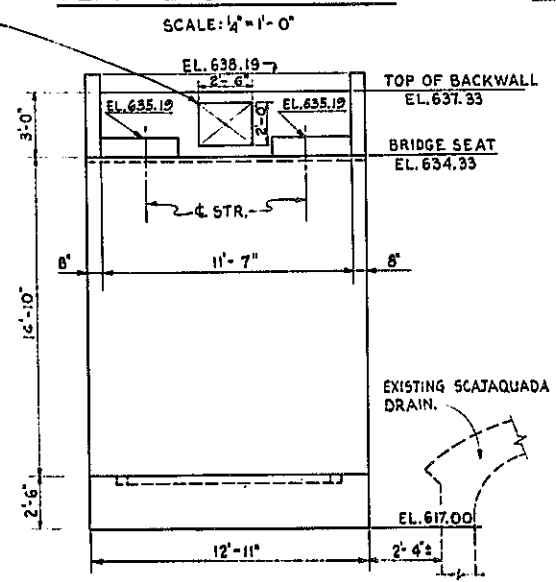
PAYMENT LINE
SCALE: 1/4" = 1'-0"



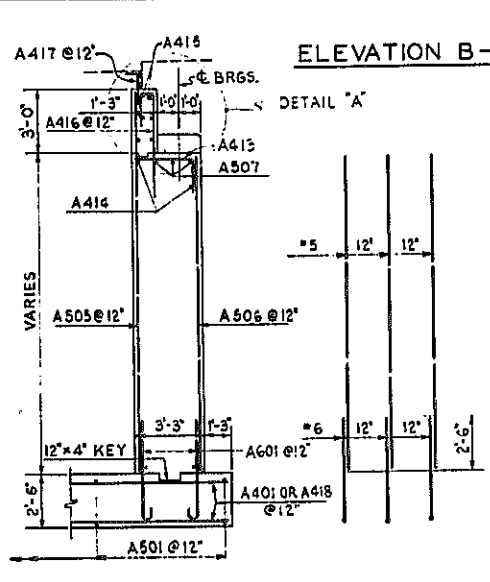
SECTION C-C
SCALE: 1/4" = 1'-0"



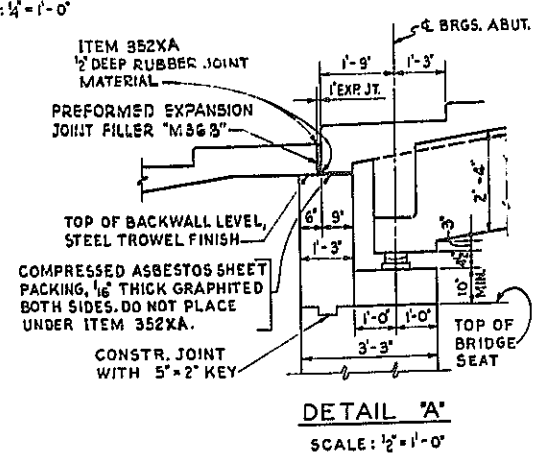
ELEVATION - W. ABUTMENT
SCALE: 1/4" = 1'-0"



ELEVATION - E. ABUTMENT
SCALE: 1/4" = 1'-0"



SECTION D-D
SCALE: 1/4" = 1'-0"



DETAIL 'A'
SCALE: 1/2" = 1'-0"

- NOTES:
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - ALL CONCRETE IN ABUTMENT AND SIDEWALLS SHALL BE ITEM 20Y, CLASS I CONCRETE.
 - ALL CONCRETE IN RAMPS AND BEAMS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.
 - BITUMINOUS MATERIAL, ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND SIDEWALLS FROM TOP OF FOOTING TO FINISHED GRADE LINE.
 - FOR RAILING DETAILS, SEE SH. NO. 12.

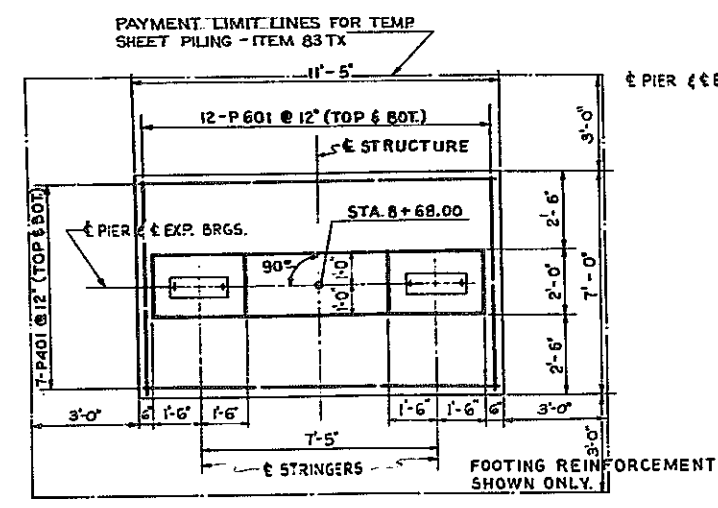
PEDESTRIAN OVERPASS - E. B. STA. 191+09.37
ABUTMENTS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

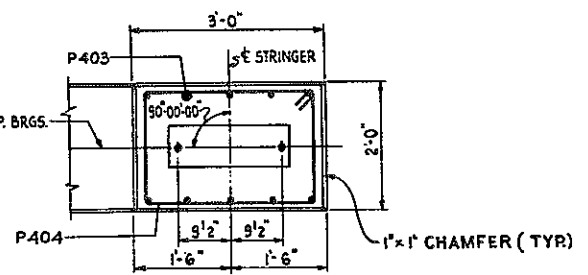
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E. B. W. T. E. B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(23)	73	173

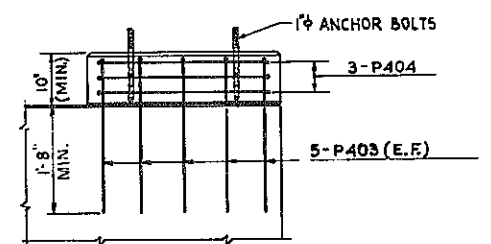
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN - PIER 1
SCALE: 3/8" = 1'-0"

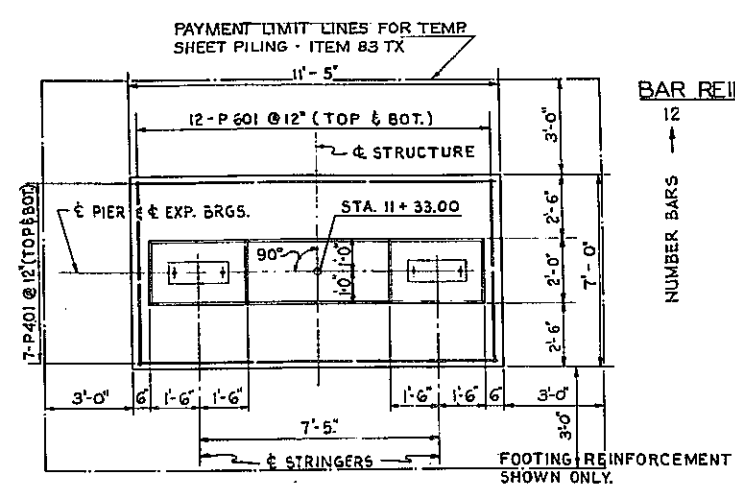


PLAN



ELEVATION

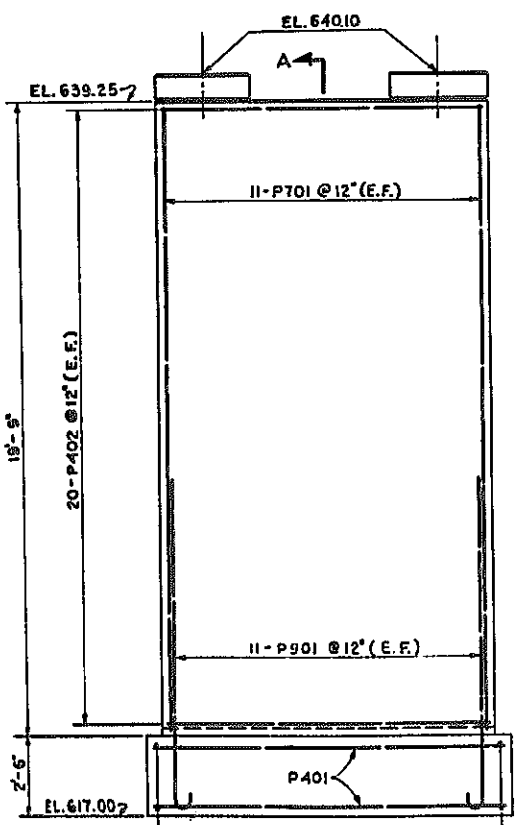
PEDESTAL DETAILS
SCALE: 3/4" = 1'-0"



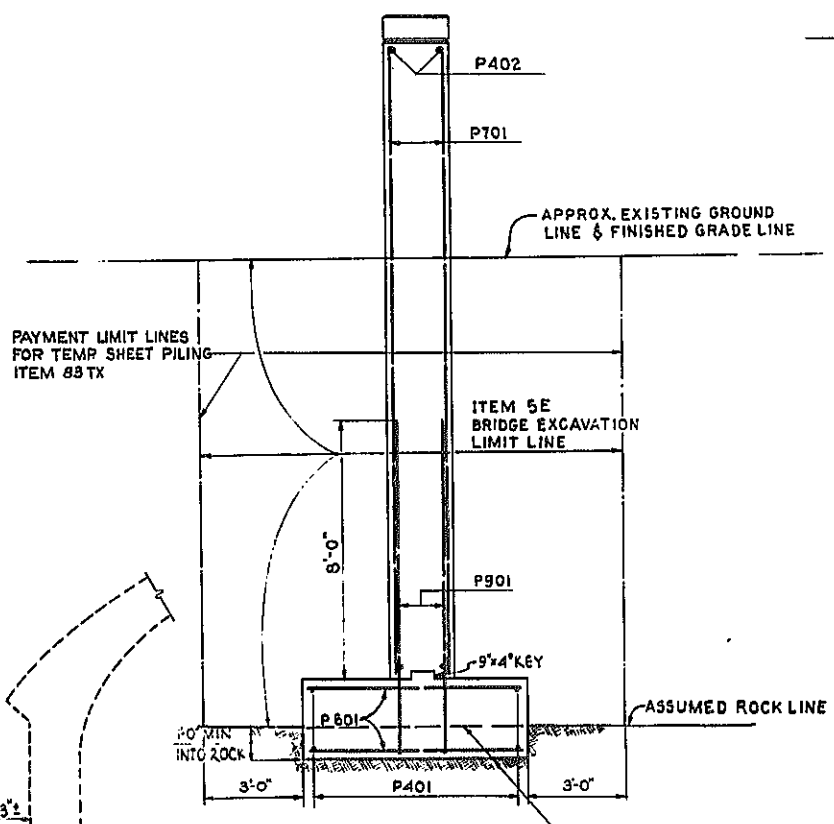
PLAN - PIER 5
SCALE: 3/8" = 1'-0"

BAR REINFORCING LEGEND

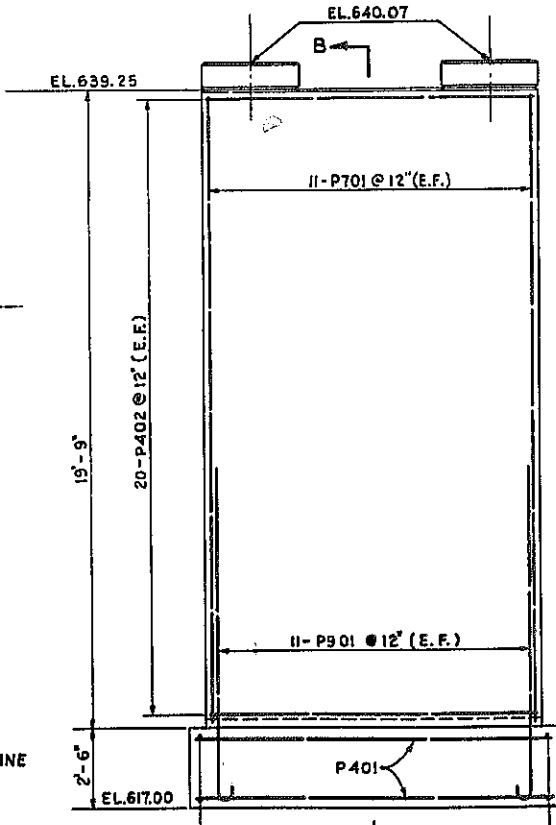
NUMBER BARS	PIER	BAR SIZE	BAR MARK
12	P	6	01



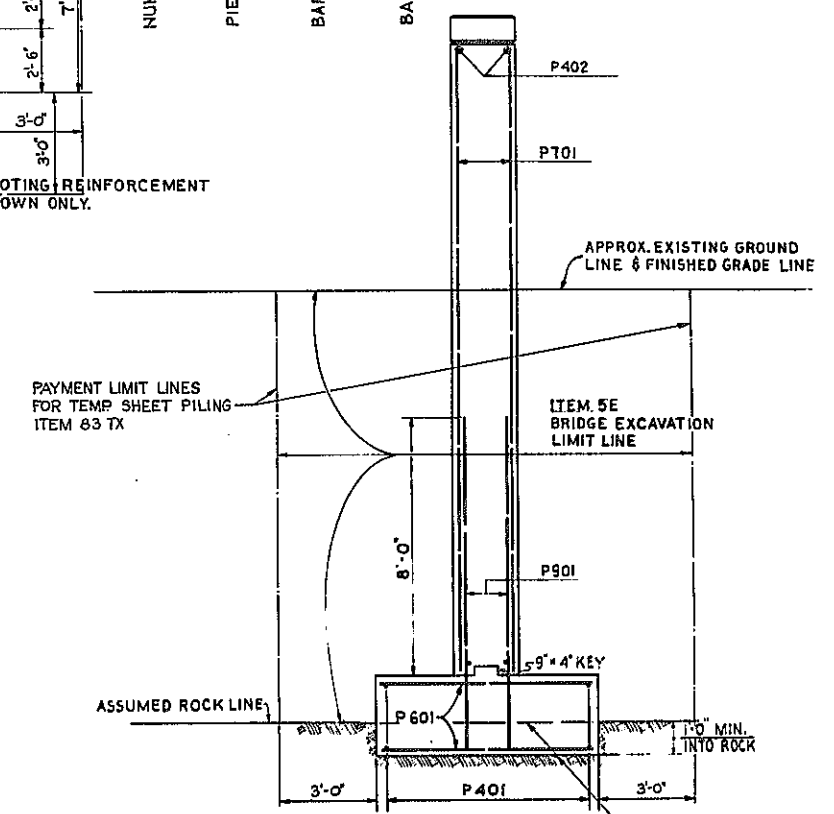
ELEVATION - PIER 1
SCALE: 3/8" = 1'-0"



SECTION A - A
SCALE: 3/8" = 1'-0"



ELEVATION - PIER 5
SCALE: 3/8" = 1'-0"



SECTION B - B
SCALE: 3/8" = 1'-0"

- NOTES:
1. FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 2. CONCRETE IN PIER FOOTINGS SHALL BE ITEM 20Y, CLASS I CONCRETE.
 3. CONCRETE IN PIER STEMS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 4. ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.
 5. MINIMUM COVER 2", EXCEPT FOOTING 3".

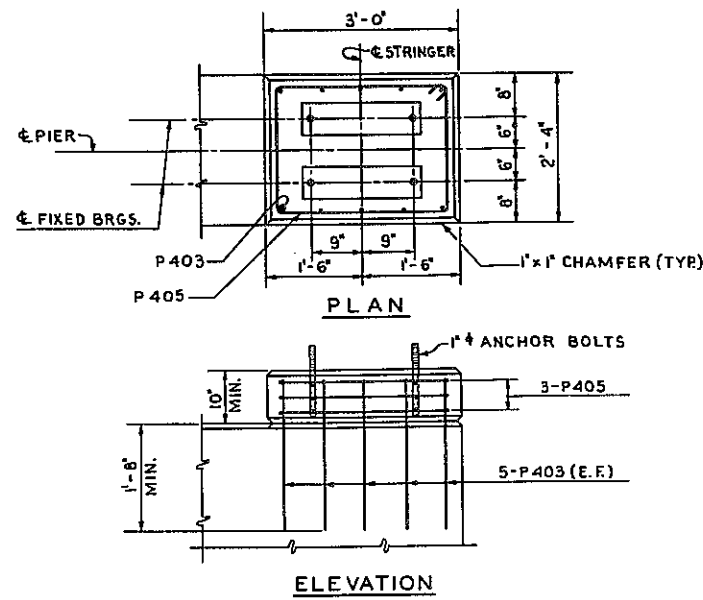
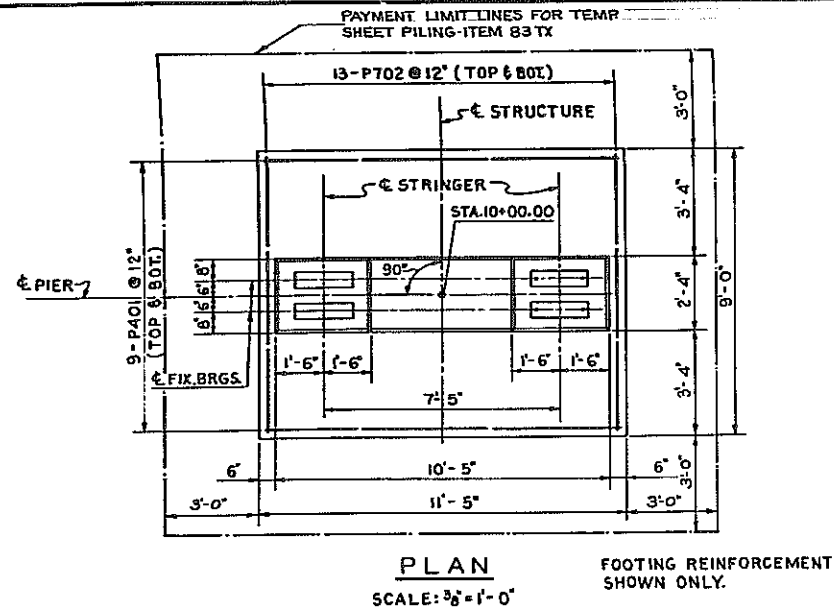
PEDESTRIAN OVERPASS - E.B. STA. 191+09.37
PIERS 1 & 5

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

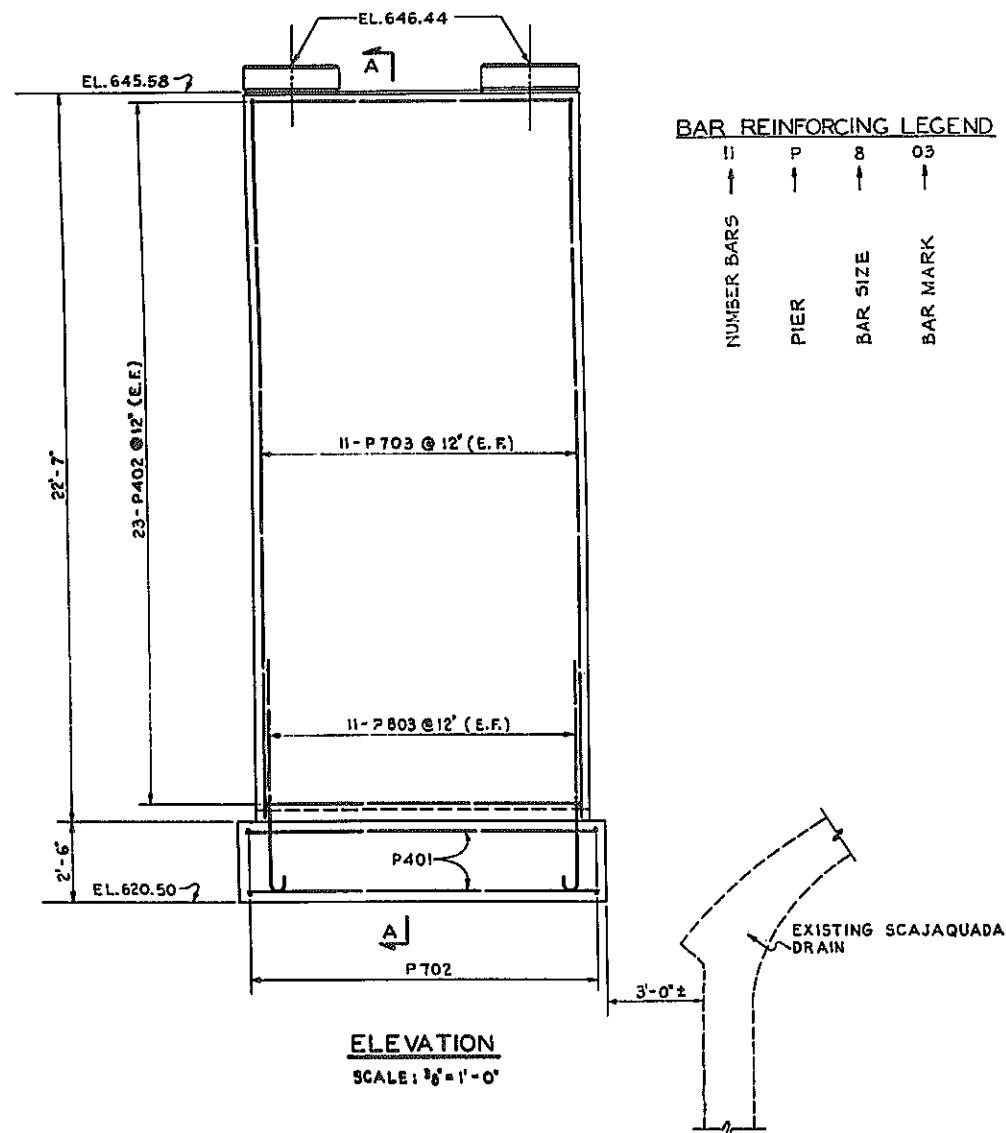
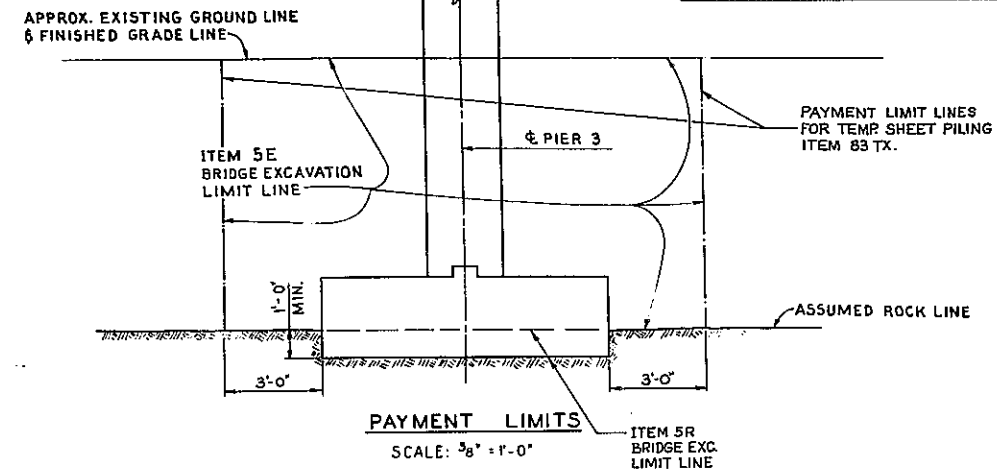
DE LEUW, GATHER & BRILL ENGINEERS - ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	E. B. W. T. E. B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	75	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE

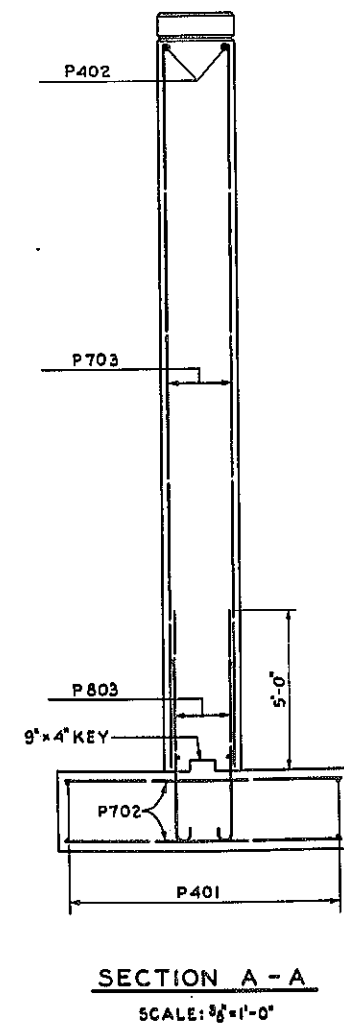


PEDESTAL DETAILS
SCALE: 3/8" = 1'-0"



BAR REINFORCING LEGEND

NUMBER BARS	PIER	BAR SIZE	BAR MARK
11	P	8	03

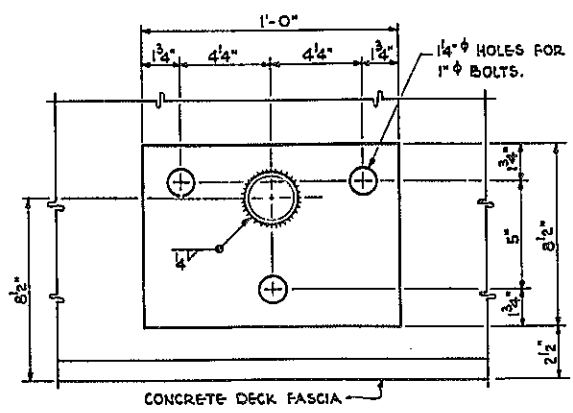
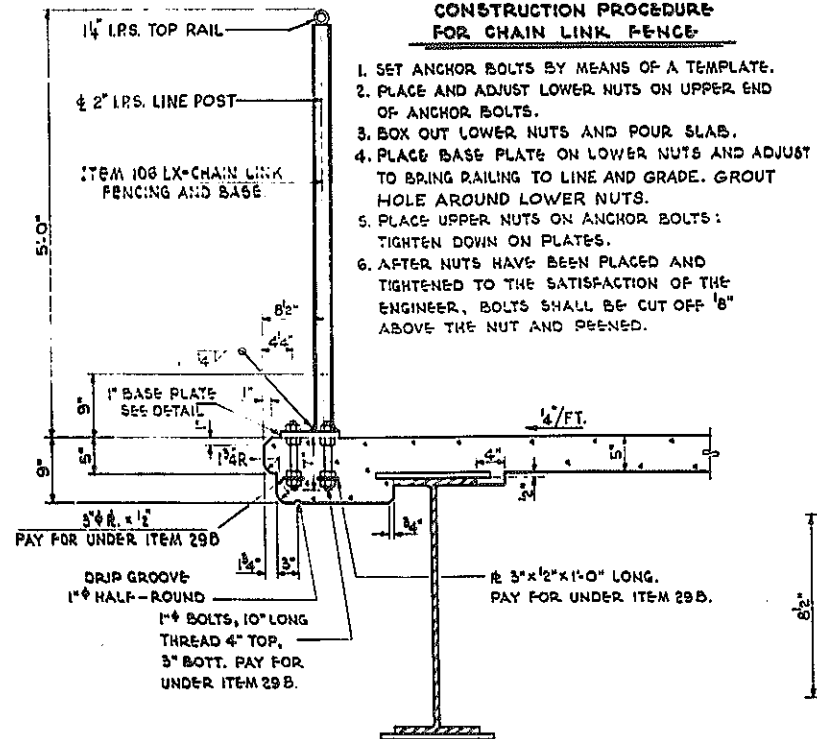
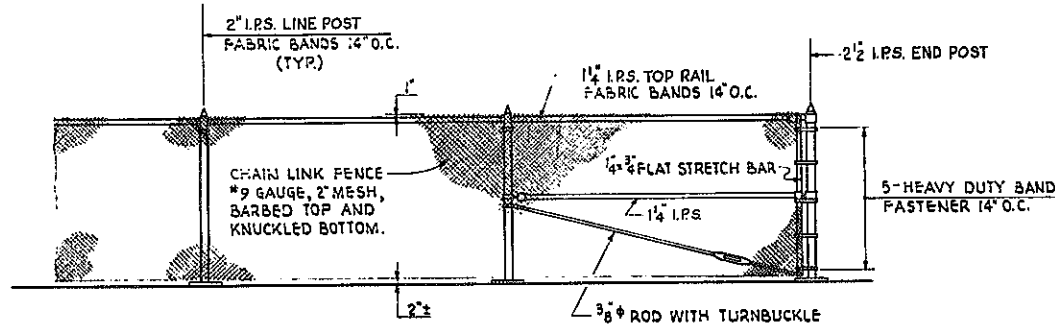
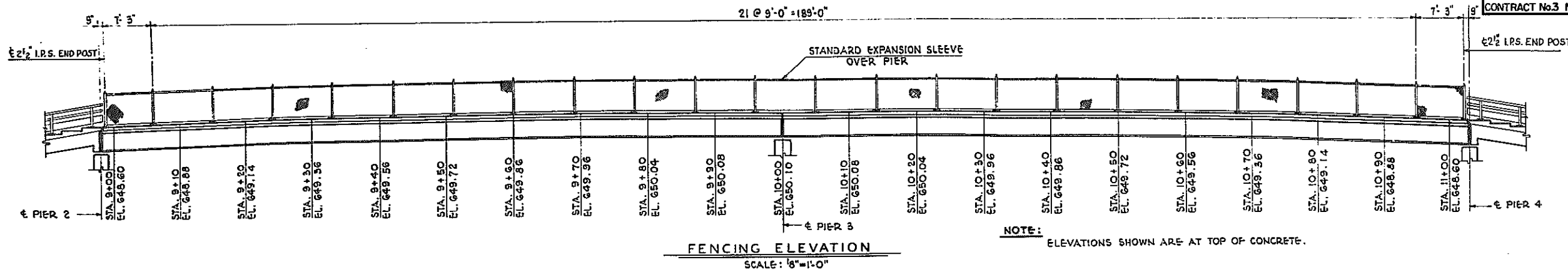


- NOTES:**
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS/SQ. FT.
 - CONCRETE IN PIER FOOTINGS SHALL BE ITEM 20Y, CLASS I CONCRETE.
 - CONCRETE IN PIER STEMS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 - ALL ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS 1'-2" LONG, 2 EACH PER STRINGER.
 - MINIMUM COVER 2", EXCEPT FOOTING 3".

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37		
PIER 3		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LIEW, GATHER & BRILL	DRAWN	E. B.
ENGINEERS-ARCHITECTS	CHECKED	W.T.
NEW YORK	TRACED	E. B.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	79	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No.3 NORTHLAND AVENUE TO FILLMORE AVENUE



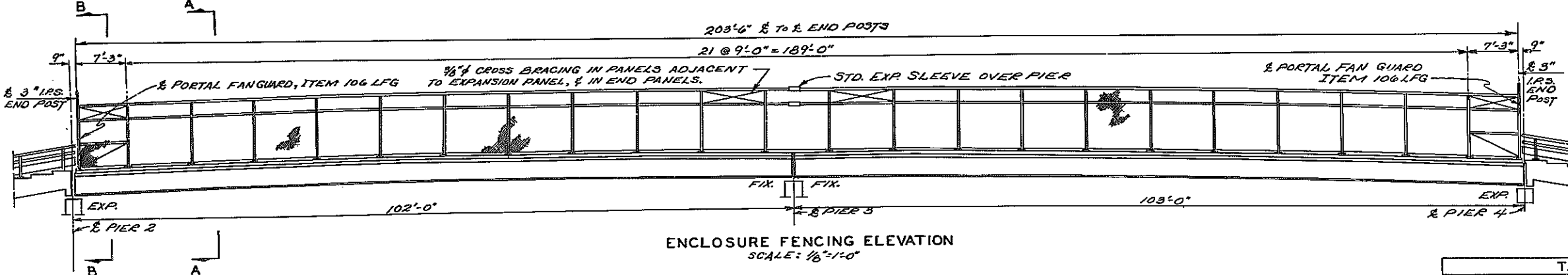
CHAIN LINK FENCING NOTES

1. SIZES OF RAILS AND POSTS ARE GIVEN IN NOMINAL PIPE SIZES (I.R.S.)
2. ALL FENCING POSTS TO BE SET TRULY VERTICAL.
3. FOR REQUIREMENTS OF BASE PLATE AND CHAIN LINK FENCE, SEE SPECIAL PROVISIONS ITEM 106 LX.
4. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
5. PIPE RAILS AND POSTS SHALL MEET ALL THE REQUIREMENTS OF A.S.T.M. A-120-54, STANDARD WEIGHT PIPE (SCHEDULE 40).

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37		
CHAIN LINK FENCING DETAILS		
STATE OF NEW YORK DEP'T. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	A.C. W.T. A.C.

BR. SHEET NO. II

FIELD CHANGE

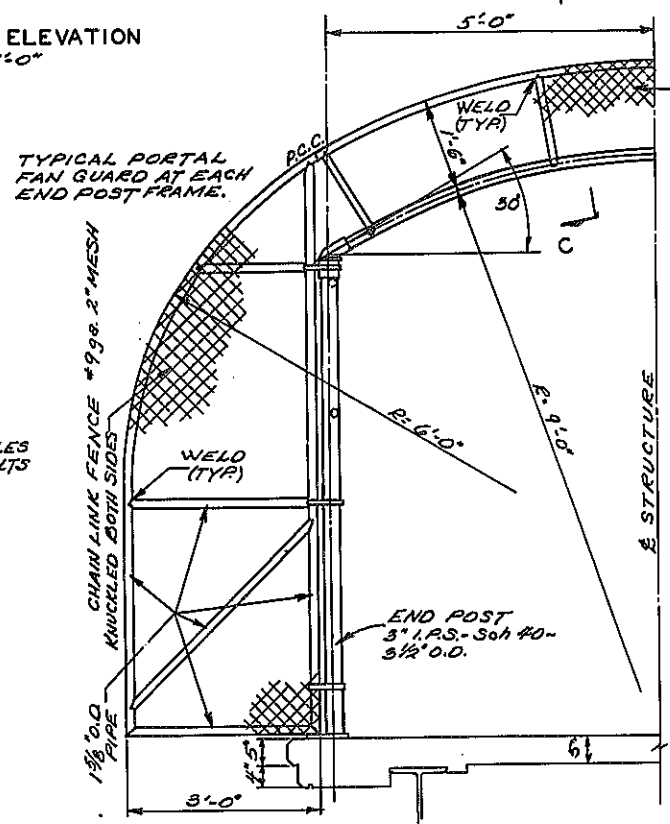
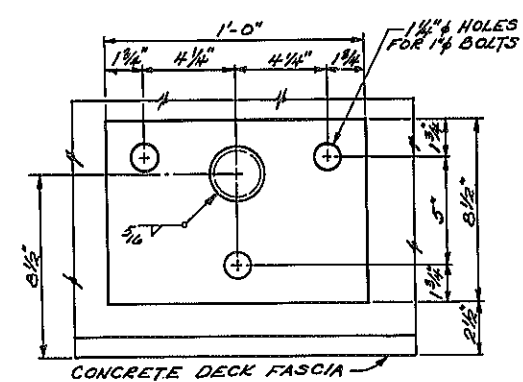
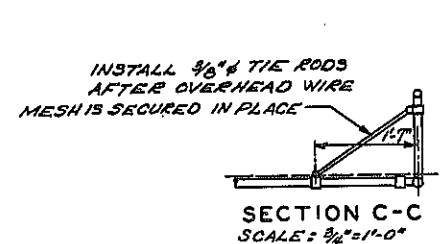
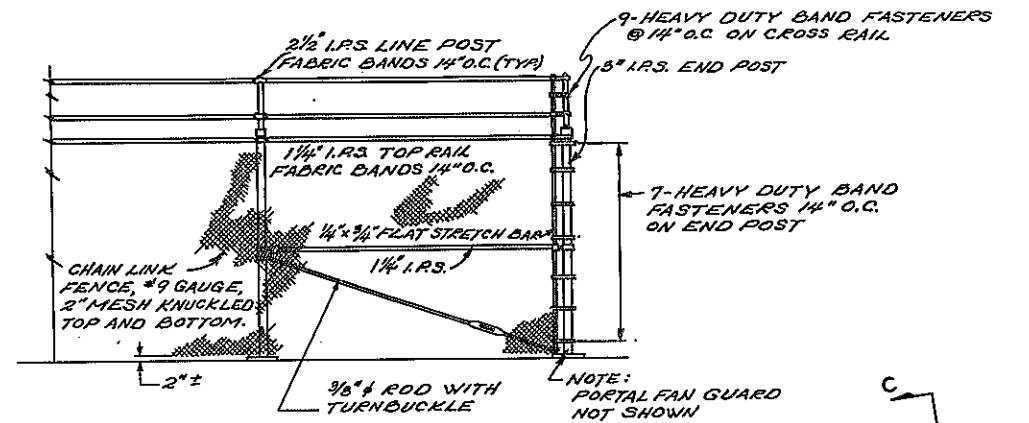
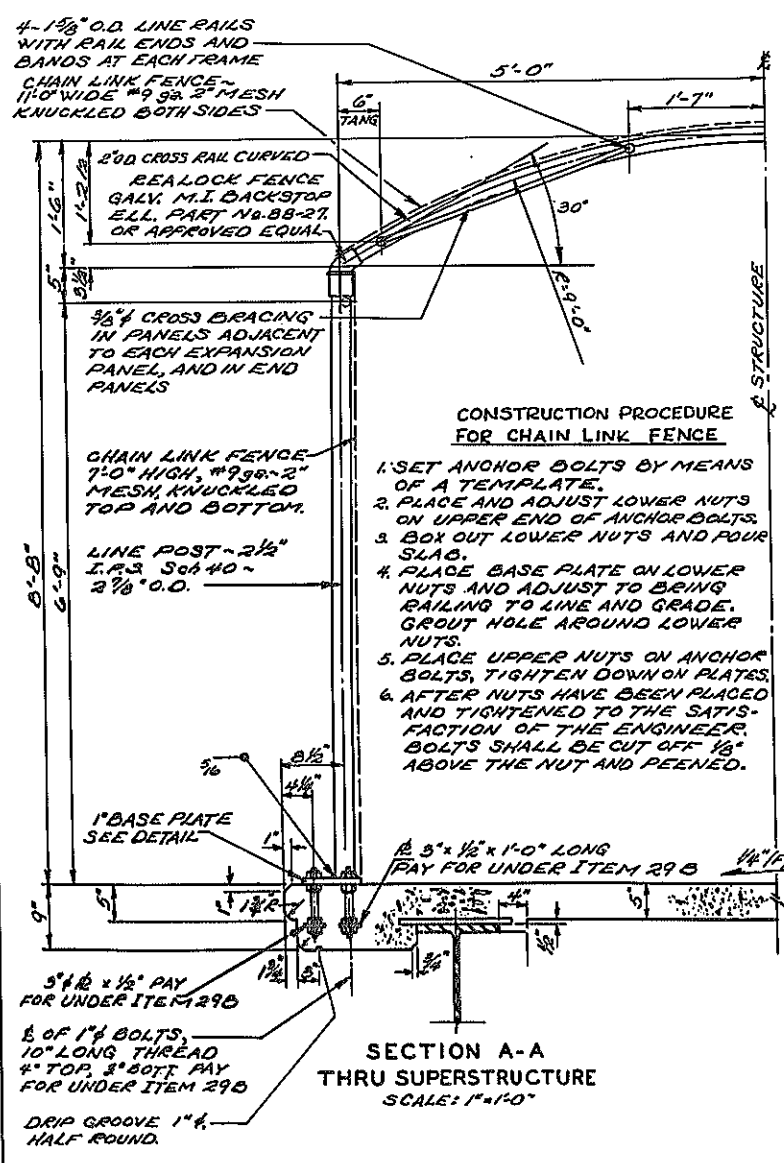


FED. RD. REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N.Y.	U-UG-377(25)	79S	173
KENSINGTON EXPRESSWAY, SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVE. TO FILLMORE AVE.				

FIELD CHANGE

MF79F1

TABLE OF QUANTITIES				
ITEM	DESCRIPTION	UNIT	NET	ALLOW.
106LXS	CHAIN LINK FENCING ENCLOSURE ON PEDESTRIAN OVERPASS.	L.F.	204	205
106LFG	CHAIN LINK FENCE PORTAL FAN GUARDS.	EACH	2	2



APPROVED Nov. 14, 1963
V. J. BURNS
DEPUTY CHIEF ENGINEER

PREPARED PURSUANT TO THE HIGHWAY LAW AND RECOMMENDED BY Norman H. Draper
Nov. 7, 1963 DIST. ENGINEER, DIST. NO. 4

CHAIN LINK FENCING NOTES

1. SIZES OF RAILS AND POSTS ARE GIVEN IN NOMINAL PIPE SIZES (I.R.S.)
2. ALL FENCING POSTS TO BE SET TRULY VERTICAL.
3. FOR REQUIREMENTS OF BASE PLATE AND CHAIN LINK FENCE SEE SPECIAL PROVISIONS ITEM 106 LXS.
4. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29B.
5. PIPE RAILS AND POSTS SHALL MEET ALL THE REQUIREMENTS OF A.S.T.M. A-120-54, STANDARD WEIGHT PIPE (SCHEDULE 40).

FIELD CHANGE

PEDESTRIAN OVERPASS - E.B. STA. 191+09.37	
CHAIN LINK FENCING DETAILS	
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	
CITY OF BUFFALO ARTERIAL	
KENSINGTON EXPRESSWAY, SEC. NO. III	
DRAWN	A.C.
CHECKED	W.T.
TRACED	A.C.

BR. SHEET NO. IIS

FIELD CHANGE

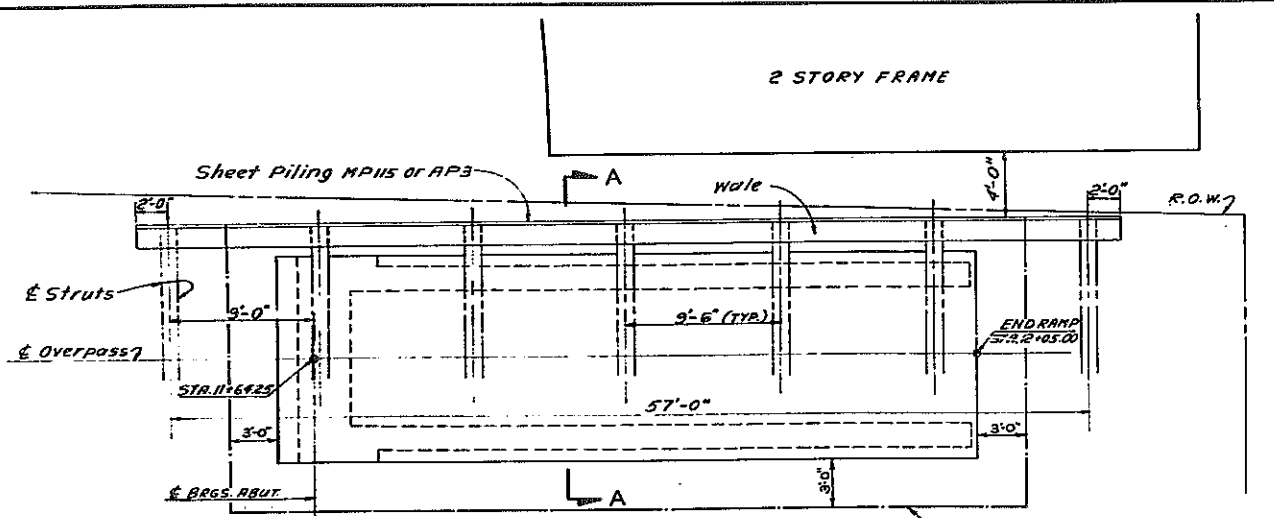
DESIGNED BY C. Ebdendick
DESIGN CHECKED BY
TRACING CHECKED BY C. Ebdendick

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	61	173

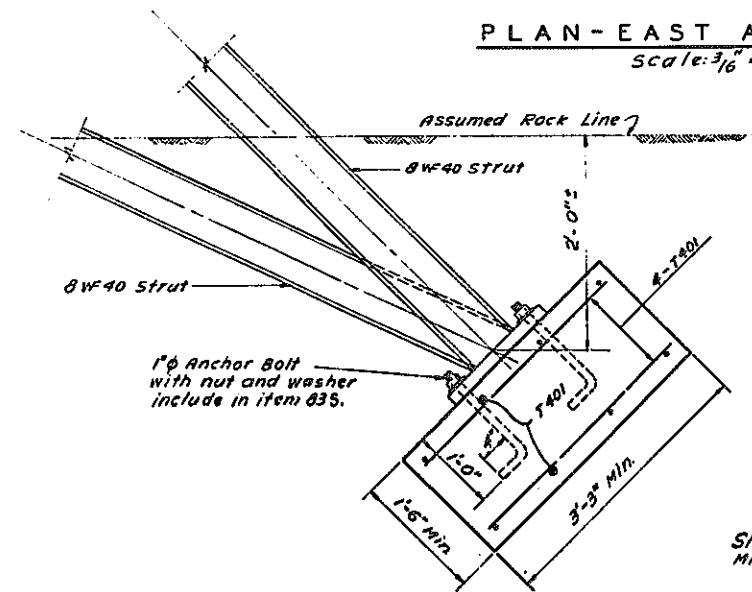
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT No. 3 NORTH AND AVENUE TO FILLMORE AVENUE

- CONSTRUCTION PROCEDURE FOR SHEETING**
- 1- Drive steel sheet piling where and as indicated on the plans to the required depth.
 - 2- Excavate, within the payment limits shown, to a point two (2) feet below the Top Wale.
 - 3- Install Top Wale as indicated.
 - 4- Excavate, within the payment lines shown, for the installation of the struts. No excavation will be permitted at this time outside the payment limits indicated.
 - 5- Pour concrete base for struts on rock and set anchor bolts as indicated.
 - 6- Install Top Struts as indicated.
 - 7- Excavate, within the payment limits shown, to a point two (2) feet below the bottom wale. Provide temporary bracing as required during the construction operation.
 - 8- Install Bottom Wale as indicated.
 - 9- Install Bottom Struts as required.
 - 10- Complete Abut. Construction.

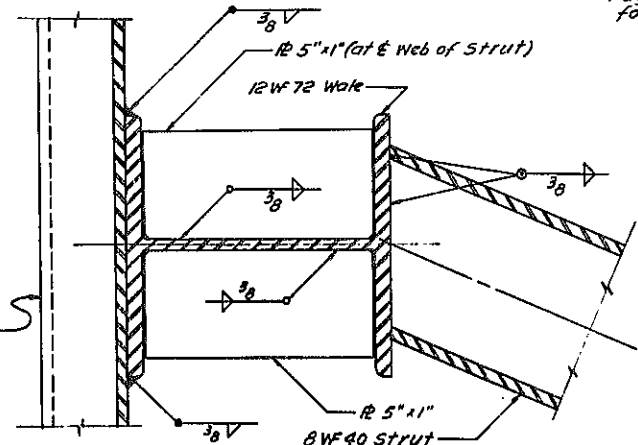
- NOTES:**
- 1- Work and material involved will be paid for under their respective items.
 - 2- Concrete and reinforcing for property protection shall conform to Item 18K and 28 respectively. Both will be paid for under item 835. Structural steel shall conform to item 23B and shall be paid for under item 835.
 - 3- Items for property protection are in the Overpass Estimate.



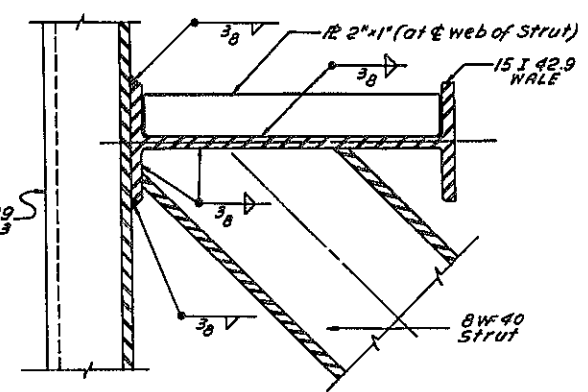
PLAN - EAST ABUTMENT
Scale: 3/16" = 1'-0"
Payment Limit Lines For Temp. Sheet Piling - Item 837X



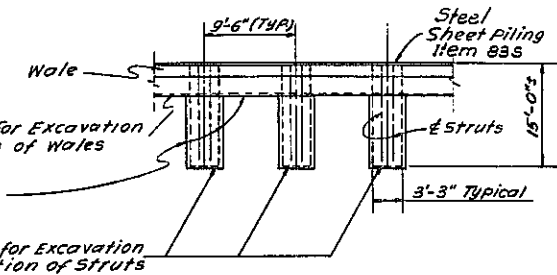
SECTION C-C
Scale: 1" = 1'-0"



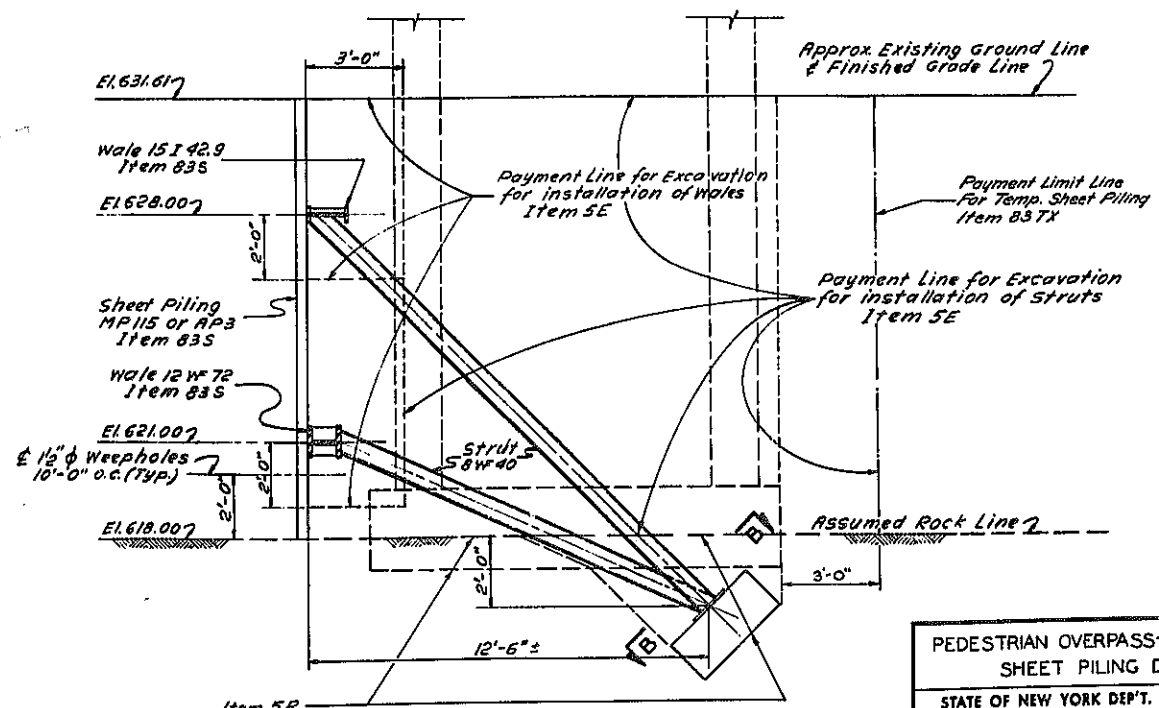
BOTTOM WALE DETAIL
Scale: 3" = 1'-0"



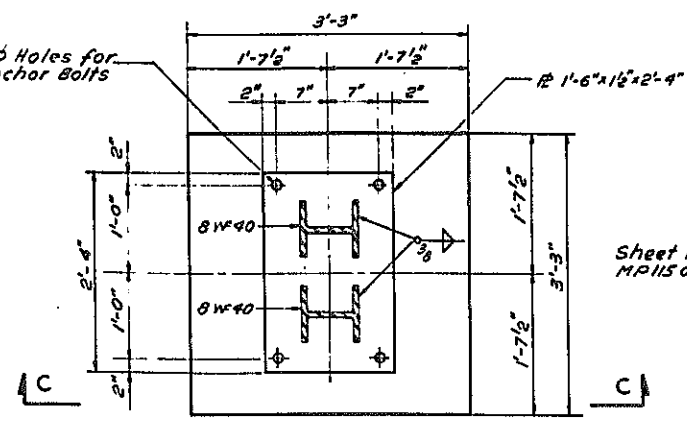
TOP WALE DETAIL
Scale: 3" = 1'-0"



PAYMENT LINE DETAIL
Not to Scale



SECTION A-A
Scale: 3/8" = 1'-0"



STRUT BASE DETAIL SECTION B-B
Scale: 1" = 1'-0"

PEDESTRIAN OVERPASS - E.B. STA. 19+09.37
SHEET PILING DETAILS

STATE OF NEW YORK DEP'T. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	W.K. W.T. W.A.
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BR. SHEET NO. 13

2276-6-63

FAC 63-2
K2 81.0P

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	82	173

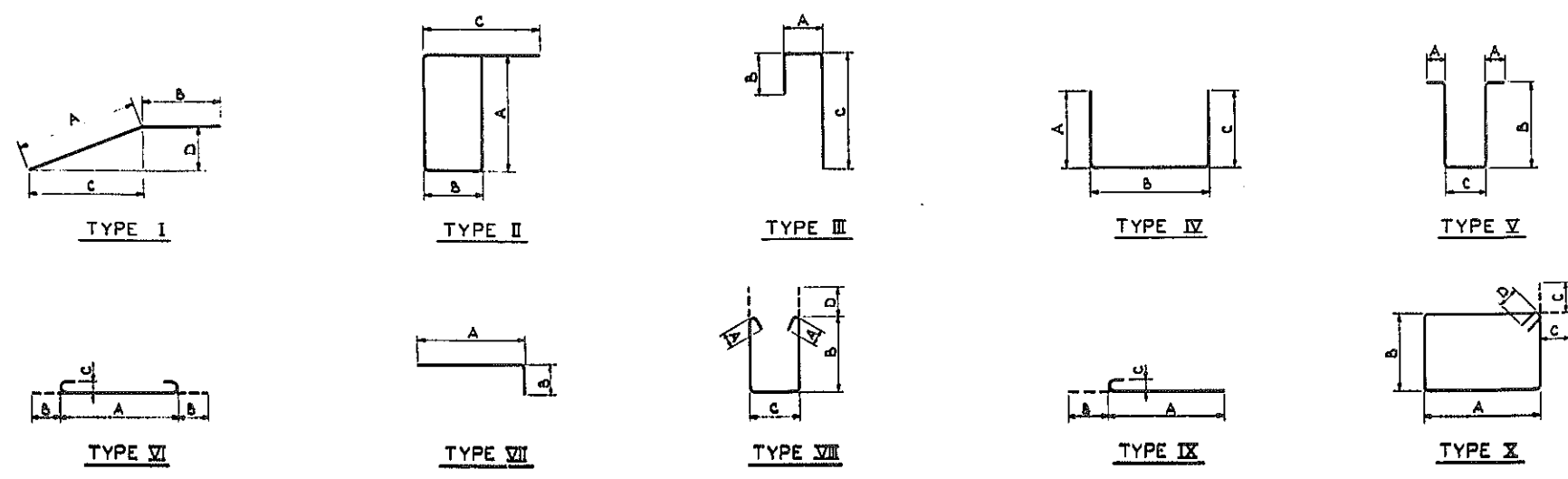
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

ABUTMENTS													
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	D	DESCRIPTION		
				WEST	EAST	TOTAL							
A401	#4	36-10	STR.	26	—	26					FOOTING, LONGITUDINAL, TOP & BOTTOM		
A402		35-5		44	—	44					SIDEWALL, HORIZONTAL		
A403		AVG 29-5		20	—	20					SIDEWALL, HORIZONTAL, EACH INCREMENT 6'-0" FROM 3-5 TO 29-5		
A404		30-3		4	—	4					SIDEWALL, INCLINED		
A405		2-0	STR.	132	156	288					SIDEWALL, DOWEL		
A406		35-10	I	6	—	6	30-3	5-7	29-10	5-0	SIDEWALL, INCLINED		
A407		35-10	I	6	—	6	30-3	5-7	29-10	5-0	"		
A408		9-6	II	66	78	144	3-0	1-2	2-4		SIDEWALL, STIRRUP		
A409		5-0	STR.	4	4	8					CURTAIN WALL, VERTICAL		
A410		4-8		40	38	78					CURTAIN WALL, HORIZONTAL		
A411		2-0		2	—	2					"		
A412		32-7	STR.	16	—	16					SLAB, LONGITUDINAL, TOP & BOTTOM		
A413		4-5	VII	12	12	24	2-11	1-6			BRIDGE SEAT		
A414		12-7	STR.	31	31	62					WALL, HORIZONTAL		
A415		11-1	STR.	6	6	12					BACKWALL, HORIZONTAL		
A416		7-3	III	12	12	24	0-11	1-6	4-10		BACKWALL, VERTICAL		
A417		3-0	VII	12	12	24	2-0	1-0			BACKWALL, DOWEL		
A418		42-10	STR.	—	26	26					FOOTING, LONGITUDINAL, TOP & BOTTOM		
A419		41-5		—	36	36					SIDEWALL, HORIZONTAL		
A420		AVG 29-5		—	24	24					SIDEWALL, HORIZONTAL, EACH INCREMENT 6'-0" FROM 5-5 TO 35-5		
A421		36-5	STR.	—	4	4					SIDEWALL, INCLINED		
A422		42-0	I	—	6	6	36-5	5-7	35-10	6-0	"		
A423		40-0	I	—	6	6	36-5	5-7	35-10	6-0	"		
A424		35-7	STR.	—	16	16					SLAB, LONGITUDINAL, TOP & BOTTOM		
A425		11-1	STR.	34	40	74					STEP, TRANSVERSE		
A426		4-2	VII	120	144	264	3-6	0-8			STEP, LONGITUDINAL		
A427		3-4	VII	12	12	24	2-8	0-8			"		
A428		8-11	IV	6	6	12	2-10	3-3	2-10		PEDESTAL		
A429	#4	2-10	IX	8	8	16	2-4	0-6	0-4		"		
A501	#5	12-5	STR.	76	88	164					FOOTING, TRANSVERSE, TOP & BOTTOM		
A502		AVG 19-8		132	—	132					SIDEWALL, VERTICAL, EACH INCREMENT 1'-6" FROM 10-2 TO 19-8		
A503		19-0		4	—	4					CURTAIN WALL, VERTICAL		
A504		11-1		50	59	109					SLAB, TRANSVERSE, BOTTOM		
A505		17-8		12	12	24					WALL, VERTICAL		
A506		14-8		12	12	24					"		
A507		12-7		3	3	6					BRIDGE SEAT		
A508		AVG 11-6		—	156	156					SIDEWALL, VERTICAL, EACH INCREMENT 1'-6" FROM 0-7 TO 14-8		
A509	#5	18-6	STR.	—	4	4					CURTAIN WALL, VERTICAL		
A601	#6	5-5	IX	164	188	352	4-9	0-8	0-6		FOOTING, DOWEL		
A602	#6	5-0	STR.	4	4	8					VENTILATING LOUVER		
A603	#6	4-0	STR.	4	4	8					"		
T 4Q	#4	2-9	STR.	—	112	112					SHEET PILING BASE		

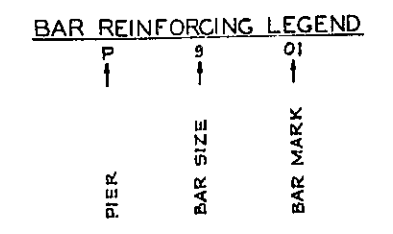
RAMPS													
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	D	DESCRIPTION		
				WEST	EAST	TOTAL							
R401	#4	6-11	V	118	118	236	0-6	2-6	0-11		BEAM, STIRRUP		
R402		11-0	STR.	84	84	168					SLAB, TRANSVERSE, BOTTOM		
R403		32-3	STR.	28	28	56					SLAB, LONGITUDINAL, TOP & BOTTOM		
R404		12-1	VI	64	64	128	11-1	0-6	0-4		STEP, TRANSVERSE		
R405		4-2	VII	240	240	480	3-6	0-8			STEP, LONGITUDINAL		
R406		3-4	VI	12	12	24	2-8	0-8			"		
R407		5-5	VII	12	12	24	0-5	2-1	0-5	0-5	INTERMEDIATE DIAPHRAGM, STIRRUP		
R408		4-2		18	18	36	0-5	1-4	0-8	0-5	END DIAPHRAGM, STIRRUP		
R409		8-1		16	16	32	0-5	3-2	0-11	0-5	BEAM SUPPORT		
R410	#4	3-2	VII	126	126	252	0-5	0-7	1-2	0-5	SLAB, CANTILEVER, STIRRUP		
R501	#5	31-8	STR.	8	8	16					BEAM, SIDE		
R502	#5	7-5	STR.	14	14	28					DIAPHRAGM		
R1001	#10	35-8	IX	8	8	16	32-3	1-5	1-0		BEAM, BOTTOM		
R1002		20-2	STR.	4	4	8					"		
R1003		17-2		4	4	8					BEAM, TOP		
R1004		49-2		4	4	8					"		
R1005	#10	10-0	STR.	4	4	8					"		

DECK SLABS													
MARK	SIZE	LENGTH	TYPE	NUMBER			A	B	C	D	DESCRIPTION		
				WEST	EAST	TOTAL							
S401	#4	11-1	STR.	136	188	324					BOTTOM, TRANSVERSE		
S402		12-1	VI	102	103	205	11-1	0-6	0-4		TOP, TRANSVERSE		
S403		2-11	VII	204	206	410	0-5	0-7	0-11	0-5	CANTILEVER, STIRRUP		
S404		34-6	STR.	39	—	39					TOP & BOTTOM, LONGITUDINAL		
S405	#4	34-10	STR.	—	39	39					"		

PIERS																
MARK	SIZE	LENGTH	TYPE	NUMBER						A	B	C	D	DESCRIPTION		
				PIER 1	PIER 2	PIER 3	PIER 4	PIER 5	TOTAL							
P401	#4	10-11	STR.	14	14	18	14	14	74					FOOTING, LONGITUDINAL, TOP & BOTTOM		
P402		10-1	STR.	40	48	46	48	40	222					WALL, HORIZONTAL		
P403		2-10	IX	20	20	20	20	20	100	2-4	0-6	0-4		PEDESTAL		
P404		9-6	X	6	—	—	—	6	12	2-8	1-8	0-5	0-5	"		
P405	#4	10-2	X	—	6	6	—	—	18	2-8	2-0	0-5	0-5	"		
P601	#6	6-6	STR.	24	24	—	24	24	96					FOOTING, TRANSVERSE, TOP & BOTTOM		
P602	#6	23-9	STR.	—	22	—	22	—	44					WALL, VERTICAL		
PT01	#7	19-7	STR.	22	—	—	—	22	44					WALL, VERTICAL		
PT02		8-6		—	—	26	—	—	26					FOOTING, TRANSVERSE, TOP & BOTTOM		
PT03	#7	22-6	STR.	—	—	22	—	—	22					WALL, VERTICAL		
P801	#8	11-0	STR.	—	—	12	—	—	12					SEWER PROTECTION BOX		
P802		15-0	STR.	—	—	11	—	—	11					"		
P803		8-4	IX	—	—	22	—	—	22	7-3	1-1	0-10		FOOTING, DOWEL		
P804	#8	18-10	IX	—	—	22	—	—	22	12-9	1-1	0-10		"		
P801	#9	11-6	IX	22	—	—	—	22	44	10-3	1-3	0-11		FOOTING, DOWEL		



NUMBER	SIZE
#4	1/2"
#5	5/8"
#6	3/4"
#7	7/8"
#8	1"
#9	1 1/8"
#10	1 1/4"



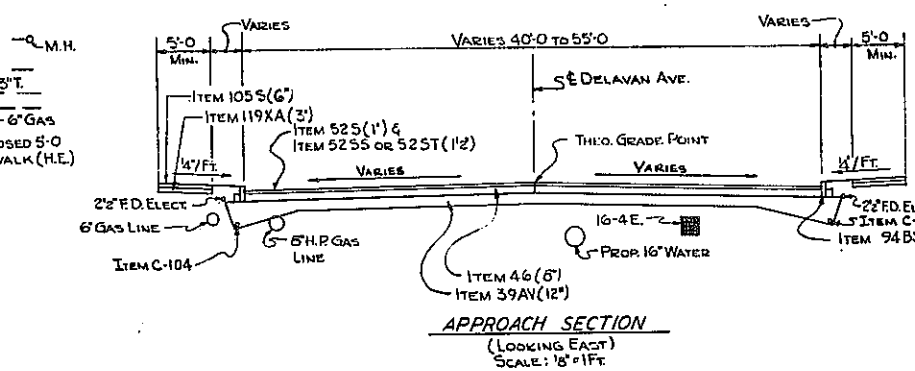
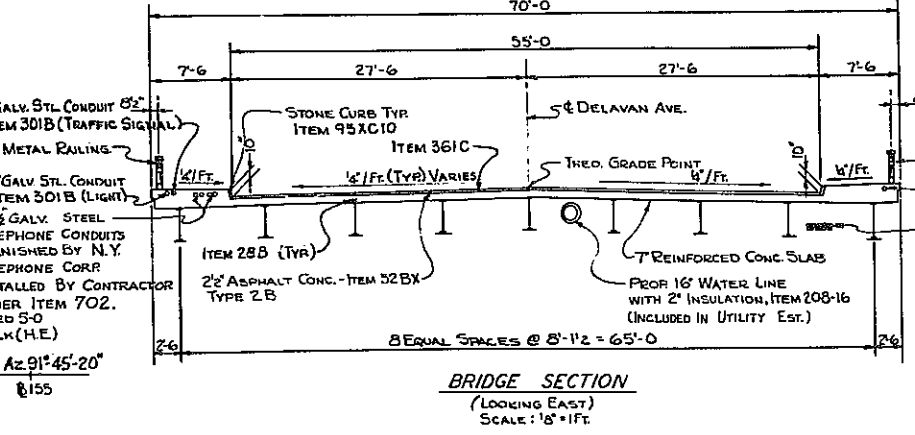
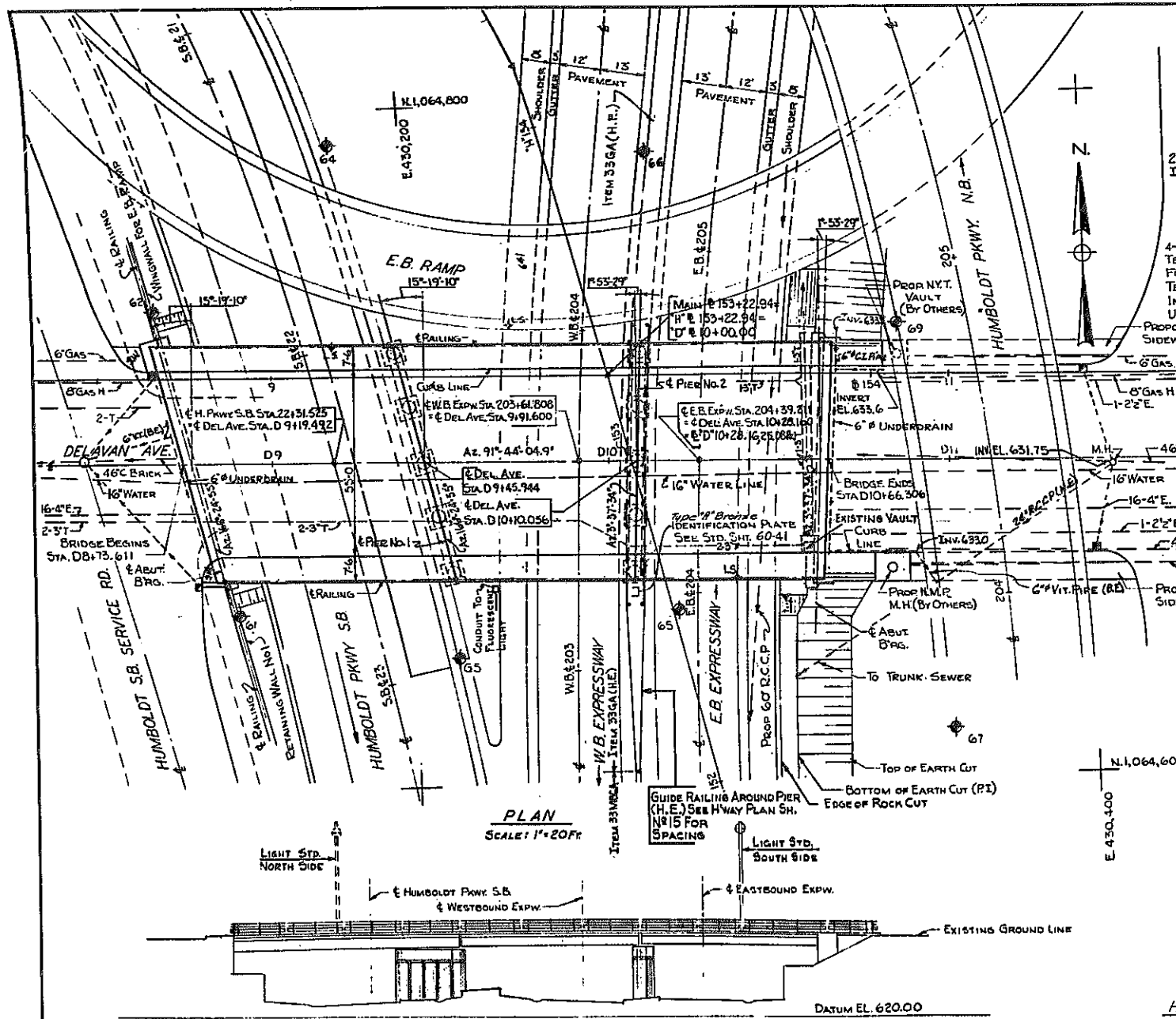
NOTE:
ALL DIMENSIONS ARE OUT TO OUT OF BARS.

