



LAKWOOD CITY COUNCIL AGENDA

Monday, April 5, 2021

7:00 P.M.

City of Lakewood

The State Legislature passed a Concurrent Resolution modifying the Open Public Meetings Act (OPMA) during the COVID-19 pandemic until the end of the emergency. As a result, City Hall Council Chambers will **NOT** be open for City Council meetings.

Residents can virtually attend City Council meetings by watching them live on the city's YouTube channel: <https://www.youtube.com/user/cityoflakewoodwa>

Those who do not have access to YouTube can participate via Zoom by either visiting <https://us02web.zoom.us/j/86872632373> or calling by telephone: Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373.

To participate in Public Comment and/or Public Testimony:

Public Comments and Public Testimony on Public Hearings will be accepted by mail, email or by live virtual comment. Send comments by mail or email to Briana Schumacher, City Clerk at 6000 Main Street SW Lakewood, WA 98499 or bschumacher@cityoflakewood.us. Comments received up to one hour before the meeting will be provided to the City Council electronically.

Virtual Comments: If you would like to provide live Public Comments or Public Testimony during the meeting, you will need to join the Zoom meeting as an attendee by calling by telephone Dial +1(253) 215- 8782 and enter participant ID: 868 7263 2373 or visiting <https://us02web.zoom.us/j/86872632373>.

By Phone: For those participating by calling in by telephone (+1(253) 215- 8782 and enter participant ID: 868 7263 2373), the Mayor will call on you during the Public Comment and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during the Public Comment and at each Public Hearing.

By ZOOM: For those using the ZOOM link (<https://us02web.zoom.us/j/86872632373>), upon entering the meeting, please enter your name or other chosen identifier. Use the "Raise Hand" feature to be called upon by the Mayor during the Public Comments and/or Public Hearings portions of the agenda. When you are unmuted please provide your name and city of residence. Each speaker will be allowed (3) three minutes to speak during the Public Comment and at each Public Hearing.

Outside of Public Comments and Public Hearings, all attendees on ZOOM will continue to have the ability to virtually raise your hand for the duration of the meeting. You will not be acknowledged and your microphone will remain muted except for when you are called upon.

Page No.

CALL TO ORDER

ROLL CALL

PLEDGE OF ALLEGIANCE

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

<http://www.cityoflakewood.us>

PROCLAMATIONS AND PRESENTATIONS

- (4) 1. Proclamation honoring and expressing deepest appreciation for Ron Lucas.
- (5) 2. Presentation of the City of Lakewood's 25th Anniversary Art Contest.
- 3. Clover Park School District Report.

PUBLIC COMMENTS**C O N S E N T A G E N D A**

- (41) A. Approval of the minutes of the City Council meeting March 1, 2021.
- (47) B. Approval of the minutes of the City Council study session March 8, 2021.
- (52) C. Approval of the minutes of the City Council meeting of March 15, 2021.
- (58) D. Approval of claims vouchers, in the amount of \$1,583,133.94, for the period of February 17, 2021 through March 15, 2021.
- (72) E. Approval of payroll checks, in the amount of \$2,300,388.89, for the period of February 16, 2021 through March 15, 2021.
- (74) F. Motion No. 2021-20

Authorizing the execution of an agreement with Hemisphere Design, in the amount of \$141,500, for the Build Your Better Here imaging campaign.
- (77) G. Motion No. 2021-21

Authorizing the execution of an extra duty police services contract with the Clover Park School District for School Resources Officers.
- (84) H. Motion No. 2021-22

Appointing Linn Larsen to serve on the Planning Commission through December 15, 2021.

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

- (88) I. Items filed in the Office of the City Clerk:
1. Lakewood Arts Commission meeting minutes of February 1, 2021.
 2. Planning Commission meeting minutes of February 17, 2021.
 3. Parks and Recreation Advisory Board meeting minutes of February 23, 2021.

R E G U L A R A G E N D A

PUBLIC HEARINGS AND APPEALS

- (94) This is the date set for a public hearing on the Lakewood Station District Subarea Plan (LSDS), Development Code and SEPA Planned Action.

ORDINANCE

- (288) Ordinance No. 750

Amending Chapter 9.06 of the Lakewood Municipal Code related to Controlled Substances.

UNFINISHED BUSINESS

NEW BUSINESS

REPORTS BY THE CITY MANAGER

- (292) Review Cooperative Use Agreement with the Clover Park School District.
- (300) South Sound Housing Affordability Partners (SSHA³P) Update.

CITY COUNCIL COMMENTS

ADJOURNMENT

Persons requesting special accommodations or language interpreters should contact the City Clerk, 253-983-7705, as soon as possible in advance of the Council meeting so that an attempt to provide the special accommodations can be made.

CITY OF LAKEWOOD



PROCLAMATION

WHEREAS, Ron Lucas was commissioned as a Officer in the United States Army in 1967, retiring in 1995, after completing more than 28 years of honorable service; and

WHEREAS, his distinguished service included multiple assignments in Southeast Asia, Europe and Washington, D.C.; and

WHEREAS, upon his retirement from the military Ron Lucas dedicated himself to the education of youth working as a math educator for the Tacoma Public School District; and

WHEREAS, in 2003 the Tacoma Public School District recognized Lucas with the Angelo Giaudrone Teacher of the Year Award; and

WHEREAS, his service to the Town of Steilacoom began in 1993 when he was elected as a Councilmember a title he held until he was elected as Mayor in 2001; and

WHEREAS, Mr. Lucas demonstrated the highest levels of dedication and excellence throughout his tenure as Mayor of the Town of Steilacoom, a capacity in which he served for over 20 years; and

WHEREAS, Mayor Lucas deserves particular recognition for his efforts and community involvement "Serving the Children of the World" as Past-President and member of the Kiwanis Club of Steilacoom; and

WHEREAS, in 2003 Mayor Lucas was inducted into the JBLM Civilian Hall of Fame as a Charter Member, in recognition of his continued support of service members and their families; and

WHEREAS, Mayor Lucas was instrumental in the creation and leadership of the South Sound 911 organization; and

WHEREAS, Mayor Lucas further developed relationships and oversaw the allocation of over \$170 Billion in Federal and State Transportation funds as an appointed member of the Puget Sound Regional Council Executive Board and Transportation Policy Board; and

WHEREAS, Mayor Lucas demonstrated caring leadership and strong vision as he helped orchestrate a number of policy priorities from conception to completion through his many community involvement roles with organizations such as the State of Washington Transportation Improvement Board, Pierce County Regional Council, Tacoma Pierce County Board of Health, Rainier Communications Commission, Sound Transit and Pierce County Cities and Towns Association; and

WHEREAS, Mayor Lucas was a friend to the Lakewood community showing his generous support of local park improvements and development initiatives and as a Partners for Parks Board Member.

NOW, THEREFORE, the Lakewood City Council and citizens of the City of Lakewood do hereby honor and express our deepest appreciation of

RON LUCAS

for his exemplary accomplishments and for setting the standard for which quality leadership, both civilian and military, should aspire to emulate.

PROCLAIMED this 5th day of April, 2021.

Don Anderson, Mayor



TO: Mayor and City Council

FROM: Sally Martinez, Recreation Coordinator

THROUGH: John Caulfield, City Manager *John E. Caulfield*

DATE: April 5, 2021

SUBJECT: 25th Anniversary Art Contest

ATTACHMENTS: [Link to flipbook](#)

Summary: To celebrate the City’s 25th Anniversary and to provide an activity that met Covid guidelines, an art contest was created. Two themes were provided: Love Lakewood and Unity. The contest was open to Lakewood residents, those attending school in Lakewood or who work in Lakewood in grades 1-12 and included an adult category. During the development of the contest, the City partnered with the Sisters City Association who invited students from Gimhae, South Korea to participate. Prizes were awarded for various categories. A “flip book” of all the entries has been created which will allow us to virtually share the art with our local and international communities.

Process: In December, 2020, the City’s Arts Commission created an inventive way to both commemorate and celebrate the upcoming 25th anniversary in light of COVID restrictions. The City launched an art contest with several twists. The entries would be submitted digitally and the results would be displayed in a virtual “flip book” that is easily shared with the community. Another unique twist is that the City partnered with our Sister City Association (SCA) to bring an international flare to the event. With SCA President Connie Coleman Lacadie’s leadership and connections, we were able to invite students from our sister city, Gimhae South Korea to participate. All entries would follow two themes, “Love Lakewood” and “Unity” which coincide with the tag line of the 25th Anniversary “Building CommUNITY.”

Partnership: The first step to having a successful contest was securing partnerships. We are thankful to efforts of our Lakewood Arts Commission and to the Twin Star Credit Union for stepping up and sponsoring monetary prizes in the amount of \$1,900. We also want to thank our partners from the Lakewood Branch of the Pierce County Library (Anna Shelton and Amy Anderson) and Linda Smith from the Chamber of Commerce who provided prizes

for a drawing recognizing all entrants who didn't win a cash prize. The Clover Park School District was instrumental in advertising the contest to students and families. Another twist to the contest was having the entries from Gimhae translated and this was an extremely time consuming task. Thank you to the translator's James Kim, a Lakewood Sister Cities Board Member and Son, Eun Kyon (June), Exchange and Cooperation Team, from the City of Gimhae who volunteered their time, outside of their fulltime jobs, to translate the entries for us.

The Public Art Committee of the Arts Commission, and representatives from our partners and sponsors, used a rubric to score and judge the entries. The winners were then notified, and mailed a certificate and a gift card. A total of 50 students and adults submitted entries from the City of Lakewood and over 80 students participated from Gimhae! Each entry included a description which adds a very personal and meaningful touch to the art.

We hope you can take the time to read the inspiring entries!

The City of Lakewood student categories were grades 1-4, 5-8, and 9-12. 1st, 2nd, and 3rd place categories had a corresponding prize: \$150, \$75, \$25 per age group. Adults 1st, 2nd, and 3rd place categories had a corresponding prize of \$ 500, \$250, \$100. The Gimhae categories were grade 1-6, 7-9, and 10-12 and 1st, 2nd, and 3rd place categories had a corresponding prize: \$75, \$50, and \$25 per age group.

The Lakewood artists and winners are:

Category	Place	Artwork Title, Artist name and grade
1-4 th grade	1 st place (tie)	My Family by Sarah Jung, grade 1 Lakewood Summer Fun by Catalina Cruz, grade 3
	2nd place	Together we are better by Calypso Cruz, grade 4
	3rd Place	All Needed For a Full Life by Lillyan Field, grade 4
5-8 grade	1st Place	The broken glass by Rosyln Pitts, grade 8
	2nd Place	A Kitty Night by Khilei Cline, grade 5
	3rd Place	Doodle by kainoa Demei, Grade 7
9-12 grade	1st Place	The Connection Bridge by Noemi Duncan, grade 10
	2nd Place	Hearts of Lakewood by Arianaie Esperon, grade 10
	3rd Place	Two lovers unified in clouds by Luke Keser, grade 11
Adult	1st Place	Stapled Apple by Angela Tyree
	2nd place	Rainbow love + Unity Through Diversity by Meredith Russell
	3rd place	The Oak on Dower by Heidi Merrick

The Gimhae student artists and winners are:

Category	Place	Artwork Title, Artist name and grade
1-4 th grade	1 st place	Bird of Paradise Flying for Integration (Unity) by Um, Su-Ha 3 rd grade
	2nd place	Play Without discrimination by Kim, Naeun, 1 st grade
	3rd Place	Let's Overcome Covid-19 by World Unity! by Kim Siwon, 4 th grade
5-8 grade	1st Place	The Hands of Unity and the 4-leaf Clover of Hope by Junkee Lee, 7 th grade

	2nd Place	World Unity in the Current Divided Situation Joo, Hyunsuk, 8 th grade
	3rd Place	The Divided Current Situation and the Meaning of Unity by Joo, Hyunsuk, 6 th grade
9-12 grade	1st Place	One Unity by Park Hanbin, 9 th grade
	1 st Place (tie)	We Are One”by Haw Yun Jeong, 9 th grade
	2nd Place	“Nostalgic Eiffel Tower” by Noo Doo Bin, 9 th grade

Art Exhibit at 25th Celebration Event

The City is planning to hold an in-person celebration on September 18 on the new Motor Avenue Plaza. We anticipate displaying all the art at this event.

City of Lakewood



1996 Anniversary 2021

Building CommUNITY

Art Contest

Presented by



Special Thanks to



International Gimhae Submissions



1st – 4th Grade

Um, Su-Ha
3rd Grade

“Bird of Paradise Flying for Integration (Unity)”



Integration(Unity) is important because if we only fight, the whole world will become a completely devastated area because of the war. A bird was taken as the subject in this painting. It expresses that one body is made with each feather of Gimhae, Lakewood and others. As if a bird is flying, we are moving forward without any difficulty.¹²

Kim, Naeun
1st Grade

“Play Without Discrimination”



I pictured having fun in the sunflower forest with my friends in Lakewood, the city of international friendship and cooperation. As soon as the coronavirus disappears, I want to play with my friends without a mask.

Kim Siwon
4th Grade

“Let's Overcome Covid-19 by World Unity!”



The picture description is to defeat the coronavirus with a unified will, without discrimination between black, yellow, and white races. I wish people around the world would be united as one. I hope that the global village will be united in peace.

5th – 8th Grade

Junkee Lee
7th grade

“The Hands of Unity and the 4-leaf Clover of Hope”



The hand in the picture expresses the unity of mankind by bringing together the hands of various races. A four-leaf clover is a symbol of good luck and hope. I wanted to express that it is a platform for unity and hope by placing it in the center of the gathered hands. The mountains and buildings around the painting symbolize the unity between countries.

Joo, Hyunsuk
8th Grade

“World Unity in the Current Divided Situation”



First of all, I did a few parodies to express the theme of 'UNITY'. Overall, I referred to 'Lucy in the sky diamonds' of The Beatles, and many other celebrities from around the world were drawn in the work. Unity has been a dream of humanity since the formation of civilization, and countless thinkers have been contemplating this issue for thousands of years. I expressed this as a picture.

Jeong Jimin
6th Grade

“The Divided Current Situation and the Meaning of Unity”

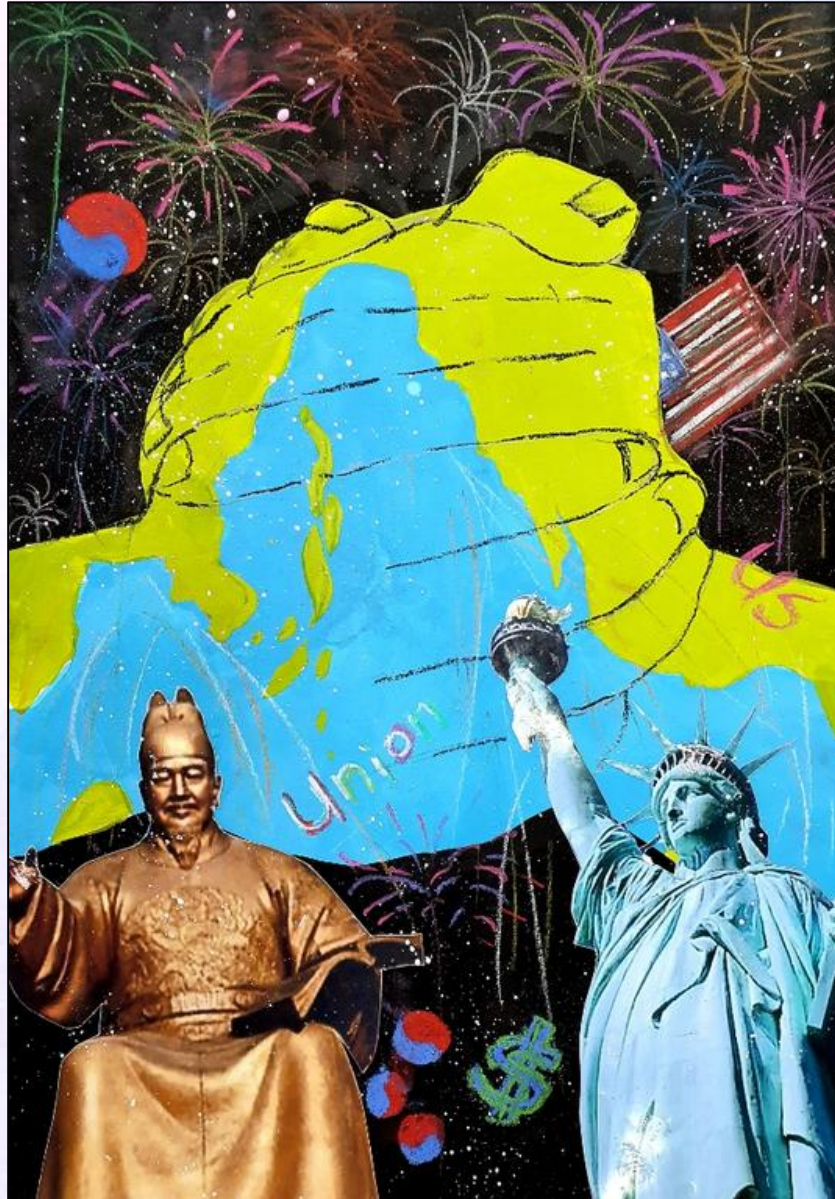


Currently, there is a stronger tendency to hate and reject each other in the global Covid-19 pandemic, natural disaster, and economic crisis. I think that this is a time when 'the spirit of benefiting people widely', the founding myth of our country, is necessary. By learning and sharing this spirit with people around the world, I hope that all people will be able to escape from the crisis and live happily. It is the context underlying in this painting.

9th – 12th Grade

Park Hanbin
9th Grade

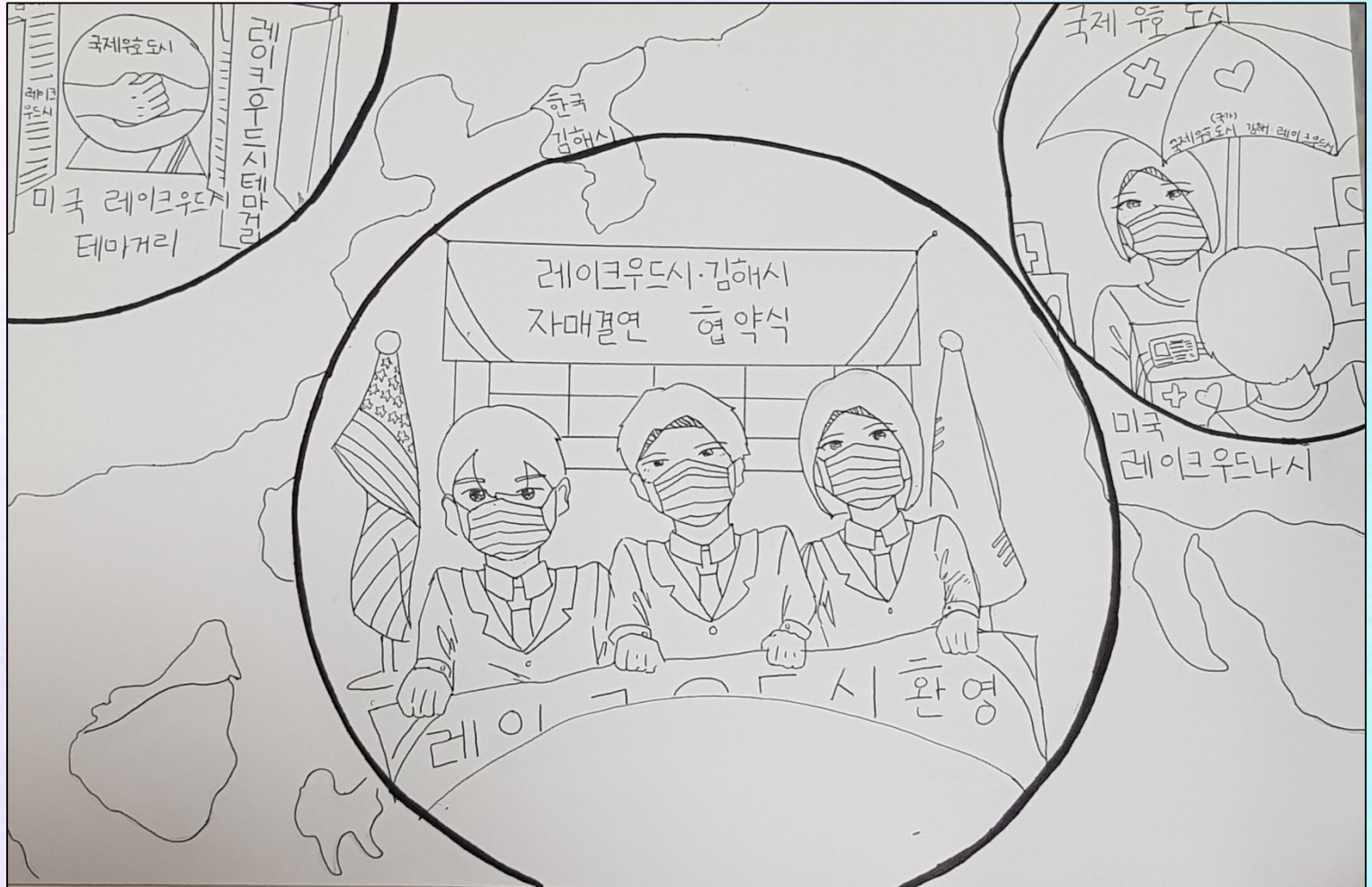
“One Unity”



“Expressing the unity of the two countries through symbolic objects.”

Haw Yun Jeong
9th Grade

“We Are One”



Noo Doo Bin
9th Grade

“Nostalgic Eiffel Tower”



The city I want to go to is France, and I made the famous attraction, the Eiffel Tower. I thought I would be able to go anytime if I liked it, but it is so sad that I cannot go even if I want due to Covid-19. I will become an adult after 3 years and I imagine myself taking a picture at the front of the Eiffel Tower.

City of Lakewood Submissions



1st – 4th Grade

Sarah Jung
1st Grade

“My Family”



When my mom explained about Unity, my family came to my mind. One beautiful family can make a beautiful Lakewood and it can also make a beautiful nation. That's why I like to draw my family. I love my family.



Catalina Cruz
3rd Grade

“Lakewood Summer Fun”



In my community of Lakewood City, it is so much fun to ride bikes around town and in the parks. There are a lot of trails to try and explore. On any sunny hot summer day my favorite thing to do is go bike riding and eat ice cream. It's so much fun! 26

Calypso Cruz
4th Grade

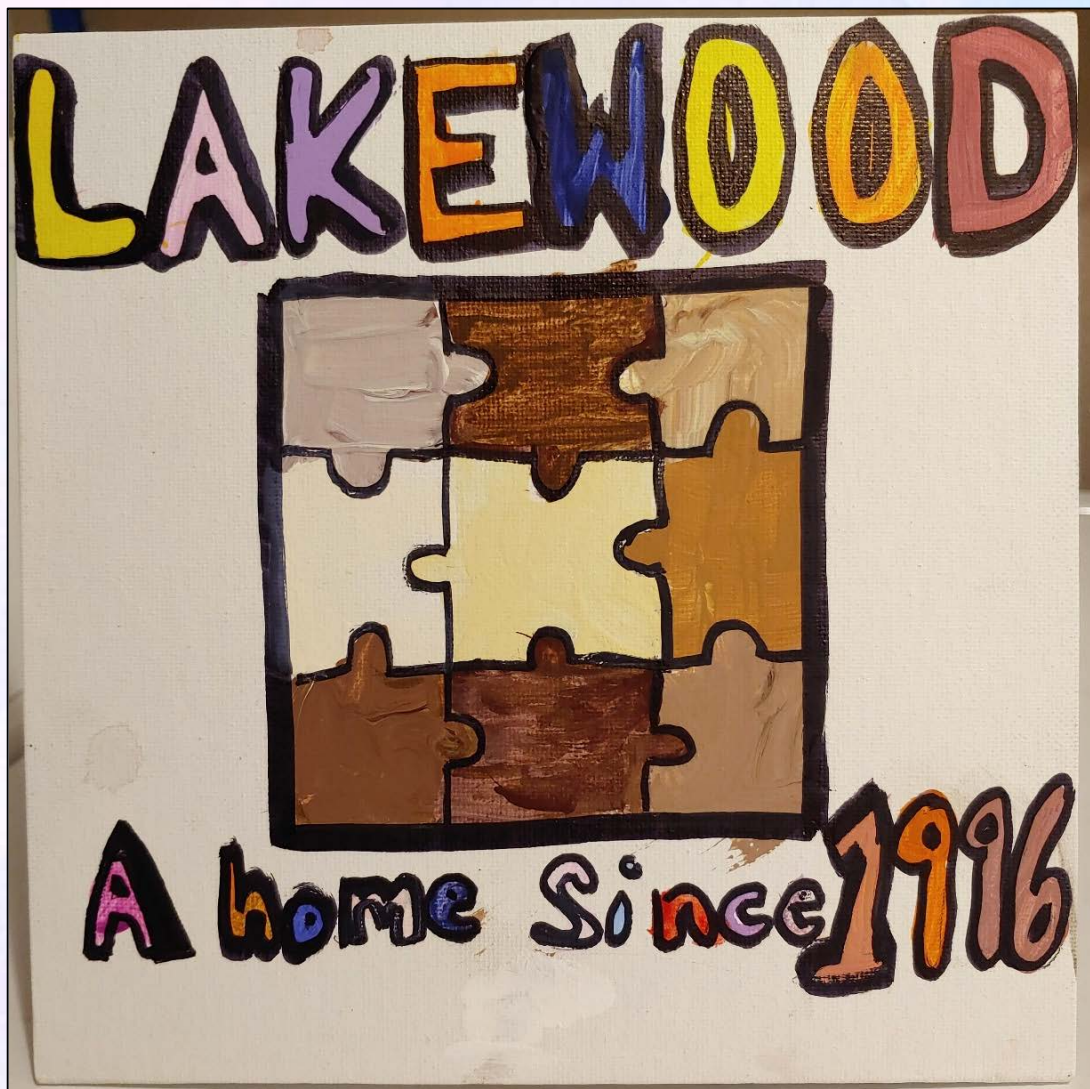
“Together we are better”



Humans need humans. If we come together with kindness and unite as people no matter where we live and how we look, then we will be better people. It starts with you and where you live. I live in Lakewood and I will try my best and be kind.

Lillyan Field
4th Grade

“All Needed For A Full Life”

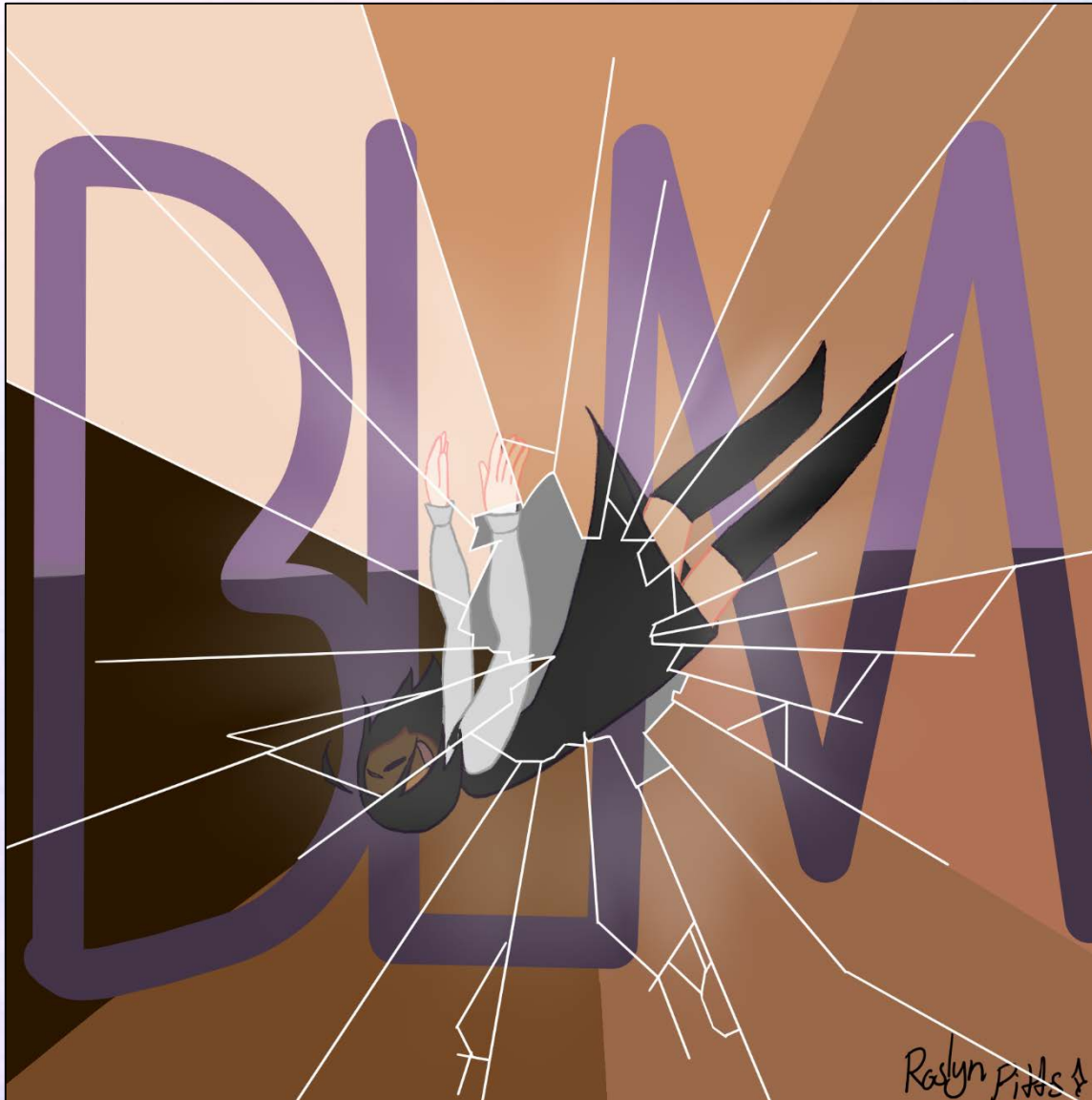


When everyone comes together like a puzzle, where we are all equal pieces will we see the full picture of humanity.

5th – 8th Grade

Roslyn Pitts
8th Grade

“The Broken Glass”



The reasoning or thought processes behind the making of my artwork was making a character that broke through the glass. The glass represents the "wall" between all the different skin colors to then show that everyone is all the same and just looks different.

Khilei Cline
5th Grade



“A Kitty Night”



The reason behind “A Kitty Night” is about my cats, Jiji (he has a tail) and Olli (he has a nub tail). Jiji and Olli are brothers, and are always together, united as one, they are bonded and can’t be separated. I was inspired by a “Starry Night” because it’s one of my favorite art pieces.

Kainoa Demei
7th Grade

“Doodle”



Unity brings me back to when I used to play Little League Baseball, a few years ago. Being in a team meant we had to work together and help each other get better. Outside of baseball we also have a get together where we can enjoy each other's company. We lost and won together without pointing a finger. We were just in it together.

With what's going on around us today. I feel like we need to be a team. Be there for each other. Even if you can't lend a helping hand, a simple smile can lift a spirit. With unity, nothing can bring us down. We are all going through it and together we will get out of it.

9th – 12th Grade

Noemi Duncan
10th Grade

“The Connection Bridge”



How do you view Lakewood? I see a wide range of diversity, lively people, and many attractive sights, but mainly I see a community I can call home. Lakewood reminds me of a bridge, connecting you to all the unique art, places of interest, and overall sociable environment. From the minute you cross the bridge into Lakewood, to the minute you take one step out, you experience all of the emotions Lakewood has to offer as well. From joy, to curiosity, to being inspired. What makes Lakewood stand out is the people you meet and the experiences you face.

Arianie Esperon
10th Grade

“Hearts of Lakewood”



At first, I was going to use symbols that represent some of the ethnicities that are commonly seen in Lakewood but there are so many. I decided flags were a great symbol so I chose to draw flags of major countries into hearts as a symbol of love and diversity here in ³⁵ Lakewood!

Luke Keser
11th Grade

“Two Lovers Unified in the Clouds.”



This is a photo I snapped at a beach in April of 2019 at complete random. But it ended up being powerful thanks to the figures in it. It displays two lovers, being unified in this enchanted looking area of sky.

Adults

Angela Tyree
Adult

“Mended”



Unity in 2021 has a new meaning. We are different. We are one. The stapled apple was brought to life and inspired by our differences and vulnerabilities as a community as we navigate a new, uncertain world. By joining two different types of apples together in this way, it is representative of different races and ideas - but one people. This piece outlines our most basic need to connect, in spite of our contradictions³⁸.



Meredith Russell
Adult

“Rainbow Love”



Rainbow Love is all about embracing the different cultures and people in the Lakewood community through all seasons. Fluid art is an experiment where colors meld together both intentionally and unintentionally and I feel like that really embraces the community of Lakewood as well. We have all these different cultures and traditions and yet we come together as a community in both times of happiness and times of need. I've only lived here in Lakewood for 5 years and yet it felt like home right away.

“Unity Through Diversity”



Unity Through Diversity is the idea that we need each other in order to create a true community. Relying solely on the individual is a mistake when there are so many different view points. In this piece I really tried to capture how our different backgrounds can form an amazing image if we can only back up and look at the big picture. Yes it's a bit of a mess and a little chaotic, but isn't all of life?

Heidi Merrick
Adult

“The Oak on Dower ”



I moved to Lakewood from Seattle in 2013. I had never been to Lakewood before then. In my 7 years here I have come to realize the uniqueness of Lakewood's population of Garry Oaks. I am absolutely enthralled with them and they inspired me to pick up a brush and start painting again. I hope we continue to protect these old stands for years to come!



LAKWOOD CITY COUNCIL MINUTES

Monday, March 1, 2021

City of Lakewood

6000 Main Street SW

Lakewood, WA 98499

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253)215-8782

Participant ID: 86872632373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

SWEARING-IN CEREMONY

City of Kent Mayor Dana Ralph conducted the Oath of Office for Patti Belle. Ms. Belle joined the City Council.

ROLL CALL

Councilmembers Present: 7 – Mayor Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Linda Farmer and Paul Bocchi.

PROCLAMATIONS AND PRESENTATIONS

Proclamation recognizing the City of Lakewood's 25th Anniversary.

MAYOR ANDERSON PRESENTED A PROCLAMATION RECOGNIZING THE CITY OF LAKEWOOD'S 25th ANNIVERSARY.

Proclamation recognizing Captain Rudolph Dambeck as Honorary City Attorney.

MAYOR ANDERSON PRESENTED A PROCLAMATION RECOGNIZING CAPTAIN RUDOLPH DAMBECK, 2/2 STRYKER BRIGADE COMBAT TEAM, AS HONORARY CITY ATTORNEY.

Youth Council Report.

Youth Councilmember Josephine Kaiser shared that at today's meeting the students had discussion related to youth mental health and affordable housing. She also shared that Harrison Prep students started hybrid learning today.

Clover Park School District Report.

Clover Park School District (CPSD) Board Director Marty Schafer reported that hybrid learning presentations will be held throughout the week and high school will begin on March 8th starting with 9th grade students. He shared that virtual parent teacher conferences are scheduled for March 17th through March 19th and that Classified Employee's Week is March 8th through March 12th. He congratulated Lakes High School Senior Sarah Baim who was recognized as a National Merit Scholarship Finalist. He then spoke the social and emotional health of students, the CPSD Open Doors Program noting that 40 students recently graduated and shared that free meals continue to be available for all students.