PEDESTRIAN OVERPASS - E. B. STA. 191+09.37
BAR LIST

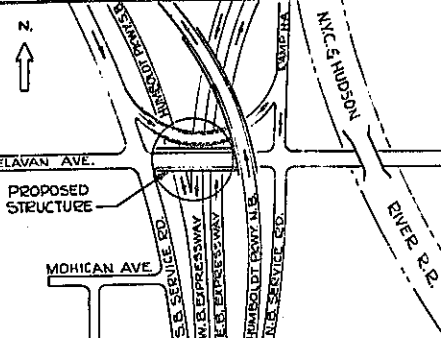
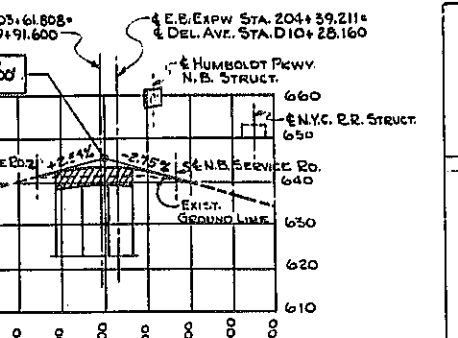
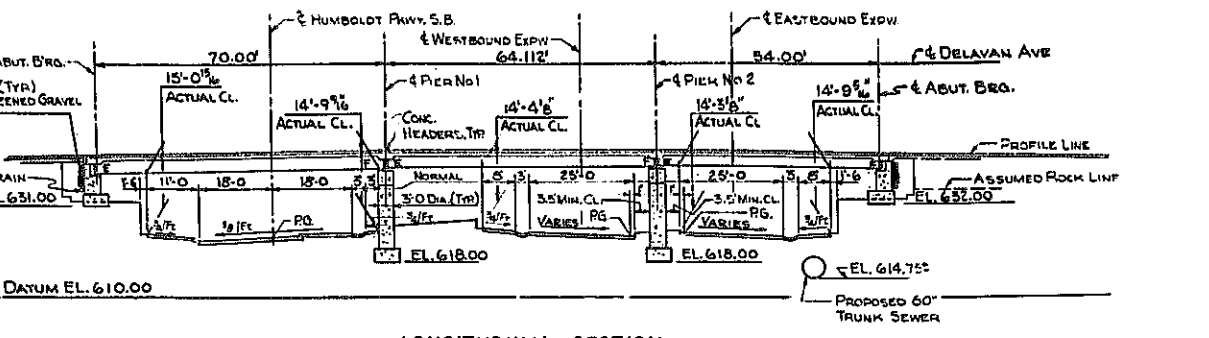
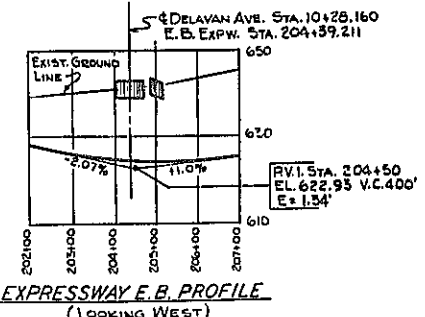
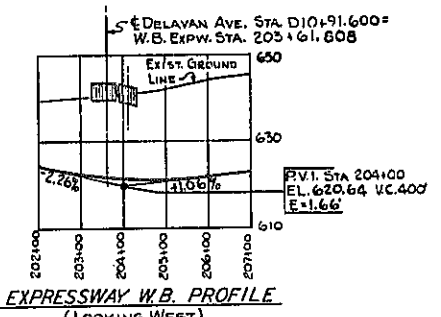
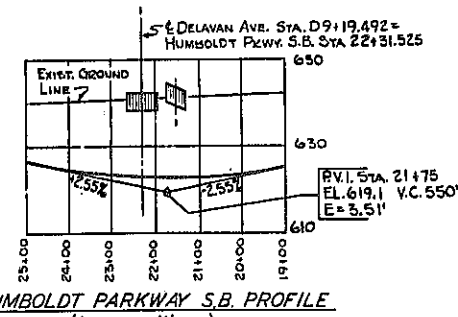
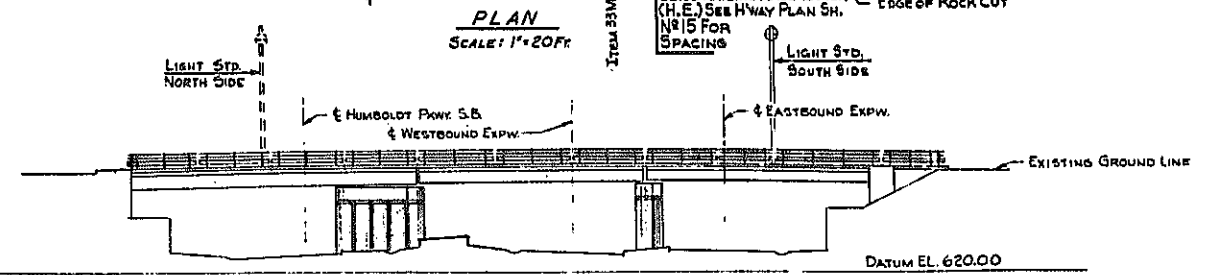
STATE OF NEW YORK DEPT. OF PUBLIC WORKS	DRAWN	L. M.
CITY OF BUFFALO ARTERIAL	CHECKED	W. T.
KENSINGTON EXPRESSWAY, SEC. NO. III	TRACED	L. M.
DE LEUW, CATHY & BRILL		
ENGINEERS-ARCHITECTS		
NEW YORK		

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	83	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



INDICATES BORING LOCATION.
F = FIXED BEARINGS.
E = EXPANSION BEARINGS.
P.G. = PROFILE GRADE
UTILITIES SHOWN ON BRIDGE APPROACH ARE EXISTING, EXCEPT AS NOTED.



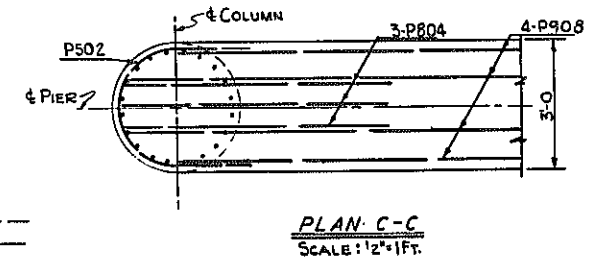
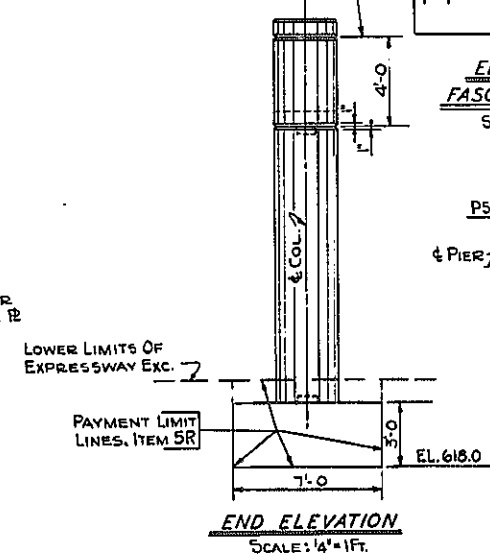
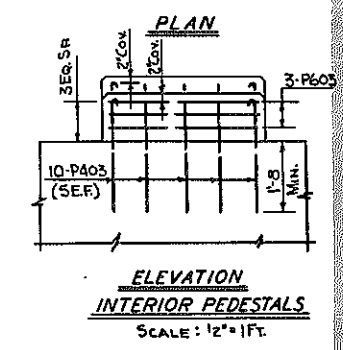
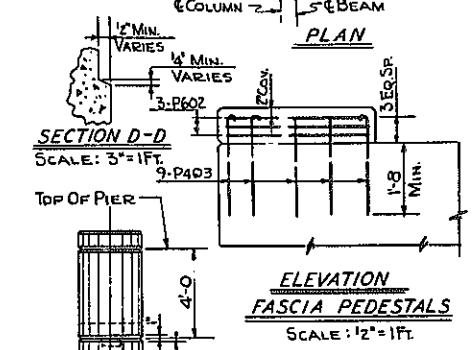
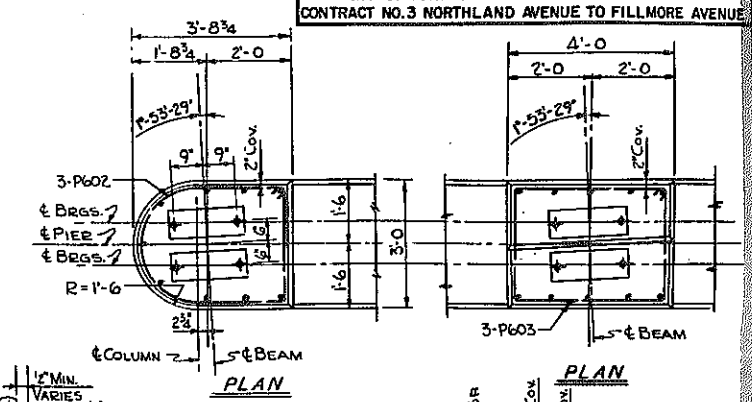
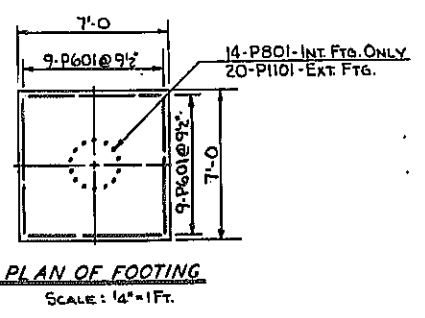
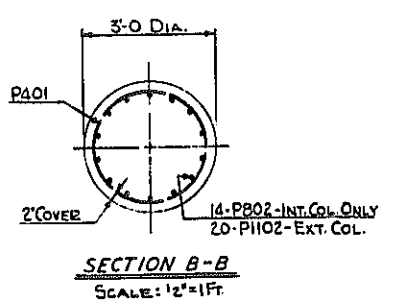
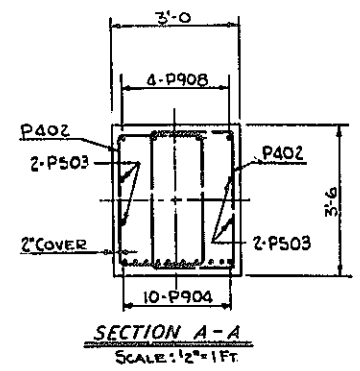
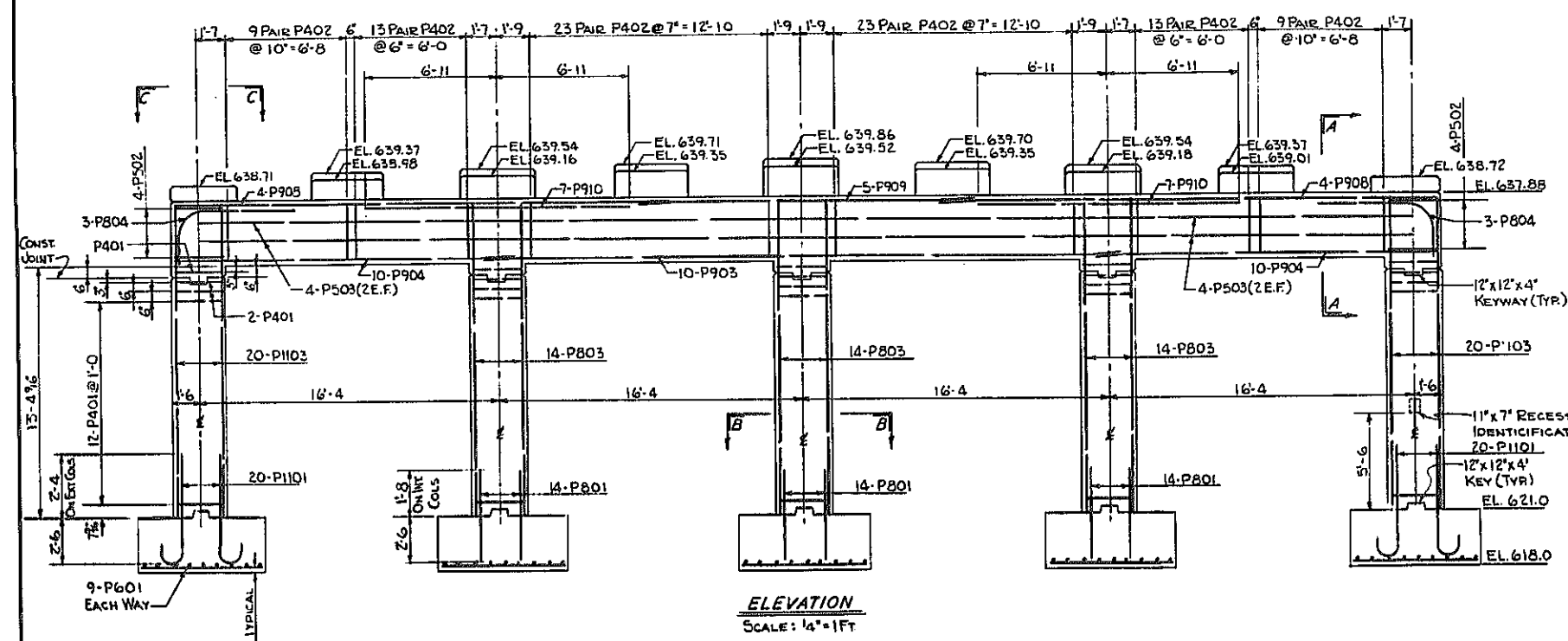
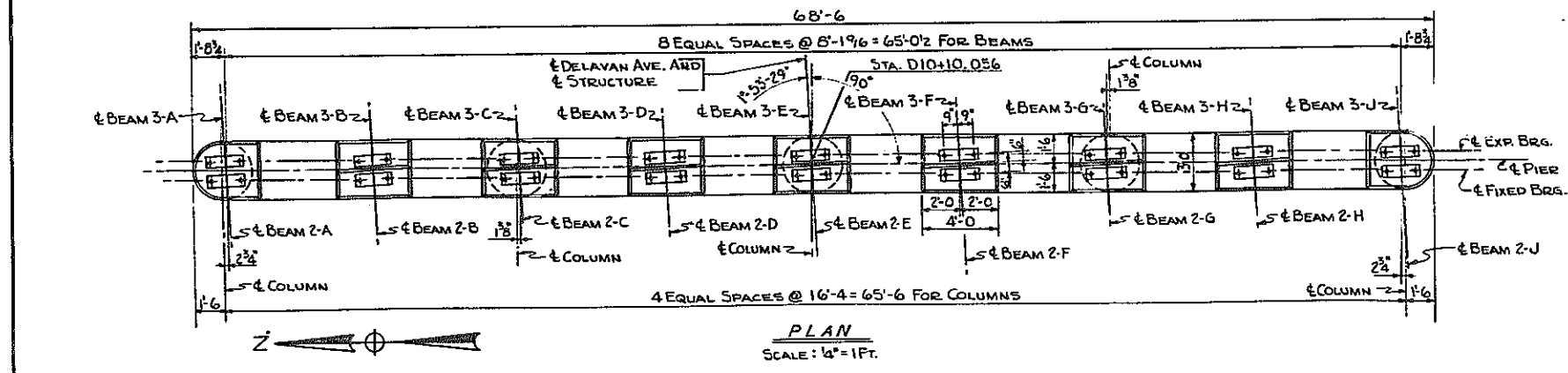
DELAVAN AVE. OVER EXPRESSWAY GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.B.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	86	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTES:

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5.7 TONS PER SQ. FT. AT PIERS.

CONCRETE IN PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS I CONCRETE.

CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.

ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS AND SET 10" INTO MASONRY.

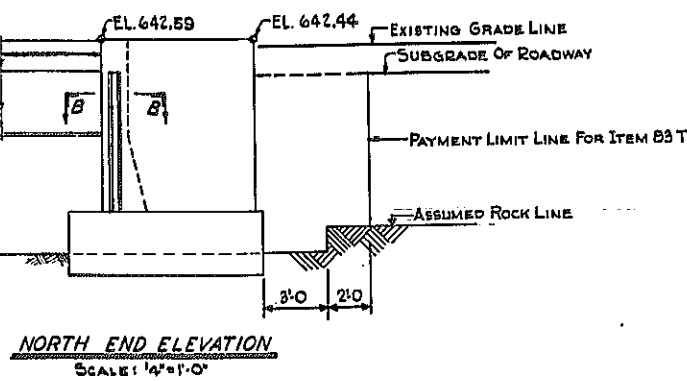
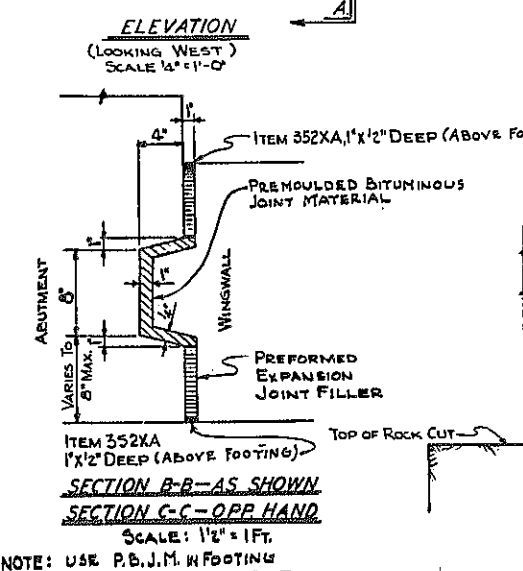
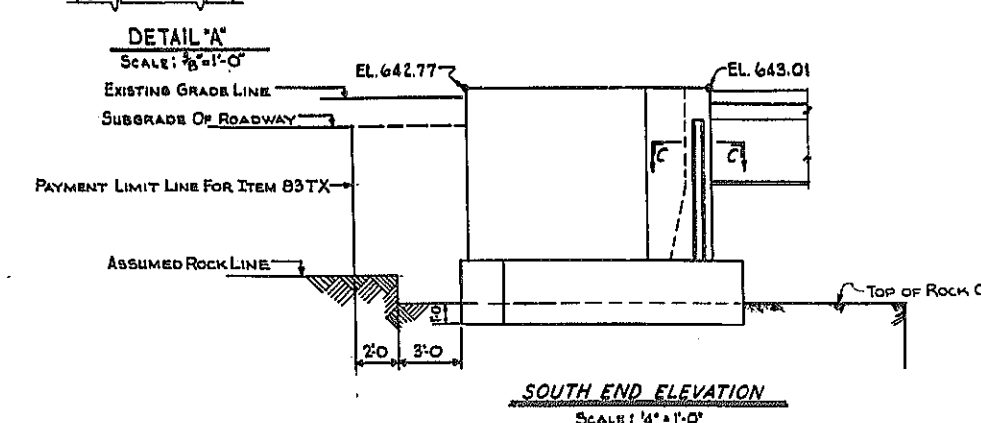
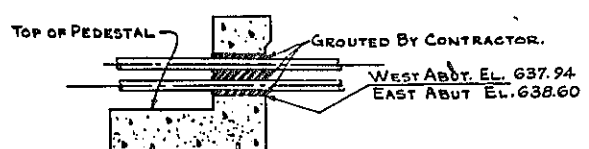
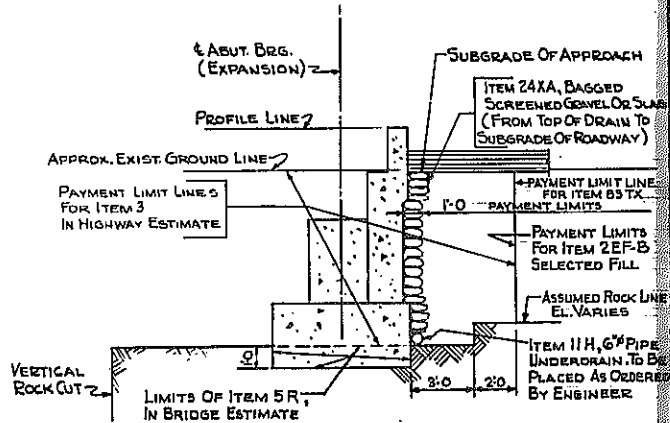
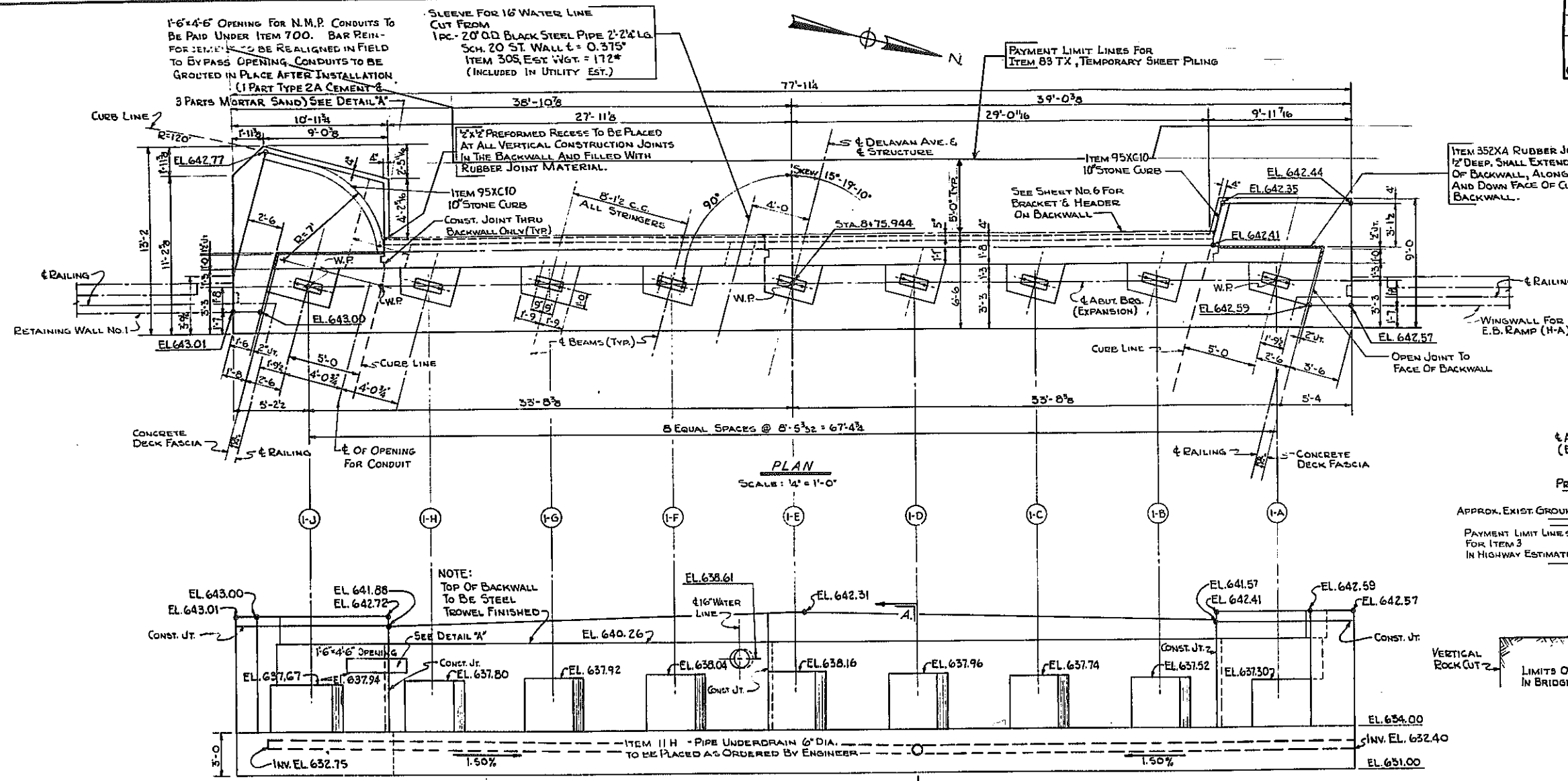
SEE SHEET NO. 2 FOR ANCHOR BOLT LENGTHS.

BAR NUMBER SYSTEM: PIER BAR NUMBERS ARE PREFIXED BY LETTER P. SIZE OF BAR IS INDICATED BY NUMBER IN HUNDREDS COLUMN.

DELAVAN AVE. OVER EXPRESSWAY PIER NO. 2			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.	

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	87	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:**
- CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 - CONCRETE IN HEADER & SUBSTRUCTURE SIDEWALKS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES
 - FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 57 TONS PER SQ. FT.
 - FOR SECTIONS AND REINFORCING, SEE SHT. NO. 6
 - FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 2.
 - BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.

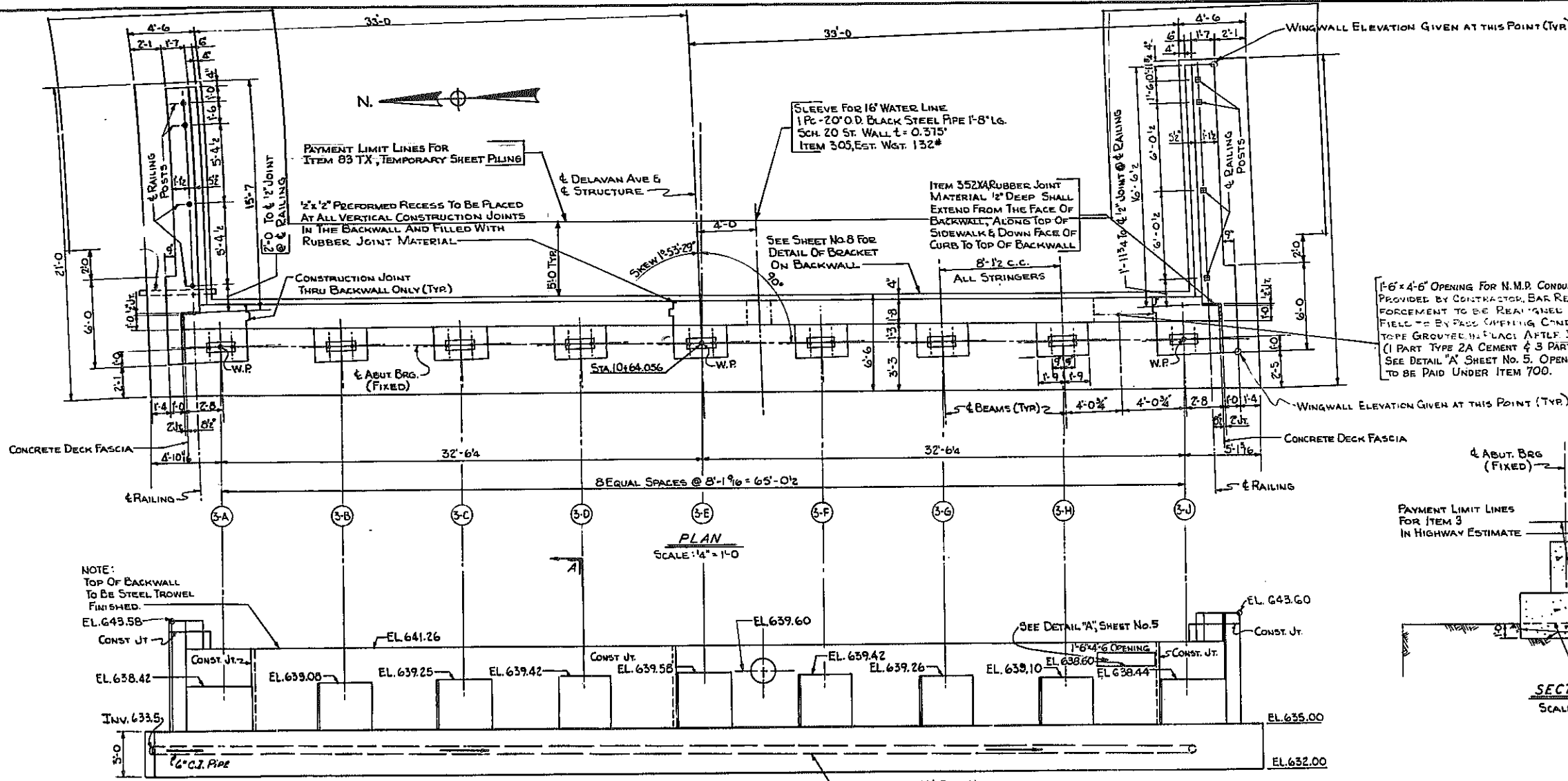
DELAVAN AVE. OVER EXPRESSWAY WEST ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E. R.B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	89	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



1'-6" x 4'-6" OPENING FOR N.M.P. CONDUITS TO BE PROVIDED BY CONTRACTOR. BAR REINFORCEMENT TO BE REINFORCED IN FIELD TO BE BY PASS OPENING CONDUITS TO BE GROUDED IN PLACE AFTER INSTALLATION (1 PART TYPE 2A CEMENT & 3 PARTS MORTAR SAND) SEE DETAIL 'A' SHEET NO. 5. OPENING AND GROUING TO BE PAID UNDER ITEM 700.

NOTES:

- CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
- CONCRETE IN WINGWALL FASCIAE SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES
- FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5.7 TONS PER SQ. FT.
- FOR SECTIONS AND REINFORCING, SEE SHEET NO. 8
- FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 2
- BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.

DELAVAN AVE OVER EXPRESSWAY EAST ABUTMENT

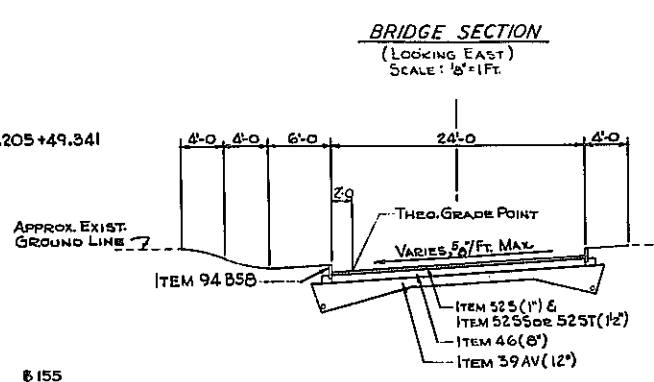
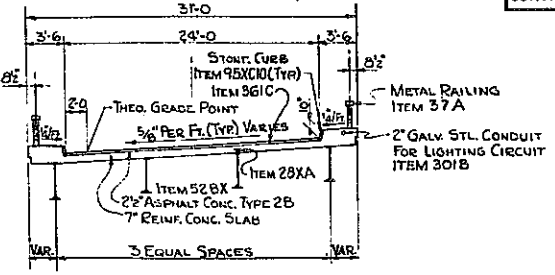
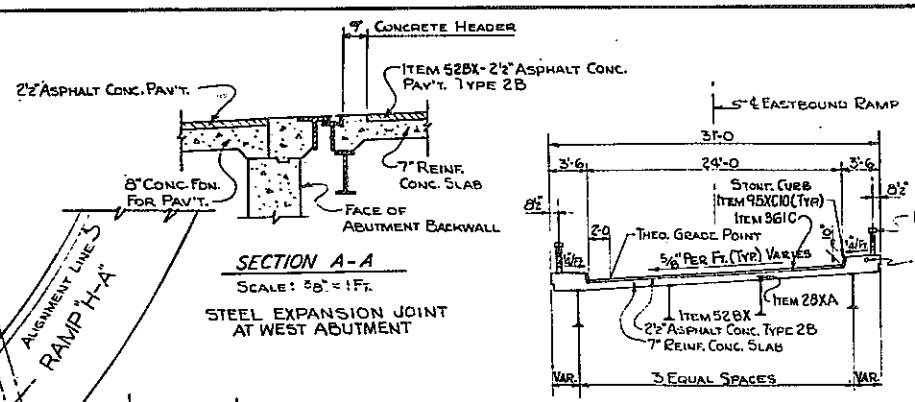
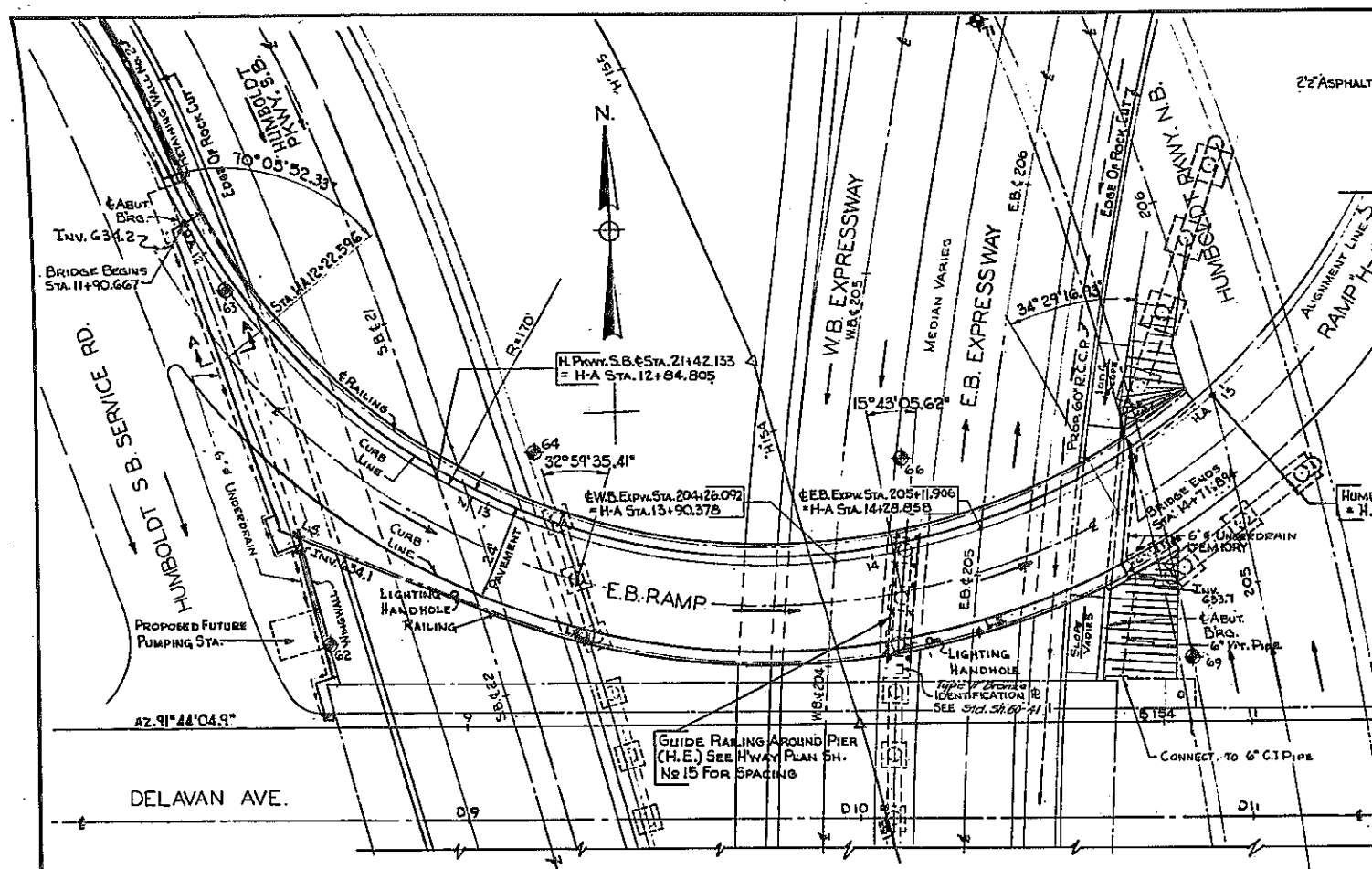
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LUW, GATHER & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.B.

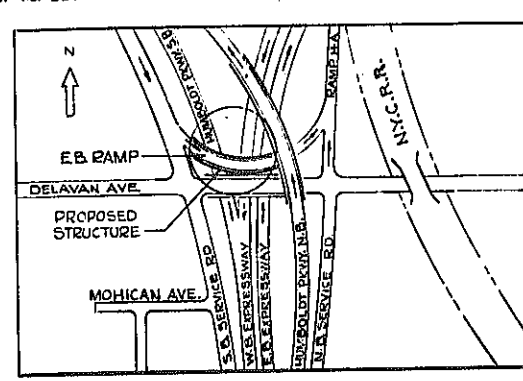
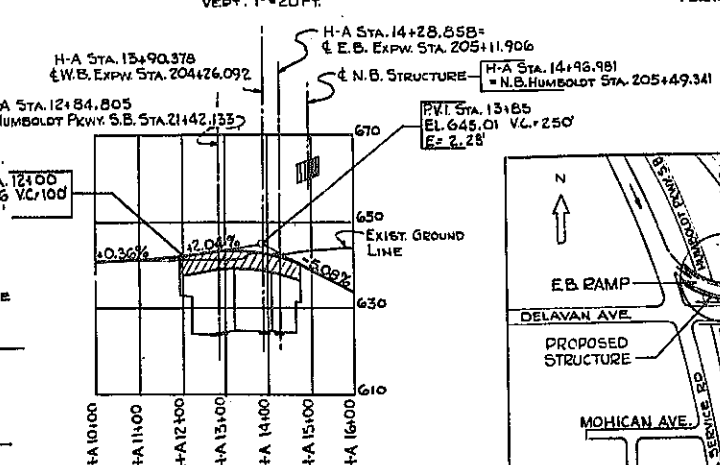
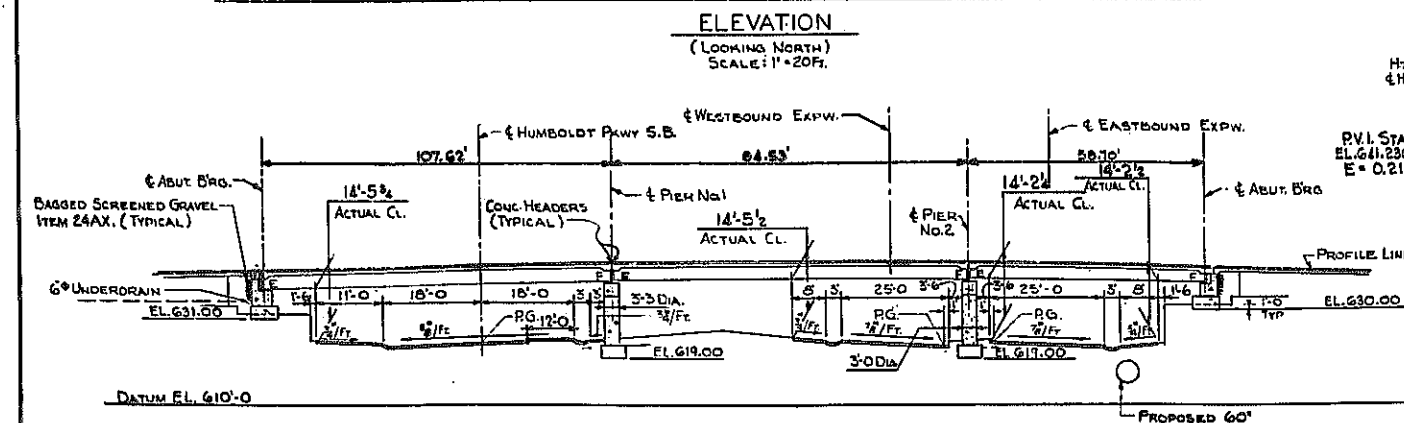
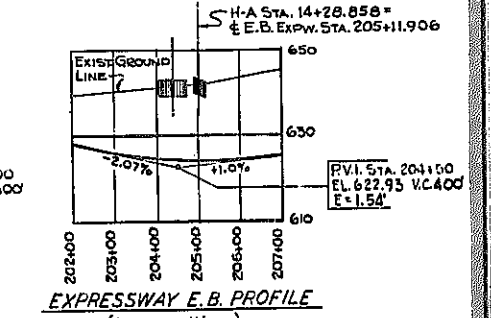
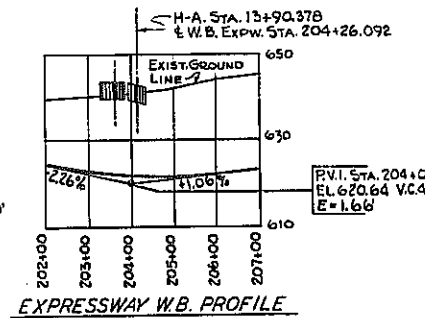
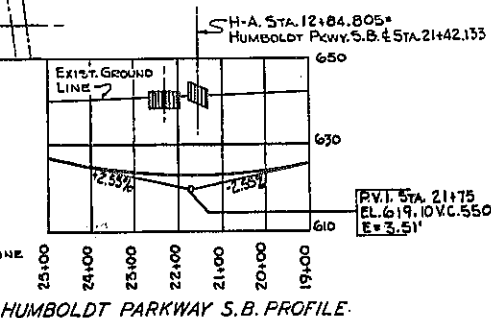
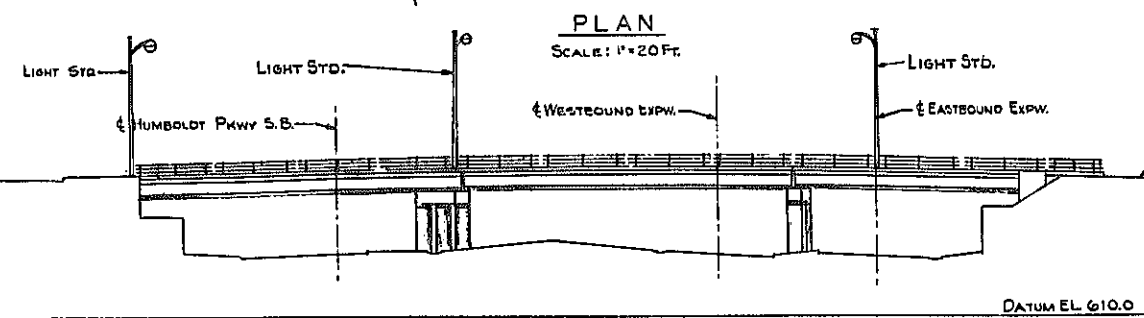
BR, SHEET NO 7

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	96	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



⊕ INDICATES BORING LOCATION
F = FIXED BEARINGS
E = EXPANSION BEARINGS
P.G. = PROFILE GRADE



(RAMP H-A)

E. B. RAMP OVER EXPRESSWAY GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	R.B.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	

BR. SHEET NO. 1

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	97	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.R.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : #20-516-44 LIVE LOAD

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR: THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.

THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, ABUTMENT SIDE WALK POURS AND WINGWALL (BETWEEN E.B. RAMP & DELAVAN AVE.) SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS (EXCEPT IN SIDEWALK POURS) AND ABUTMENT WINGWALL SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

CEMENT : CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A, ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE: ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET 60-41.

OTHER : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED.

SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.

THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 28 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREMOULDED BITUMINOUS JOINT, COMPRESSED ASBESTOS SHEET PACKING, PREF. EXP. JT. FILLER AND ASPHALT HOOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

FOOTINGS: ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

NOTE: FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

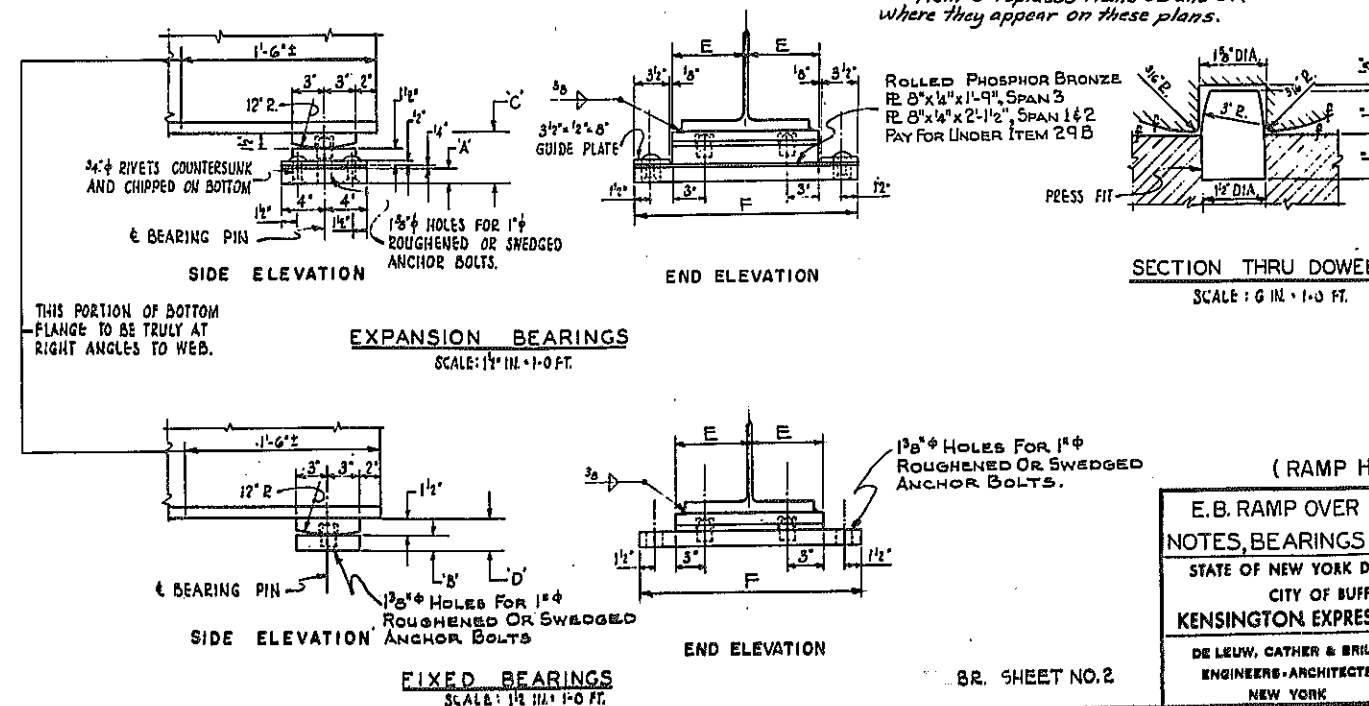
LOCATION	STRINGER	N ^o	A	B	C	D	1" ANCHOR BOLTS
EXPANSION BEARINGS							
West Abut.	1A to 1-H		1 3/8"		4 5/8"		16 1'-2"
Pier No. 1	2A to 2-D		1 3/8"		4 5/8"		8 1'-2"
Pier No. 2	3A to 3-D		2 7/8"		5 3/8"		8 1'-3"
FIXED BEARINGS							
Pier No. 1	1A to 1-D			3 3/8"		4 5/8"	8 1'-3"
	1-E			1 5/8"		3 3/8"	2 1'-2"
Pier No. 2	2A to 2-D			1 5/8"		3 3/8"	8 1'-2"
East Abut.	3A to 3-D			1 5/8"		3 3/8"	8 1'-2"

BEARING DIMENSIONS

STRINGER	E	F
1A to 1E	9 1/8"	2'-1 1/2"
1F to 1H	6 7/8"	1'-9"
2A to 2D	9 7/8"	2'-1 1/2"
2E	6 7/8"	1'-9"
3A to 3D	6 7/8"	1'-9"

ITEM NO	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED
2EF-B	SELECTED FILL	CY.	314	350
5	TRENCH, CULVERT & BRIDGE Exc.	CY.	87	95
11 H	PERF. CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	208	215
152A	PORTLAND CEMENT TYPE 2A	Bbl.	1088	1,130
18 X	CLASS 1A CONCRETE FOR STRUCTURES	CY.	361	370
20 Y	CLASS 1 CONCRETE	CY.	329	340
24 AX	BAGGED SCREENED GRAVEL OR SLAG	CY.	54	55
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	84,843	86,500
28 B	SHEAR CONNECTORS	L.F.	1,174	1,250
29 B	STRUCTURAL STEEL	Lb.	351,761	362,300
37 A	METAL RAILING	L.F.	529	540
52 BX	ASPHALT CONCRETE TYPE 2B (2 1/2")	TON	98	100
61	BITUMINOUS MATERIAL	GAL.	13.1	140
95 XC10	10" STONE CURB (BRIDGE)	L.F.	560	570
301 B	2" GALVANIZED STEEL CONDUIT	L.F.	241	250
303 CG	FURNISH & INSTALL LIGHT STDS. 30' MTS. HT.	EA.	2	2
9 VB	SEWER PIPE (VITRIFIED) 6" DIA.	L.F.	25	30
13 B	CAST IRON PIPE, 6" DIA.	L.F.	6	6
37 AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	49	50
30 S	MISCELLANEOUS METALS	Lb.	150	160
301 C	2 1/2" GALVANIZED STEEL CONDUIT	L.F.	15	15
363 C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	18	20
303 CB	FURNISH & INSTALL LIGHT STDS. 30' MTS. HT.	EA.	1	1
361 C	PROTECTIVE COATING FOR CONCRETE	GAL.	78	80
352 XA	RUBBER JOINT MATERIAL	GAL.	9	10
83 TX	TEMPORARY SHEET PILING	SF	1021	1070

Note: Item 5 replaces Items 5E and 5F where they appear on these plans.



(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY

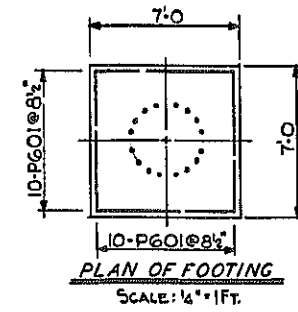
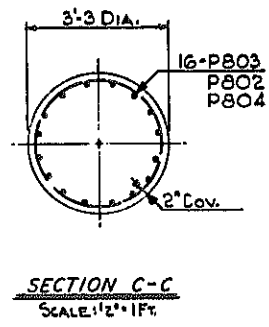
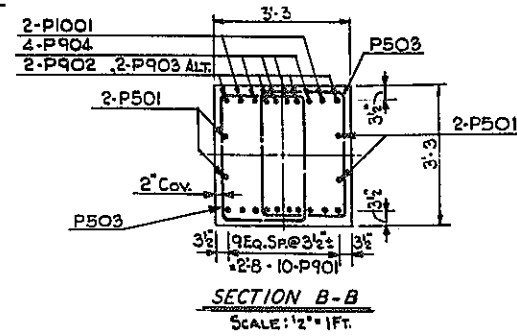
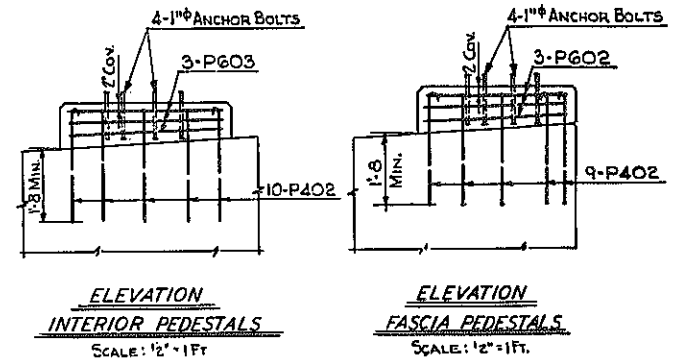
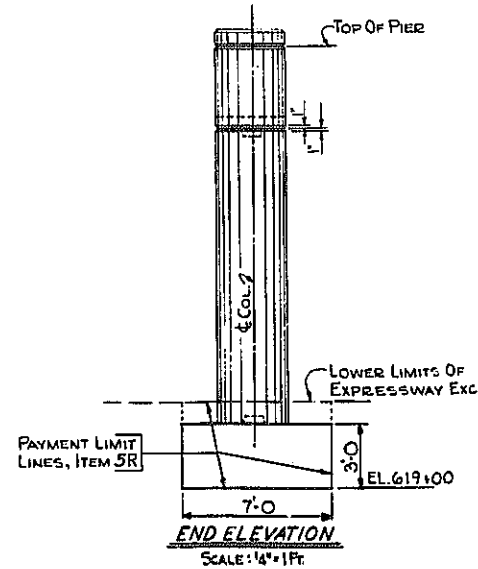
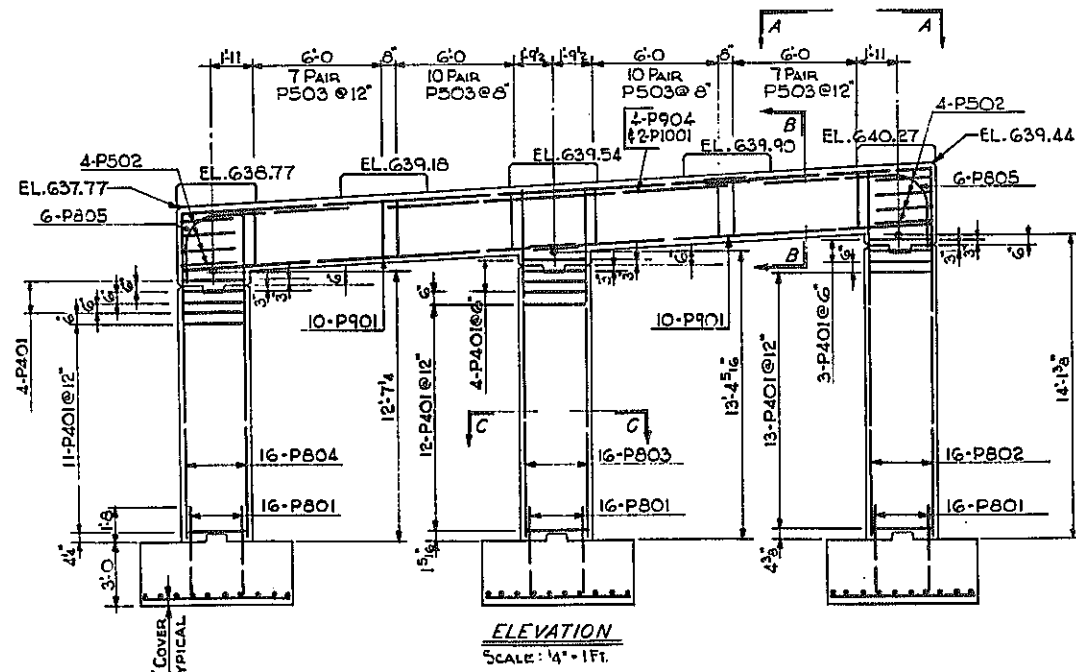
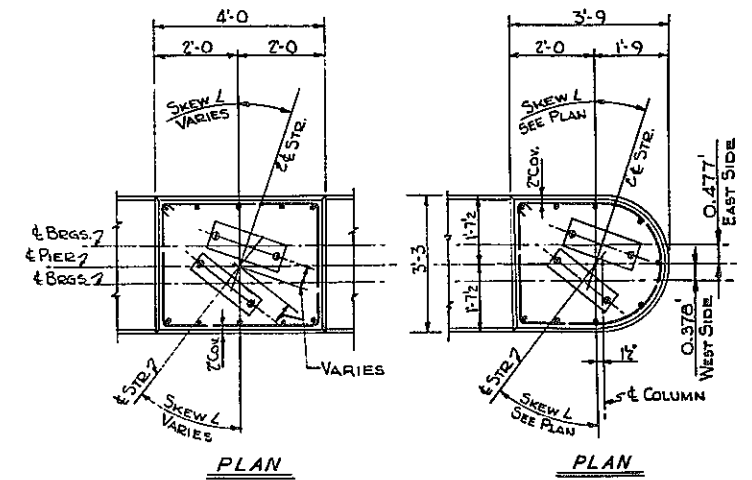
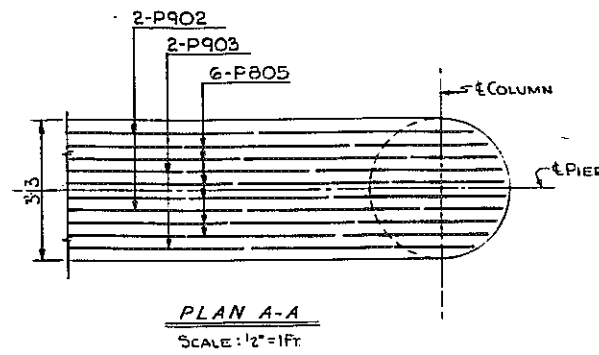
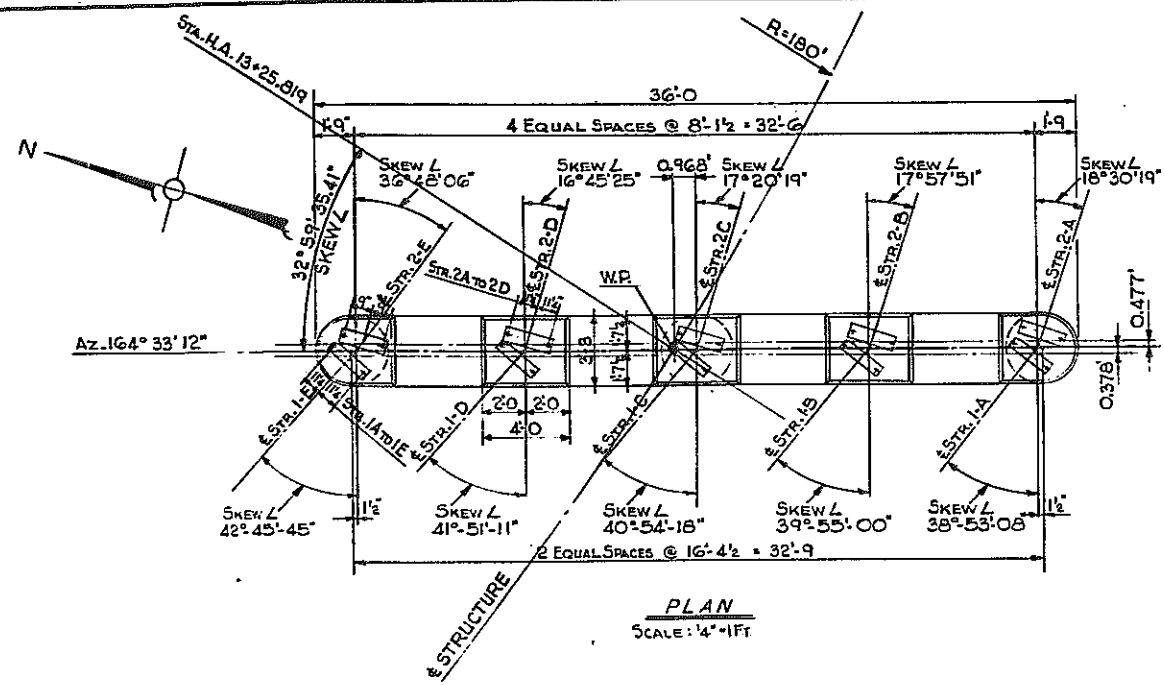
NOTES, BEARINGS AND ESTIMATE

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	J.S.
ENGINEERS-ARCHITECTS	CHECKED	D.E.
NEW YORK	TRACED	J.S.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	98	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



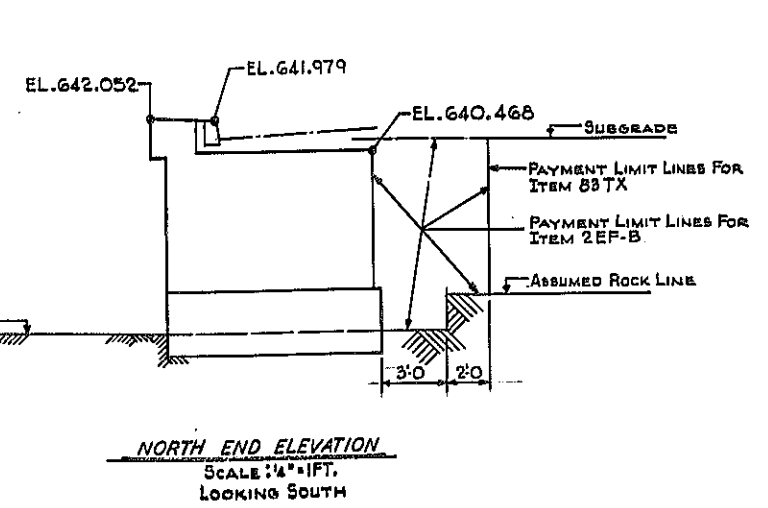
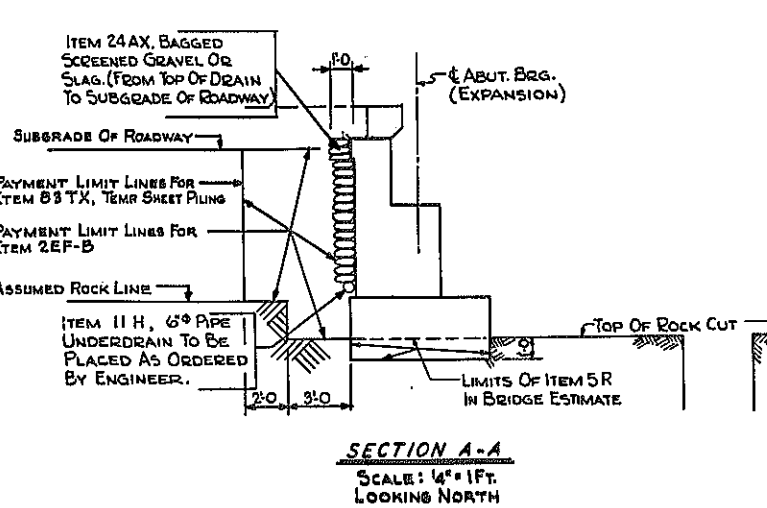
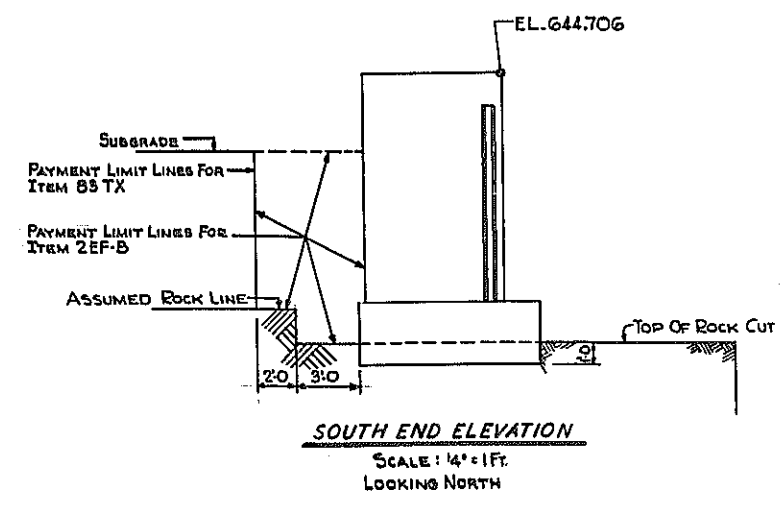
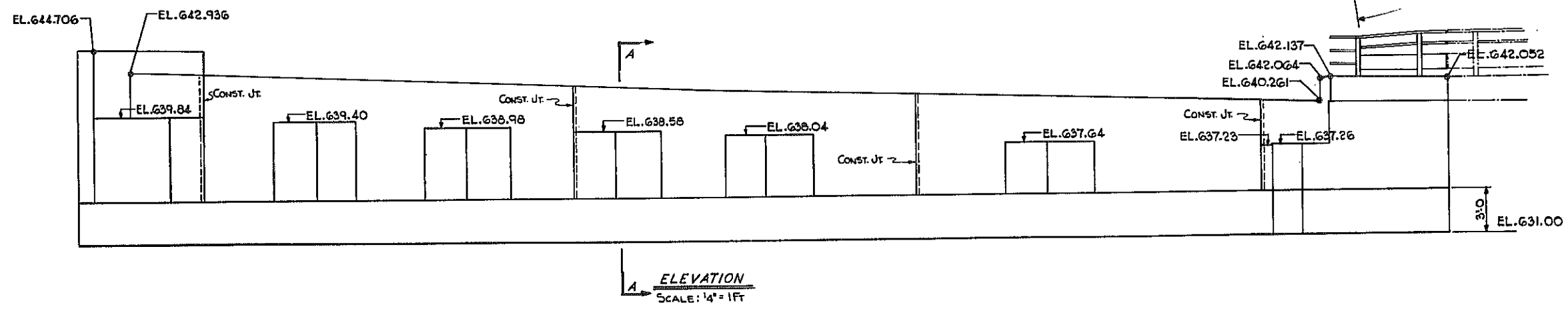
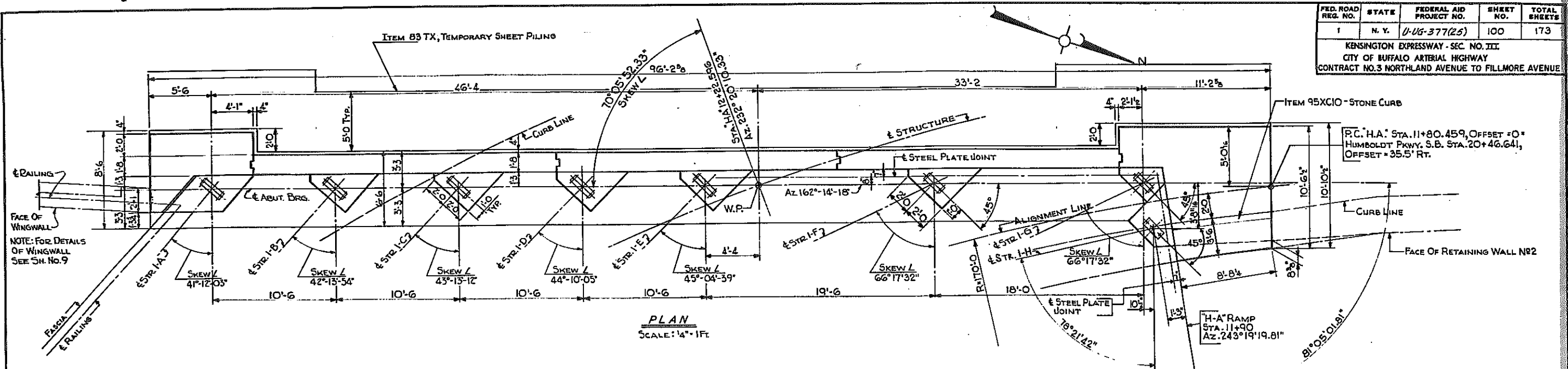
NOTES:
 FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.
 CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 CONCRETE IN THE PIER FOOTINGS, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.
 ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEDGED BOLTS AND SHALL BE ACCURATELY PLACED BY MEANS OF A TEMPLATE AND SET 10" INTO MASONRY.
 SEE SHEET NO. 2 FOR ANCHOR BOLT LENGTHS.