PUBLIC COMMENTS

Speaking before Council were:

Dennis Haugen, Sioux Falls South Dakota, spoke about Washington sanctuary state rules, drug trade laws, homelessness, and changes to the Republican Party.

Christopher Escobedo, Lakewood resident, spoke about the need for Native American's to serve as local elected officials and oppression and racism in the community.

Julian Wheeler, Lakewood resident, congratulated Patti Belle on her appointment to the City Council and shared that the Pierce County Accessible Communities Advisory Committee meeting will be held on March 9, 2021 at 9:00 a.m.

C O N S E N T A G E N D A

- A. Approval of the minutes of the City Council meeting of February 1, 2021.
- B. Approval of the minutes of the City Council study session February 8, 2021.
- C. Approval of the minutes of the City Council special meeting of February 9, 2021.
- D. Approval of the minutes of the City Council special meeting of February 10, 2021.
- E. Approval of claims vouchers, in the amount of \$2,953,553.90, for the period of January 16, 2021 through February 16, 2021.
- F. Approval of payroll checks, in the amount of \$2,452,357.17, for the period of January 16, 2021 through February 15, 2021.

G. Motion No. 2021-12

Accepting a donation from CHI Franciscan Health, in the amount of \$8,200, for the Lakewood Farmers Market and one drive-in movie.

H. Motion No. 2021-13

Authorizing the award of a construction contract to Northwest Cascade, Inc., in the amount of \$628,628, for the construction of the 108th Street Overlay project.

I. Motion No. 2021-14

Authorizing the execution of a contract with AHBL, Inc., in the amount of \$715,000, to develop a new JBLM Growth Coordination Plan.

J. Items filed in the Office of the City Clerk:

1. Planning Commission meeting minutes of February 3, 2021.

Mayor Anderson removed Motion No. 2021-14 from the Consent Agenda.

COUNCILMEMBER FARMER MOVED TO ADOPT THE CONSENT AGENDA ITEMS NO. A THROUGH ITEM NO. H. AND ITEM NO. J. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

COUNCILMEMBER MOSS MOVED TO ADOPT AMENDED MOTION NO. 2021-14 TO AUTHORIZE THE EXECUTION OF A CONTRACT WITH AHBL, INC., IN THE AMOUNT OF ~~\$715,000~~ \$643,500, TO DEVELOP A NEW JBLM GROWTH COORDINATION PLAN. SECONDED BY COUNCILMEMBER FARMER. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

UNFINISHED BUSINESS

None.

NEW BUSINESS

None.

REPORTS BY THE CITY MANAGER**City of Lakewood 25th Anniversary Update.**

Parks, Recreation and Community Services Director Dodsworth shared a video highlighting 25 years of incorporation. She then shared a PowerPoint honoring the history and accomplishments of the City. She reported that special events and activities will be scheduled this year based around the theme of building unity in the community. She shared that a 25th Anniversary Party is scheduled for September 18, 2021 at Colonial Plaza. Discussion ensued.

Special Events Update.

Parks, Recreation and Community Services Director Dodsworth provided an update on special events and programs scheduled for 2021. She shared that several events and programs will be determined based on COVID-19 guidance. Discussion ensued.

South Sound Proud Mural Update.

Parks, Recreation and Community Services Director Dodsworth shared that in 2019 the City received a grant from South Sound Together to create a mural at American Lake Park. She shared that the theme of the mural is live like the mountain is out. Discussion ensued.

City Council Representation on Public Art Selection Committee.

Parks, Recreation and Community Services Director Dodsworth requested that two City Councilmembers be designated to participate in the Colonial Plaza public art selection panel. Discussion ensued and Deputy Mayor Whalen and Councilmember Belle will be appointed to represent the City Council.

Parks, Recreation and Community Services Director Dodsworth then provided an update on the Fort Steilacoom Park All Access Playground noting that the toddler play area is being replaced with an all abilities play area.

City Manager Caulfield shared that the City continues to inquire with State Department of Health and Department of Emergency Management to get an understanding of what operations and metrics will be in Phase 3.

He shared that Clover Park High School student Azalia Riviere was recognized as the South Puget Sound Boys and Girls Club Youth of the Year. She will be invited to a future City Council meeting to be recognized.

He reported that an update on the City's Hazard Mitigation Plan is scheduled for City Council review on April 12th followed by a Resolution adopting the plan on April 19th.

He reported that REI is interested in office space in Pierce County and last week he met with representatives to discuss opportunities for locations in Lakewood or at City Hall. In addition, Congresswoman Strickland is interested in expanding office space at City Hall.

He shared that the Lakewood Drive from Steilacoom Boulevard to Flett Creek improvement project has begun and speed limits have been decreased in this area.

He then shared that The Homeownership Center of Tacoma has invited City officials to participate in a groundbreaking ceremony for two affordable homes that will be built in the area of 9000 Washington Boulevard SW.

He then reported that an update from the South Sound Housing Affordability Partners (SSHAP) and Annual Housing Report will come forward during the March 8th study session.

He shared that Senator Murray contacted the City requesting information about priority infrastructure projects for the community, the city provided three projects valued at \$18.7 Million, to set the foundation for future federal transportation funding opportunities or earmarks.

He shared that he met with CPSD Superintendent Banner to review and discuss a joint Resolution related to diversity, equity and inclusion efforts, the facilities cooperative use agreement and the return of School Resource Officers (SRO) to high schools.

He shared that retail sales tax has exceeded budgetary expectations although did not grow compared to 2019, a 4th quarter 2020 Financial Report will come forward for discussion during the April 26th study session.

CITY COUNCIL COMMENTS

Councilmember Belle thanked Councilmembers and shared that she is excited to join the City Council.

Councilmember Moss congratulated and welcomed Councilmember Belle.

Councilmember Brandstetter congratulated and welcomed Councilmember Belle.

Councilmember Farmer congratulated and welcomed Councilmember Belle. She shared that this week she will participate in an Equity Policy discussion hosted by the Clover Park School District.

Councilmember Bocchi congratulated and welcomed Councilmember Belle. He shared that last weekend he participated in a neighborhood clean-up event coordinated by the North Lakewood Neighborhood Association.

Deputy Mayor Whalen congratulated and welcomed Councilmember Belle. He shared that he attended the Lakewood Gardens Board Retreat where there was discussion of their strategic plan and updates to the Old Carriage House.

Mayor Anderson shared that last week he met with the Master Builders Association; he spoke about federal funding for rental assistance and identifying other needs within the community. He encouraged those who are eligible to schedule their COVID-19 vaccination.

Mayor Anderson announced that the City Council will adjourn and meet in Closed Session pursuant to 42.30.140(4)(a).

ADJOURNMENT

There being no further business, the meeting adjourned at 9:17 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



LAKWOOD CITY COUNCIL STUDY SESSION MINUTES

Monday, March 8, 2021

City of Lakewood

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253) 215- 8782

Participant ID: 868 7263 2373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Don Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Linda Farmer and Paul Bocchi.

ITEMS FOR DISCUSSION:

South Sound Housing Affordability Partnership (SSHAP) Update.

Planning Manager Tiffany Speir introduced Mr. John Howell, Cedar River Group and Mr. Marty Kooistra, Housing Development Consortium, who have been facilitating conversations on behalf of SSHAP which includes Pierce County and a consortium of cities.

Mr. Howell shared that SSHAP convened in 2019 to shape Pierce County's future by creating access to affordable and attainable housing.

Mr. Kooistra highlighted the history of SSHAP and Lakewood's involvement. He shared that access to affordable and attainable housing is a regional challenge and by pooling resources more can be accomplished than by single jurisdictions. He provided examples of what SSHAP might do which includes development of locally supported policies, establishment of a Housing Capital Fund, coordinate activities around a legislative housing agenda, create housing units, advocate for state and federal resources and grants. He shared that a targeted work plan will be created with key performance measures which will support participating government's needs.

Mr. Howell then shared that next steps include agency review of the drafted intergovernmental agreement and provide a financial commitment which will ultimately be based on how many jurisdictions participate. Discussion ensued.

Annual Housing Report.

Assistant City Manager for Development Services Bugher provided an overview of current housing in Lakewood. He shared that there is a stronger rate of growth and shortage of inventory due to increased net-migration and demand from King County.

He shared that apartment market conditions in the Tacoma-Lakewood are tight with a 2.9% vacancy rate during the 3rd quarter of 2020 and Lakewood's average rental rate was \$1,285 in 2020. He shared apartment rent ranges noting that housing units are 40% renter occupied and 59% owner occupied. He then highlighted rents by community noting that Lakewood it is the second lowest in Pierce County. He reviewed stats related to Lakewood's housing production which totaled 167 new units in 2020, platting activity which totaled 116 lots, and the City registered 10,487 properties through the Rental Housing Safety Program (RHSP).

He reviewed Lakewood's role in housing noting that the City controls zoning and housing subsidies although does not control land values, access to credit or rising costs of labor and materials. He outlined some of Lakewood's housing policies and programs that are in place to maintain and promote affordable housing within the city. Discussion ensued.

2020 Business Retention and Expansion Report.

Economic Development Manager Becky Newton shared that the Business Retention and Expansion (BRE) is a core program for economic development and the department increased collaboration and partnerships in 2020. She highlighted impacts due to COVID-19 to the hospitality industry and reported that taxable retail sales trends flattened at 2%. She shared that in 2020, 127 businesses were contacted which represents approximately 10,000 jobs.

She shared results of a survey of businesses that was conducted to identify specific needs, workforce demographics, level of satisfaction and to identify impacts and restrictions of COVID-19 on business vitality. Discussion ensued.

Review Lakewood Station District Subarea Plan.

Planning Manager Speir was joined by Lisa Grueter, BERK and Associates. Ms. Speir highlighted the proposed Lakewood Station District Subarea Plan (LSDS) boundaries which is located in the southeast area of the city which includes 330 acres and stretches north from Pacific Highway to 108th Street SW and east to State Route 512.

She highlighted legislative review to date conducted by the Planning Commission and City Council as well as public engagement events that were held both in person and virtually. She shared partial results from a public survey that was conducted to verify the vision and focus of the LSDS. She reported that 3% of the city's

population is in the subarea which includes 170 businesses and 2300 employees and highlighted income, employment and population demographics.

She then reviewed the proposed land use and density of the area which will allow for residential capacity of 1722 new dwellings and employment capacity of 1276 new jobs and key concepts such as the proximately to transit, affordability, walkability, design standards, transportation mitigation measures and parking requirements. She then shared environmental constraints and issues that have been identified by stakeholders such as the risk of displacement, current and future affordable housing options and parking standards.

She shared that next steps are for the City Council to conduct a public hearing on the proposed LSDS at the April 5th meeting followed by action on April 19th. Discussion ensued.

ITEMS TENTATIVELY SCHEDULED FOR THE MARCH 15, 2021 REGULAR CITY COUNCIL MEETING:

1. 28th and 29th District Legislators Mid-Legislative Session Update.
2. Sound Transit Update. – *Ms. Katie Drewel, Government and Community Relations Officer*
3. Authorizing award of a contract for the construction of the JBLM North Access Improvement project. (Motion – Consent Agenda)
4. Authorizing the execution of a collective bargaining agreement with the Lakewood Police Independent Guild. – (Motion – Consent Agenda)
5. Reappointing Sylvia Allen and Michael Lacadie to serve on the Parks and Recreation Advisory Board through September 19, 2023 and appointing Fred Ramey to serve on the Park and Recreation Advisory Board through September 19, 2022. – (Motion – Consent Agenda)
6. Appointing Christine Manetti to serve on the Landmarks and Heritage Advisory Board through December 31, 2023. – (Motion – Consent Agenda)
7. Appointing Sylviann Johnson to serve on the Arts Commission through October 16, 2023. – (Motion – Consent Agenda)

REPORTS BY THE CITY MANAGER

City Manager Caulfield shared that Sound Transit will provide the City Council an update at the regular meeting of March 15, 2021.

He shared that the Senate approved the American Rescue Plan which includes funding for local governments and a review and discussion of options for use of

funding will take place during the City Council Retreat scheduled for Saturday, March 27th, in the City Hall Council Chambers.

He shared that the Puget Sound Regional Council (PSRC) Transportation Coordinating Committee has recommended the Steilacoom Boulevard between Weller Road and 83rd Street construction project for funding.

He shared that the City of Lakewood's Spring Community Clean Up event will be held on Saturday, April 17th and Sunday, April 18th. Those interested in volunteering should contact City Clerk Briana Schumacher.

He shared that a contract approval for the JBLM North Access Improvement Phase 1 project will come forward for City Council consideration during the March 15th regular meeting.

He reported that the Pierce County Council approved grant funding for a Rental and Mortgage assistance program which is available to tenants and landlords.

He recognized several current and prior employees who graduated from the Northwest Women's Leadership Academy (NWWLA) and noted that the purpose of the academy is to advance women from a variety of backgrounds in local government into leadership roles.

He then announced the following upcoming meetings and events:

- March 10 at 11:00 A.M.; Briefing with Senator Murray and Senator Cantwell; virtual event
- March 11 at 4:30 P.M.; Ground Breaking Ceremony for Homeownership Center of Tacoma
- March 24 at 8:00 A.M. to 11:00 A.M.; 2021 AWC Lobby Day, virtual event
- March 24 at 12:00 P.M.; Community Healthcare Lunch and Laughter; virtual event
- March 31; 8:00 A.M; Communities in Schools Champions for Youth Breakfast; virtual event

CITY COUNCIL COMMENTS

Councilmember Farmer shared that she provided Council Remarks at the Clover Park School District (CPSD) Board meeting this evening. She encouraged the City Council to participate in the Emergency Food Network Hunger Walk virtual event. She commented on the CPSD Resolution related to diversity, equity and inclusion and it was requested that the Resolution come forward for City Council consideration at next week's meeting.

Deputy Mayor Whalen shared that last week he attended the Pierce Transit Board of Commissioners meeting.

Mayor Anderson commented that today is International Women’s Day.

Mayor Anderson announced that the City Council will recess into Executive Session for approximately 60 minutes pursuant to RCW 42.30.110(1)(i) to discuss with legal counsel representing the city litigation or potential litigation to which the agency, the governing body, or a member acting in an official capacity is, or is likely to become, a party, when public knowledge regarding the discussion is likely to result in an adverse legal or financial consequence to the agency.

Council recessed into Executive Session at 9:59 p.m. and reconvened at 10:59 p.m.

ADJOURNMENT

There being no further business, the meeting 10:59 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



LAKWOOD CITY COUNCIL MINUTES

Monday, March 15, 2021

City of Lakewood

6000 Main Street SW

Lakewood, WA 98499

<https://www.youtube.com/user/cityoflakewoodwa>

Telephone via Zoom: +1(253)215-8782

Participant ID: 86872632373

CALL TO ORDER

Mayor Anderson called the meeting to order at 7:00 p.m.

ROLL CALL

Councilmembers Present: 7 – Mayor Anderson, Deputy Mayor Jason Whalen; Councilmembers Mary Moss, Mike Brandstetter, Patti Belle, Linda Farmer and Paul Bocchi.

PROCLAMATIONS AND PRESENTATIONS

28th and 29th District Legislators Mid-Legislative Session Update.

City Manager Caulfield shared that the State Legislators will be joining us to provide a mid-legislative session update. He shared that key State Legislative initiatives for the city include a request for Tax Increment Financing, funding in support of Western State Hospital community policing program, capital budget requests for a Living Access Support Alliance (LASA) expansion, Wards Lake and American Lake Park expansion and funding for a Multimodal Transportation Study.

Mayor Anderson introduced Representative Leavitt. Representative Leavitt shared that work continues this legislative session in response to COVID-19 and in support of small businesses, rental assistance and childcare programs. She spoke about HB1095 which ensures small businesses will not have to pay B&O tax on personal protective equipment and HB1332 which allows for 2020 property tax deferrals and HB1121 related to graduation credit or waivers for students.

Representative Bronoske shared that this legislative session has been unprecedented and he spoke about technology challenges and opportunities for engagement with constituents online. He spoke about unemployment insurance and benefits and police accountability.

Discussion ensued related American Rescue Plan and what role the legislature will play in allocation of funds, the eviction moratorium and landlord assistance including mitigation funds and dispute resolution assistance.

Sound Transit Update.

Planning Manager Speir introduced Ms. Katie Drewel, Sound Transit Government and Community Relations Officer and Mr. Zac Eskenazi, Planning Manager.

Ms. Drewel provided an update on the Sound Transit Capital Program Realignment. She shared that due to lower revenue and higher construction costs Sound Transit will not be able to deliver projects on their original timelines. She noted that projects not yet in construction will be considered by the Board for realignment which may include reducing the scope, suspension or cancelling of projects. She shared that next steps will include continued discussion, public engagement and feedback followed by adoption of realignment plans in July.

Mr. Eskenazi shared that the Lakewood Station Access Improvement project kicked off in 2021. The project includes identifying access improvements to the station such as pedestrian and bike infrastructure, bus speed and reliability investments, parking improvements and the project cost is estimated at \$44 Million. He shared a map of the station area, highlighted the project goals and objectives which focus on affordability, access, environment, partnership and passenger experience. He then highlighted project activities which will be conducted through Fall of 2021. He shared that external community engagement about the project includes a Technical Advisory Group that meets monthly, online open houses to solicit feedback and an online survey component. Discussion ensued.

PUBLIC COMMENTS

Speaking before Council were:

Dennis Haugen, Sioux Falls South Dakota, spoke about immigration and drug policies, the crisis at the border and human trafficking.

C O N S E N T A G E N D A

- A. Approval of the minutes of the City Council meeting of February 16, 2021.
- B. Approval of the minutes of the City Council special meeting February 22, 2021.
- C. Approval of the minutes of the City Council study session of February 22, 2021.
- D. Motion No. 2021-15

Reappointing Sylvia Allen and Michael Lacadie to serve on the Parks and Recreation Advisory Board through September 19, 2023 and appointing Fred Ramey to serve on the Parks and Recreation Advisory Board through September 19, 2022.

E. Motion No. 2021-16

Appointing Christina Manetti to serve on the Landmarks and Heritage Advisory Board through December 31, 2023.

F. Motion No. 2021-17

Appointing Sylviann Johnson to serve on the Arts Commission through October 16, 2023.

G. Items filed in the Office of the City Clerk:

1. Parks and Recreation Advisory Board meeting minutes of January 26, 2021.
2. Landmarks and Heritage Advisory Board meeting minutes of January 28, 2021.
3. Lakewood's Promise Advisory Board meeting minutes of February 4, 2021.

COUNCILMEMBER BOCCHI MOVED TO ADOPT THE CONSENT AGENDA. SECONDED BY COUNCILMEMBER FARMER. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

R E G U L A R A G E N D A

RESOLUTION

Resolution No. 2021-04 Expressing a commitment to valuing diversity, equity and inclusion.

COUNCILMEMBER BRANDSTETTER MOVED TO CONTINUE CONSIDERATION OF RESOLUTION NO. 2021-04 UNTIL THE CITY COUNCIL REGULAR MEETING OF APRIL 19, 2021. SECONDED BY COUNCILMEMBER BOCCHI. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

UNFINISHED BUSINESS

None.

NEW BUSINESS

Motion No. 2021-18 Authorizing the award of a construction contract to R. L. Alia Company, in the amount of \$7,558,504.76, for the JBLM North Access Improvement, Phase 1 project.

Public Works Engineering Director Bucich provided an overview of the project area and traffic control plans. Discussion ensued related to the project, impacts to the Lotus Blossom public art and community outreach efforts.

COUNCILMEMBER BOCCHI MOVED TO ADOPT MOTION NO. 2021-18. SECONDED BY DEPUTY MAYOR WHALEN. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

Motion No. 2021-19 Authorizing 2021 Community Development Block Grant (CDBG) funding in support of a transportation project.

Public Works Engineering Director Bucich provided an overview of three potential locations that were evaluated for use of CDBG funding in support of a sidewalk improvement project.

COUNCILMEMBER BRANDSTETTER MOVED TO ADOPT MOTION NO. 2021-19, PHILLIPS ROAD SIDEWALK PROJECT. SECONDED BY COUNCILMEMBER FARMER. VOICE VOTE WAS TAKEN AND CARRIED UNANIMOUSLY.

REPORTS BY THE CITY MANAGER

Review and update on Congressional Community Project Funding.

City Manager Caulfield introduced Public Works Engineer Director Bucich and Jake Johnston, Johnston Group. City Manager Caulfield shared that Congresswoman Strickland has requested that the City submit a Community Project Funding Request, formerly known as an Earmark, for the FY 2022 Federal Appropriations Bill.

Mr. Johnston provided an overview of the Community Project Funding program and eligibility criteria noting that each member of Congress is only eligible to submit ten projects for consideration.

Public Works Engineering Director Bucich provided an overview of infrastructure projects to be considered for submission. Discussion ensued and it is recommended that the City submit a Community Project Funding request for the Nyanza Road project.

City Manager Caulfield reported that Congress signed the American Rescue Plan last week of which includes a set aside of \$1.1 Billion for Washington State and \$14.86 Million for the City of Lakewood. A memo including additional details and

background information will be prepared for City Council review and discussion at the March 27th Retreat.

He then reported Washington State Governor Inslee announced that the State moved to Phase 3, further guidelines are expected to be released later this week and essential employees are now eligible for vaccinations.

He shared that the City has provided language to Congresswoman Strickland related to the FY2022 National Defense Authorization Act (NDAA).

He also shared that the Youth Council recommended Clover Park High school student Gloria Arauja Ruiz for the 2021 Association of Washington Cities (AWC) Quality Communities Scholarship.

He then shared that the Spring Community Clean Up event will be held on Saturday April 17th and Sunday, April 18th, those interested in volunteering can contact the City Manager or City Clerk.

He noted that the Safe Routes to School sidewalk infill project at Farwest Drive between 112th Street and Lakes High School has been ranked number 8, should the program be funded, it is anticipated this project will be selected.

He then provided an update on the mailboxes on Onyx Drive noting that 85% of mailbox keys have been provided to residents. In addition, ribbons have been provided, those who want to keep their mailboxes should place the ribbon on the box so it isn't picked up. Those who have not received keys can visit the post office to pick up keys.

CITY COUNCIL COMMENTS

Councilmember Belle shared that last week she met with her colleagues on the City Council, attended the City's 25th Anniversary planning meeting, the groundbreaking ceremony for the Homeownership Center of Tacoma and a Rotary meeting.

Councilmember Brandstetter shared that he testified in support of rail safety bill which supports recommendations from the National Transportation Safety Board (NTSB).

Councilmember Bocchi shared that last week he attended the Lake City Neighborhood Association meeting.

Councilmember Moss shared that last week she attended the 25th Anniversary planning meeting and the groundbreaking ceremony for the Homeownership Center of Tacoma.

Deputy Mayor Whalen shared this week he will attend Pierce County Regional Council (PCRC) meeting.

Mayor Anderson shared that last week he attended Coffee with the Mayor where Planning Manager Speir presented on the Lakewood Station District Subarea Plan, he attended the State of the County presentation, a 25th Anniversary planning meeting and the groundbreaking ceremony for the Homeownership Center of Tacoma.

He commented on considering a summer Job Corps program for disadvantaged youth with COVID-19 funds. He also shared that Pierce County has a Crisis Triage Center which will be located in the Parkland area.

ADJOURNMENT


There being no further business, the meeting adjourned at 9:50 p.m.

DON ANDERSON, MAYOR

ATTEST:

BRIANA SCHUMACHER
CITY CLERK



To: Mayor and City Councilmembers
From: Tho Kraus, Assistant City Manager/Chief Financial Officer
Through: John J. Caulfield, City Manager 
Date: April 5, 2021
Subject: Claims Voucher Approval

Check Run Period: February 17, 2021- March 15, 2021

Total Amount: \$ 1,583,133.94

Checks Issued:

02/25/21	Checks 94258-94308	\$ 113,963.51
03/03/21	Check 94309	\$ 5,200.00
03/15/21	Checks 94310-94390	\$ 230,424.94

EFT Checks Issued:


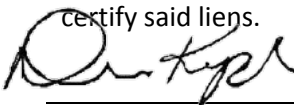
02/25/21	Checks 17006-17063	\$ 696,597.88
03/15/21	Checks 17064-17138	\$ 545,447.57

Voided Checks:

03/03/21	Check 94260	\$ 5,200.00
03/10/21	Check 93990	\$ 1,799.96
03/12/21	Check 94280	\$ 1,500.00

Grand Total \$ 1,583,133.94

I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered, or the labor performed as described herein and that the claims are just and due obligations against the City of Lakewood, Washington, and that I am authorized to authenticate and certify said liens.



Dana Kapla
Finance Supervisor

Tho Kraus
Assistant City Manager/
Chief Financial Officer



John J. Caulfield
City Manager

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
504.0000.09.518.35.48.001			2/12/2021	29381	RM Claim #2021-006A	4,454.17	
94280	2/25/2021	012934		JBLM STONE EDUCATON CENTER,			\$1,500.00
106.0000.11.362.40.00.001			2/18/2021	CM-00055	PA City Day Refund	1,500.00	
94281	2/25/2021	011124		KHATIBI, N. CYRUS			\$120.00
001.0000.02.512.51.49.009			1/26/2021	01/26/21	MC 01/26 Interpreter	120.00	
94282	2/25/2021	004680		LANGUAGE LINE SERVICES,			\$32.03
001.0000.02.512.51.49.009			1/31/2021	4938517	MC 01/21	32.03	
94283	2/25/2021	005685		LEMAY MOBILE SHREDDING,			\$60.00
001.0000.02.512.50.41.001			2/1/2021	4689613	MC 01/22 Shredding	60.00	
94284	2/25/2021	000309		LES SCHWAB TIRE CENTER,			\$863.48
501.9999.51.594.21.64.005			2/10/2021	30500646511	PDFL Build	213.65	
501.0000.51.548.79.48.005			2/18/2021	30500648032	PKFL Tire	517.30	
501.0000.51.548.79.31.006			2/11/2021	30500646778	PKFL Battery	132.53	
94285	2/25/2021	004073		MACDONALD-MILLER FACILITY SOL,			\$1,002.29
502.0000.17.521.50.48.001			2/1/2021	PM106887	PKFC Qtrly Mechanical Maint. B	1,002.29	
94286	2/25/2021	007962		NATIONAL SAFETY INC.,			\$172.76
101.0000.11.544.90.31.010			2/17/2021	0607802-IN	PKST Nemesis Blk Frm Smoke Mir	172.76	
94287	2/25/2021	002421		NORTHWEST PLAYGROUND EQUIP,			\$3,538.78
301.0017.11.594.76.63.001			2/23/2021	47715	PK Play Chips for Active and P	3,220.00	
301.0017.11.594.76.63.001			2/23/2021	47715	Sales Tax	318.78	
94288	2/25/2021	012946		PARK CHASE ASSOC. LP,			\$9,239.45
190.2003.53.559.32.41.001			2/24/2021	AG 2021-038	CDBG AG 2021-038 Rental Assist	1,604.04	
190.2003.53.559.32.41.001			2/24/2021	AG 2021-039	CDBG AG 2021-039 Rental Assist	2,141.70	
190.2003.53.559.32.41.001			2/24/2021	AG 2021-044	CDBG AG 2021-044 Rental Assist	3,721.30	
190.2003.53.559.32.41.001			2/24/2021	AG 2021-051	CDBG AG 2021-051 Rental Assist	1,772.41	
94289	2/25/2021	000405		PCCFOA,			\$20.00
001.0000.06.514.30.49.001			2/18/2021	2021 Schumacher Dues	LG 2021 PCCFOA Dues: Schumache	20.00	
94290	2/25/2021	000420		PIERCE TRANSIT,			\$804.61

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
195.0021.02.512.53.41.001			1/29/2021	AR000295	MC 01/29 Transit Svc	804.61	
94291	2/25/2021	010064		PINTO, MICHELLE			\$908.16
001.0000.02.512.51.49.009			1/5/2021	01/05-01/28/21	MC 01/05-01/28 Interpreter	908.16	
94292	2/25/2021	000445		PUGET SOUND ENERGY,			\$24,816.73
101.0000.11.542.63.47.006			2/4/2021	300000007165 2/4/21	PKST 01/01-02/01 N of Lk WA Bl	23,565.57	
502.0000.17.518.35.47.011			2/18/2021	200018357661 2/18/21	PKFC 01/19-02/17 6000 Main St	988.88	
502.0000.17.521.50.47.011			2/17/2021	200008745289 2/17/21	PKFC 01/15-02/16 9401 Lkwd Dr	262.28	
94293	2/25/2021	010896		PUGET SOUND TITLE - TACOMA,			\$1,615.53
105.0001.07.559.20.41.001			1/19/2021	218857	AB Lininger Litigation Guarant	461.58	
105.0001.07.559.20.41.001			1/25/2021	218904	AB One Mind Presbyterian Churc	461.58	
191.0000.01.559.20.41.001			1/27/2021	218923	NSP Zhao Litigation Guarantee	692.37	
94294	2/25/2021	005342		RAINIER LIGHTING & ELECTRICAL,			\$395.65
101.0000.11.542.30.31.001			1/12/2021	537959-1	PKST Headlamp, AC Santronics T	95.56	
101.0000.11.542.30.31.001				537990-1	PKST Credit Headlamp	-75.78	
502.0000.17.521.50.31.001			2/18/2021	539628-1	PKFC Lights, P-Nut W/ Lever	375.87	
94295	2/25/2021	012825		READY SET TOW LLC,			\$175.84
001.0000.15.521.10.41.070			2/9/2021	7524	PD 02/08	87.92	
001.0000.15.521.10.41.070			2/9/2021	7525	PD 02/08	87.92	
94296	2/25/2021	012311		REEDER PROPERTY MANAGEMENT,			\$288.00
105.0002.07.342.40.00.000			2/8/2021	02/02/21	AB Refund RH Bus License Overc	288.00	
94297	2/25/2021	012955		RICK, JAMES AND DIANA			\$6,600.00
105.0001.07.559.20.41.001			2/25/2021	02/25/21 Relocate	AB 7407-7409 146th St SW Reloc	6,600.00	
94298	2/25/2021	012940		ROBINSON, MARJORY			\$36.00
105.0002.07.342.40.00.000			2/16/2021	RBL47-1712057766	AB RH Refund RH Bus License	36.00	
94299	2/25/2021	011507		SEUI, MICHAEL			\$252.32
001.0000.02.512.51.49.009			1/31/2021	01/21	MC 01/21 Interpreter	252.32	
94300	2/25/2021	012951		SILVERWOOD ALLIANCE LLC,			\$977.35
190.2003.53.559.32.41.001			2/24/2021	AG 2021-035	CDBG AG 2021-0035 Rental Assis	977.35	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94301	2/25/2021		010180	SIX ROBBLEES' INC,			\$462.05
501.0000.51.548.79.31.006			2/18/2021	2-750072	PKFL Jack For Plow	462.05	
94302	2/25/2021		011013	TANNE, CARLO			\$120.00
001.0000.02.512.51.49.009			12/31/2020	12/21	MC 12/21 Interpreter	120.00	
94303	2/25/2021		005831	TOWN OF STEILACOOM,			\$4,891.23
001.0000.02.229.10.00.002			2/17/2021	01/21 Court Remit	MC 01/21 Court Remit	4,544.88	
101.0000.11.542.63.41.001			2/12/2021	2021-02-12-01	PKST AG 2019-107 01/14 St. Lig	346.35	
94304	2/25/2021		011127	US BANK VOYAGER FLEET SYSTEMS,			\$957.71
501.0000.51.521.10.32.001			2/1/2021	869343012106	PDFL/PKFL 01/21 Fuel	235.14	
180.0000.15.521.21.32.001			2/1/2021	869343012106	PDFL/PKFL 01/21 Fuel	139.85	
181.0000.15.521.30.32.001			2/1/2021	869343012106	PDFL/PKFL 01/21 Fuel	25.28	
501.0000.51.548.79.32.001			2/1/2021	869343012106	PDFL/PKFL 01/21 Fuel	373.03	
501.0000.51.548.79.32.002			2/1/2021	869343012106	PDFL/PKFL 01/21 Fuel	184.41	
94305	2/25/2021		002509	VERIZON WIRELESS,			\$245.09
503.0000.04.518.80.42.001			2/10/2021	INV22224682	IT 12/20 GPS	245.09	
94306	2/25/2021		011595	WALTER E NELSON CO,			\$117.59
502.0000.17.521.50.31.001			2/16/2021	795712	PKFC Disinfectant	117.59	
94307	2/25/2021		012957	WARNSTADT, TODD AND CARMEN			\$116.67
204.0000.00.389.01.00.001			2/25/2021	PWTF Loan 31 Refund	Refund Overpayment of PWTF Loa	94.65	
204.0000.00.361.40.03.003			2/25/2021	PWTF Loan 31 Refund	Refund Overpayment of PWTF Loa	22.02	
94308	2/25/2021		000598	WATER MANAGEMENT LABORATORIES,			\$118.00
401.0000.41.531.10.41.001			2/19/2021	191740	PWSW 12/21/20 Water Sampling	118.00	
94309	3/3/2021		012956	PAYNE, DOROTHY			\$5,200.00
105.0001.07.559.20.41.001			2/25/2021	02/25/21 Relocate	AB 7407-7409 146th St SW Reloc	5,200.00	
94310	3/15/2021		002293	AHBL INC,			\$1,150.00
001.9999.07.558.65.41.001			1/31/2021	123620	CD 12/26/20-01/25/21 Lkwd Ener	1,150.00	
94311	3/15/2021		012968	BECKER, DON			\$15.00
001.0101.11.347.30.06.001			3/15/2021	001518	PK Refund Boat Launch: Purchas	13.65	
001.0000.00.229.10.00.004			3/15/2021	001518	PK Refund Boat Launch: Purchas	1.35	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94312	3/15/2021	009770		BRUCE DEES & ASSOCIATES,			\$8,240.00
301.0032.11.594.76.41.001			3/4/2021	6359R	PK AG 2020-169 Springbrook Par	8,240.00	
94313	3/15/2021	000095		CHOUGH, KWANG S			\$376.80
001.0000.02.512.51.49.009			2/4/2021	02/04-02/16/21	MC 02/04-02/16 Interpreter	376.80	
94314	3/15/2021	012966		CLAMPITT, LARRY AND MARILYN			\$36.00
105.0002.07.342.40.00.000			3/15/2021	RBL03-1907299971	AB Refund Duplicate Payments	36.00	
94315	3/15/2021	011584		COBAN TECHNOLOGIES INC,			\$2,813.45
181.0000.15.521.21.35.010			2/26/2021	38268	PD - 1 COBAN G5 10-Bay Microph	830.00	
181.0000.15.521.21.35.010			2/26/2021	38268	PD - 6 COBAN Transmitters - G5	1,680.00	
181.0000.15.521.21.35.010			2/26/2021	38268	freight	50.00	
181.0000.15.521.21.35.010			2/26/2021	38268	Sales Tax	166.33	
181.0000.15.521.21.35.010			2/26/2021	38268	Sales Tax	82.17	
181.0000.15.521.21.35.010			2/26/2021	38268	Sales Tax	4.95	
94316	3/15/2021	003948		COMCAST CORPORATION,			\$488.05
503.0000.04.518.80.42.001			2/15/2021	8498 35 011 2205662	IT 02/25-03/24 9420 Front St S	208.25	
180.0000.15.521.21.42.001			3/6/2021	8498 30 099 0003937	PD 03/16-04/15 TLSO Modem	279.80	
94317	3/15/2021	002406		COSTCO,			\$120.00
001.0000.04.514.20.49.001			3/15/2021	519181263	FN Costco Renewal Thru 05/31/2	120.00	
94318	3/15/2021	001531		DEPT OF ECOLOGY,			\$13,149.15
401.0000.41.531.10.41.001			2/10/2021	21-WAR045012B-1	PWSW 07/01/20-06/30/21 2nd 1/2	13,149.15	
94319	3/15/2021	009472		DISH NETWORK LLC,			\$165.02
503.0000.04.518.80.42.001			3/4/2021	8255 7070 8168 1616	IT 03/16-04/15 PD TV/HD Receiv	165.02	
94320	3/15/2021	011744		DR. STEPHEN FRIEDRICK,			\$1,200.00
001.0000.15.521.10.41.001			1/21/2021	102	PD 2020 Advisor Phlebotomy Pro	1,200.00	
94321	3/15/2021	012967		DURR, DANIEL AND LORI			\$24.00
105.0002.07.342.40.00.000			3/15/2021	RBL90-1801266565	AB Refund: Wrong Account Charg	24.00	
94322	3/15/2021	001716		FENCE SPECIALISTS INC,			\$925.64
001.9999.11.576.81.31.001			2/23/2021	0046670	PKFC Rail Posts	925.64	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94323	3/15/2021	000196		GOV'T FINANCE OFFICERS ASSOC,			\$595.00
001.0000.04.514.20.49.001			2/17/2021	2137470	FN 04/01/21-03/31/22 GFOA: T K	595.00	
94324	3/15/2021	008664		HOLROYD COMPANY INC,			\$256.07
101.0000.11.542.70.31.030			2/25/2021	373115	PKST Rock For Shoulder Grading	256.07	
94325	3/15/2021	009728		HSA BANK,			\$76.50
001.0000.09.518.10.41.001			3/4/2021	W294584	HR 02/21 Svc Fee	76.50	
94326	3/15/2021	010885		JOHNSTON GROUP LLC,			\$4,500.00
001.0000.03.513.10.41.001			3/1/2021	1148	CM AG 2020-276 03/21 Fed. Gov.	4,500.00	
94327	3/15/2021	008332		KAR-GOR INC,			\$11,436.52
302.0080.21.595.30.63.001			1/29/2021	KI012128	PWCP 108th St SW & Pac. Hwy SW	10,406.30	
302.0080.21.595.30.63.001			1/29/2021	KI012128	Sales Tax	1,030.22	
94328	3/15/2021	008414		LAKWOOD FORD,			\$144.74
501.0000.51.548.79.48.005			12/20/2020	LCCS472183	PKFL Vehicle Maint	42.58	
501.0000.51.548.79.32.001			12/20/2020	LCCS472183	PKFL Vehicle Maint	21.76	
501.0000.51.548.79.31.006			1/4/2021	LCW104466F	PKFL Vehcile Supplies	80.40	
94329	3/15/2021	000300		LAKWOOD WATER DISTRICT,			\$782.05
101.0000.11.542.70.47.001			2/16/2021	22087.01 02/16/21	PKST 12/19/20-02/19/21 0 75th	64.97	
001.0000.11.576.80.47.001			2/9/2021	14449.03 02/09/21	PKFC 12/12/20-02/12/21 9222 Ve	43.12	
101.0000.11.542.70.47.001			2/9/2021	27146.02 02/09/21	PKST 12/12/20-02/12/21 9420 Fr	43.12	
101.0000.11.542.70.47.001			2/9/2021	30353.01 02/09/21	PKST 12/12/20-02/12/21 Lake Ci	43.12	
001.0000.11.576.80.47.001			2/23/2021	24214.01 02/23/21	PKFC 12/26/20-02/26/21 Oakbroo	43.12	
001.0000.11.576.80.47.001			3/2/2021	25956.03 03/02/21	PKFC 01/09-03/05 8807 25th Ave	63.64	
001.0000.11.576.80.47.001			3/2/2021	26121.03 03/02/21	PKFC 01/09-03/05 8421 Pine St	43.12	
502.0000.17.521.50.47.001			3/2/2021	26834.02 03/02/21	PKFC 01/09-03/05 9401 Lkwd Dr	105.09	
001.0000.11.576.80.47.001			3/9/2021	15040.02 03/09/21	PKFC 01/16-03/12 4723 127th St	64.97	
502.0000.17.518.35.47.001			3/9/2021	16702.02 03/09/21	PKFC 01/16-03/12 6000 Main St	159.69	
502.0000.17.518.35.47.001			3/9/2021	16706.02 03/09/21	PKFC 01/16-03/12 6000 Main St	43.12	
101.0000.11.542.70.47.001			3/9/2021	16713.03 03/09/21	PKST 1/16-03/12 0 59th & Main	64.97	
94330	3/15/2021	011263		LAW OFFICES OF MATTHEW RUSNAK,			\$500.00
001.0000.02.512.51.41.035			3/1/2021	328	MC 02/08 & 02/19	500.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94331	3/15/2021	005685		LEMAY MOBILE SHREDDING,			\$220.60
001.0000.15.521.10.41.001			3/10/2021	4690903	PD 02/19 Shredding	220.60	
94332	3/15/2021	004073		MACDONALD-MILLER FACILITY SOL,			\$8,727.21
101.0000.11.544.90.48.001			3/1/2021	PM107618	PKST Semi-Annual Mechanical Ma	547.30	
301.0035.11.594.76.41.001			2/28/2021	JC66875	PK Lakewood Pavillion RR Ducts	1,859.51	
502.0000.17.518.35.48.001			2/25/2021	SVC199672	PKFC Repairs To FCU-6, AHU-1,2	2,774.98	
502.0000.17.518.35.48.001			11/12/2020	SVC193186	PKFC 11/05/20 Install New Pump	3,545.42	
94333	3/15/2021	011393		NAVIA BENEFIT SOLUTIONS,			\$224.10
001.0000.09.518.10.41.001			2/28/2021	10335532	HR 02/21 Participant Fee	224.10	
94334	3/15/2021	010743		NISQUALLY INDIAN TRIBE,			\$16,611.64
001.0000.15.521.10.41.125			2/28/2021	25764	PD 02/21	16,575.00	
001.0000.15.521.10.41.125			1/31/2021	25776	PD 01/21 Pharmacy Reimb	36.64	
94335	3/15/2021	011424		OLBRECHTS & ASSOCIATES PLLC,			\$1,541.25
001.0000.07.558.60.41.007			12/31/2020	12/20 LU-20-00014	CD 12/20 LU-20-00014 Hearing E	1,541.25	
94336	3/15/2021	006117		PETTY CASH,			\$123.50
501.9999.51.594.21.64.005			3/4/2021	12/20 JL	PD Vehicle Registration	61.75	
501.9999.51.594.21.64.005			3/4/2021	12/20 JL	PD Vehicle Registration	61.75	
94337	3/15/2021	012965		PIERCE MILITARY AND BUSINESS,			\$1,500.00
106.0000.11.362.40.00.001			3/15/2021	CM-00055	PA City Day Refund	1,500.00	
94338	3/15/2021	010064		PINTO, MICHELLE			\$1,359.52
001.0000.02.512.51.49.009			2/2/2021	02/02-02/26/21	MC 02/02-02/26 Interpreter	1,359.52	
94339	3/15/2021	007866		PLANNING ASSOC OF WASHINGTON,			\$450.00
001.0000.07.558.60.49.001			3/10/2021	1716	CD 2021 PAW Organization Membe	450.00	
94340	3/15/2021	010429		PMAM CORPORATION,			\$1,715.00
001.0000.15.521.10.41.015			3/4/2021	20210308	PD 02/21 Alarm Monitoring	1,715.00	
94341	3/15/2021	011181		POLLARD, ANDREA			\$1,040.00
001.0000.11.576.81.49.003			3/3/2021	179	MC/PK/PW 03/03 Flagger Trainin	455.00	
101.0000.11.542.30.49.003			3/3/2021	179	MC/PK/PW 03/03 Flagger Trainin	390.00	
101.0000.21.543.10.49.003			3/3/2021	179	MC/PK/PW 03/03 Flagger Trainin	130.00	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.02.523.30.49.003			3/3/2021	179	MC/PK/PW 03/03 Flagger Trainin	65.00	
94342	3/15/2021	010204		PROTECT YOUTH SPORTS,			\$39.80
001.0000.09.518.10.41.001			2/28/2021	848778	HR 02/21 Na'tl Combo Searches	39.80	
94343	3/15/2021	000445		PUGET SOUND ENERGY,			\$2,900.55
001.0000.11.576.81.47.005			2/23/2021	300000010896 2/23/21	PKFC 01/20-02/18 Ft Steil Park	445.95	
001.0000.11.576.81.47.005			2/23/2021	300000010938 2/23/21	PKFC 01/20-02/18 8802 Dresden	367.15	
001.0000.11.576.81.47.005			2/23/2021	200001527346 2/23/21	PKFC 01/22-02/22 8714 87th Ave	10.93	
001.0000.11.576.81.47.005			2/23/2021	220017468871 2/23/21	PKFC 01/22-02/22 9107 Angle La	218.65	
001.0000.11.576.81.47.005			2/23/2021	220024933081 2/23/21	PKFC 01/22-02/22 8714 87th Ave	75.83	
001.0000.11.576.80.47.005			2/24/2021	220002793168 2/24/21	PKFC 01/25-02/23 8807 25th Ave	166.01	
101.0000.11.542.64.47.005			2/19/2021	300000005037 2/19/21	PKST 12/30/20-01/29/21 GLD & V	358.39	
001.0000.11.576.81.47.005			2/19/2021	200001527551 2/19/21	PKFC 01/20-02/18 9115 Angle Ln	147.78	
001.0000.11.576.80.47.005			3/2/2021	300000000129 3/02/21	PKFC 01/29-03/01 11500 Militar	309.43	
001.0000.11.576.80.47.005			3/2/2021	300000010268 3/02/21	PKFC 01/29-03/01 Woodlawn Ave	123.79	
001.0000.11.576.80.47.005			3/2/2021	200001526637 3/02/21	PKFC 01/29-03/01 9222 Veteran'	64.49	
101.0000.11.542.63.47.006			3/2/2021	200006381095 3/2/21	PKST 01/29-03/01 7819 150th St	26.07	
101.0000.11.542.63.47.006			3/2/2021	220008814687 3/2/21	PKST 01/29-03/01 7000 150th St	24.99	
101.0000.11.542.63.47.006			3/2/2021	220017817689 3/02/21	PKST 01/29-03/01 11521 GLD SW	99.65	
001.0000.11.576.80.47.005			3/2/2021	220018963391 3/02/21	PKFC 01/29-03/01 10365 112th S	66.11	
101.0000.11.542.63.47.005			3/2/2021	220025290614 3/2/21	PKST 01/29-03/01 12702 Vernon	234.02	
101.0000.11.542.63.47.005			3/2/2021	220025290630 3/02/21	PKST 01/29-03/01 8299 Veterans	161.31	
94344	3/15/2021	012380		RACE FORWARD,			\$1,000.00
001.9999.09.518.10.49.001			9/16/2020	0000001890	HR 05/01/20-04/30/21 Core Memb	1,000.00	
94345	3/15/2021	005342		RAINIER LIGHTING & ELECTRICAL,			\$1,462.77
502.0000.17.518.35.31.001			3/3/2021	540267-1	PKFC Electrical Tape, AC Santr	120.89	
502.0000.17.521.50.31.001			3/5/2021	540419-1	PKFC Lights	193.42	
502.0016.17.594.18.63.001			3/8/2021	540357-1	PKFC Lights	1,148.46	
94346	3/15/2021	012960		RAINIER RENTALS,			\$1,496.48
190.2003.53.559.32.41.001			3/8/2021	AG 2021-058	CDBG AG 2021-058 Rent Assist:	746.48	
190.2003.53.559.32.41.001			3/8/2021	AG 2021-059	CDBG AG 2021-060 Rent Assist:	750.00	
94347	3/15/2021	012825		READY SET TOW LLC,			\$175.84
001.0000.15.521.10.41.070			2/21/2021	21-07644	PD 02/21	87.92	
001.0000.15.521.10.41.070			2/21/2021	7645	PDFL 02/21	87.92	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94348	3/15/2021	010478		RICOH USA INC,			\$375.85
503.0000.04.518.80.45.002			2/19/2021	104694805	IT IT 02/18-03/17 Copier	375.85	
94349	3/15/2021	012959		RODRIGUEZ, ANSELMO & ELISABETH			\$36.00
105.0002.07.342.40.00.000			3/1/2021	RBL43-1711307889	AB RH Refund: RBL43-1711307889	36.00	
94350	3/15/2021	011507		SEUI, MICHAEL			\$132.32
001.0000.02.512.51.49.009			2/9/2021	02/09/21	MC 02/09 Interpreter	132.32	
94351	3/15/2021	011271		SITE ONE LANDSCAPE SUPPLY LLC,			\$443.64
001.0000.11.576.80.31.030			3/8/2021	106780560-001	PKFC Herbicide	443.64	
94352	3/15/2021	010447		SPECIAL SERVICES GROUP LLC,			\$2,637.60
180.0000.15.521.21.41.001			3/5/2021	15052	PD - Annual renewal of covert	2,400.00	
180.0000.15.521.21.41.001			3/5/2021	15052	Sales Tax	237.60	
94353	3/15/2021	005575		SUMNER VETERINARY HOSPITAL,			\$871.92
001.0000.15.521.10.41.001			2/23/2021	761215	PD 02/22 Canine Rock Vet Svcs	871.92	
94354	3/15/2021	010842		SUPPRESSION SYSTEMS INC,			\$549.50
101.0000.11.542.64.41.001			2/26/2021	20756	PKST Troubleshoot Fire Alarm P	549.50	
94355	3/15/2021	002667		TACOMA TOWING LLC,			\$87.92
001.0000.15.521.10.41.070			1/11/2021	243547	PD 01/09	87.92	
94356	3/15/2021	011013		TANNE, CARLO			\$676.16
001.0000.02.512.51.49.009			2/12/2021	0212-21-LKWDMUNCT-M3	MC 02/12 Interpreter	120.00	
001.0000.02.512.51.49.009			2/17/2021	0218-21-LKWDMUNCT-M5	mc 02/17 -02/18 Interpreter	240.00	
001.0000.02.512.51.49.009			3/9/2021	02/4/2021	MC 02/02-02/04 Interpreter	316.16	
94357	3/15/2021	010626		TIMCO INC,			\$292.31
101.0000.11.542.66.31.001			3/9/2021	T032910	PKST Quick Disconnect Fittings	292.31	
94358	3/15/2021	009354		TK ELEVATOR,			\$1,411.81
502.0000.17.518.35.48.001			2/28/2021	3005788977	PKFC 02/21 Elevator Svc	524.72	
502.0000.17.521.50.48.001			2/28/2021	3005788977	PKFC 02/21 Elevator Svc	262.36	
502.0000.17.542.65.48.001			2/28/2021	3005788977	PKFC 02/21 Elevator Svc	624.73	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
94359	3/15/2021	012961		TRAN, THAI			\$250.00
105.0002.07.342.40.00.000			3/8/2021	RBL01-1907108334	AB RH Insp Refund RBL01-190710	250.00	
94360	3/15/2021	010640		TRANSUNION RISK AND,			\$105.29
001.0000.15.521.21.41.001			3/1/2021	212084 02/21	PD 02/21 People Searches	105.29	
94361	3/15/2021	001924		TRI-TEC COMMUNICATIONS INC,			\$11,923.05
503.0000.04.518.80.42.001			2/10/2021	652140	IT 03/13/21-03/12/22 Advanced	10,849.00	
503.0000.04.518.80.42.001			2/10/2021	652140	Sales Tax	1,074.05	
94362	3/15/2021	000564		TUCCI & SONS, INC,			\$89,956.18
301.0014.11.594.76.63.001			2/23/2021	91605	PK AG 2020-106 Thru 02/21 Ft.	94,690.72	
301.0000.00.223.40.00.000			2/23/2021	91605	PK AG 2020-106 Retainage	-4,734.54	
94363	3/15/2021	011127		US BANK VOYAGER FLEET SYSTEMS,			\$1,101.36
501.0000.51.521.10.32.001			3/1/2021	8693430122110	PKFL/PDFL 02/21 Fuel	237.52	
180.0000.15.521.21.32.001			3/1/2021	8693430122110	PKFL/PDFL 02/21 Fuel	303.86	
181.0000.15.521.30.32.001			3/1/2021	8693430122110	PKFL/PDFL 02/21 Fuel	100.29	
501.0000.51.548.79.32.001			3/1/2021	8693430122110	PKFL/PDFL 02/21 Fuel	401.50	
501.0000.51.548.79.32.002			3/1/2021	8693430122110	PKFL/PDFL 02/21 Fuel	58.19	
94364	3/15/2021	009856		UTILITIES UNDERGROUND LOCATION,			\$352.17
101.0000.11.544.90.41.001			2/28/2021	1020169	PKST/PKSW 02/21 Excavation Not	176.09	
401.0000.11.531.10.41.001			2/28/2021	1020169	PKST/PKSW 02/21 Excavation Not	176.08	
94365	3/15/2021	002509		VERIZON WIRELESS,			\$682.76
503.0000.04.518.80.42.001			2/16/2021	9873498639	IT 01/17-02/16 Phone	17.63	
503.0000.04.518.80.42.001			2/16/2021	9873498639	IT 01/17-02/16 Phone	170.92	
503.0000.04.518.80.42.001			2/16/2021	9873498639	IT 01/17-02/16 Phone	140.95	
503.0000.04.518.80.42.001			2/16/2021	9873498639	IT 01/17-02/16 Phone	35.24	
503.0000.04.518.80.42.001			2/16/2021	9873498640	IT 01/17-02/16 Phone	141.26	
503.0000.04.518.80.42.001			2/16/2021	9873498640	IT 01/17-02/16 Phone	176.76	
94366	3/15/2021	011755		VISA - 0349,			\$1,601.16
001.0000.15.521.22.31.008			2/24/2021	0349/Meeks 02/24/21	PD CDT Uniforms	1,601.16	
94367	3/15/2021	011749		VISA - 0513,			\$475.20
502.0000.17.518.35.31.001			2/24/2021	0513/Ferm 02/24/21	PKFC Vacuum Cleaner Parts	219.68	
502.0000.17.521.50.31.001			2/24/2021	0513/Ferm 02/24/21	PKFC Restroom Partion Parts	100.26	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
101.0000.11.542.30.35.001			2/24/2021	0513/Ferm 02/24/21	PKST Flushmate Replacement Tan	155.26	
94368	3/15/2021	011958		VISA - 0975,			\$717.44
001.0000.99.518.40.42.002			2/24/2021	0975/Gumm 02/24/21	ND Postage	1.20	
105.0002.07.559.20.31.001			2/24/2021	0975/Gumm 02/24/21	AB ICC Code Books	175.24	
105.0001.07.559.20.31.001			2/24/2021	0975/Gumm 02/24/21	AB ICC Code Books	175.25	
001.0000.99.518.40.42.002			2/24/2021	0975/Gumm 02/24/21	ND Postage	15.10	
105.0001.07.559.20.42.002			2/24/2021	0975/Gumm 02/24/21	AB Dangerous Bldg. Postage	177.15	
105.0001.07.559.20.42.002			2/24/2021	0975/Gumm 02/24/21	AB Dangerous Bldg. Postage	16.70	
190.6003.52.558.31.31.001			2/24/2021	0975/Gumm 02/24/21	CDBG Tabs, Pens, Staple Remove	97.70	
105.0001.07.559.20.42.002			2/24/2021	0975/Gumm 02/24/21	AB Dangerous Bldg. Postage	41.80	
105.0001.07.559.20.42.002			2/24/2021	0975/Gumm 02/24/21	AB Dangerous Bldg. Postage	17.30	
94369	3/15/2021	012354		VISA - 1105,			\$68.14
001.0000.11.571.20.31.050			2/24/2021	1105/Martin 02/24/21	PKRC Flowers For MLK Celebrati	68.14	
94370	3/15/2021	011540		VISA - 1371,			\$1,526.92
001.0000.15.521.26.35.010			2/24/2020	1371/Gildeh 02/24/21	PD Helmet Harnesses	677.99	
001.0000.15.521.26.35.010			2/24/2020	1371/Gildeh 02/24/21	PD Face Shields	194.49	
001.0000.15.521.70.35.010			2/24/2020	1371/Gildeh 02/24/21	PD Camera	604.44	
001.0000.15.521.10.49.001			2/24/2020	1371/Gildeh 02/24/21	PD FBI LEEDA Membership: Gilde	50.00	
94371	3/15/2021	012401		VISA - 3408,			\$842.84
195.0024.15.521.30.31.001			2/24/2021	3408/Carroll 02/24/2	PD Comcast 01/23-02/22	347.87	
195.0024.15.521.30.31.001			2/24/2021	3408/Carroll 02/24/2	PD LPR Subscriptiion 01/14-02/	48.98	
001.0000.15.521.10.31.001			2/24/2021	3408/Carroll 02/24/2	PD Bike Shed	389.34	
195.0024.15.521.30.31.001			2/24/2021	3408/Carroll 02/24/2	PD Cleaning Supplies	56.65	
94372	3/15/2021	012656		VISA - 4197,			\$1,998.50
401.0000.41.531.10.49.003			2/24/2021	4197/Fin 6 02/24/21	PWSW Hazardous Waste Refresher	205.00	
001.0000.09.518.91.31.009			2/24/2021	4197/Fin 6 02/24/21	HR Wellness Valentines Day Can	24.50	
503.0000.04.518.80.35.001			2/24/2021	4197/Fin 6 02/24/21	IT LTE Card For Router	93.22	
302.0077.21.595.12.31.001			2/24/2021	4197/Fin 6 02/24/21	PWCP Card Stock - Mailers	395.44	
001.0000.06.515.30.41.001			2/24/2021	4197/Fin 6 02/24/21	LG Protection Orders, Cert. Ju	52.50	
302.0077.21.595.12.42.002			2/24/2021	4197/Fin 6 02/24/21	PW Postage	1,227.84	
94373	3/15/2021	012715		VISA - 5244,			\$591.02
001.0000.06.515.30.49.003			2/24/2021	5244/Schuma 02/24/21	LG Roles & Responsibilities We	35.00	
001.0000.06.515.30.41.001			2/24/2021	5244/Schuma 02/24/21	LG Case 21-2-04470-2 Filing Fe	241.50	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
104.0007.01.557.30.35.001			2/24/2021	5244/Schuma 02/24/21	HM Wireless Go Compact Mircrop	305.52	
001.0000.06.515.30.41.001			2/24/2021	5244/Schuma 02/24/21	LG Zoom Countdown Timer	9.00	
94374	3/15/2021	011659		VISA - 7212,			\$3,048.23
501.0000.51.548.79.48.005			2/24/2021	7212/PWOM 02/24/21	PKFL 1000 Hr. Svc, Oil Change	1,949.23	
501.0000.51.548.79.48.005			2/24/2021	7212/PWOM 02/24/21	PKFL Spray Landscape Trailer	1,099.00	
94375	3/15/2021	011136		VISA - 7750,			\$218.01
001.0000.15.521.80.31.001			2/24/2021	7750/Allen 02/24/21	PD Envelopes	153.86	
001.0000.15.521.40.49.001			2/24/2021	7750/Allen 02/24/21	PD IAPE Membership Renewal:All	50.00	
001.0000.99.518.40.42.002			2/24/2021	7750/Allen 02/24/21	ND PD Shipping: Tox Lab	14.15	
94376	3/15/2021	011137		VISA - 7768,			\$417.90
501.0000.51.521.10.31.006			2/24/2021	7768/Alwine 02/24/21	PD Wiper Blades	142.10	
001.0000.15.521.22.31.008			2/24/2021	7768/Alwine 02/24/21	PD CDT Uniforms	381.59	
001.0000.15.521.40.43.002				7768/Alwine 08/27/20	PD Room Cancellation Refund	-105.79	
94377	3/15/2021	011138		VISA - 7776,			\$1,287.35
501.0000.51.548.79.48.005			2/24/2021	7776/Anders 02/24/21	PKFL Bobcat Repair	1,287.35	
94378	3/15/2021	011140		VISA - 7800,			\$5,627.35
401.0000.11.531.10.49.003			2/24/2021	7800/Cummin 02/24/21	PKSW Hazardous Waste Refresher	235.00	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Pants: J Fors	284.88	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Pants: J Cotta	305.52	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Pants: T Cummins	305.52	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Gear: Powell, Cota,	1,435.06	
001.0000.11.576.81.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Gear: Higgins, Bird,	1,846.59	
101.0000.11.544.90.31.001			2/24/2021	7800/Cummin 02/24/21	PKST Rags For Shop	87.19	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Pants: R Toy	284.88	
101.0000.11.542.66.31.030			2/24/2021	7800/Cummin 02/24/21	PKST Salt Off	98.89	
501.0000.51.548.79.32.002			2/24/2021	7800/Cummin 02/24/21	PKFL Diesel For Truck	69.29	
101.0000.11.544.90.31.008			2/24/2021	7800/Cummin 02/24/21	PKST Work Pants: J Powell	305.52	
101.0000.11.542.66.31.001			2/24/2021	7800/Cummin 02/24/21	PKST Pump For Brine System	616.91	
101.0000.11.544.90.31.008				7800/Cummin 02/24/21	PKST Return Work Pants: J Powe	-247.90	
94379	3/15/2021	012863		VISA - 7935,			\$1,694.27
503.0015.04.518.80.35.030			2/24/2021	7935/White 02/24/21	IT Wireless Keyboards	181.30	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Pandora For CH	29.62	
503.0000.04.518.80.35.001			2/24/2021	7935/White 02/24/21	IT Power Adapter Cable	30.22	

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Creative Cloud	58.24	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Photography Plan	10.98	
503.0000.04.518.80.35.001			2/24/2021	7935/White 02/24/21	IT 50mm 12v Fan For Pi Devices	23.04	
503.0000.04.518.80.35.001			2/24/2021	7935/White 02/24/21	IT Mount Bracket	48.34	
503.0000.04.518.80.48.002			2/24/2021	7935/White 02/24/21	IT 3 Yr Electronics Protection	37.36	
503.0015.04.518.80.35.030			2/24/2021	7935/White 02/24/21	IT Cameras	639.60	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Monthly MailChimp	96.70	
503.0000.04.518.80.35.001			2/24/2021	7935/White 02/24/21	IT Switching Converter	34.94	
503.0000.04.518.80.35.001			2/24/2021	7935/White 02/24/21	IT Power Adapter Cable	43.18	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT 02/11-03/11 Fix & Protect	29.99	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Int'l Trx Fee On Fix & Prot	0.30	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT Thru 01/20/22 DreamHost Web	299.40	
503.0000.04.518.80.49.004			2/24/2021	7935/White 02/24/21	IT InDesign	23.07	
503.0015.04.518.80.35.030			2/24/2021	7935/White 02/24/21	IT External Hard Drive	107.99	
94380	3/15/2021	011158		VISA - 7966,			\$3,556.60
001.0000.15.521.10.31.020			2/24/2021	7966/Pitts 02/24/21	PD Range Supplies	118.13	
001.0000.15.521.40.49.005			2/24/2021	7966/Pitts 02/24/21	PD Recruiting Cards	65.93	
001.0000.15.521.40.49.001			2/24/2021	7966/Pitts 02/24/21	PD IACP Membership: Lawler	190.00	
001.0000.15.521.40.49.001			2/24/2021	7966/Pitts 02/24/21	PD WSPCA Membership: Bucat, Su	150.00	
001.0000.15.521.40.49.003			2/24/2021	7966/Pitts 02/24/21	PD Prof Sports & Ent Safety/Se	69.00	
001.0000.15.521.40.49.005			2/24/2021	7966/Pitts 02/24/21	PD Recruitment Postcards	76.04	
180.0000.15.521.21.49.003			2/24/2021	7966/Pitts 02/24/21	PD NTOA Registration: Dier, Cr	1,500.00	
001.0000.15.521.40.49.001			2/24/2021	7966/Pitts 02/24/21	PD Phlebotomy Licensel: Merril	222.50	
001.0000.15.521.40.49.003			2/24/2021	7966/Pitts 02/24/21	PD Backgrd Invest Reg: Catlett	770.00	
180.0000.15.521.21.49.003			2/24/2021	7966/Pitts 02/24/21	PD WSNIA Registration:Hamilton	395.00	
94381	3/15/2021	011163		VISA - 8014,			\$311.25
501.0000.51.548.79.48.005			2/24/2021	8014/Willia 02/24/21	PKFL Bobcat Repair	81.25	
001.0000.11.576.80.49.003			2/24/2021	8014/Willia 02/24/21	PK ISA Recertification: Bird	230.00	
94382	3/15/2021	011164		VISA - 8022,			\$115.00
001.0000.15.521.10.49.001			2/24/2021	8022/Zaro 02/24/21	PD FBI NAA Membership: Zaro	115.00	
94383	3/15/2021	011167		VISA - 8055,			\$128.91
001.0000.99.518.40.42.002			2/24/2021	8055/Fin 3 02/24/21	FN Stamps	77.00	
001.0000.04.514.20.31.001			2/24/2021	8055/Fin 3 02/24/21	FN 1099 Envelopes, Moisteners	51.91	
94384	3/15/2021	011172		VISA - 8105,			\$160.00

Check No.	Date	Vendor	Inv Date	Invoice	Description	Amount	Check Total
001.0000.15.521.40.49.001			2/24/2021	8105/PD2 02/24/21	PD Drone Pilot License: Nobles	160.00	
94385	3/15/2021	011714		VISA - 8434,			\$447.98
001.0000.15.521.10.49.001			2/24/2021	8434/LaVerg 02/24/21	PD IACP Membership: Zaro	190.00	
001.0000.15.521.10.49.001			2/24/2021	8434/LaVerg 02/24/21	PD Notary Renewal Lavergne	40.00	
001.0000.15.521.10.31.001			2/24/2021	8434/LaVerg 02/24/21	PD Pens	217.98	
94386	3/15/2021	011177		VISA - 8550,			\$131.28
001.0000.13.558.70.31.001			2/24/2021	8550/Newton 02/24/21	ED Gift Card For Business Surv	54.95	
001.0000.13.558.70.49.004			2/24/2021	8550/Newton 02/24/21	ED 01/29-02/28 Dropbox	13.21	
001.0000.13.558.70.49.003			2/24/2021	8550/Newton 02/24/21	ED WEDA 2021 Winter Conference	63.12	
94387	3/15/2021	012924		VISA - 9329,			\$2,109.32
001.0000.15.521.10.31.001			2/24/2021	9329/PD1 02/24/21	PD In/Out Board	27.38	
001.0000.99.518.40.42.002			2/24/2021	9329/PD1 02/24/21	ND PD 02/02 Certified Letters	70.00	
001.0000.15.521.26.31.001			2/24/2021	9329/PD1 02/24/21	PD Supplies	144.94	
001.0000.99.518.40.42.002			2/24/2021	9329/PD1 02/24/21	ND PD 02/04-02/23 Certified Le	92.95	
001.0000.15.521.10.31.020			2/24/2021	9329/PD1 02/24/21	PD Target Brackets	1,759.77	
001.0000.15.521.10.31.001			2/24/2021	9329/PD1 02/24/21	PD Phone Chargers	14.28	
94388	3/15/2021	011595		WALTER E NELSON CO,			\$1,283.45
502.0000.17.521.50.31.001			12/28/2020	789782	PKFC Janitorial Supplies	969.82	
502.0000.17.542.65.31.001			3/2/2021	797415	PKFC Paper Towels, Cleaning Su	313.63	
94389	3/15/2021	000595		WASHINGTON ASSOC OF SHERIFFS,			\$798.77
001.0000.02.523.30.41.001			1/31/2021	EM 2021-00158	MC 01/21 Home Monitoring	798.77	
94390	3/15/2021	012963		WIRELESS POLICY GROUP,			\$1,799.96
001.0000.07.237.20.00.003			3/15/2021	03/15/21	CD Small Cell Application	5,000.00	
001.0000.07.321.91.01.001			3/15/2021	03/15/21	CD Administrative Fee Revenue	-3,200.04	
# of Checks Issued		265					
Total		\$1,586,433.90					

Voids

\$1,799.96 Check 93990 Issued 12/31/2020

\$1,500.00 Check 94280 Issued 02/25/2021

TOTAL \$1,583,133.94



To: Mayor and City Councilmembers
From: Tho Kraus, Assistant City Manager/Chief Finance Officer
Through: John J. Caulfield, City Manager *John J. Caulfield*
Date: April 5, 2021
Subject: Payroll Check Approval

Payroll Period(s): February 16-28, 2021 and March 1-15, 2021

Total Amount: \$2,300,388.89

Checks Issued:

Check Numbers: 114291-114295

Total Amount of Checks Issued: \$17,258.83

Electronic Funds Transfer:

Total Amount of EFT Payments: \$509,528.10

Direct Deposit:

Total Amount of Direct Deposit Payments: \$1,578,010.09

Federal Tax Deposit:

Total Amount of Deposit: \$195,591.87

I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered, or the labor performed as described herein and that the claims are just and due obligations against the City of Lakewood, Washington, and that I am authorized to authenticate and certify said liens.

Dana Kapla
Finance Supervisor

Tho Kraus
Assistant City Manager
Chief Finance Officer

John J. Caulfield
City Manager

Payroll Distribution

City of Lakewood

Pay Period ending 02-16-2021 thru 03-15-2021

Direct Deposit and ACH in the amount of : \$2,283,130.06
 Payroll Ck#'s 114291-114295 in the amount of : \$17,258.83
Total Payroll Distribution: \$2,300,388.89

Employee Pay Total by Fund:

<u>Fund 001 - General</u>	<u>Amount</u>
City Council	\$ 9,400.00
Municipal Court	\$ 60,043.04
City Manager	\$ 16,583.34
Administrative Services	\$ 94,105.50
Legal	\$ 68,048.39
Community and Economic Development	\$ 91,732.04
Parks, Recreation and Community Services	\$ 87,624.79
Police	\$ 947,842.66
Non-Departmental	\$ -
General Fund Total	\$ 1,375,379.76

Fund 101 - Street	\$ 50,484.16
Fund 102 - Real Estate Excise	\$ -
Fund 104 - Hotel / Motel Lodging Tax	\$ -
Fund 105 - Property Abatement/Rental Housing Safety Program	\$ 16,177.30
Fund 180 - Narcotics Seizure	\$ 709.94
Fund 181 - Felony Seizure	\$ -
Fund 182 - Federal Seizure	\$ -
Fund 190 - CDBG Grants	\$ 17,432.80
Fund 191 - Neighborhood Stabilization Program	\$ -
Fund 192 - Office of Economic Adjustment/SSMCP	\$ 10,070.00
Fund 195 - Public Safety Grants	\$ 20,470.83
Fund 301 - Parks CIP	\$ 1,020.00
Fund 302 - Transportation CIP	\$ 68,956.65
Fund 311 - Sewer Capital Project	\$ 5,097.73
Fund 401 - Surface Water Management	\$ 41,967.14
Fund 502 - Property Management	\$ 11,801.41
Fund 503 - Information Technology	\$ 34,325.00
Fund 504 - Risk Management	\$ 994.25
Other Funds Total	\$ 279,507.21

Employee Gross Pay Total	\$ 1,654,886.97
Benefits and Deductions:	\$ 645,501.92
Grand Total	\$ 2,300,388.89

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 5, 2021	TITLE: Build Your Better Here Image Campaign 2.0	TYPE OF ACTION: <input type="checkbox"/> ORDINANCE NO. <input type="checkbox"/> RESOLUTION NO. <input checked="" type="checkbox"/> MOTION NO. 2021-20 <input type="checkbox"/> OTHER
REVIEW: February 8, 2021	ATTACHMENTS: BYBH Campaign 2019-2020 overview and 2021-2022 scope of work and budget	

SUBMITTED BY: Becky Newton, Economic Development Manager

RECOMMENDATION: It is recommended that the City Council authorize the City Manager to sign this agreement.