(RAMP H-A)
 E.B. RAMP OVER EXPRESSWAY
 PIER NO. 1
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	100	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



(RAMP H-A)
E. B. RAMP OVER EXPRESSWAY
WEST ABUTMENT

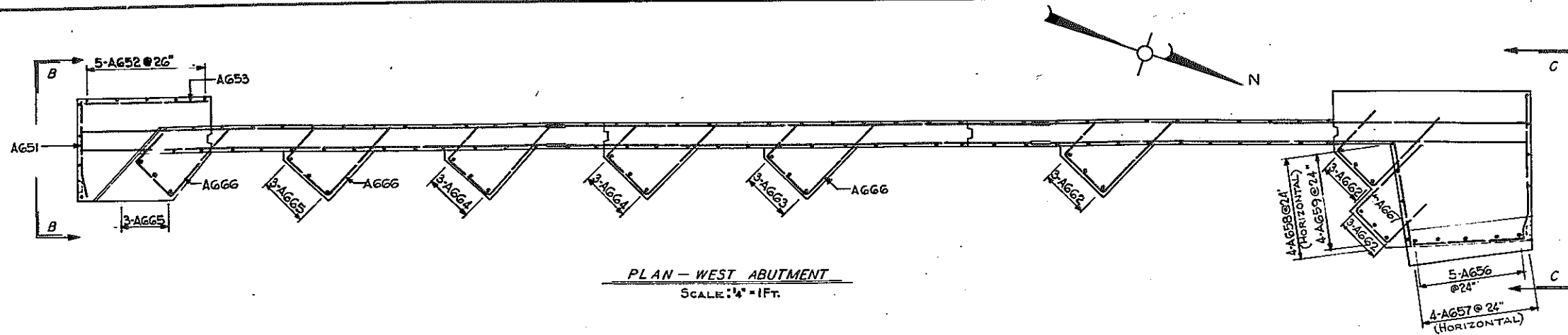
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.
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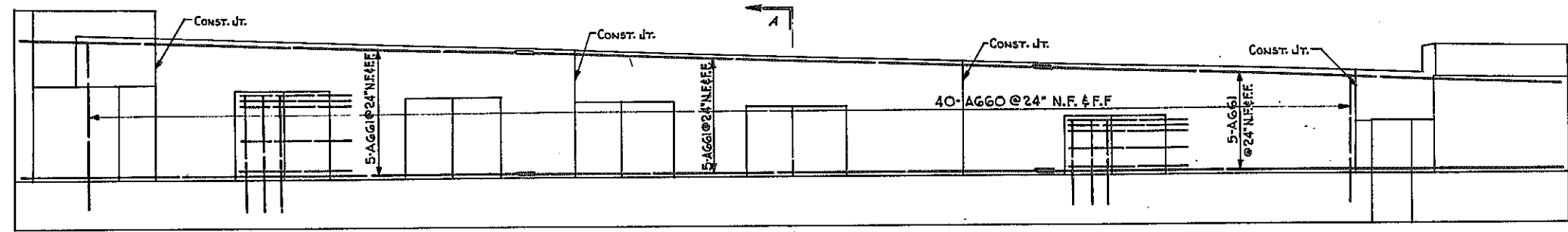
BR. SHEET NO. 5

FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377(25)	101	173

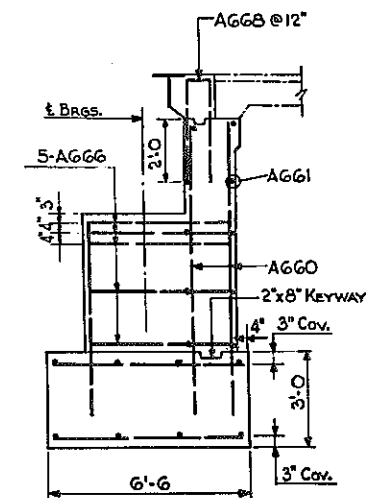
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



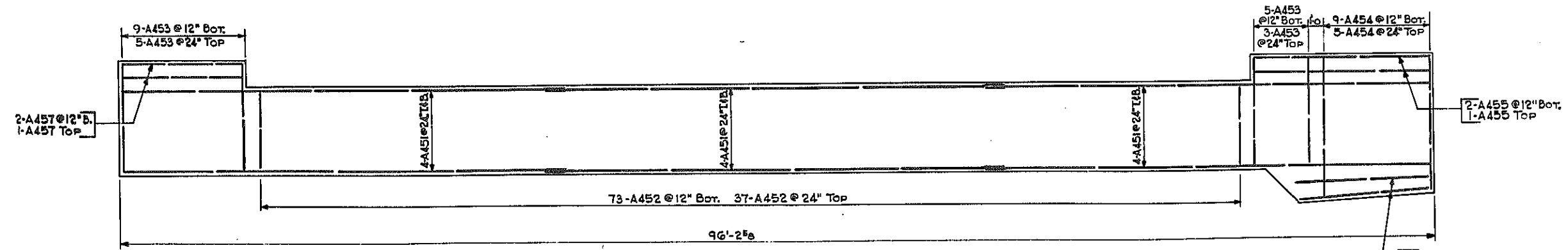
PLAN - WEST ABUTMENT
SCALE: 1/4" = 1 FT.



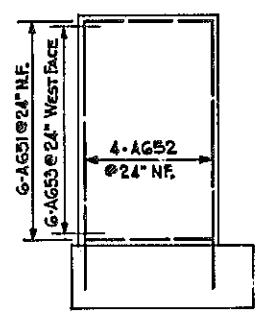
ELEVATION
SCALE: 1/4" = 1 FT.



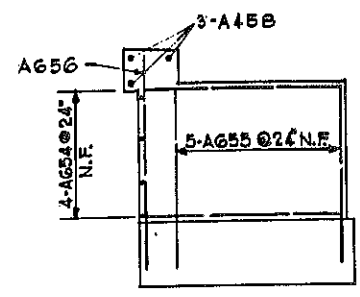
SECTION A - A
SCALE: 3/8" = 1 FT.



PLAN OF FOOTING
SCALE: 1/4" = 1 FT.



ELEVATION B - B
SCALE: 1/4" = 1 FT.



ELEVATION C - C
SCALE: 1/4" = 1 FT.

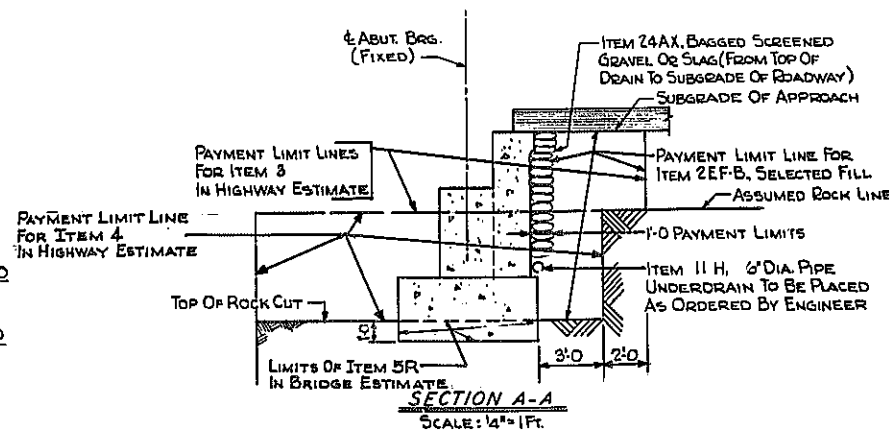
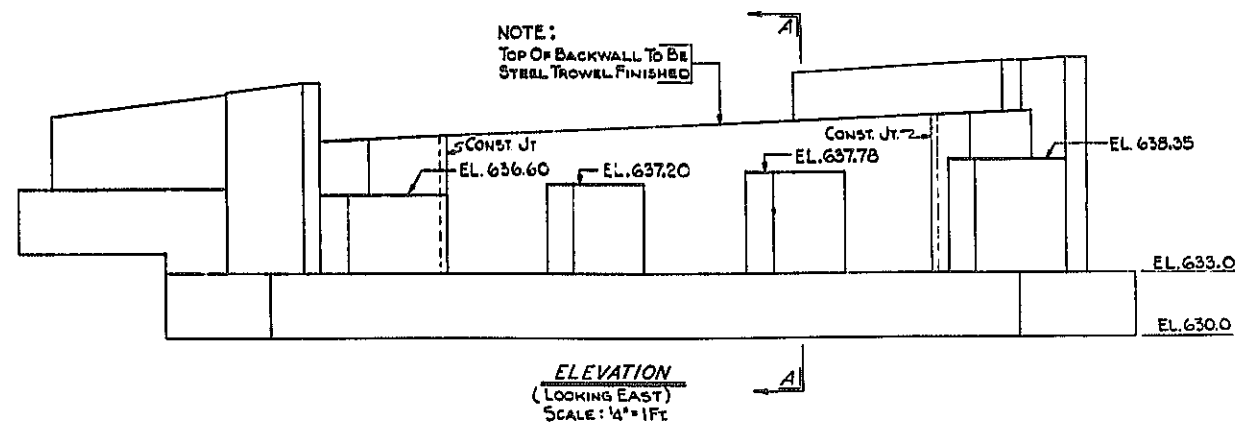
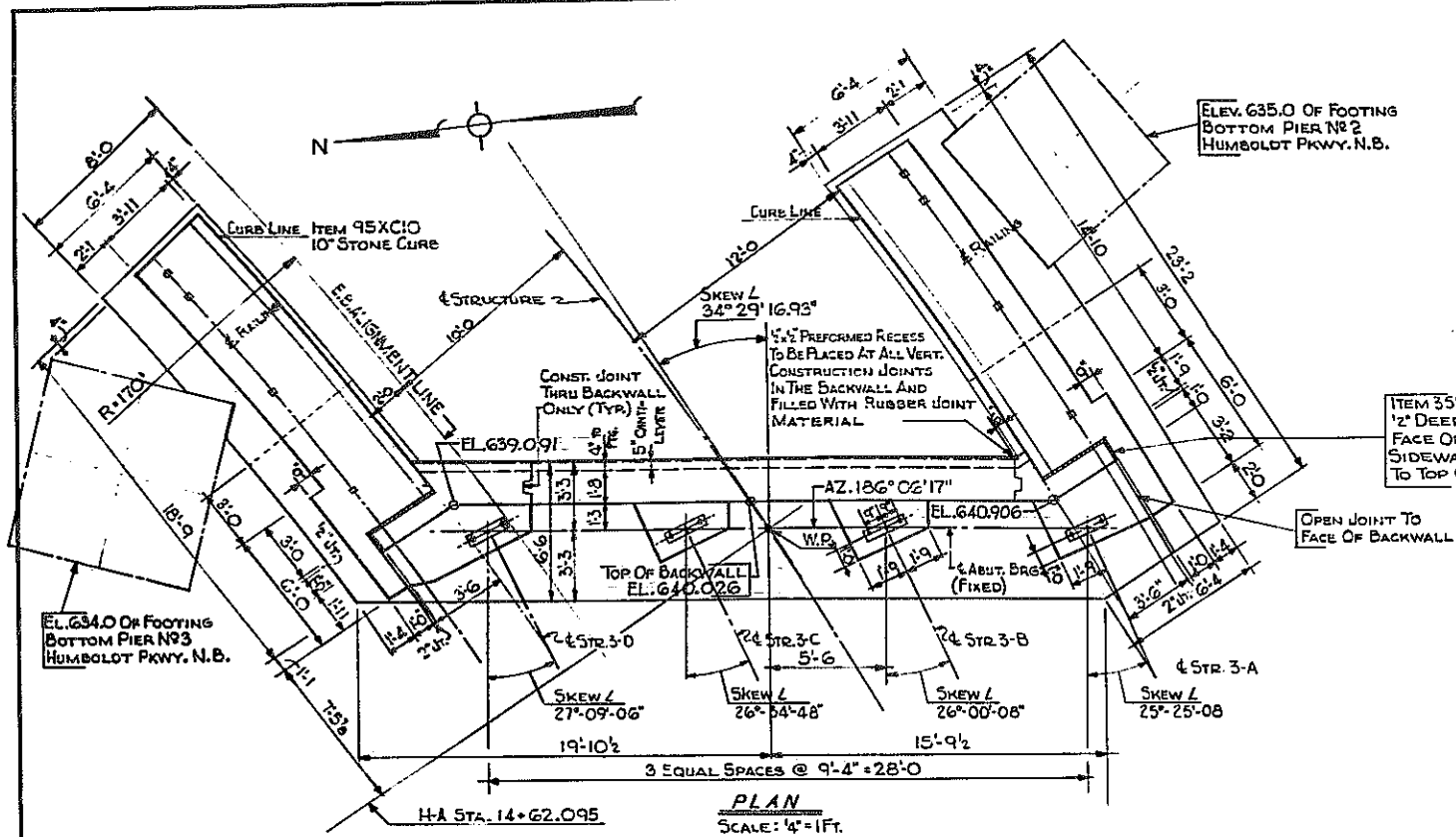
(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY
WEST ABUTMENT DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	102	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



NOTES:

CONCRETE IN ABUTMENT SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

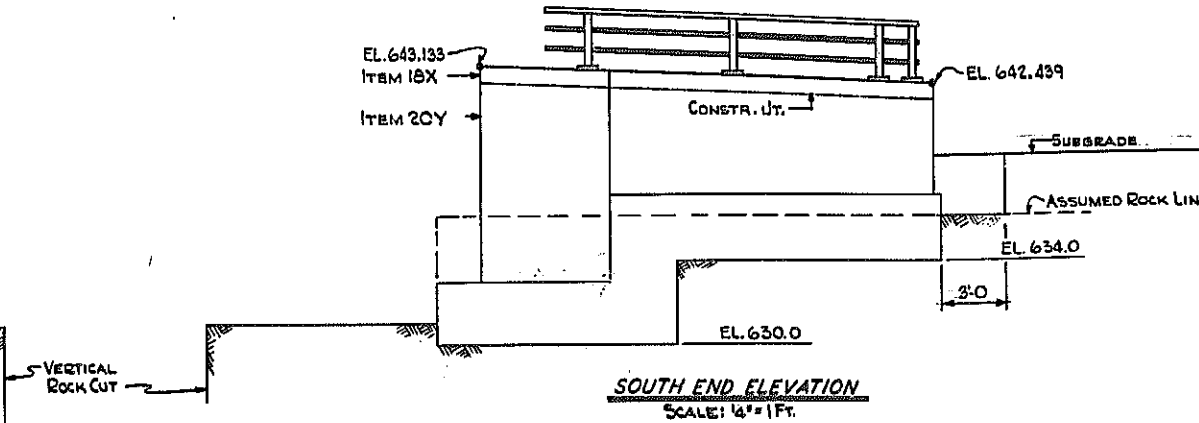
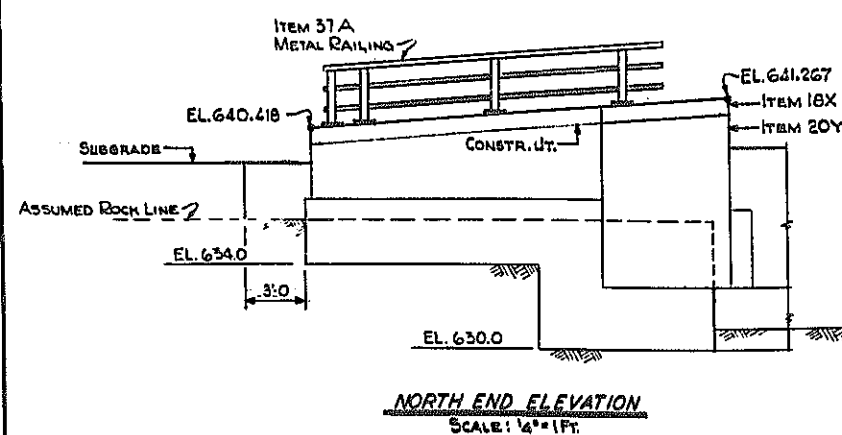
CONCRETE IN WINGWALL FASCIA SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.

FOR SECTIONS AND REINFORCING, SEE SHEET NO. 8

FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 2.

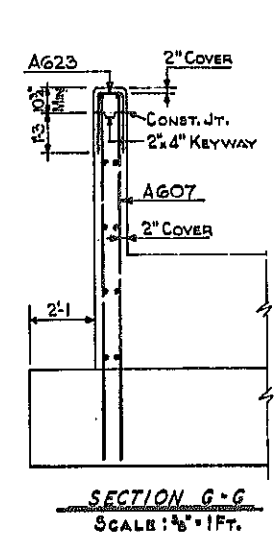
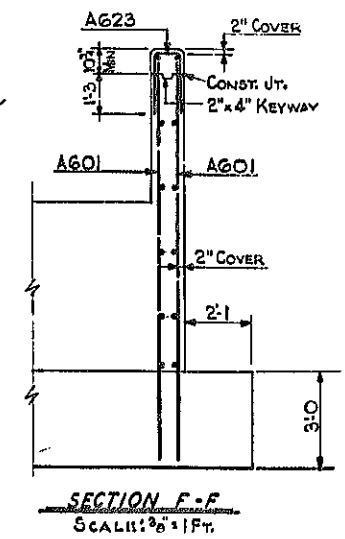
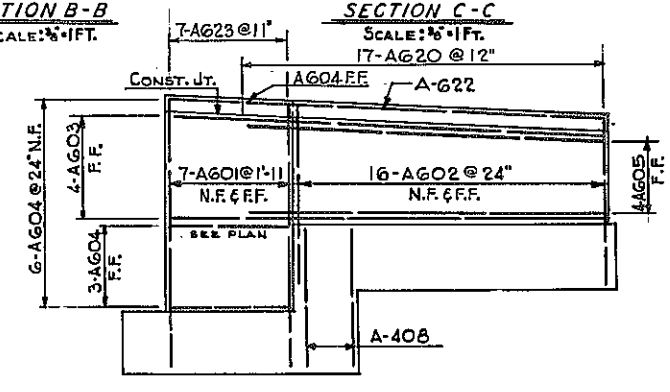
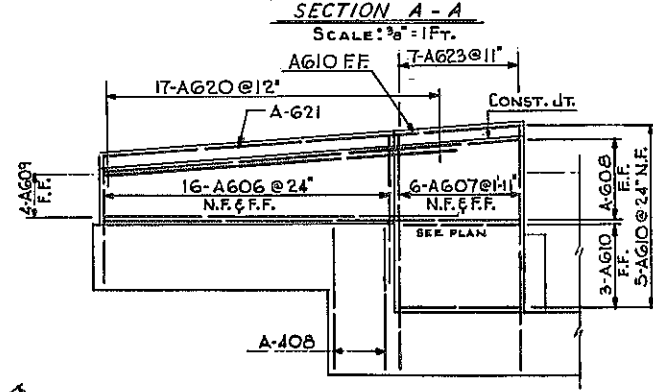
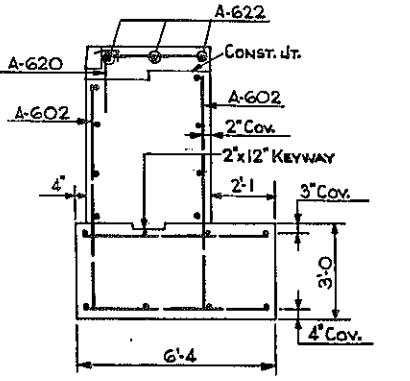
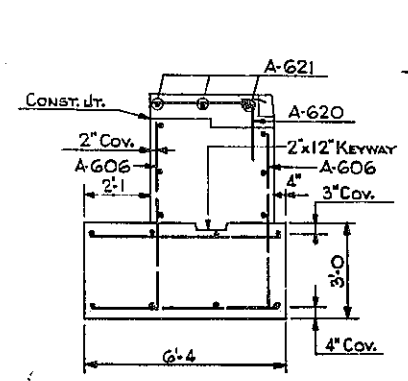
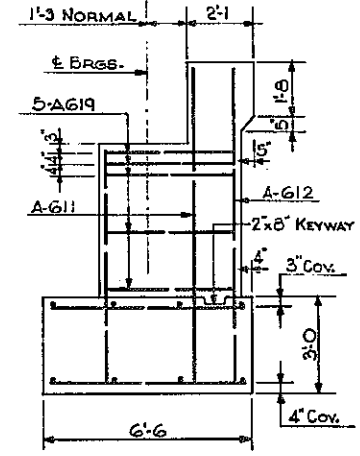
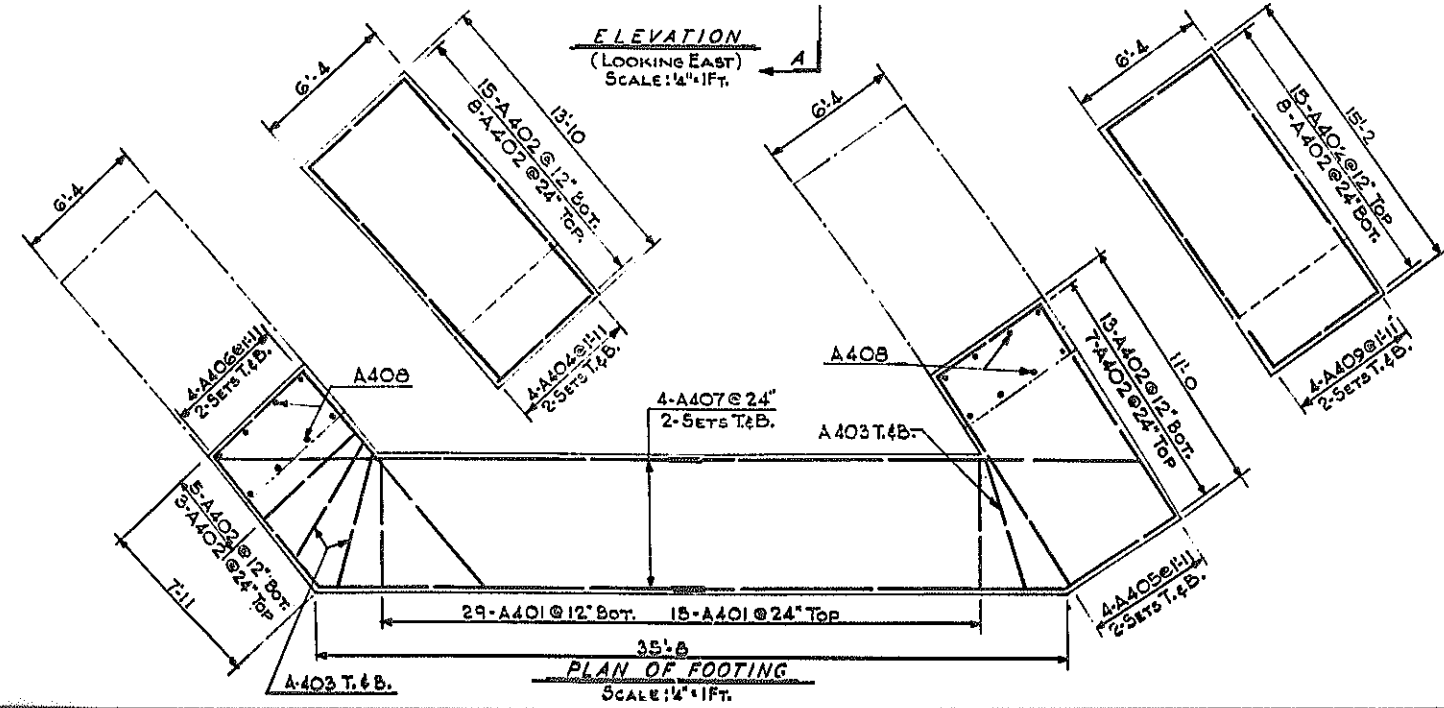
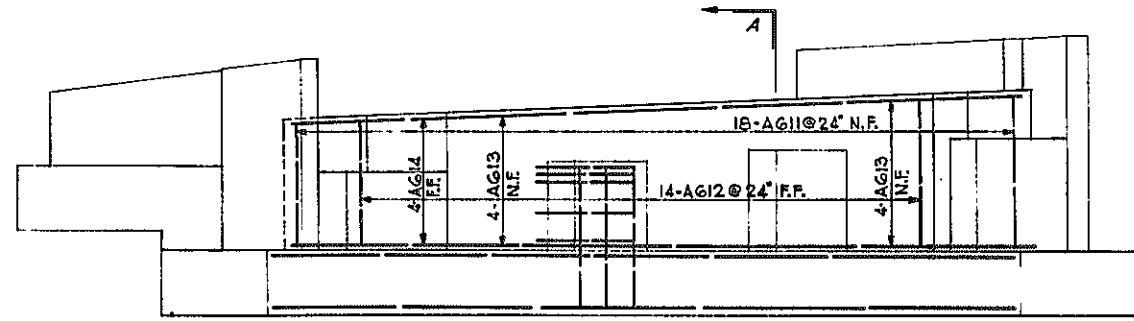
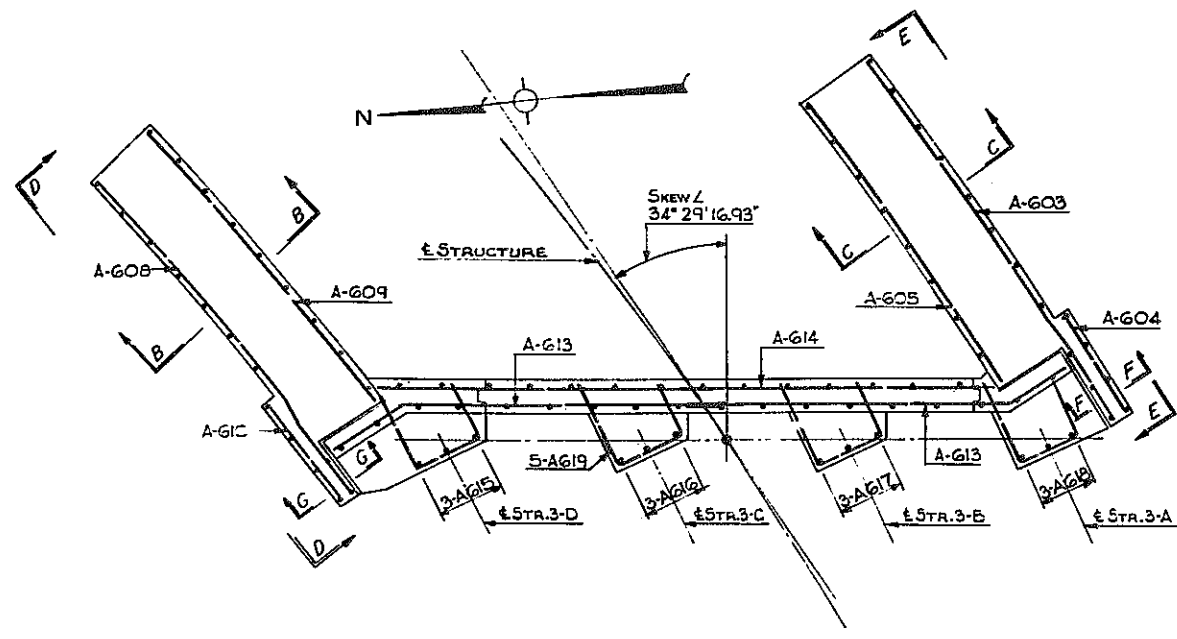
BITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.



(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY EAST ABUTMENT		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-116-377(25)	103	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

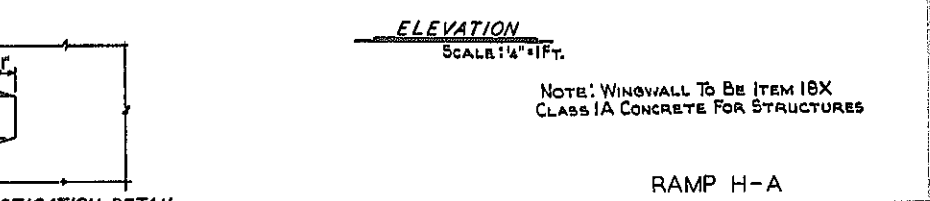
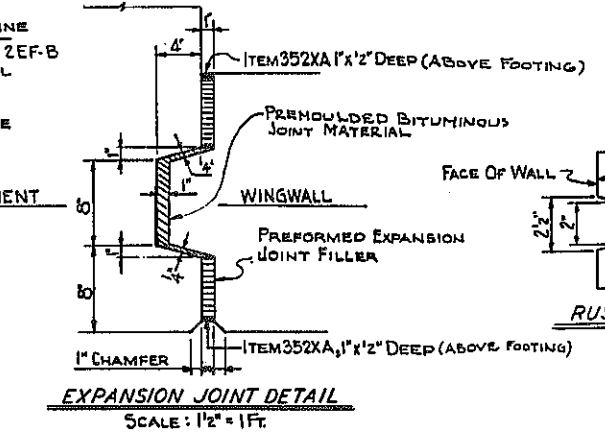
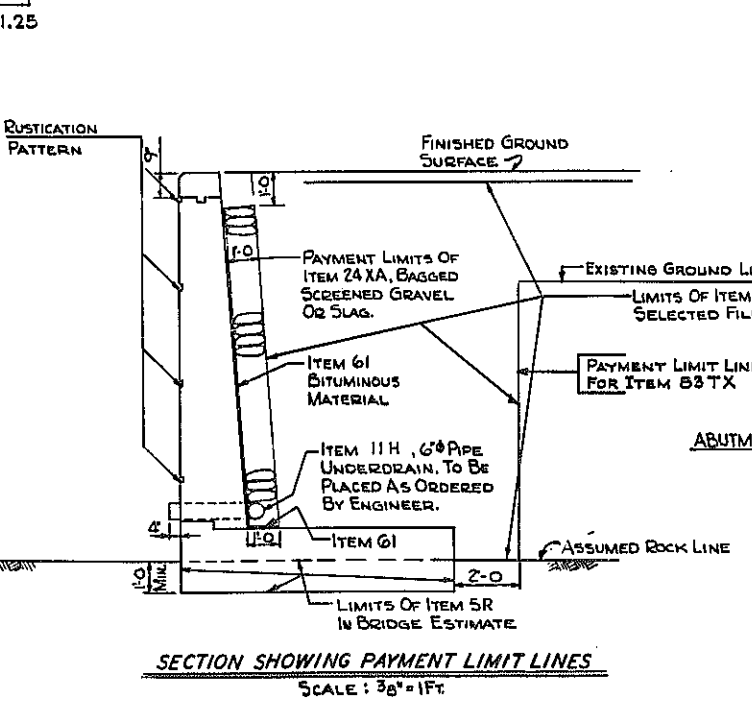
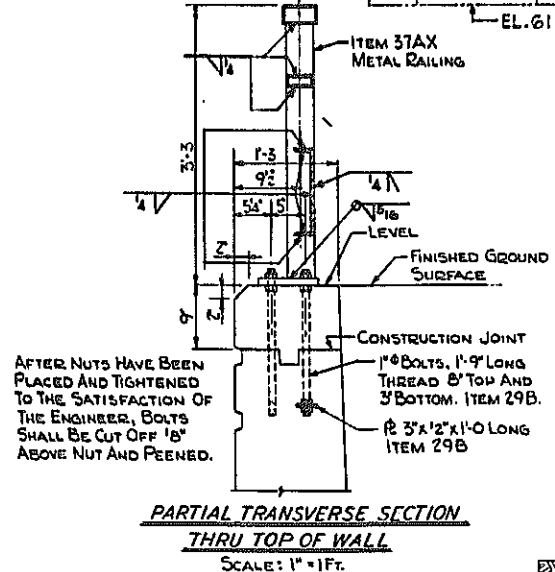
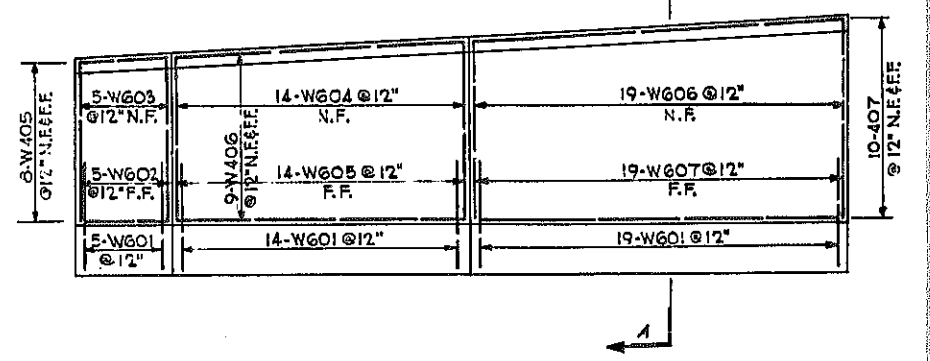
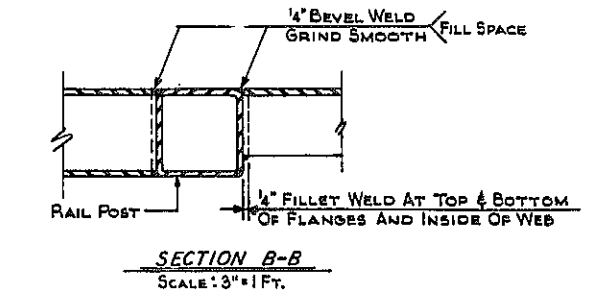
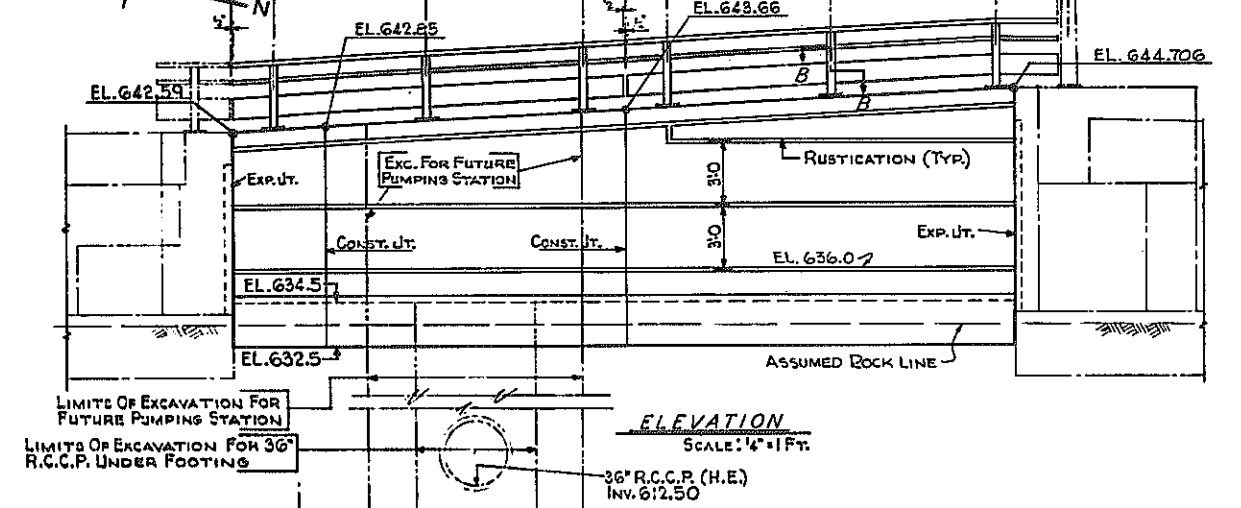
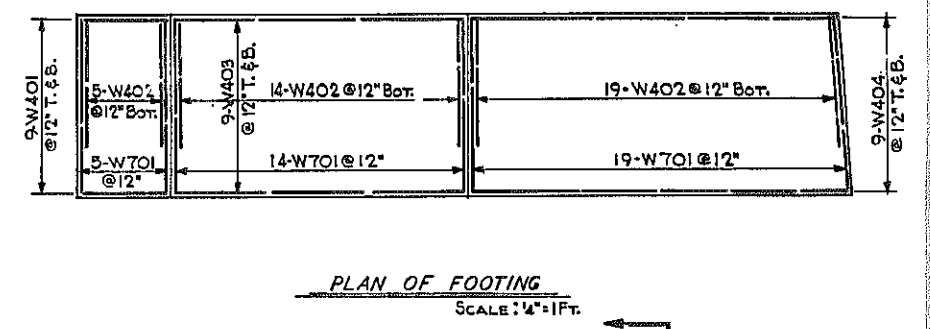
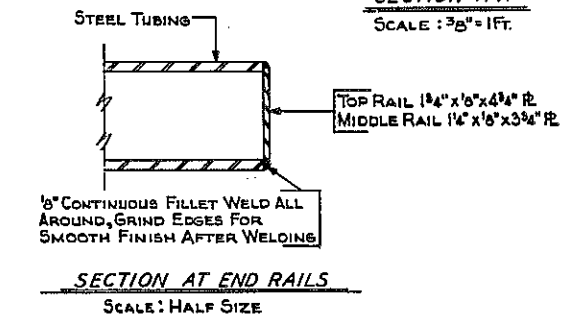
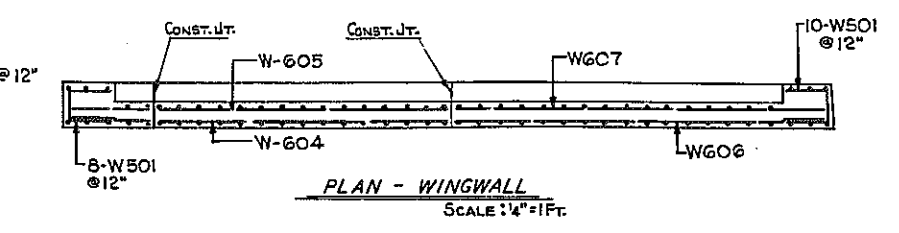
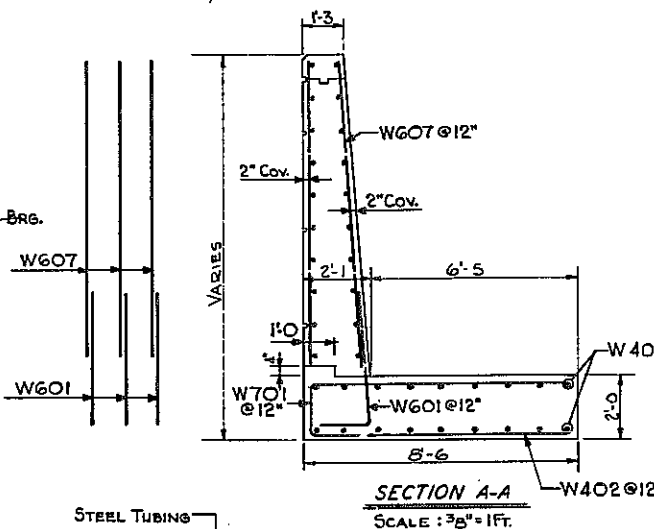
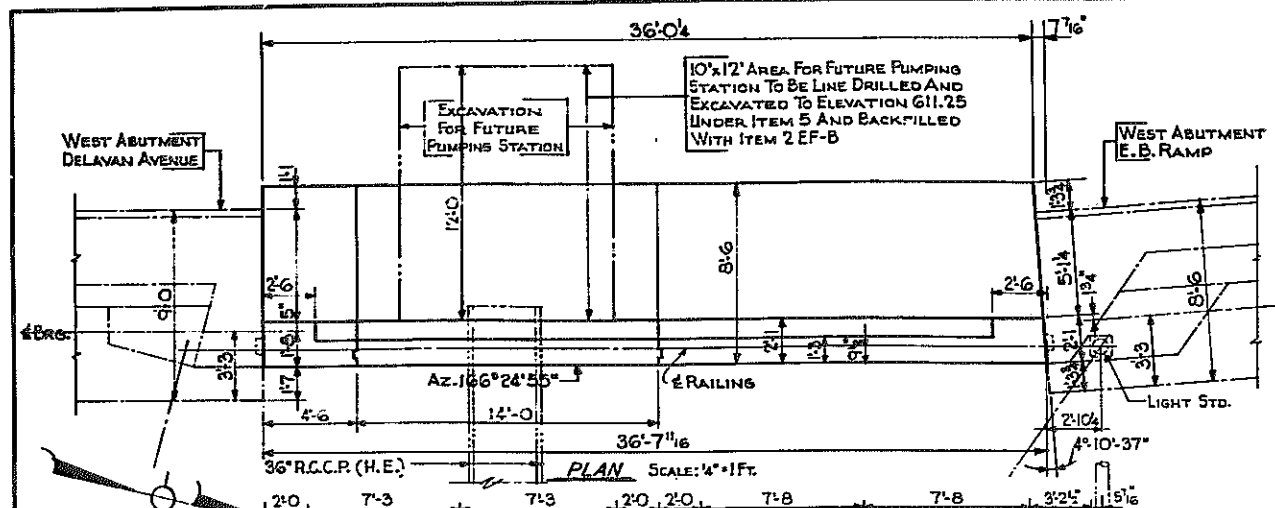


(RAMP H-A)
E.B. RAMP OVER EXPRESSWAY
EAST ABUTMENT DETAILS
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III
DE LEUW, CATHY & BRILL
ENGINEERS-ARCHITECTS
NEW YORK
DRAWN
CHECKED
TRACED
R.E.K.
C.E.

RP. SHEET NO. 8

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377 (25)	104	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: Wingwall To Be ITEM 18X CLASS 1A CONCRETE FOR STRUCTURES

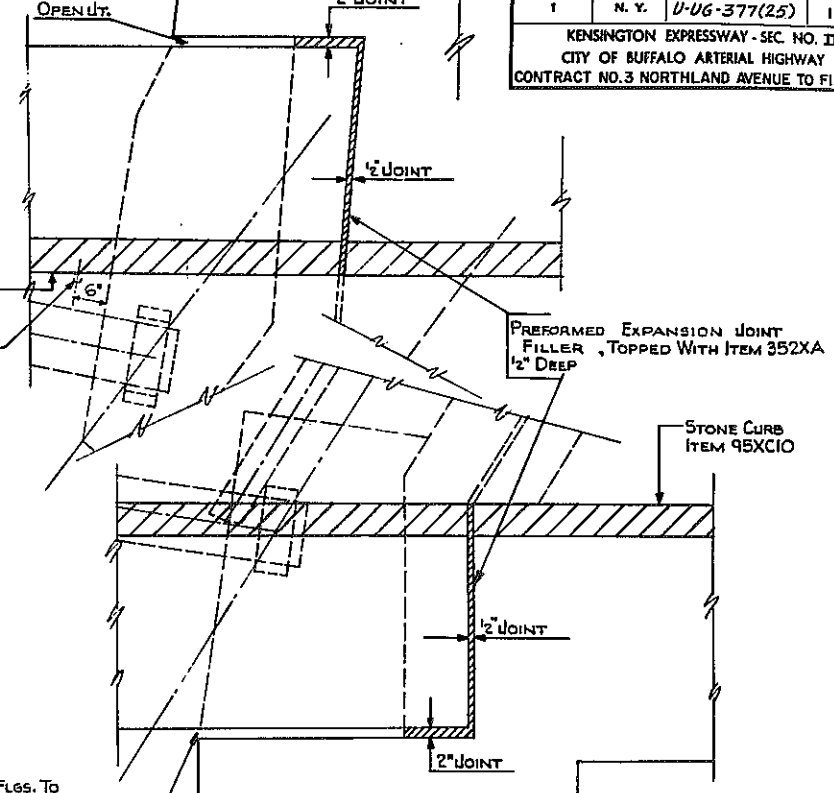
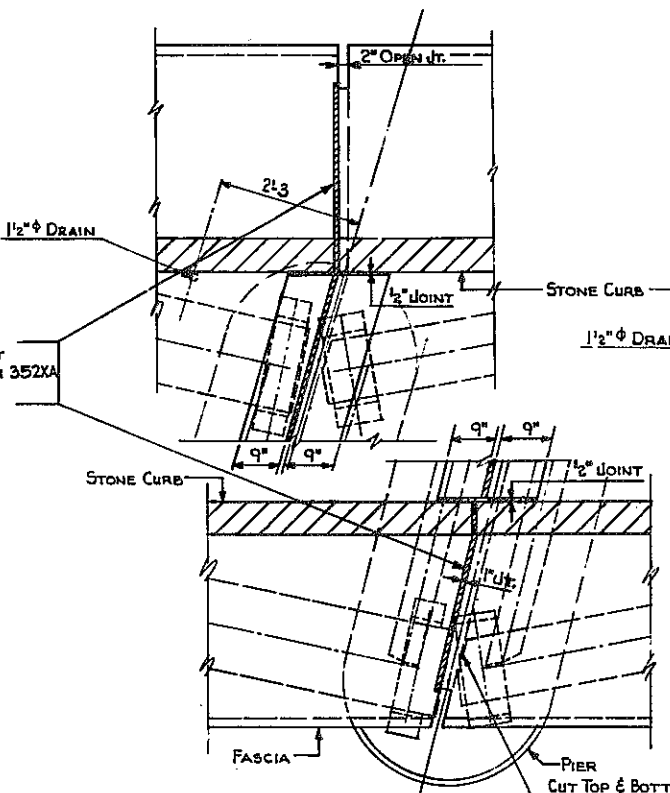
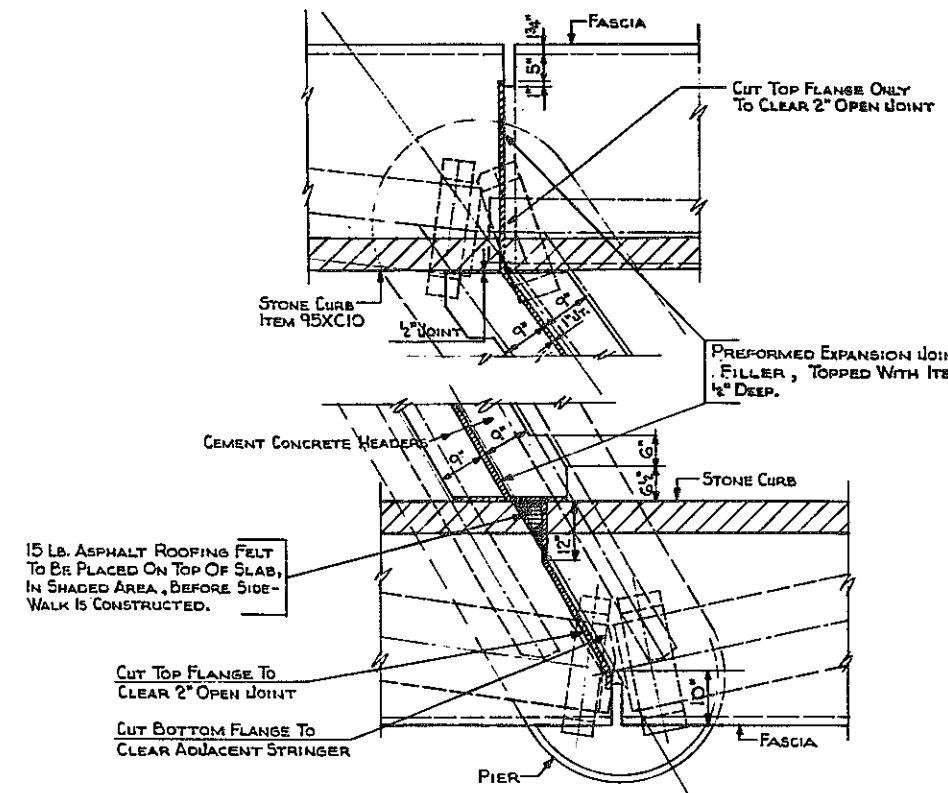
RAMP H-A

E.B. RAMP OVER EXPRESSWAY WINGWALL

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	107	173
KENSINGTON EXPRESSWAY - SEC. NO. III				
CITY OF BUFFALO ARTERIAL HIGHWAY				
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



15 LB. ASPHALT ROOFING FELT TO BE PLACED ON TOP OF SLAB, IN SHADED AREA, BEFORE SIDE-WALK IS CONSTRUCTED.

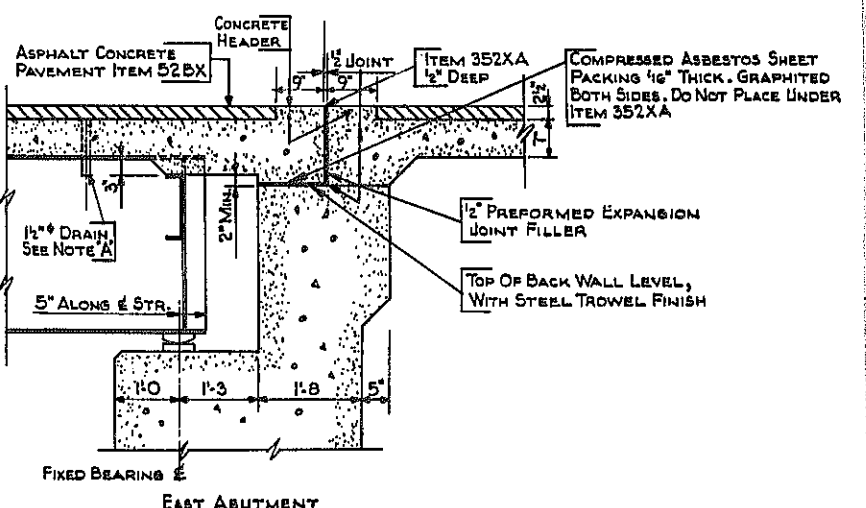
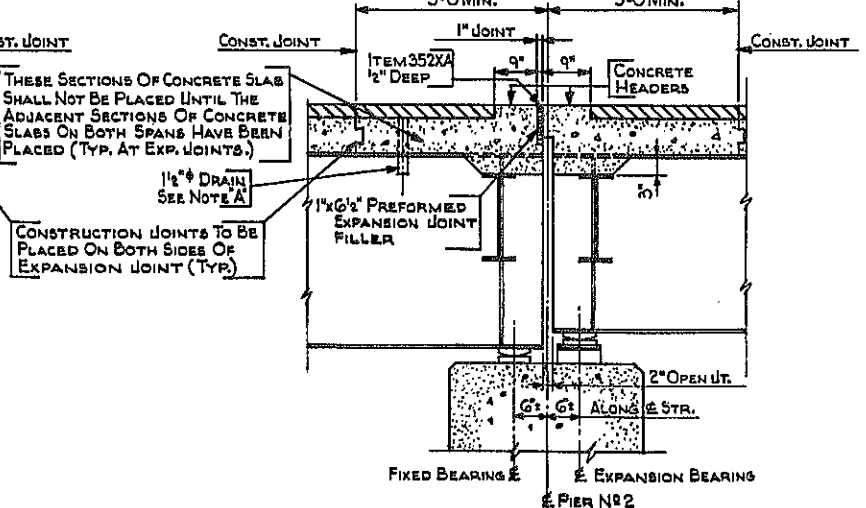
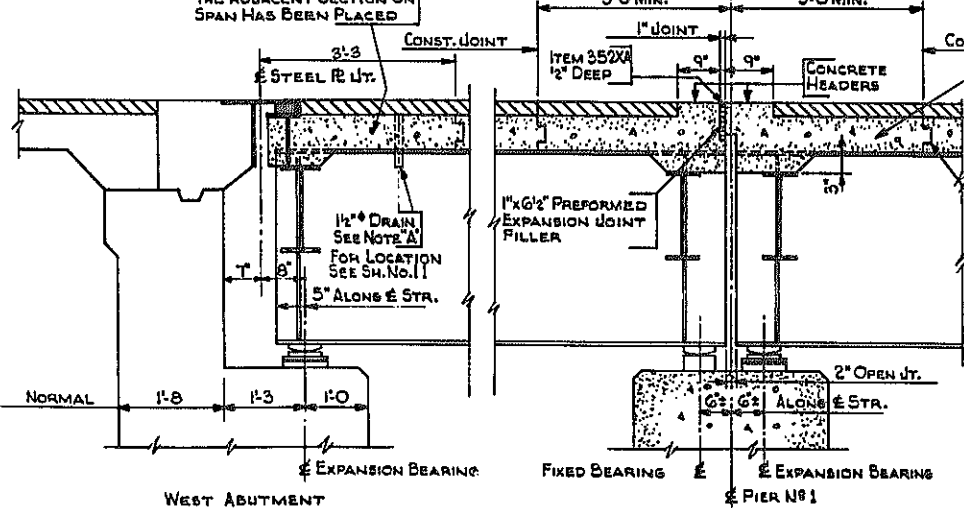
CUT TOP FLANGE TO CLEAR 2" OPEN JOINT
CUT BOTTOM FLANGE TO CLEAR ADJACENT STRINGER

THIS SECTION OF CONC. SLAB SHALL NOT BE PLACED UNTIL THE ADJACENT SECTION ON SPAN HAS BEEN PLACED

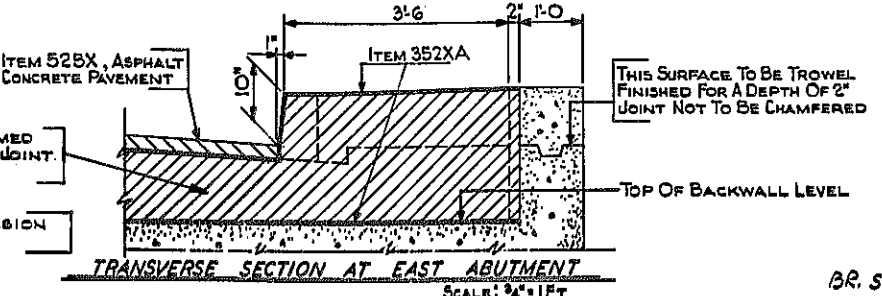
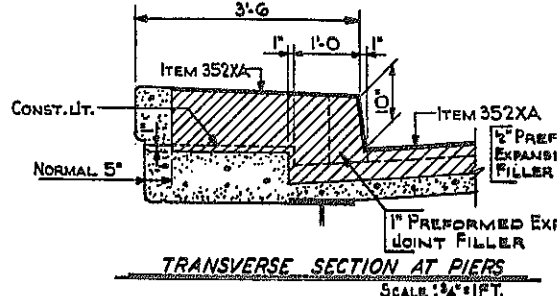
PLAN AT PIER NO. 1
SCALE: 3/4" = 1 FT.

PLAN AT PIER NO. 2
SCALE: 3/4" = 1 FT.

PLAN AT EAST ABUTMENT
SCALE: 3/4" = 1 FT.



LONGITUDINAL SECTION ALONG PROFILE GRADE LINE
SCALE: 3/4" = 1 FT.



TRANSVERSE SECTION AT PIERS
SCALE: 3/4" = 1 FT.

TRANSVERSE SECTION AT EAST ABUTMENT
SCALE: 3/4" = 1 FT.

NOTE:
SEE SHEET N211 FOR DESCRIPTION OF MATERIAL AND DIMENSIONS OF STEEL PLATE JOINT

NOTE "A"
1/2" STD. GALV. W.I. O:9" LG. DRAIN AT LOW POINTS. PLACE TO DRIP FREE OF BEARINGS AND PEDESTALS. PAYMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM 18 X. PITCH SLAB 1/8" MIN. TO DRAIN TO ELIMINATE POCKET.

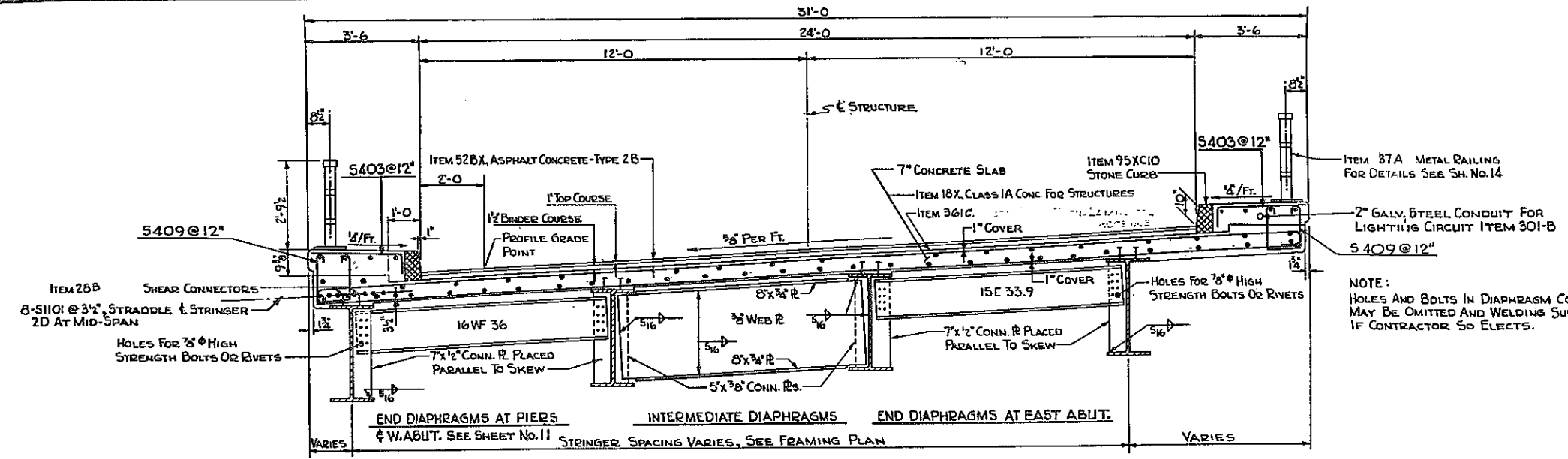
(RAMP H-A)

E.B. RAMP OVER EXPRESSWAY LONGITUDINAL SECTION & DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E. C.E.

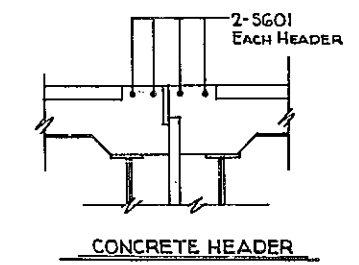
BR. SHEET NO. 12

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	108	173

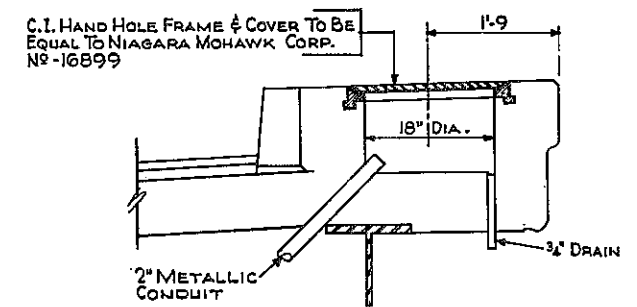
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



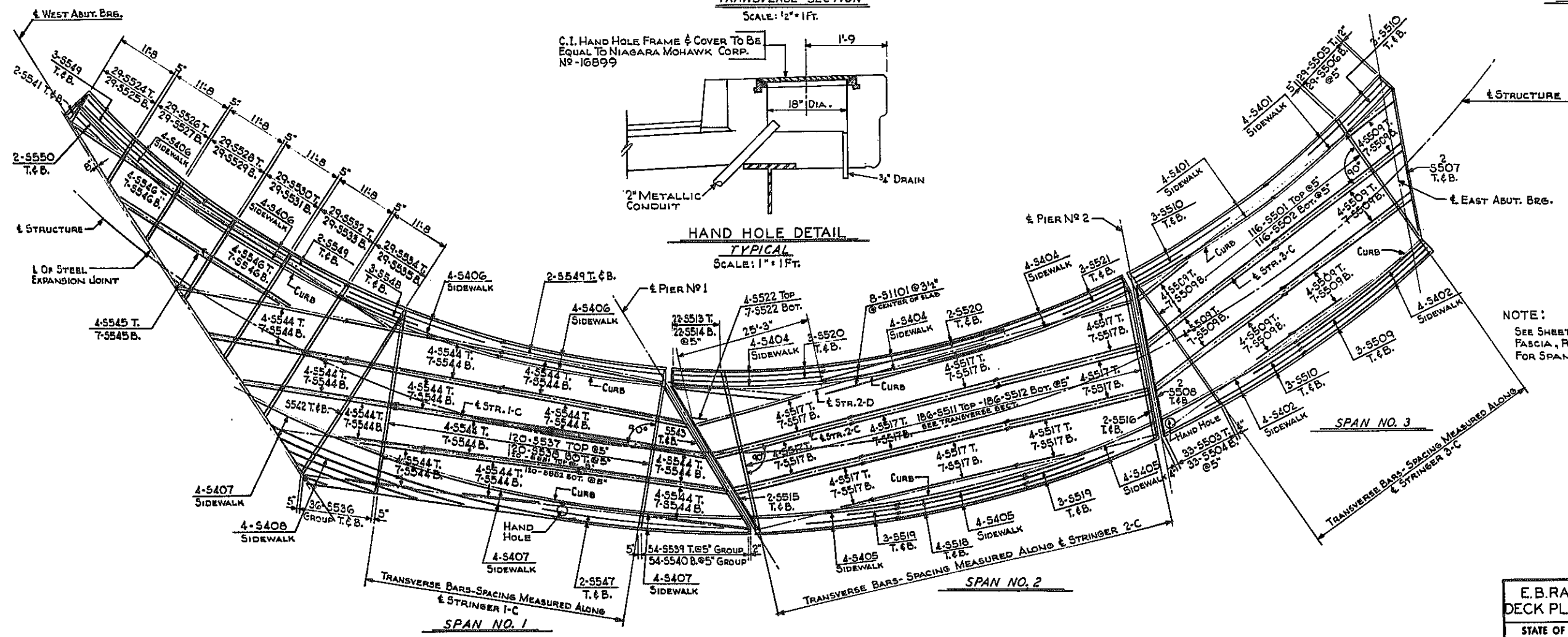
NOTE:
HOLES AND BOLTS IN DIAPHRAGM CONNECTIONS MAY BE OMITTED AND WELDING SUBSTITUTED IF CONTRACTOR SO ELECTS.