DISCUSSION: The Build Your Better Here Image Campaign 2.0 is part of the 2021-2022 carry forward budget.


This is a refresh of the 2019-2020 efforts to include new video and photo assets, digital ad design and placement, Build Your Better Here website updates and landing pages, print ad design and landing pages, and two brand awareness surveys.

The goal of the campaign has been to develop and implement a strategic image to promote the many benefits of living and working in our growing community. Key stories have included public safety, quality education, parks and recreation, and business development in various sectors with messaging that advertises Lakewood as the premier place for Better Living and Better Business.

The value proposition is that “The American Dream is more achievable here”. Target audiences are Starters (young, single, professional, or middle-class parents of young families), Builders (creators and established businesses and entrepreneurs), Long-Termers (long-term residents), and Warriors (Military service members, their families, and veterans).

ALTERNATIVE(S): 1) Authorize the City Manager to sign the agreement; 2) Deny this request

FISCAL IMPACT: The 1-time cost of this campaign is \$141,500 and is included in the upcoming proposed carry forward budget adjustment as a new request funded by general fund.

_____ Becky Newton Prepared by _____ Department Director	 _____ City Manager Review
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BUILD YOUR
BETTER HERE
2019-20 Review

2019–2020 Campaign Budget (\$179,250)

As demonstrated in this document, we set a plan and executed it while also adding value and responding to the COVID-19 pandemic.

Successes

- **Follow-through:** We created and executed a multimedia plan on-time and on-budget
- **Print ads:** we reached 100,000+ readers on the West Coast and led to engagement
- **Digital ads:** we reached 4,700,000 users, leading to 2,000,000 video views
- **Social media:** our posts reached almost 80,000 users and 2,000+ new Facebook page likes
- **Website:** BuildYourBetterHere.com was visited by 14,000 unique users who saw 39,000 pages
- **Downloads:** industry-specific business brochures were downloaded over 1,000 times
- **Links:** demographic-specific housing links were clicked over 1,000 times
- **Residents:** opinions of Lakewood as “Extremely favorable” rose 5%, NPS increased 10 points
- **Non-resident:** opinions of Lakewood as “Extremely favorable” rose 9%, NPS increased 2 points

Limitations

- **Brand awareness campaigns** are measured in exposure, not traditional ROI (e.g. item sales)
- Directly attributing businesses growth and home buying to this campaign is complicated
- COVID-19 presented many unique challenges to marketing (digital marketing was key)



**BUILD YOUR
BETTER HERE**
2019-20 Review

2021-2022 Tasks

We suggest **continuing** with the plan established in 2019 and **refreshing** the brand assets (images, videos, etc.).

- Refresh brand assets
- Fine tune outreach to Builders
- Intensify outreach to Starters
- Start pursuing Warriors
- Strengthen relationship with Long-termers

Rationale

- As seen with the website traffic stats and the video view stats, there is a direct relationship between these **marketing efforts** and the **brand exposure**.
- There is also a clear decline over time in the effectiveness of **reusing** the same assets (e.g. the 2019 brand video).
- As demonstrated in the brand sentiment surveys, continuing to invest in these efforts will support the improved **favorable opinions** of the City.

2021-2022 Budget

One-time Costs

- Refresh video + photo assets \$20,000
- Digital ad design, landing pages \$2,500
- Print ad design, landing pages \$2,500
- Brand awareness survey (x2) \$2,500
- \$27,500**

On-going Costs

- Digital ad spend \$4,000/mo
- Radio ad spend \$1,000/mo
- Print ad spend \$500/mo
- Ad mgmt., reporting (digital, radio, print) \$2,500/mo
- Social media videos, posts, etc. \$1,500/mo
- \$9,500/mo**

12-month Total

- One-time cost total \$27,500
- On-going cost total (12 months) \$114,000
- \$141,500**

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 5, 2021 REVIEW: April 5, 2021	TITLE: Authorizing the execution of an agreement with Clover Park School District for Extra Duty police services; School Resource Officers ATTACHMENTS:	TYPE OF ACTION: — ORDINANCE NO. — RESOLUTION NO. — MOTION NO. 2021-21 — OTHER
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SUBMITTED BY: Heidi Wachter, City Attorney

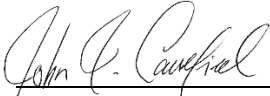
RECOMMENDATION: It is recommended that the City Council approve the proposed agreement between the City and the Clover Park School District (School District) for extra duty police services.

DISCUSSION: The City and School District have operated pursuant to earlier versions of this agreement for many years. Execution of the agreement has followed the same course as any other Extra-Duty contract. It has become apparent that the partnership has more of an ongoing nature and is of a more significant dollar value than other Extra Duty contracts and thus merits Council authorization.

Duties performed pursuant to this agreement involve police presence at school facilities, most typically as School Resource Officers. This work has been provided by agreement for many years and reflects a strong collaboration between the School District and the City.

ALTERNATIVE(S): This collaboration is a strong link between the City and the School District. Alternatives break this link.

FISCAL IMPACT: The amount paid by the City in the past two years is as follows: 2019: \$313,350.47 2020: \$101,239.47. The 2019 figure more accurately represents the cost as the more recent figure reflects the impact of COVID-19 (schools buildings were not in use). Pursuant to the contract, the School District pays the City this amount, which shows as revenue to the City. Thus the fiscal impact is equaled in revenue.

Heidi Wachter Prepared by Mike Zaro Department Director	 _____ City Manager Review
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City of Lakewood
EXTRA-DUTY POLICE SERVICES CONTRACT

This agreement is made and entered into this ____ day of _____, 2021, by and between the City of Lakewood (“City”) and Clover Park School District #400 (“Contractor.”)

WHEREAS, the Contractor owns, operates or manages a business for profit or non- profit within the City and, in connection therewith, requires police related services or assistance; and

WHEREAS, the City is able to provide duly commissioned law enforcement officers to provide police related services to the Contractor subject to payment for services and subject to the approval of the City’s Chief of Police; and

WHEREAS, the duly commissioned officers who may provide such service will be assigned to extra-duty under this contract during their off-duty hours which will not conflict with their law enforcement responsibilities for and duties to the City;

NOW, THEREFORE, the parties agree as follows:

1. SCOPE OF PERFORMANCE.

The City shall provide the Contractor with extra-duty police officer services as described below:

(a) **Number of officers:** As requested by the Contractor and subject to the approval of the City’s Chief of Police, Contractor requests four (4) officers per school day.

(b) **Hours and dates to be worked:** As requested by the Contractor and subject to the approval of the City’s Chief of Police, with a four (4) hour minimum extra duty time per event.

If Contractor is requesting various dates and times, the dates will occur between April 1, 2021 and June 30, 2022. The days and hours of work are subject to change as business warrants. Specific dates requested by Contractor shall be provided to the City in writing at least three (3) days in advance of the requested actual performance date(s).

(c) **Specific location of service.** As requested by the Contractor and subject to the approval of the City’s Chief of Police, Contractor requests service at various Clover Park School District schools, facilities, and properties.

(d) **Scope of Performance:** The City agrees to provide proactive law enforcement services for, and at, various Clover Park School District schools, facilities, and properties. Such

services shall include but not be limited to, issues of safety and security, crowd and/or traffic control; also, shall include responding to calls for service, conducting criminal investigations, making arrests, writing official reports, issuing exclusion orders, monitoring activities and ensuring the safety and security of the public at the aforementioned location(s). The services shall be provided in full uniform and shall be accompanied by a patrol vehicle. It is the expectation that the officers scheduled for pre-determined shifts will be present to complete their shifts except for the following circumstances: agency callback, late call/report from primary employment, family emergency, personal illness, all of which will be considered excused. The officers understand that it shall be their responsibility to make appropriate notifications to the LPD Extra Duty Coordinator and to make every attempt to find a replacement for their shifts. Failure to show for a scheduled shift for reasons other than described as excused will be considered a breach of this agreement and may result in sanctions up to, and including, removal from this Extra Duty detail.

2. CONTRACT TERM.

(a) This agreement shall commence on April 1, 2021 and shall expire on June 30, 2022. The parties may extend this agreement in writing for an agreed period of time.

(b) Either party may terminate this contract for any reason and at any time when, in its sole discretion, it is in the best interest of the terminating party by giving ten (10) days prior written notice to the other party. Cancellation of extra-duty assignment by the Contractor less than twenty-four (24) hours before the duty is scheduled to begin results in charge of two (2) hour minimum fee for each officer assigned. Should an officer be called from contractor service (agency callback, late call/report from primary employment, family emergency, personal illness) there will be no charge to the contractor.

3. COMPENSATION.

Contractor shall pay the City as full compensation for all police services furnished under this Contract that are provided during normal school hours a fee of \$70.59 per hour of officer time for each officer as itemized below.

a. Hourly Wage Rate (four-hour minimum)	\$58.23
b. Social Security Cost	\$ 2.78 (4.77%)
c. Retirement	\$ 5.11 (8.77%)
d. Medicare	\$.84 (1.45%)
e. Unemployment	\$.12 (0.2%)
f. Labor & Industries	\$ 1.51
g. Administrative Overhead	<u>\$ 2.00</u>

TOTAL COST PER OFFICER PER HOUR: \$70.59

For special event security services outside of normal school hours, Contractor shall pay the City as full compensation for all police services furnished under this contract a fee of \$75.58 per hour of officer time as itemized below.

a. Hourly Wage Rate (four hour minimum)	\$62.56
b. Social Security Cost	\$ 2.98 (4.77%)
c. Retirement	\$ 5.49 (8.77%)
d. Medicare	\$.91 (1.45%)
e. Unemployment	\$.13 (0.2%)
f. Labor & Industries	\$ 1.51
g. Administrative Overhead	<u>\$ 2.00</u>

TOTAL COST PER OFFICER PER HOUR: \$75.58

4. PAYMENT.

The Contractor shall compensate the City for law enforcement services described herein pursuant to the all-inclusive payment rate shown above. The City shall invoice the Contractor on the 15th of the month following the service and include detailed service information, including actual service hours and all-inclusive hourly rate per officer. Payment shall be made by a check payable to the City of Lakewood and sent to the City within thirty (30) calendar days after receipt of invoice. Payments will include a processing fee of \$10.00. Payments to the City that are received later than thirty (30) days following the invoice dates shall accrue interest at the rate of 12% per annum on the balance due. All payments shall first be applied to the accrued interest.

The Contractor shall not be financially liable for scheduled law enforcement services which are not actually performed, if the work performed was because an officer was unable to complete his shift. However, the Contractor is financially liable for scheduled law enforcement services at the designated hourly minimum in all other circumstances.

5. SCHOOL RESOURCE OFFICER (SRO)

Police officers who serve as school resource officers must complete the training requirements as prescribed in RCW 28A.320.124 before they are eligible to work for and at schools, facilities, and properties. School resource officers and school officials will also adhere to the mutually agreed upon standard operating procedures as written in Clover Park School Districts Procedure 4311-P1 and 4311-P2.

6. DATA COLLECTION AND REPORTING (SROs ONLY)

The SRO, City, and the Contractor shall work together to ensure the proper collection and reporting of data using the Clover Park School District SRO Reporting Form (Procedure 4311-F1) regarding calls for law enforcement service and the outcome of each call. The data will be completed on the Contractors website, disaggregated by school, offense type, race, gender, age, and students who have an individualized education program or plan developed under section 504 of the federal rehabilitation act of 1973. Data collection shall be maintained by the Contractor.

7. COMPLAINT PROCESS AGAINST AN SRO

Any complaint about an SRO should be referred to the Patrol Supervisor on-duty for the City at the time the complaint is received. This can be done by calling the Police Non-Emergency line at (253) 287-4455. In addition, complaints may be made in person at the Lakewood Police Station, by phone, in writing, or anonymously. Additional information on this process is located on the Lakewood Police Department's website.

8. DUTY STATUS.

Each police officer assigned to work pursuant to this Contract is subject to call by the City's Chief of Police or the Chief's designee at any time for emergencies, special assignment, or overtime duty. Extra duty employment shall not infringe or interfere with this obligation.

9. ADHERENCE TO CITY POLICIES AND PROCEDURES – SCOPE OF WORK.

Police officers engaged in extra duty employment are obligated to discharge all duties of their office and adhere to the City's Police department policies and procedures at all times. Such officers shall obey, uphold and enforce the laws of the City and State of Washington and the Constitutions of the State of Washington and the United States of America at all times. Such officers shall understand that while they are on duty or engaged in extra duty assignment under this Contract, they may be subject to discipline by the City. Police officers performing work pursuant to this Contract shall only provide law enforcement/peacekeeping services as specified in Section 1(d) of this Contract and they are at all times while performing said services subject to the direction and control of the City Police Department only.

10. NO SPECIAL DUTY TO CONTRACTOR OR OTHERS.

Police officers on extra duty assignment have a primary obligation to the City, not the Contractor. They are expected to discharge all duties of their office while performing pursuant to this Contract and will not perform any non-law enforcement/peacekeeping functions for Contractor. Furthermore, this contract and performance thereof by the City police officers shall not create any special relationship with any person or duties to protect any specific persons from harm or injury including the party signing this contract. The law enforcement/peacekeeping duties to be performed pursuant to this contract are the same in extent and scope as those provided by police officers to every member of the public.

11. NONDISCRIMINATION.

The Contractor shall not discriminate on the basis of race, color, sex, religion, national origin, creed, the presence of any sensory, mental or physical handicap or any other protected class status.

12. LIABILITY.

Each party shall be responsible and liable for the consequences of any act or failure to act on the part of itself, its employees and its agents. Each party shall be responsible for its own negligence; neither party shall indemnify or hold the other party harmless.

13. NOTICES.

All notices and other material to be delivered under this Contract shall be in writing and shall be delivered or mailed to the following addresses:

City: City of Lakewood
ATTN: Finance Department 6000 Main Street SW
Lakewood, WA 98499

Contractor: Clover Park School District
Attn: Administrator of Business Services
10903 Gravelly Lake Drive SW
Lakewood, WA 98499

Or such other addresses as either party may, from time to time, designate in writing.

14. SIGNATURES.

The parties agree that all signatures, whether original, copy or faxed on this agreement will be considered originals.

15. ENTIRE AGREEMENT.

No modification or amendment of this contract shall be effective unless in writing and signed by authorized representatives of the parties. This contract contains the entire agreement between the parties and may not be enlarged, modified or altered except in writing signed by the parties.

IN WITNESS WHEREOF, the parties have executed this Agreement on this date, the _____ day of _____, 2021.

City of Lakewood

Clover Park School District

John Caulfield, City Manager / Date
City of Lakewood

Ron Banner, Superintendent / Date
Clover Park School District #400

Attest:

Briana Schumacher, City Clerk / Date

Approved as to legal form only:



3/15/2021
Samantha Johnson, Asst City Attorney / Date

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 5, 2021	TITLE: Appointing Linn Larsen to serve on the Planning Commission through December 15, 2021.	TYPE OF ACTION: <input type="checkbox"/> ORDINANCE <input type="checkbox"/> RESOLUTION <input checked="" type="checkbox"/> MOTION NO 2021-22 <input type="checkbox"/> OTHER
REVIEW:	ATTACHMENTS: Candidate application	

SUBMITTED BY: Briana Schumacher, City Clerk on behalf of Mayor Don Anderson

RECOMMENDATION: It is recommended that the City Council confirm the Mayor’s appointment of Linn Larsen to serve on the Planning Commission through December 15, 2021.

DISCUSSION: Notices and outreach to seek volunteers to fill vacancies on the Planning Commission were sent to neighborhood associations, civic groups, community organizations, The News Tribune and The Suburban Times. In addition, articles were distributed through the City’s Weekly Informational Bulletins and notices were posted on the City’s website.

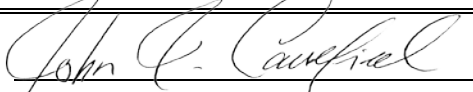
The role of the Planning Commission is to assist the City Council in the following areas:

Planning Issues:

- Assists City personnel in preparing a comprehensive plan for the City in accordance with state law to be submitted to the City Council for consideration of adoption.
- Recommends to the City Council such changes, amendments or additions to the comprehensive plan as may be deemed desirable.

ALTERNATIVE(S): The Council could choose not to confirm the appointment or re-advertise for this position.

FISCAL IMPACT: There is no fiscal impact.

Briana Schumacher Prepared by Heidi Wachter Department Director	 City Manager Review
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DISCUSSION: (Continued)

- Recommends land use and zoning regulations and other development regulations as deemed necessary and/or appropriate. Act as the research and fact finding agency of the City in regard to land uses, housing, capital facilities, utilities, transportation, and in regard to classification of lands as agriculture, forest, mineral lands, critical areas, wetlands and geologically hazardous areas. Undertakes surveys, analyses, research and reports as may be generally authorized or requested by the City Council.
- Cooperates with planning agencies of other cities and counties, to include regional planning agencies, in furtherance of such research and planning; and
- Annually provides to the City Council a report on progress made in implementing the goals and requirements of State law and on the status of land use policies and procedures within the city.

Redevelopment Issues:

- Facilitate cooperation and coordination between various business groups and impacted neighborhoods on business issues;
- Facilitate the formation of specific neighborhood commercial business groups to assist in the enhancement of various existing commercial areas, aid in stabilizing and retaining commercial enterprises within these areas to maintain viability as a commercial area, and help in identifying specific needs of businesses within various commercial areas.
- Make recommendations to the City Council and to City staff for programs in which the City could or should participate to enhance commercial development opportunities in the City, which programs may be in cooperation with any appropriate private, public, civic or community agency, group or association of or in the City, county, state or federal government;
- Recommend ways and means of obtaining private, local, county, state or federal funds and other participation for the promotion of business development projects within the City, especially those of an incubator type;
- Work with City of Lakewood staff, City Council, task forces and other City/community based groups, as directed by the City Council, on relevant issues and projects; and
- Assist in data base development for the creation and maintenance of a community profile.

Transportation Issues:

- Facilitate cooperation and coordination with the Public Works Department of the City on street, public works and transportation and infrastructure related projects and plans,
- Identify, evaluate and recommend to the City Council, City Manager and/or City staff policies and projects for the City, annual update of its Six-Year Transportation Plan, and for other transportation and infrastructure planning purpose of the City,
- Recommend ways and means of obtaining private, local county, state or federal funds for promotion of transportation and infrastructure facilities of the City, and
- Advise the City Council on acquisition, replacement and maintenance of transportation and infrastructure facilities of the City
- Advise the City as to the manner that public information on street related projects can best be disseminated, given the nature and/or scope of the projects.
- Advise the City Council regarding transportation related facilities, needs and programs of the City, as may be referred by the City Council.



CITY OF LAKEWOOD
6000 Main Street SW
Lakewood, WA 98499

APPLICATION FOR APPOINTMENT

*The information in this document is subject to public disclosure and can be made available to the public.
(Attach additional pages if necessary to complete answers.)*

I wish to be considered for appointment to the following committee, board or commission:

- | | |
|--|--|
| <input type="checkbox"/> Arts Commission | <input type="checkbox"/> Parks and Recreation Advisory Board |
| <input type="checkbox"/> Community Services Advisory Board | <input checked="" type="checkbox"/> Planning Commission |
| <input type="checkbox"/> Lakewood's Promise Advisory Board | <input type="checkbox"/> Public Safety Advisory Committee |
| <input type="checkbox"/> Landmarks and Heritage Advisory Board | <input type="checkbox"/> Salary Commission |
| <input type="checkbox"/> Lodging Tax Advisory Committee (Members of this committee must be representative of an agency involved in tourism promotion.) | |

EXPECTATIONS: Adhere to City of Lakewood's Code of Ethics and regular attendance at meetings is required.

PLEASE RETURN THIS FORM TO: City of Lakewood - City Clerk's Office
6000 Main Street SW
Lakewood, WA 98499
(253) 983-7705 Fax: (253) 589-3774
Email: bschumacher@cityoflakewood.us

Name: Linn Larsen
(Please Print)

Home Address: 11531 Clovercrest DR. SW

City: Lakewood State: WA Zip: 98499

Home Phone Number: 2539847299 E-mail: linn@linnlarsen.com

Present Employer: Linn Larsen, LLC

Address: P.O. Box 2061, Tacoma WA 98401 Work Phone: 2535892222

Cell: 2535892222

LODGING TAX ADVISORY COMMITTEE APPLICANT QUESTIONS:

Are you representing a business that is required to collect lodging tax? Yes No

Are you involved in activities authorized to be funded by revenues received from lodging tax? Yes No

Have you previously served or are you currently on one of the Lakewood's Boards, Committees or Commissions? Yes No If yes, please explain (include names of Boards, Committees or Commissions and the dates that you served:

Lakewood Redevelopment Advisory Commission, since terminated, 10 years, major achievements the development guidance of Pacific Highway South (Ponders) and Sound Transit Garage facility (originally scheduled as a surface parking lot, we suggested the City require a garage), other community service.

Date available for appointment: **March 15, 2021**

Are you available to attend evening meetings? Yes No

Are you available to attend daytime meetings? Yes No

Recommended by: **Don Anderson**

Education:

BABA Major Field Real Estate and Urban Development, University of Washington '69. Various Courses in Real Estate and Related matters over time, taught real estate fundamentals classes at Tacoma Community College in 1970-1980's.

Professional and/or community activities:

Tacoma Beautification Committee (scoped initial creation of Schuster Parkway development by the City of Tacoma, various other work), approx 35 years Tacoma Rotary #8, Scoutmaster Troop 51, St. Mary's Church, family a founding family of St. Francis Cabrini Catholic Church and School, others., Lakewood REDAB Commission as cited above.

Please share some of the experiences or qualifications that you have relating to the work of this board, committee or commission:

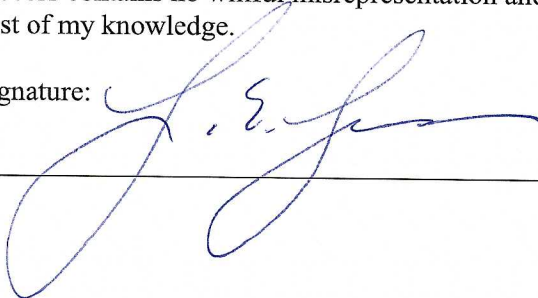
Lived and worked in Lakewood since 1961, with intermittent "absences" due to military service and out of area, mostly Tacoma, residences. have lived in Lakewood since 1999. High degree of familiarity with aspects of real property. Over 50 years experience in Real Estate as Broker and property owner, including commercial property in Lakewood, last 46 years with emphasis in Commercial and Industrial Real Estate, High degree of experience in compatible/in-compatible property uses, with a strong creative and collaborative approach to accomplish compatibility (or not).

Please explain why you would like to be part of this board, committee or commission:

I wish to contribute to the betterment of Lakewood as a place to live and work. I believe my experience and judgement are highly applicable to accomplishment of this goal.

I hereby certify that this application and any other materials and/or documents provided in this application process contains no willful misrepresentation and that the information given is true and complete to the best of my knowledge.

Signature:



Date: **March 11, 2021**



**LAKESWOOD ARTS COMMISSION
REGULAR MONTHLY MEETING Minutes**
Date: Monday, February 1, 2021 Time: 5:00PM – 6:15 PM
Virtual Zoom Meeting

CALL TO ORDER

The meeting was called to order at 5:05 p.m.

ATTENDANCE:

Arts Commission Members Present: Linda McDermott, Earl Borgert, Adriana Bayer, Paige Hansen, Lani Neil, Darryl Owens, Susan Warner, Phil Raschke

Arts Commission Members Excused: Patti Belle, Lua Pritchard

Youth Council Liaison: Angel Lee- present, Estaban Panangelinan-absent, Kayala Purdie-absent, Arianie Eperson- present

Staff: Sally Martinez, Recreation Coordinator, Nicolette York, Office Assistant

Council Liaison: Jason Whalen

Guest: Kelly Pajek Director of creative art at 4 Culture.

Continuing Education: Linda welcomed Kelly Pajek, Director for public art at 4 Culture, Seattle. She has over 20 years' experience working on high profile, large scale temporary and permanent public art nationwide. Kelly presented a Power Point presentation with information about 4 Culture, different programs areas, the gallery in Pioneer Square and the value of public art. Public art can reflect a community and give community pride. It can make connections. Public art can be an economic driver and creates cultural tourism. She shared an example of art at the SeaTac airport. The dollars that we spend don't just go to the artist, there are other jobs for engineers, lighting, fabricators and installers. She shared many examples of types of public art. She brought up partnerships to make public art dollars go further. She highlighted that artists can affect social change or highlight larger social issues. She discussed an artist developed master plan to discuss how creative projects can move through the community. She highlighted some best practices. Make sure the process to select artist is fair, inclusive, consistent. Be transparent about this process. Diffuse controversy. Think about what you can do to make the art extraordinary and create cultural experiences. Make sure that artists are paid fairly for their work. Susan Warner asked Kelly, "What is the greatest lesson you have learned over that you would be willing to share?" Kelly said she probably had many but what came to mind was Don't underestimate what artists can do and what they are capable of. Be open about what the opportunity is. Sometimes when projects are too tightly scoped it might not offer an opportunity for an artist to be as inventive as they can be. The board thanked Kelly for her time and presentation.

Approval of Minutes: Adriana Bayer moved to approve the January 4, 2021 minutes and Susan Warner seconded. MPU.

Appointments: Sylvi Johnson's application is still pending.

Unfinished Business:

Committees: Linda reviewed the new sub committees. Members are more than welcome to be on either or both.

Public Art subcommittee: (includes Colonial Plaza Art, Utility Box wraps, Special Events, Rotating art) Lead Earl Borgert, Darryl Owens, Jason Whalen, Lani Neil, Patti Belle, Susan Warner.

Performing Arts subcommittee: (Includes Film Festival, Special Events, Summer Concert Series) Lead Linda McDermott, Lani Neil, Lua Prichard, Paige Hansen, Adriana Bayer, Phil Raschke)

Public Art Committee Reports:

Colonial Plaza Art update: Linda advised that Sally will be making a presentation to council on February 8th to review the plan for the art at the colonial plaza. The budget is \$90,000. It is going to be a call to artists from a state list. The subcommittee will review and select 3 artists to make a presentation to council.

Utility Box Wrap Update: Earl reviewed the utility box plan. The subcommittee still needs to determine which boxes will be selected. There is only enough money in the budget for two wraps to start. Sally will be working on sending out a call to artists soon. Looking into other funding options. Jason Whalen asked about meeting with Sally to see if we can get a few more utility boxes. Another subcommittee meeting will be scheduled. Earl asked for the board to keep a look out for the best locations for the utility wraps to be placed.

Performing Arts: No updates as the film festival is tabled. The summer concert series bands have already been selected.

25th Anniversary Art Contest: The deadline has been extended to February 15th. Please spread the word. The contest is open to local artists. <https://cityoflakewood.us/25th-anniversary-art-contest/> Paige will send out to other Facebook groups. Susan said Lakewold Gardens will share with their constituents. Susan asked about sending out to the listserv. Adriana will ask the district to send out again now that elementary is going back to in person learning and kids will have access to materials.

Mural project: There is thought to create a moveable mural. Lakewood Multicultural Coalition (LMCC) has contributed \$2,500. Pierce College has contributed \$500. The project still needs additional funding.

Council Comments: None

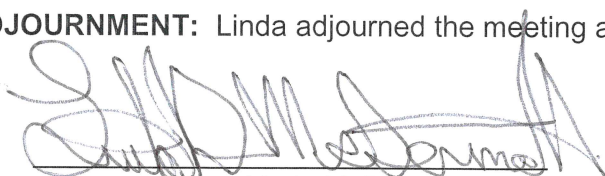
Board Comments:

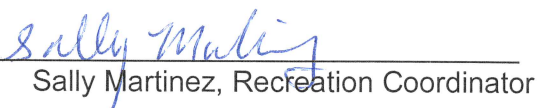
Paige loves the new shelter near Waughop Lake at Fort Steilacoom Park and is looking forward to busking there. She can't wait to be out and singing again.

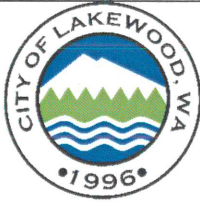
NEXT MEETING:

Monday, March 1, 2021 at 5:00 pm via Zoom

ADJOURNMENT: Linda adjourned the meeting at 6:10 p.m.


Linda McDermott, Chair


Sally Martinez, Recreation Coordinator



**PLANNING COMMISSION
REGULAR MEETING MINUTES
February 17, 2021
Zoom Meeting
6000 Main Street SW
Lakewood, WA 98499**

Call to Order

Mr. Don Daniels, Chair called the ZOOM meeting to order at 6:30 p.m.

Roll Call

Planning Commission Members Present: Donne Daniels, Chair; Connie Coleman-Lacadie, Paul Wagemann, James Guerrero, Nancy Hudson-Echols, Ryan Pearson and Phillip Combs

Planning Commission Members Excused: None

Commission Members Absent: None

Staff Present: Dave Bugher, Assistant City Manager for Development Services; Tiffany Speir, Long Range & Strategic Planning Manager; and Karen Devereaux, Administrative Assistant

Council Liaison: None

Approval of Minutes

The minutes of the meeting held on February 3, 2021 were approved as written by voice vote M/S/C Wagemann/Combs. The motion passed unanimously, 7 - 0.

Agenda Updates

None

Guest Attendees

Members of the City's Community Services Advisory Board (CSAB) John Mayfield, Sara Yamamoto, and Edith Owen-Wallace attended the Commission meeting to hear the 2020 Annual Housing Report presentation.

Public Comments

This meeting was held virtually to comply with Governor Inslee's Emergency Proclamations 20-28 and its addendums. Citizens were encouraged to virtually attend and to provide written comments prior to the meeting. No public comments were received.

Public Hearings

None

Unfinished Business

None

New Business

Annual Housing Report

Mr. Bugher presented the Lakewood 2020 Annual Housing Report that included the following topics: Housing and Urban Development (HUD) overview of current housing in the Tacoma-Lakewood area; Rents in Lakewood and surrounding communities; Lakewood's housing production in 2020; Recap of the City's CDBG/HOME Housing Programs; Annual report on the Rental Housing and Safety Program RHSP); Recap of "The State of Affordable Housing in Pierce County 2020", and closing thoughts on what to expect for the remainder of 2021.

Proposed Comprehensive Plan Amendment 2021-01: Energy & Climate Change Chapter

Mr. Bugher presented the purpose of the preliminary draft Energy & Climate Change Chapter, which discusses impacts from COVID-19, climate change impacts to Washington and Lakewood, climate change advantages and challenges for Lakewood, local energy generation and use, and citywide current and projected GHG emissions and reductions.

If adopted, the Energy & Climate Change Chapter would replace the current Comprehensive Plan Sustainability Chapter. Since Lakewood is not a “full service city” (i.e., it does not own or operate the water, sewer or power utilities - Lakewood’s energy utilities are operated by either public or private agencies, water is provided by the Lakewood Water District, and public sewer is provided by Pierce County), data was difficult to gather on Lakewood’s share of the region’s greenhouse gas (GHG) emissions; the draft chapter uses an extrapolation of data gleaned from a 2015 report prepared by the Puget Sound Clean Air Agency.

Report from Council Liaison

None.

Reports from Commission Members and Staff

Future Planning Commission Agenda Topics

March 24:

- Review of proposed 2021 Comprehensive Plan amendments 2021-02, -03, -05, -07, -09 and -11

April 7:

- Review of Comprehensive Plan Amendments 2021-01, -04, -06, -08, and -10
- 2020 Shoreline Restoration Report

April 21:

- 2021 Comprehensive Plan Amendment Package Public Hearing
- Six Year Transportation Improvement Program 2022-2027

Regional Planning Land Use Updates

None

Other

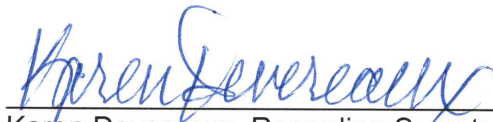
None

Next Regular Meeting: The next regular meeting would be held on March 24, 2021. (The March 3, 2021 meeting was cancelled and the March 17 meeting was rescheduled.)

Meeting Adjourned at 7:55 p.m.



Don Daniels, Chair
Planning Commission 03/24/2021



Karen Devereaux, Recording Secretary
Planning Commission 03/24/2021



**LAKESWOOD PARKS & RECREATION ADVISORY BOARD
REGULAR MEETING MINUTES
Tuesday February 23, 2021– 5:30 PM
Zoom Meeting**

CALL TO ORDER

Jason Gerwen called the meeting to order at 5:31 p.m.

ATTENDANCE

PRAB Members Present: Jason Gerwen-Chair, Vito Iacobazzi-Vice-Chair, Sylvia Allen, Alan Billingsley, Susan Dellinger, Michael Lacadie

Youth Council: Arianie Esperon-present, Micah Kim-absent, Carly Sherman-absent

Staff Present: Mary Dodsworth – Director, Nikki York – Office Assistant

Council Liaison: Linda Farmer, absent

Guests: Cameron Fairfield, Elizabeth Foster, Christy C

APPROVAL OF MINUTES: Michael moved and Vito Iacobazzi seconded the motion to approve the minutes of the January 26, 2021 meeting as written. MPU.

Public Comment: Elizabeth Foster noted that a basketball court at Fort Steilacoom would be great for her teenage boys

NEW BUSINESS:

Park Entry Signs Update: Mary Dodsworth presented a power point with the history of the parks entry signs. They are old, sad looking signs. New signs are a part of the Parks 2021 CIP update. Mary highlighted the Fort Steilacoom sign at the entrance that looks like a Gateway Sign. There needs to be a sign at the Angle Ln/Elwood entrance. Mary shared options to consider. Considerations in selecting the new sign are maintenance and graffiti. The board really likes option 1. It has similarities to the gateways. The colors are nice. The 2nd one has nice symmetry. The block font on the first one is easy to read. The board liked the kiosk.

Outdoor Adventure Programming: Cameron Fairfield shared a power point presentation. He highlighted the new Agents of Discovery Program an educational mobile gaming platform that uses augmented reality to get Youth Active. It is similar to Pokémon Go. It's a free app. Once downloaded it doesn't require wifi to work. Our field agent is a raccoon customized with a City of Lakewood log. Cameron described some of the types of challenges. There can be multiple challenges is a mission. Discussed possible prizes. Missions will be updated seasonally and for special events. Spanish and English are available. This program is a subscription service to the City. The missions are geared mostly for kids. Mary shared other possibilities with older adults and the youth council. We will work with our communications manager to promote and use A frames at the park. Summer programming has led us to applying for the No Child Left Inside Grant. The campaign will be call Go & Play Lakewood (Get Outside & Play, Laugh, be Active Youth). It will consist of different monthly outdoor activities including (orienteering, geocaching,

fishing event, water sports, kite making/flying, youth fitness day, birds, birds, birds, camping skills, art & nature, milk boat races, gardening and bubble stations). The board agreed that these are great activities.