TRANSVERSE SECTION
SCALE: 1/2\"/>



HAND HOLE DETAIL
TYPICAL
SCALE: 1\"/>



DECK PLAN
SCALE: 1\"/>

NOTE:
SEE SHEET NO. 10 FOR DIMENSIONS AT FASCIA, ROADWAY LINES AND STRINGER, FOR SPANS 1, 2 & 3.

(RAMP H-A)

**E. B. RAMP OVER EXPRESSWAY
DECK PLAN LAYOUT AND SECTION**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.B.
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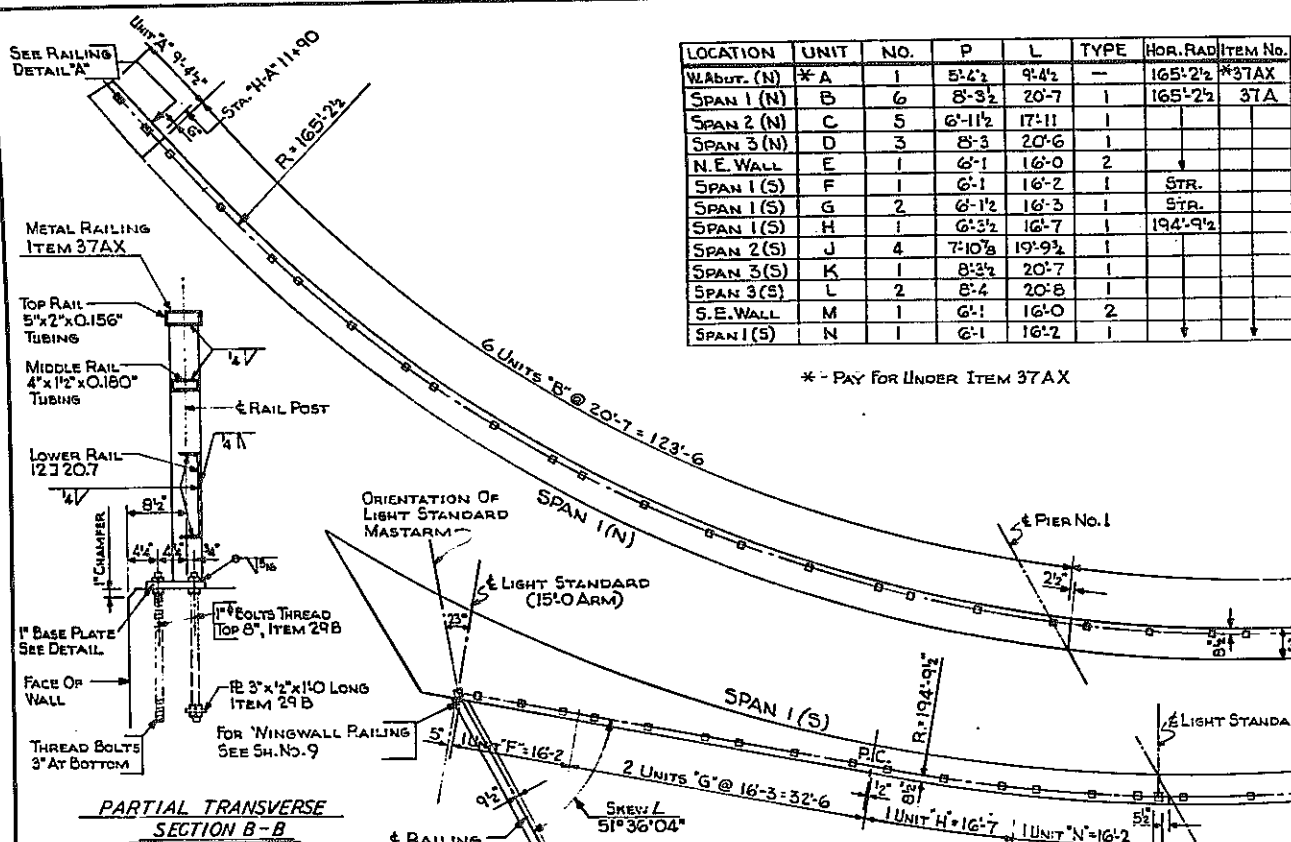
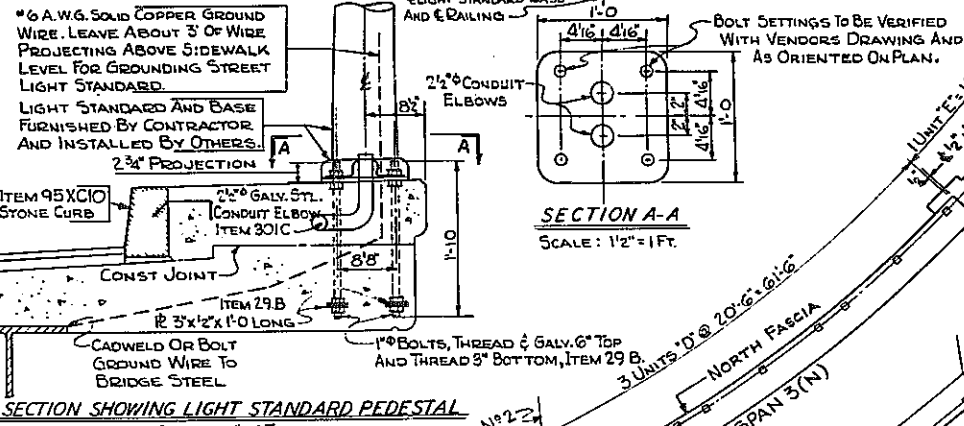
BR. SHEET NO. 13

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377(25)	109	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

LOCATION	UNIT	NO.	P	L	TYPE	HOR. RAD.	ITEM NO.
W. Abut. (N)	*A	1	5'-4 1/2	9'-4 1/2	-	165'-2 1/2	*37AX
SPAN 1 (N)	B	6	8'-3 1/2	20'-7	1	165'-2 1/2	37A
SPAN 2 (N)	C	5	6'-11 1/2	17'-11	1		
SPAN 3 (N)	D	3	8'-3	20'-6	1		
N.E. WALL	E	1	6'-1	16'-0	2		
SPAN 1 (S)	F	1	6'-1	16'-2	1	STR.	
SPAN 1 (S)	G	2	6'-1 1/2	16'-3	1	STR.	
SPAN 1 (S)	H	1	6'-3 1/2	16'-7	1	194'-9 1/2	
SPAN 2 (S)	J	4	7'-10 3/8	19'-9 1/2	1		
SPAN 3 (S)	K	1	8'-3 1/2	20'-7	1		
SPAN 3 (S)	L	2	8'-4	20'-8	1		
S.E. WALL	M	1	6'-1	16'-0	2		
SPAN 1 (S)	N	1	6'-1	16'-2	1		

* - PAY FOR UNDER ITEM 37 AX



CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE

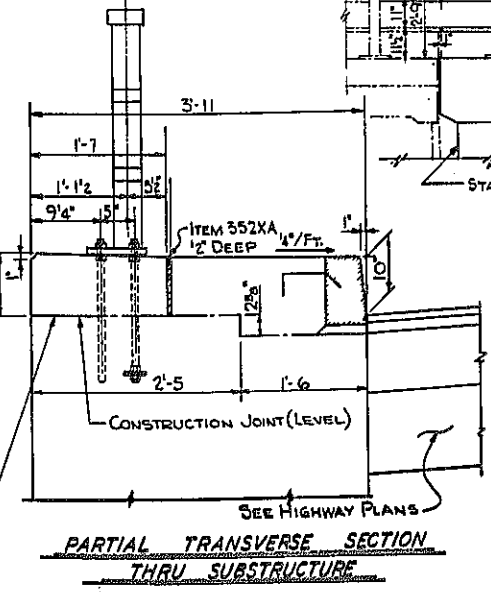
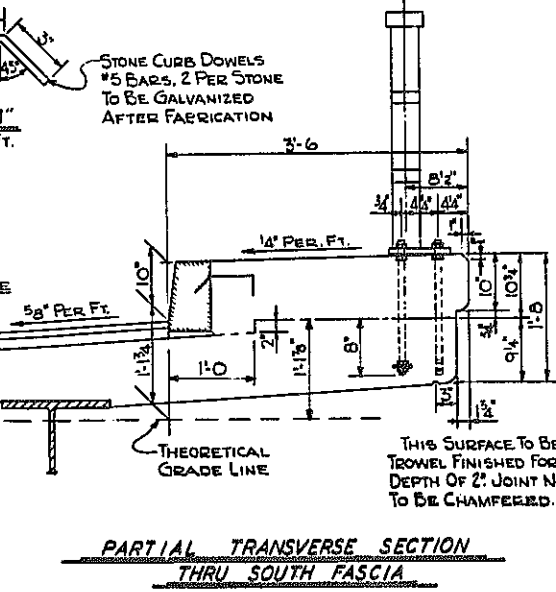
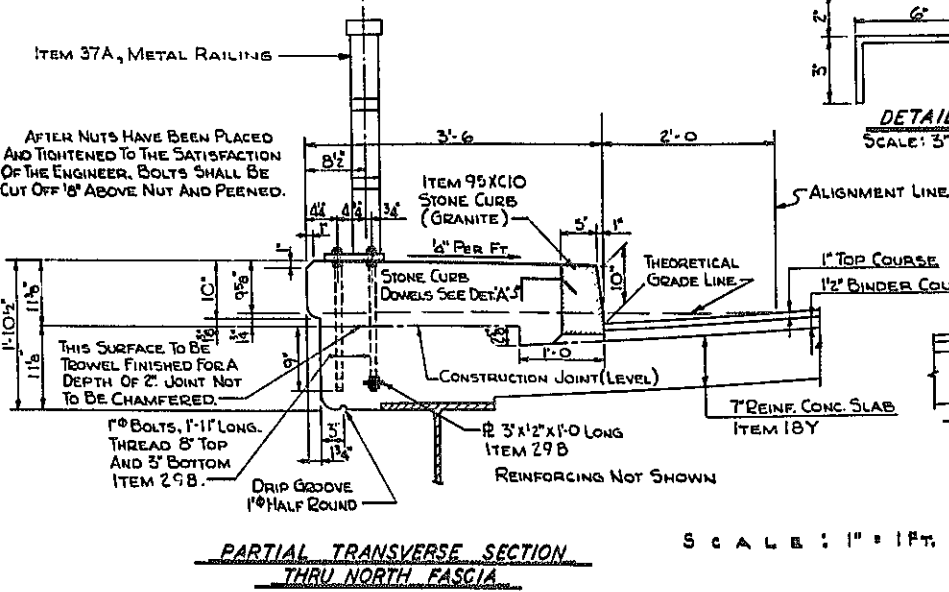
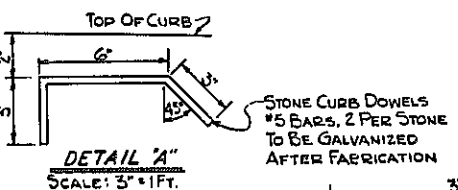
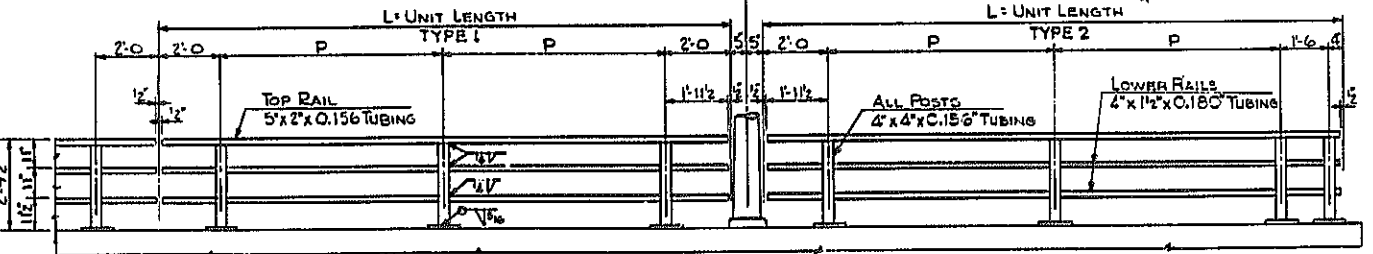
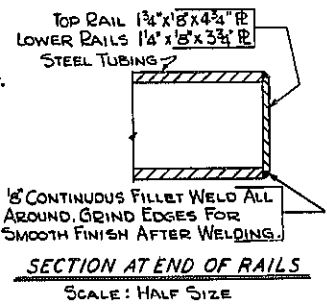
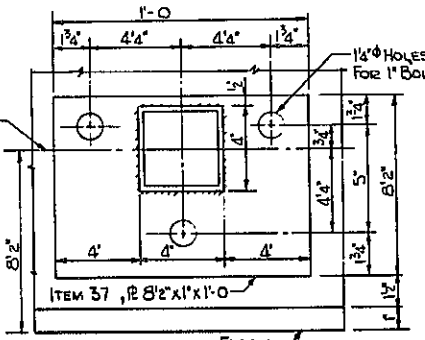
1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR SLAB. BROOM FINISH TOP OF SLAB BETWEEN CURBS.
2. PLACE ITEM 95XC10, STONE CURB (GRANITE).
3. PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
4. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
5. PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON PLATES.
6. POUR SIDEWALK TO PROPER LINE AND GRADE. BROOM SURFACE OF SLAB TO BE GIVEN A FIBER GLASS WATERPROOFING ITEM 450 B.
7. PLACE ROADWAY PAVEMENT.

CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE

1. SET ANCHOR BOLTS BY MEANS OF A TEMPLATE AND POUR WALL TO CONSTRUCTION JOINT.
2. PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
3. PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
4. PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON PLATES.
5. POUR WALL ABOVE CONSTRUCTION JOINT.
6. POUR SIDEWALK TO PROPER LINE AND GRADE.
7. PLACE ROADWAY PAVEMENT.

GENERAL NOTES

1. DIMENSIONS FOR TUBING ARE OUTSIDE DIMENSIONS.
2. ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO EACH OTHER AND TO THE TOP OF FASCIA AND SO THAT THE POSTS ARE TRULY VERTICAL.
3. ALL SURFACES OF WELDS ON RAILING TO BE GROUND SMOOTH.
4. THE RAILS AND POSTS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT AND GRADE TO THE FULL SATISFACTION OF THE ENGINEER.
5. IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION WORK IS PERFORMED.
6. ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29 B.
7. TUBULAR RAILS, TUBULAR POST AND BASE PLATES WILL BE PAID FOR UNDER ITEMS 37A & 37AX.



(RAMP H-A)

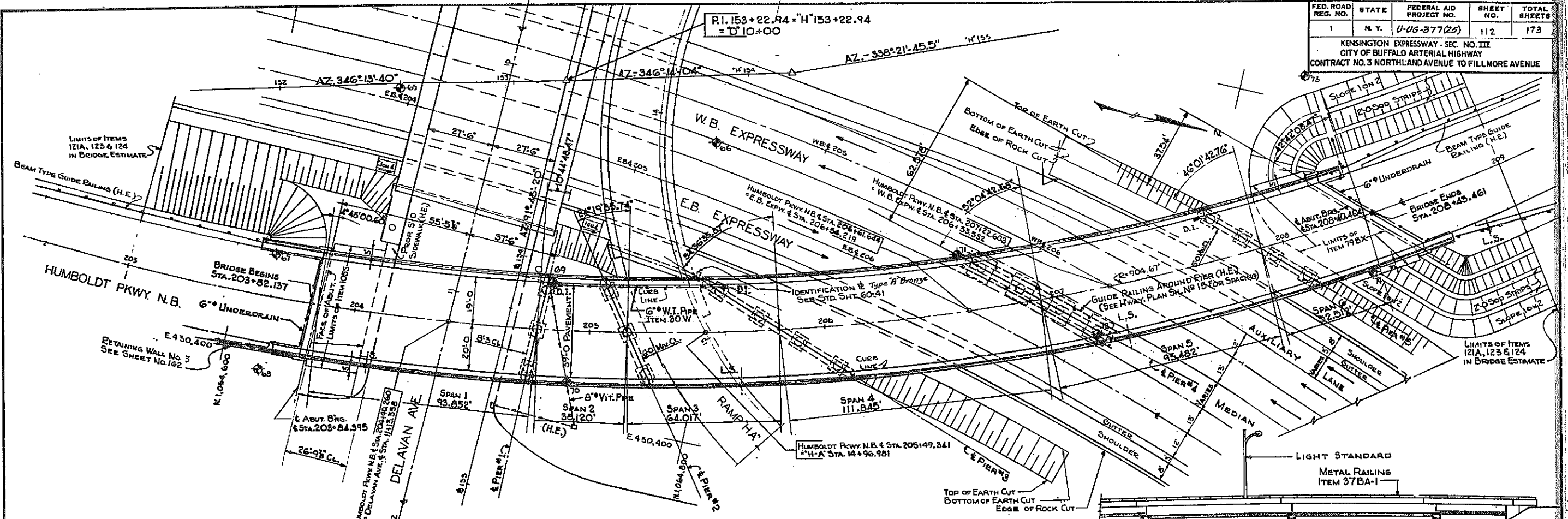
E.B. RAMP OVER EXPRESSWAY RAILING LAYOUT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL HIGHWAY
KENSINGTON EXPRESSWAY, SEC. NO.

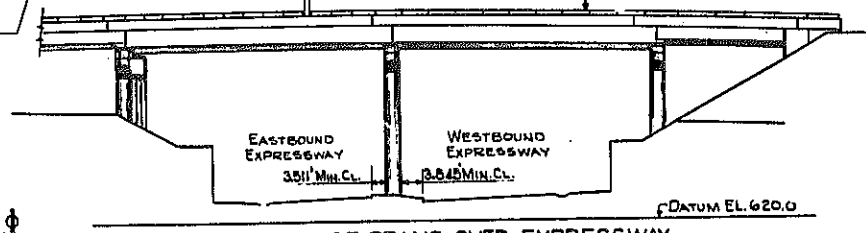
DE LIEW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.V. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	112	173

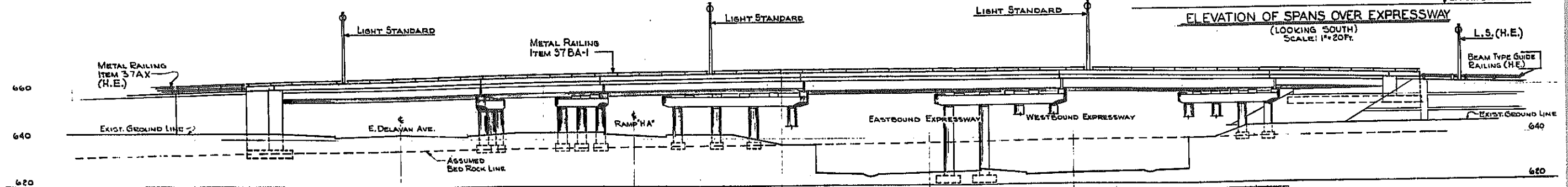
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



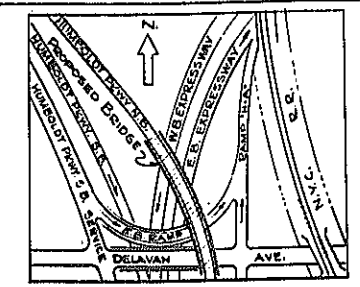
PLAN
SCALE: 1" = 20 FT.



ELEVATION OF SPANS OVER EXPRESSWAY
(LOOKING SOUTH)
SCALE: 1" = 20 FT.



DEVELOPED ELEVATION - LOOKING WEST
SCALE: 1" = 20'



LOCATION PLAN
SCALE: 1" = 200 FT.

HUMBOLDT PKWY N.B. OVER EXPW.
GENERAL PLAN

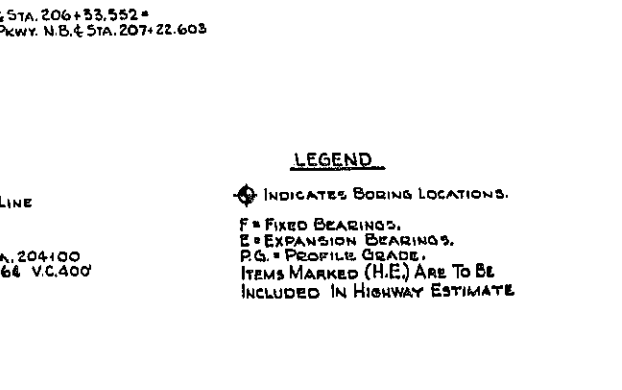
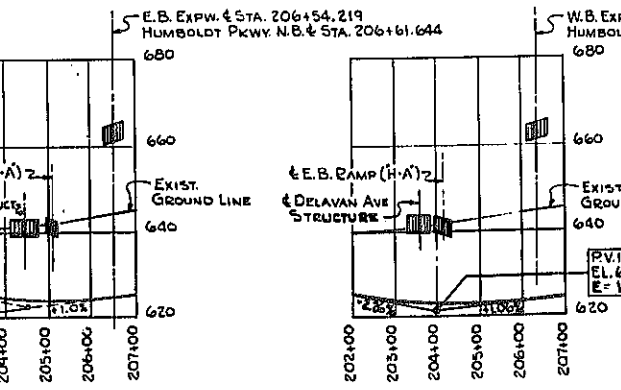
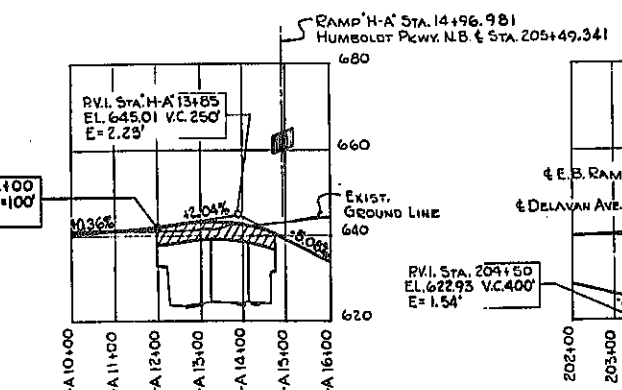
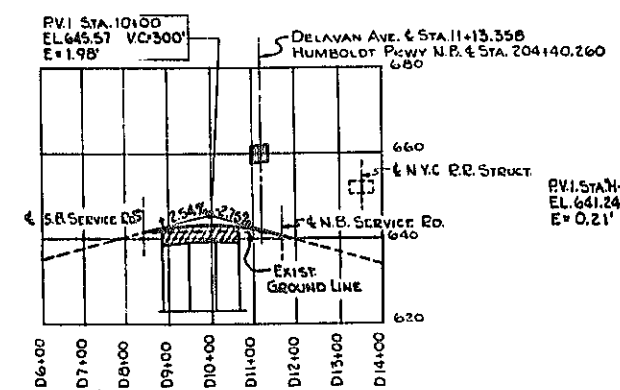
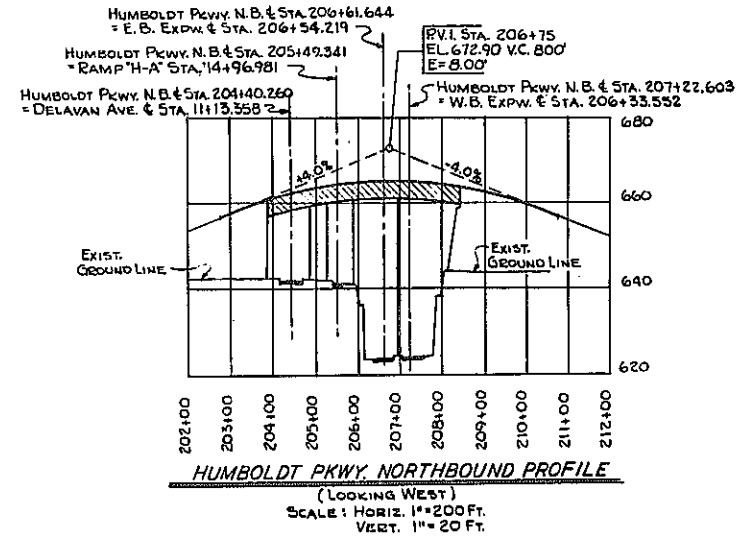
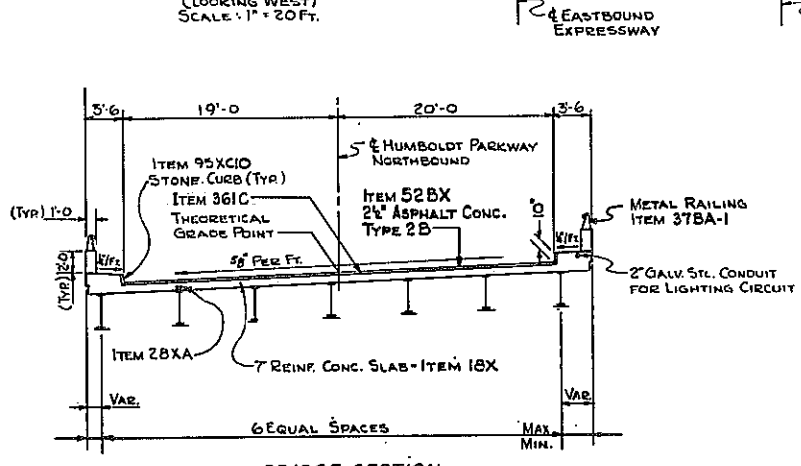
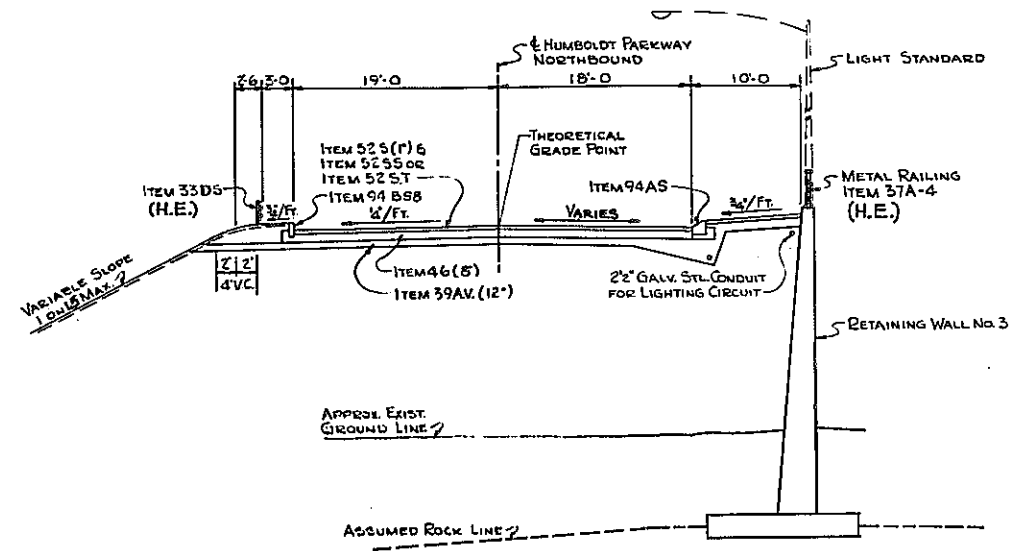
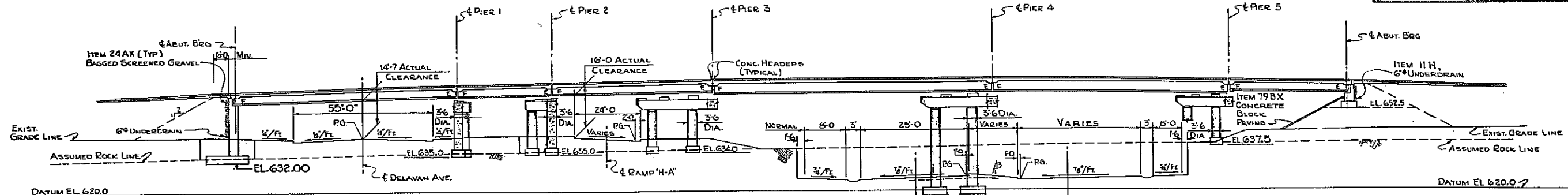
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. C.E.
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BR. SHEET NO. 1

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	113	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



LEGEND
 * INDICATES BORING LOCATIONS.
 F = FIXED BEARINGS.
 E = EXPANSION BEARINGS.
 P.G. = PROFILE GRADE.
 ITEMS MARKED (H.E.) ARE TO BE INCLUDED IN HIGHWAY ESTIMATE

HUMBOLDT PKWY. N.B. OVER EXPW. PROFILES & SECTIONS
 STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.B. G.E.
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FED. ROAD REG. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	114	175

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : E20-516-45 LIVE LOAD OR THE ALTERNATE LOADING OF TABLE 25, 000 LB. AXLES SPACED AT 14 FT. CENTERS.

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, OR THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.

THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENT DOES NOT EXCEED 5 TONS PER SQUARE FOOT, AND DOES NOT EXCEED 1.5 TONS PER SQUARE FOOT ON EARTH.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, IN SOUTH ABUTMENT, AND IN SIDEWALK POURS ON ALL ABUTMENT WINGWALLS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, NORTH ABUTMENT AND WINGWALLS (EXCEPT IN SIDEWALK POURS) SHALL BE ITEM 20Y, CLASS I CONCRETE.

CEMENT : CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET 60-41.

OTHER : REINFORCEMENT BARS SHALL BE LAPPED 30 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED.

SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH NUMBERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.

THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 20 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING REMOLDED BITUMINOUS JOINT, COMPRESSED ASBESTOS SHEET PACKING, PREF. EPR/JL FILLER AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

FOOTINGS : ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS I CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE HEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE. EXCAVATION AS ORDERED OR SHOWN ON THE PLANS SHALL BE ITEMS 5E AND 5R TRENCH, CULVERT AND BRIDGE EXCAVATION.

WELDING : WELDED JOINTS SHALL BE RADIOGRAPHED IN CONFORMANCE WITH THE REQUIREMENTS OF THE NEW YORK STATE SPECIFICATION FOR RADIOGRAPHIC INSPECTION. THE JOINTS TO BE RADIOGRAPHED ARE DESCRIBED THEREIN.

ALL WELDING SHALL CONFORM TO THE CURRENT "STANDARD SPECIFICATIONS FOR WELDED HIGHWAY AND RAILROAD BRIDGES" OF THE AMERICAN WELDING SOCIETY.

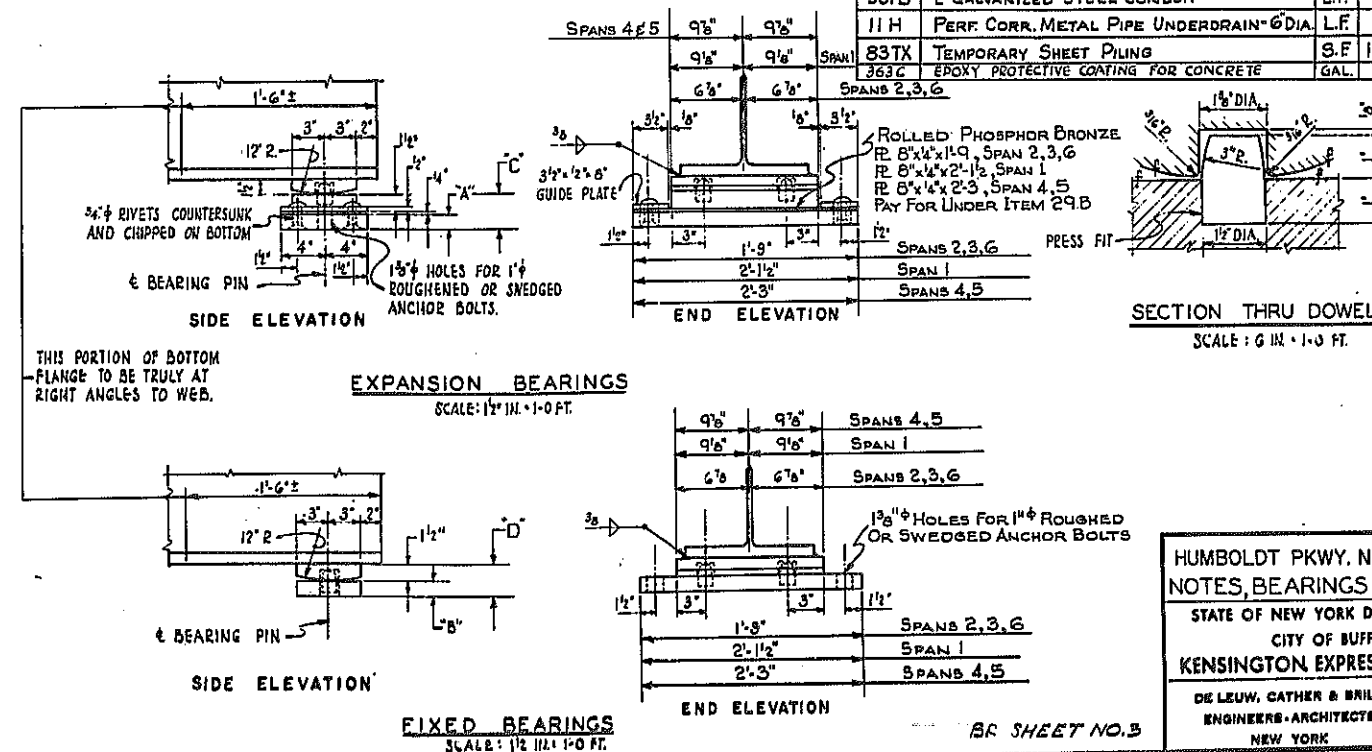
FORMS : APPROVED CORRUGATED METAL FORM MAY BE USED FOR THE STRUCTURAL SLAB, PROVIDED THE COVER FOR THE REINFORCING STEEL IS MAINTAINED ABOVE THE CREST OF THE CORRUGATION.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

TABLE OF BEARINGS									
LOCATION	STRINGER	№	A	B	C	D	1" ANCHOR BOLTS	NO.	LENGTH
EXPANSION BEARINGS									
PIER No.1	1A-1G	7	1 3/8"		4 5/8"		14	1'-2"	
PIER No.2	2A-2G	7	1 3/8"		4 5/8"		14	1'-2"	
PIER No.3	3A-3G	7	1 3/8"		4 5/8"		14	1'-2"	
PIER No.4	4A-4G	7	1 3/8"		4 5/8"		14	1'-2"	
PIER No.5	5A-5G	7	1 3/8"		4 5/8"		14	1'-2"	
NORTH ABUT.	6A-6G	7	1 3/8"		4 5/8"		14	1'-2"	
FIXED BEARINGS									
SOUTH ABUT.	1A-1G			1 3/8"		3 3/8"	14	1'-2"	
PIER No.1	2A-2G			3 3/8"		5 3/8"	14	1'-4"	
PIER No.2	3A-3E			3 3/8"		4 7/8"	10	1'-4"	
PIER No.2	3F			3"		4 1/2"	2	1'-3"	
PIER No.2	3G			2 5/8"		4 5/8"	2	1'-3"	
PIER No.3	4A-4G			1 3/8"		3 3/8"	14	1'-2"	
PIER No.4	5A-5G			3 1/2"		5"	14	1'-4"	
PIER No.5	6A-6G			1 3/8"		3 3/8"	14	1'-2"	

ESTIMATE OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	TOTAL NET	TOTAL ROUNDED
2EF-B	SELECTED FILL	C.Y.	1,862	2,050
5	TRENCH, CULVERT & BRIDGE Exc.	C.Y.	604	672
13B	CAST IRON PIPE 6" DIA.	L.F.	20	21
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	2,640	2,750
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	1,379	1,420
20Y	CLASS I CONCRETE	C.Y.	279	290
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	57	60
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	201,430	287,000
28B	SHEAR CONNECTORS	L.F.	3,202	3,300
29B	STRUCTURAL STEEL	Lb.	905,603	932,700
30S	MISCELLANEOUS METAL	Lb.	540	560
30W	MISC. METAL - W. I. PIPE 6" DIA.	Lb.	3,802	3,900
37BA-1	METAL RAILING - TYPE B-1 RAIL	L.F.	989	1,020
52BX	ASPHALT CONCRETE TYPE 2 B (2 1/2)	TON	273	290
61	BITUMINOUS MATERIAL	GAL	194	200
79BX	CONCRETE BLOCK PAVING	S.Y.	162	170
95XCIO	10" STONE CURB (BRIDGE)	L.F.	989	1,010
121A	TOPSOIL FURNISHED & PLACED	C.Y.	102	107
123	SEEDING	ACRES	0.21	0.22
124	SODDING	S.Y.	98	100
301C	2 1/2 GALVANIZED STEEL CONDUIT	L.F.	15	15
303CGX	FURNISH & INSTALL LIGHT STDS. 30" HT. 6" ARM	Ea.	3	3
352XA	RUBBER JOINT MATERIAL	GAL.	19	21
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	237	250
9VC	SEWER PIPE VITRIFIED	L.F.	57	60
301B	2" GALVANIZED STEEL CONDUIT	L.F.	518	530
11H	PERF. CORR. METAL PIPE UNDERDRAIN 6" DIA.	L.F.	168	175
83TX	TEMPORARY SHEET PILING	S.F.	1507	1580
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	24	25



HUMBOLDT PKWY. N.B. OVER EXPW.
NOTES, BEARINGS AND ESTIMATE

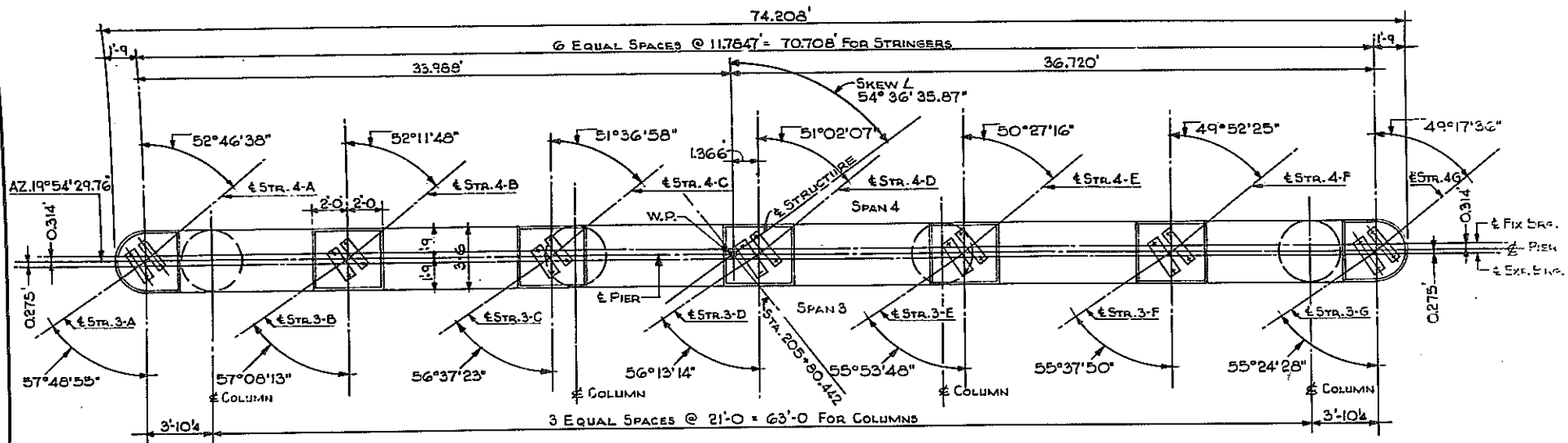
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	J.S. C.E. J.S.
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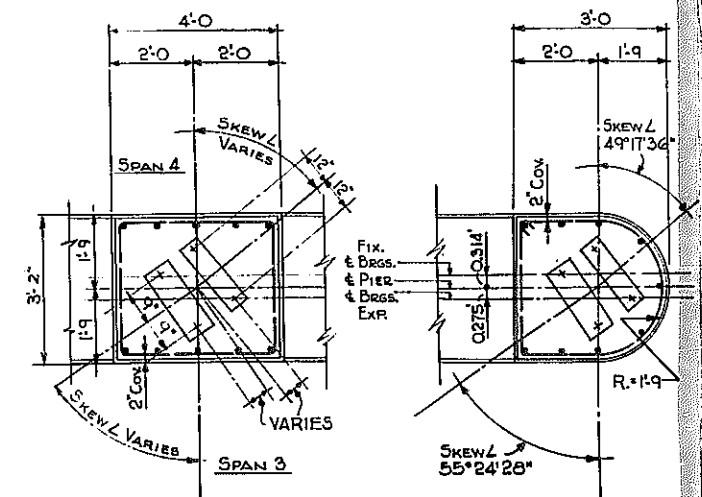
BR SHEET NO. 3

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	117	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

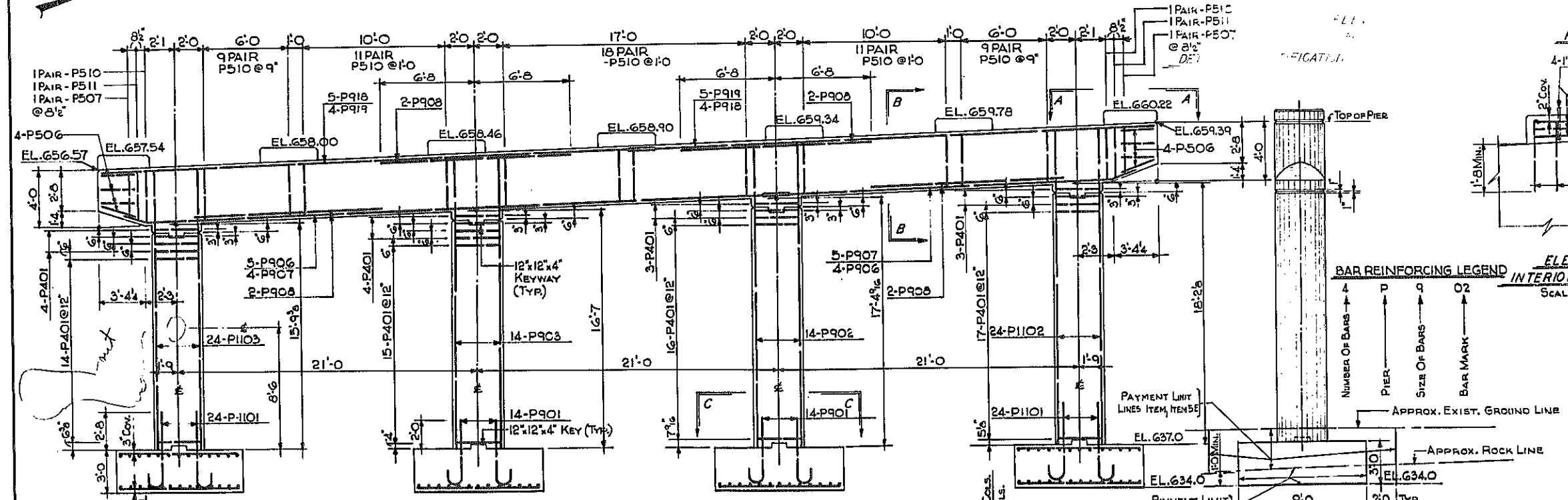


PLAN SCALE: 1/4" = 1 FT.



PLAN

PLAN



BAR REINFORCING LEGEND

↑	4	P	0	02
↑	↑	↑	↑	↑
NUMBER OF BARS	PIER	SIZE OF BARS	BAR MARK	

NOTES

FOR DESIGN PURPOSES, SOIL PRESSURE (ROCK) DOES NOT EXCEED 5 TONS PER SQ. FT.

CONCRETE IN THE PIER COLUMNS & CAPS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

CONCRETE IN THE PIER FOOTING, SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

ANCHOR BOLTS FOR BEARINGS SHALL BE 1" Ø ROUGHENED OR SWEAGED BOLTS. SET 10" INTO MASONRY.

SEE SHEET NO. 3 FOR ANCHOR BOLT LENGTHS.

PLAN A-A SCALE: 1/4" = 1 FT.

SECTION B-B SCALE: 1/4" = 1 FT.

SECTION G-G SCALE: 1/4" = 1 FT.

PLAN OF FOOTING SCALE: 1/4" = 1 FT.

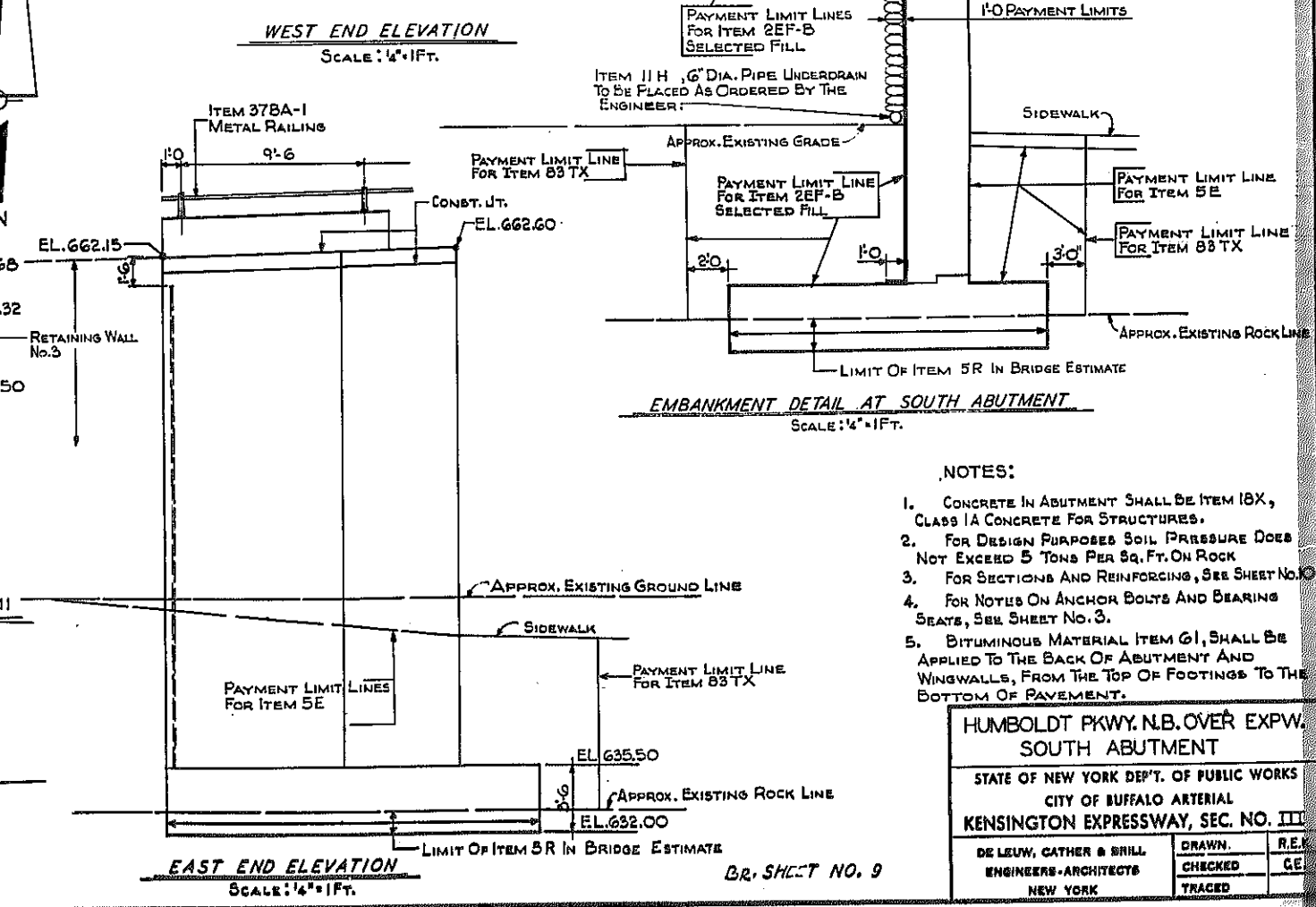
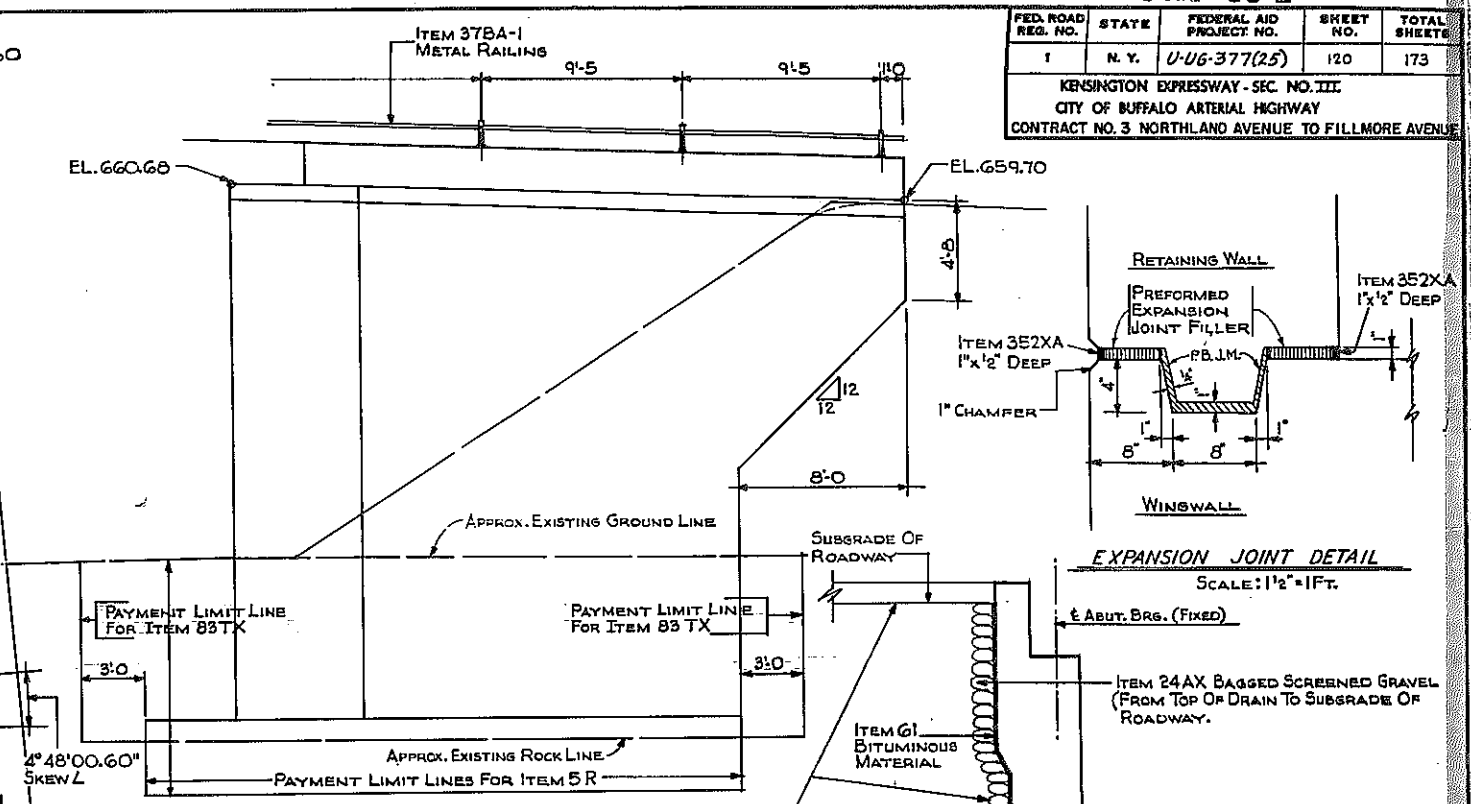
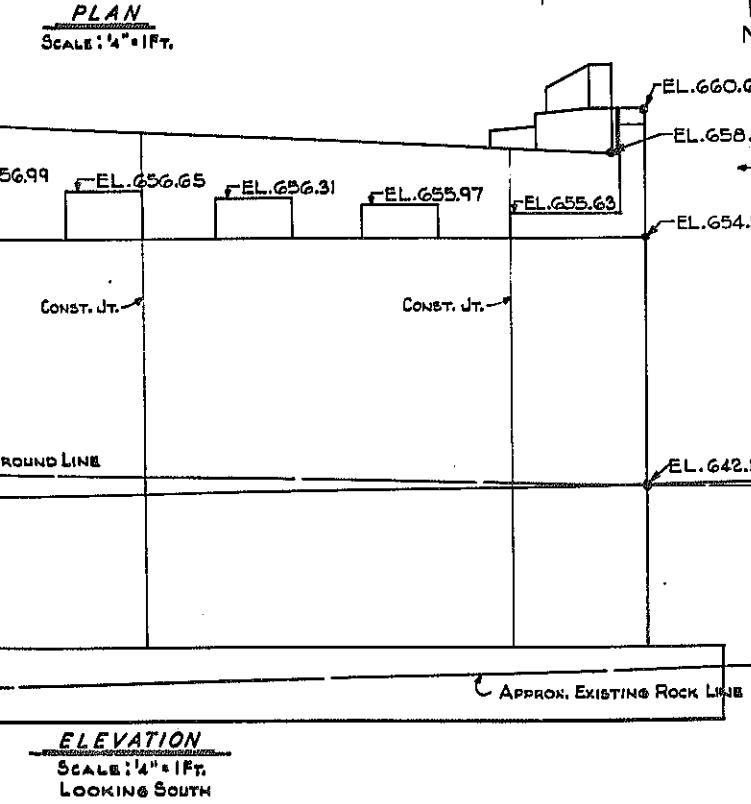
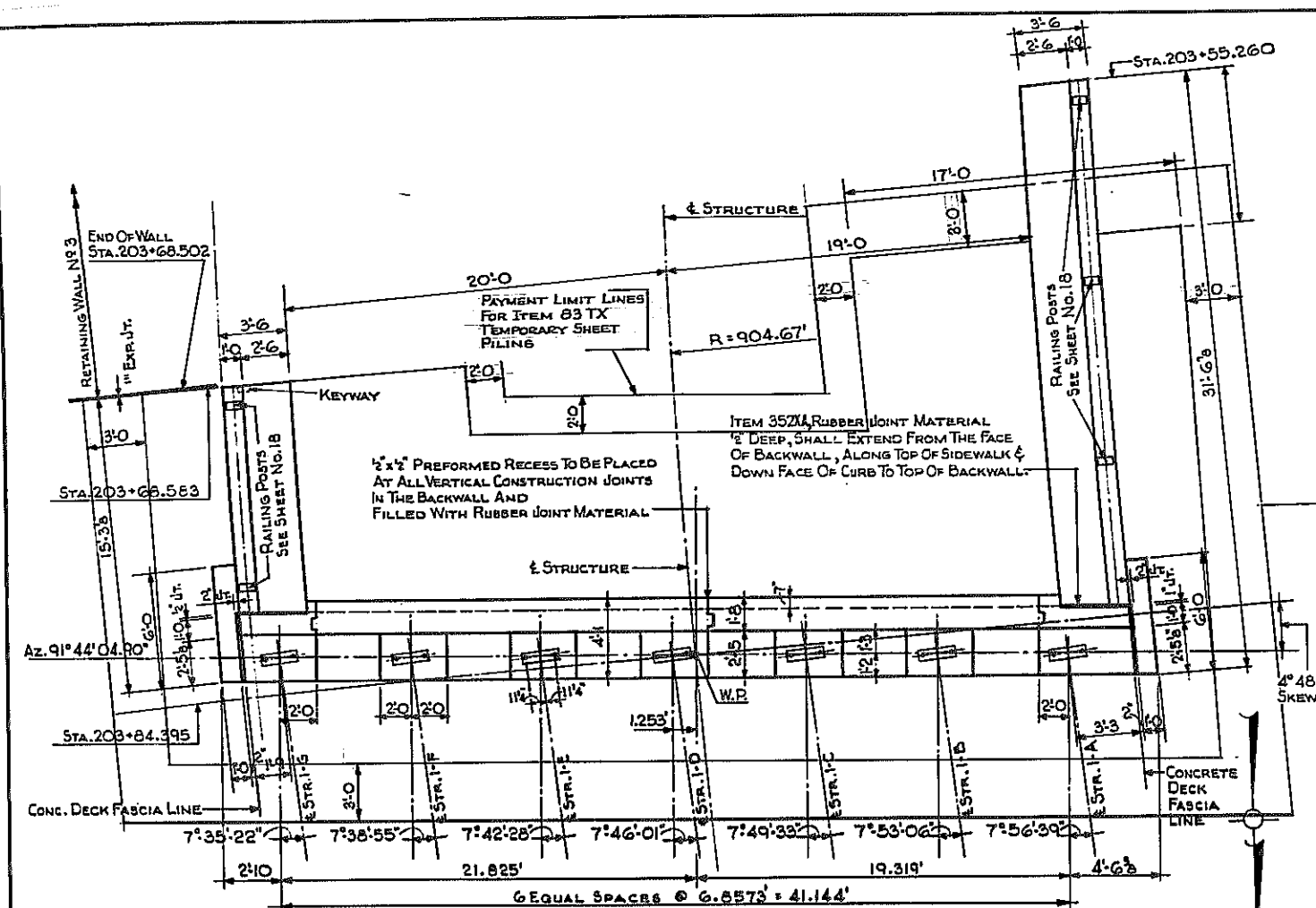
HUMBOLDT PKWY. N.B. OVER EXPW. PIER NO. 3

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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	120	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- NOTES:
1. CONCRETE IN ABUTMENT SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.
 2. FOR DESIGN PURPOSES SOIL PRESSURE DOES NOT EXCEED 5 TONS PER SQ. FT. ON ROCK
 3. FOR SECTIONS AND REINFORCING, SEE SHEET NO. 10
 4. FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS, SEE SHEET NO. 3.
 5. BITUMINOUS MATERIAL ITEM G1, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE BOTTOM OF PAVEMENT.

HUMBOLDT PKWY. N.B. OVER EXPW. SOUTH ABUTMENT

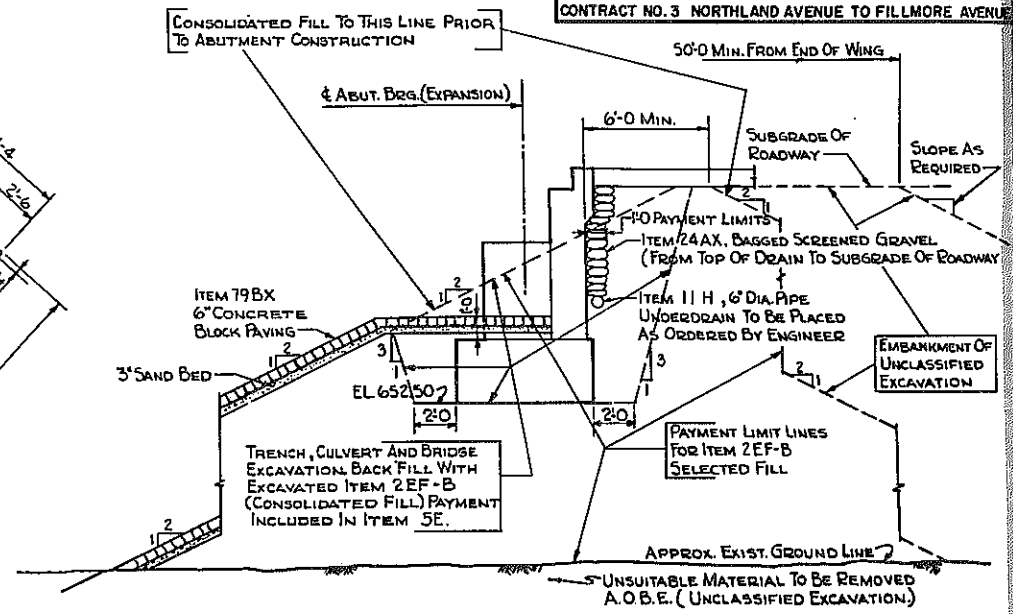
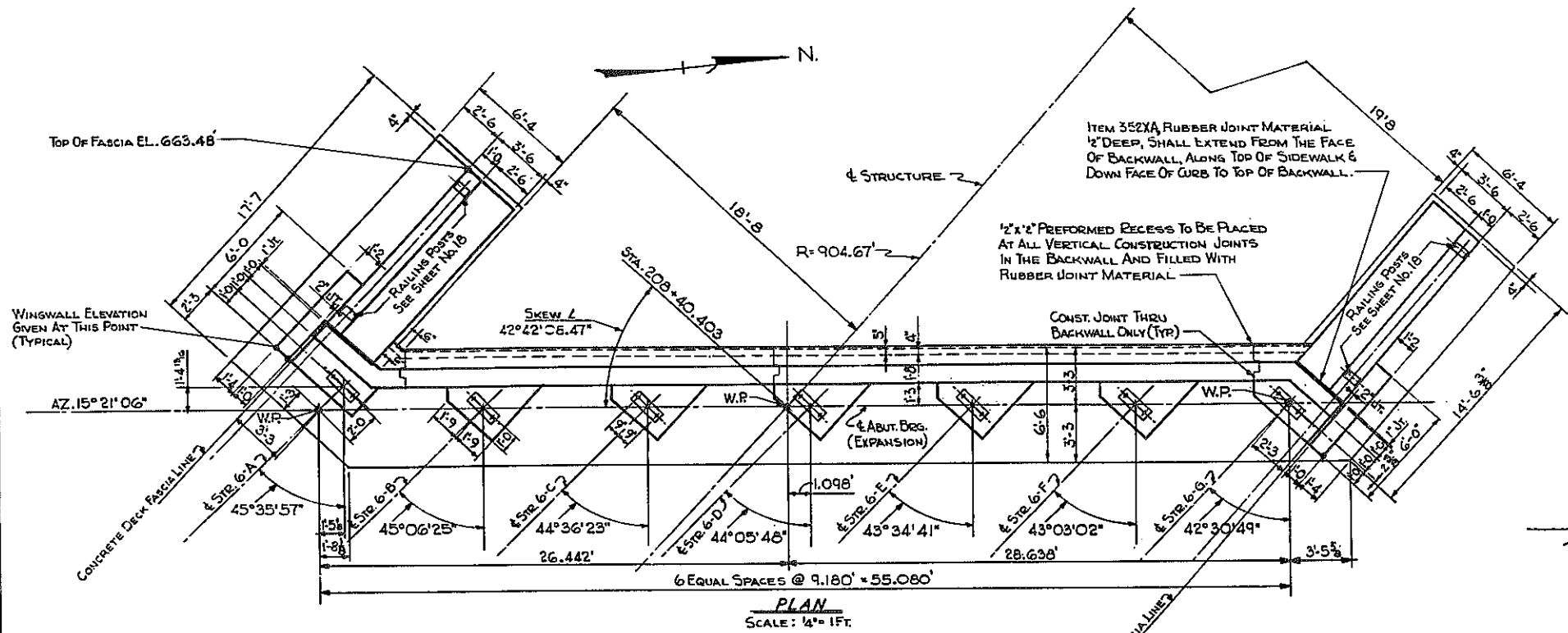
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

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ENGINEERS-ARCHITECTS	CHECKED	CE
NEW YORK	TRACED	

BR. SHEET NO. 9

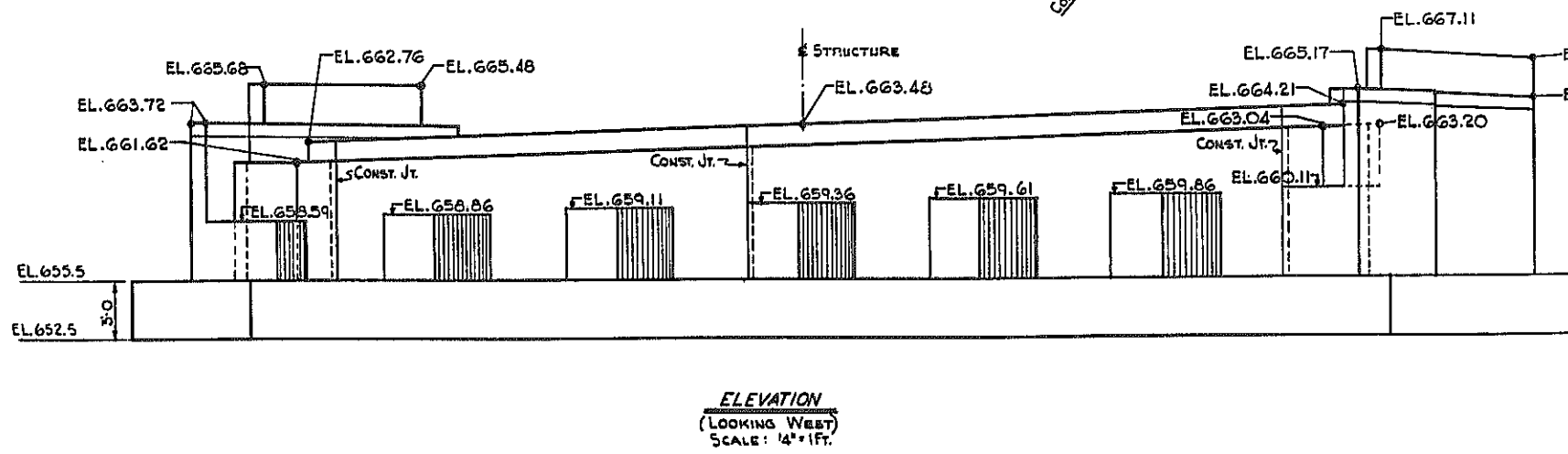
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	122	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

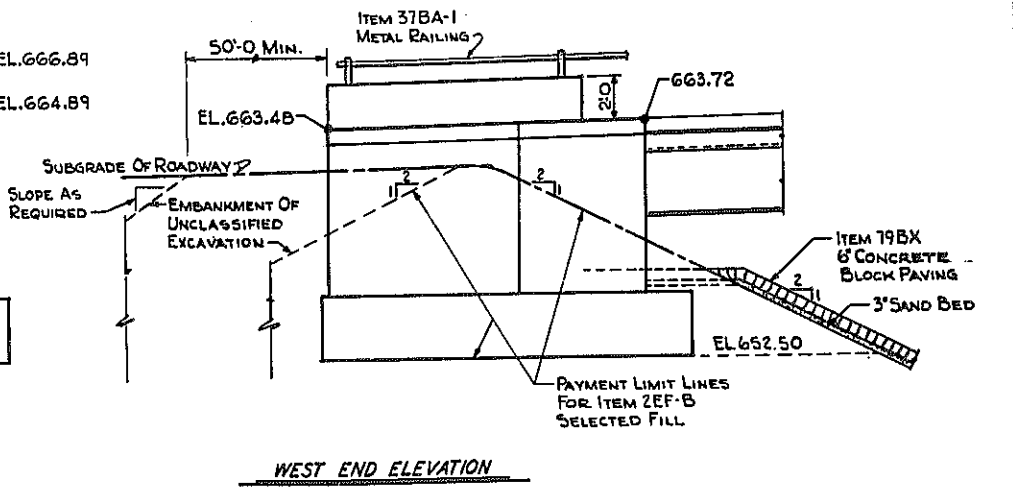


EMBANKMENT DETAIL AT NORTH ABUTMENT
SCALE: 1/4" = 1 FT.