Parks CIP Update- Harry Todd Park: Mary Dodsworth provided an update regarding the improvements at Harry Todd Park in the Tillicum neighborhood. It is an isolated area. The site is 17 acres. Phase I & II were able to be combined due to grants. Mary reviewed the site and needed updates. The final design allowed us to save 14 oak trees, move the restroom, add a fishing pier. The restroom will have individual ada units. Mary discussed making a decision to have lifeguards this summer.

Directors Report: Mary Dodsworth reviewed the Directors Report including, Capital Project updates, City Hall will remain closed until June, Senior Center update including (zoom happy hour, zoom presentations, drive thru events, zoom reunions, exercise videos and computer assistance), Public art including (colonial plaza art, signal box art, 25th anniversary art contest), 25th anniversary proclamation, approval of non-motorized trail and Community outreach.

Board Comments: None

NEXT MEETING: The next PRAB meeting is scheduled for Tuesday March 23, 2021 at 5:30 p.m. This will be a virtual Zoom Meeting.

ADJOURNMENT: Michael Lacadie moved to adjourn the meeting at 6:34 p.m. Sylvia Allen seconded. MPU



Jason Gerwen, Chairman



Nikki York, Office Assistant



TO: City Council
FROM: Tiffany Speir, Long Range & Strategic Planning Manager
THROUGH: John Caulfield, City Manager *John P. Caulfield*
Dave Bugher, Assistant City Manager for Development Services
DATE: April 5, 2021
SUBJECT: Draft Lakewood Station District Subarea (LSDS) Plan, Development Code and SEPA Planned Action for Public Hearing

ATTACHMENTS: **Attachment A** (LSDS Plan); **Attachment B** (LSDS Hybrid Form-Based Code (proposed LMC Title 18C); **Attachment C** (LSDS SEPA Planned Action); **Attachment D** (Amendments to LMC Title 18A to reflect adoption of LSDS Subarea Plan and LMC Title 18C); **Attachment E** (Identified pending 2021 amendments to Comprehensive Plan text reflecting adoption of LSDS package); **Attachment F** (Table of public comments received at Planning Commission level and CEDD responses); **Attachment G** (February 3, 2021 Planning Commission Meeting Minutes)

BACKGROUND

On March 8, 2021, the City Council reviewed the draft Lakewood Station District Subarea (LSDS) Plan and the implementing Hybrid Form-Based Code as recommended by the Planning Commission, as well as the Planned Action. The public hearing is April 5.

In its recommendation, the Planning Commission included several amendments to the parking requirements that the CEDD has recommended be removed so the Plan and Code include the original parking standards. The Commission also included a Whereas clause in its Resolution 2021-01 stating: “the Planning Commission requests that the City Council consider how to preserve existing affordable housing that is safe and secure as well as encourage new affordable housing to minimize the risk of current residents’ displacement.”

At its March 8 study session, the Council agreed to have Commissioner James Guerrero present the information he provided the Planning Commission regarding parking to the Council at the start of their April 5 public hearing.

The attached draft Lakewood Station Plan, Code and SEPA Planned Action have been updated (i.e., updated maps, amendments to development code to include plaza streets and alleys and a park fee-in-lieu option similar to that in the Downtown Subarea) as well as several technical and scrivener corrections made since the March 8 study session. The attached code also includes the edits to the parking requirements as recommended by the Planning Commission.

RECOMMENDATION: It is recommended that the City Council adopt the proposed LSDS Plan, Code and SEPA Planned Action after amending the Commission-recommended parking requirements to the requirements originally included in the documents.

ATTACHMENT A

STATION DISTRICT SUBAREA PLAN



City of Lakewood – DRAFT March 29, 2021

Prepared for the City of Lakewood

Prepared by: BERK, Fehr & Peers, Framework, and Skipstone



Acknowledgements

Lakewood's elected and appointed officials and City staff wish to acknowledge those in the community that participated and contributed their valuable assistance in the preparation and production of this document.

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 Paul Bocchi
 Michael Brandstetter
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Table of Contents

The Lakewood Station District 1

 Vision and Concept..... 2

 Summary of Existing Conditions..... 10

Land Use and Urban Design 12

 Context..... 12

 Future Land Use 17

 Land Use Regulation..... 19

 Public Spaces 30

 Policies and Strategies..... 33

Housing 35

 Context..... 35

 Housing Concepts 37

 Preventing Displacement..... 42

 Policies and Strategies..... 44

Economic Development, Business, & Employment 46

 Context..... 46

 Policies and Strategies..... 48

Transportation 49

 Context..... 49

 Level of Service 50

 Improvements 51

 Policies and Strategies..... 54

Utilities & Public Services 56

 Context..... 56

 Policies and Strategies..... 61

Implementation Plan 62

List of Exhibits

Exhibit 1. Lakewood Station District Subarea and Vicinity	2
Exhibit 2. Lakewood Station District Subarea.....	4
Exhibit 3. Lakewood Landing Location and Concept.....	5
Exhibit 3a. Lakewood Station District Location and Access.....	5
Exhibit 4. LSDS Planning Concept.....	7
Exhibit 5. Survey Responses - Future LSDS Priorities	9
Exhibit 6. Existing Conditions Summary Table.....	11
Exhibit 7. Existing Land Use	14
Exhibit 8. Parcel Size in the LSDS.....	15
Exhibit 9. Summary of Environmental Conditions in the LSDS ..	16
Exhibit 10. Environmental Clean-up Sites in the LSDS	17
Exhibit 11. Future Land Use in the LSDS.....	19
Exhibit 12. Zoning in the LSDS	22
Exhibit 13. LSDS Hybrid Form-Based Code Overlay	24
Exhibit 14. Green Street Concept #1 – 80’ Right-of-Way	26
Exhibit 15. Green Street Concept #2 – 60’ Right-of-Way	26
Exhibit 16. Boulevard Street Concept – 80’ Right-of-Way	27
Exhibit 17. Residential Street Typology – 60’ Right-of-Way..	28
Exhibit 18. Residential Street Typology - 80’ Right-of-Way ..	28
Exhibit 19. Green Space Opportunities in the LSDS.....	32
Exhibit 20. Existing Residential Structures in the LSDS.....	37
Exhibit 21. Residential Target Areas	42
Exhibit 22. Estimated Displacement Risk	43
Exhibit 23. Transportation Features in the LSDS and Vicinity ..	50
Exhibit 24. Proposed Mitigation Measures.....	53
Exhibit 25. Water Utility Infrastructure in LSDS.....	57
Exhibit 26. Wastewater Infrastructure in the LSDS.....	58

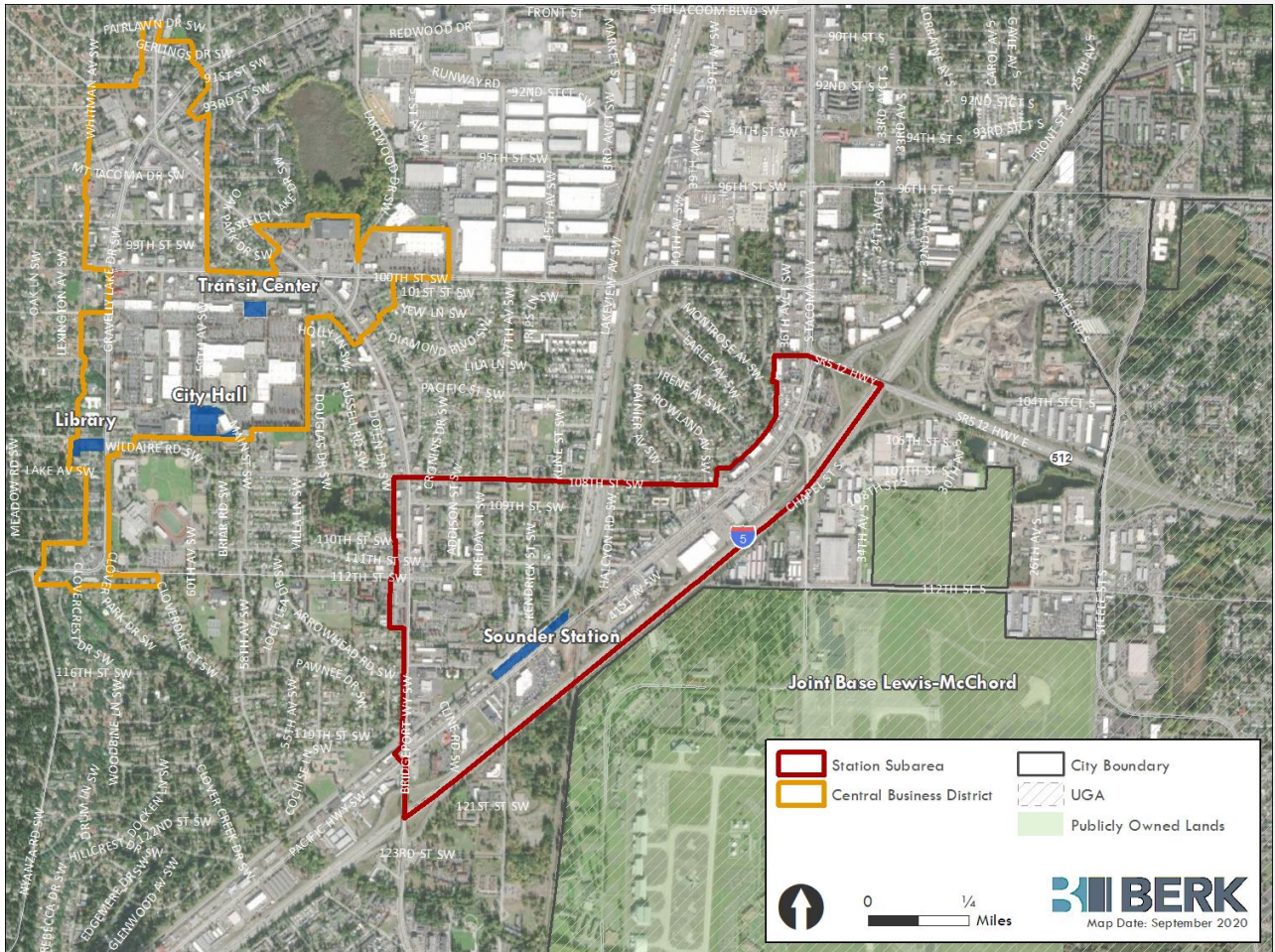


The Lakewood Station District

The Lakewood Station District Subarea (LSDS) is an area of opportunity for future growth and development within Lakewood's Urban Center. It is located adjacent to I-5 and home to the terminus of the Sounder, which provides passenger rail service to Tacoma, Seattle, and Everett and. It already has a strong employment base that provides jobs in medical services, hospitality, retail, and restaurants. Proximity to high capacity transit and employment also make this a feasible area for compact residential development that can take advantage of such amenities. These factors make this a desirable area for a portion of Lakewood's future development.

Generally, the LSDS boundary incorporates the area within a half mile of the Sounder station but does not include areas southeast of I-5, since the freeway provides a significant barrier. Exhibit 1 shows the boundaries of the planning area for the LSDS. To capture the full transportation corridor to the north and south of the station, the subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512. Just outside the LSDS is Joint Base Lewis-McChord (JBLM), which relies on retail and services within the study area boundary. Lakewood's Town Center District is also about one mile away.

Exhibit 1. Lakewood Station District Subarea and Vicinity



Source: BERK, 2020; Pierce County Assessor, 2020.

Vision and Concept

The Lakewood Station District is a multi-modal commuter hub of Lakewood and the southern terminus of Sound Transit’s commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the Lakewood commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

Exhibit 2 shows the land use concept for the LSDS. Key features of the concept for the LSDS include:

- **Transit-Oriented Development.** Residential and employment growth can be supported here because of the proximity to local and regional transit. Residential densities would be up to 40 units per acre in the residential zone (MF3) and up to 54 units per acre in the mixed-use zone (TOC). Commercial development will add employment opportunities, retail, and restaurants to serve residents and workers. As amenities and more compact urban forms are added to the LSDS, those who live and work in this area are less likely to be auto-dependent.
- **Hybrid Form-Based Code.** Land use regulations will include a hybrid form-based code. Under such a system, zoning directs the appropriate areas for residential, commercial, and mixed-use. Building and site design is based on a series of identified streetscape typologies. This allows for the continuation of a variety of uses but with a uniformed development standard that creates an identifiable look and feel for the district.
- **Affordable Housing and Homeownership.** Current residents rely on this area for low and moderate income housing, and displacement is a concern. Residential development and preservation will target housing serving households at 65%-110% of the area median income. Rowhouse residential development allows for compact residential development at an affordable price point. Ground-related units provide private and semi-private outdoor space and the opportunity for zero-lot line platted development. This provides homeownership opportunities and the chance to build wealth and equity for moderate income households in the subarea.
- **Walkable District.** New streetscapes and sidewalk improvements will make getting around the LSDS safer and more comfortable for people on foot. As housing and other amenities are added to this area, increased walkability makes it much more feasible to live and work in the subarea and reduces automobile dependency.

Exhibit 2. Lakewood Station District Subarea



Source: BERK, 2020; Pierce County Assessor, 2020.

Lakewood Landing is a 30-acre site for master planned development. The site is likely to include a mix of residential, retail, entertainment, and employment uses. Conceptually, an initial mixed-use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the “Central Core” on 11.5 acres of the 30-acre site (see Exhibit 3). Water features, carefully placed structures and sound-absorbing walls will help to mitigate noise. Future phases are under consideration. Development of the site is supported by the subarea plan but more detailed master planning will proceed on a separate track.

Exhibit 3. Lakewood Landing Location and Concept



Exhibit 4a. Lakewood Station District Location and Access



Source: Sound Transit, 2021.

Subarea Growth

Net residential growth within the LSDS is estimated to be 1,772 dwelling units, which assumes 760 units in Lakewood Landing and 962 new units from residential growth in the rest of the of the subarea over a 20-year period. Employment capacity would include 1,105 new jobs, plus 171 jobs for Lakewood Landing.¹ This complements the growth anticipated for the city center in the Lakewood Downtown Subarea Plan, which plans for about 2,257 dwelling units and 7,369 jobs over roughly the same time period.

Lakewood Comprehensive Plan

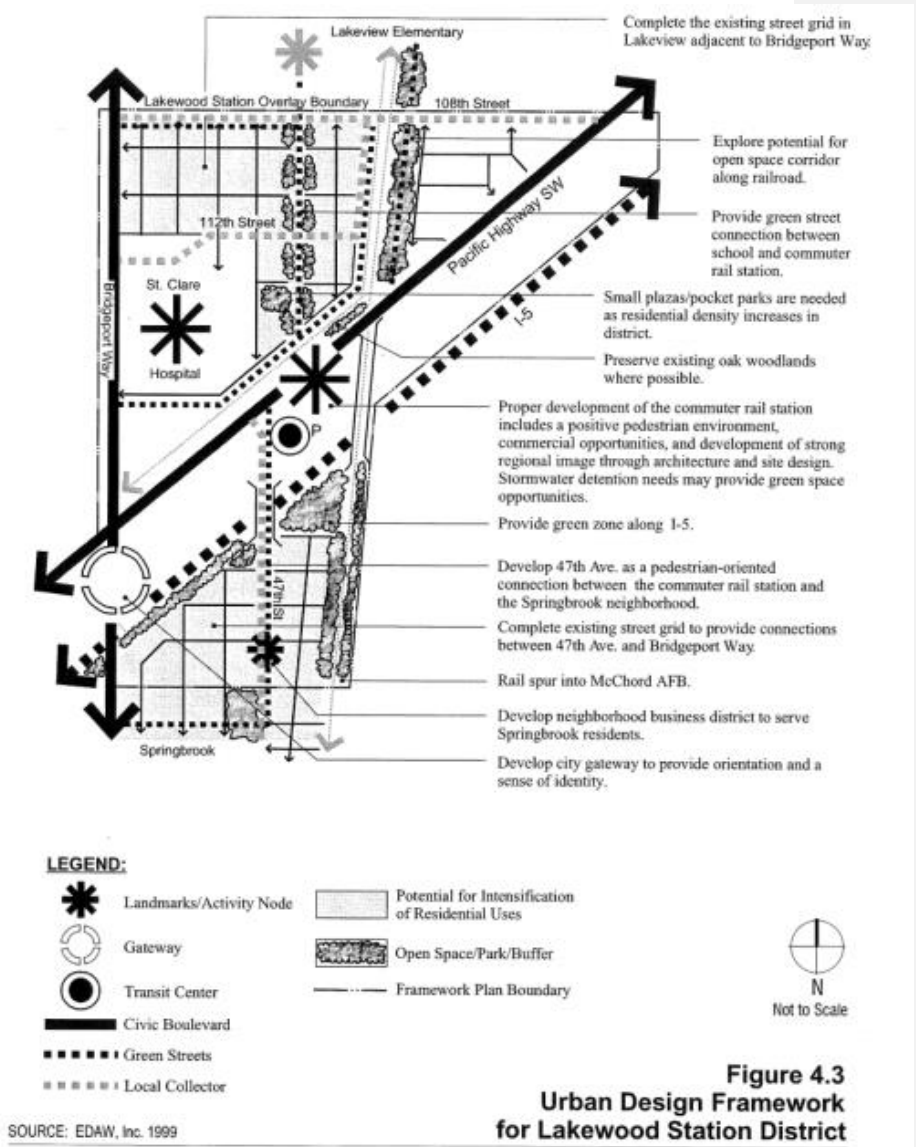
Section 3.3.5 of Lakewood's Comprehensive Plan focuses on the LSDS. Goal LU-25 identifies LSDS as Lakewood's multi-modal commuter hub with supporting policies that call for a transit-oriented development district, development of a subarea plan, coordination with other agencies, and the use of bonus densities and incentives to achieve this goal. A rich mix of land uses around the station is the goal of LU-26 including regional offices, major institutions, high density residential, neighborhood businesses, and open space. The Comprehensive Plan also supports the citywide economic goal to promote a dynamic local economy with diverse housing stock and transportation options.² Transportation linkages between the LSDS and Downtown Subarea will connect people to the amenities of both places and to the region.³ An urban design framework to guide livable and attractive development is the focus of Goal LU-27, which include design guidelines, open spaces, and pedestrian connections. Both the station itself and the I-5 interchanges are major gateways into the city that should have a distinctive look and feel that continue along Pacific Highway, Bridgeport Way, and throughout the rest of the subarea.

¹ Growth numbers include anticipated development of Phase I of Lakewood Landing. Planning for future phases will occur when such phases are fully defined.

² *Lakewood Comprehensive Plan* Economic Development objectives 1.3 and 1.5.

³ *Lakewood Comprehensive Plan* section 2.4.

Exhibit 5. LSDS Planning Concept



Source: Lakewood Comprehensive Plan, 2014.

Development of a special district around the Lakewood Station has been a consideration since the community’s first Comprehensive Plan. It remains a part of the policy framework of Lakewood’s Comprehensive Plan and is identified as a future community landmark.⁴ The 1999 high-level concept for the LSDS was included in the 2014 Comprehensive Plan as shown in Exhibit 5.

⁴ Lakewood Comprehensive Plan section 4.4

Community Input

Since the vision and concept for the Lakewood Station District had been a longstanding part of Lakewood's Comprehensive Plan, as a starting point for developing the LSDS the City tested this direction with the community to see if updates were needed. An in-person meeting conducted in late February, before public health orders were issued, with over 24 attendees invited from 17 agency and organizational stakeholders. This meeting collected information about existing conditions and planning efforts occurring in the study area. In addition, the launch of this project coincided with the early stages of the global COVID-19 pandemic. Knowing that the residents and workers in the subarea were vulnerable to the economic effects of the pandemic⁵ the City hoped to better understand local impacts to identify potential short term and long term responses.

In compliance with public health orders to the limit the spread of COVID-19, the City used remote outreach engagement options to solicit and collect community input. The City conducted an online survey during the summer of 2020. Outreach for the survey included a variety of techniques over an engagement period of about six weeks. Subarea residents received postcards and posters were hung in essential businesses and community locations to advertise the survey. Partner organizations, such as Sound Transit, shared the survey with their local mailing lists. Social media messages encouraged survey participation as well as a self-guided walkshop exercise that asked participants to walk the subarea and record their

⁵ Based on socio-economic data collected in the *Situation Assessment*

Kid's Walkshop Response

KIDS ONLY

Lakewood Station District Subarea Plan

WHAT DO YOU THINK?

First Name: Jo Age: 12

After your adventure, write or draw answers to these questions:

What did you LIKE BEST?
how there's places with room for more people

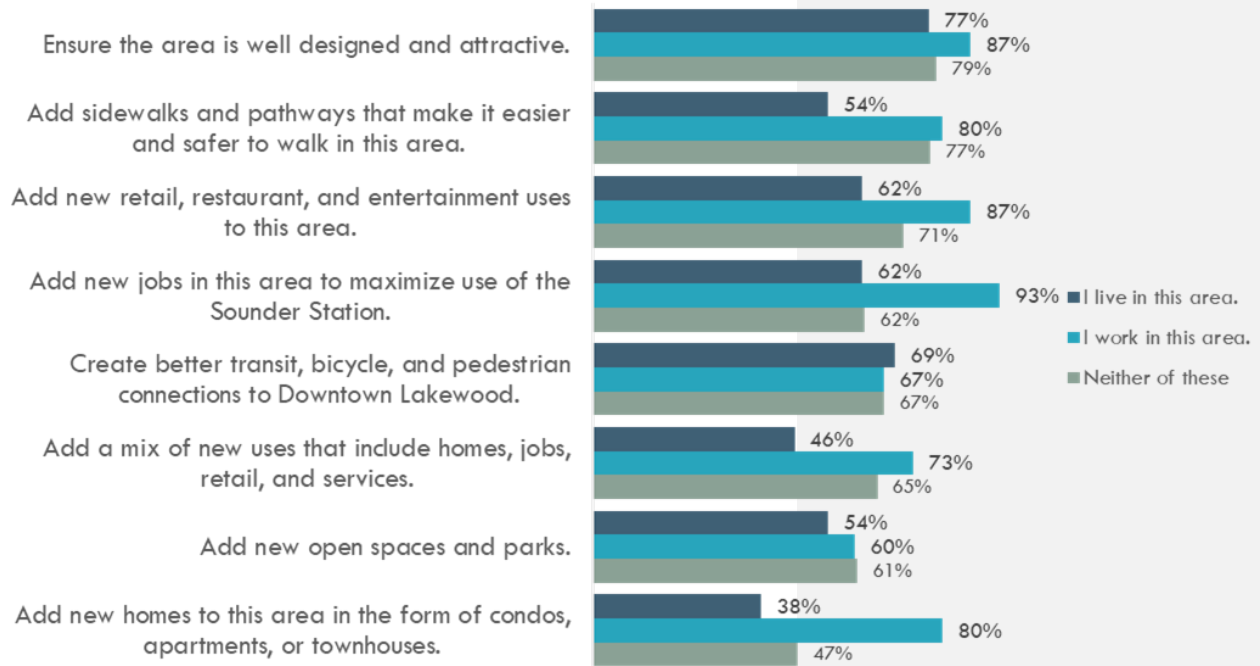
What did you LIKE LEAST?
how some places look nice but they don't take care of that landscape and public park

What do you WISH FOR?
A park for kids to play at. my public were: it's not a danger to get to. so priority in a neighborhood.

impressions. The walkshop materials included an opportunity for kids to submit their ideas as well, as shown in the Kid’s Walkshop Response sidebar.

The survey was offered in both English and Spanish, but the City received no responses in Spanish. Responses were analyzed as a whole and also disaggregated to look for differences in opinion based on whether the respondent lived in the subarea, worked in the subarea, or identified as neither. About 31% of the 91 respondents live or work in the subarea. Of those who identified as working in the area, 20% are business owners. More than half (58%) of the respondents live in Lakewood but outside of the study area.

Exhibit 6. Survey Responses - Future LSDS Priorities
Percentage of Group Rating Priority as Important or Very Important



Source: BERK, 2020.

Survey responses summarized in Exhibit 6 confirmed the vision and concept for the LSDS from the Comprehensive Plan. The following bullets highlight key results:

- Those who work in the subarea more strongly favored adding new homes to the area (80%) than those who live in the subarea (38%).
- Respondents identified the presence of the Sounder Station as the most important asset of the area, contributing to an

easy commute for residents and employees. The station's location was an asset for those working in the subarea because employees or customers live close by or it is easy to get there. An important asset to residents was the ability to have a home with some outdoor space.

- Changes that respondents would most like to see in the subarea included: more green space or parks, better walkability, more and new businesses, and additional housing options. More affordable housing was a desire of those who worked in the area and more community or neighborhood events were important to current residents.

Future Planned Engagement

The Draft LSDS plan will go through a public engagement process to share and invite feedback on the draft vision, goals, policies, and environmental review information. This will include workshops with the Lakewood Planning Commission, an online open house, and a stakeholders meeting. The City of Lakewood will hold a public hearing and a 30-day comment period to take comment on the draft Plan and Planned Action. Comments received will be used to revise the Plan, which the Planning Commission will review prior to voting on a recommendation to City Council. City Council will also hold a public hearing prior to deciding on adoption.

Note: once the engagement is completed this section will be updated prior to finalization of the plan.

Summary of Existing Conditions

Lakewood completed a *Situation Assessment* to identify existing conditions in the LSDS at the initiation of plan development. Information from the assessment is integrated into the analysis supporting the Planned Action and Environmental Checklist for this project. Exhibit 7 summarizes the results from the *Situation Assessment*.

Exhibit 7. Existing Conditions Summary Table

Topic	Summary
Demographics and Housing	<p>The subarea is a racially and ethnically concentrated area of poverty. There are low rates of homeownership and housing in the area is characterized by low values. These conditions place existing residents at risk of economic displacement.</p> <p>The majority of subarea residents work in service and retail industries and the majority of current employment is in these sectors. However, only about 10% of those who work in the subarea live in Lakewood. Additional moderate income housing in the subarea could provide the opportunity for some workers to live closer to work.</p> <p>In theory, the median household income in the LSDS should be able to pay the typical housing cost in the area. However, households in the subarea have high levels of housing cost burden, meaning they are paying more than 30% of their income for housing. Affordable housing and subsidized housing units could help alleviate the cost burden of existing residents.</p> <p>New multifamily housing is needed in Lakewood due to low vacancy rates. There is also demand for housing to serve military personnel and their families stationed at Joint Base Lewis McChord.</p>
Land Use	<p>Land use policies are in place to support the development of the subarea plan. Current land use patterns are auto-oriented, so transitional patterns of development will be necessary to achieve the desired land use.</p> <p>Residential and commercial land uses are in close proximity to each other. Although they are not currently integrated, this presents opportunities for mixed-use development. There are also a number of vacant and underutilized properties zoned for commercial and residential uses that provide opportunities for redevelopment.</p> <p>The subarea has few natural features and sensitive areas and does not include natural hazards that may limit future development. However, there are some environmental health hazards that will need to be addressed with future redevelopment.</p>
Zoning, Regulation, and Incentives	<p>Zoning is consistent with future land use but zoning and regulatory changes will be needed to support the transition of land use from its current condition to the desired future condition. Temporary activation of spaces in the subarea may be helpful during the transition period.</p> <p>Housing incentives may help the LSDS remain an area for households with low and moderate incomes while increasing the quality of housing through redevelopment.</p> <p>Attached residential development that can be subdivided for ownership can increase density and offer homeownership options, but parking and other development standards should be addressed.</p> <p>Proximity to Joint Base Lewis-McChord brings additional regulations to maintain safe airspace, but the City’s current integrated review process minimizes complexity for applicants.</p>
Transportation	<p>Intersections currently operate at an acceptable Level of Service.</p> <p>The area is well served by two transit hubs: the Lakewood Station and the SR 512 Park and Ride.</p> <p>The City of Lakewood, Sound Transit, and Pierce Transit have plans to enhance transit connectivity and multimodal mobility in the subarea in the next decade. <i>Lost revenues due to COVID may affect these plans, but details are currently not available.</i></p>
Utilities and Public Services	<p>Infrastructure upgrades will be needed for some redevelopment plans. This includes the upgrade of water mains for capacity and fire flow, and capacity upgrades for sewer. Coordination with the Lakewood Water District and Pierce County Wastewater Utility during the update of their system plans is needed.</p> <p>Sponsoring the upfront costs of infrastructure investment may be a strategy to support new development in the LSDS.</p>

2



Land Use and Urban Design

The Lakewood Comprehensive Plan established land use designations based on the vision and concept for the Lakewood Station District. Implementing zoning was adopted consistent with the land use designations. These basic land use policy and regulatory structures are carried on through the LSDS Plan. In addition, the Plan applies an LSDS regulatory overlay. This overlay includes the adoption of a hybrid form-based code to direct new development according to street and building typologies. The use of these typologies will guide urban design to give the LSDS a unique feel and identity and create a thriving environment for those who live and work in the district.

Context

Existing Land Use and Design

The LSDS vision and development concepts are not reflected in the existing land use or urban design. Development intensity is lower than what is planned for the subarea, as shown in Exhibit 8. Wide streets prioritize the movement of automobiles. This can make major arterials like Bridgeport Way or Pacific Highway act as barriers for pedestrian and bicyclists. Residential streets also sport rights of way over 50 ft wide with few traffic calming measures to buffer automobile traffic.

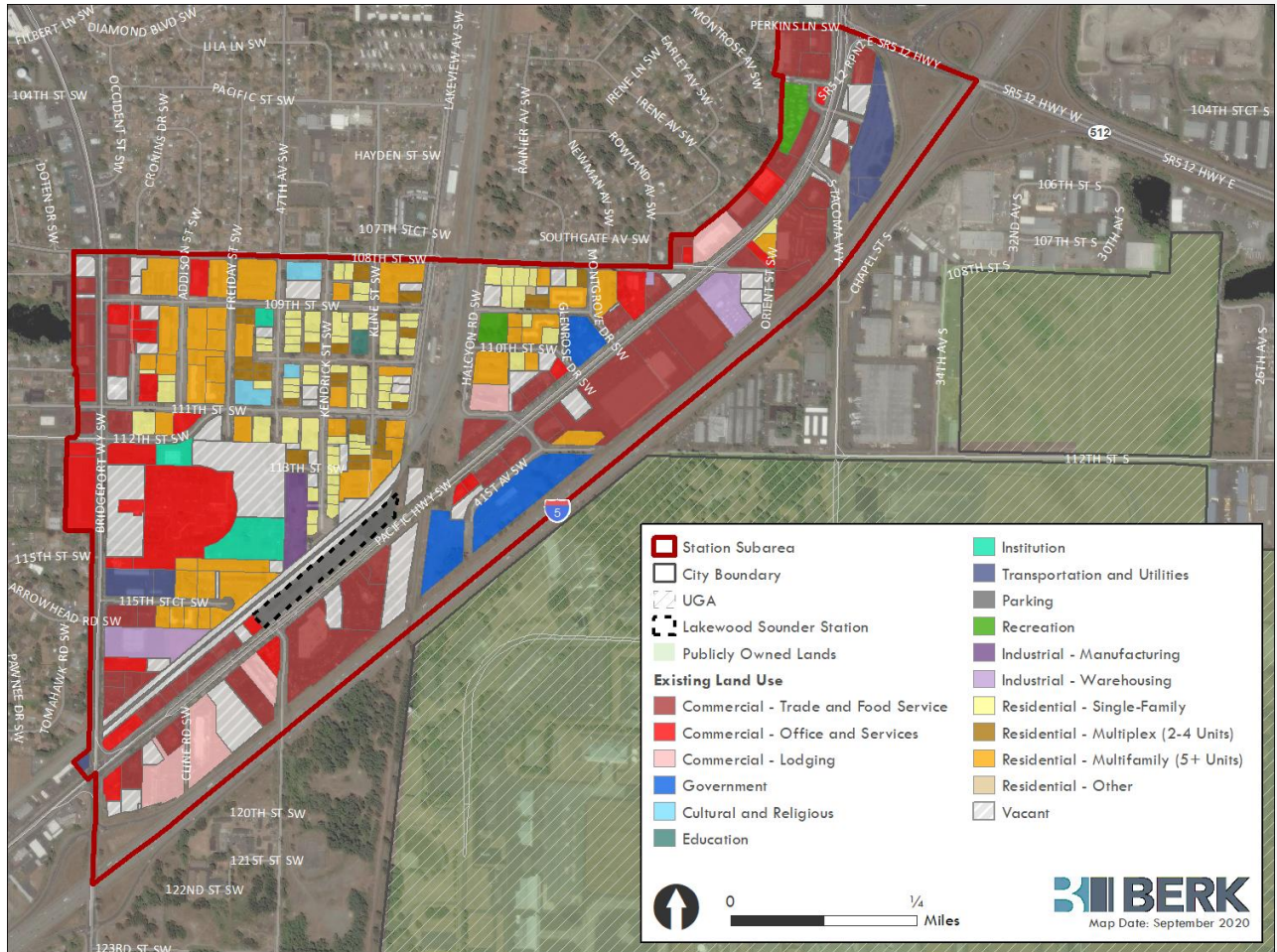


Current Urban Form in the LSDS

Sources: City of Lakewood, 2020; Google Earth, 2020.

The variety of commercial developments in the LSDS is one of its assets. Companies include a range of small unique businesses, local chains, and national companies in a range of building types that include low rise office buildings, strip commercial developments, corporate architecture, and commercial pads. Most of these building sites and adjacent streetscapes are built for auto-access with entries set far back from the sidewalk and limited pedestrian connectivity except through the parking lot. This type of streetscape and site planning is not designed at a human-scale that feels safe for walking. During public engagement walkability improvements were a top desire of residents and employees currently in the subarea.

Exhibit 8. Existing Land Use



Note: Recreation includes RV park and recreation activities.

Sources: BERK, 2020; Pierce County Assessor, 2020.

Most of the subarea is in commercial use, but there is a significant amount of residential use and some vacant lands as well. Residential uses include single-family homes (60%), small duplex or multiplex buildings (26%), and low-rise multifamily units (14%). Parcel size in the subarea is varied, as shown in Exhibit 9. Half the parcels are a half-acre or less in size, but these only account for about 40 acres in the 339-acre subarea. The 13 largest parcels account for 38% of the land in the subarea. This variety of parcels provides different types of opportunities for small and large scale redevelopment.

Exhibit 9. Parcel Size in the LSDS

Parcel Size	Parcel Count	Sum of Acreage
0.5 acre or less	169	40.25
0.5 - 1 acre	71	51.37
>1 - 5 acres	61	119.55
Greater than 5 acres	13	128.09
Total	314	339.26

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

Although residential development is allowed at up to 54 units per acre in the LSDS currently, the average density of existing development is 11.6 units per acre. Policies and strategies in this plan aim to better utilize land for housing in the area around Lakewood Station.

Environmental Conditions

The Lakewood Station District Subarea is a highly developed urban environment that contains few natural features. It is characterized by parcelized land, pavement, and existing vegetation mostly planted for ornamental purposes. A summary of environmental conditions is shown in Exhibit 10. Additional detail can be found in the *Situation Assessment* or *SEPA Environmental Checklist*.