NOTE: AREAS WITHIN PAYMENT LIMIT LINES IN BACK OF WINGWALLS SHALL BE BACKFILLED WITH ITEM 2EF-B, SELECTED FILL. PAYMENT INCLUDED IN ITEMS 5E AND 5R.



ELEVATION
(LOOKING WEST)
SCALE: 1/4" = 1 FT.



WEST END ELEVATION

- NOTES:
- CONCRETE IN ABUTMENT SHALL BE ITEM 20Y CLASS I CONCRETE.
 - CONCRETE IN HEADER AND WINGWALL FASCIAE AND PARAPETS SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES.
 - FOR DESIGN PURPOSES SOIL PRESSURE DOES NOT EXCEED 1.5 TONS PER SQ. FT.
 - FOR SECTIONS AND REINFORCING, SEE SHEET NO. 12
 - FOR NOTES ON ANCHOR BOLTS AND BEARING SEATS SEE SHEET NO. 3
 - DITUMINOUS MATERIAL ITEM 61, SHALL BE APPLIED TO THE BACK OF ABUTMENT AND WINGWALLS, FROM THE TOP OF FOOTINGS TO THE TOP OF PAVEMENT.

D.R. SHEET NO. 11

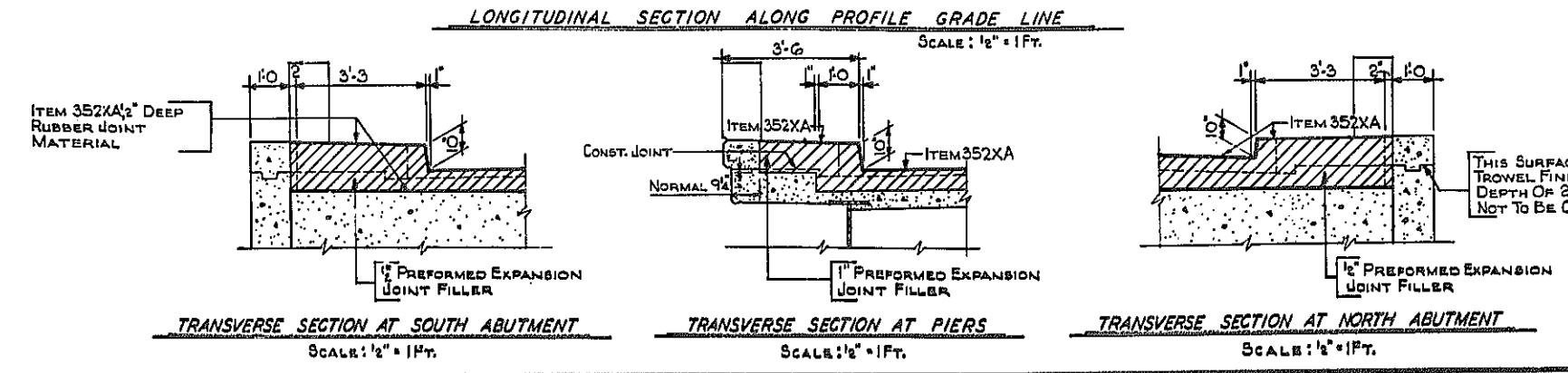
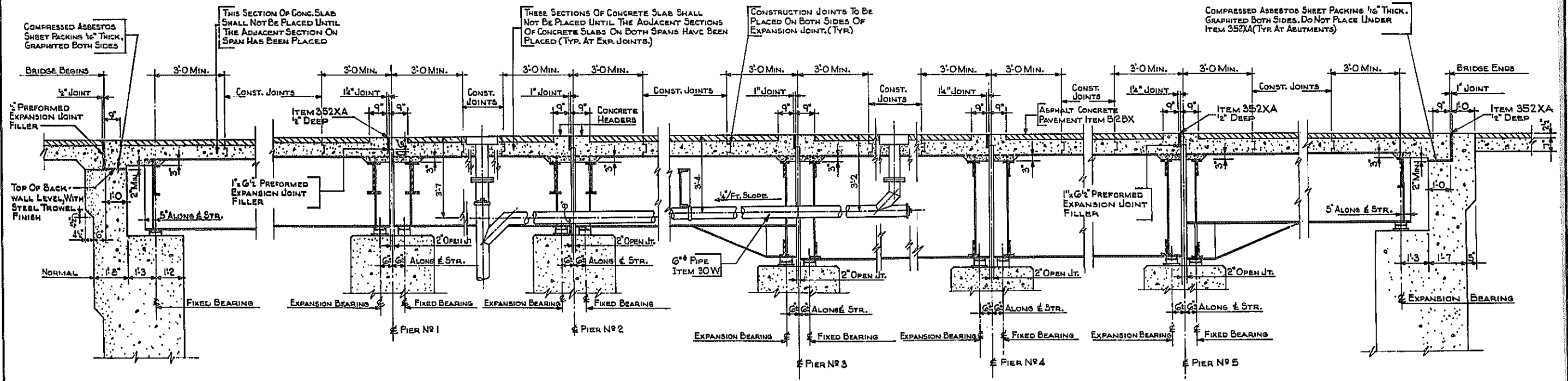
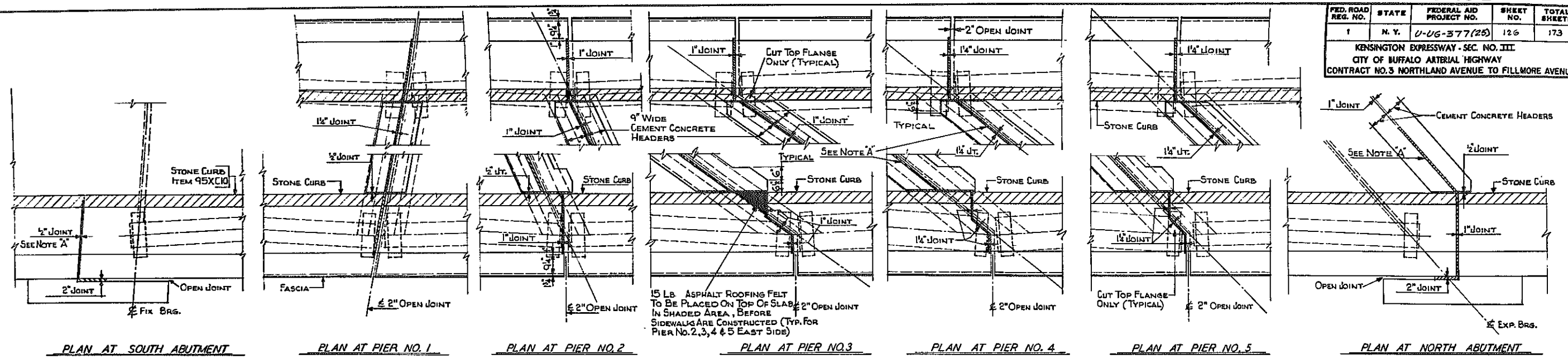
HUMBOLDT PKWY. NB. OVER EXPW.
NORTH ABUTMENT

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KENSINGTON EXPRESSWAY, SEC. NO. III

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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	126	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



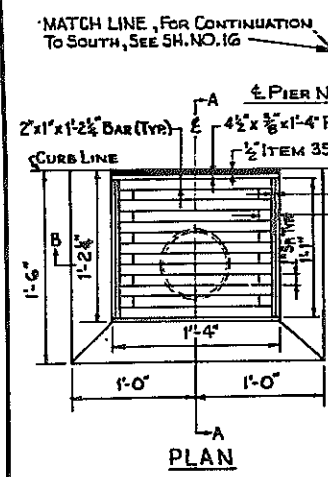
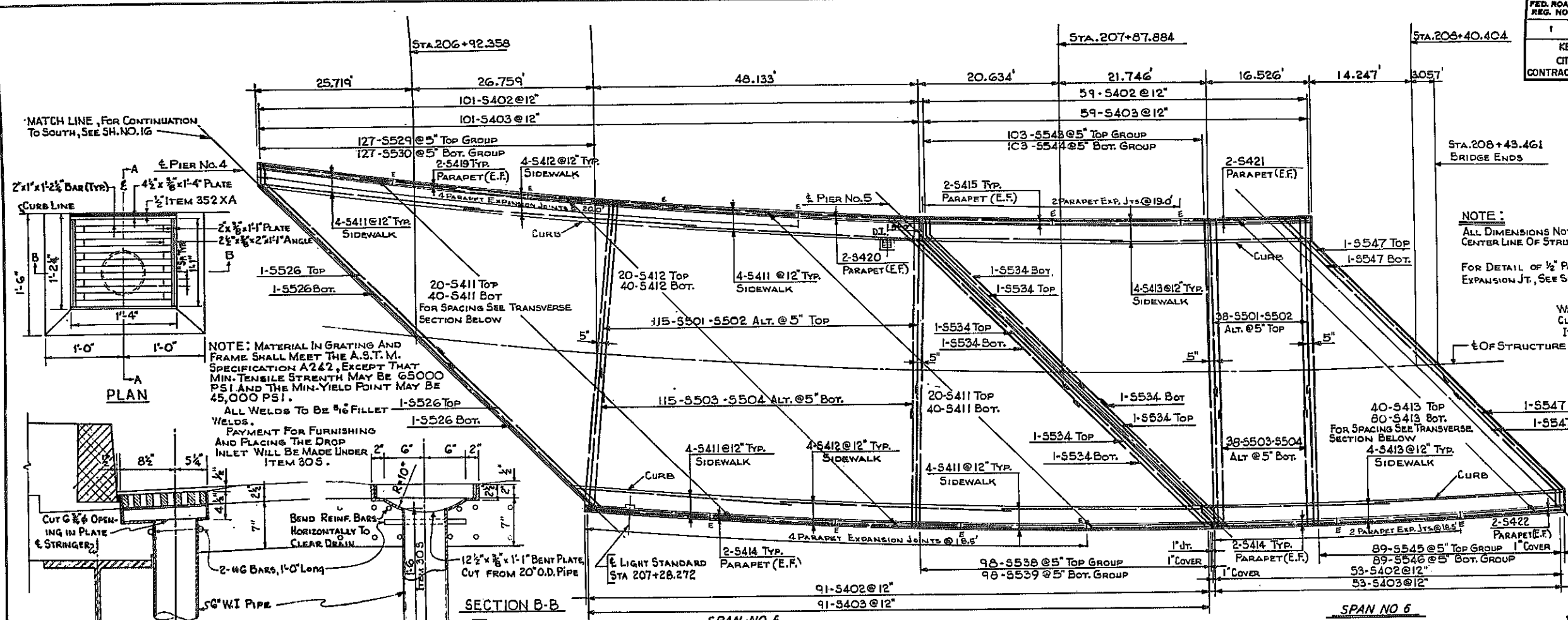
NOTE: 'A' PREFORMED EXPANSION JOINT FILLER, TOPPED WITH ITEM 352XA, 1/2\"/>

THIS SURFACE TO BE TROWEL FINISHED FOR A DEPTH OF 2\"/>

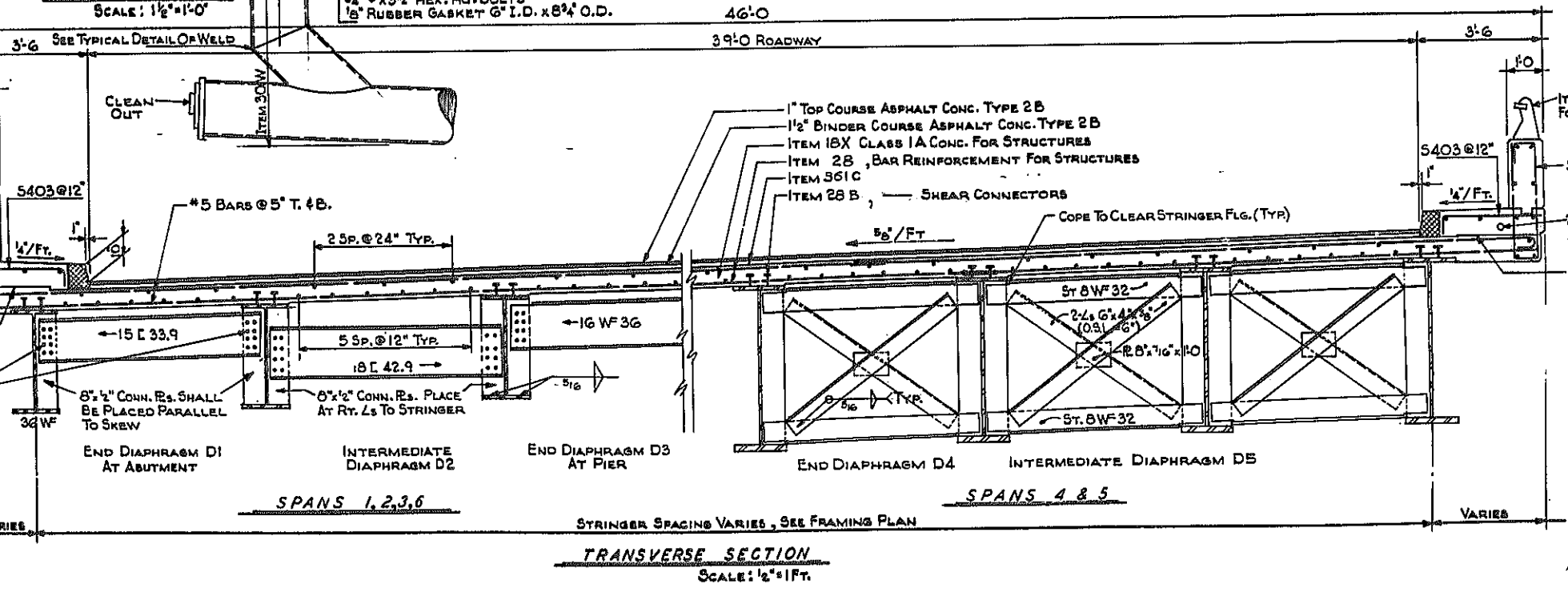
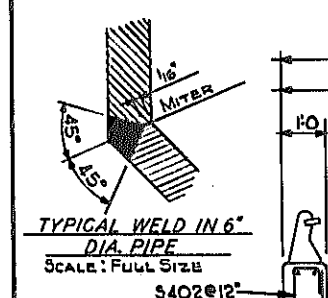
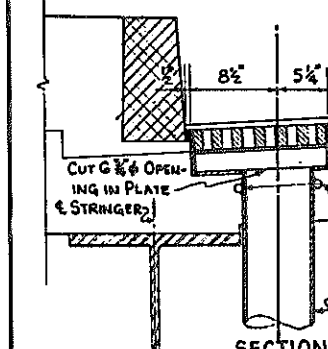
HUMBOLDT PKWY. N. B. OVER EXPW. LONGITUDINAL SECTION & DETAILS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS		
CITY OF BUFFALO ARTERIAL		
KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHY & BRILL	DRAWN	R.E.K.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	128	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



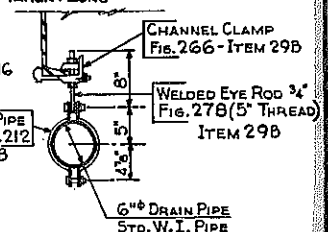
NOTE: MATERIAL IN GRATING AND FRAME SHALL MEET THE A.S.T.M. SPECIFICATION A242, EXCEPT THAT MIN. TENSILE STRENGTH MAY BE 65000 PSI AND THE MIN. YIELD POINT MAY BE 45,000 PSI.
ALL WELDS TO BE 1/16" FILLET WELDS.
PAYMENT FOR FURNISHING AND PLACING THE DROP INLET WILL BE MADE UNDER ITEM 305.



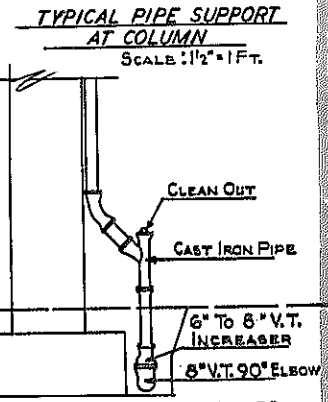
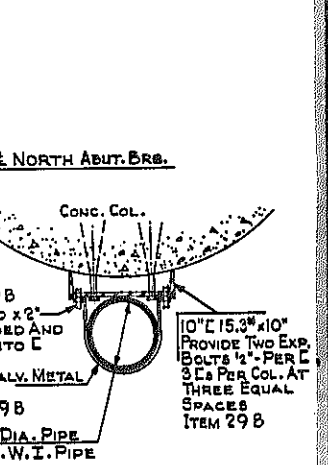
BAR REINFORCING LEGEND

NUMBER OF BARS	SLAB	BAR SIZE	BAR MARK
48	S	5	01

NOTE: ALL DIMENSIONS NOTED ARE TAKEN ALONG CENTER LINE OF STRUCTURE.
FOR DETAIL OF 1/2" PARAPET EXPANSION JT., SEE SHEET NO. 16



ITEM 29B
1/2" STUD x 2" THREADED AND WELDED INTO C
3"x6" GALV. METAL STRAP ITEM 29B
6" DIA. PIPE STD. W. I. PIPE



HUMBOLDT PKWY. N.B. OVER EXPW. DECK PLAN AND SECTION

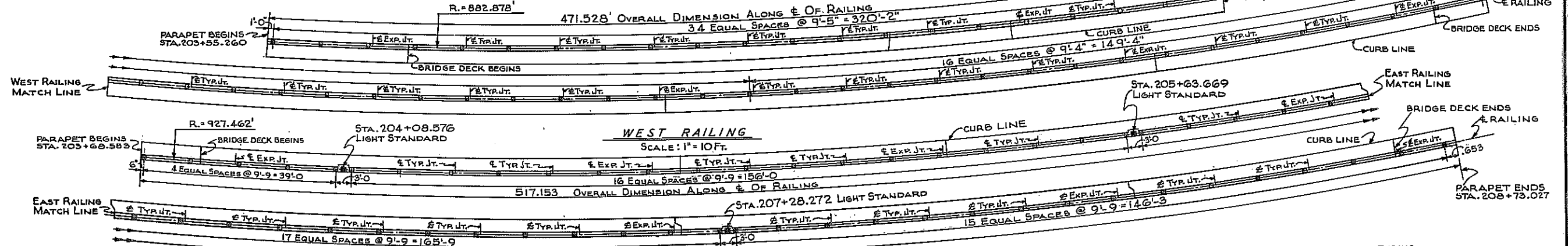
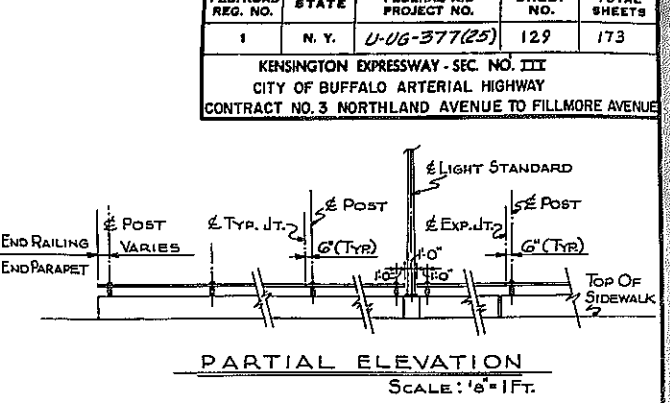
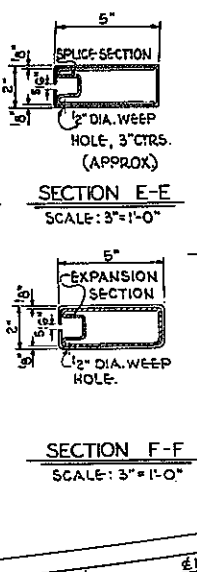
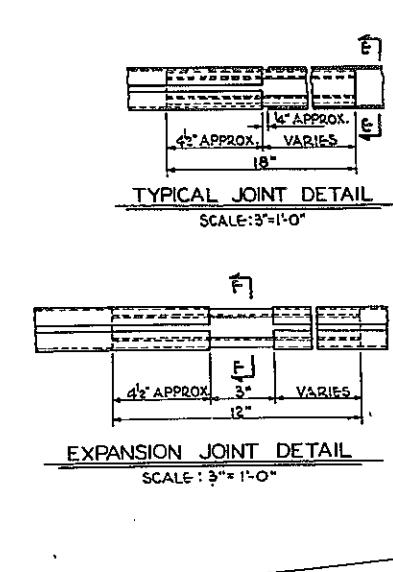
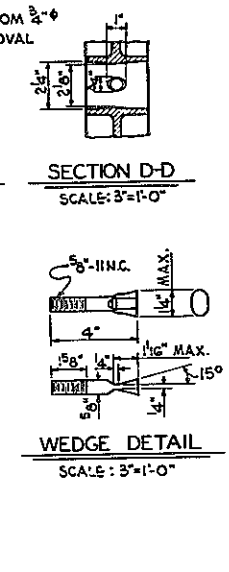
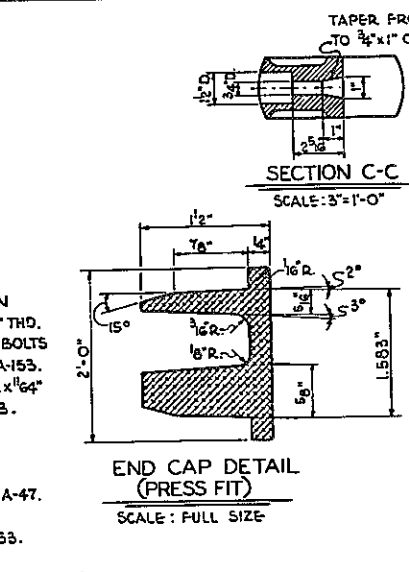
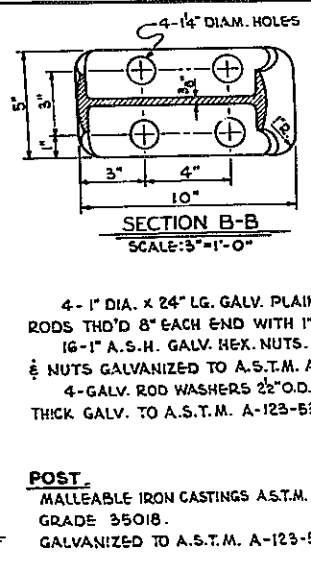
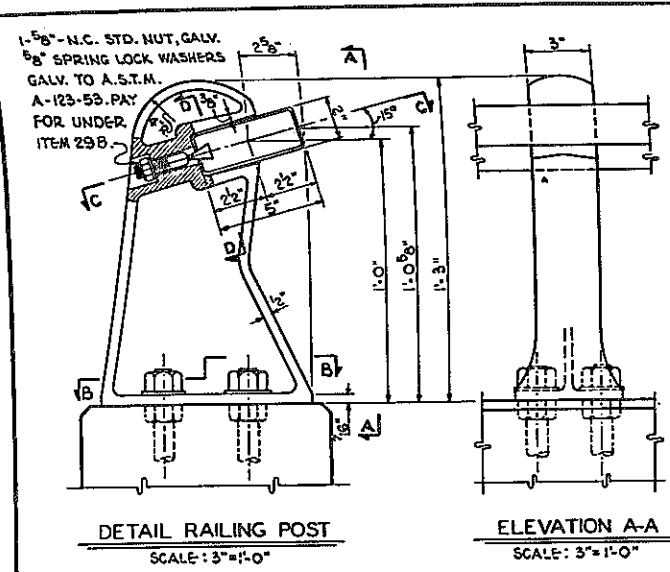
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
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KENSINGTON EXPRESSWAY, SEC. NO. III

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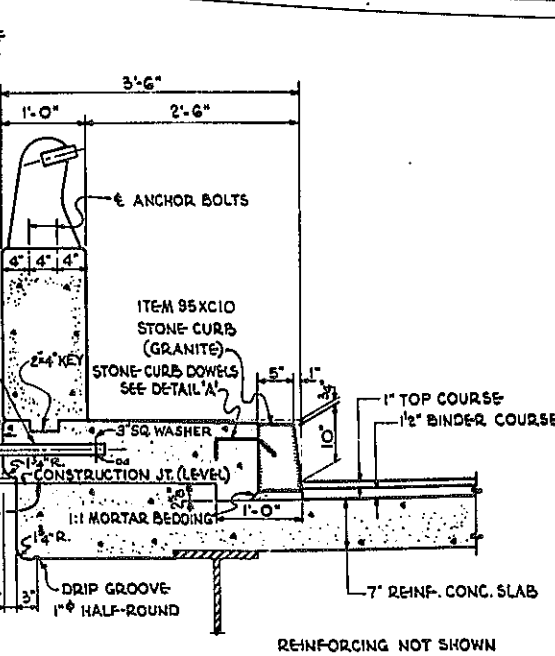
Re SHEET NO. 17

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	129	173

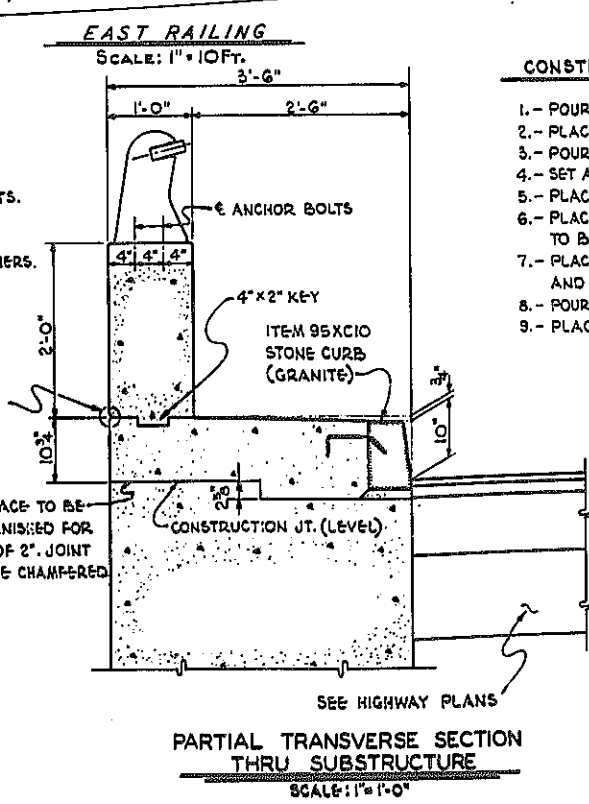
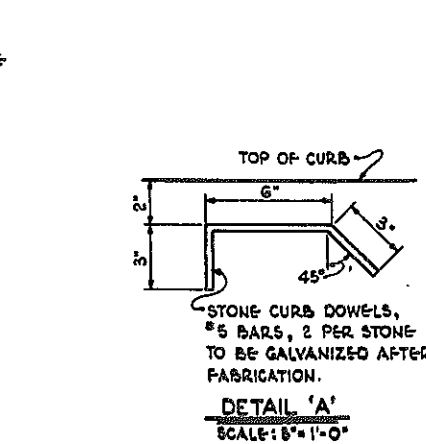
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



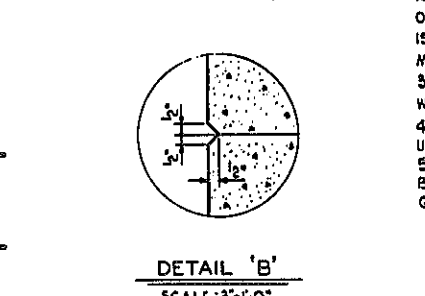
NOTE:
ANCHOR BOLTS SHALL BE CUT OFF 1/8" ABOVE NUT AND PEENED, THE CUT TOPS OF ANCHOR BOLTS SHALL BE PAINTED AS REQUIRED BY THE SPECIFICATIONS FOR M18A EXCEPT THAT THE FINAL TWO COATS SHALL BE ALUMINUM PAINT COMPLYING WITH THE SPECIFICATIONS FOR M25.



- CONSTRUCTION PROCEDURE FOR SUPERSTRUCTURE**
- 1.- POUR SLAB & BROOM FINISH TOP OF SLAB BETWEEN CURBS.
 - 2.- PLACE ITEM 95XC10 STONE CURB (GRANITE).
 - 3.- POUR SIDEWALK TO PROPER LINE AND GRADE.
 - 4.- SET ANCHOR BOLTS BY MEANS OF A TEMPLATE.
 - 5.- PLACE AND ADJUST LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 - 6.- PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 - 7.- PLACE UPPER NUTS ON ANCHOR BOLTS; TIGHTEN DOWN ON WASHERS.
 - 8.- POUR PARAPET TO PROPER LINE AND GRADE.
 - 9.- BROOMED SURFACE OF SLAB TO BE GIVEN A WATERPROOFING, ITEM 361C.
 - 10.- PLACE ROADWAY PAVEMENT.



- CONSTRUCTION PROCEDURE FOR SUBSTRUCTURE**
- 1.- POUR WALL UP TO CONSTRUCTION JOINT.
 - 2.- PLACE STONE CURB (GRANITE).
 - 3.- POUR WALL ABOVE CONSTRUCTION JOINT.
 - 4.- SET ANCHOR BOLTS BY MEANS OF A TEMPLATE.
 - 5.- PLACE LOWER NUTS ON UPPER END OF ANCHOR BOLTS.
 - 6.- PLACE RAILING ON LOWER NUTS AND ADJUST TO BRING RAILING TO LINE AND GRADE.
 - 7.- PLACE UPPER NUTS ON ANCHOR BOLTS AND TIGHTEN DOWN ON WASHERS.
 - 8.- POUR PARAPET TO PROPER LINE AND GRADE.
 - 9.- PLACE ROADWAY PAVEMENT.



- TUBING**
5"x2" STEEL RAIL A.S.T.M. A-245, CLASS C, GALVANIZED TO A.S.T.M. SPEC. A-123-53.
- RAILING NOTES:**
- 1.- ALL RAILINGS ARE TO BE FABRICATED AND ERECTED SO THAT THE RAILS ARE CONCENTRIC TO THE TOP OF THE PARAPET AND SO THAT THE POSTS ARE TRULY VERTICAL.
 - 2.- SINCE THE FINISHED RAILINGS MUST MEET ALL REQUIREMENTS OF FIT, ALIGNMENT, GRADE AND VERTICALITY OF POST TO THE FULL SATISFACTION OF THE ENGINEER IT IS SUGGESTED THAT COMPLETE FIELD MEASUREMENTS BE MADE BEFORE ANY SHOP FABRICATION IS PERFORMED.
 - 3.- TUBULAR RAILS, MALLEABLE IRON POST, INSERTS, WEDGES AND END CAPS WILL BE PAID FOR UNDER ITEM 378A-1.
 - 4.- ANCHOR BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29 B.
 - 5.- ALL SHIMS REQUIRED TO ALIGN THE POSTS SHALL BE PAID FOR UNDER ITEM 378A-1 ALL SHIMS TO BE GALVANIZED TO A.S.T.M. A-153.

HUMBOLDT PKWY. N. B. OVER EXPW.
RAILING LAYOUT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	R.E.K.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	R.E.K.

BAR LIST

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	150	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				

ABUTMENTS (2)									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
NORTH ABUTMENT - FOOTING									
A401	#4	6'-0	STR.	68				TRANSVERSE BARS	
A402		5'-10		25				TRANSVERSE BARS WINGWALL	
A403		6'-2		6				TRANSVERSE BARS	
A404		6'-4		4				TRANSVERSE BARS	
A405		7'-0		6				TRANSVERSE BARS	
A801	#8	16'-4		16				LONG. BARS E. WW. VARIES 14'-0 TO 19'-2	
A802	#8	17'-1		16				LONGITUDINAL BARS	
A901	#9	52'-3		52				LONG. BARS VARIES 29'-5 TO 35'-1	
NORTH ABUTMENT - WALL									
A601	#6	8'-9	STR.	64				VERT. BARS VARIES 7'-10 TO 9'-4 - ABUT.	
A602		9'-2		5				VERTICAL BARS W. WW.	
A603		10'-4		5				" " E. WW.	
A604		9'-2		5				" " W. WW.	
A605		10'-4		5				" " E. WW.	
A606		10'-1		6				" " W. WW.	
A607		11'-6		6				" " E. WW.	
A608		4'-11		6				" " PEDESTAL	
A609		5'-5		9				" " " "	
A610		5'-11		9				" " " "	
A611		6'-5		3				" " " "	
A612		30'-2		20				HORIZONTAL BARS ABUTMENT	
A613		11'-6	I	25	3'-2	3'-0	5'-6	HORIZONTAL BARS PEDESTAL	
A614		9'-0	II	5	3'-2	3'-0		" " " "	
A615		14'-8	III	6	9'-4	2'-0	3'-4	" " W. WW.	
A616		14'-10	III	6	9'-6	2'-0	3'-4	" " E. WW.	
A617		11'-0	II	5	3'-2	4'-0		" " PEDESTAL	
A618		11'-10	STR.	10				" " E. & W. WW.	
A619		5'-8	STR.	10				" " " "	
A620		4'-6	II	8	8'	2'-0		U BARS CURTAIN WALL	
A621		7'-0	IV	26	8'	2'-6		U BARS PARAPET E. & W. WW.	
A622		5'-2	II	29	8'	2'-4		U BARS CONC. HEADER	
A623		4'-5	V	26	2'-8	1'-10		CURS BARS	
A624		12'-0	STR.	4				HORIZONTAL BARS E. & W. PARAPET	
A625		11'-10	STR.	6				" " " " WW.	
SOUTH ABUTMENT - FOOTING									
A500	#5	24'-2	STR.	30				LONGITUDINAL BARS ABUTMENT	
A501		12'-0		14				" " W. WW.	
A502		27'-9		14				" " ABUTMENT	
A503		19'-7		3				" " W. WW.	
A504		23'-1		4				" " " "	
A505		11'-4		3				" " E. WW.	
A506		14'-10		4				" " " "	
A910	#9	15'-4		25				TRANSVERSE ABUTMENT	
A911	#9	14'-0		96				TRANSVERSE ABUT. & WW.	
A1100	#11	15'-0	STR.	159	7'-1	1'-7	6'-4	DOWELS	
SOUTH ABUTMENT - WALL									
A411	#4	27'-9	STR.	3				LONGITUDINAL BARS W. WW.	
A412		11'-8		3				" " E. WW.	
A413		27'-9		2				" " PARAPET W. WW.	
A414		11'-8		2				" " E. WW.	
A415		23'-5		4				" " ABUT. BACKWALL	
A650	#6	18'-6		24				VERTICAL BARS ABUTMENT	
A651		24'-2		11				" " W. WW.	
A652		25'-4		7				" " E. WW.	
A653		25'-0		6				" " W. CURTAIN WALL	
A654		26'-4		6				" " E. " "	
A655		7'-8		8				" " W. WW. VAR. 10'-6 TO 14'-6	
A656		24'-9		56				HORIZONTAL BARS ABUTMENT	
A657		23'-2	III	4	4'-2	1'-0	18'-0	" " W. WW.	
A658		14'-8	III	6	4'-2	1'-0	9'-6	" " E. WW.	
A659		5'-8	STR.	27				" " E. & W. CURTAIN WALL	
A660		8'-8		14				" " W. WW.	
A661		5'-0		21				VERTICAL BARS PEDESTALS	
A662		11'-0	II	26	3'-8	3'-9		HORIZONTAL BARS PEDESTALS	
A663		5'-4	V	22	2'-8	2'-9		SIDEWALK BARS E. & W. WW.	
A664		7'-10	II	22	8	3'-8		U BARS PARAPET E. & W. WW.	
A665		4'-6	II	8	8	2'-0		U BARS CURTAIN WALLS	
A666		16'-10	II	24	1'-4	7'-10		U BARS CONC. HEADER	
A667		12'-0	STR.	4				LONGITUDINAL BARS W. WW.	
A1101	#11	10'-0		78				VERTICAL BARS ABUT. & WW.	
A1102		14'-6		99				" " " "	
A1103		24'-4		11				" " W. WW.	
A1104		25'-6		17				" " E. WW. & BR. SEAT	
A1105		18'-10		24				" " ABUTMENT	
A668	#6	11'-6	STR.	37				HORIZONTAL BARS - E. WW.	
A669	#6	15'-8	STR.	35				" " W. WW.	
A670	#6	3'-8	STR.	28				TIE BARS VARIES 29'-5 TO 35'-1	

PIERS (5)															
MARK	SIZE	LEN.	TYPE	NUMBER					A	B	C	DESCRIPTION			
				PIER 1	PIER 2	PIER 3	PIER 4	PIER 5					TOTAL		
FOOTINGS															
P601	#6	7'-0	STR.	60	60				120			HORIZONTAL BARS			
P602	#6	8'-6	STR.			156			156	312		" "			
P603	#6	9'-6	STR.				112		112			" "			
P901	#9	5'-11	X			28			28	4'-8	1'-3	11'-4	DOWELS - INT. COLS.		
P1001	#10	6'-3	X	66	66				132	4'-10	1'-5	1'-0 1/2	" "		
P1002	#10	6'-9	X				88		88	5'-4	1'-5	1'-0 1/2	" "		
P1101	#11	6'-8	X				48		48	9'-6	5'-1	1'-7	1'-2	" - EXT. COLS.	
COLUMNS & BEAMS															
P401	#4	10'-9	VII	55	57	76	140	62	390	3'-2				COLUMN TIES	
P402	#4	3'-0	XI	68	68	68	68	68	340	2'-6	6"			CAP PEDESTALS - DOWELS	
P501	#5	41'-9	STR.	4					4					HORIZONTAL BARS SIDES	
P502		45'-0			4				4					" " " "	
P503		35'-10				8			8					" " " "	
P504		33'-10					8		8					" " " "	
P505		30'-0						8	8					" " " "	
P506		7'-1	IX	8	8	8	8	8	40	1'-1	4'-11	1'-1		" " " FASCIA ENDS	
P507		11'-1	XIV	84	76	4			164	3'-2	2'-2 1/2	4'-2		CAP STIRRUPS	
P508		10'-9	XIV		4				4	3'-0	2'-2 1/2	4'-2		" " " "	
P509		10'-2	XIV		4				4	2'-8 1/2	2'-2 1/2	4'-2		" " " "	
P510		12'-1	XIV			120	120	92	332	3'-8	2'-2 1/2	4'-2		" " " "	
P511		11'-8	XIV			4			4	3'-5 1/2	2'-2 1/2	4'-2		" " " "	
P512		11'-11	XIV				4		4	3'-7	2'-2 1/2	4'-2		" " " "	
P513		11'-5	XIV				4		4	3'-4	2'-2 1/2	4'-2		" " " "	
P514		10'-11	XIV				4		4	3'-1	2'-2 1/2	4'-2		" " " "	
P515		11'-6	XIV				4		4	3'-4 1/2	2'-2 1/2	4'-2		" " " "	
P604	#6	12'-0	XII	6	6	6	6	6	30	1'-10	3'-2	4'-10 1/2		HORIZONTAL BARS FASCIA PEDESTALS	
P605	#6	15'-4	XIII	15	15	15	15	15	75	3'-8	3'-2	1'-0 1/2		" " " INT. PEDESTALS	
P801	#8	23'-5	VIII	8					8	1'-2 1/2	20'-7	6'-2		HORIZONTAL BARS BOTTOM	
P802		17'-6	STR.	12					12					" " " "	
P803		23'-7	VIII		8				8	1'-7	22'-0	7'-2		" " " "	
P804		37'-6	STR.		5				5					" " " "	
P902	#9	21'-0				14			14					VERTICAL BARS INT. COL.	
P903		20'-4				14			14					" " " "	
P904		17'-2						14	14					" " " "	
P905		16'-8						14	14					" " " "	
P906		47'-0	VIII			9			9	1'-9	45'-3	9'		HORIZONTAL BARS BOTTOM	
P907		26'-0	VIII			9			9	1'-9	24'-3	9'		" " " "	
P908		13'-4	STR.			8			8					" " " " & TOP	
P909		44'-4	VIII			7			7	1'-7	42'-9	8'-2		" " " "	
P910		24'-7	VIII			7			7	1'-7	23'-0	8'-2		" " " "	
P911		21'-9	STR.			2			2					" " " "	
P912		14'-10						10	10					" " " "	
P913		10'-10						2	2					" " " "	
P914		33'-10		10					10					" " " TOP	
P915		13'-0		10					10					" " " "	
P916		36'-5			10				10					" " " "	
P917		13'-9			10				10					" " " "	
P918		38'-11				9			9					" " " "	
P919		36'-11				9	9		18					" " " "	
P920		34'-11					9		9					" " " "	
P1003	#10	20'-6		22					22					VERTICAL BARS	
P1004		19'-8		22					22					" " " "	
P1005		18'-10		22					22					" " " "	
P1006		21'-4			22				22					" " " "	
P1007		20'-4			22				22					" " " "	
P1008		19'-4			22				22					" " " "	
P1009		57'-5				22			22					" " " "	
P1010		36'-9					22		22					" " " "	
P1011		36'-2					22		22					" " " "	
P1012		35'-6					22		22					" " " "	
P1013		39'-6	VIII			4	4	1'-2	38'-4 1/2	7'				HORIZONTAL BARS BOTTOM	
P1014		22'-0	VIII			4	4	1'-1 1/2	20'-10 1/2	7'				" " " "	
P1015		33'-2	STR.			4	4		4					" " " TOP	
P1016		31'-0				4	4		4					" " " "	
P1017		33'-4				5	5		5					" " " "	
P1018		10'-7				2	2		2					" " " "	
P1102	#11	21'-10				24			24					VERTICAL BARS EXT. COL.	
P1103		19'-6				24			24					" " " "	
P1104		17'-9					24		24					" " " "	
P1105		16'-5					24		24					" " " "	

HUMBOLDT PKWY. N.B. OVER EXPW. BAR LIST

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL	DRAWN	R.E.K.
ENGINEERS-ARCHITECTS	CHECKED	C.E.
NEW YORK	TRACED	

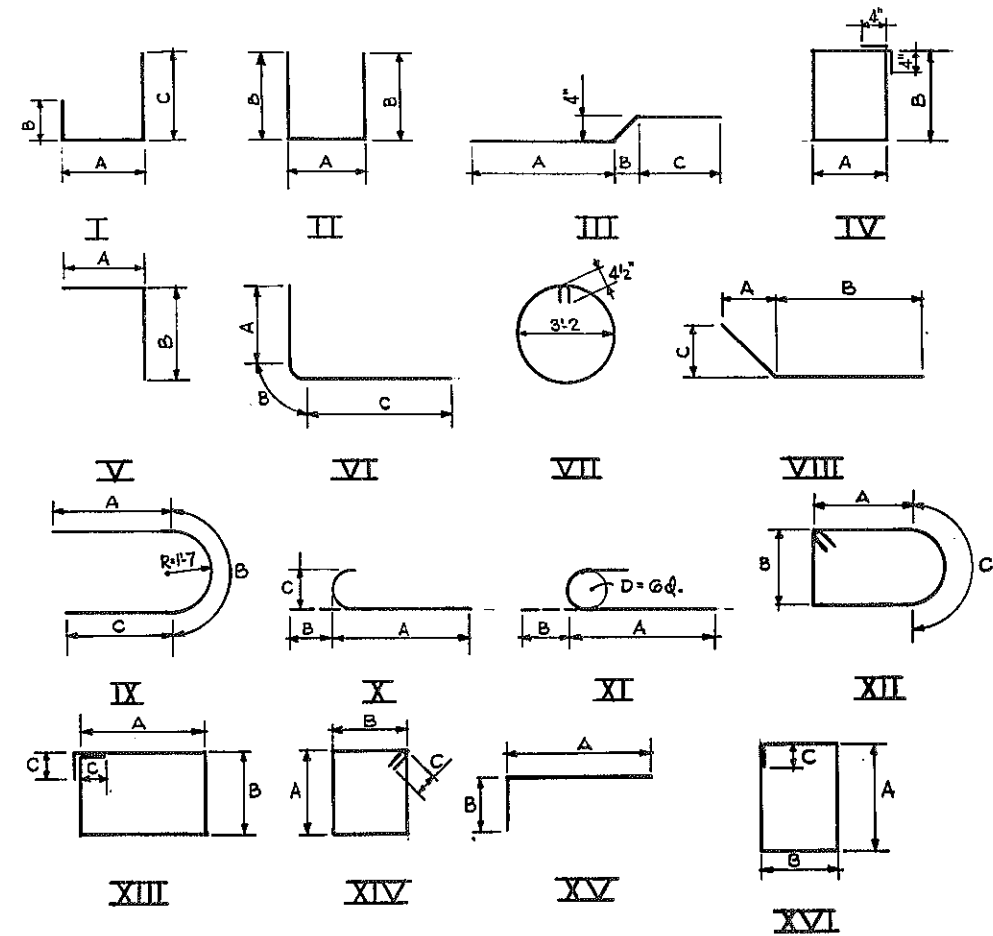
BAR LIST

DECK SLABS

MARK	SIZE	LEN.	TYPE	NUMBER							A	B	C	DESCRIPTION
				SPAN 1	SPAN 2	SPAN 3	SPAN 4	SPAN 5	SPAN 6	TOTAL				
S401	#4	24'-8"	STR.	272						272				LONG. BARS - DECK SLAB, SW
S402		8'-8"	XVI	192	76	129	223	192	112	924	3'-4"	8"	1'-0"	BENT BARS SIDEWALK & PARAPET
S403		3'-4"	XV	192	76	129	223	192	112	924	2'-8"	8"		TRANSVERSE SIDE WALK
S404		28'-3"	STR.		60					60				LONGITUDINAL BARS - VARIES 24'-0" TO 25'-10"
S405		26'-11"			8					8				LONG. BARS SIDEWALK & PARAPET
S406		23'-6"			4					4				" " " "
S407		64'-4"				60				60				LONGITUDINAL BARS VARIES 43'-4" TO 25'-4"
S408		29'-0"				12				12				LONG. BARS SIDEWALK
S409		22'-2"				8				8				" " " "
S410		28'-5"							272	272				LONGITUDINAL BARS
S411		34'-0"							136	136				" " " "
S412		30'-6"							68	68				" " " VARIES 27'-0" TO 34'-0"
S413		28'-6"							136	136				" " " "
S414		18'-2"		4			20	20	8	52				LONGITUDINAL BARS - PARAPET
S415		18'-8"		12			4		8	24				" " " "
S416		20'-8"		4		16				20				" " " "
S417		11'-10"			8					8				" " " "
S418		22'-2"				8				8				" " " "
S419		19'-8"		16			4	16		36				" " " "
S420		17'-8"			8			4		12				" " " "
S421		19'-2"					16		4	20				" " " "
S422		15'-8"		4					4	8				" " " "
S423		17'-0"		4						4				" " " "
S424		13'-8"				4				4				" " " "
S501	#5	27'-3"	XI	231	60	61	136	115	38	641	26'-8"	7"		TRANSVERSE BARS TOP
S502		20'-6"	XI	231	60	61	136	115	38	641	19'-11"	7"		" " " "
S503		30'-0"	STR.	231	60	61	136	115	38	641				" " BOT.
S504		16'-6"	STR.	231	60	61	136	115	38	641				" " " "
S505		25'-6"	STR.		4					4				" " TOP & BOT.
S506														" " " "
S507														" " " "
S508		23'-2"	STR.		63					63				" BOT. VAR. 3'-4" TO 43'-0"
S509		23'-10"	XI		63					63	23'-3"	7"		" TOP 'A' VAR. 3'-4" TO 43'-2"
S510		24'-3"	XI			41				41	23'-8"	7"		" TOP 'A' VAR. 3'-4" TO 44'-0"
S511		23'-8"	STR.			41				41				" BOT. VAR. 3'-4" TO 44'-0"
S512		24'-9"	STR.			4				4				" TOP & BOT.
S513														" " " "
S514														" " " "
S515		23'-10"	XI			137	137			274	22'-10"	7"		" TOP 'A' VAR. 3'-4" TO 42'-4"
S516		22'-10"	STR.			137	137			274				" BOT. VAR. 3'-4" TO 42'-4"
S517		39'-6"	STR.			4	4			8				" TOP & BOT.
S518														" " " "
S519														" " " "
S520														" " " "
S521														" " " "
S522														" " " "
S523														" " " "
S524		23'-15"	XI				125			125	22'-10"	7"		" TOP 'A' VAR. 3'-4" TO 42'-4"
S525		22'-10"	STR.				125			125				" BOT. VAR. 3'-4" TO 42'-4"
S526		37'-6"	STR.				4	4		8				" TOP & BOT.
S527														" " " "
S528														" " " "
S529		24'-8"	XI					127		127	23'-8"	7"		" TOP 'A' VAR. 3'-4" TO 44'-0"
S530		23'-8"	STR.					127		127				" TOP VAR. 3'-4" TO 44'-0"
S531														" " " "
S532														" " " "
S533														" " " "
S534		33'-3"	STR.					4	4	8				" TOP & BOT.
S535														" " " "
S536														" " " "
S537														" " " "
S538		23'-8"	XI					98		98	22'-10"	7"		" TOP 'A' VAR. 3'-4" TO 42'-4"
S539		22'-10"	STR.					98		98				" BOT. VAR. 3'-4" TO 42'-4"
S540														" " " "
S541														" " " "
S542														" " " "
S543		25'-1"	XI						103	103	24'-6"	7"		" TOP 'A' VAR. 3'-4" TO 45'-8"
S544		24'-6"	STR.						103	103				" BOT. VAR. 3'-4" TO 45'-8"
S545		23'-4"	XI						89	89	22'-8"	7"		" TOP 'A' VAR. 3'-4" TO 42'-2"
S546		22'-8"	STR.						89	89				" BOT. VAR. 3'-4" TO 42'-2"
S547		31'-6"	STR.					4	4					" TOP & BOT.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	131	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



B2 SHEET NO. 20

HUMBOLDT PKWY. NB. OVER EXPW BAR LIST CONT'D.		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.

SUBSTRUCTURE NOTES

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	133	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

GENERAL NOTES

LOADING : COOPER E72 WITH DIESEL IMPACT.

DESIGN SPECIFICATION : DESIGN AND FABRICATION TO BE IN ACCORDANCE WITH THE 1961 A.R.E.A. SPECIFICATIONS WITH CURRENT REVISIONS FOR STEEL RAILWAY BRIDGES.

MATERIALS : N.Y. STATE DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957, AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION. THE CONTRACTORS ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

FOUNDATION LOADS : FOR DESIGN PURPOSES, THE ASSUMED FOUNDATIONS LOAD ON ROCK FOR PIER, ABUTMENTS AND WINGWALLS DOES NOT EXCEED 8 TONS PER SQUARE FOOT.

CEMENT : CEMENT USED IN CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A. ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

CONCRETE : CONCRETE IN PIER BENT ABOVE FOOTINGS AND IN SUPERSTRUCTURE SHALL BE ITEM 18X, CLASS IA CONCRETE FOR STRUCTURES. CONCRETE IN ABUTMENTS, WINGWALLS AND FOOTINGS FOR PIER SHALL BE ITEM 20Y, CLASS I CONCRETE.

RAILROAD DETOUR : FOR RAILROAD DETOUR, TEMPORARY AND PERMANENT TRACKAGE, SEE SHEET NO. 33. BALLAST WILL BE FURNISHED AND PLACED BY THE N.Y.C.R.R. BUT SHALL BE REMOVED BY THE CONTRACTOR UNDER ITEM 2. TEMPORARY TRACK, HARDWARE AND TIES WILL BE FURNISHED, INSTALLED, MAINTAINED AND REMOVED BY THE N.Y.C.R.R.

ELEVATIONS : ALL ELEVATIONS REFER TO U.S.C.G.S. DATUM. SUBTRACT 1.904' TO REFER TO N.Y.C.R.R. DATUM.
U.S.C.G.S. DATUM = CITY DATUM + 575.454'
N.Y.C.R.R. DATUM = CITY DATUM + 573.55'
U.S.C.G.S. DATUM - 1.904' = N.Y.C.R.R. DATUM
NEW YORK CENTRAL RAILROAD DATUM 0.00 EQUALS MEAN HIGH WATER AT THE FOOT OF EAST 26th STREET, NEW YORK CITY, WHICH IS 2.75 FEET ABOVE MEAN TIDE AT SANDY HOOK.

MAINTENANCE : THE DIVISION OF MAINTENANCE FOR THE VARIOUS PORTIONS OF THIS CONTRACT SHALL BE AS FOLLOWS:
THE BRIDGE AND ITS SUPPORTING ABUTMENTS AND PIER CARRYING THE N.Y.C.R.R. TRACKS OVER KENSINGTON EXPRESSWAY SHALL BE MAINTAINED BY THE NEW YORK CENTRAL RAILROAD COMPANY.
ALL EXPRESSWAY CURBS, SHOULDERS AND DRAINAGE PIPES SHALL BE MAINTAINED AND KEPT IN REPAIR BY THE STATE OF NEW YORK.
ALL COMBINED SEWERS SHALL BE MAINTAINED AND KEPT IN REPAIR BY THE BUFFALO SEWER AUTHORITY.

OTHERS : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOP OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED. SURFACE OF ABUTMENTS AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATIONS SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATIONS

MAXIMUM PAYMENT LINES FOR ITEM 5 SHALL BE AS SHOWN ON THE PLANS. BITUMINOUS MATERIAL, ITEM 61, SHALL BE APPLIED TO THE BACKS OF ABUTMENTS AND WINGWALLS. THE ANCHOR BOLTS FOR THE BRIDGE BEARINGS SHALL BE SET PRIOR TO THE POURING OF THE CONCRETE. GIRDER BEARINGS SHALL BE PROVIDED WITH A 1/2" THICK BEARING PAD, ITEM 115. THE SIZE OF THE PAD SHALL BE EQUAL TO THE SIZE OF THE BEARING PLATE. HOLES 1 5/8" DIAM. SHALL BE PROVIDED IN THE BEARING PADS AT LOCATIONS TO MATCH THE HOLES IN THE BEARING PLATES.

ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED ONE INCH UNLESS OTHERWISE NOTED. PAYMENT FOR FURNISHING AND PLACING SPONGE RUBBER AND CAULKING COMPOUND SHALL BE INCLUDED IN THE UNIT PRICE OF CONCRETE ITEMS. SPONGE RUBBER JOINTS SHALL BE FILLED WITH SPONGE RUBBER 1/4" WIDER THAN THE JOINT WIDTH SHOWN ON THE PLAN AND SHALL CONFORM TO A.S.T.M. SPECIFICATIONS D544.

SUPERSTRUCTURE NOTES

SUPERSTRUCTURE : RIVETED PLATE GIRDER CONSTRUCTION. MAIN GIRDER STEEL : A.A.S.H.O. HIGH-STRENGTH LOW ALLOY STRUCTURAL STEEL A.S.T.M. A-440 FLOOR SYSTEM : STRUCTURAL STEEL, A.S.T.M. A7-56T. RIVETS : STRUCTURAL RIVET STEEL, A.S.T.M. A141. A.S.T.M. A-440 HIGH-STRENGTH STRUCTURAL STEEL WILL BE PAID FOR UNDER ITEM 29X. STRUCTURAL STEEL, STEEL CASTINGS, BRIDGE ANCHOR BOLTS, RIVETS, BOLTS, NUTS AND WASHERS WILL BE PAID FOR UNDER ITEM 29-D ITEM 29 OF THE NEW YORK STATE DEPARTMENT OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS SHALL APPLY EXCEPT THAT DESIGN AND FABRICATION SHALL BE IN ACCORDANCE WITH THE 1961 A.R.E.A. SPECIFICATIONS FOR STEEL RAILWAY BRIDGES. RIVETS, 1" Ø, OPEN HOLES 1 1/16" Ø, EXCEPT AS OTHERWISE NOTED. ERECTION SHALL BE IN ACCORDANCE WITH THE CURRENT A.R.E.A. SPECIFICATIONS FOR THE ERECTION OF STEEL RAILWAY BRIDGES. WELDING SHALL COMPLY WITH THE PROVISIONS OF THE CURRENT SPECIFICATIONS FOR WELDED HIGHWAY AND RAILWAY BRIDGES OF THE AMERICAN WELDING SOCIETY. FLOOR SYSTEM TO GIRDERS AND DIAPHRAGM CONNECTIONS SHALL BE REAMED IN THE SHOP WITH MEMBERS ASSEMBLED. THE COST OF FURNISHING AND PLACING LEAD PLATES UNDER DECK PLATE ON BACKWALLS WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 18X.

FOOTINGS : ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS I CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

ESTIMATE OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL NEAT	TOTAL ROUNDED
2EF-B	SELECTED FILL	C. Y.	287	310
5	TRENCH, CULVERT & BRIDGE EXC.	C. Y.	133	145
11H	PERF CORR. METAL PIPE UNDERDRAIN 6" DIAM.	L. F.	248	255
13B	CAST IRON PIPE, 6" DIAM.	L. F.	22	23
15-2A	PORTLAND CEMENT TYPE 2A	B B L.	1223	1285
18X	CLASS IA CONCRETE FOR STRUCTURES	C. Y.	293	300
20Y	CLASS I CONCRETE	C. Y.	462	480
24AX	BAGGED SCREENED GRAVEL OR SLAG	C. Y.	58	60
28	BAR REINFORCEMENT FOR STRUCTURES	L.B.	50,422	51,500
29B	STRUCTURAL STEEL	L.B.	920,599	948,200
29X	A.S.T.M. A-440 HIGH STRENGTH STRUCT. STEEL	L.B.	546,184	562,000
61	BITUMINOUS MATERIAL	GAL.	138	150
62	MEMBRANE WATERPROOFING	S. Y.	954	1,000
63	PROTECTIVE COURSE FOR MEMBRANE W.P.	S. Y.	787	820
83TX	TEMPORARY SHEET PILING	S. F.	365	380
115	BRIDGE BEARING PADS	S. F.	90	100
121A	TOPSOIL - FURNISHED & PLACED	C. Y.	12	13
123	SEEDING	ACRES	.022	.02
30S	MISCELLANEOUS METALS	L.B.	228	240
352XA	RUBBER JOINT MATERIAL	GAL.	1	1
9VC	SEWER PIPE VITRIFIED, 8" DIAM.	L. F.	102	110
21	CLASS 2 CONCRETE	C. Y.	27	30
30W	W.I. PIPE, 6" DIAM.	L.B.	1176	1200
363G	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	32	38

Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREFORMED JOINT, COMPRESSED ASBESTOS SHEET PACKING AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT. COMMON BRICK INCLUDED IN THE PRICE BID FOR ITEM 63 SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT STANDARD SPECIFICATIONS FOR BUILDING BRICK (MADE OF CLAY OR SHALE) DESIGNATION C-62 OF THE AMERICAN SOCIETY FOR TESTING MATERIALS, GRADE SW.

IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STANDARD SHEET 60-41.

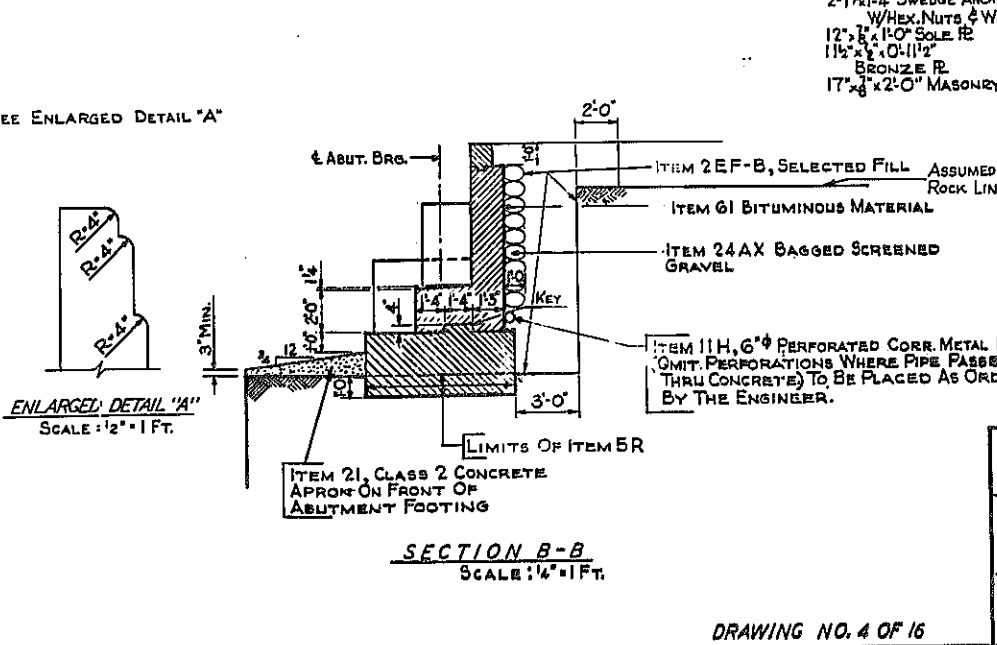
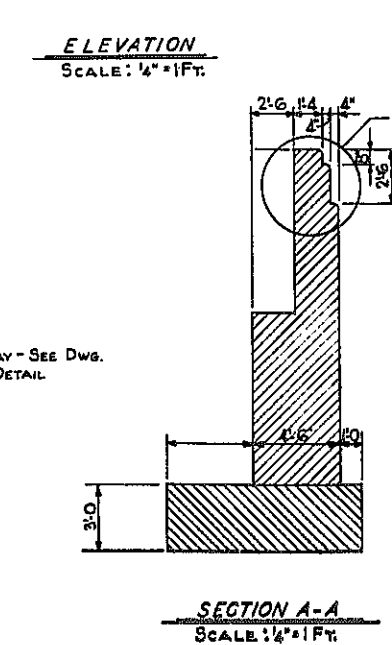
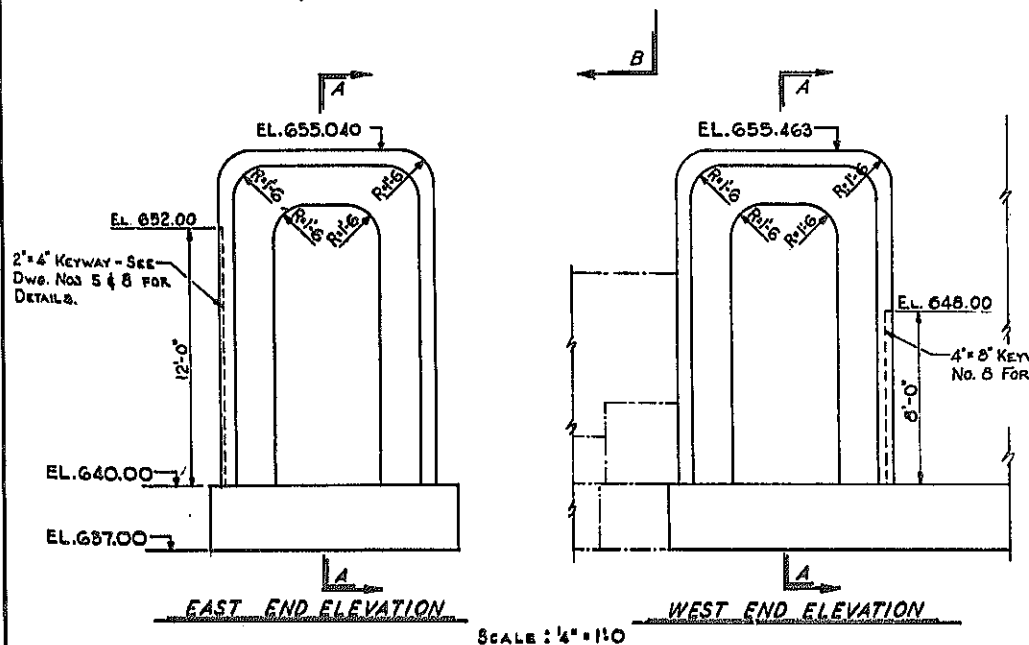
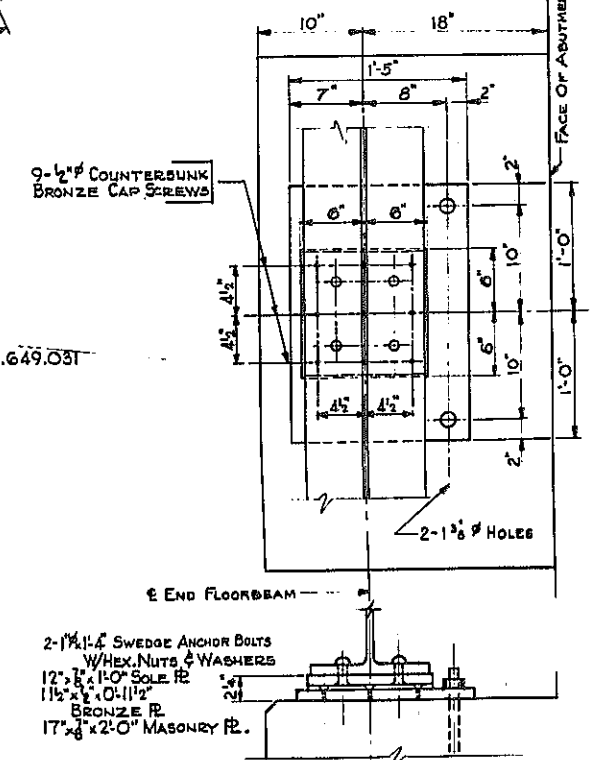
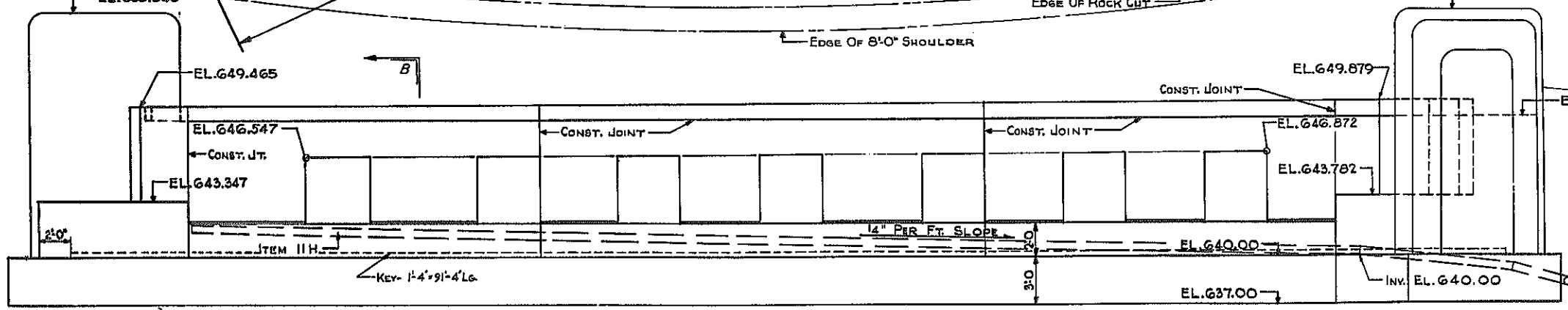
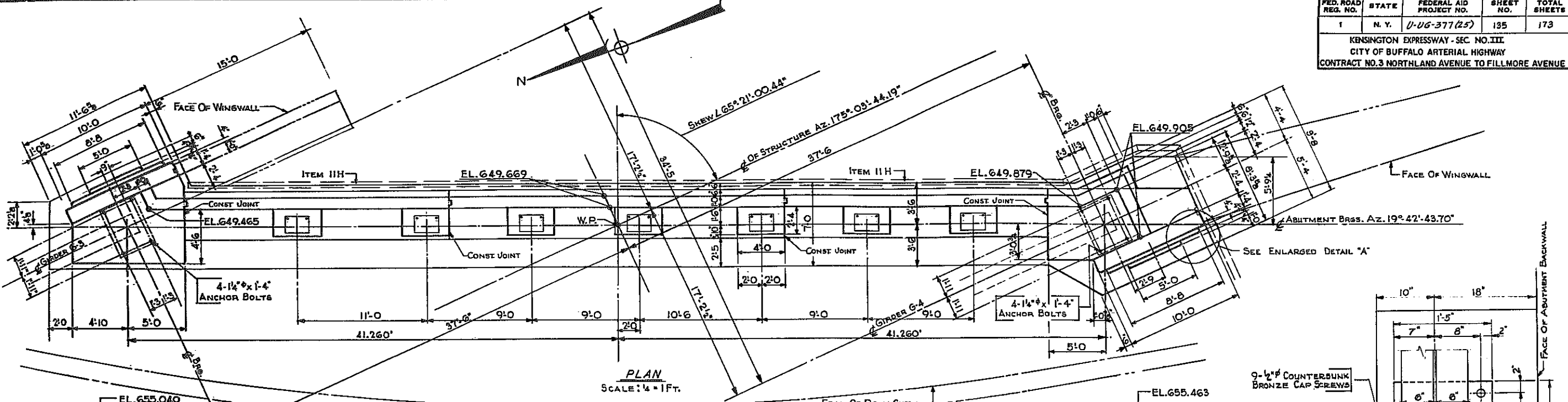
N.Y.C.R.R. OVER EXPRESSWAY
GENERAL NOTES & ESTIMATE
OF QUANTITIES

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHRIN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	A.E. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	135	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



N.Y.C.R.R. OVER EXPRESSWAY
SOUTH ABUTMENT

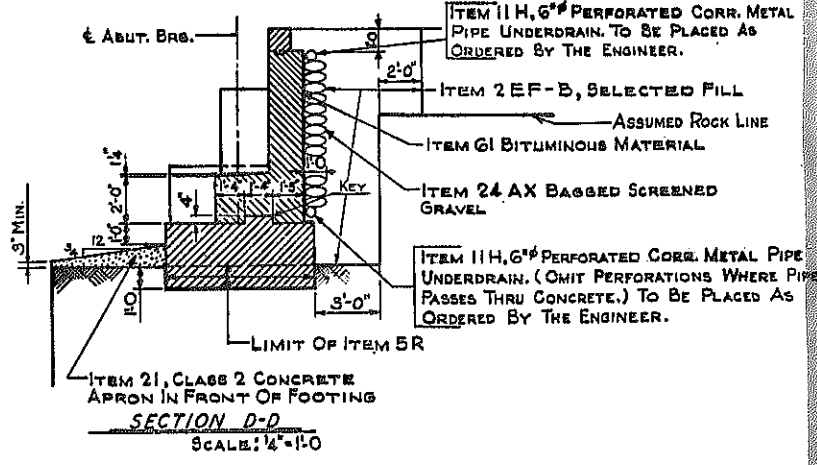
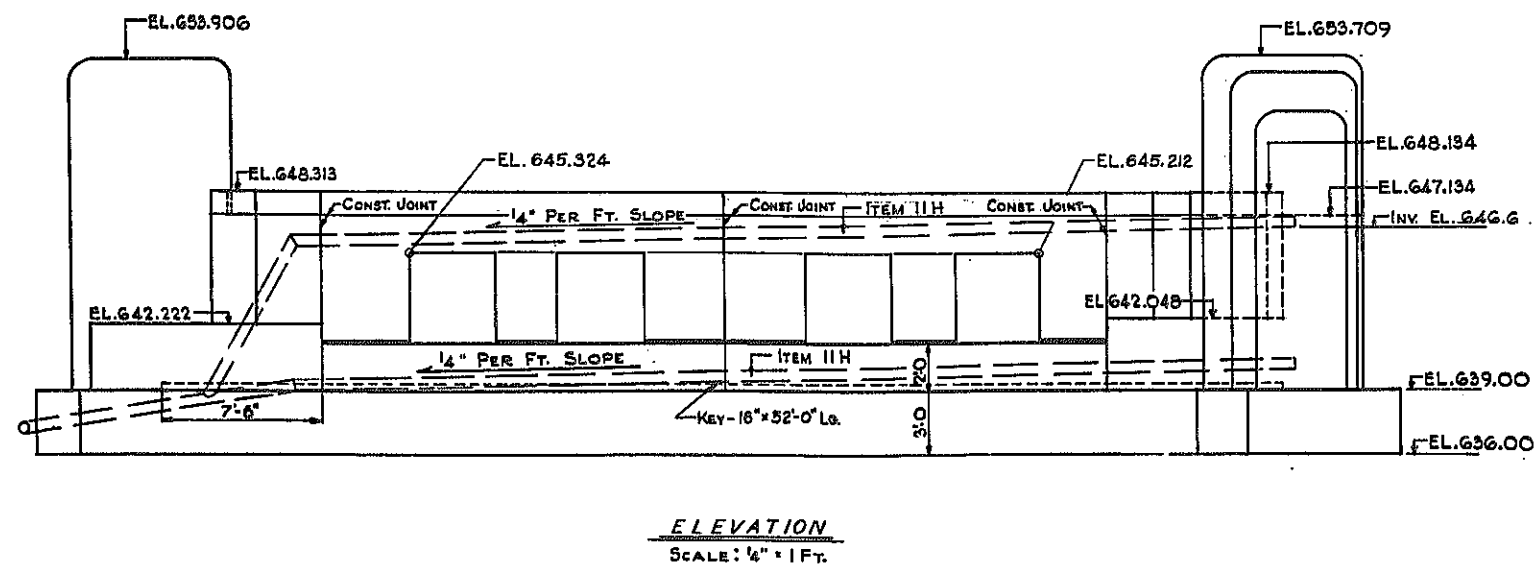
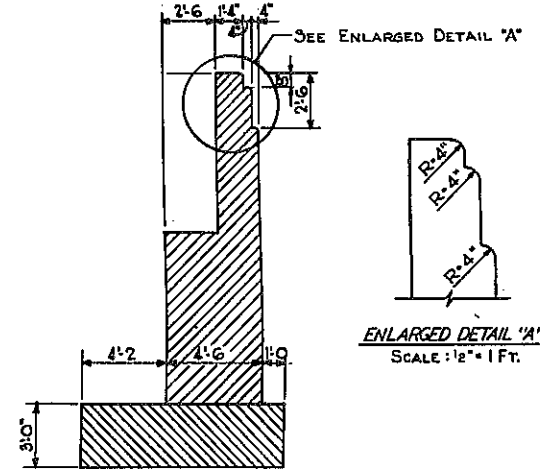
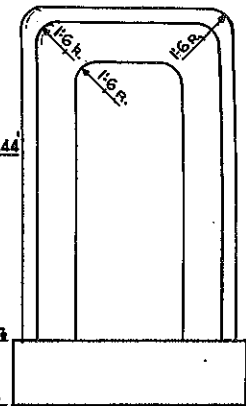
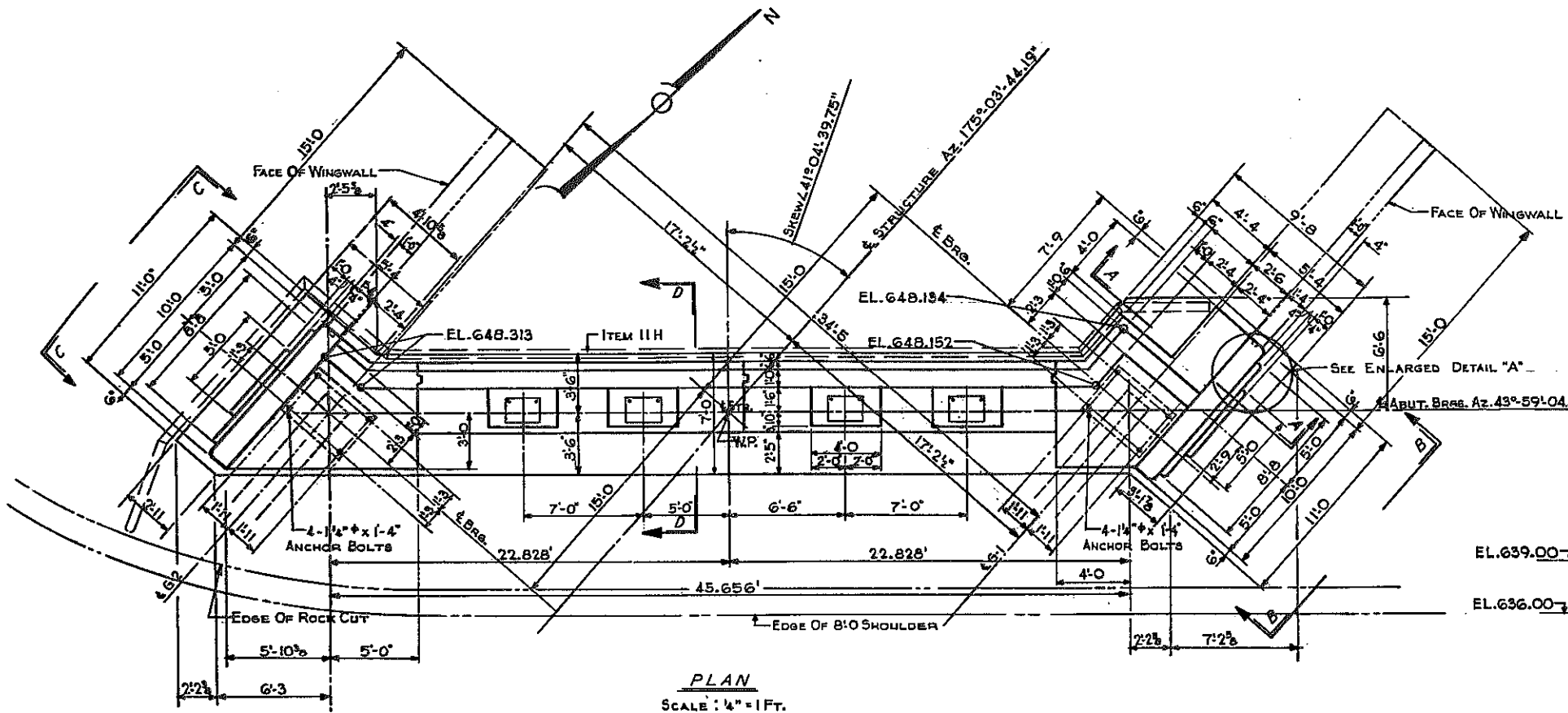
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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DRAWING NO. 4 OF 16

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	137	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE: SEE DRAWING NO. 4 FOR DETAIL OF END FLOORBEAM BEARING.

DRAWING NO 6 OF 16

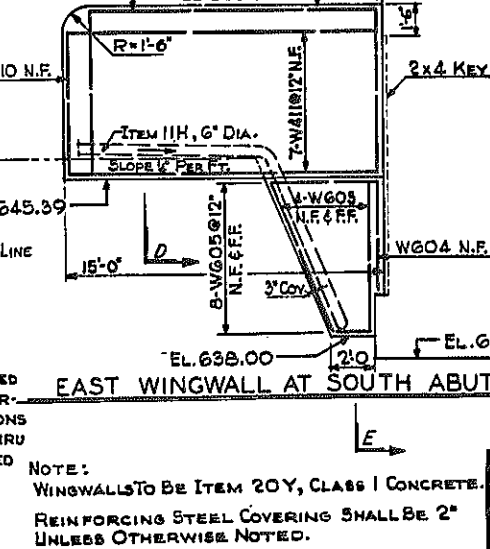
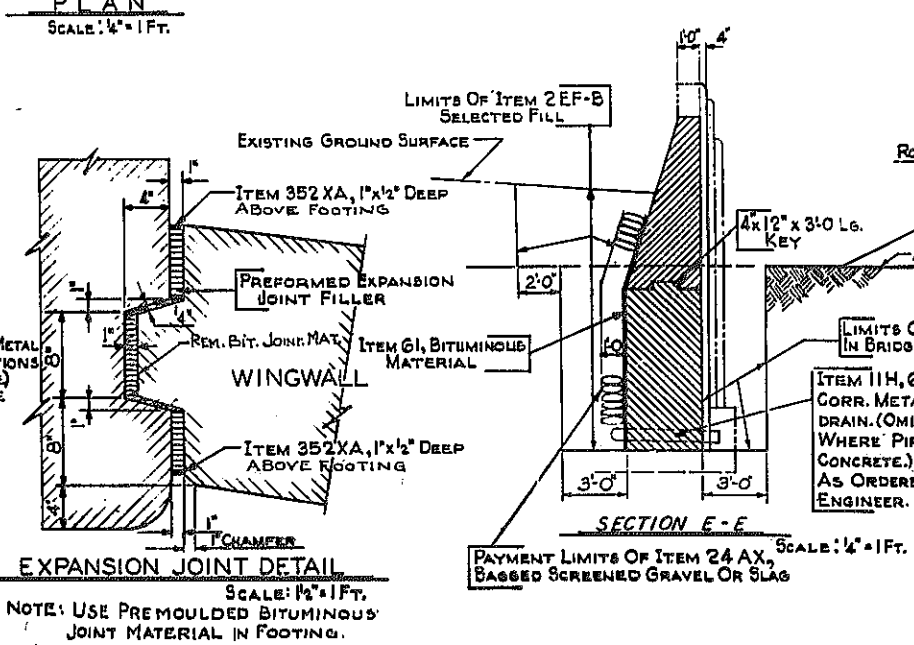
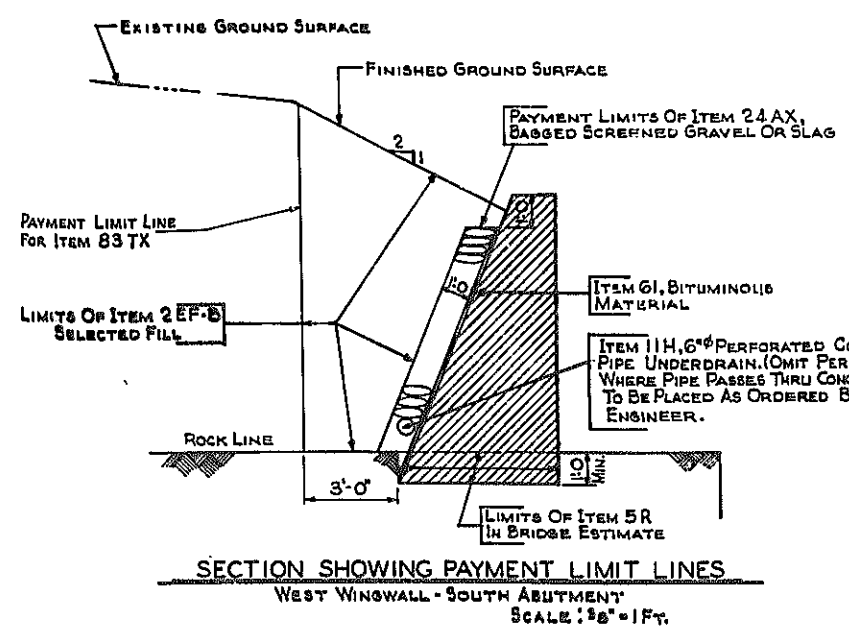
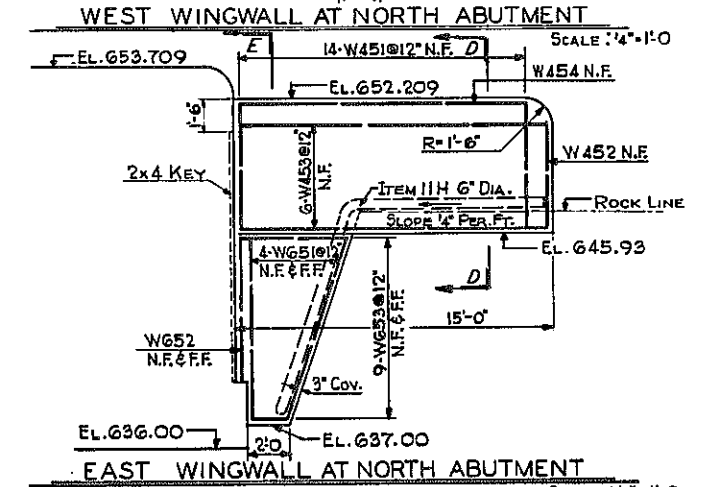
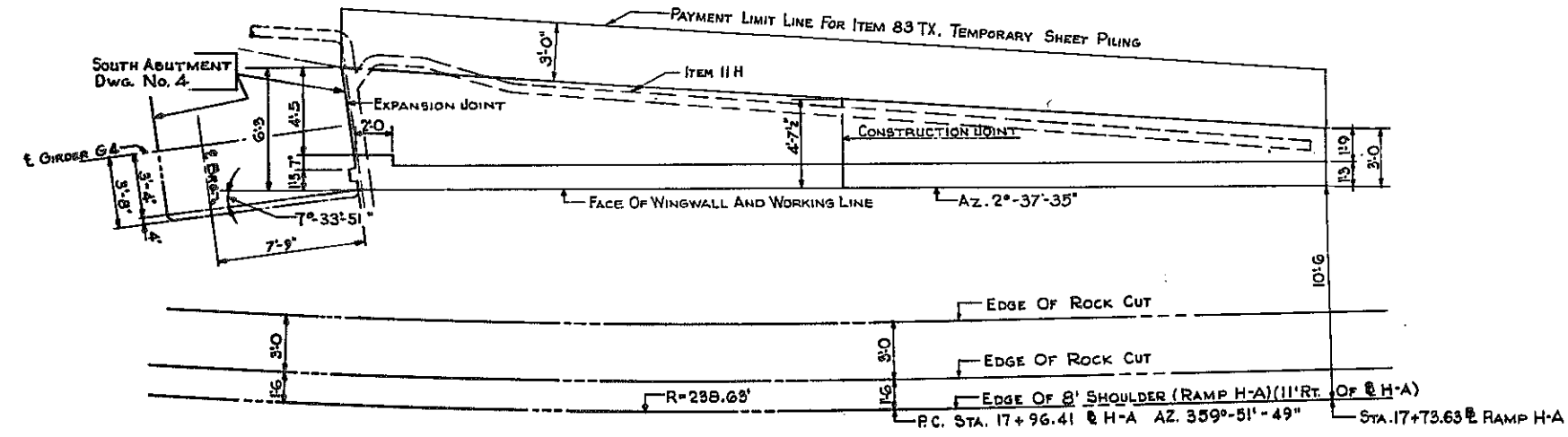
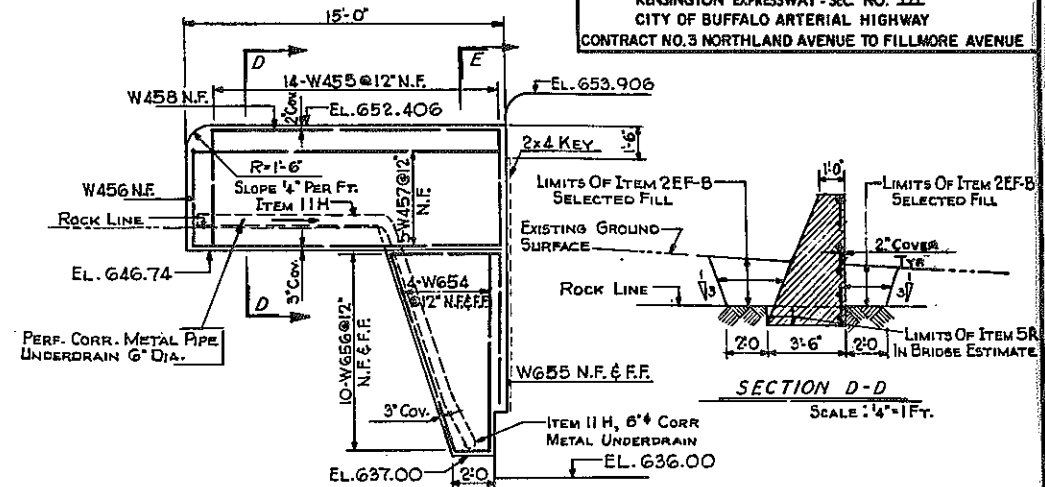
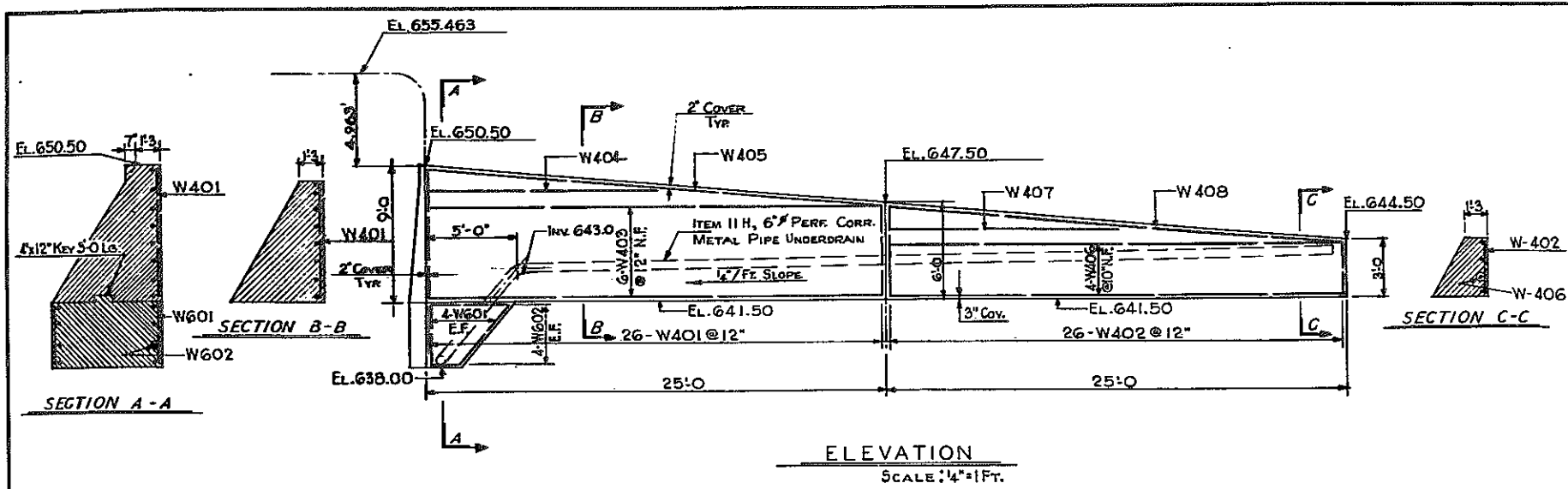
N.Y.C.R.R. OVER EXPRESSWAY NORTH ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO.

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.M. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(20)	139	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

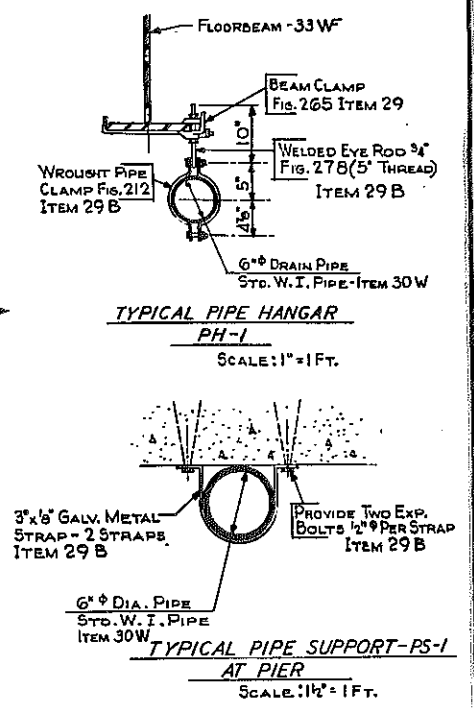
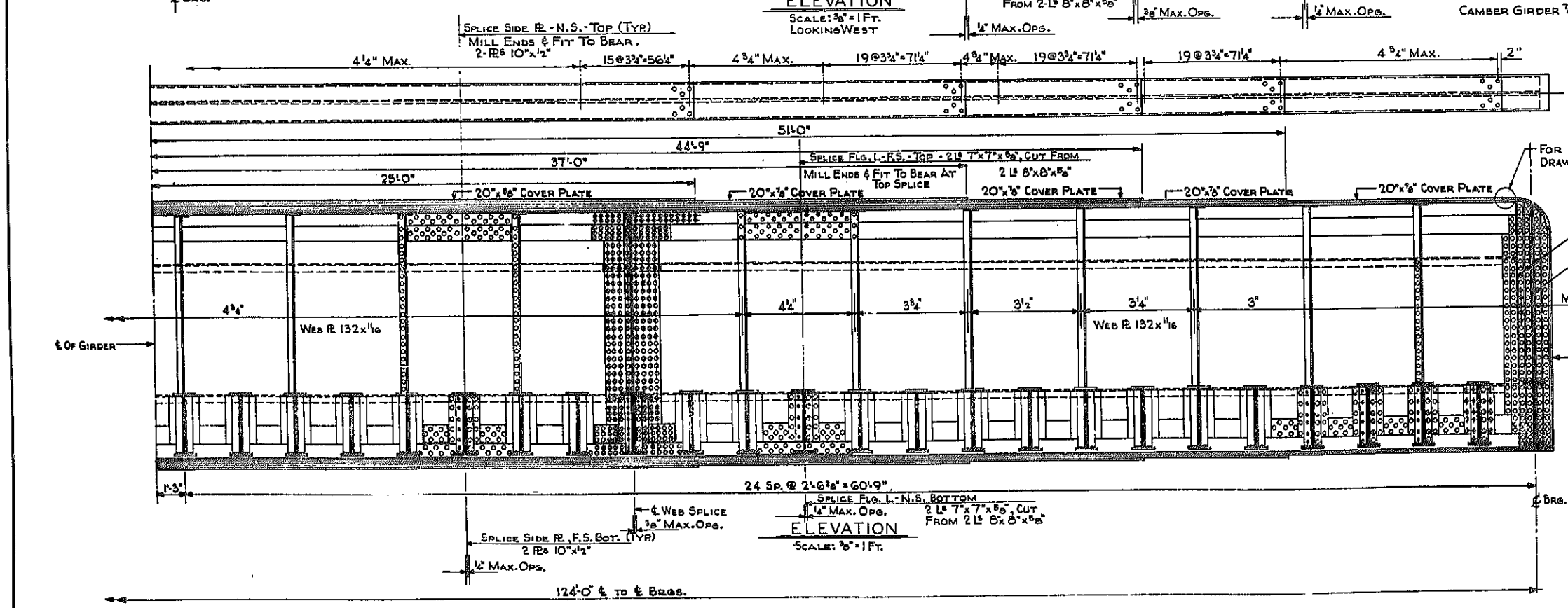
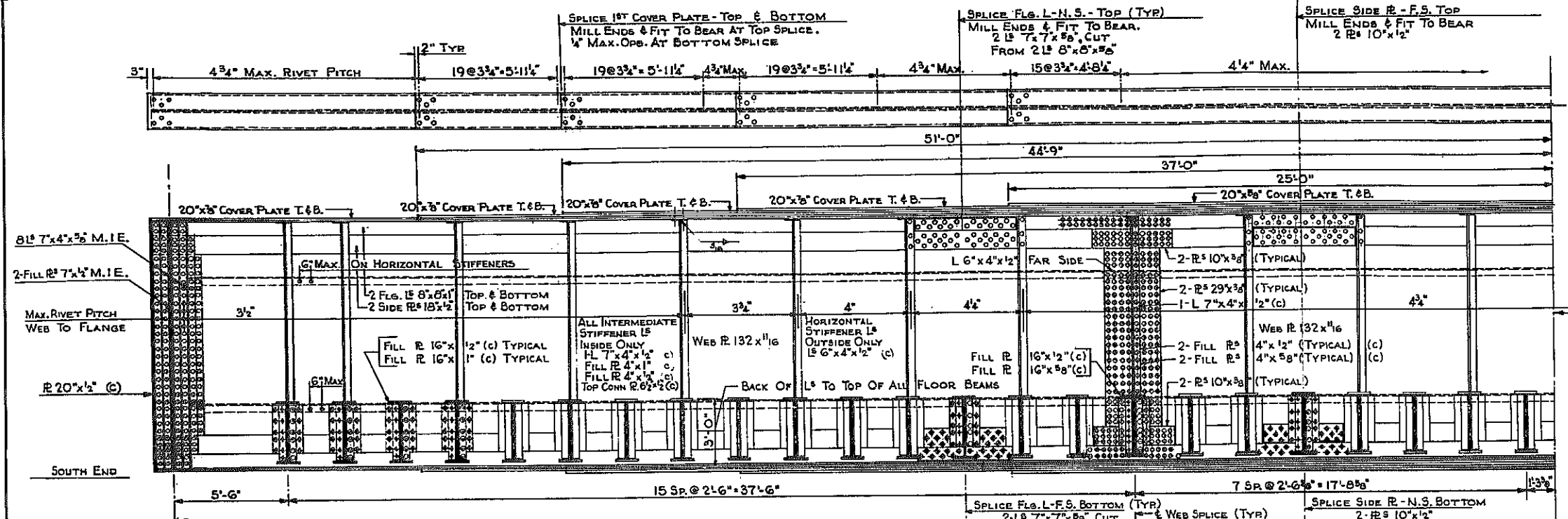


NOTE:
WINGWALLS TO BE ITEM 20 Y, CLASS I CONCRETE.
REINFORCING STEEL COVERING SHALL BE 2" UNLESS OTHERWISE NOTED.

N.Y.C.R.R. OVER EXPRESSWAY WINGWALLS		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	143	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



SEE DWG. NO. 13 FOR SCUPPER DETAILS

FOR WELD SEE DETAIL 'A', DRAWING NO. 11.

NOTES:
SEE SHEET NO. 2 FOR NOTES ON GIRDER MATERIAL.
PARTS MARKED (C) ARE STRUCTURAL STEEL, A.S.T.M.-A-7-56T.
RIVETS 1" Ø, OPEN HOLES 1/16" Ø EXCEPT AS OTHERWISE NOTED.

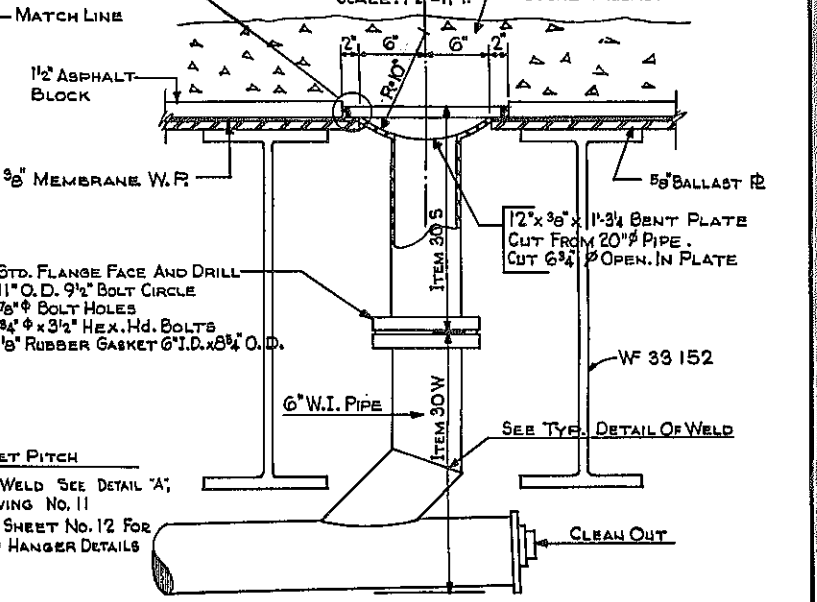
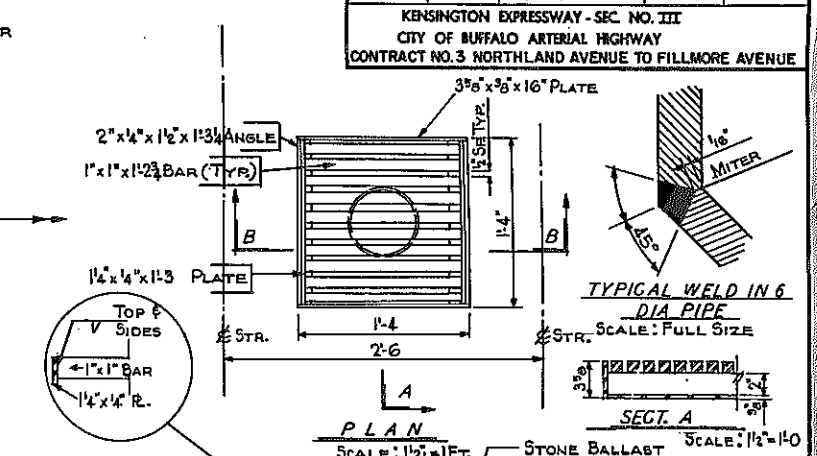
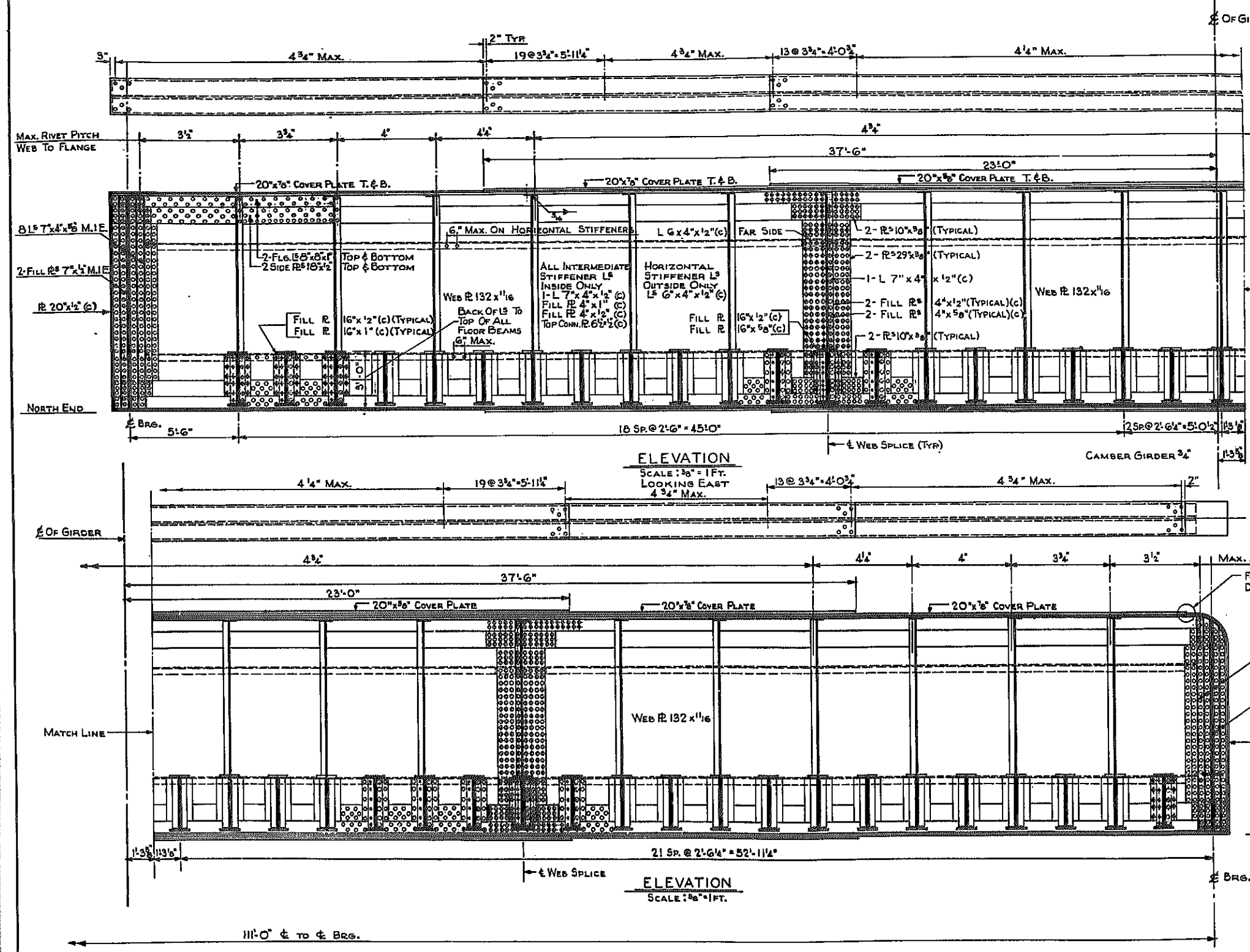
N.Y.C.R.R. OVER EXPRESSWAY GIRDER G-2

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	144	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



DETAILS OF SCUPPER DRAINS
SCALE: 1 1/2" = 1 FT.

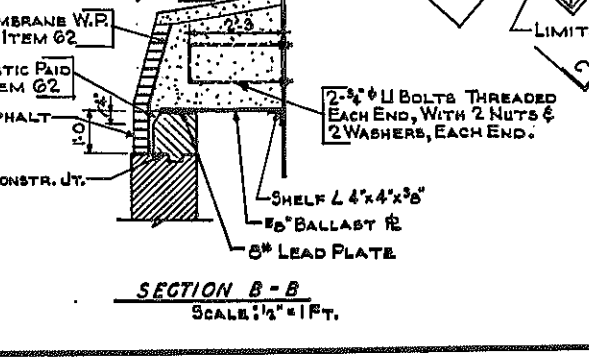
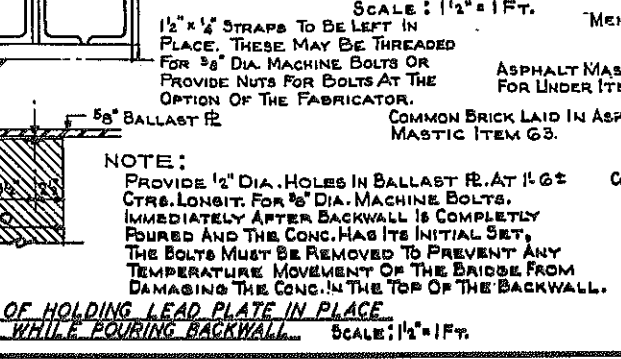
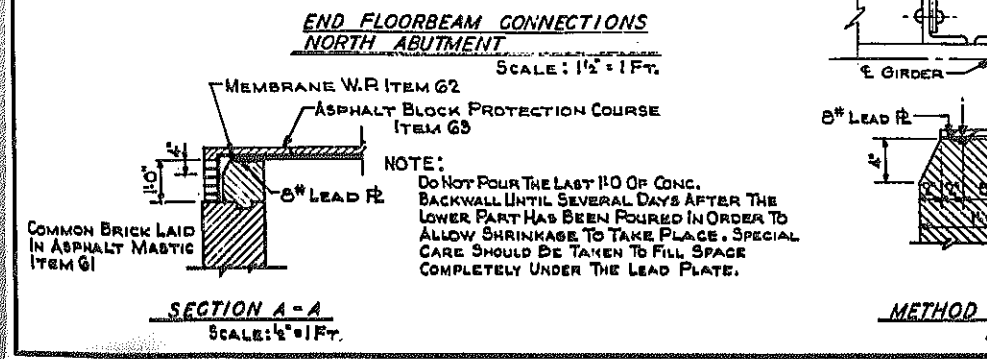
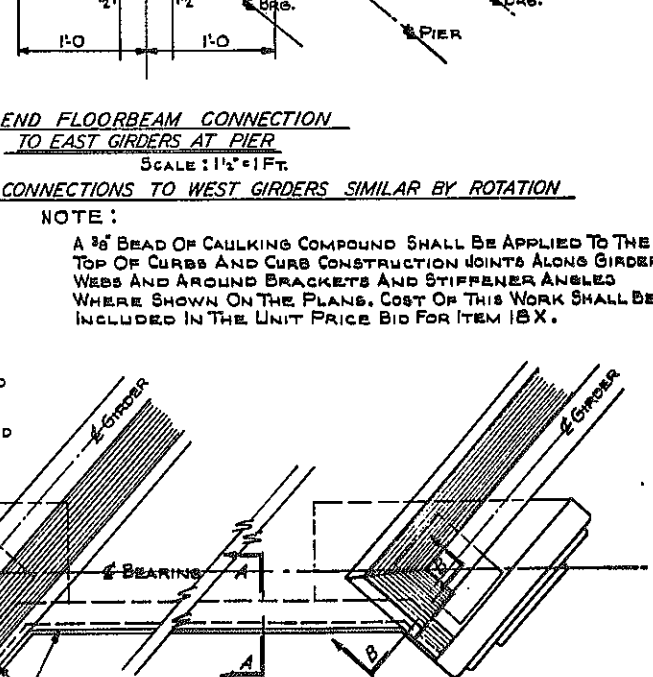
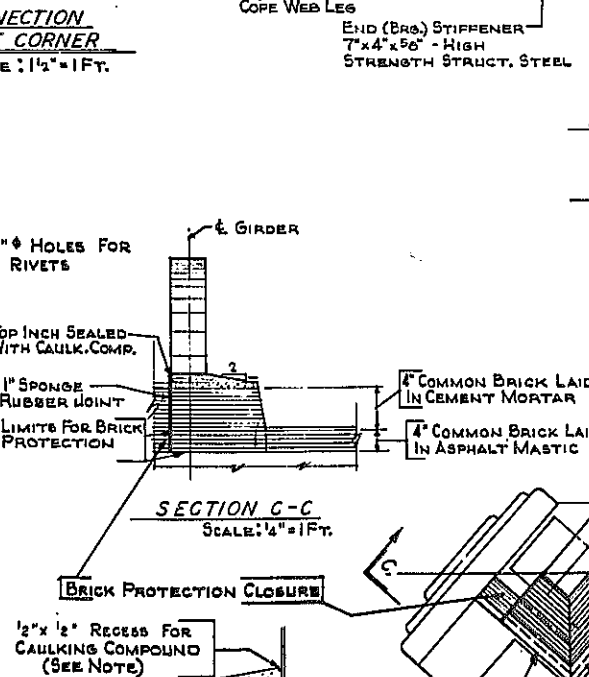
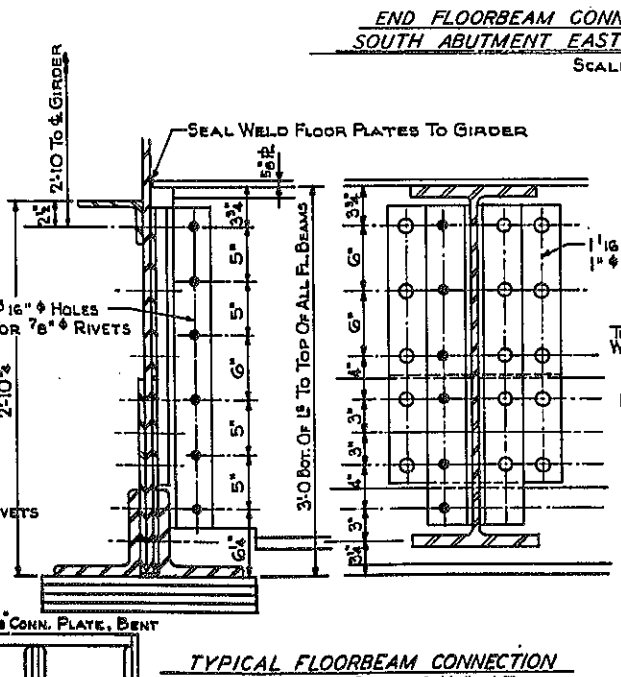
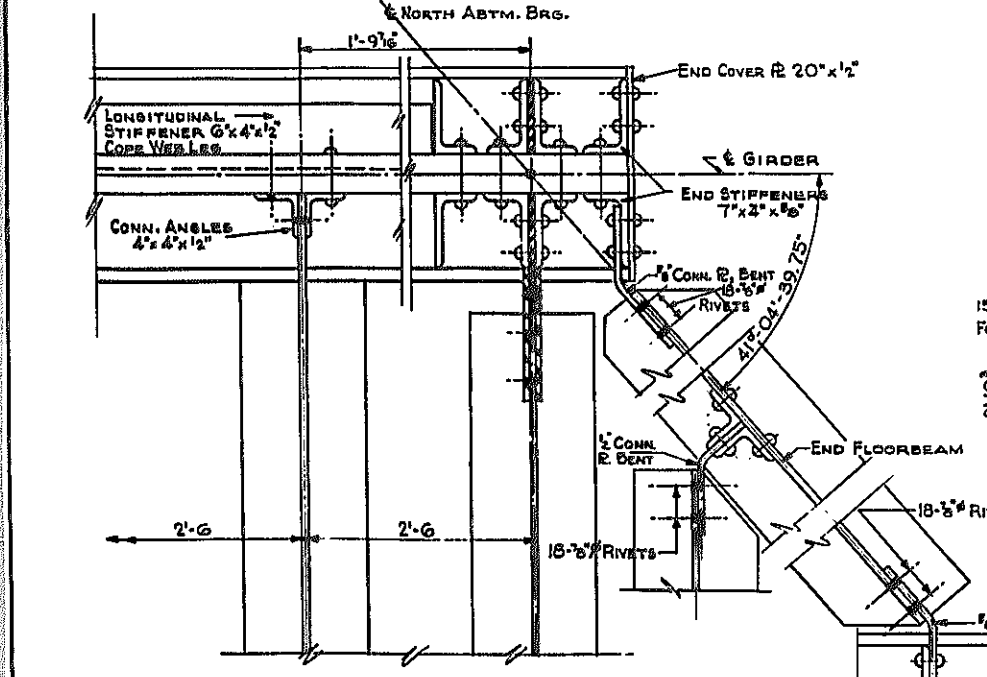
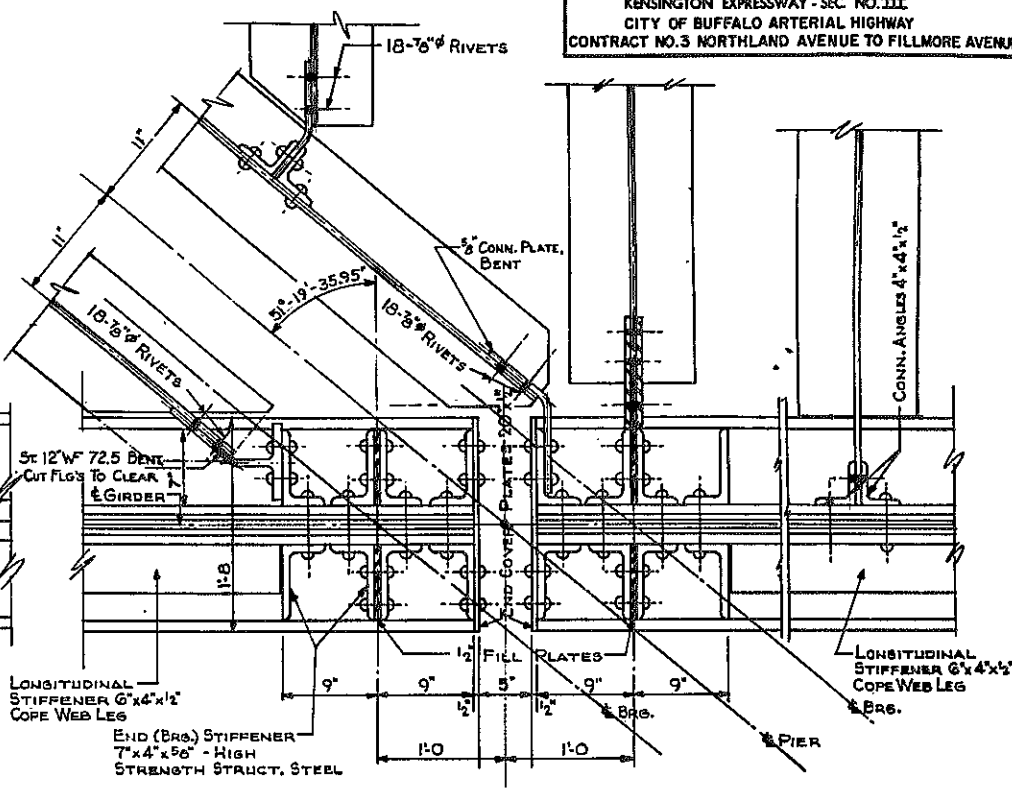
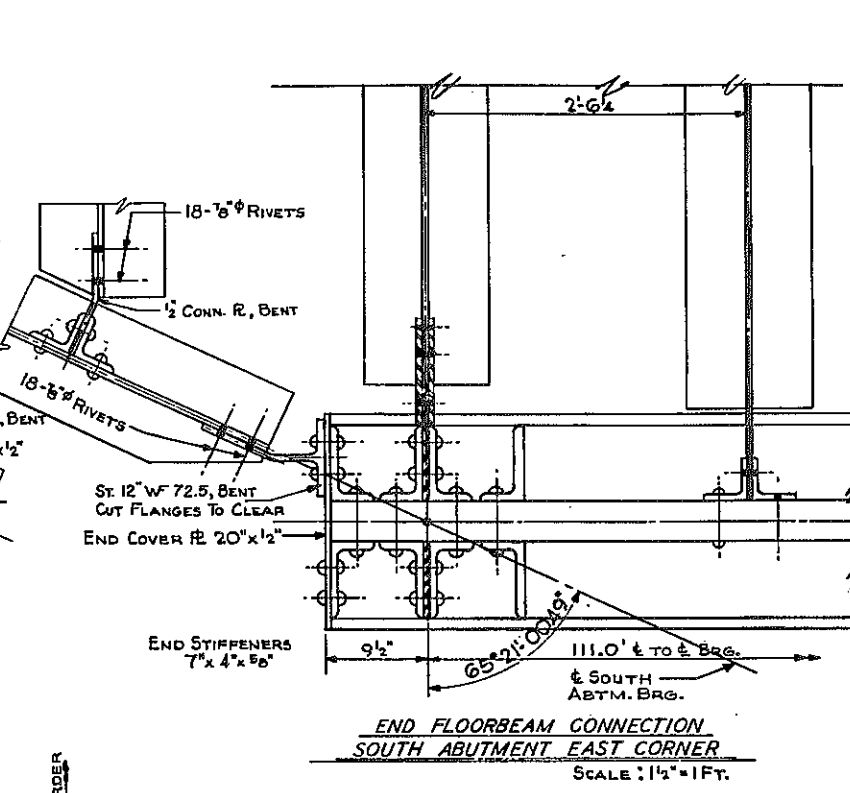
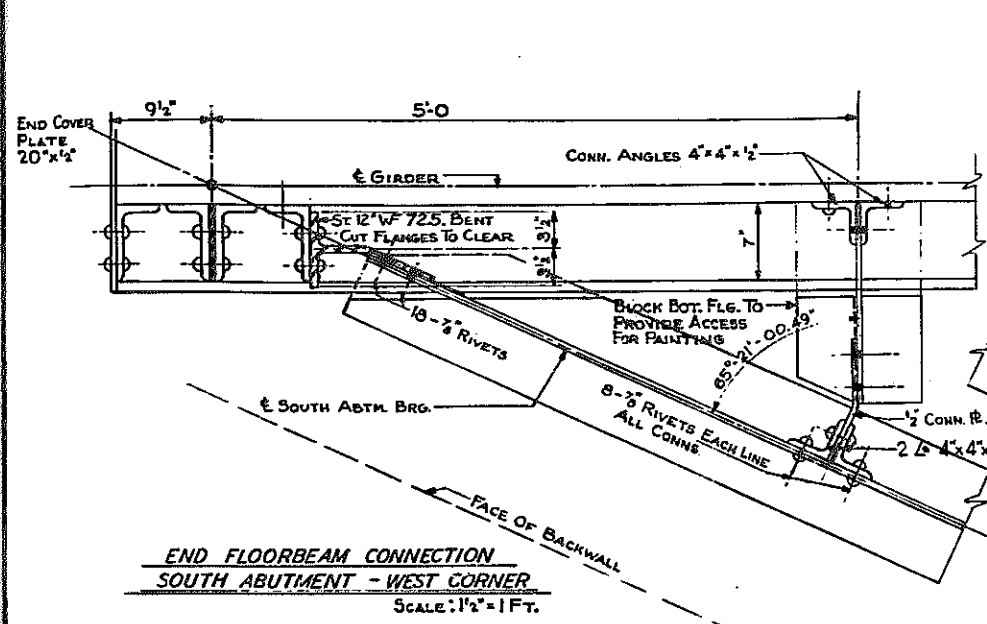
NOTE:
MATERIAL IN GRATING AND FRAME SHALL MEET THE A.S.T.M. SPECIFICATION A 242, EXCEPT THAT MIN. TENSILE STRENGTH MAY BE 65,000 P.S.I. AND THE MIN. YIELD POINT MAY BE 45,000 P.S.I.
ALL WELDS TO BE 5/16" FILLET WELDS.
PAYMENT FOR FURNISHING AND PLACING THE SCUPPER DRAINS WILL BE MADE UNDER ITEM 305, SEE DRAWING NO. 3 FOR PLAN ARRANGEMENT.

NOTES:
SEE SHEET NO. 2 FOR NOTES ON GIRDER MATERIAL.
PARTS MARKED (C) ARE STRUCTURAL STEEL A. S. T. M. - A 7-56 T.
RIVETS 1" φ, OPEN HOLES 1 1/16" φ EXCEPT AS OTHERWISE NOTED.

N.Y.C.R.R. OVER EXPRESSWAY GIRDER G-3			
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III			
DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.I.	

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	D-UG-377(25)	146	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



NOTE:
A 3/8" BEAD OF CAULKING COMPOUND SHALL BE APPLIED TO THE TOP OF CURBS AND CURB CONSTRUCTION JOINTS ALONG GIRDER WEBS AND AROUND BRACKETS AND STIFFENER ANGLES WHERE SHOWN ON THE PLANS. COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 18 X.