Exhibit 10. Summary of Environmental Conditions in the LSDS

Environmental Feature	LSDS Conditions	Effects on the LSDS
Waterbodies and Shorelines	There are no significant features within the LSDS.	Not applicable.
Wetlands	There are three small, isolated areas of mapped wetlands adjacent to I-5. There is a delineated wetland outside the LSDS west of Bridgeport Way.	The isolated, mapped wetlands will need to be investigated at the time of development and follow critical areas regulations. A limited number of LSDS parcels may be subject to buffer requirements identified for the wetland outside the LSDS west of Bridgeport Way.
Floodplains	A portion of the study area is a moderate (or 500-year) flood area. This is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW.	Areas of moderate flooding are not regulated by the City’s flood hazard regulations.
Aquifer Recharge Area	All of Lakewood is within the Central Pierce County Aquifer Area. This a sole source aquifer.	New land uses that could result in groundwater contamination are limited, including things such as storage tanks, dry cleaning, and auto repair.
Geological Hazards	There are no significant features within the LSDS.	Not applicable.
Environmental Clean-up Sites	There are eight sites identified for environmental clean-up in the LSDS, as shown in Exhibit 11.	Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites. ⁶ Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State’s Model Toxics Control Act. ⁷

Sources: Pierce County Assessor, 2020; BERK Consulting, 2020.

⁶ Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

⁷ RCW 70.105D.

Exhibit 11. Environmental Clean-up Sites in the LSDS



Sources: BERK, 2020; Pierce County Assessor, 2020.

Future Land Use

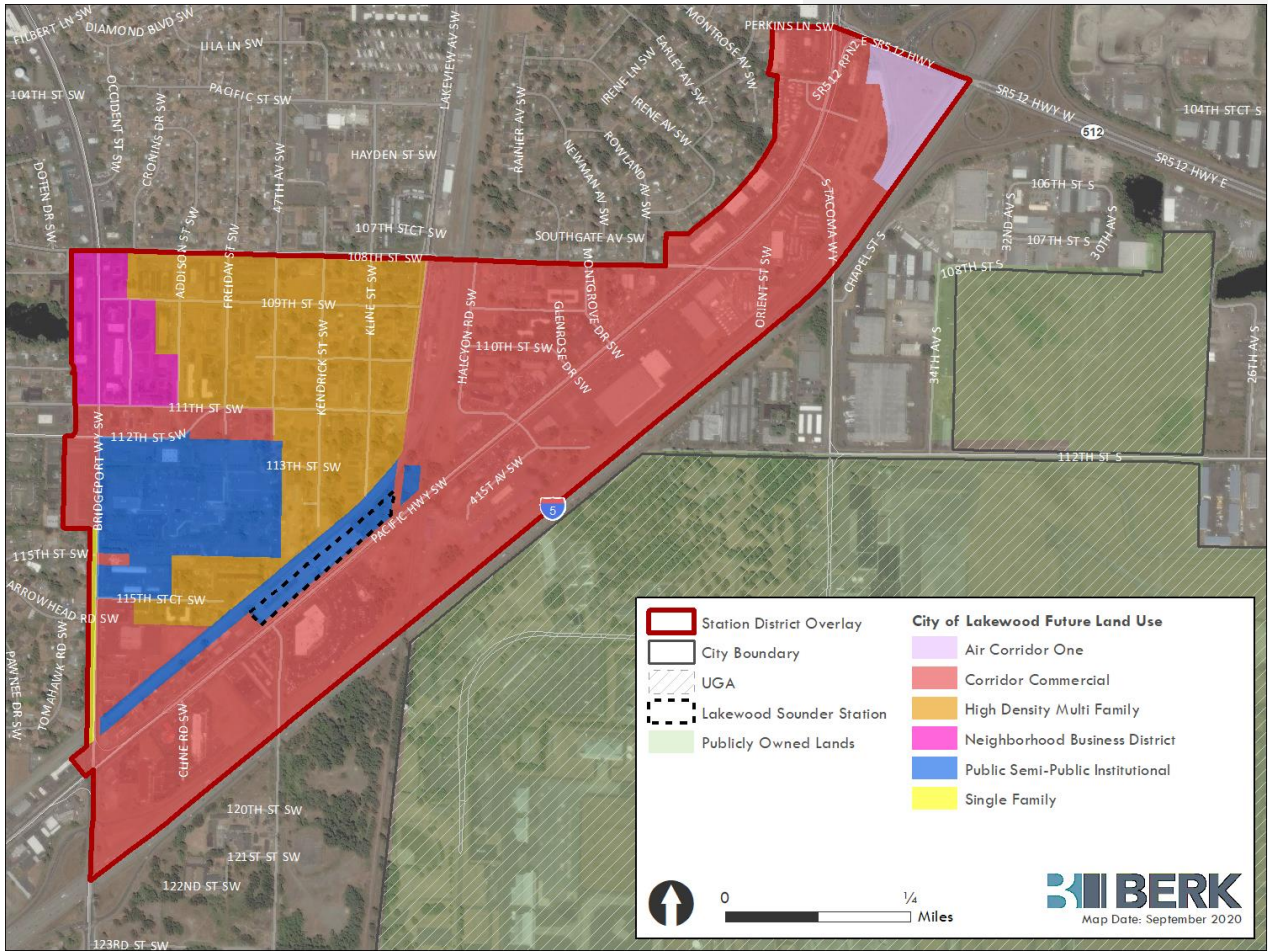
The Comprehensive Plan identifies the following land use designations in the subarea, as shown in Exhibit 12:

- Corridor Commercial.** This land use is designated along Pacific Highway SW (Pacific Highway) and just north of the Hospital. It recognizes Lakewood’s pattern of existing strip commercial development and is implemented by the Transit-Oriented Commercial, Commercial 1, and Commercial 2 zones. Within the LSDS, this district will remain and include opportunities for mixed-use development.

- **Neighborhood Business District.** This designation is implemented through the Neighborhood Commercial 1 zoning along Bridgeport Way SW (Bridgeport Way) that allows for smaller or strip commercial business areas to transform into compact urban development over time. It allows commercial development that serves surrounding neighborhoods and beyond, and allows for mixed-use residential development.
- **Public & Semi-Public Institutions.** St. Clare Hospital and the Lakewood Station are in this land use designation that recognizes essential moderate and large scale facilities that serve all of Lakewood.
- **High Density Multi Family.** The existing residential areas zoned Multi Family 3 are in the High Density Multi Family future land use designation. The purpose of this designation is to integrate a variety of high density housing types into adopted subareas and business districts. The LSDS Plan envisions rowhouse and other residential developments that are integrated with surrounding commercial and mixed-use development through pedestrian connections and urban design.

This Subarea Plan adds the Station District Overlay to the Comprehensive Plan Future Land Use Map. The purpose of the overlay is to recognize the area in which the LSDS vision will be applied.

Exhibit 12. Future Land Use in the LSDS



Sources: BERK, 2020; Pierce County Assessor, 2020.

Land Use Regulation

Land use regulation in the LSDS is driven by a hybrid form-based code. Assigned zoning identifies the types of uses allowed in each zoning and is consistent citywide. The form-based code creates a development regulation overlay that is unique to the LSDS. It regulates building forms and land use in a way that is unique to the subarea. Regulations to maintain land use compatibility with airfield operations at Joint Base Lewis-McChord remain in effect.⁸

⁸ Within the LSDS these regulations primarily implement lighting standards to prevent interference with aircraft operations. Additional detail is available in the *Situation Assessment* and the Lakewood Municipal Code 18.10.135.

Zoning

Zoning in the LSDS is shown in Exhibit 13. These zones are consistent with the Future Land Use map and include:

- **TOC – Transit-Oriented Commercial.** TOC zoning is shown along most of Pacific Highway in the subarea around Lakewood Station and the proposed Lakewood Landing site. This zone is unique to the LSDS. The purpose of TOC is *“an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.”*⁹ The mix of uses allowed in the TOC is very similar to those allowed in the Central Business District. They focus on retail and services, prohibiting space-intensive uses like auto sales, furniture and appliance stores, or industrial uses that may cause compatibility issues in a compact urban environment such as manufacturing or recycling stations. Mixed-use and multifamily residential uses are allowed at densities up to 54 units per acre.
- **C1 - Commercial One and C2 - Commercial Two.** C1 and C2 are commercial corridor districts that incorporate employment, shopping, services, offices, and light industrial uses near major arterials. A small strip of C1 is located north of the hospital, just off Bridgeport Way. Along Pacific Highway near the intersection of SR 512 is an area of C2. Both districts allow a range of businesses as permitted uses. Hotels and motels are allowed in both districts, permitted in C2, but a conditional use in C1. Commercial recreation, heavy manufacturing, shopping centers, and recycling and transfer stations characterize the type of uses that are prohibited. Residential uses are not



Lakewood Station is at the heart of the TOC zone

Source: HewittSeattle.com

⁹ Lakewood Municipal Code 18A.10.120D.5

allowed, except for allowing a caretaker’s unit.

- **NC2 - Neighborhood Commercial Two.** The commercial area on Bridgeport is zoned NC2 with the intent to create a sense of urban community that serves surrounding neighborhoods that may also attract people from other areas. This zone allows a mix of residential, retail, office, and services. Residential may be multifamily or mixed-use development up to 35 units per acre. Permitted commercial uses tend to be small or midsized. Most light industrial and larger commercial uses are prohibited. The few that may be considered, such as auto sales or breweries, are conditional uses to help mitigate impacts and ensure district and neighborhood compatibility.
- **MF3 - Multi Family Three.** The existing residential area of attached and detached single-family homes and low rise multifamily is zoned MF3. MF3 zoning is in areas where there is both an arterial and a nearby commercial or mixed-use district. This is intended to be a high density multifamily environment with multi-story housing with densities up to 54 units per acre. The LSDS overlay will provide form-based regulations that effectively limit densities to 30-40 units per acres and incentivize rowhouse development. Where multifamily development occurs within the LSDS, ground floor commercial use is allowed.¹⁰ Many of the existing uses in the LSDS are non-conforming in this zone. Non-conforming structures may be maintained but not altered or enlarged.¹¹
- **PI - Public/ Institutional.** This zone recognizes the sites of St. Clare hospital and Lakewood Station, which are major facilities serving all of Lakewood and beyond.



St. Clare provides regional medical service

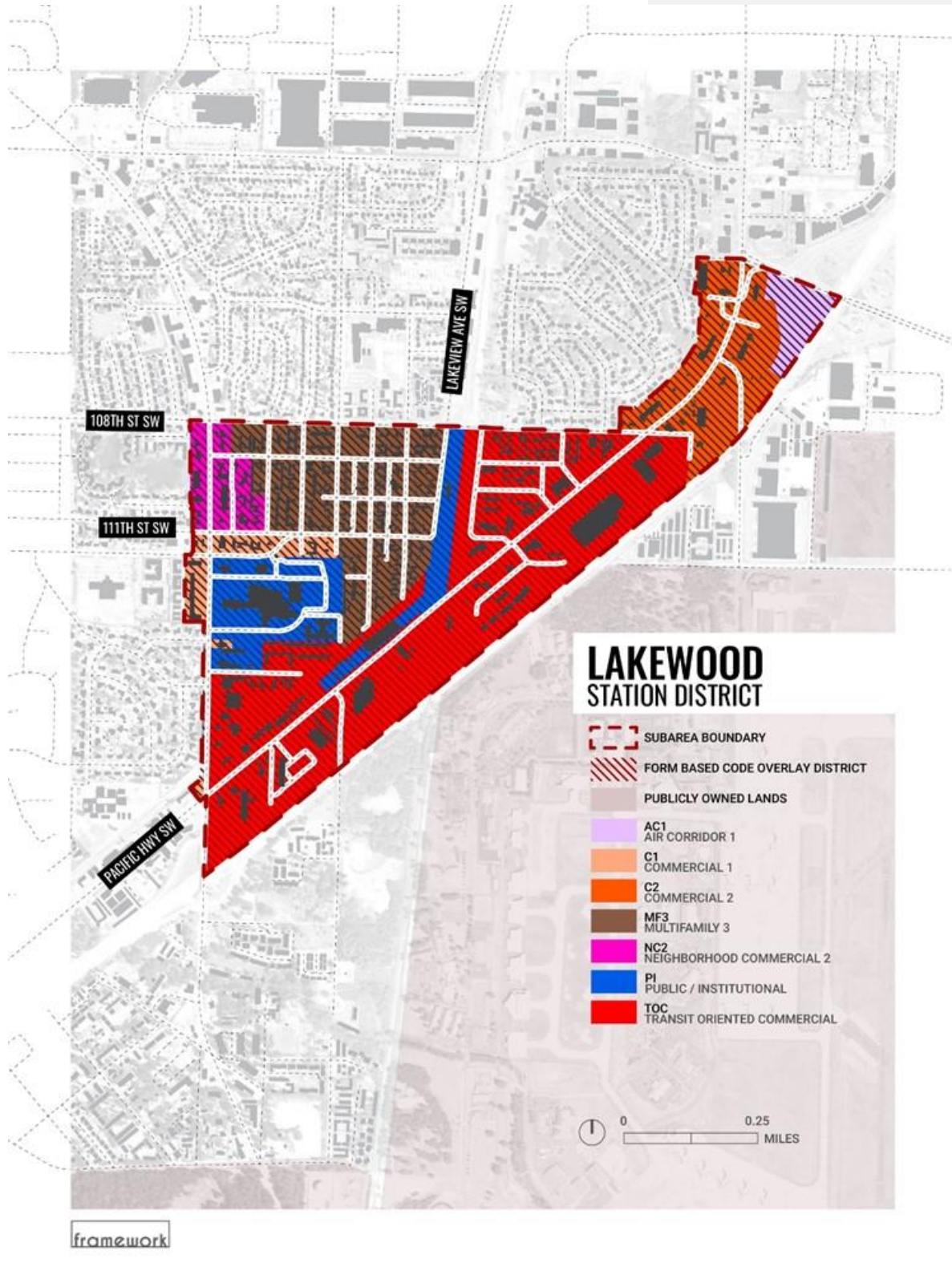
Source: CHIFranciscan.org

The zoning map adds an overlay for the Lakewood Station District (see Exhibit 13). This area links the vision to specific regulating provisions in the Lakewood Municipal Code. It allows for the implementation of a hybrid form-based code, incentive programs, and other measures within this area.

¹⁰ Lakewood Municipal Code 18A.40.040B.1

¹¹ Lakewood Municipal Code 18A.20.200

Exhibit 13. Zoning in the LSDS



Sources: BERK, 2020; Pierce County Assessor, 2021.

Hybrid Form-Based Code

Form-based codes address the relationship between buildings and the public realm, the form and mass of buildings in relation to one another, and the scale and types of streets and blocks. This code type works best when applied to targeted areas that have a defined vision, such as the LSDS. Lakewood adopted a hybrid form-based code for its Downtown. The form-based code for the LSDS draws on the work done for Downtown, but is customized for the vision, goals, and conditions of the Station District.

While uses are still regulated by zone, the hybrid form-based code shapes building design to reinforce a human, neighborhood scale that feels safe and welcoming. The use of a hybrid form-based code will standardize streetscapes to provide a consistent look and feel as well as facilitate the walkability desired by the community. Off-street parking, which is necessary to support retail as the area transitions to a more compact form, will be moved behind or under buildings that are located closer to the street. New development will include 8-14' wide sidewalks and pedestrian connections between the sidewalk and the building to enhance walkability.

Commented [LG1]: The FBC says 8-10 feet for most streets and up to public works on arterial

Transitioning from the current land use and urban form to a more compact and pedestrian-friendly environment will occur over time. The hybrid form-based code focuses retail along designated streetscapes with high traffic. This helps preserve existing retail until the area is ready for higher intensity mixed-use redevelopment with ground floor commercial.

Exhibit 14. LSDS Hybrid Form-Based Code Overlay



Sources: Lakewood Municipal Code, 2020; Framework, 2021.

The hybrid form-based code is administered through an overlay that covers the entire LSDS as shown in Exhibit 14. It overrides zoning and development regulations in the Lakewood Municipal Code if there are areas of conflict. Uses are defined by the underlying zoning designation, with some additional prohibitions intended to limit uses at a scale or impact that is incompatible with the LSDS vision. Maximum densities and height limits are also set by the underlying zoning code.

This code uses street typologies to guide the development of building types and development regulations within the subarea as shown below. The hybrid form-based code can be found in Lakewood Municipal Code Title 18C.

Street improvements in this area will work within the existing right-of-way (ROW) to ensure the safe and efficient movement of vehicles, bicycles, and pedestrians, as well as to facilitate travel by transit. As a result, street sections may vary and are subject to the approval of the Public Works Department.

Arterial Mixed-use Streets

Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots. This streetscape applies to Bridgeport Way and Pacific Highway SW.

Mixed-Use Streets

Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

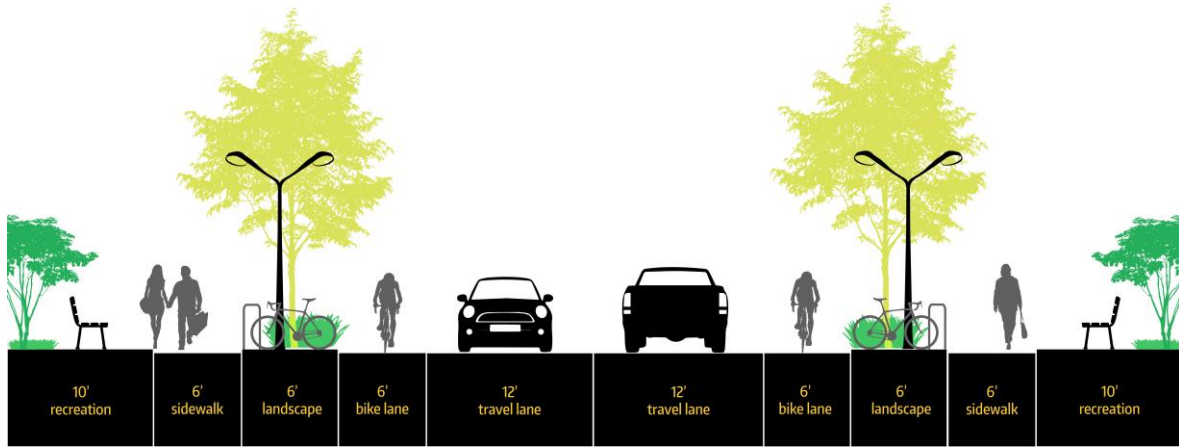
Green Streets

Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space. Green streets contribute both functional and amenity features to the streetscape to make an attractive and welcoming district as shown in Exhibit 15 and Exhibit 16. The Boulevard street in Exhibit 17 is a variety of green street that

may be carried beyond the district, highlighting connections to Downtown or other nearby areas.

Exhibit 15. Green Street Concept #1 – 80’ Right-of-Way

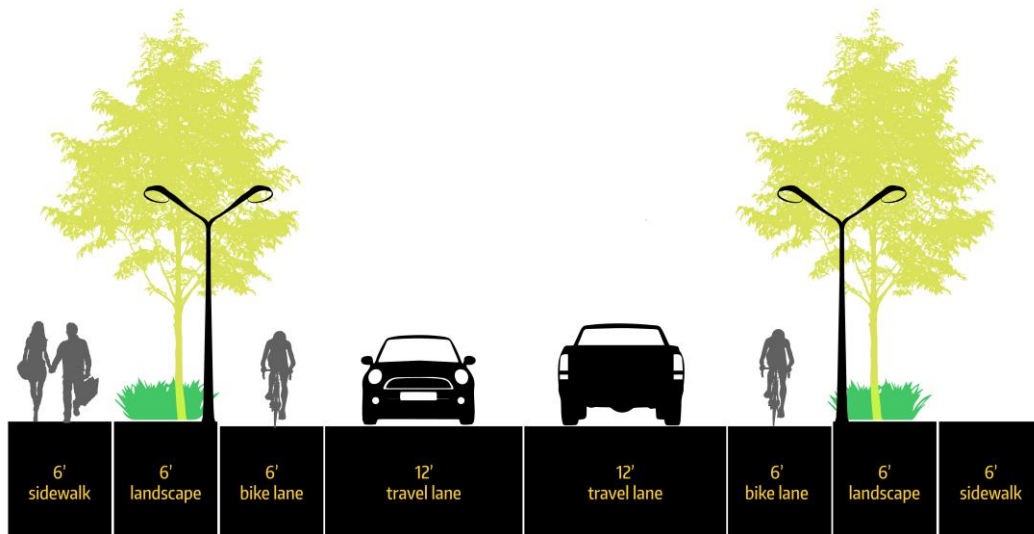
GREEN STREET CONCEPT
80’ Right-of-Way



Source: Framework, 2020.

Exhibit 16. Green Street Concept #2 – 60’ Right-of-Way

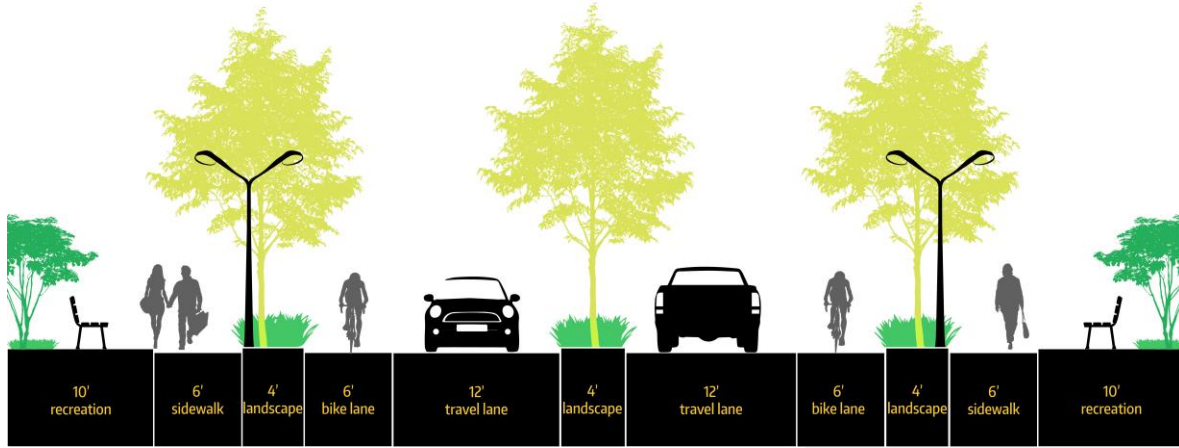
GREEN STREET CONCEPT
60’ Right-of-Way



Source: Framework, 2020.

Exhibit 17. Boulevard Street Concept – 80’ Right-of-Way

BOULEVARD CONCEPT
80’ Right-of-Way



Source: Framework, 2020.

Residential Streets

Residential Streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes. Many of the current streets identified as Residential Streets do not have curbs, gutter, or sidewalks, and there is no definition between the public right-of-way and private property. Parking occurs along the street edge, often in gravel shoulders and without a consistent pattern.

The residential street ROW in the LSDS area varies between 60’ and 80’ based on historical platting and construction. Exhibit 18 and Exhibit 19 show a standard residential street sections with either a 60’ right-of-way or 80’ right-or-way that include travel lanes, sidewalks, and bike lanes, and landscaping. In the future, bike lanes could be converted to on-street parking.

Exhibit 18. Residential Street Typology – 60' Right-of-Way

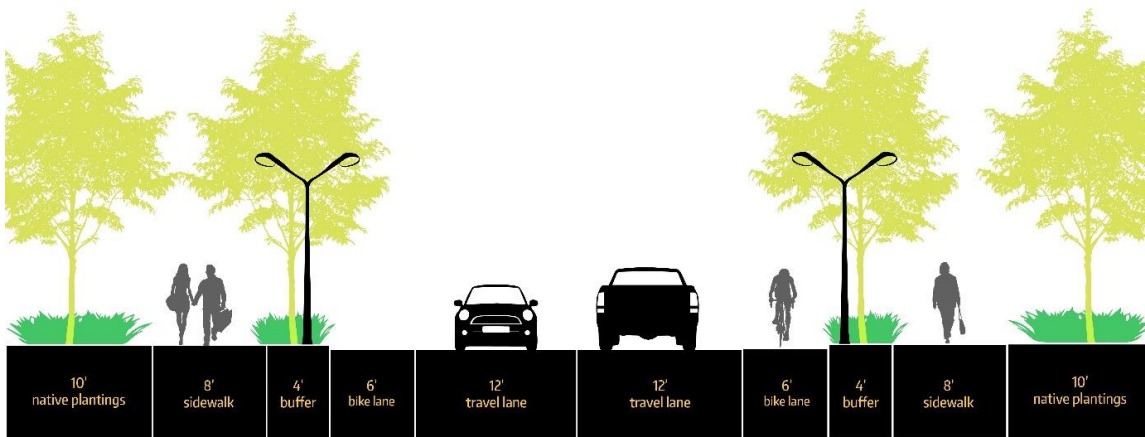
RESIDENTIAL STREET CONCEPT
60' Right-of-Way



Source: Framework, 2020.

Exhibit 19. Residential Street Typology - 80' Right-of-Way

RESIDENTIAL STREET CONCEPT
80' Right-of-Way



Source: Framework, 2020.

Alleys and Plazas

Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys would meet Public Works engineering standards. Along Occidental, a special Plaza designation identifies acceptable development frontages similar to Mixed Use Streets. The Plaza street concept is shared below.

Commented [LG2]: Added since this is on the map.

Commented [LG3]: Added with update to Occidental / Green Street Map

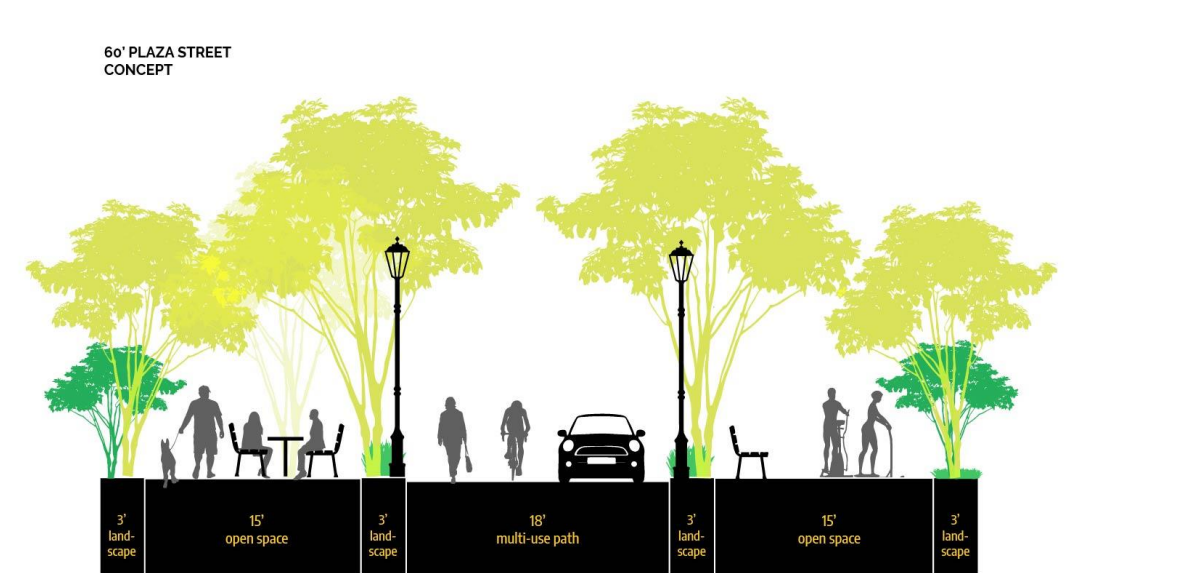
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Exhibit 20. Plaza Street Concept – 80’ Right-of-Way



Framework, 2020

Exhibit 21. Plaza Street Concept – 60’ Right-of-Way



Framework, 2020

Public Spaces

Green spaces and public open spaces should be located throughout the LSDS. This will result in an attractive district, provides gathering or recreation space, and is a desire of the community. The *Legacy Plan 2020*, Lakewood's plan for parks, recreation, and open space, aims to provide a public park or open space facility within a 10-minute walk of each resident.¹² Most of the LSDS is outside of a 10-minute walkshed from a City facility.¹³

Land consolidation would be needed to create a public park or open space in this area. While that opportunity is being explored, there are options to create linear open spaces and green corridors using available ROW. Such opportunities include:

- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street would be a plaza street as shown in Exhibit 22.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

¹² *Legacy Plan 2020*, Goal 2, Action Strategy 2.

¹³ *Legacy Plan 2020*, Figure 3-14.

Development standards will require street frontage improvements and landscaping that will increase greenery in the public realm. Mixed-use development standards will include provisions for public spaces and open spaces. Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.



Conceptual Landscaped Public Plaza for Lakewood Landing

Source: Lee & Associates, 2019.

Exhibit 22. Green Space Opportunities in the LSDS



Source: Framework, 2021.

Policies and Strategies

Policies

- Support transit-oriented development in the LSDS to capitalize on access to regional transit and proximity to Lakewood Station.
- Support and incentivize mixed-use development near Lakewood Station and in the commercial areas along Pacific Highway and Bridgeport Way where there is capacity for increased intensity of development.
- Integrate public open spaces into new development to create opportunities for public gathering and green spaces in the LSDS.
- Maintain land use and zoning designations that support high density residential development as part of mixed-use developments and residential redevelopment.
- Apply a hybrid-form based code to create an identifiable look and feel for the LSDS and to support safe pedestrian spaces.
- Adopt standards, uses and incentives that allow the subarea to transition from its present condition to its desired vision.
- Consider the designation of new Green Streets as opportunities arise, in coordination with public input.

Strategies

- Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.
- Pursue opportunities to enhance landscaping or add linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.
- Work through the *Legacy Plan 2020* to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.
- Identify standards and guidelines that support transitional

parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.

- Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.

5



Housing

Context

There are approximately 900 housing units in the LSDS. The majority (69%) of the units are renter-occupied.¹⁴ Vacancy rates are 13%. However, a 2018 study of the citywide Lakewood rental market showed lower vacancies for multifamily units of 3% or less.¹⁵ Market rate multifamily units had a 97% occupancy rate, while affordable units had a 99% occupancy rate citywide. Low multifamily vacancy had the effect of increasing the average costs of rent.

Market rate rental prices ranged from about \$900 to \$1,650 a month in fall 2018, depending on the unit size. Rates for affordable housing in Lakewood ranged from \$600 to \$1,800 a month. Although approximately 10% of renters within about a half mile of Lakewood Station receive housing subsidies, more than 50% experience cost burden, meaning they pay more than 30% of their household income on housing costs and earn less than 80% of the area median income.¹⁶

Median home values are about \$165,000, which is significantly lower than Lakewood's median value of \$292,000 or Pierce County's \$333,000. It is estimated that more than 30% of local homeowners experience cost-burden.

¹⁴ Estimated by ESRI Business Analyst (2019). This source makes estimates based on proportions of census blocks.

¹⁵ McCament & Rogers, *2018 Lakewood Rental Market Survey*, October 2018.

¹⁶ City of Lakewood *Draft Consolidated Plan*, March 2020, p.13.

About 94% of subarea residents were employed with about half working in service industries, 18% in retail, and 10% in construction.¹⁷ Median income in the LSDS is \$34,673, which is lower than the \$52,002 median for Lakewood or the \$70,635 median for Pierce County.¹⁸ A household making the LSDS median income could afford to pay about \$900 in housing costs based on the housing burden definition.

Joint Base Lewis-McChord has a 99% housing occupancy rate for more than 5,000 housing units. The 70% of military households that live off-base receive a subsidy to offset rental or purchase costs. Typically, these households spend about \$1,500 to \$2,000 a month on housing.¹⁹ Despite its proximity, less than 10% of the base's workforce lived in Lakewood. This is attributed to a lack of suitable housing in good condition that is affordable to the base workforce.

Low rates of vacancy, rising rental costs, and a steady demand for off-base housing all indicate a need for low and moderate income housing in Lakewood. The LSDS is well suited to provide such housing because there is ample space for redevelopment.²⁰ Proximity to local and regional transit resources and areas of employment in the LSDS and Downtown would also allow households to manage transportation costs.

Existing housing stock in the LSDS already includes a variety of housing types, as shown in Exhibit 23.

Rental Housing Licensing Program

Lakewood has a rental housing licensing program to ensure that existing rental stock is safe and maintained and to prevent deterioration and blight. It is codified in Chapter 5.60 of the Lakewood Municipal Code (LMC). This program requires rental units more than ten years old to pass a compliance inspection every five years as a condition of receiving a business license. If deficiencies are found, the property owner must reimburse the City for tenant costs related to relocation or transitional housing. This provision prevents displacement and reduces the financial burden tenants may experience.

¹⁷ This information came from the ESRI Community Profile in February 2020, which uses 2019 data. Unless deemed essential, these top three employment sectors for subarea residents would be sensitive to the significant slowdown in economic activity due to the COVID-19 pandemic.

¹⁸ ESRI Community Profile, 2020.

¹⁹ 42% spend \$1,501 to \$2,000 on housing.

²⁰ In 2019 the density of existing development in the LSDS is only about 11.6 units per acre, far below the maximum density of 54 units per acre allowed under the zone.

Exhibit 23. Existing Residential Structures in the LSDS

Type of Residential Structures	Sum of Units
1 1/2 Story (finished space in attic or basement is the 1/2 story)	14
1 Story	68
2 Story	4
Apartment <= 3 Stories	20
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home for the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

Sources: Pierce County Assessor, BERK 2020.

Housing Concepts

Lifecycle Housing

A variety of housing types is needed to meet the needs of people throughout their lifecycle. Housing needs also can vary based on factors such as household size, income, cultural traditions, interests, and preferences. All people need to have access to housing choices that are affordable to them. A family with children may have different housing needs and desires than an elderly couple or a military service person. Even within these categories, there are differences. Some families may desire a larger home to accommodate grandparents or other extended family members; other families may desire an efficient space close to civic amenities. An older couple may prefer a home with minimal maintenance to spend time on other pursuits; another couple may enjoy access to and outdoor space for gardening.

A variety of housing types allows people to find homes that meet their needs and desires at a level that they can afford. It contributes to quality of life and the flexibility to change their housing as needed. Lifecycle housing not only includes the single-family housing, apartment buildings present in the LSDS area, but also “missing middle” housing such as duplexes, triplexes, rowhouses, and smaller apartment buildings.

Exhibit 23 shows that the LSDS already includes a mix of housing types. During engagement, residents acknowledged the importance of having access to ground-related outdoor space, such as a private yard or semi-private shared courtyard. Many housing types can include ground-related outdoor space associated with individual units or incorporated as common space. They also can be built efficiently and affordably. By providing missing middle housing types, the LSDS will continue to provide important options for lifecycle housing, including small units, housing diversity, outdoor space, and affordability to those with moderate incomes.

Rowhouses

The LSDS encourages the development of rowhouses for residential areas. This type of development can successfully blend into the existing neighborhood as infill and redevelopment occurs because it is ground-related, has modest building heights, and is made of wood. It is also a compact development type that achieves densities of 30-40 units per acre. Units can be platted to offer homeownership opportunities that help local residents build wealth and equity as the neighborhood transforms and offers additional amenities.



Example Rowhouse Style Development

Sources: CGAArchitects.com, 2020 (left); Dwell.com, 2020 (middle); TheTownhouseGuy.ca, 2020 (right).