N.Y.C.R.R. OVER EXPRESSWAY SUPERSTRUCTURE DETAILS

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	R.E.K. C.E.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	147	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR LIST

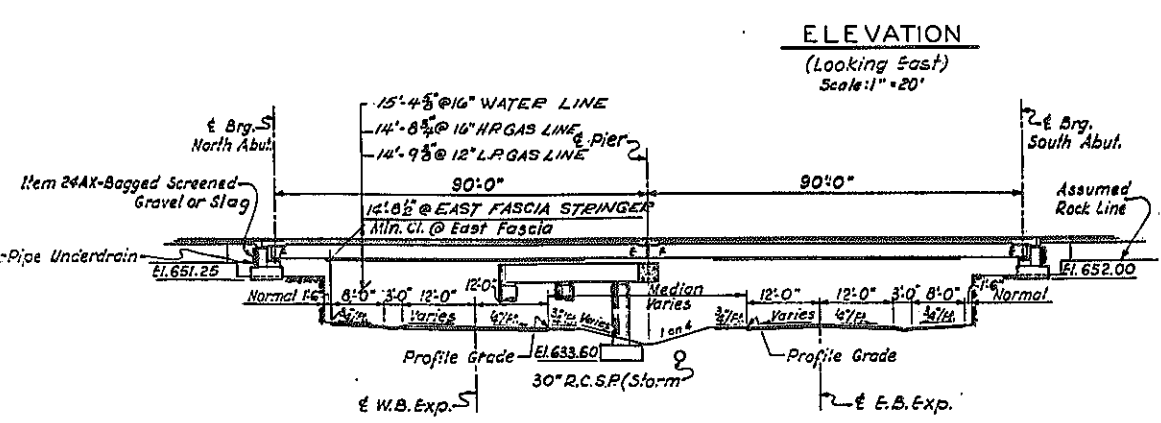
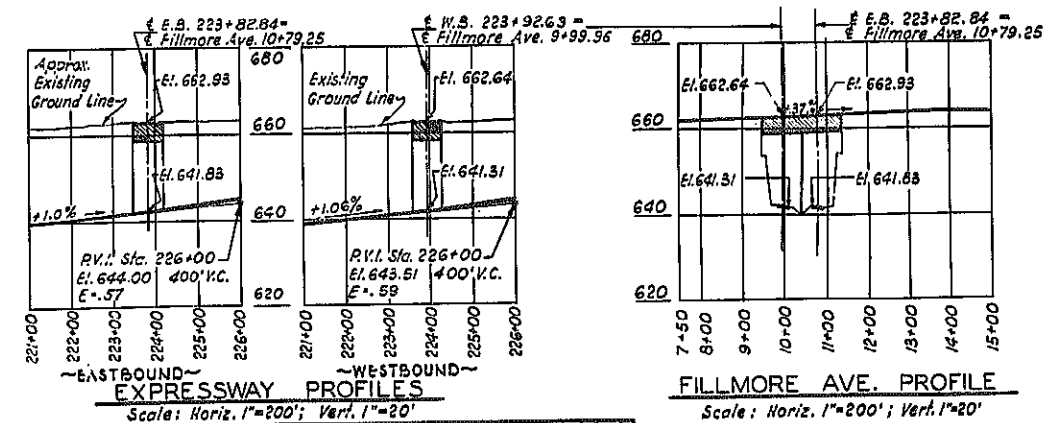
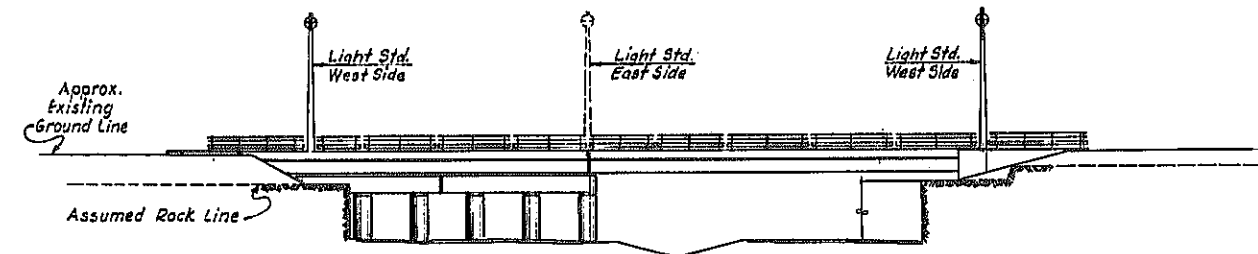
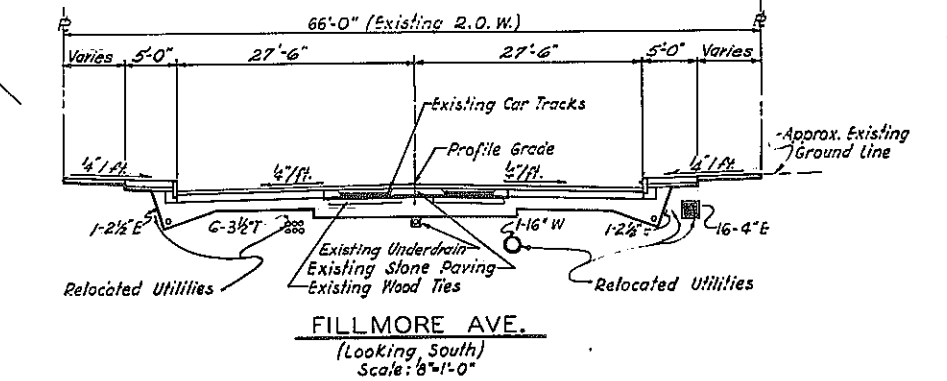
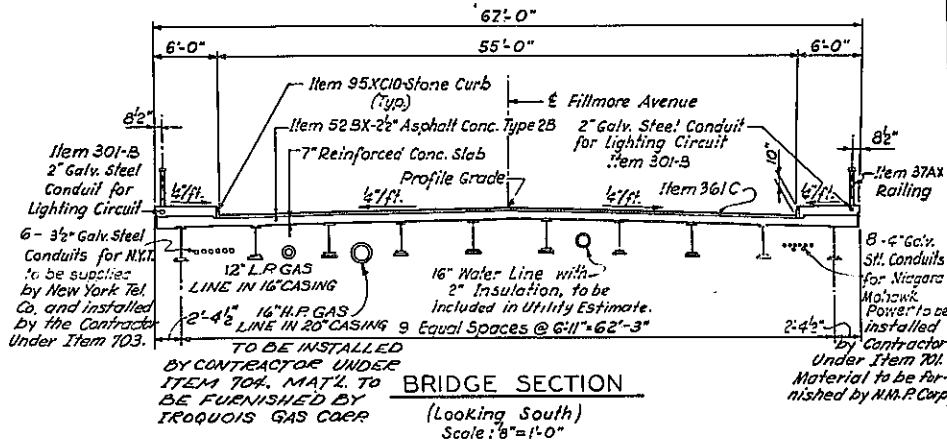
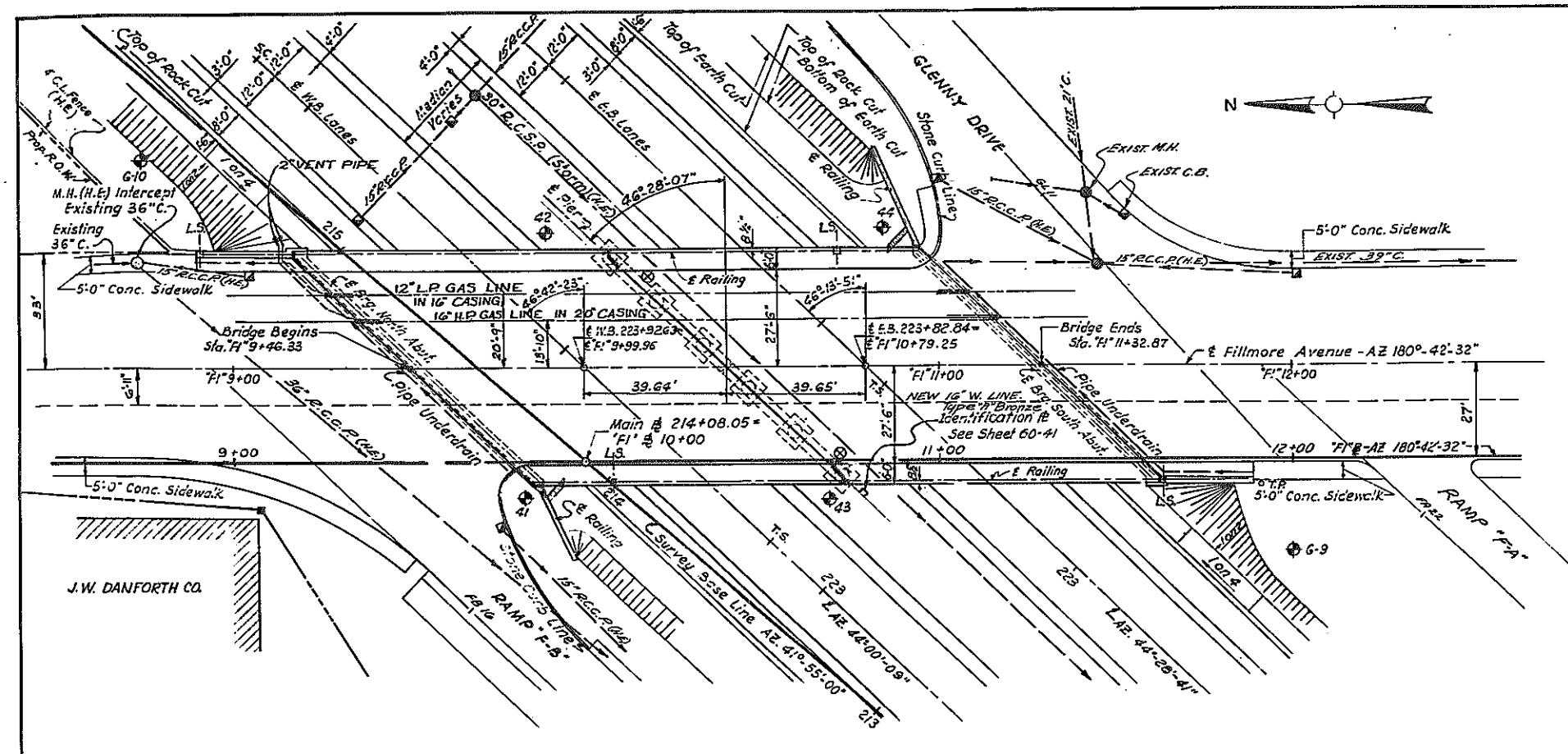
ABUTMENTS									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
SOUTH ABUTMENT - FOOTING									
A-401	#4	6'-6"	STR	110					TRANS. BARS
A-402	#4	5'-5"	STR	17					TRANS. BARS (VARIES 5'-6" TO 10'-0")
A-403	#4	9'-2"	STR	12					TRANS. BARS
A-404	#4	8'-5"	STR	7					TRANS. BARS (VARIES 7'-8" TO 9'-2")
A-405	#4	8'-0"	STR	2					TRANS. BARS
A-801	#8	8'-5"	STR	6					LONG. BARS (VARIES 4'-6" TO 8'-6")
A-802	#8	8'-8"	STR	8					LONG. BARS (VARIES 1'-8" TO 7'-8")
A-804	#8	5'-0"	STR	2					LONG. BARS
A-805	#8	5'-0"	STR	2					LONG. BARS
A-806	#8	10'-0"	STR	2					LONG. BARS
A-807	#8	4'-5"	STR	12					LONG. BARS (VARIES 1'-6" TO 8'-0")
A-808	#8	4'-0"	STR	2					LONG. BARS
A-809	#8	11'-0"	STR	2					LONG. BARS
A-901	#9	48'-0"	STR	56					LONG. BARS
SOUTH ABUTMENT WALLS									
A-810	#8	9'-6"	STR	14					HORIZ. BARS ABUT.
A-406	#4	11'-0"	STR	24					VERT. BARS - CURTAIN WALLS
A-407	#4	13'-6"	STR	24					VERT. BARS - CURTAIN WALLS
A-408	#4	4'-0"	I	24	1'-0"	3'-0"			BENT BARS - CURTAIN WALLS
A-409	#4	3'-10"	I	20	1'-4"	2'-6"			BENT BARS - CURTAIN WALLS
A-410	#4	4'-0"	STR	28					DOWELS - CURTAIN WALLS
A-411	#4	7'-10"	II	13	4'-8"	1'-8"			HORIZ. BARS - CURTAIN WALLS
A-412	#4	10'-10"	II	15	8'-4"	1'-4"			HORIZ. BARS - CURTAIN WALLS
A-413	#4	11'-6"	II	17	9'-8"	1'-0"			HORIZ. BARS - CURTAIN WALLS
A-414	#4	2'-4"	STR	5					HORIZ. BARS - ABUT.
A-415	#4	8'-4"	STR	4					VERT. BARS - ABUT.
A-416	#4	4'-0"	STR	5					HORIZ. BARS - ABUT.
A-417	#4	4'-4"	II	46	8"	1'-10"			BENT BARS - ABUT.
A-401	#6	10'-9"	STR	88					VERT. BARS - ABUT.
A-402	#6	42'-0"	STR	20					HORIZ. BARS - ABUT.
A-403	#6	36'-10"	STR	4					HORIZ. BARS - ABUT.
A-404	#6	9'-6"	STR	4					HORIZ. BARS - ABUT.
A-405	#6	3'-6"	STR	4					HORIZ. BARS - ABUT.
A-406	#6	5'-6"	STR	8					HORIZ. BARS - ABUT.
A-407	#6	5'-0"	STR	25					VERT. BARS - ABUT.
A-408	#6	6'-8"	STR	21					VERT. BARS - ABUT.
A-409	#6	10'-6"	II	35	3'-8"	3'-6"			BENT HORIZ. BARS - PEDESTALS
A-410	#6	3'-6"	STR	87					DOWELS - ABUT. - FTG.
A-411	#6	3'-0"	STR	4					HORIZ. BARS - ABUT.
A-412	#6	7'-10"	STR	8					HORIZ. BARS - ABUT.
NORTH ABUTMENT - FOOTING									
A-451	#4	6'-6"	STR	62					TRANS. BARS
A-452	#4	5'-4"	STR	4					TRANS. BARS
A-453	#4	10'-6"	STR	2					TRANS. BARS
A-454	#4	10'-9"	STR	2					TRANS. BARS
A-455	#4	10'-0"	STR	2					TRANS. BARS
A-456	#4	9'-5"	STR	2					TRANS. BARS
A-457	#4	7'-8"	STR	2					TRANS. BARS
A-458	#4	9'-8"	STR	2					TRANS. BARS
A-459	#4	8'-5"	STR	2					TRANS. BARS
A-460	#4	7'-5"	STR	2					TRANS. BARS
A-851	#8	10'-6"	STR	16					LONG. BARS
A-852	#8	7'-11"	STR	12					LONG. BARS (VARIES 5'-4" TO 10'-0")
A-853	#8	5'-8"	STR	10					LONG. BARS (VARIES 1'-8" TO 5'-8")
A-854	#8	4'-0"	STR	2					HORIZ. BARS
A-951	#9	50'-6"	STR	56					LONG. BARS

ABUTMENTS									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
NORTH ABUTMENT - WALLS									
A-462	#4	12'-0"	STR	24					VERT. BARS - CURTAIN WALLS
A-463	#4	14'-6"	STR	24					VERT. BARS - CURTAIN WALLS
A-464	#4	4'-0"	I	24	1'-0"	3'-0"			BENT BARS - CURTAIN WALLS
A-465	#4	3'-10"	I	20	1'-4"	2'-6"			BENT BARS - CURTAIN WALLS
A-466	#4	4'-0"	STR	29					DOWELS - CURTAIN WALLS
A-467	#4	7'-10"	II	14	4'-8"	1'-8"			HORIZ. BARS - CURTAIN WALLS
A-468	#4	10'-10"	II	16	8'-4"	1'-4"			HORIZ. BARS - CURTAIN WALLS
A-469	#4	11'-6"	II	16	9'-8"	1'-0"			HORIZ. BARS - CURTAIN WALLS
A-470	#4	9'-6"	STR	12					HORIZ. BARS - CURTAIN WALLS
A-471	#4	4'-0"	STR	5					HORIZ. BARS - CURTAIN WALLS
A-472	#4	8'-0"	STR	5					VERT. BARS - CURTAIN WALLS
A-473	#4	3'-4"	STR	5					HORIZ. BARS - CURTAIN WALLS
A-474	#4	4'-4"	II	27	8"	1'-10"			BENT BARS - ABUT.
A-651	#6	8'-0"	STR	34					VERT. BARS - ABUT.
A-652	#6	26'-8"	STR	20					HORIZ. BARS - ABUT.
A-653	#6	10'-6"	II	20	3'-8"	3'-6"			HORIZ. BENT BARS - PEDESTALS
A-654	#6	3'-9"	STR	18					DOWELS - ABUT.
A-655	#6	18'-0"	STR	4					HORIZ. BARS - ABUT.
A-656	#6	10'-6"	STR	4					HORIZ. BARS - ABUT.
A-657	#6	5'-8"	STR	8					HORIZ. BARS - ABUT.
A-658	#6	4'-0"	STR	4					HORIZ. BARS - ABUT.
A-659	#6	5'-0"	STR	26					VERT. BARS - ABUT.
A-660	#6	6'-0"	STR	12					VERT. BARS - ABUT.
A-855	#8	8'-8"	STR	14					HORIZ. BARS ABUT. (VARIES 10'-0" TO 6'-0")
SOUTH ABUTMENT - WEST WINGWALL									
W-401	#4	7'-4"	STR	26					VERT. BARS (VARIES 8'-10" TO 5'-10")
W-402	#4	1'-4"	STR	26					VERT. BARS (VARIES 5'-10" TO 2'-10")
W-403	#4	24'-8"	STR	6					HORIZ. BARS
W-404	#4	12'-0"	STR	1					HORIZ. BARS
W-405	#4	24'-8"	STR	1					HORIZ. BARS
W-406	#4	24'-8"	STR	4					HORIZ. BARS
W-407	#4	12'-0"	STR	1					HORIZ. BARS
W-408	#4	24'-8"	STR	1					HORIZ. BARS
W-409	#6	3'-0"	STR	8					VERT. BARS
W-402	#6	AVG. 3'-0"	STR	8					HORIZ. BARS (VARIES 1'-6" TO 4'-6")
SOUTH ABUTMENT - EAST WINGWALL									
W-409	#4	7'-10"	STR	14					VERT. BARS
W-410	#4	6'-4"	STR	1					VERT. BARS
W-411	#4	14'-8"	STR	8					HORIZ. BARS
W-412	#4	13'-2"	STR	1					HORIZ. BARS
W-403	#6	6'-11"	STR	8					VERT. BARS
W-404	#6	4'-11"	STR	2					VERT. BARS
W-405	#6	AVG. 3'-5"	STR	16					HORIZ. BARS (VARIES 1'-6" TO 5'-0")
NORTH ABUTMENT - EAST WINGWALL									
W-451	#4	5'-11"	STR	14					VERT. BARS
W-452	#4	4'-5"	STR	1					VERT. BARS
W-453	#4	14'-8"	STR	6					HORIZ. BARS
W-454	#4	13'-2"	STR	1					HORIZ. BARS
W-451	#6	8'-5"	STR	8					VERT. BARS
W-452	#6	6'-6"	STR	2					VERT. BARS
W-453	#6	AVG. 3'-5"	STR	18					HORIZ. BARS (VARIES 1'-6" TO 5'-0")
NORTH ABUTMENT - WEST WINGWALL									
W-455	#4	5'-4"	STR	14					VERT. BARS
W-456	#4	3'-10"	STR	1					VERT. BARS
W-457	#4	14'-8"	STR	5					HORIZ. BARS
W-458	#4	13'-2"	STR	1					HORIZ. BARS
W-454	#6	9'-3"	STR	8					VERT. BARS
W-455	#6	7'-3"	STR	2					VERT. BARS
W-456	#6	AVG. 3'-5"	STR	20					HORIZ. BARS (VARIES 1'-6" TO 5'-0")

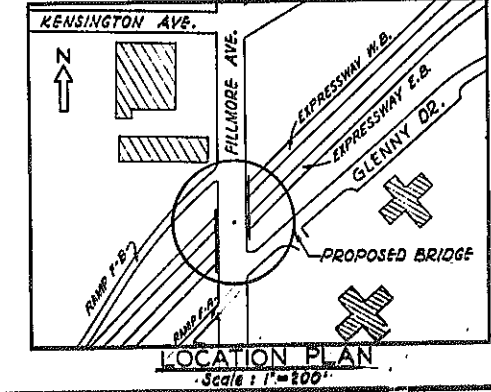
PIER									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
FOOTING									
P401	#4	AVG. 6'-10"	STR.	22					TRANS. BARS T. & B. VARIES 4'-6" TO 9'-3"
P402	#4	9'-6"	STR.	92					TRANSVERSE BARS TOP & BOT.
P901	#9	37'-9"	STR.	80					HORIZONTAL BARS TOP & BOT.
PI001	#10	6'-0"	V	48	4'-11"	1'-1"	9"		DOWELS - FTG.
COLUMNS - BEAM & WINGWALL									
P403	#4	24'-4"	STR.	12					VERTICAL BARS - E.W.W.
P404	#4	26'-10"	STR.	12					VERTICAL BARS - E.W.W.
P405	#4	4'-0"	I	24	1'-0"	3'-3"			BENT BARS E. & W.W.W.
P406	#4	5'-1"	I	20	1'-4"	4'-0"			BENT BARS E. & W.W.W.
P407	#4	7'-6"	II	28	4'-8"	1'-8"			HORIZONTAL BARS E. & W.W.W.
P408	#4	10'-6"	II	28	8'-4"	1'-4"			HORIZONTAL BARS E. & W.W.W.
P409	#4	11'-2"	II	30	9'-8"	1'-0"			HORIZONTAL BARS E. & W.W.W.
P410	#4	2'-3"	STR.	24					DOWELS - FTG.
P411	#4	24'-7"	STR.	12					VERTICAL BARS - W.W.W.
P412	#4	27'-1"	STR.	12					VERTICAL BARS - W.W.W.
P413	#4	10'-9"	III	32	3'-2"				COLUMN TIES
P414	#4	3'-0"	V	40	2'-6"	6"			CAP PEDESTALS DOWELS
P501	#5	14'-2"	IV	82	4'-8"	2'-2"	4'-2"		CAP STIRRUPS
P502	#5	4'-8"	STR.	10					VERTICAL BARS - CAP BEAM
P601	#6	16'-6"	STR.	54					VERTICAL BARS - EXT. COLS.
P602	#6	14'-6"	IV	12	3'-8"	3'-2"	7"		HORIZONTAL BARS PEDESTALS
P902	#9	24'-4"	STR.	14					HORIZONTAL BARS - TOP OF CAP BEAM
P903	#9	24'-0"	STR.	8					HORIZONTAL BARS - SIDES OF CAP BEAM
P904	#9	16'-7"	STR.	14					HORIZONTAL BARS - BOT. OF CAP BEAM
P905	#9	17'-5"	STR.	7					HORIZONTAL BARS - BOT. OF CAP BEAM
P906	#9	13'-11"	STR.	36					HORIZONTAL BARS - EXT. COLS.
P907	#9	6'-8"	STR.	36					HORIZONTAL BARS - EXT. COLS.
P908	#9	7'-0"	STR.	36					

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(2.5)	14B	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



- LEGEND**
- ⊕ Indicates Boring Locations.
 - E = Expansion Bearings
 - F = Fixed Bearings
 - Items marked (H.E.) to be included in Highway Estimate.
 - ⊙ Indicates Location of 1 1/2" Galvanized Pipe Drains.



BE, SHEET NO. I

FILLMORE AVENUE OVER EXPRESSWAY GENERAL PLAN

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHAR & BRILL	DRAWN	A.C.
ENGINEERS-ARCHITECTS	CHECKED	D.H.
NEW YORK	TRACED	A.C.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	149	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE.

GENERAL NOTES

DESIGN SPECIFICATIONS : A.A.S.H.O. 1957 AND CURRENT MODIFICATIONS.

LOADING : H20-S16-44 LIVE LOAD

MATERIAL FABRICATION AND MANUFACTURE : NEW YORK STATE DEPT. OF PUBLIC WORKS SPECIFICATIONS DATED JANUARY 2, 1957 AND CURRENT MODIFICATIONS.

ATTENTION OF CONTRACTOR : THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE SPECIAL NOTES FOR STRUCTURE WHICH APPEAR ON THE PLANS, ON THE STANDARD SHEETS THAT ARE INCORPORATED IN THE PLANS OR IN THE PROPOSAL. PARTICULAR ATTENTION SHOULD BE GIVEN TO THE FOUNDATION NOTES WHICH BRIEFLY OUTLINE THE ANTICIPATED SUBSURFACE CONDITIONS AT THE SITE OF THE STRUCTURE AND WHICH SPECIFY CERTAIN REQUIREMENTS RELATIVE TO CONSTRUCTION.
THE CONTRACTOR'S ATTENTION IS ALSO CALLED TO THE NECESSITY OF RECOGNIZING THE ELEVATION OF GROUND WATER, SO THAT HE MAY PROGRESS THE WORK WITH FULL KNOWLEDGE THAT PAYMENT FOR MAINTAINING THE EXCAVATION FREE OF WATER WILL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 5 TRENCH, CULVERT AND BRIDGE EXCAVATION.

SOIL LOADS : FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK, AT THE PIERS AND ABUTMENTS DOES NOT EXCEED 5 TONS PER SQUARE FOOT.

CONCRETE : ALL CONCRETE IN THE SUPERSTRUCTURE, THE PIERS ABOVE FOOTINGS, AND IN SIDEWALK POURS ON ABUTMENT WINGWALLS SHALL BE ITEM 18X, CLASS 1A CONCRETE FOR STRUCTURES.

THE CONCRETE IN THE PIER FOOTINGS, ABUTMENTS AND WINGWALLS (EXCEPT IN SIDEWALK POURS) SHALL BE ITEM 20Y, CLASS 1 CONCRETE.

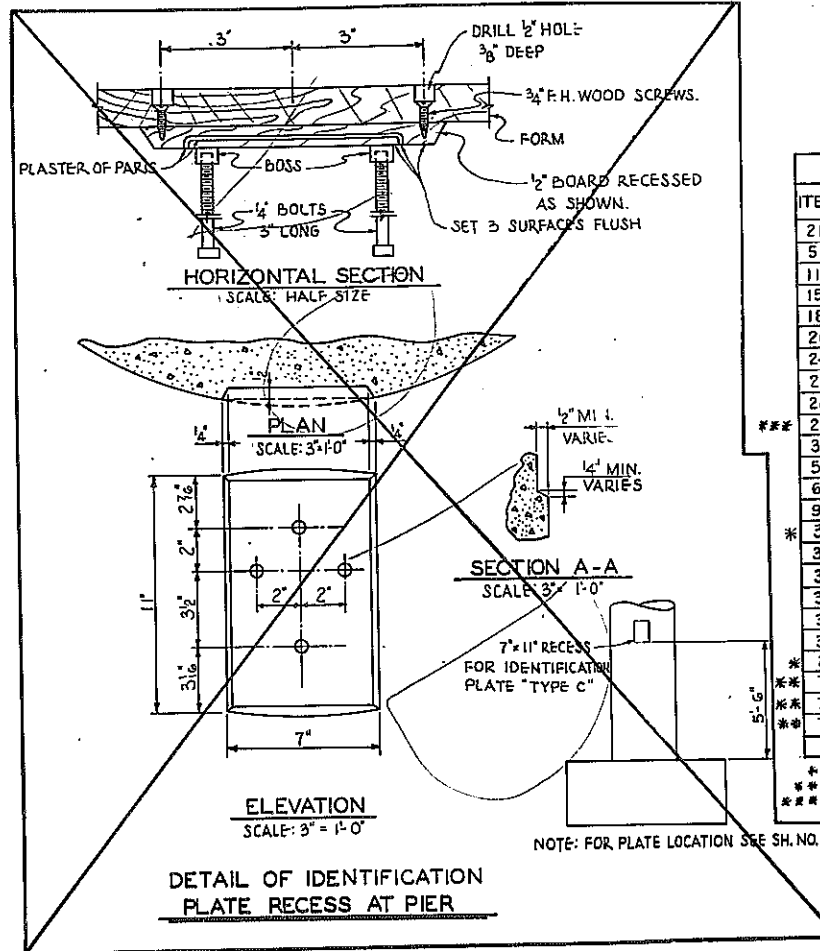
CEMENT : CEMENT USED IN THE CONCRETE ITEMS SHALL BE PORTLAND CEMENT TYPE 2A. ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.

IDENTIFICATION PLATE : ONE IDENTIFICATION PLATE WILL BE FURNISHED TO THE CONTRACTOR FREE OF CHARGE FOR INSTALLATION BY HIM IN ACCORDANCE WITH STD. SHEET 60-41.

OTHER : REINFORCEMENT BARS SHALL BE LAPPED 20 DIAMETERS, UNLESS OTHERWISE NOTED. SIDEWALKS SHALL NOT BE SCORED. SURFACE OF STRUCTURAL CONCRETE EXPOSED TO VIEW SHALL BE RUBBED IF ORDERED BY THE ENGINEER. TOPS OF BACKWALLS OF ABUTMENTS SHALL BE STEEL TROWEL FINISHED.
SURFACE OF ABUTMENT AND PIER PEDESTALS SHALL BE POURED 1/4" HIGHER THAN ELEVATION SPECIFIED AND BUSH HAMMERED TO EXACT ELEVATION. STRINGERS SHALL BE WELDED TO SOLE PLATES AFTER THE BEARINGS HAVE BEEN SET AND ALIGNED TO THEIR PROPER POSITIONS ON THE BRIDGE SEATS.
THE MINIMUM WAITING PERIOD BEFORE LOADS ARE PERMITTED ON THE STRUCTURE AFTER THE STRUCTURAL SLAB IS POURED, SHALL BE 10 DAYS FOR LOADS OF 2 TO 10 TONS, AND 28 DAYS FOR LOADS OF 10 TONS TO LEGAL LOAD LIMIT. CONSTRUCTION JOINTS, OTHER THAN THOSE SHOWN, WILL NOT BE PERMITTED WITHOUT WRITTEN PERMISSION OF THE DEPUTY CHIEF ENGINEER (BRIDGES). THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER, COMPRESSED ASBESTOS SHEET PACKING, PRISM G.C. MATERIAL AND ASPHALT ROOFING FELT, SHALL BE INCLUDED IN THE PRICES BID FOR THE VARIOUS ITEMS IN THIS CONTRACT.

ABUTMENT FOOTINGS : THE ELEVATION OF SOUND ROCK FOR ABUTMENT FOOTINGS IS TENTATIVE AND ACTUAL FINDINGS IN THE FIELD MAY REQUIRE, IF SO ORDERED BY THE DEPUTY CHIEF ENGINEER, THAT THE ABUTMENT DETAILS BE MODIFIED, OR REVISED. WHERE NECESSARY TO EXCAVATE BELOW THE ASSUMED ELEVATION OF BOTTOM OF FOOTINGS OF ABUTMENTS TO OBTAIN A BASE ON SOUND ROCK, SUCH EXCAVATION SHALL BE MADE AND LOOSE ROCK REMOVED TO THE SATISFACTION OF THE ENGINEER. THE AREA BETWEEN THE TOP OF SOUND ROCK AND THE ASSUMED BOTTOM OF ABUTMENT FOOTINGS SHALL BE FILLED WITH ITEM 20Y, CLASS 1 CONCRETE. ROCK REMOVED BEYOND THE LINES AND LEVELS ORDERED BY THE ENGINEER WILL NOT BE PAID FOR AND THE AREA SHALL BE BACKFILLED WITH CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE. EXCAVATION AS ORDERED OR SHOWN ON THE PLANS SHALL BE ITEMS 6E AND 5R TRENCH, CULVERT AND BRIDGE EXCAVATION.

NOTE : FOR GENERAL NOTES, EPOXY PROTECTIVE COATING, SHEAR CONNECTORS, CORRUGATED METAL FORMS AND AMENDMENTS TO PAINT NOTES SPECIFICATIONS SEE SHEET NO. 84.

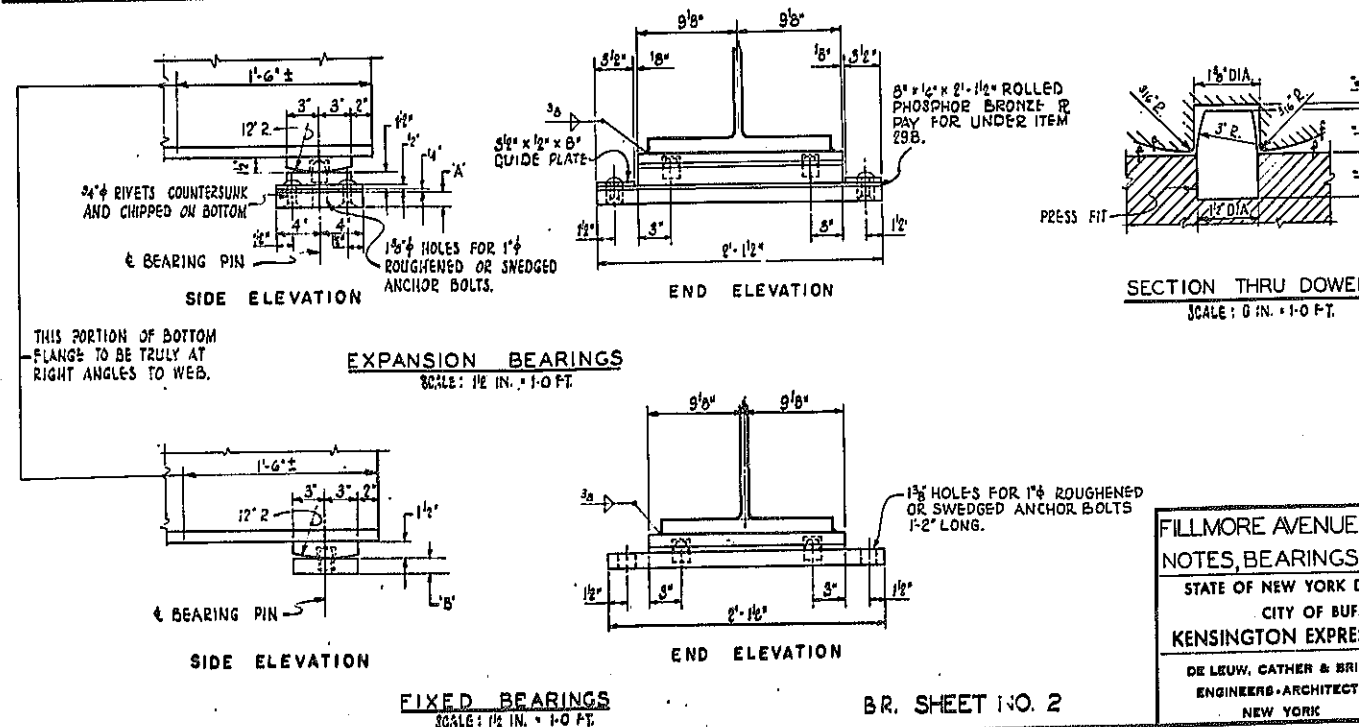


Note: Item 5 replaces Items 5E and 5R where they appear on these plans.

ITEM NO.	DESCRIPTION	UNIT	ESTIMATE OF QUANTITIES		IROQUOIS GAS	
			TOTAL NEAT	TOTAL ROUNDED	TOTAL NEAT	TOTAL ROUNDED
2EF-B	SELECTED FILL	C.Y.	310	340		
5	TRENCH, CULVERT & BRIDGE EXC.	C.Y.	125	140		
11 H	PERF CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	296	305		
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	1,354	1,420		
18X	CLASS 1A CONCRETE FOR STRUCTURES	C.Y.	461	470		
20Y	CLASS 1 CONCRETE	C.Y.	398	410		
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	66	70		
28	BAR REINFORCEMENT FOR STRUCTURES	Lb.	106,700	109,000		
28B	SHEAR CONNECTORS	L.F.	2,023	2,100		
29B	STRUCTURAL STEEL	Lb.	599,400	617,200	4,400	4,500
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	463	480		
52 BX	ASPHALT CONCRETE, TYPE 2 B	TON	161	170		
61	BITUMINOUS MATERIAL	GAL.	170	180		
95XC10	10" STONE CURB (BRIDGE)	L.F.	374	380		
305	MISCELLANEOUS METALS	Lb.	380	390		
301B	2" GALVANIZED STEEL CONDUIT	L.F.	415	430		
301C	2 1/2" GALVANIZED STEEL CONDUIT	L.F.	20	20		
303B	FURNISH & INSTALL LIGHT STD.	Ea.	4	4		
352 XA	RUBBER JOINT MATERIAL	GAL.	16	18		
361C	PROTECTIVE COATING FOR CONCRETE	GAL.	138	150		
20B-16	FURNISH & INSTALL STEEL WATER PIPE (6" DIA)	L.F.	187	190		
701	INSTALL N.M.P. CORR COND. ON FILLMORE AVE. STRUCT.	L.S.	NEC.	NEC.		
703	INSTALL N.Y. TEL. CO. COND. ON FILLMORE AVE. STRUCT.	L.S.	NEC.	NEC.		
704	INSTALLING GAS MAINS	L.S.	NEC.	NEC.		
363C	EPOXY PROTECTIVE COATING FOR CONCRETE	GAL.	24	25		

* INCLUDED IN PUBLICLY OWNED WATER WORKS ESTIMATE.
** PAID FOR BY UTILITIES INVOLVED.
*** Total includes portion paid for by Iroquois Gas & Water Utility.

Note: The pieces of structural steel which make up the 4,500 pounds of structural steel chargeable to the Iroquois Gas Corp. are as follows:
8 pieces of 21 WF 96 diaphragms (D1) plus the pipe supports.



FILLMORE AVENUE OVER EXPRESSWAY
NOTES, BEARINGS AND ESTIMATE
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL	DRAWN	J.S.
ENGINEERS-ARCHITECTS	CHECKED	D.H.
NEW YORK	TRACED	J.S.

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(23)	151	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

24" SLEEVE FOR 16" GAS LINE CUT FROM 1 PC. 24" O.D. BLACK STEEL PIPE 4'-6" LG. WALL t = 0.375" EST. WGT = 426# ITEM 704 TO BE INSTALLED BY THE CONTRACTOR MATERIALS FURNISHED BY OTHERS

20" SLEEVE FOR 12" LP GAS LINE CUT FROM 1 PC. 20" O.D. BLACK STEEL PIPE 4'-2" LG. WALL t = 0.375" EST. WGT = 328# ITEM 704 TO BE INSTALLED BY THE CONTRACTOR MATERIALS FURNISHED BY OTHERS

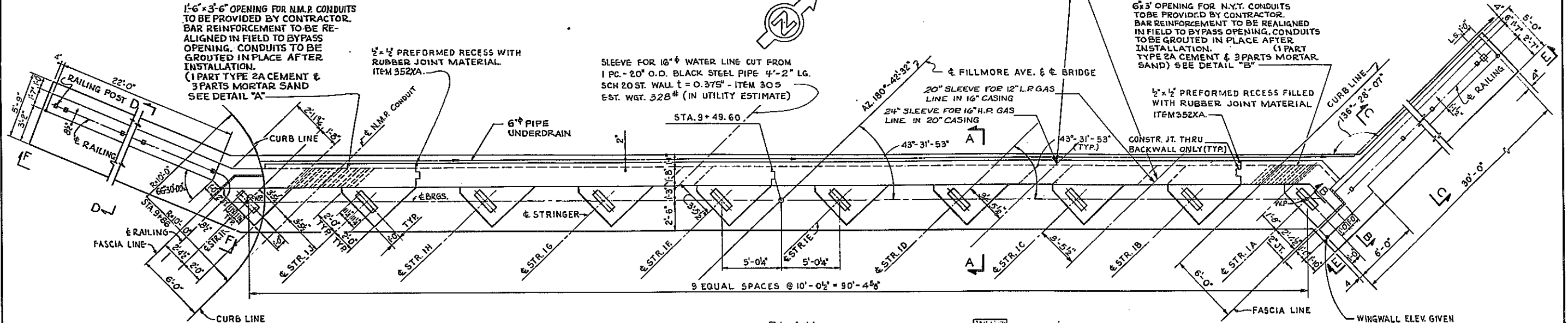
1'-6" x 3'-6" OPENING FOR N.M.P. CONDUITS TO BE PROVIDED BY CONTRACTOR. BAR REINFORCEMENT TO BE REALIGNED IN FIELD TO BYPASS OPENING. CONDUITS TO BE GROUDED IN PLACE AFTER INSTALLATION. (1 PART TYPE 2A CEMENT & 3 PARTS MORTAR SAND SEE DETAIL "A")

1/2" x 1/2" PREFORMED RECESS WITH RUBBER JOINT MATERIAL ITEM 352XA.

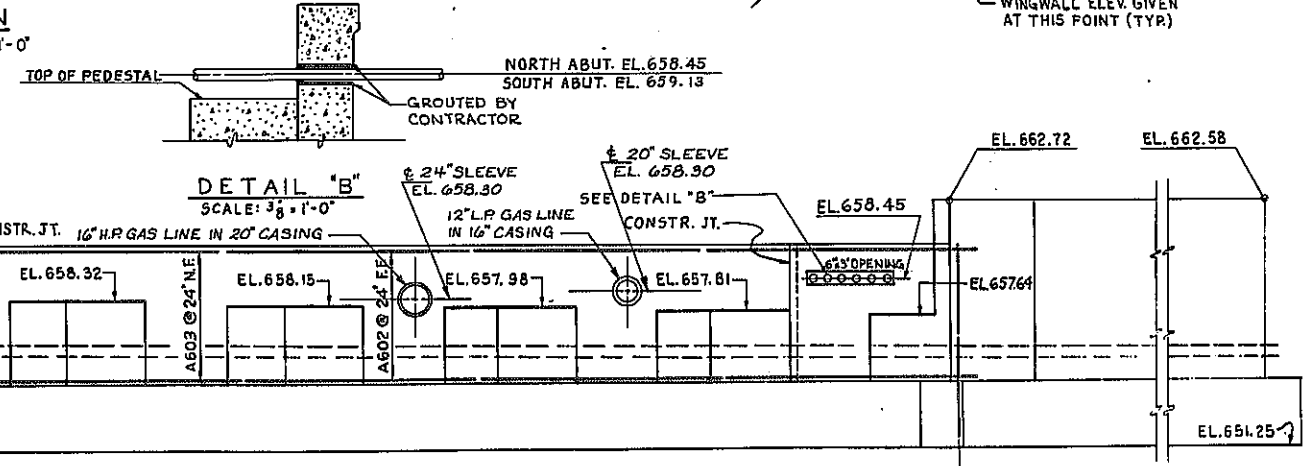
SLEEVE FOR 16" WATER LINE CUT FROM 1 PC. 20" O.D. BLACK STEEL PIPE 4'-2" LG. SCH 20 ST. WALL t = 0.375" - ITEM 305 EST. WGT. 328# (IN UTILITY ESTIMATE)

6' x 3' OPENING FOR N.Y.T. CONDUITS TO BE PROVIDED BY CONTRACTOR. BAR REINFORCEMENT TO BE REALIGNED IN FIELD TO BYPASS OPENING. CONDUITS TO BE GROUDED IN PLACE AFTER INSTALLATION. (1 PART TYPE 2A CEMENT & 3 PARTS MORTAR SAND) SEE DETAIL "B"

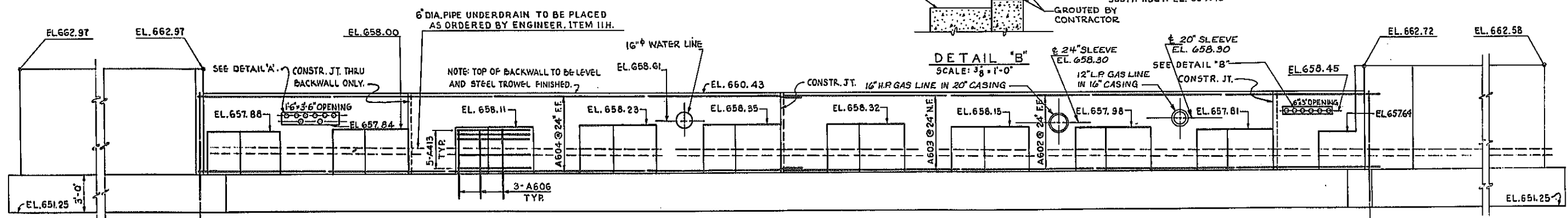
1/2" x 1/2" PREFORMED RECESS FILLED WITH RUBBER JOINT MATERIAL ITEM 352XA.



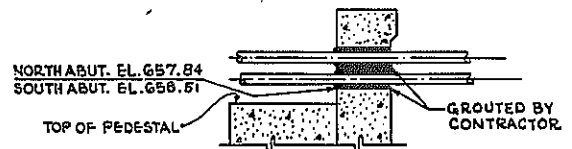
PLAN SCALE: 1/4" = 1'-0"



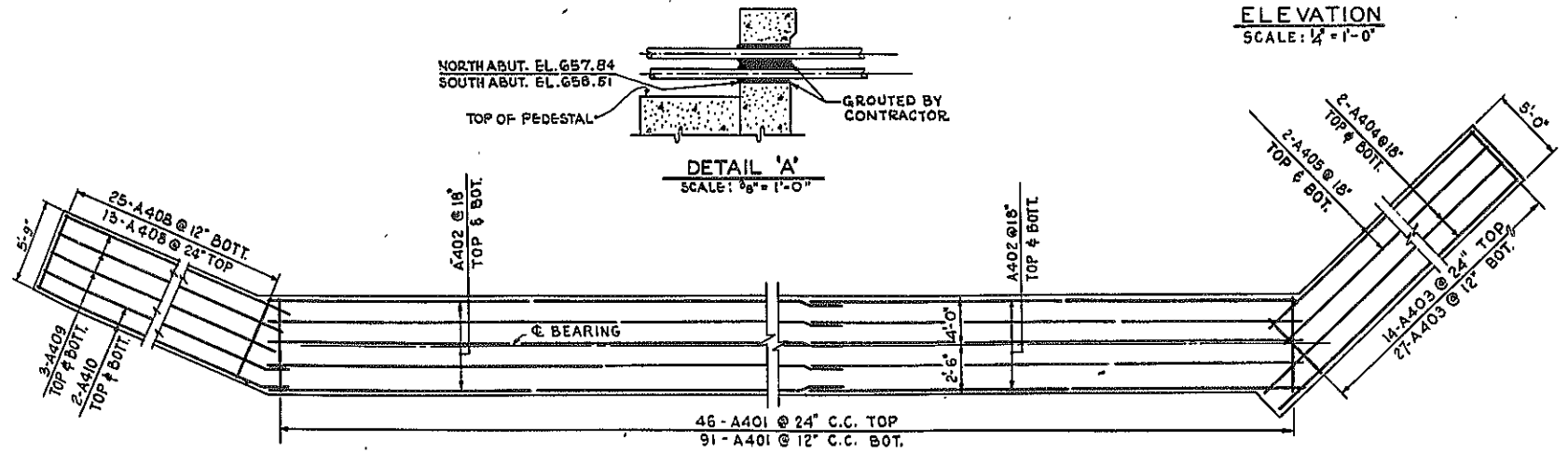
DETAIL "B" SCALE: 3/8" = 1'-0"



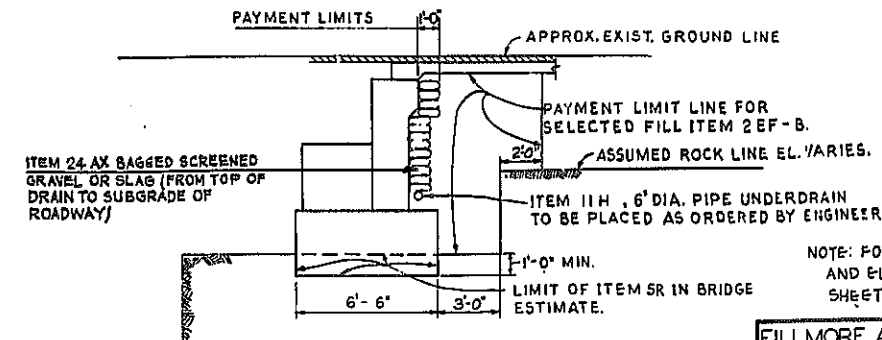
ELEVATION SCALE: 1/4" = 1'-0"



DETAIL "A" SCALE: 3/8" = 1'-0"



FOOTING PLAN SCALE: 3/8" = 1'-0"



EMBANKMENT DETAILS AT ABUTMENTS SCALE: 1/4" = 1'-0"

BAR REINFORCING LEGEND

46	A	A	01
NUMBER OF BARS	ABUTMENT	BAR SIZE	BAR MARK

NOTE: FOR SECTIONS A-A, B-B, C-C & D-D AND ELEVATIONS E-E & F-F, SEE SHEET N9 G.

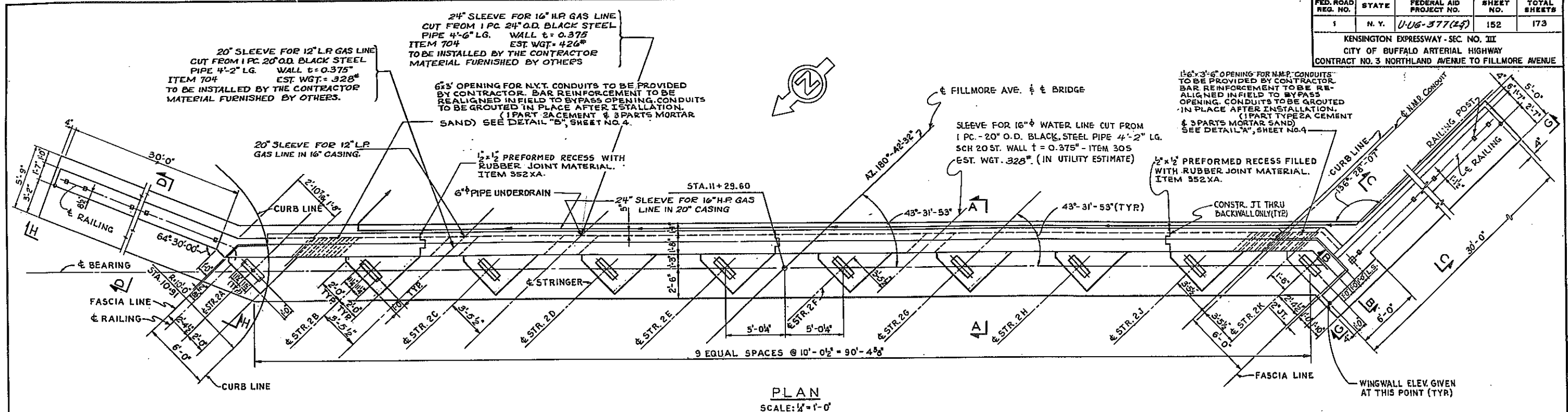
FILLMORE AVENUE OVER EXPRESSWAY NORTH ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

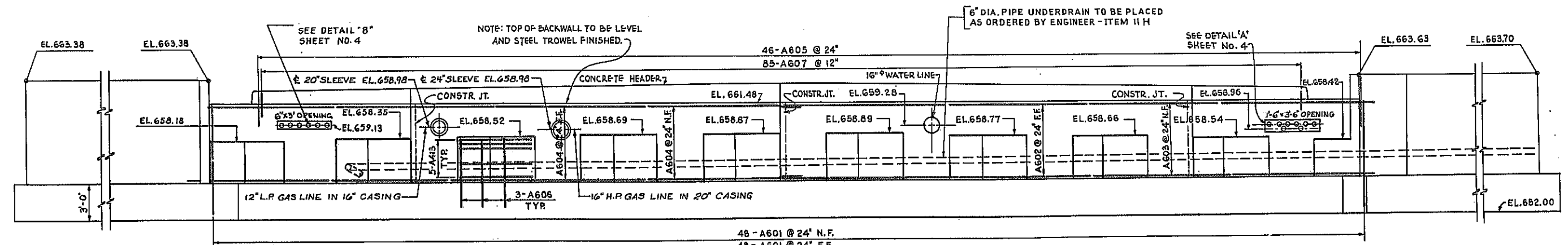
DE LEUW, GATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.L. D.H. E.B.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(24)	152	173

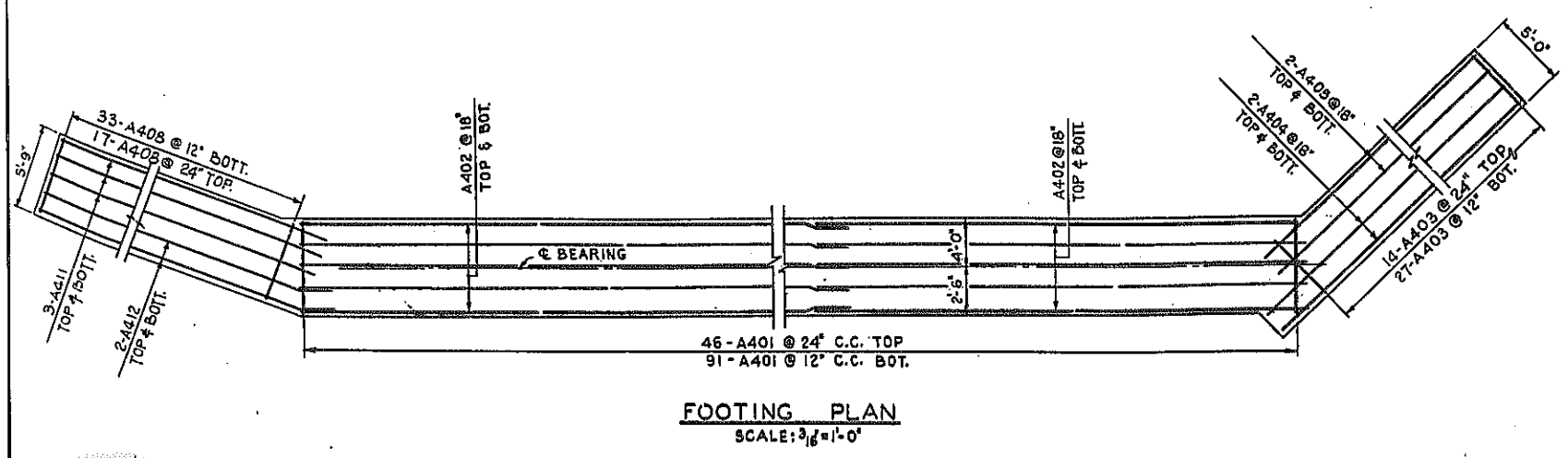
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



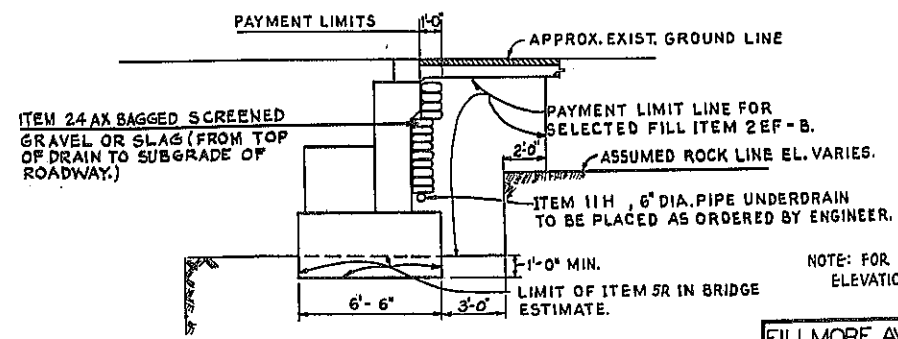
PLAN
SCALE: 1/4" = 1'-0"



ELEVATION
SCALE: 1/4" = 1'-0"



FOOTING PLAN
SCALE: 3/8" = 1'-0"



EMBANKMENT DETAILS AT ABUTMENTS.
SCALE: 1/4" = 1'-0"

BAR REINFORCING LEGEND

46	A	6	01
NUMBER OF BARS	ABUTMENT	BAR SIZE	BAR MARK

NOTE: FOR SECTIONS A-A, B-B, C-C & D-D AND ELEVATIONS F-F & G-G SEE SHEET NO. 6.

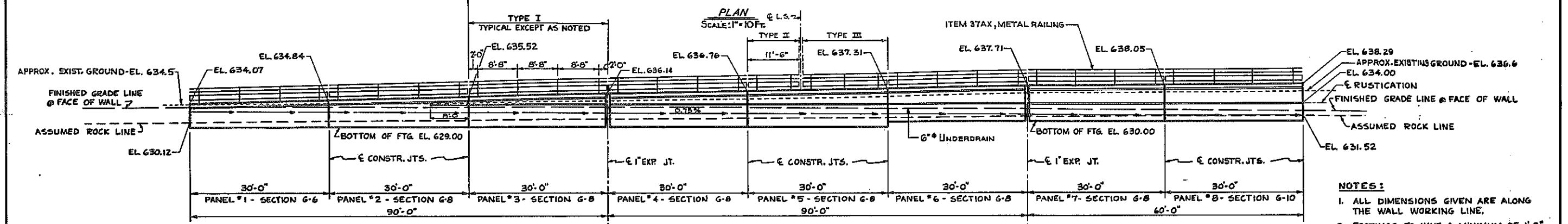
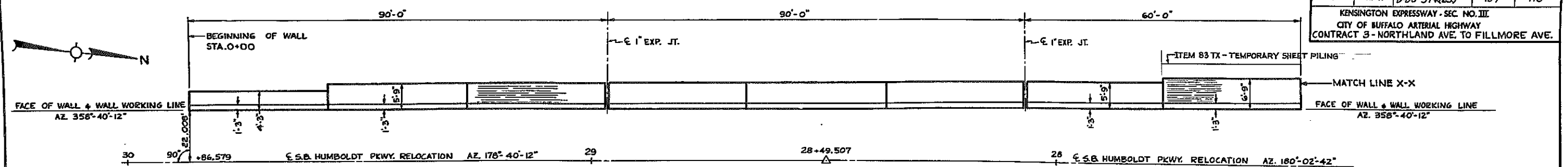
FILLMORE AVENUE OVER EXPRESSWAY
SOUTH ABUTMENT

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.L. D.H. M.L./E
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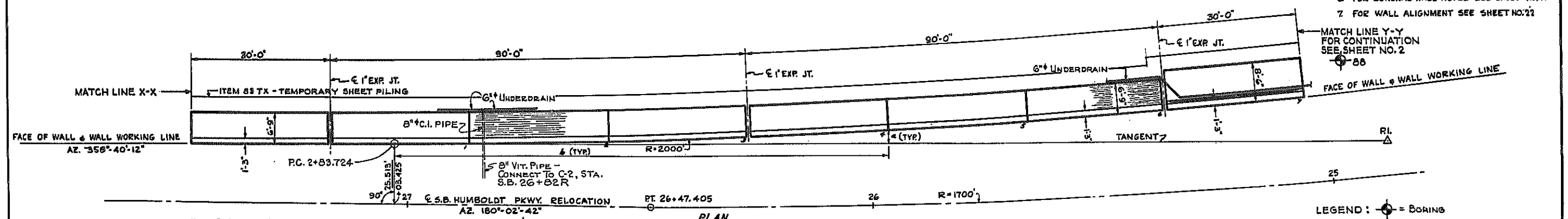
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	D-16-374(25)	159	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT 3 - NORTHLAND AVE. TO FILLMORE AVE.

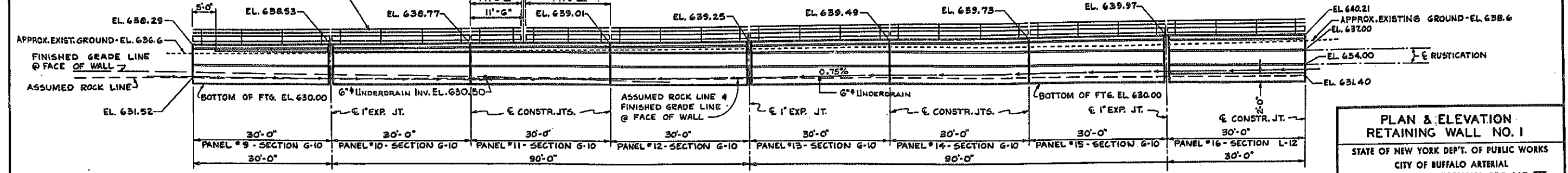


DEVELOPED ELEVATION
 SCALE: 1"=10' FT.

- NOTES:**
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILING SEE SHEET NO. 12
 6. FOR GENERAL WALL NOTES SEE SHEET NO. 11
 7. FOR WALL ALIGNMENT SEE SHEET NO. 22



PLAN
 SCALE: 1"=10' FT.



DEVELOPED ELEVATION
 SCALE: 1"=10' FT.

LEGEND: = BORING

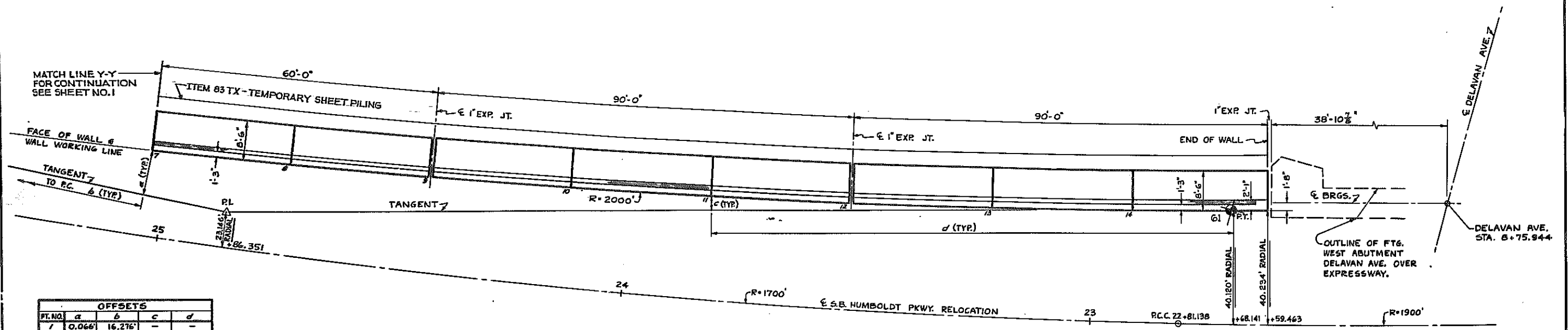
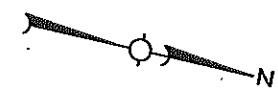
**PLAN & ELEVATION
 RETAINING WALL NO. 1**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

DE LIEW, CATHAR & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D. T.E.K.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-16-377 (25)	160	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT 3 NORTHLAND AVENUE TO FILLMORE

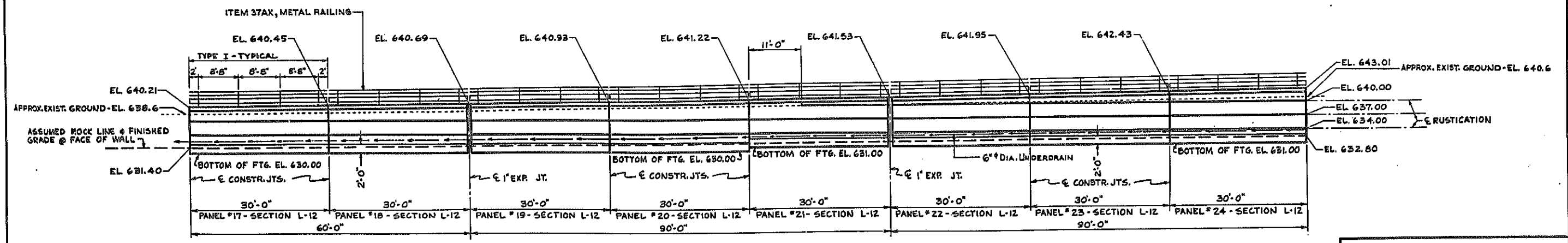


PT. NO.	a	b	c	d
1	0.066'	16.276'	-	-
2	0.535'	46.272'	-	-
3	1.454'	76.257'	-	-
4	2.823'	106.226'	-	-
5	4.641'	136.170'	-	-
6	6.908'	166.084'	-	-
7	9.623'	195.961'	-	-
8	-	-	10.141'	201.154'
9	-	-	7.348'	171.285'
10	-	-	5.008'	141.377'
11	-	-	3.107'	111.457'
12	-	-	1.660'	81.472'
13	-	-	0.663'	51.489'
14	-	-	0.116'	21.495'

R = 2000'
Δ = 12° 15' 17.1"
T = 214.705'
L = 427.771'
PC = 2 + 83.724
PI = 4 + 89.429
PT = 7 + 11.459

PLAN
SCALE: 1" = 10 FT.

- NOTES:
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILING SEE SHEET NO. 12
 6. FOR GENERAL WALL NOTES SEE SHEET NO. 11



DEVELOPED ELEVATION
SCALE: 1" = 10 FT.

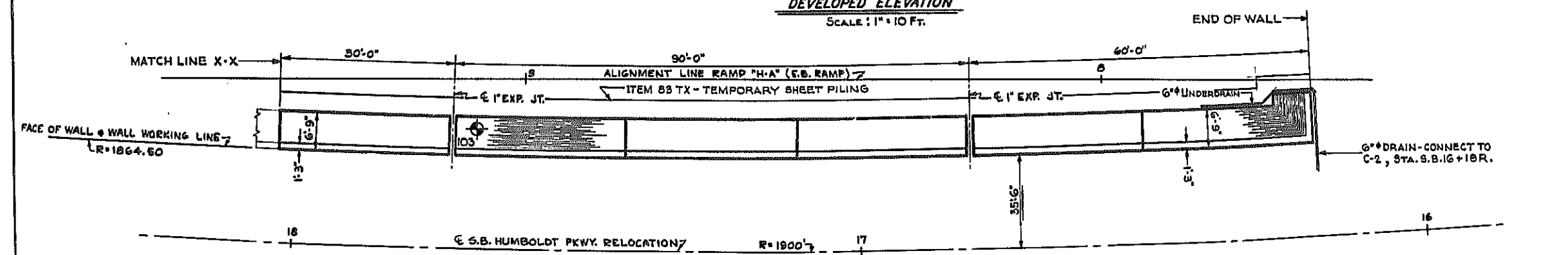
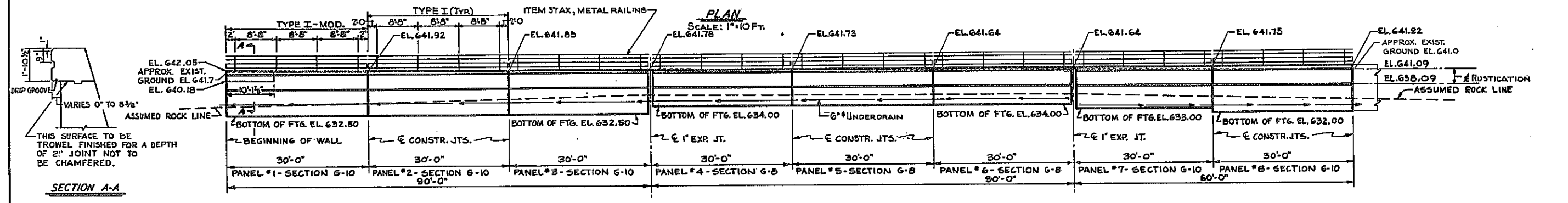
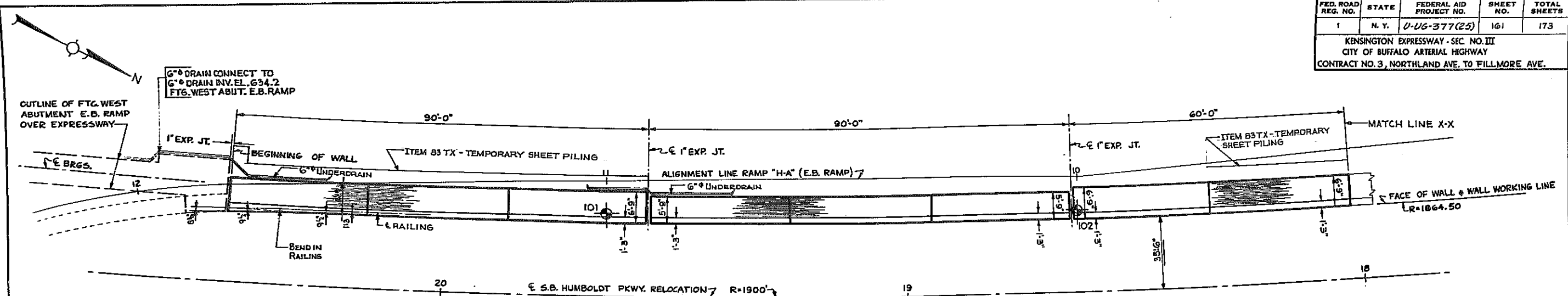
PLAN & ELEVATION
RETAINING WALL NO. 1 [CONT'D]

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	H.M. R.J.D. T.E.K.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	161	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3, NORTHLAND AVE. TO FILLMORE AVE.



- NOTES:**
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 1'-0" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILING SEE SHEET NO. 12
 6. FOR GENERAL WALL NOTES SEE SHEET NO. 11

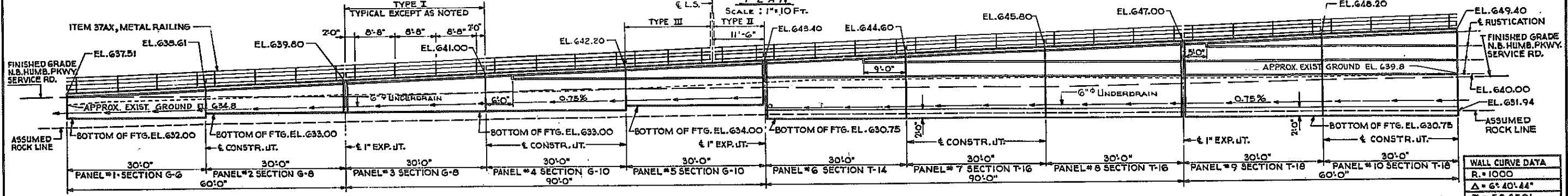
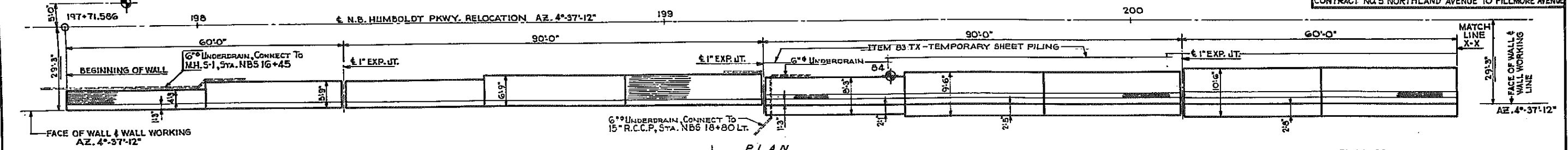
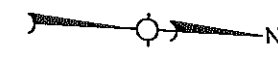
**PLAN & ELEVATION
RETAINING WALL NO. 2**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

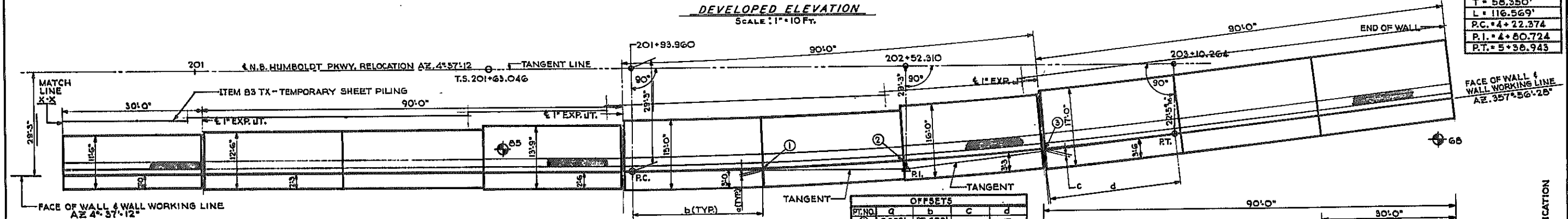
DE LEUW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D. T.E.K.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	162	173

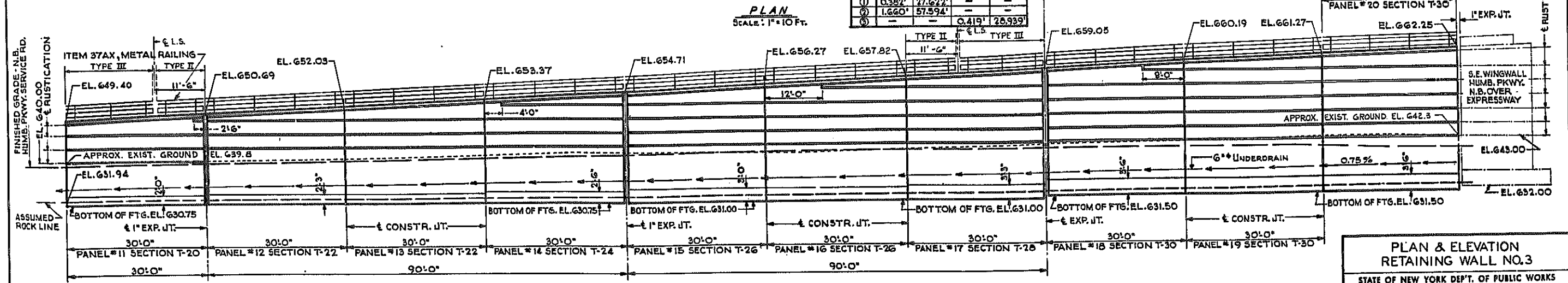
KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



R.	1000
Δ	6° 40' 44"
T	58.350'
L	116.569'
P.C.	4+22.374
P.I.	4+80.724
P.T.	5+38.943



PT. NO.	a	b	c	d
1	0.382'	27.622'	-	-
2	1.660'	57.594'	-	-
3	-	-	0.419'	26.939'



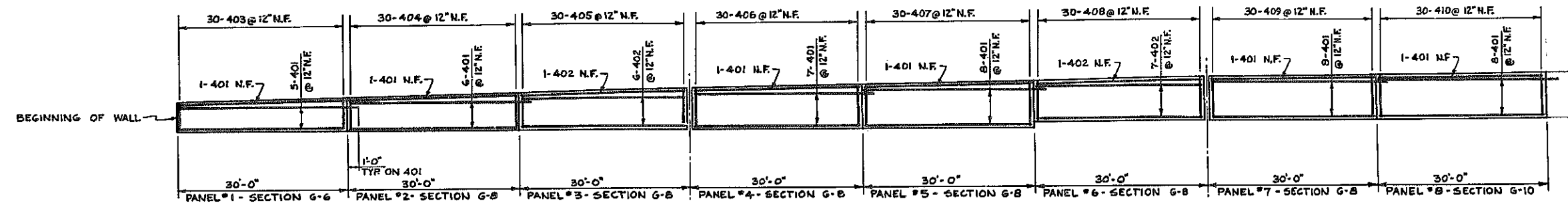
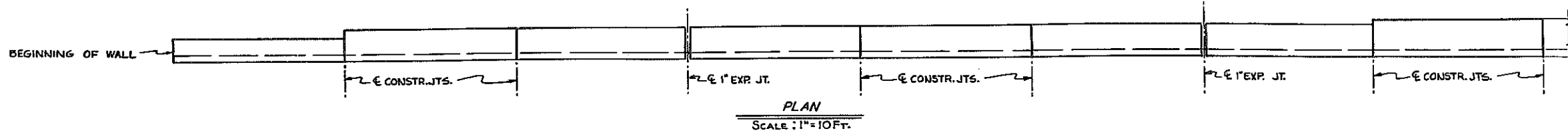
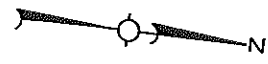
- NOTES:
1. ALL DIMENSIONS GIVEN ARE ALONG THE WALL WORKING LINE.
 2. FOOTINGS TO HAVE A MINIMUM OF 10" EMBEDMENT IN ROCK.
 3. FOR WALL SECTIONS SEE SHEET NO. 9 & 10
 4. FOR DETAILS OF RUSTICATION SEE SHEET NO. 11
 5. FOR DETAILS OF RAILINGS SEE SHEET NO. 12
 6. FOR GENERAL NOTES SEE SHEET NO. 11

PLAN & ELEVATION RETAINING WALL NO. 3

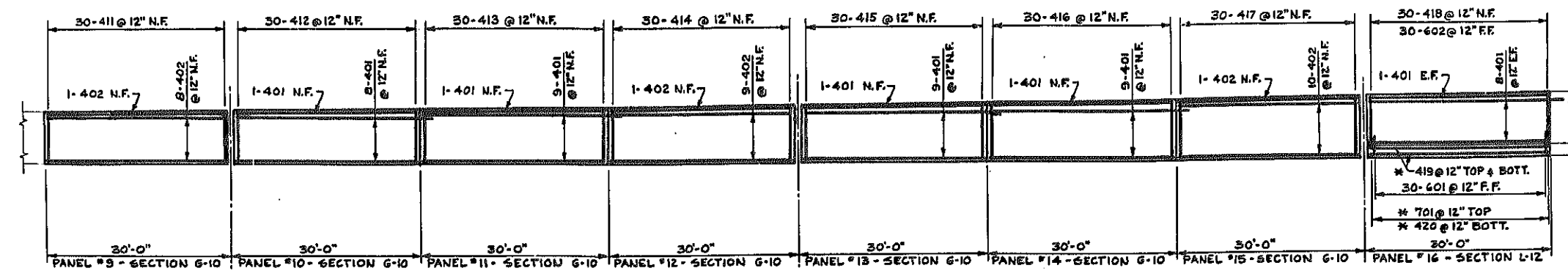
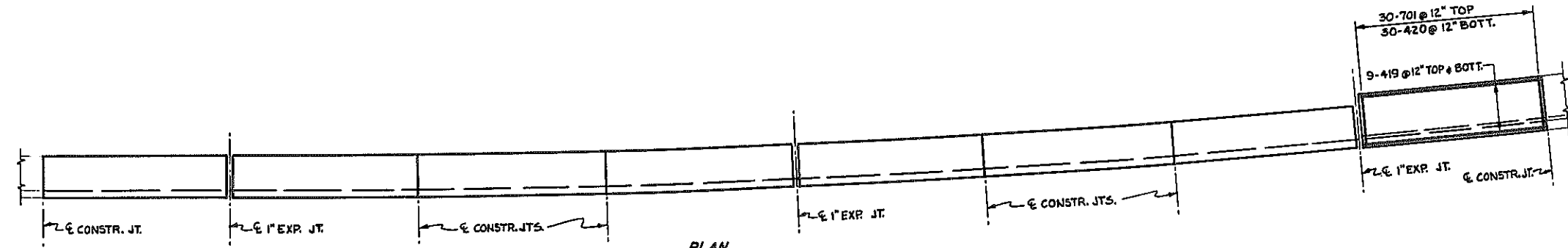
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REK R.J.O.
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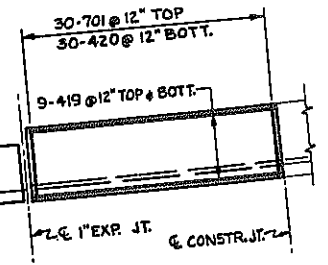
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-26-377(25)	168	173
KENSINGTON EXPRESSWAY - SEC. NO. III CITY OF BUFFALO ARTERIAL HIGHWAY CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE				



ELEVATION
SCALE: 1"=10FT.