Rowhouses have minimal off-street parking requirements, which makes them functional and affordable for families. Rather than losing the ground floor to parking, they get additional ground floor living space. They are particularly well-suited for families who want the convenience of a yard, but the advantages of a more compact, walkable neighborhood, and access to transit. Where lots can be consolidated, rowhouses can scale into perimeter block housing with access to an interior common space. Parking for rowhouses is provided in collective lots on the development site and can be reconfigured or redeveloped as the area becomes less auto-dependent.

Apartments and Mixed-Use Residential Development

Apartments, whether stand-alone stacked-flats or part of a mixed-use development, are allowed in the LSDS commercial zones located along Pacific Highway and Bridgeport Way. Placing high intensity residential development in commercial areas gives residents easy access to goods, services, and entertainment. The highest density residential development is identified for the Transit-Oriented Commercial zone due to its proximity to Lakewood Station.

Larger lot sizes near the station also support larger scale development options that improve the financial feasibility of mixed-use or apartment development. Lakewood Landing is a good example of a master planned development utilizing a large available lot to build a residential development with a mix of retail and entertainment uses. Mixed-use developments like this provide housing, make an efficient use of land, and bring in amenities that reduce auto-dependence.

Mobile Home Park

The study area also contains the Oaknoll Trailer Park, located near the SR 512 interchange, but west of Pacific Highway/South Tacoma Way. Oaknoll rents about two dozen spaces that accommodate mobile homes and RVs. Some of these units are offered for rent and others are individually owned and lease a space only. Currently a non-conforming use in the C2 zone, state law protects the park from removal based on its non-conforming status.²¹



Oaknoll Trailer Park

Source: Google Earth, 2020.

The City could consider adding incentives to improve the trailer park while retaining it as a source of affordable housing. This may include incentives for landscaping and private streetscape or allowing for tiny homes on wheels to replace standard RV or obsolete manufactured homes (e.g. units built before 1976). State law allows tiny house villages in mobile home parks.²²

Affordability

There is sufficient land capacity to replace existing housing units as well as absorb additional units as redevelopment occurs in the LSDS. Supplementary policies and programs to support existing residents as the area transitions could reduce involuntary displacement. Other communities have used notice of intent to sell ordinances, right of first refusal incentives²³, and housing navigator programs²⁴ to support neighborhoods in transition. A citywide just cause eviction protection ordinance

²¹ State law indicates that cities and counties may not order the removal or phased elimination of an existing manufactured housing community because of it being a nonconforming use. Cities also cannot prohibit the entry or require the removal of a manufactured/mobile home, park model, or recreational vehicle authorized in a manufactured housing community because the community is a nonconforming use. (See RCW 35.63.160-161, RCW 35A.63.145-146, and RCW 36.70.493.)

²² State Law (SB 5383) also expanded the subdivision statute (RCW 58.17.040(5)) to allow the creation of tiny house villages and stops cities from prohibiting tiny houses in manufactured/mobile home parks.

²³ Notice of intent to sell or right of first refusal programs provide opportunities for existing tenants to reserve units when the property they live in is redeveloped.

²⁴ Housing navigator programs provide customized assistance to households in redevelopment areas.

would target reductions in arbitrary evictions.²⁵ A policy review of these types of measures, in consideration of existing policies and programs, is a recommended strategy for the LSDS.

The City's Housing Incentives Program encourages the development of housing for people regardless of economic means.²⁶ Inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing are described in LMC Chapter 18A.90. Incentives are available to support the development of rental housing in all zones that allow it.²⁷ Those who create units affordable to households with very low incomes receive a bonus market rate unit or one and a half bonus market rates units for each unit affordable to households with extremely low incomes. Density bonuses are capped as a percentage of the base zoning district. This includes a 20% base density increase in MF3, a 15% increase in NC2, and a 25% increase in the TOC zone. Modifications in zone development standards such as coverage, parking, and height are allowed for projects participating in the housing incentives program. There is also a reduction in permitting fees.

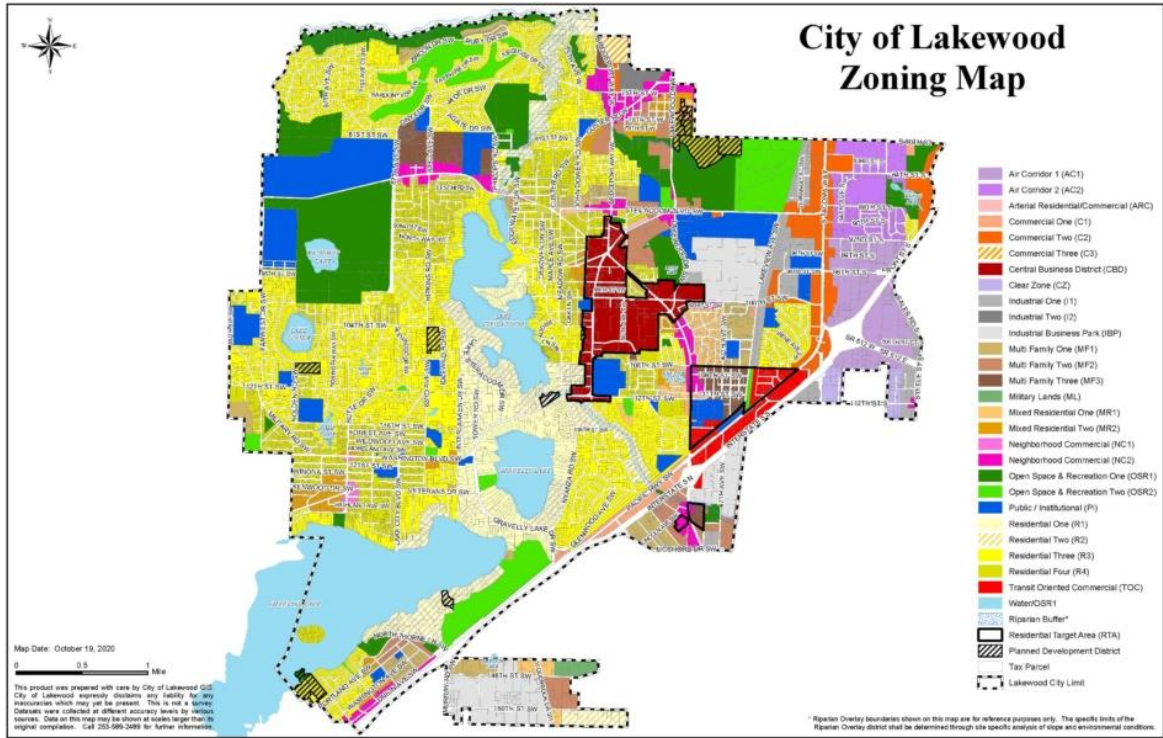
Lakewood also has a multifamily property tax exemption, which exempts some types of new housing from paying ad valorem property taxes. Currently, most of the LSDS is within one of the residential target areas where the exemption may be applied. The current boundary is outlined in black as a Residential Target Area in Exhibit 24. This offers an incentive to support the types of housing envisioned for the subarea.

²⁵ Adoption would require consideration of citywide applicability and impacts.

²⁶ Lakewood Municipal Code 18A.090

²⁷ With the exception of the construction of one single-family dwelling on one lot. 18A.090.030.

Exhibit 24. Residential Target Areas

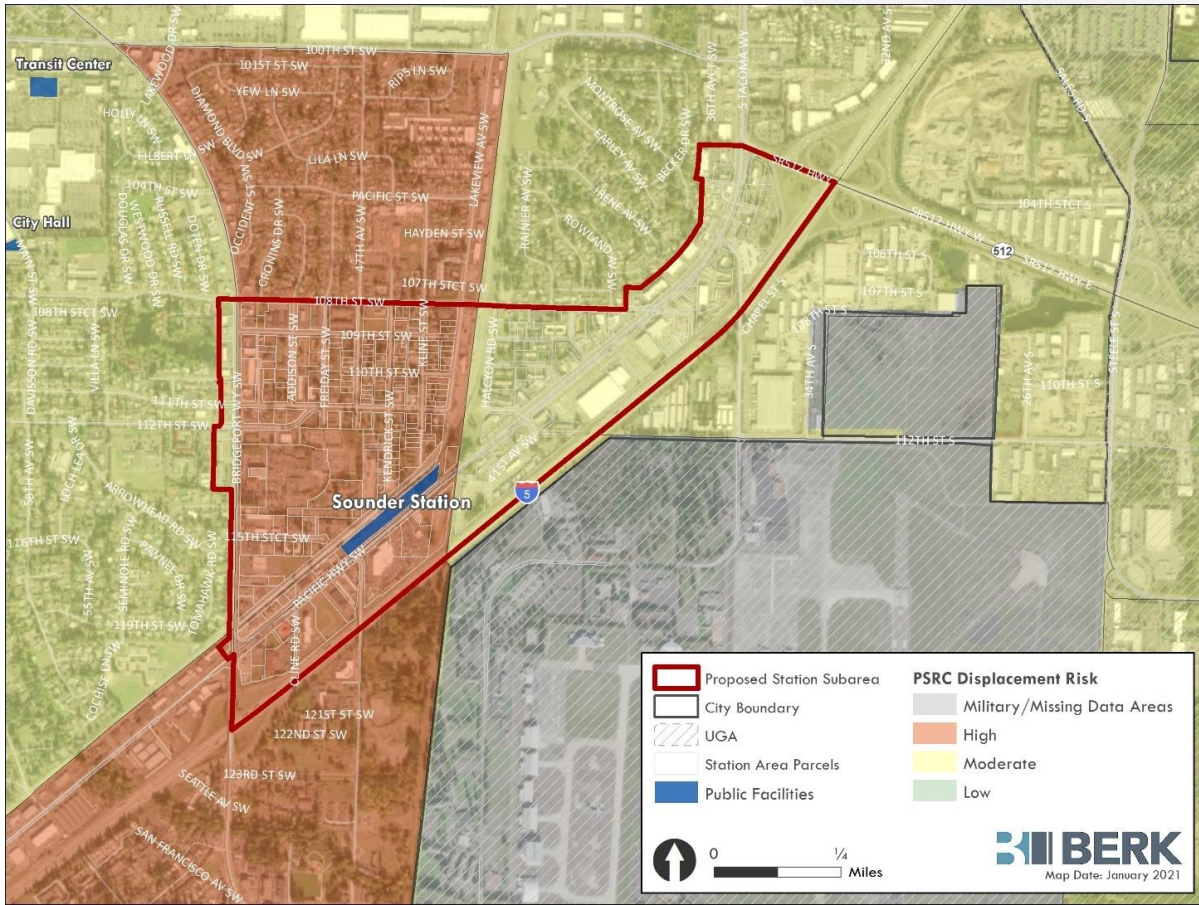


Source: City of Lakewood, 2020.

Preventing Displacement

Current residents in the LSDS may be vulnerable to displacement with redevelopment in this area due to high rates of cost burden, low household incomes, and other factors. Exhibit 25 shows high and moderate risks of displacement in the subarea based on an analysis done by the Puget Sound Regional Council in 2017. Existing low density housing is anticipated to be replaced through small or moderate-scale redevelopment projects over the next 20 years that will increase the number of available units in the area. The focus of this plan is support additional options for low and moderate income housing, as well as a variety of housing types, is one way to prevent economic displacement.

Exhibit 25. Estimated Displacement Risk



Source: Puget Sound Regional Council, 2017; BERK, 2021.

The City of Lakewood leads or participates in plans and programs to support, create, and maintain affordable housing options for its residents. In addition, the policies and strategies in the Housing section of this plan aim to incentivize new affordable housing creation, support increased rates of homeownership, and assist residents at risk of displacement.

The following **programs support the creation of new affordable housing** in the City and study area:

- Housing Incentive Program (LMC 18.90) provides inclusionary density bonuses, development standard flexibility, and fee reductions for affordable housing development.
- Multi-family Property Tax Exemption (MFTE) gives a tax break for affordable housing development in most of the Lakewood Station District.

- Community Development Block Group (CDBG)/HOME funds from the federal government are used to support affordable housing development.
- Affordable Housing Initiative (2060) and Homeless Housing Act (2163) Programs that are joint with Pierce County and distribute funds to support affordable housing and homelessness.
- SHB 1406 Program applies a portion of the sales and use tax to use for supportive and affordable housing.

The following **programs support residents** in obtaining and maintaining high quality housing they can afford:

- Down payment assistance program by City of Lakewood and Pierce County.
- Pierce County property tax exemption for seniors and disabled.
- On-going partnership with Habitat for Humanity.
- Rental Housing Safety Program: ensures all housing units meet a consistent, minimum level of safety for tenants.

Policies and Strategies

Policies

- Encourage a variety of housing types to ensure choice for current and future residents, workers, military families, and to strengthen commercial areas.
- Support housing affordable to current LSDS residents though preservation where possible, as well as redevelopment activities.
- Incentivize the development of market rate and affordable housing options for households with incomes at 60-120% of the area median income to preserve affordability in the LSDS and avoid displacement of existing residents.
- Encourage homeownership options that allow local residents to invest in the community to gain equity and wealth.
- Attract a mix of large and small builders that can provide new housing options at a variety of scales and levels of

affordability.

Strategies

- Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.
- Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.
- Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.
- Support the development of housing that meets the needs of military households.
- Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.
- Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.
- Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption (MFTE).
- Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.
- Develop community benefit agreements with large investors in the LSDS.
- Provide information and support to help investors navigate affordable housing financing programs.
- Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.

5



Economic Development, Business, & Employment

The LSDS is an area ripe for new investment due to its location near I-5 and the availability of local and regional transit options. Proximity to Joint Base Lewis-McChord (JBLM) provides a unique and regional market opportunity. A 2018 JBLM Regional Economic Impact Analysis shows that the base has a \$9.2 billion impact on the regional economy.²⁸ The LSDS also has the opportunity to become a health care hub with proximity to Madigan Hospital on JBLM as well as to St. Clare Hospital and Western State Hospital. Business development in the subarea could attract a greater portion of regional investment to Lakewood.

Context

The LSDS supports over 170 businesses and nearly 2,300 employees.²⁹ These numbers grow to over 640 businesses and 6,550 employees within the half mile extended study area.

²⁸ South Sound Military Partnership and University of Washington Tacoma, 2018.

²⁹ Data in this paragraph comes from ESRI Business Summary, 2019.

Retail and services are the primary businesses sectors. Services, including lodging, health care, automotive, legal, and other service industries comprise nearly half of the businesses in the subarea (46%) and employ 65% of the workers. Retail businesses, including restaurants and the sales of merchandise, gas stations, food, and other goods, comprise 29% of the businesses in the subarea and employ 22% of the workers.

Local businesses and small businesses serve the LSDS by providing opportunities for shopping, employment, and entrepreneurship. Stakeholders and public participants acknowledged the importance of small and local businesses in the LSDS visioning survey. Some provide service in Korean or Spanish to support the local community and create spaces of belonging. As the area transitions and grows, measures that prevent residential displacement and support small business will help avoid business displacement by maintaining local customer base and providing resources for continued business development.

Jobs in this area provide moderate incomes: about 44% provide an annual income above \$40,000 a year, approximately half of the median household income for Pierce County. Almost one-fifth (18%) of the workers in the subarea earned under \$15,000 a year.

The LSDS is part of the federally designated Lakeview/Kendrick Street Opportunity Zone. The opportunity zone includes two census tracts. These tracts overlap with portions of the LSDS including the Bridgeport Way interchange with I-5, St. Clare Hospital, the Sounder Station, the proposed Lakewood Landing site, and a portion of the residential area. Opportunity zones were created by the 2017 Tax Cuts and Jobs Act with the intention of supporting economic development and employment in distressed communities.³⁰ The program allows investors to defer capital gains tax for up to nine years by investing their gains in a Qualified Opportunity Zone. The federal program is funded through 2026 and supports redevelopment in the LSDS.



Local businesses serve residents and provide entrepreneurship opportunities

Source: El Mercado Latino

³⁰ Department of Commerce, 2020.

<https://www.commerce.wa.gov/growing-the-economy/opportunity-zones/>

Policies and Strategies

Policies

- Support medical office and commercial office opportunities.
- Encourage mixed-use development to expand opportunities for business and employment in the LSDS.
- Support small business development to build on the diverse offering of goods and services to residents and workers in the LSDS.

Strategies

- Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.
- Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See related Land Use policy.
- Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.
- Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.
- Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces.
- Invest in infrastructure and civic amenities consistent with this plan to attract business owners and investors.



Transportation

Context

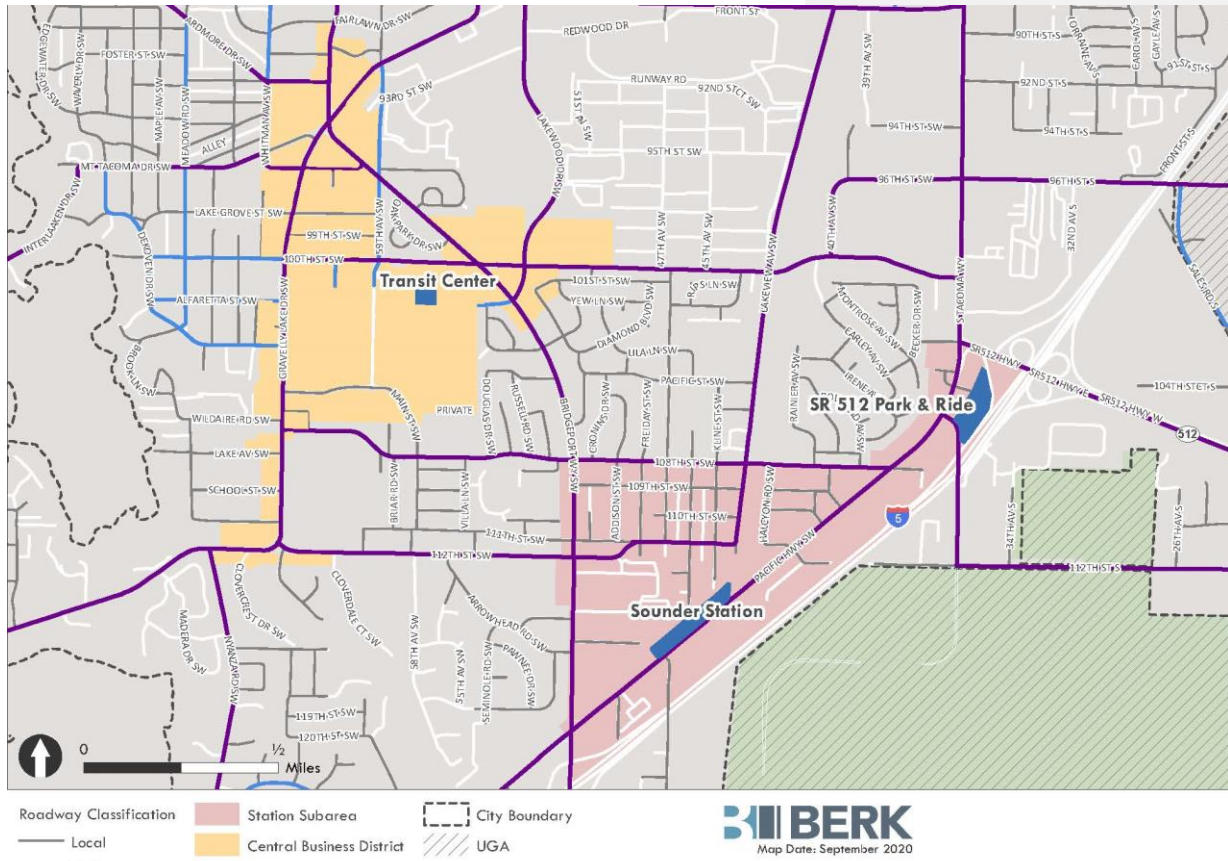
The LSDS is a predominantly auto-oriented environment framed by access to I-5 to the north at the SR 512 interchange and at the Bridgeport Way interchange to the south. It is also served by major arterials such as Pacific Highway SW, Bridgeport Way, and 108th Avenue SW and a network of local access streets (see Exhibit 26).

There are two key regional transit facilities: Lakewood Station and SR 512 Park & Ride. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users. The Downtown Transit Center is only about a mile away from the study area along Bridgeport Way. Enhanced streetscapes could provide better bike and pedestrian accessibility to transit resources within and nearby the LSDS.

The subarea is bisected by an existing rail line on which the Sounder and freight trains operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Marked bicycle lanes are located in the LSDS on Pacific Highway and 108th Street SW. While sidewalks are generally located on all major streets in the project area, the residential area lacks sidewalks on most roadways. In addition to adding sidewalks and bike lanes, there is an opportunity to enhance existing street facilities. Adding landscaping that buffers non-motorized travelers from automobile traffic flows could improve safety and comfort.

Exhibit 26. Transportation Features in the LSDS and Vicinity



Sources: Fehr & Peers, 2020; Pierce County, 2020.

Level of Service

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically calls for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. The transportation element designates LOS guidelines for the city's

arterial streets and intersections. Within the study area, the City sets a LOS standard of LOS D³¹ during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Redevelopment in the LSDS is expected to increase roadway volumes that will impact the movement of automobiles, freight, and transit. The areas where LOS may be affected, without mitigation interventions (mitigations are discussed on the following pages), include the following intersections:

- Pacific Highway / Sharondale Street SW – Increase in delay, LOS F to LOS F
- Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Improvements

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th



111th and 112th Street Corridors

Source: City of Lakewood.

³¹ LOS D is defined in the Lakewood Comprehensive Plan as a level of traffic that is approaching unstable flow. Travel speed and freedom to maneuver are somewhat restricted, with average delays of 25 to 40 seconds per vehicle at signalized intersections. Small increases in traffic flow can cause operational difficulties at this level.

Street SW – sidewalks, street lighting, bicycle facilities.

- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. At this time there is no specific plan for a pedestrian crossing of Pacific Highway to connect Lakewood Station to the future Lakewood Landing site, but options should be considered. Pierce Transit's *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup's South Hill.

General Mitigation Measures

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts.

Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.

Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not



Transit near employment helps reduce auto demand

Source: City of Lakewood.

affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed as part of the Subarea Plan.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 27 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 27. Proposed Mitigation Measures

INTERSECTION	IMPROVEMENT
Pacific Highway / Sharondale Street	Install signal using existing channelization and signal length consistent with adjacent intersections
Pacific Highway / Bridgeport Way	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.
Bridgeport Way / SB I-5 Ramp	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Street Standards

As noted in Land Use & Urban Design, new street standards are an important tool in the transformation of the LSDS according to its vision. New development brings the opportunity to modify frontages to improve safety and comfort for pedestrians and bicyclists. It also provides opportunities to increase green space and open space. By adopting new street standards, the LSDS will improve travel conditions across all modes.

The City of Lakewood hopes to offset the costs of complying with street standards. It will identify programs and funding sources aimed at sharing or minimizing the cost of street improvements in the LSDS.

Policies and Strategies

Policies

- Provide multi-modal transportation options to improve the flow of people and goods in the LSDS and to reduce auto-dependence.
- Support pedestrians in the LSDS with sidewalk and infrastructure improvements that enhance safety and provide a more comfortable travel experience.
- Enhance non-motorized connections to Lakewood Station and Pierce Transit facilities to encourage the use of regional and local transit.
- Connect the LSDS to Downtown with enhanced pedestrian and bicycle facilities.
- Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.
- Consider options for crossing Pacific Highway to provide a safe and efficient linkage between Lakewood Station and the Lakewood Landing development site.

Strategies

- Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.
- Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.
- Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.
- Require street frontage improvements that enhance walkability and support economically viable and quality residential development.
- Identify funds, funding mechanisms, grants, and programs that allow the City of Lakewood to develop and build streetscapes in the LSDS.



Utilities & Public Services

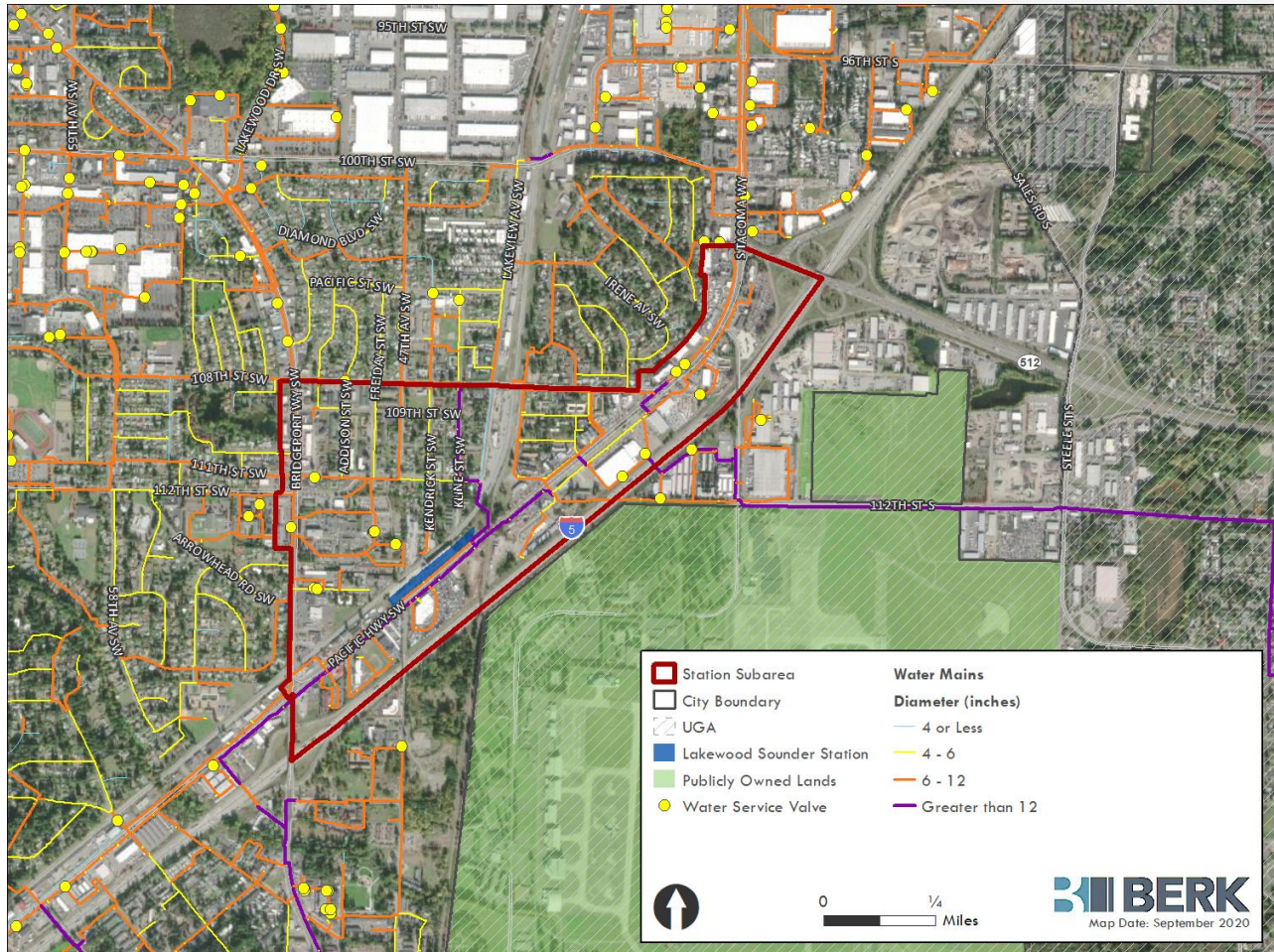
A variety of providers serve the LSDS with utility infrastructure and public services. Infrastructure updates and capacity changes related to the subarea plan will need to be coordinated with these providers. This should be done through participation in the updates of long-range system plans as well as shorter term capital facility improvement plans and project specific planning and permitting.

Context

Water

Water is provided by the Lakewood Water District and the subarea is fully served. As shown in Exhibit 28, the largest water mains primarily run along Pacific Highway, but a main larger than 12 inches runs roughly under Kline Street SW into the residential area. There is also a main larger than 12 inches that runs into the subarea from the southeast side of I-5. Mains 6 to 12 inches in size are well distributed throughout the subarea, running under several streets, including Bridgeport Way. These mid-sized mains serve St. Clare Hospital and the existing commercial development in the subarea.

Exhibit 28. Water Utility Infrastructure in LSDS and Surrounding Area



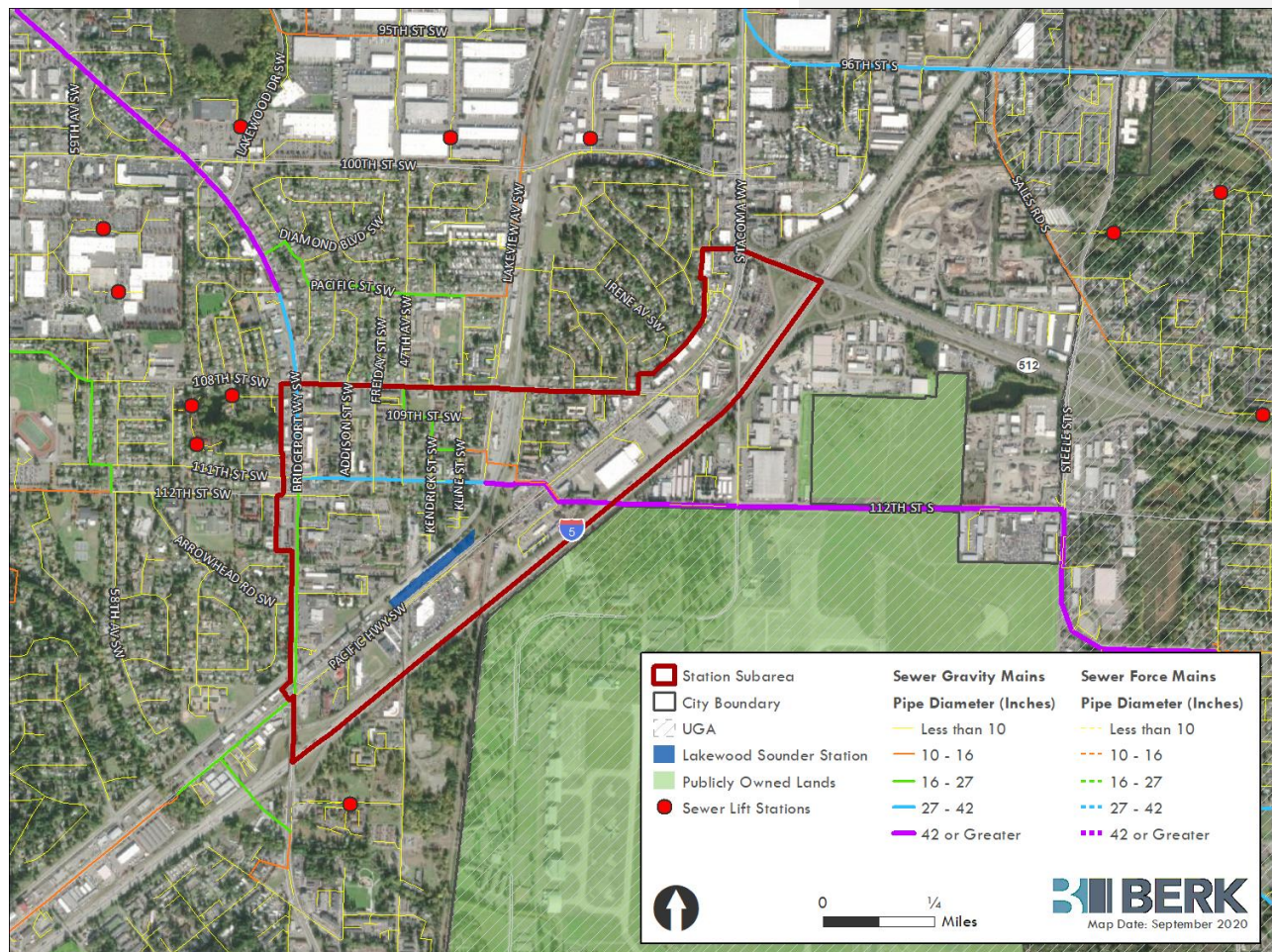
Sources: BERK, 2020; Pierce County Assessor, 2020.

The Lakewood Water District initiated a 50-year repair and replacement plan in 2014 to replace 181 miles of aging water mains. Some replacements have already occurred in the subarea, including replacements along Bridgeport Way. The repair and replacement plan focuses on the replacement of facilities that are nearing the end of their useful life and does not account for upgrades or extensions to support new development. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea, this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of upgrades with the 50-year repair and replacement program is recommended and could provide cost and timing efficiency for redevelopment projects in the subarea.

Wastewater

Wastewater service in Lakewood is provided by Pierce County Planning and Public Works. Most of the sewer pipes shown in Exhibit 29 were installed in the 1980s (some later) and are primarily 8-inch diameter PVC pipes, which have a 100-year lifetime. Larger interceptors are typically made of concrete, which has a service life of 50 to 100 years. Generally, the sewer infrastructure is considered in good condition with plenty of remaining service life and no current need for large scale line replacements or upgrades.

Exhibit 29. Wastewater Infrastructure in the LSDS and Surrounding Area



Sources: BERK, 2020; Pierce County Public Works, 2020; Pierce County Assessor, 2020.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County was scheduled to begin an update in late 2020. The Plan accounted for zoning densities up to 54 units per acre, which should be adequate for most development within the LSDS. Master planned development at

Lakewood Landing may occur at a higher intensity and require wastewater system capacity upgrades. The update of the Unified Sewer Plan provides an opportunity to plan for future development at Lakewood Landing.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan (SIP) was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. Reviewing the SIP in light of the LSDS and Lakewood Landing development is recommended.

Energy

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Maps showing the locations of the lines providing natural gas service are not available and most facilities are located underground. Puget Sound Energy is working on an updated Integrated Resource Plan to ensure continued delivery of service to its existing service areas in the region and to address opportunities for enhanced conservation and reduced environmental impacts.

Lakeview Light and Power is working on increased system capacity and flexibility. There is a five-year capital budget for repowering the substation at Pacific Highway and Bridgeport Way. Improvements to the substation will allow Lakeview Light and Power to rebalance loads as needed to accommodate fluctuations in usage. Increased demand for electrical service should be possible to accommodate, but specific plans may be needed to address any heavy commercial users and electric vehicle charging capacity. Line infrastructure is located mostly above ground within the subarea. As redevelopment occurs, taller buildings will likely require utility undergrounding for safety, but Lakewood does not have a policy or regulation requiring undergrounding with new development.

Emergency Response

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department.

The Lakewood Police Department is one of the largest departments in the state. It operates six patrol districts as well as specialized units in criminal investigation, K9, traffic, and marine response. The subarea is part of the Pacific Patrol District, which is authorized for up to 28 sworn personnel.³² All patrol districts, including the Pacific District, are operated out of the police station in the civic campus in Downtown, less than a mile from the subarea.

Emergency management functions for Lakewood are part of the Police Department. The mission of emergency management is to assist with mitigation, preparation, response, and recovery from natural disasters and other community emergencies. This includes a variety of trainings and informational materials to help residents and businesses prepare for major emergency events.

West Pierce Fire and Rescue provides fire and emergency medical response to Lakewood, University Place, and Steilacoom. Station 20 is located within the subarea along Pacific Highway. Services offered by West Pierce Fire and Rescue include emergency medical response, fire suppression, fire prevention, and community education. They also sponsor Community Emergency Response Team (CERT) trainings that prepare residents to be the first line of response in their neighborhoods following a disaster prior to the arrival of emergency responders.



Emergency response is readily available

Source: West Pierce Fire and Rescue.

³² Lakewood Police Department 2018 Annual Report, updated in 2019.