ELEVATION
SCALE: 1"=10FT.



BAR REINFORCEMENT LEGEND

7	4	01
↑	↑	↑
NUMBER OF BARS	BAR SIZE	BAR MARK

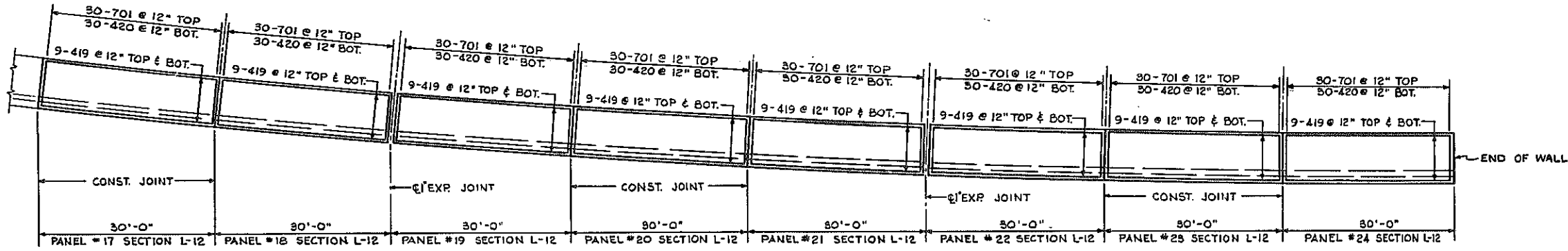
BAR REINFORCEMENT RETAINING WALL NO. 1

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

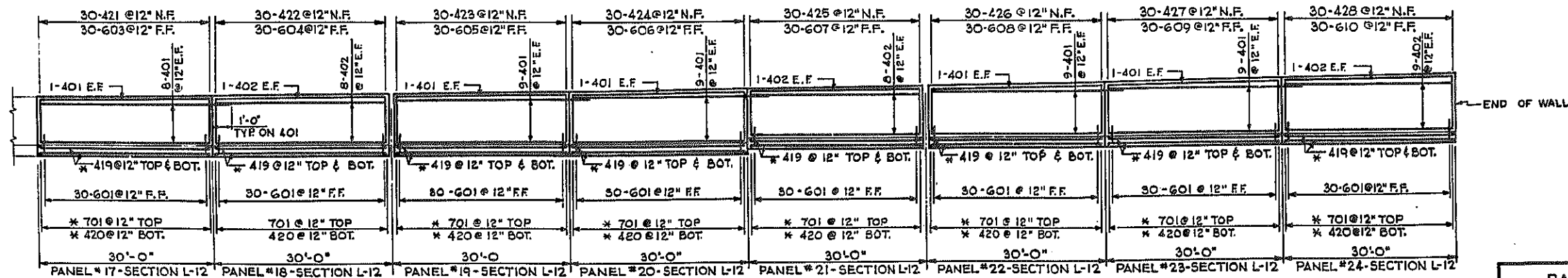
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	M.M. R.J.D.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	164	173

KENSINGTON EXPRESSWAY - SEC. NO. III
 CITY OF BUFFALO ARTERIAL HIGHWAY
 CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



PLAN
 SCALE: 1" = 10 FT.



ELEVATION
 SCALE: 1" = 10 FT.

BAR REINFORCEMENT LEGEND

7	4	01
↑	↑	↑
NUMBER OF BARS	BAR SIZE	BAR MARK

* DUPLICATION

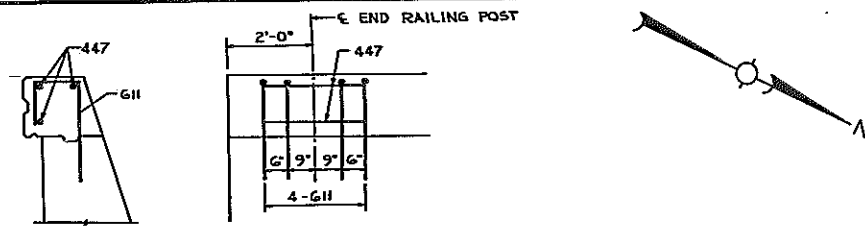
BAR REINFORCEMENT RETAINING WALL NO. I (CONT.)

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
 CITY OF BUFFALO ARTERIAL
 KENSINGTON EXPRESSWAY, SEC. NO. III

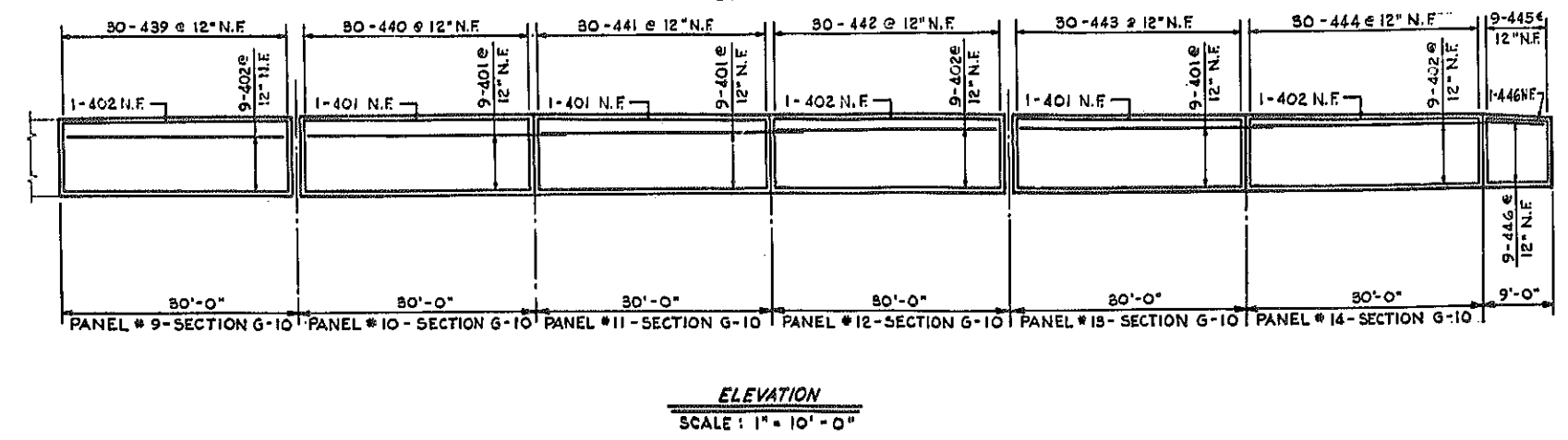
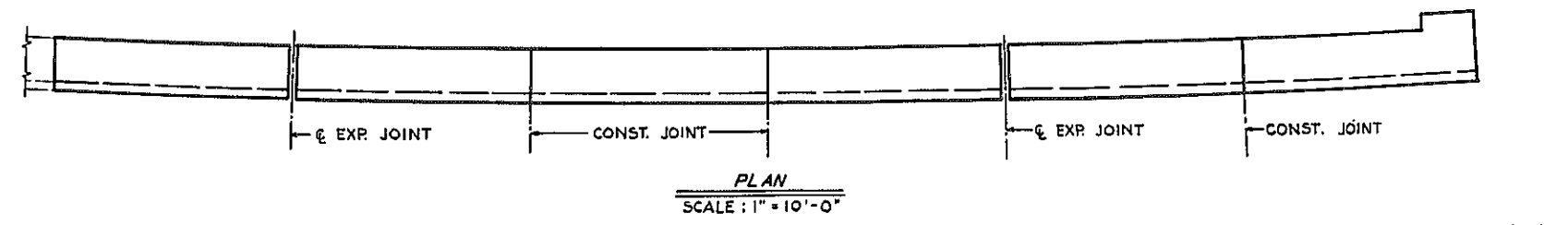
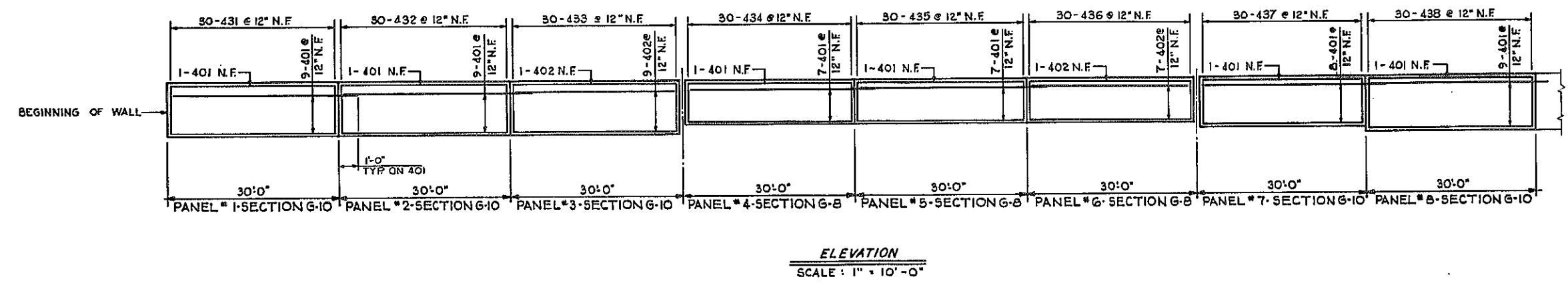
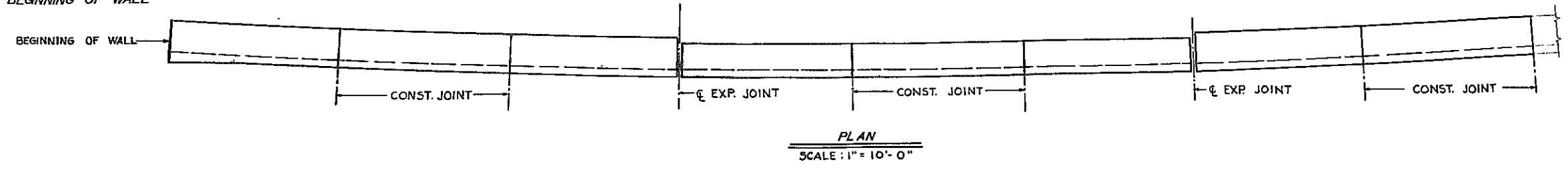
DE LEUW, CATHY & BRILL	DRAWN	M.M.
ENGINEERS-ARCHITECTS	CHECKED	R.J.D.
NEW YORK	TRACED	

FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	165	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



ADDED REINFORCEMENT AT BEGINNING OF WALL



BAR REINFORCEMENT LEGEND

NUMBER OF BARS	BAR SIZE	BAR MARK
7	4	Q

BAR REINFORCEMENT RETAINING WALL NO. 2

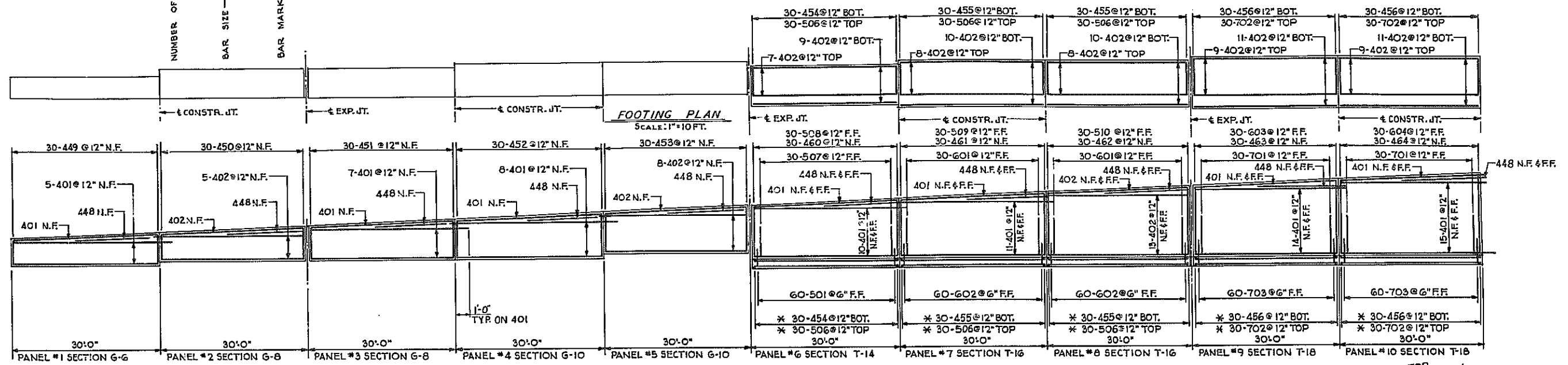
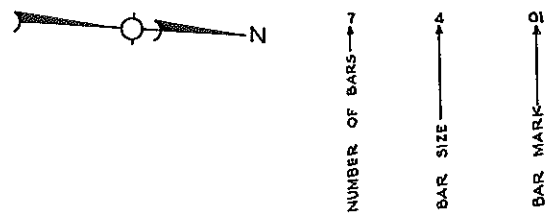
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LIEW, CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	REC. R.I.D.
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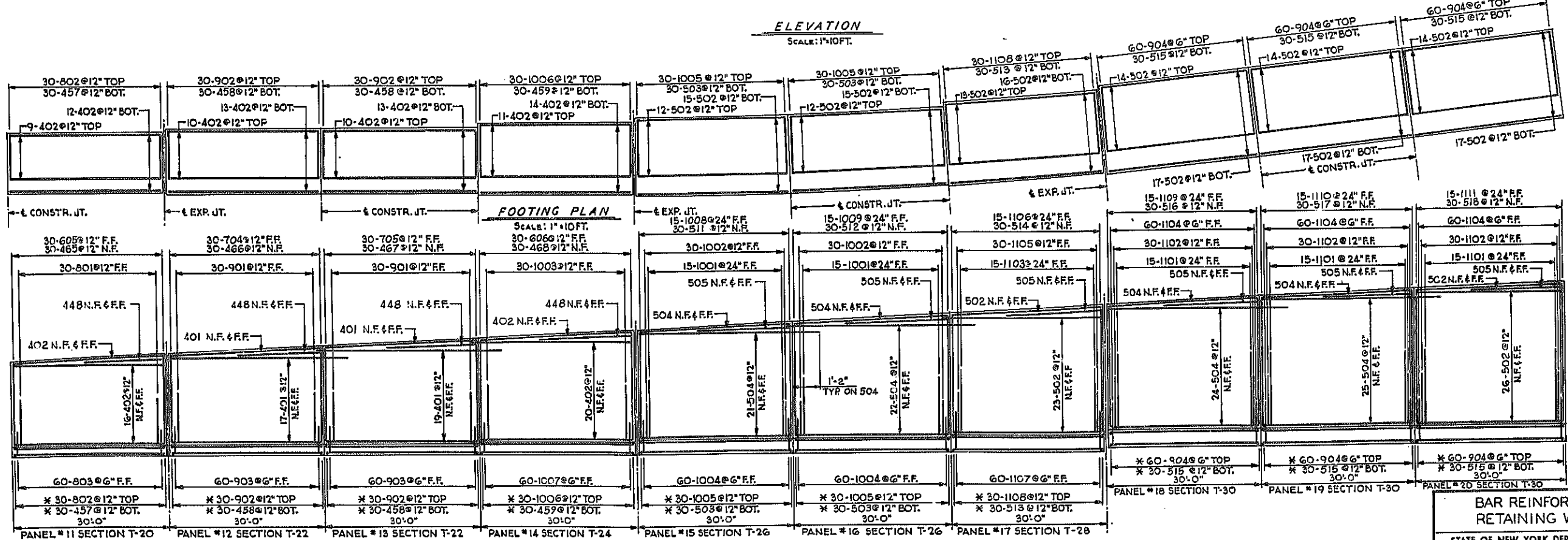
FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	166	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 5 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR REINFORCEMENT LEGEND



ELEVATION
SCALE: 1"=10' FT.



ELEVATION
SCALE: 1"=10' FT.

BAR REINFORCEMENT RETAINING WALL NO. 3

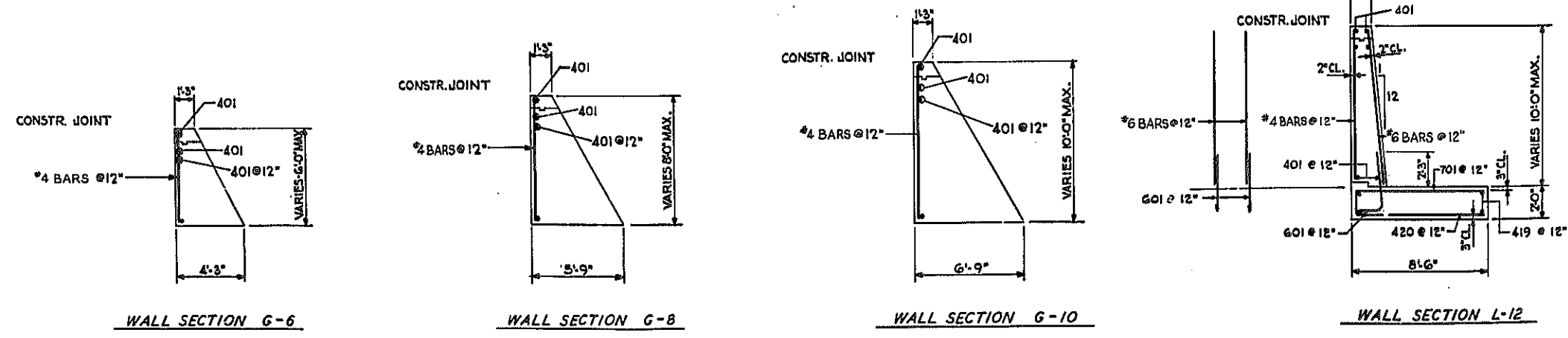
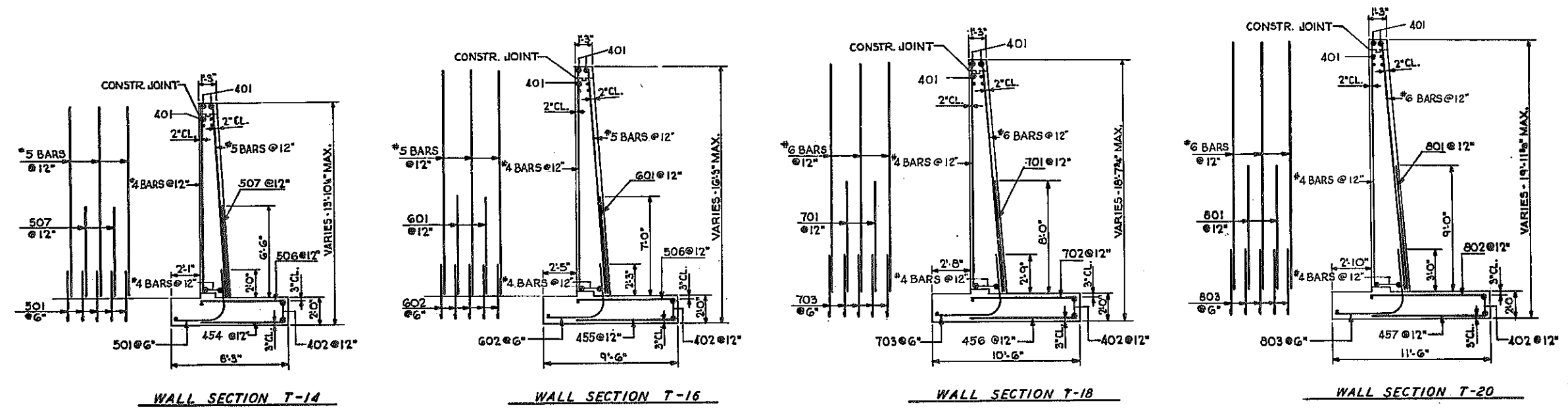
STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACKED	REK R.J.D.
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* DUPLICATION

FED. ROAD DIST. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	167	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



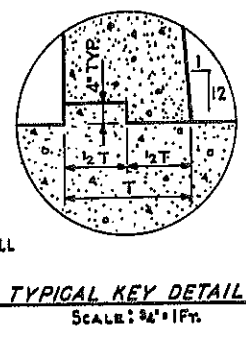
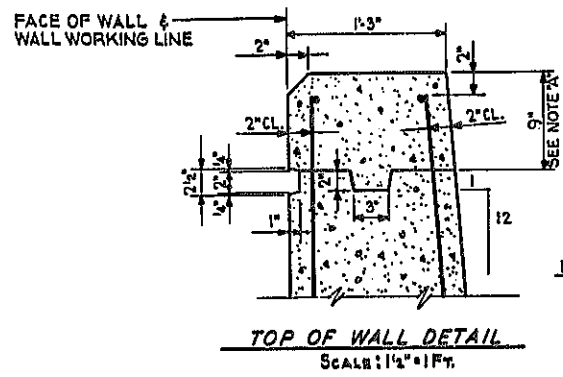
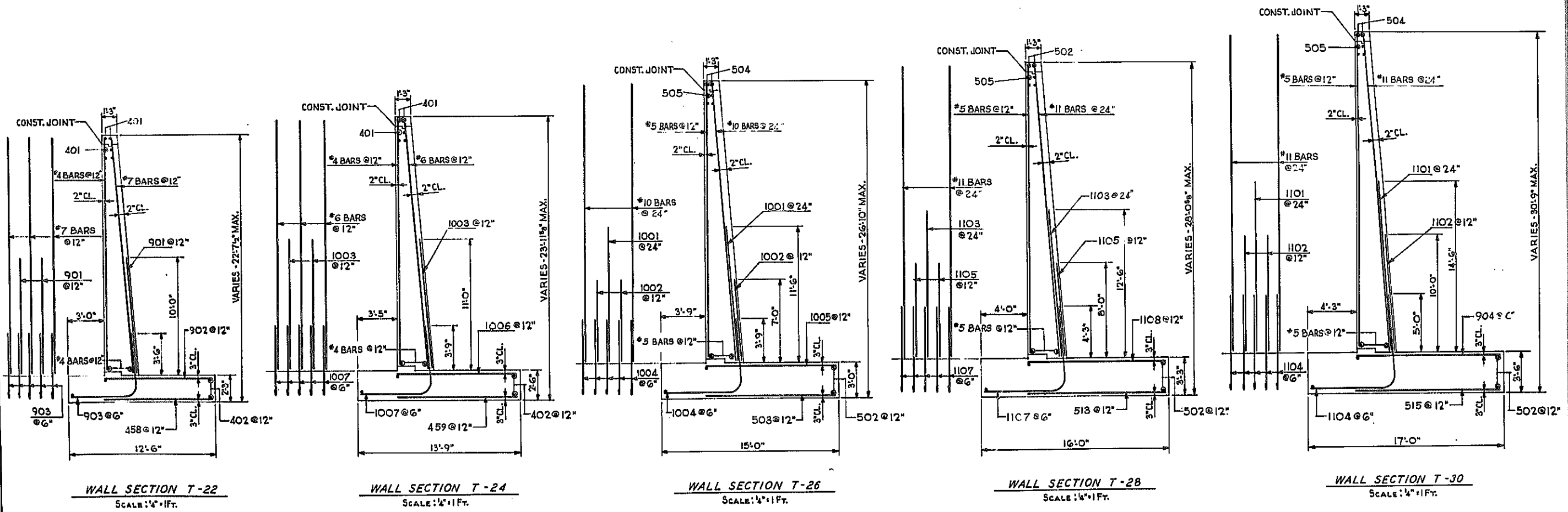
WALL SECTIONS
G-6, G-8, G-10, L-12, T-14, T-16, T-18, T-20

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DR. LEW. CATHY & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	BEK. R.J.D.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-06-377(25)	168	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE



WALL SECTIONS T-22, T-24, T-26, T-28, T-30		
STATE OF NEW YORK DEPT. OF PUBLIC WORKS CITY OF BUFFALO ARTERIAL KENSINGTON EXPRESSWAY, SEC. NO. III		
DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	EEK R.U.P.

FED. ROAD PROJ. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(23)	169	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3, NORTHLAND AVE. TO FILLMORE AVE.

ESTIMATE OF QUANTITIES - WALL NO. 1

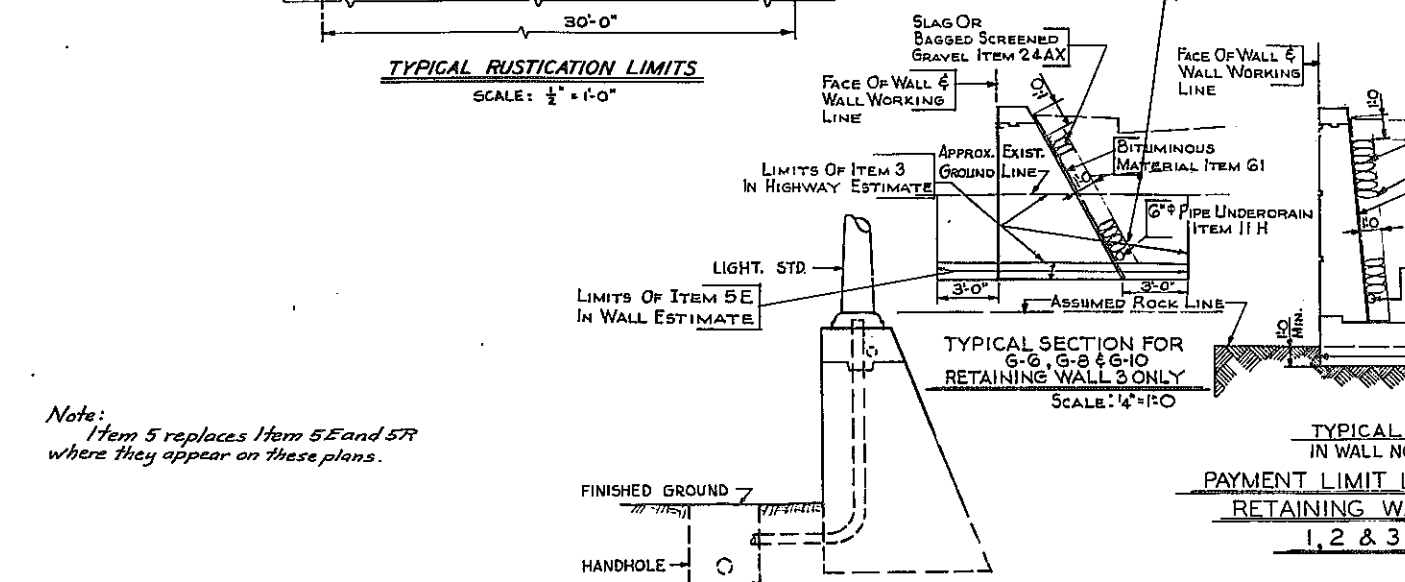
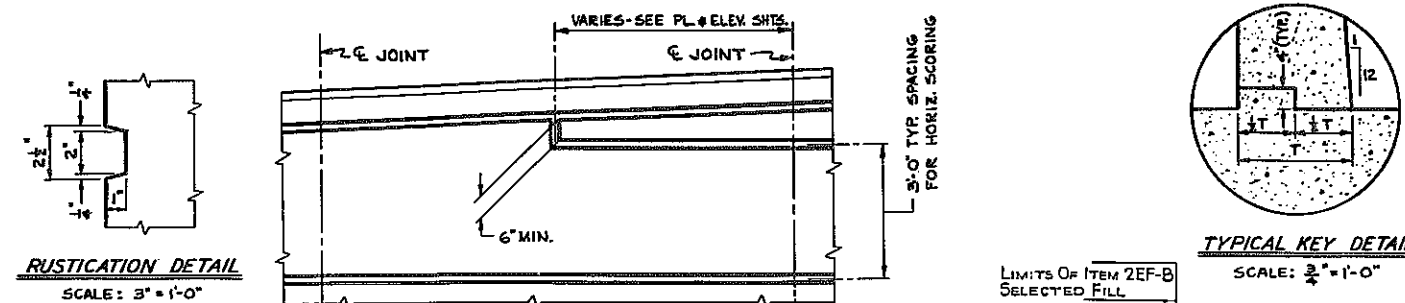
NO.	ITEM	UNIT	NEAT	ROUNDED
2EF-B	SELECTED FILL	C.Y.	1,249	
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	285	
11 H	PERF CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	720	
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	1,287	
18-X	CLASS 1A CONC. FOR STRUCTURES	C.Y.	312	
20Y	CLASS 1 CONCRETE	C.Y.	508	
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	185	
28	BAR REINFORCEMENT FOR STRUCTURES	L.B.	26,315	
29B	STRUCTURAL STEEL	L.B.	2,369	
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	717	
61	BITUMINOUS MATERIAL	Gal.	502	
301B	2" GALV. STEEL CONDUIT	L.F.	335	
301C	2 1/2" GALV. STEEL CONDUIT	L.F.	8	
303C9A	FURNISH & INSTALL LIGHT STDS. 30'MTS. HGT.	Ea.	1	
303C9B	FURNISH & INSTALL LIGHT STDS. 30'MTG. HGT.	Ea.	1	
352XA	RUBBER JOINT MATERIAL	Gal.	16	
9VC	SEWER PIPE (VITRIFIED) 8"	L.F.	7	
13C	CAST IRON PIPE (8")	L.F.	8	
83TX	TEMPORARY SHEET PILING	S.F.	3,468	

ESTIMATE OF QUANTITIES - WALL NO. 2

NO.	ITEM	UNIT	NEAT	ROUNDED
2EF-B	SELECTED FILL	C.Y.	515	
5	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	239	
11 H	PERF CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	460	
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	871	
20Y	CLASS 1 CONCRETE	C.Y.	562	
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	99	
28	BAR REINFORCEMENT FOR STRUCTURES	L.B.	5,330	
29B	STRUCTURAL STEEL	L.B.	1,310	
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	420	
61	BITUMINOUS MATERIAL	Gal.	309	
352XA	RUBBER JOINT MATERIAL	Gal.	9	
83TX	TEMPORARY SHEET PILING	S.F.	1,935	

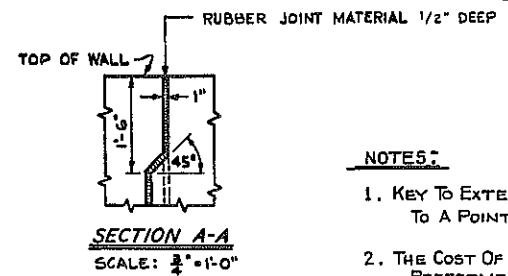
ESTIMATE OF QUANTITIES - WALL NO. 3

NO.	ITEM	UNIT	NEAT	ROUNDED
2EF-B	SELECTED FILL	C.Y.	3,581	
11 H	PERF CORR. METAL PIPE UNDERDRAIN - 6" DIA.	L.F.	600	
15-2A	PORTLAND CEMENT, TYPE 2A	Bbl.	2,279	
18-X	CLASS 1A CONC. FOR STRUCTURES	C.Y.	1,280	
20Y	CLASS 1 CONCRETE	C.Y.	149	
24AX	BAGGED SCREENED GRAVEL OR SLAG	C.Y.	351	
28	BAR REINFORCEMENT FOR STRUCTURES	L.B.	180,901	
29B	STRUCTURAL STEEL	L.B.	2,057	
37AX	METAL RAILING - 4 RAIL MODIFIED	L.F.	598	
61	BITUMINOUS MATERIAL	Gal.	750	
83TX	TEMPORARY SHEET PILING	S.F.	3,673	
301B	2" GALV. STEEL CONDUIT	L.F.	615	
301C	2 1/2" GALV. STEEL CONDUIT	L.F.	15	
303C15	FURNISH & INSTALL LIGHT STDS. 30'MTG. HGT.	Ea.	3	
352XA	RUBBER JOINT MATERIAL	Gal.	18	
9VC	SEWER PIPE (VITRIFIED) 8"	L.F.	35	
13C	CAST IRON PIPE (8")	L.F.	4	
51	TRENCH, CULVERT & BRIDGE EXCAVATION	C.Y.	1,873	

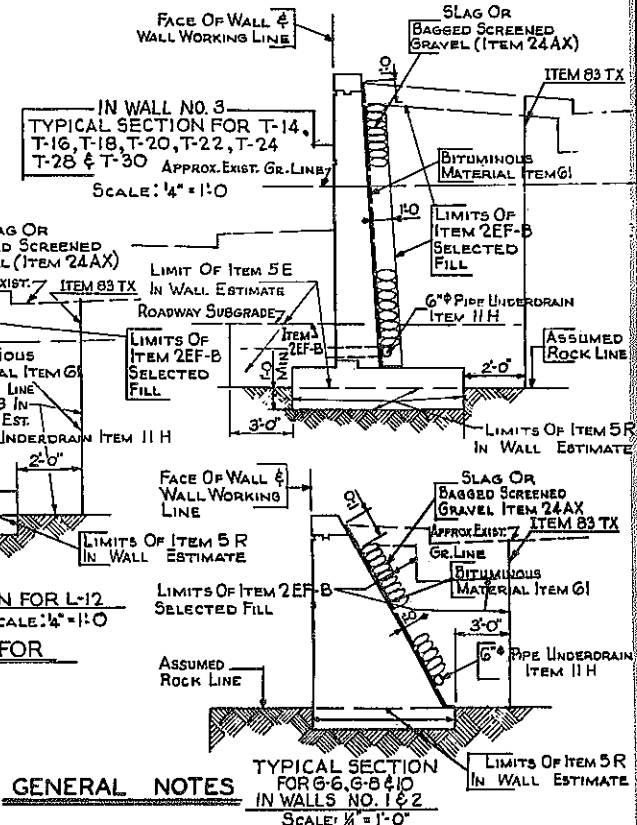
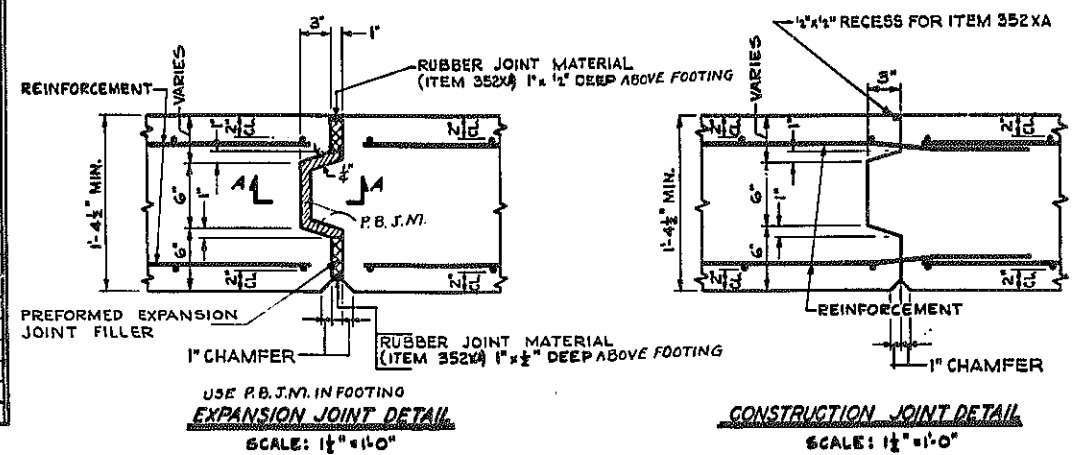


Note: Item 5 replaces Item 5E and 5R where they appear on these plans.

SECTION - PANEL 5 & 11 - WALL NO. 1 ELECTRICAL CONDUIT



- NOTES:
- KEY TO EXTEND FROM TOP OF FOOTING TO A POINT 1'-6" BELOW TOP OF WALL.
 - THE COST OF FURNISHING AND INSTALLING PREFORMED EXPANSION JOINT FILLER AND R.B.J.M. SHALL BE INCLUDED IN THE PRICES BID FOR ITEMS 18X AND 20Y.



DESIGN SPECIFICATIONS: A.A.S.M.O. 1957 AND CURRENT MODIFICATIONS.

- CONCRETE: CONCRETE IN GRAVITY WALL SECTIONS SHALL BE ITEM 20Y CLASS 1 CONCRETE.
- CEMENT: ALL CEMENT USED IN THE CONCRETE ITEMS FOR WALLS SHALL BE PORTLAND CEMENT TYPE 2A ITEM 15-2A, ITEMS 18X AND 20Y TO BE FURNISHED WITH RETARDING DENSIFIER.
- SOIL LOADS: FOR DESIGN PURPOSES THE MAXIMUM SOIL PRESSURE ON ROCK DOES NOT EXCEED 5 TONS PER SQUARE FOOT AND ON EARTH DOES NOT EXCEED 1.5 TONS PER SQUARE FOOT.
- FOOTINGS: ALL DISINTEGRATED OR SHATTERED MATERIAL SHALL BE REMOVED TO LINES AND LEVELS ORDERED BY THE ENGINEER. WHERE SOUND ROCK IS FOUND BELOW THE PLANNED LEVELS OF THE BOTTOM OF FOOTINGS BACKFILL OF CLASS 1 CONCRETE - ITEM 20Y SHALL BE INSTALLED TO THE LEVELS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER. ROCK REMOVED BELOW THE LEVELS DIRECTED BY THE ENGINEER AND OUTSIDE THE NEAT LINES MUST BE REPLACED BY BACKFILL OF CLASS 2 CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.
- LIMITS OF ITEM 61 APPLICATION SHALL BE FROM TOP OF FOOTING TO FINISHED GRADE LINE AT BACK OF WALL.

DETAILS
RETAINING WALLS 1, 2 & 3

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHEN & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	T.E.K. R.L.O. T.E.K.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377 (25)	171	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR LIST

RETAINING WALL NO. I									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #1 SECTION G-6									
401	4	30'-10"	STR	5				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
403	4	5'-0"	STR	30				VERT. BARS (VAR. 4'-8" TO 5'-2", 1/4" INC.)	
PANEL #2 SECTION G-8									
401	4	30'-10"	STR	6				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
404	4	5'-8"	STR	30				VERT. BARS (VAR. 5'-5" TO 6'-0", 1/4" INC.)	
PANEL #3 SECTION G-8									
402	4	29'-8"	STR	6				HORIZ. BARS	
402	4	29'-8"	STR	1				HORIZ. CAP BAR	
405	4	6'-2"	STR	30				VERT. BARS (VAR. 6'-1" TO 6'-8", 1/4" INC.)	
PANEL #4 SECTION G-8									
401	4	30'-10"	STR	7				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
406	4	7'-0"	STR	30				VERT. BARS (VAR. 6'-9" TO 7'-4", 1/4" INC.)	
PANEL #5 SECTION G-8									
401	4	30'-10"	STR	8				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
407	4	7'-8"	STR	30				VERT. BARS (VAR. 7'-4" TO 7'-11", 1/4" INC.)	
PANEL #6 SECTION G-8									
402	4	29'-8"	STR	7				HORIZ. BARS	
402	4	29'-8"	STR	1				HORIZ. CAP BAR	
408	4	7'-3"	STR	30				VERT. BARS (VAR. 6'-10" TO 7'-5", 1/4" INC.)	
PANEL #7 SECTION G-8									
401	4	30'-10"	STR	8				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
409	4	7'-5"	STR	30				VERT. BARS (VAR. 7'-4" TO 7'-5", 1/4" INC.)	
PANEL #8 SECTION G-10									
401	4	30'-10"	STR	8				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
410	4	7'-5"	STR	30				VERT. BARS (VAR. 7'-7", 1/4" TO 7'-10", 1/4" INC.)	
PANEL #9 SECTION G-10									
402	4	29'-8"	STR	8				HORIZ. BARS	
402	4	29'-8"	STR	1				HORIZ. CAP BAR	
411	4	7'-1"	STR	30				VERT. BARS (VAR. 7'-10" TO 8'-1", 1/4" INC.)	
PANEL #10 SECTION G-10									
401	4	30'-10"	STR	8				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
412	4	8'-2"	STR	30				VERT. BARS (VAR. 8'-1" TO 8'-4", 1/4" INC.)	
PANEL #11 SECTION G-10									
401	4	30'-10"	STR	9				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
413	4	8'-4"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-7", 1/4" INC.)	
PANEL #12 SECTION G-10									
402	4	29'-8"	STR	9				HORIZ. BARS	
402	4	29'-8"	STR	1				HORIZ. CAP BAR	
414	4	8'-8"	STR	30				VERT. BARS (VAR. 8'-7" TO 8'-10", 1/4" INC.)	
PANEL #13 SECTION G-10									
401	4	30'-10"	STR	9				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
415	4	8'-11"	STR	30				VERT. BARS (VAR. 8'-9", 1/4" TO 9'-1", 1/4" INC.)	
PANEL #14 SECTION G-10									
401	4	30'-10"	STR	9				HORIZ. BARS	
401	4	30'-10"	STR	1				HORIZ. CAP BAR	
416	4	9'-2"	STR	30				VERT. BARS (VAR. 9'-0", 1/4" TO 9'-4", 1/4" INC.)	
PANEL #15 SECTION G-10									
402	4	29'-8"	STR	10				HORIZ. BARS	
402	4	29'-8"	STR	1				HORIZ. CAP BAR	
417	4	9'-3"	STR	30				VERT. BARS (VAR. 9'-3", 1/4" TO 9'-7", 1/4" INC.)	
PANEL #16 SECTION L-12									
401	4	30'-10"	STR	16				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
418	4	7'-5"	STR	30				VERT. BARS (VAR. 7'-3", 1/4" TO 7'-7", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
602	6	7'-6"	STR	30				VERT. BARS	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	

RETAINING WALL NO. I									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #17 SECTION L-12									
401	4	30'-10"	STR	16				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
421	4	7'-5"	STR	30				VERT. BARS (VAR. 7'-6" TO 7'-9", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
603	6	7'-9"	STR	30				VERT. BARS	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #18 SECTION L-12									
402	4	29'-8"	STR	16				HORIZ. BARS	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
422	4	7'-10"	STR	30				VERT. BARS (VAR. 7'-9" TO 8'-0", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
604	6	8'-2"	STR	30				VERT. BARS (VAR. 8'-1" TO 8'-4", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #19 SECTION L-12									
401	4	30'-10"	STR	18				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
423	4	8'-1"	STR	30				VERT. BARS (VAR. 8'-0" TO 8'-3", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
605	6	8'-5"	STR	30				VERT. BARS (VAR. 8'-4" TO 8'-7", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #20 SECTION L-12									
401	4	30'-10"	STR	18				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
424	4	8'-4"	STR	30				VERT. BARS (VAR. 8'-3" TO 8'-6", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
606	6	8'-8"	STR	30				VERT. BARS (VAR. 8'-7" TO 8'-10", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #21 SECTION L-12									
402	4	29'-8"	STR	16				HORIZ. BARS	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
425	4	7'-8"	STR	30				VERT. BARS (VAR. 7'-7" TO 7'-10", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
607	6	8'-0"	STR	30				VERT. BARS (VAR. 7'-11" TO 8'-2", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #22 SECTION L-12									
401	4	30'-10"	STR	18				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
426	4	8'-1"	STR	30				VERT. BARS (VAR. 7'-10" TO 8'-5", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
608	6	8'-5"	STR	30				VERT. BARS (VAR. 8'-2" TO 8'-9", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #23 SECTION L-12									
401	4	30'-10"	STR	18				HORIZ. BARS	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
427	4	8'-6"	STR	30				VERT. BARS (VAR. 8'-3" TO 8'-10", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
609	6	8'-10"	STR	30				VERT. BARS (VAR. 8'-7" TO 9'-2", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	
PANEL #24 SECTION L-12									
402	4	29'-8"	STR	16				HORIZ. BARS	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
428	4	9'-0"	STR	30				VERT. BARS (VAR. 8'-9", 1/4" TO 9'-4", 1/4" INC.)	
419	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT.	
420	4	7'-1"	STR	30				TRANS. FTG. BARS, BOT.	
601	6	5'-7"	II	30	1'-5"	0'-4"	3'-9"	DOWELS	
610	6	9'-4"	STR	30				VERT. BARS (VAR. 9'-1", 1/4" TO 9'-5", 1/4" INC.)	
701	7	11'-2"	III	30	8'-0"	1'-6"	1'-8"	TRANS. FTG. BARS, TOP	

RETAINING WALLS 1,2,&3
BAR LIST

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	D.E.L. R.J.D.
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	U-UG-377(25)	172	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

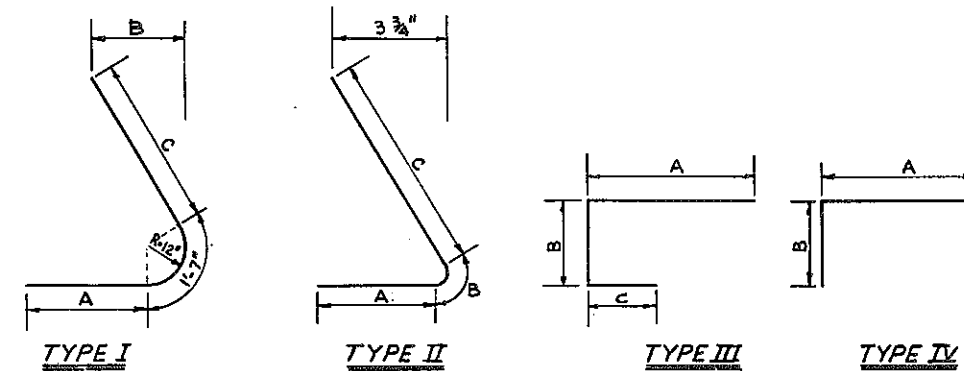
BAR LIST

RETAINING WALL NO. 2

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
PANEL #1 SECTION G-10 (MODIFIED)								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
431	4	9'-1"	STR	30				VERT. BARS
447	4	2'-6"	STR	3				HORIZ. CAP BARS (MODIFIED)
G11	G	6'-7"	III	4	3'-8"	1'-5"	1'-6"	VERT. CAP BARS (MODIFIED)
PANEL #2 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
432	4	9'-0"	STR	30				VERT. BARS
PANEL #3 SECTION G-10								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR
433	4	8'-11"	STR	30				VERT. BARS
PANEL #4 SECTION G-8								
401	4	30'-10"	STR	7				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BARS
434	4	7'-4"	STR	30				VERT. BARS
PANEL #5 SECTION G-8								
401	4	30'-10"	STR	7				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
435	4	7'-3"	STR	30				VERT. BARS
PANEL #6 SECTION G-8								
402	4	29'-8"	STR	7				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR
436	4	7'-3"	STR	30				VERT. BARS
PANEL #7 SECTION G-10								
401	4	30'-10"	STR	8				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BARS
437	4	8'-3"	STR	30				VERT. BARS
PANEL #8 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
438	4	9'-5"	STR	30				VERT. BARS
PANEL #9 SECTION G-10								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR
439	4	9'-7"	STR	30				VERT. BARS
PANEL #10 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
440	4	9'-8"	STR	30				VERT. BARS
PANEL #11 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
441	4	9'-7"	STR	30				VERT. BARS
PANEL #12 SECTION G-10								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR
442	4	9'-5"	STR	30				VERT. BARS
PANEL #13 SECTION G-10								
401	4	30'-10"	STR	9				HORIZ. BARS
401	4	30'-10"	STR	1				HORIZ. CAP BAR
443	4	9'-5"	STR	30				VERT. BARS
PANEL #14 SECTION G-10 & RETURN								
402	4	29'-8"	STR	9				HORIZ. BARS
402	4	29'-8"	STR	1				HORIZ. CAP BAR
444	4	9'-1"	STR	30				VERT. BARS
446	4	9'-8"	III	9	8'-8"	1'-0"		HORIZ. RETURN BARS
446	4	9'-8"	III	1	8'-8"	1'-0"		HORIZ. RETURN CAP BAR
445	4	8'-8"	STR	9				VERT. RET. BARS (VAR. 8'-5" TO 9'-0", 1/2" INC.)

RETAINING WALL NO. 3

MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION
PANEL #1 SECTION G-6								
401	4	30'-10"	STR	5				HORIZ. BARS
448	4	15'-0"	STR	1				HORIZ. BAR
401	4	30'-10"	STR	1				HORIZ. CAP BAR
449	4	5'-8 1/2"	STR	30				VERT. BARS (VAR. 5'-1" TO 6'-3 1/2", 1/2" INC.)
PANEL #2 SECTION G-8								
402	4	29'-8"	STR	5				HORIZ. BARS
448	4	15'-0"	STR	1				HORIZ. BAR
402	4	29'-8"	STR	1				HORIZ. CAP BAR
450	4	5'-8 1/2"	STR	30				VERT. BARS (VAR. 5'-2 1/2" TO 6'-5", 1/2" INC.)
PANEL #3 SECTION G-8								
401	4	30'-10"	STR	7				HORIZ. BARS
448	4	15'-0"	STR	1				HORIZ. BAR
401	4	30'-10"	STR	1				HORIZ. CAP BAR
451	4	6'-1 1/2"	STR	30				VERT. BARS (VAR. 6'-4 1/2" TO 6'-7", 1/2" INC.)
PANEL #4 SECTION G-10								
401	4	30'-10"	STR	8				HORIZ. BARS
448	4	15'-0"	STR	1				HORIZ. BAR
401	4	30'-10"	STR	1				HORIZ. CAP BAR
452	4	6'-2 1/2"	STR	30				VERT. BARS (VAR. 7'-7" TO 8'-9 1/2", 1/2" INC.)
PANEL #5 SECTION G-10								
402	4	29'-8"	STR	8				HORIZ. BARS
448	4	15'-0"	STR	1				HORIZ. BAR
402	4	29'-8"	STR	1				HORIZ. CAP BAR
453	4	8'-8 1/2"	STR	30				VERT. BARS (VAR. 7'-9 1/2" TO 9'-0", 1/2" INC.)
PANEL #6 SECTION T-14								
454	4	6'-0"	STR	30				TRANS. FTG. BARS, BOT.
402	4	29'-8"	STR	16				LONG. FTG. BARS, BOT. & TOP
460	4	10'-7"	STR	30				VERT. BARS (VAR. 10'-0" TO 11'-2 1/2", 1/2" INC.)
401	4	30'-10"	STR	2				HORIZ. CAP BARS
448	4	15'-0"	STR	2				HORIZ. BARS
401	4	30'-10"	STR	20				HORIZ. BARS
501	5	7'-4"	I	60	3'-0"	0'-2 1/2"	2'-9"	DOWELS
507	5	6'-6"	STR	30				VERT. BARS
508	5	12'-8"	STR	30				VERT. BARS (VAR. 10'-4" TO 11'-6 1/2", 1/2" INC.)
506	5	7'-0"	STR	30				TRANS. FTG. BARS, TOP
PANEL #7 SECTION T-16								
455	4	7'-0"	STR	30				TRANS. FTG. BARS, BOT.
402	4	29'-8"	STR	18				LONG. FTG. BARS, BOT. & TOP
461	4	11'-8 1/2"	STR	30				VERT. BARS (VAR. 11'-2" TO 12'-4 1/2", 1/2" INC.)
401	4	30'-10"	STR	2				HORIZ. CAP BARS
448	4	15'-0"	STR	2				HORIZ. BARS
401	4	30'-10"	STR	22				HORIZ. BARS
506	5	7'-0"	STR	30				TRANS. FTG. BARS, TOP
509	5	12'-8"	STR	30				VERT. BARS (VAR. 11'-6" TO 12'-8 1/2", 1/2" INC.)
601	6	7'-0"	STR	30				VERT. BARS
602	G	8'-1"	I	60	3'-6"	0'-3"	3'-0"	DOWELS
PANEL #8 SECTION T-16								
455	4	7'-0"	STR	30				TRANS. FTG. BARS, BOT.
402	4	29'-8"	STR	18				LONG. FTG. BARS, TOP & BOT
462	4	12'-5"	STR	30				VERT. BARS (VAR. 12'-4 1/4" TO 13'-7", 1/2" INC.)
402	4	29'-8"	STR	2				HORIZ. CAP BARS
448	4	15'-0"	STR	2				HORIZ. BARS
402	4	29'-8"	STR	26				HORIZ. BARS
506	5	7'-0"	STR	30				TRANS. FTG. BARS, TOP
510	5	12'-8"	STR	30				VERT. BARS (VAR. 12'-8 1/4" TO 13'-11", 1/2" INC.)
601	6	7'-0"	STR	30				VERT. BARS
602	G	8'-1"	I	60	3'-6"	0'-3"	3'-0"	DOWELS
PANEL #9 SECTION T-18								
456	4	7'-7"	STR	30				TRANS. FTG. BARS, BOT.
402	4	29'-8"	STR	20				LONG. FTG. BARS, TOP & BOT
463	4	11'-9"	STR	30				VERT. BARS (VAR. 13'-7" TO 14'-9 1/2", 1/2" INC.)
401	4	30'-10"	STR	2				HORIZ. CAP BARS
448	4	15'-0"	STR	2				HORIZ. BARS
401	4	30'-10"	STR	28				HORIZ. BARS
603	G	14'-6"	STR	30				VERT. BARS (VAR. 13'-11" TO 15'-1 1/2", 1/2" INC.)
701	7	8'-0"	STR	30				VERT. BARS
702	7	7'-7"	STR	30				TRANS. FTG. BARS, TOP
703	7	9'-1"	I	60	4'-0"	0'-3"	3'-6"	DOWELS



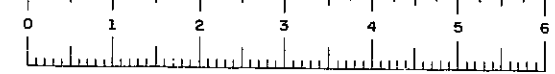
BAR REINFORCEMENT LEGEND



**RETAINING WALLS 1,2,3
BAR LIST**

STATE OF NEW YORK DEPT. OF PUBLIC WORKS
CITY OF BUFFALO ARTERIAL
KENSINGTON EXPRESSWAY, SEC. NO. III

DE LEUW, CATHER & BRILL ENGINEERS-ARCHITECTS NEW YORK	DRAWN CHECKED TRACED	DIST. BY
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FED. ROAD REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	N. Y.	0-06-377(25)	173	173

KENSINGTON EXPRESSWAY - SEC. NO. III
CITY OF BUFFALO ARTERIAL HIGHWAY
CONTRACT NO. 3 NORTHLAND AVENUE TO FILLMORE AVENUE

BAR LIST

RETAINING WALL NO. 3									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #10 SECTION T-18									
456	4	7'-7"	STR	30				TRANS. FTG. BARS, BOT	
402	4	29'-8"	STR	20				LONG. FTG. BARS, TOP & BOT	
464	4	15'-5"	STR	30				VERT. BARS (VAR. 14'-9 1/2" TO 16'-0", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	30				HORIZ. BARS	
604	6	15'-5"	STR	30				VERT. BARS (VAR. 15'-1 1/2" TO 16'-4", 1/2" INC.)	
701	7	8'-0"	STR	30				VERT. BARS	
702	7	7'-7"	STR	30				TRANS. FTG. BARS, TOP	
703	7	9'-1"	I	60	4'-0"	0'-3 1/2"	5'-6"	DOWELS	
PANEL #11 SECTION T-20									
457	4	8'-6"	STR	30				TRANS. FTG. BARS, BOT	
402	4	29'-8"	STR	21				LONG. FTG. BARS, TOP & BOT	
465	4	16'-2"	STR	30				VERT. BARS (VAR. 16'-0" TO 17'-3", 1/2" INC.)	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
402	4	29'-8"	STR	32				HORIZ. BARS	
605	6	17'-0"	STR	30				VERT. BARS (VAR. 16'-0 1/2" TO 17'-7", 1/2" INC.)	
801	8	9'-0"	STR	30				VERT. BARS	
802	8	8'-6"	STR	30				TRANS. FTG. BARS, TOP	
803	8	9'-7"	I	60	4'-3"	0'-3 1/2"	3'-9"	DOWELS	
PANEL #12 SECTION T-22									
458	4	9'-4"	STR	30				TRANS. FTG. BARS, BOT	
402	4	29'-8"	STR	23				LONG. FTG. BARS, TOP & BOT	
466	4	17'-5"	STR	30				VERT. BARS (VAR. 17'-1 1/2" TO 18'-4", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	34				HORIZ. BARS	
704	7	18'-1"	STR	30				VERT. BARS (VAR. 17'-5 1/2" TO 18'-8", 1/2" INC.)	
901	9	10'-0"	STR	30				VERT. BARS	
902	9	8'-4"	STR	30				TRANS. FTG. BARS, TOP	
903	9	10'-9"	I	60	4'-8"	0'-4 1/2"	4'-6"	DOWELS	
PANEL #13 SECTION T-22									
458	4	9'-4"	STR	30				TRANS. FTG. BARS, BOT	
402	4	29'-8"	STR	23				LONG. FTG. BARS, TOP & BOT	
467	4	19'-0 1/2"	STR	30				VERT. BARS (VAR. 18'-5" TO 19'-7 1/2", 1/2" INC.)	
401	4	30'-10"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
401	4	30'-10"	STR	38				HORIZ. BARS	
705	7	18'-2 1/2"	STR	30				VERT. BARS (VAR. 18'-9" TO 19'-1 1/2", 1/2" INC.)	
901	9	10'-0"	STR	30				VERT. BARS	
902	9	9'-4"	STR	30				TRANS. FTG. BARS, TOP	
903	9	10'-9"	I	60	4'-8"	0'-4 1/2"	4'-6"	DOWELS	
PANEL #14 SECTION T-24									
459	4	10'-3"	STR	30				TRANS. FTG. BARS, BOT	
402	4	29'-8"	STR	25				LONG. FTG. BARS, TOP & BOT	
468	4	20'-7"	STR	30				VERT. BARS (VAR. 19'-6 1/2" TO 20'-9", 1/2" INC.)	
402	4	29'-8"	STR	2				HORIZ. CAP BARS	
448	4	15'-0"	STR	2				HORIZ. BARS	
402	4	29'-8"	STR	40				HORIZ. BARS	
606	6	20'-2"	STR	30				VERT. BARS (VAR. 19'-10 1/2" TO 21'-1", 1/2" INC.)	
1003	10	11'-0"	STR	30				VERT. BARS	
1006	10	10'-3"	STR	30				TRANS. FTG. BARS, TOP	
1007	10	11'-9"	I	60	3'-2"	0'-5"	5'-0"	DOWELS	
PANEL #15 SECTION T-26									
503	5	11'-2"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	27				LONG. FTG. BARS, TOP & BOT	
511	5	20'-10"	STR	30				VERT. BARS (VAR. 20'-1" TO 21'-7 1/2", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	42				HORIZ. BARS	
1001	10	11'-6"	STR	15				VERT. BARS	
1002	10	7'-0"	STR	30				VERT. BARS	
1008	10	21'-9"	STR	15				VERT. BARS (VAR. 20'-4 1/2" TO 21'-1 1/4", 1/2" INC.)	
1004	10	12'-10"	I	60	5'-9"	0'-5 1/2"	5'-6"	DOWELS	
1005	10	11'-2"	STR	30				TRANS. FTG. BARS, TOP	

RETAINING WALL NO. 3									
MARK	SIZE	LENGTH	TYPE	NUMBER	A	B	C	DESCRIPTION	
PANEL #16 SECTION T-26									
503	5	11'-2"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	27				LONG. FTG. BARS, TOP & BOT	
512	5	22'-4"	STR	30				VERT. BARS (VAR. 21'-7 1/2" TO 23'-1 1/2", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	42				HORIZ. BARS	
1001	10	11'-6"	STR	15				VERT. BARS	
1002	10	7'-0"	STR	30				VERT. BARS	
1009	10	22'-8 1/2"	STR	15				VERT. BARS (VAR. 21'-11" TO 23'-6 1/4", 1/2" INC.)	
1004	10	12'-10"	I	60	5'-9"	0'-5 1/2"	5'-6"	DOWELS	
1005	10	11'-2"	STR	30				TRANS. FTG. BARS, TOP	
PANEL #17 SECTION T-28									
513	5	11'-10"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	29				LONG. FTG. BARS, TOP & BOT	
514	5	23'-2 1/2"	STR	30				VERT. BARS (VAR. 22'-11" TO 24'-1 1/2", 1/2" INC.)	
502	5	29'-8"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
502	5	29'-8"	STR	46				HORIZ. BARS	
1103	11	12'-6"	STR	15				VERT. BARS	
1105	11	8'-0"	STR	30				VERT. BARS	
1106	11	23'-10"	STR	15				VERT. BARS (VAR. 23'-5" TO 24'-5", 1" INC.)	
1107	11	13'-11"	I	60	6'-1"	0'-6 1/2"	6'-3"	DOWELS	
1108	11	11'-10"	STR	30				TRANS. FTG. BARS, TOP	
PANEL #18 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT	
516	5	23'-1 1/2"	STR	30				VERT. BARS (VAR. 23'-4 1/2" TO 24'-7", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	48				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1109	11	24'-8 1/2"	STR	15				VERT. BARS (VAR. 23'-8 1/2" TO 24'-10 1/4", 1" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-3"	DOWELS	
PANEL #19 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT	
517	5	25'-1 1/2"	STR	30				VERT. BARS (VAR. 24'-6" TO 25'-8 1/2", 1/2" INC.)	
504	5	31'-0"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
504	5	31'-0"	STR	50				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1110	11	25'-3"	STR	15				VERT. BARS (VAR. 24'-10" TO 26'-0", 1" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-3"	DOWELS	
PANEL #20 SECTION T-30									
515	5	12'-6"	STR	30				TRANS. FTG. BARS, BOT	
502	5	29'-8"	STR	31				LONG. FTG. BARS, TOP & BOT	
518	5	26'-3 1/2"	STR	30				VERT. BARS (VAR. 25'-8" TO 26'-6 1/2", 1/2" INC.)	
502	5	29'-8"	STR	2				HORIZ. CAP BARS	
505	5	15'-0"	STR	2				HORIZ. BARS	
502	5	29'-8"	STR	52				HORIZ. BARS	
904	9	12'-6"	STR	60				TRANS. FTG. BARS, TOP	
1101	11	14'-6"	STR	15				VERT. BARS	
1102	11	10'-0"	STR	30				VERT. BARS	
1111	11	26'-5"	STR	15				VERT. BARS (VAR. 25'-11" TO 26'-11 1/4", 1/2" INC.)	
1104	11	15'-5"	I	60	6'-7"	0'-7 1/2"	7'-3"	DOWELS	

RETAINING WALLS 1, 2, & 3
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