Policies and Strategies

Policies

- Ensure there is adequate utilities infrastructure to support redevelopment of the LSDS according to the vision.
- Consider future agreements to support infrastructure development in the LSDS such as development agreements, latecomers agreements, fee assessments, improvement districts, and other tools.

Strategies

- Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.
- Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.

8



Implementation Plan

The following table identifies responsibilities for implementing the vision for the LSDS. It outlines the direction, responsible entity, and timeline for plan actions. The timelines include ongoing, short term (up to three years), midterm (three to five years), or long term (more than five years).

	Direction	Timeline	Department
Land Use & Design	Support the master planning of the Lakewood Landing site for mixed-use development so it will provide opportunities for high density housing, employment, business, and public space.	Ongoing	Community Development and Economic Development
	Pursue opportunities for enhancing landscaping or adding linear parks within the public right-of-way to integrate green spaces and open spaces into the LSDS.	Long term	Community Development and Parks and Recreation
	Work through the <i>Legacy Plan 2020</i> to identify opportunities for public parks facilities within a 10-minute walk of subarea residents.	Midterm	Parks and Recreation
	Identify standards and guidelines that support transitional parking arrangements so space used for parking can be transitioned to other uses as the area becomes less auto-dependent.	Short term	Community Development
	Monitor the impact of the Lakewood Station District Overlay and amend the Plan as needed to improve outcomes.	Ongoing	Community Development

	Direction	Timeline	Department
Housing	Allow the highest density housing in mixed-use areas and in the Transit-Oriented Commercial zone where the availability of amenities within walking distance will encourage non-motorized transportation alternatives.	Ongoing	Community Development
	Encourage and support missing middle housing types such as duplexes, triplexes, and small-scale multifamily as infill development in the residential area to improve housing choice and preserve affordability.	Ongoing	Community Development
	Support the development of ground-related rowhouses as an affordable home ownership option for LSDS residents.	Ongoing	Community Development
	Support the development of housing that meets the needs of military households.	Ongoing	Community Development
	Partner with affordable housing organizations to support the expansion of subsidized housing in the LSDS by building new projects or by purchasing and rehabilitating existing housing.	Ongoing	Community Development
	Work with the Washington Homeownership Center to find opportunities for investment in the LSDS.	Ongoing	Community Development
	Consider expansions of existing affordable housing incentive programs and new affordability incentives in the LSDS, including the extension of the applicability area for the multi-family property tax exemption.	Short term	Community Development
	Consider programs to support residents in the LSDS as the area redevelops and reduce involuntary displacement such as a housing navigator program, a notice of intent to sell ordinance, just cause eviction protections, and right of first refusal incentives.	Short term	Community Development
	Develop community benefit agreements with large investors in the LSDS.	Ongoing	Community Development
	Provide information and support to help investors navigate affordable housing financing programs.	Short term	Community Development and Economic Development

	Direction	Timeline	Department
Housing, continued	Identify partners to support funding and a program that will rehabilitate existing homes such as a home repair program.	Short term	Community Development
Economic Development, Business, & Employment	Promote St. Clare Hospital and satellite medical office and supportive commercial uses. Connect the residential neighborhood to the hospital with improved pedestrian and linear park opportunities.	Short term	Community Development and Economic Development
	Support the master planning of the Lakewood Landing site for mixed-use development to expand opportunities for employment and retail development. See related Land Use policy.	Ongoing	Community Development and Economic Development
	Partner with developers to identify projects eligible for participation in the Lakeview/Kendrick Street Opportunity Zone.	Midterm	Community Development and Economic Development
	Provide resources for entrepreneurs and small businesses, such as grant/loan programs, to support and recruit small, local businesses in the LSDS.	Ongoing	Economic Development
	Encourage micro-retail, or businesses that cater to specific demographic segments of the community, with business development resources and support available in other languages and flexibility in the design of cultural spaces	Ongoing	Economic Development
	Invest in infrastructure and civic amenities consistent with this Plan to attract business owners and investors.	Long term	Public Works
Transportation	Apply a hybrid form-based code to support improved streetscapes that accommodate pedestrian travel.	Ongoing	Community Development
	Add signalization improvements at Pacific Highway and Sharondale Street to prevent traffic delays, and at Bridgeport Way and the I-5 northbound/southbound off ramps to prevent queuing that extends onto the freeway.	Long term	Public Works
	Work with Sound Transit and Pierce Transit on the timing and location of non-motorized improvements to avoid gaps and overlaps in the system.	Ongoing	Public Works

	Direction	Timeline	Department
	Require street frontage improvements that enhance walkability and support economically viable and quality residential development.	Ongoing	Community Development and Public Works
	Publicly invest in the development and construction of streetscapes to support redevelopment in the LSDS.	Ongoing	Community Development and Public Works
Utilities	Coordinate with public service and utility providers during the update and development of long-range plans and system improvement plans to ensure there is adequate support for anticipated growth and development of the LSDS subarea.	Midterm	Public Works
	Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea.	Midterm	Public Works

ATTACHMENT B

March 29, 2021 DRAFT

LAKESWOOD STATION DISTRICT

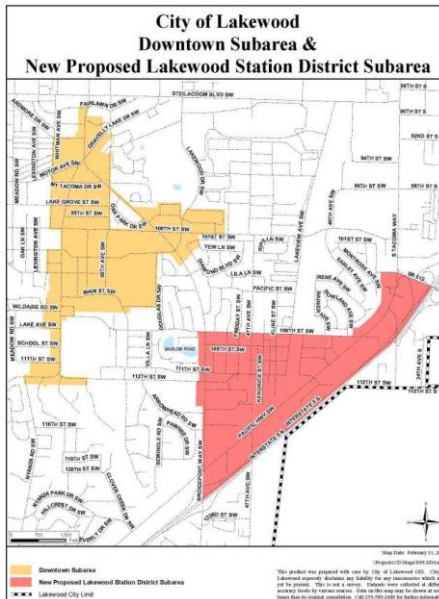
FORM-BASED CODE

Overview

The City of Lakewood is adopting a subarea plan for the Station District. The Station District is located southwest of Downtown and is centered around the Sounder Station. The City adopted a new subarea plan and hybrid form-based code for Downtown in the fall of 2018; the City intends to adopt a similar hybrid form-based code for the Station District that addresses the specific context of the Station District, including land use, streets and transportation, open space, and desired urban form. This framework will address how best to apply a hybrid form-based code to the Station District.

The draft code includes the following sections:

1. Station District
 - Purpose.
 - Vision and Objectives.
 - The Regulation Plan Map.
 - Definitions.
 - Relationship to Other Regulations.
2. Land Use and Zoning
 - Application within Station District.
 - Land Use.
 - District-Wide Development Standards.
3. Streets and Blocks
4. Site Design, Buildings, and Frontage
5. Landscaping, Open Space, and Green Infrastructure
 - Basic Standards.
 - Special Landscape Treatments.
 - Common and Open Space Standards.
 - Green Infrastructure.
6. Parking



Downtown and Station District Subareas

City of Lakewood, 2019

March 29, 2021 DRAFT

Title 18C
STATION DISTRICT DEVELOPMENT CODE

Chapters:

- 18C.100 Station District**
- 18C.200 Land Use and Zoning**
- 18C.300 Streets and Blocks**
- 18C.400 Site Design, Buildings, and Frontage**
- 18C.500 Landscaping, Open Space, and Green Infrastructure**
- 18C.600 Parking**

Chapter 18C.100 STATION DISTRICT

Sections:

18C.100.100	Background.
18C.100.105	Purpose.
18C.100.110	Vision and Objectives.
18C.100.120	The Regulating Plan Map.
18C.100.130	Definitions.
18C.100.140	Relationship to Other Regulations.

18C.100.100 Background. The Station Area District is defined by its location along major transportation corridors including Pacific Highway, I-5, and the railroad (See Figure 18C.100-1). These transportation corridors limit street, bike, and pedestrian connections. However, a pedestrian bridge connects the Sounder Station to the neighborhood north of the railroad tracks. Land use in the district varies from large auto-oriented uses to multi-family and single-family housing. Major arterial streets in the district include Pacific Highway and Bridgeport Way. Within the confines of the Regulating Plan Maps, there are several dead-end streets. Some of these streets were the result of the construction of I-5 which began in the late 1950's and culminated in November 1968. Other dead-end streets are associated with the current railroad right-of-way which existed well before the establishment of many of the residential subdivisions within the area, and, finally, prior to Lakewood's incorporation, inadequate county subdivision code regulation that did not take into account urban development.

18C.100.105 Purpose.

The purpose of the Station District is to provide for coordinated and high-quality development including new housing, commercial development, parks and open space, and multi-modal transportation infrastructure within the Subarea. The Station District Hybrid Form-Based Code Overlay is intended to guide private development and changes in the public right-of-way to support the goals and implementation of the Station District Subarea Plan.

18C.100-1. Station District

Commented [LG1]: New map



Framework, 2020

18C.100.110 Vision and Objective.

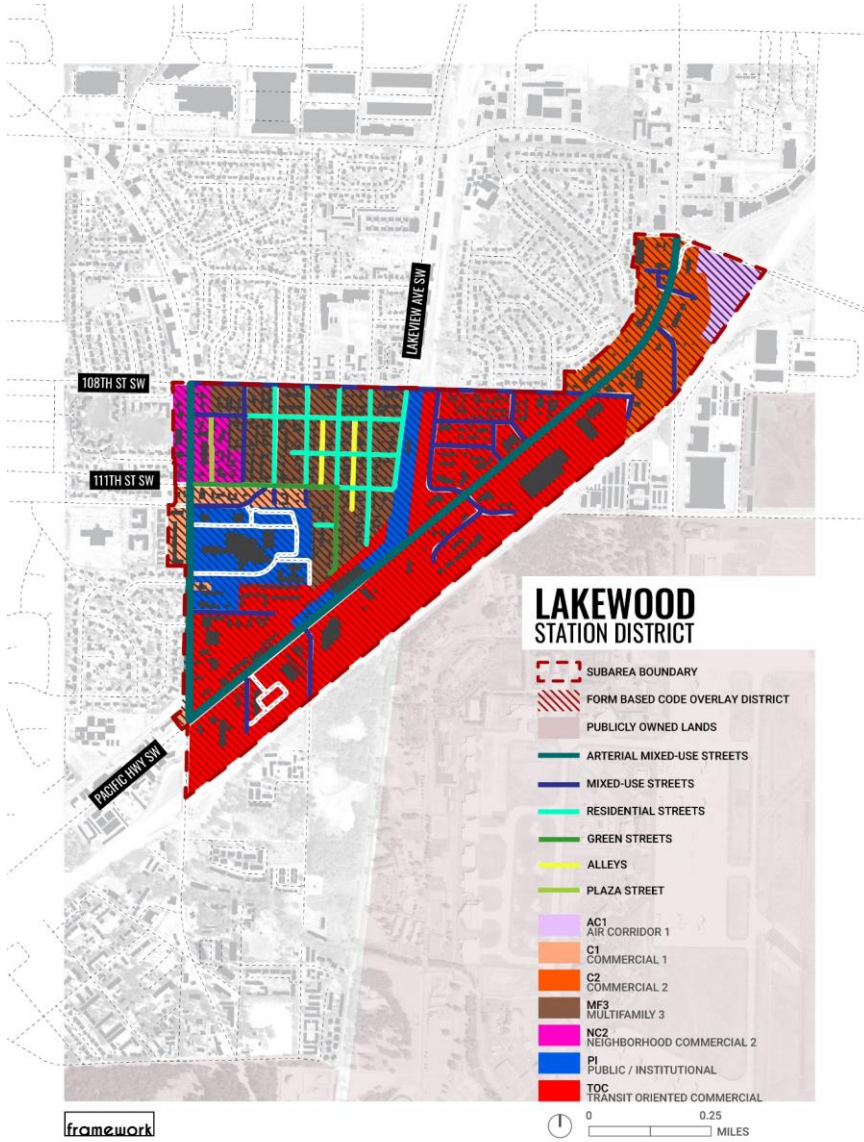
The Lakewood Station District is a multi-modal commuter hub of Lakewood and is the southern terminus of Sound Transit’s commuter rail service. The Lakewood Station District provides an amenity-rich, transit-oriented development node surrounding the Lakewood Station. This District offers a mixture of intensive land uses and activities supportive of direct regional transportation access via the commuter rail station and I-5. The District implements development standards to foster a high quality, pedestrian-oriented urban environment, including incentives to encourage a dense mix of commercial and medical office, regional and local retail, services and hospitality, and high-density residential uses offering ownership and rental housing opportunities, all supported by direct regional transportation access.

18C.100.120 The Regulating Plan Map.

The regulating plan translates the community vision into a map. The regulating plan designates the locations, subdistricts, and streets that are intended to embody specific physical characteristics. It specifies the location and applicability of specific design treatments and identifies where they are required. The regulating plan works in tandem with the development standards, tables, and figures in this chapter, the rest of Title 18A and the LMC to define the shape, size, and location of streets through connections, infill blocks, buildings, and landscaping.

18C.120-1 Regulating Plan Map with Zoning

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Framework, 2020

18C.120-2 Regulating Plan Map – Street Typologies

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Framework, 2020

March 29, 2021 DRAFT

18C.100.130 Definitions.

Definitions in this chapter shall be consistent with LMC 18A.10.180, Definitions. Interpretations of certain terms and phrases shall be consistent with LMC 18A.10.040, Rules of Construction.

18C.100.140 Relationship to Other Regulations.

In the case of a conflict between the regulations in Chapter 18C.100 LMC, Station District, and the rest of the Lakewood Municipal Code, the regulations in Chapter 18C.100 LMC, Station District Form-Based Code Overlay, shall control, with the exception of LMC Title 15, which will govern issues of public health and safety.

Chapter 18C.200 LAND USE AND ZONING

Sections:

- 18C.200.210** **Application within Lakewood Station District.**
- 18C.200.220** **Land Uses.**
- 18C.200.230** **District-Wide Development Standards.**

18C.200.210 Application within Station District.

The Lakewood Station District Subarea (LSDS) has several base zones that are applied as shown in the map below. The zones are described as follows:

- A. The Multifamily 3 (MF3) zoning district is intended to integrate urban, high-density, multi-story housing in close proximity to a principal or minor arterial, with commercial/residential districts. The MF 3 zoning districts are predominantly located adjacent to land zoned NC2, CBD, or SD.
- B. The Neighborhood Commercial 2 (NC2) zoning district is intended to foster a sense of urban community in Lakewood. The district provides for a concentrated mix of activities, including residential, retail, office, and local services, which may serve the surrounding neighborhood or may serve more than one (1) neighborhood and attract people from other areas.
- C. The Commercial 1 (C1) and Commercial 2 (C2) zoning districts promote employment, services, retail, and business uses serving and linking neighborhoods to Lakewood's major transportation networks. The geographic relationship of the corridors to major road networks and their limited integration with adjacent neighborhoods promote employment, services, retail, and business/light industrial uses linked to access the major transportation networks. The C3 zoning district is distinguished by its arterial location and focus on "big-box" type uses which form an anchor for a large-scale commercial development.
- D. The Transit-Oriented Commercial (TOC) zoning district is an interactive mixture of uses which focus on regional transportation networks while providing for urban design, people orientation, and connectivity between uses and transportation routes.
- E. The Public/Institutional (PI) zoning district provides for moderate-scale and large-scale activities relating to the purpose of state and local governmental entities, except for military uses which are separately designated and zoned; special districts; and semi-public institutions providing

March 29, 2021 DRAFT

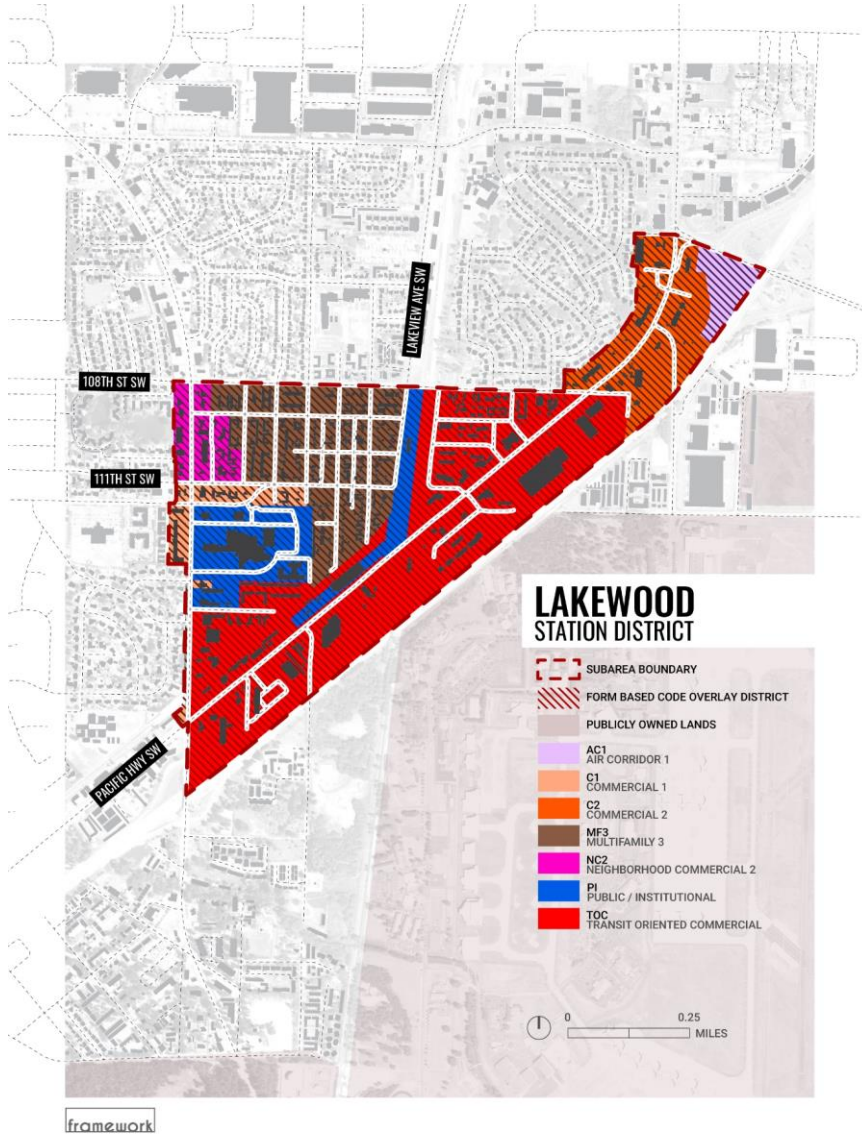
necessary public services. The designation allows for the specialized needs of providing public services to all areas of Lakewood.

F. The purpose of the Clear Zone (CZ), Air Corridor 1 (AC1), and Air Corridor 2 (AC2) zoning districts is to promote land use and development that is compatible with the aircraft noise and accident potential associated with the proximity to McChord Field, which is a part of the Joint Base Lewis-McChord (JBLM). McChord Field hosts three C-17 airlift squadrons. The potential risk to life and property from hazards associated with military aircraft operations necessitates control of the intensity, type, and design of land uses within the air corridor.

G. The Open Space and Recreation 1 (OSR1) and Open Space and Recreation 2 (OSR2) zoning districts provide for open space and public or semi-public recreational activities throughout the City.

18C.200-1. Station District Zoning Map

Commented [LG4]: New map



Framework, 2020

18C.200.220 Land Use.

A. Allowed and Prohibited Uses. All residential uses, civic uses, and commercial uses and their respective uses and their related accessory uses as defined in Chapter 18A.40 LMC are permitted within the Station District except for those prohibited uses listed below. Permitted uses are subject to the approval of all required development permits. Uses identified as prohibited below that legally existed prior to the adoption of this code are considered non-conforming.

1. *Prohibited uses in the AC1 zoning district:*
 - a. None.
2. *Prohibited uses in the MF3 zoning district:*
 - a. Building & landscape materials;
 - b. Convenience store;
 - c. General retail.
3. *Prohibited uses in the NC2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Solid waste transfer station;
 - d. Vehicle services, major repair/body work.
4. *Prohibited uses in the TOC zoning district:*
 - a. Accessory caretaker's unit.
5. *Prohibited uses in the C2 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Manufacturing, assembling, & packaging, medium;
 - c. Manufacturing, assembling, & packaging, heavy;
 - d. Metal products fabrication, machine and welding;
 - e. Mobile home, RV, and boat sales;
 - f. Recycling facility – processing facility;
 - g. Repair service - equipment, large appliances.
 - h. Vehicle services, major repair/body work;
6. *Prohibited uses in the C1 zoning district:*
 - a. Auto and vehicle sales/rental;
 - b. Building & landscape materials;
 - c. Construction/heavy equipment sales and rental;

- d. Enhanced services facility;
- e. Equipment rental;
- f. Fuel dealer;
- g. Furniture/fixtures manufacturing, cabinet shop;
- h. Laundry, dry cleaning plant;
- i. Manufacturing, assembling, & packaging, medium;
- j. Manufacturing, assembling, & packaging, heavy;
- k. Mobile home, RV, and boat sales;
- l. Outdoor storage;
- m. Pawnbrokers and secondhand dealers;
- n. Recycling facility – processing facility;
- o. Repair service - equipment, large appliances;
- p. Secondhand stores;
- q. Storage, personal storage facility;
- r. Type 4 group home;
- s. Type 5 group home;
- t. Vehicle services, major repair/body work;
- u. Vehicle storage;
- v. Warehouse;
- w. Warehouse retail;
- x. Wholesaling and distribution.

B. Allowed Use Conditions. The following uses are allowed, provided the conditions listed below are met:

1. Drive-through facilities; *provided:*

- a. Drive-through facilities are limited to one drive-through lane per establishment;
- b. Drive-through facilities must have a primary customer entrance and cannot provide customer service exclusively from a drive-through or walk-up window;
- c. Drive-through facilities shall be designed so that vehicles, while waiting in line to be served, will not block vehicle or pedestrian traffic in the right-of-way;

d. Drive-through facilities shall be appropriately and attractively screened from the public right-of-way:

ii. Drive-through lanes shall only be placed parallel to a road if separated by a distance of 30 feet, or if fully screened by a 15-foot landscape setback with a designed landscape berm (six feet high at center of berm in 15-foot landscape setback) or three-and-one-half-foot decorative masonry wall;

iii. Drive-through lanes oriented perpendicular to a public right-of-way shall include landscape screening to shield headlights from shining directly into an abutting or adjacent street right-of-way.

C. Conditionally Permitted Uses.

1. Pipelines.

2. Manufacturing, assembling and packaging, light is allowed in the Station District, but such uses shall be as part of mixed-use development with commercial, retail, or residential uses.

D. Permitted Uses in the MS3 Zone within the LSDS.

1. Rowhouses.

2. Townhouses.

3. Three-family residential, attached.

18C.200.230 District-Wide Development Standards.

Unless otherwise specifically modified by an adopted development agreement or master plan, in addition to the regulations and requirements contained in other sections of this title and LMC Title 15, the following property development standards apply to all land and buildings in the Station District:

A. *Density*. Maximum density is based on the standards in the base zoning districts.

B. *Lot Size*. There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

C. *Lot Coverage*. There is no minimum established lot size for the Station District. Proposed uses and the applicable design standards in this chapter shall be used to establish the minimum lot size for a project.

March 29, 2021 DRAFT

D. *Setbacks.* The minimum distance setbacks for the Station District shall be determined by frontage type in Chapter 18C.400 LMC, except where increased setbacks due to building/fire code requirements apply.

E. *Building Height.* The maximum building height, not including any applicable height bonus, for the Station District shall be determined by the underlying zoning districts.

F. *Large-scale Commercial Facilities.* Large-scale commercial facilities shall meet the additional requirements specified in LMC [18A.70.050\(J\)](#).

G. *Design.* Design features shall be required as set forth in Chapter 18C.300 LMC, Streets and Blocks, and Chapter 18C.400 LMC, Site Design, Buildings, and Frontage, and the community design standards in LMC [18A.70.040](#).

H. *Tree Preservation.* Significant tree identification and preservation and/or replacement shall be required as set forth in Chapter [18A.70](#) LMC, Article III, Tree Preservation.

I. *Landscaping.* Landscaping shall be provided as set forth in Chapter 18C.500 LMC and Chapter [18A.70](#) LMC, Article II, Landscaping.

J. *Parking.* Parking shall conform to the requirements of Chapter 18C.600 LMC and Chapter [18A.80](#) LMC, Parking.

K. *Signs.* Signage shall conform to the requirements of Chapter [18A.100](#) LMC, Sign Regulations.

Chapter 18C.300 STREETS AND BLOCKS

Sections:

18C.300.310 Streets and Blocks.

18C.300 Streets and Blocks.

A. *Street Grid and Blocks.*

1. *Street Grid.* A circulation plan must be submitted for review by the City as part of any development permit in the Station District unless waived by the City upon finding the project will not impact circulation or the enhancement of the public street grid. New development and redevelopment must demonstrate the plan supports and accommodates the expansion of the public street grid to improve circulation for vehicles, pedestrians, and bicyclists.

2. *Block Size.* Block Size. The maximum block size is 400 feet and the maximum distance permitted between public streets. New public street alignments shall be consistent with the regulating plan map. The City may approve modifications to the street alignments and waive the 400-foot maximum block size to take advantage of existing travel corridors, the location of utilities, and required improvements.

3. *Private Streets.* Private streets shall only be permitted when the City has determined there is no public benefit for circulation in the Station District. All private streets must be constructed to public standards.

4. *Mid-Block Connections/Alleys.* A minimum 20-foot-wide mid-block connection (alley) shall be provided at the midpoint along each block face or every 200 feet. The mid-block connections shall be designed to accommodate service needs and for pedestrian use and be free from permanent obstructions.

5. *Street Sections.* The typical street sections provided below are the minimum requirements for the design of public streets. The City may approve modifications to the typical street section based on localized conditions and adjacent land uses. Modifications may include adding or removing on-street parking, wider sidewalks, loading zones, bicycle facilities, and transit accommodations.

6. *Block Development.* The minimum number of buildings per 400 feet of block frontage is four, or one building per 100 feet, to create variety in the streetscape experience and support human-scale design. A single building may meet this requirement through building design and architecture that visually appears as multiple buildings. The City may approve modifications to this requirement based on site-specific conditions, including parcel ownership and configuration.

B. *Street Types.*

1. *Mixed-Use Streets.* Mixed-use streets support a variety of activities and functions both in the public right-of-way and development along the street edges. Street level retail is permitted, but not required, and a wider range of building frontage types are permitted, including street-level residential and office uses. Mixed-use streets also require pedestrian-oriented design and requirements may vary based on the location within Station District.

2. *Arterial Mixed-Use Street.* Arterial mixed-use streets maintain the existing vehicle capacity and allow a range of land use and building frontage types and include surface parking lots.

3. *Residential Street.* Residential streets serve residential neighborhoods and include vehicle travel with sidewalks and bike lanes.

4. *Green Street.* Green streets include vehicle travel lanes, sidewalks, bike lanes, and open space for stormwater management, landscaping, and gathering space.

C. *Street Standards and Frontage Types.* The following street standards are the minimum required and modifications to the standards may be approved by the City upon finding that the modification is consistent with the vision and objectives in LMC 18C.100.110 , supports pedestrian-oriented design, and balances the need for traffic flow to minimize congestion.

18C.300-1. Street Standard and Frontage Types

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Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	XP	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X
<u>Plaza Street</u>	<u>12' minimum or shared street</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>P</u>	<u>X</u>	<u>X</u>

P=permitted, X=prohibited, R=required

March 29, 2021 DRAFT

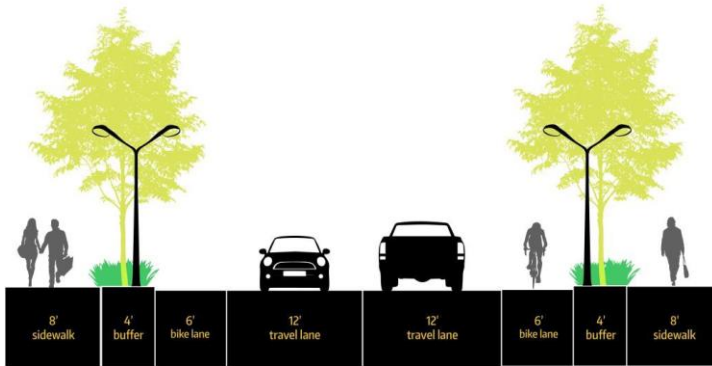
1 Required when on a corner lot.

D. *Street Sections.* The following street sections show the basic elements of the streetscape for each of the streets highlighted in Station District. The City may approve modifications to the typical cross sections based on site-specific conditions including adjacent land uses, traffic management, parking needs, and right-of-way constraints.

1. *Residential Street Typology.* Many of the streets identified as Residential Streets do not have curbs, gutter, or sidewalks and there is a lack of definition between the public right-of-way and private property. Parking occurs along the street edge often in gravel shoulders and without a consistent pattern. Established residential street ROW varies in the Station District. Figure 18C.300-2 below shows a standard residential street section with a 60' right-of-way that includes travel lanes, sidewalks, and bike lanes. In the future, bike lanes could be converted to on-street parking. Figure 18C.300-3 shows an 80' right-of-way with native plantings on either outer edge of the right-of-way.

18C.300-2. Residential Street Typology – 60' ROW

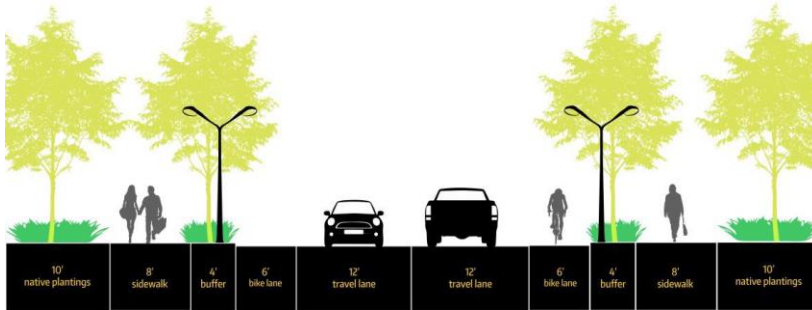
RESIDENTIAL STREET CONCEPT
60' Right-of-Way



Framework, 2020

18C.300-3. Residential Street Typology – 80' ROW

RESIDENTIAL STREET CONCEPT
80' Right-of-Way



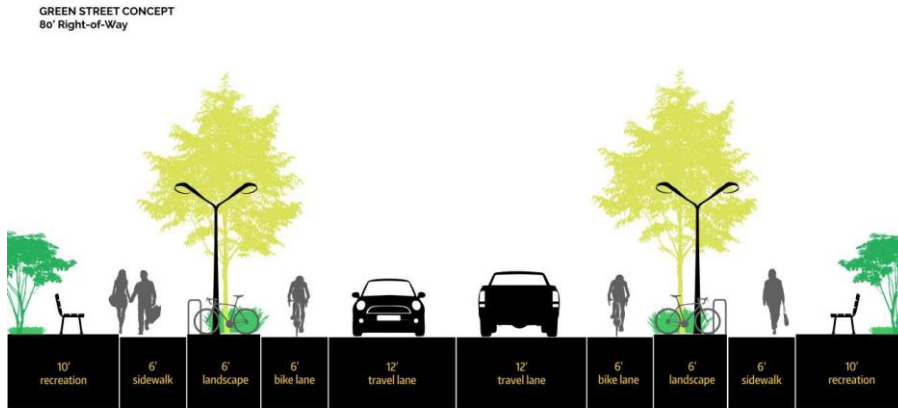
Framework, 2020

2. *Green Street Typology.* Kendrick Street SW connects to the Sounder Station to the South and to other neighborhoods to the north. The existing right-of-way width varies from between 60' and 80' and is an opportunity to integrate green street elements. The southern block of Kendrick

March 29, 2021 DRAFT

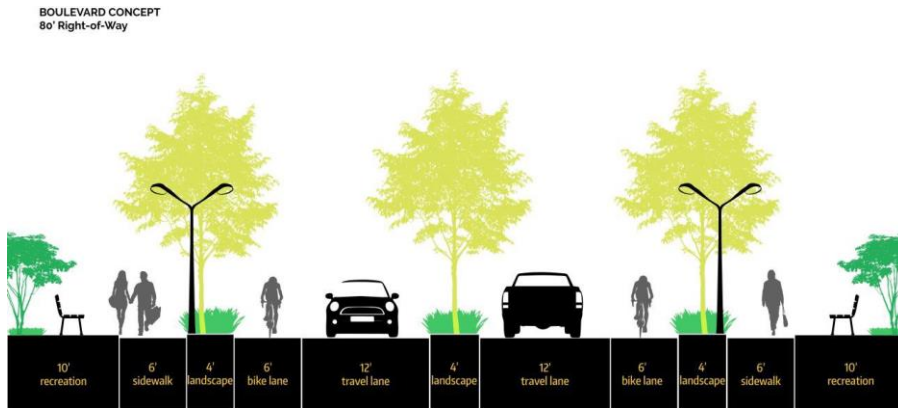
St SW appears to have been recently improved, so the following concepts focus on the blocks north of 111th St SW as shown in Figures 18C.300-4 through 6 below.

18C.300-4. Green Street Concept #1 – 80' Right-of-Way



Framework, 2020

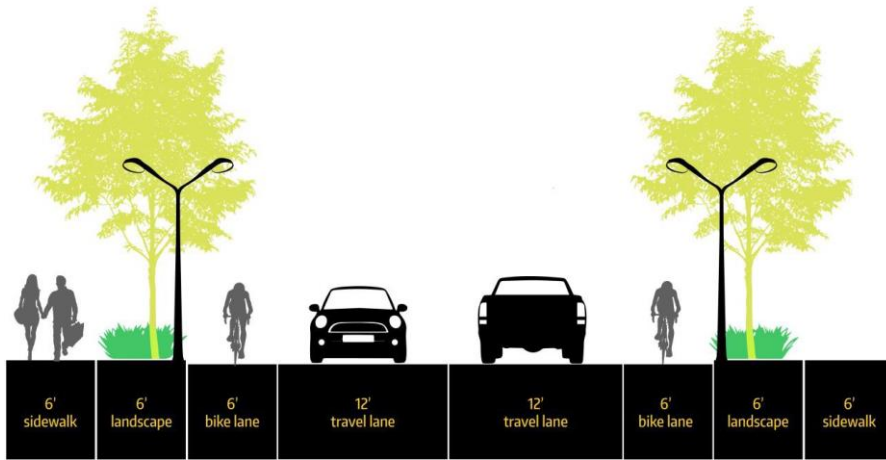
18C.300-5. Boulevard Concept – 80' Right-of-Way



Framework, 2020

18C-300-6. Green Street Concept #2 – 60' Right-of-Way

GREEN STREET CONCEPT
60' Right-of-Way



Framework, 2020

E. Plaza Street. Plaza streets are located along commercial and mixed-use zoning with active uses and may vary in width. Plaza streets are designed primarily for people and activities while allowing for vehicle access to adjacent properties. Plaza streets may have dedicated sidewalks or use a shared street design with appropriate traffic calming design features. Plaza streets should include high-quality materials, integrated landscaping and trees with open space, street furniture such as places for people to sit, bike parking, lighting, public art opportunities, and a flexible design that can support a variety of events and activities such as times where the street may be temporary closed to vehicle traffic.

March 29, 2021 DRAFT

18C-300-7. Plaza Street Concept – 80' Right-of-Way

80' PLAZA STREET
CONCEPT



Framework, 2020

18C-300-8. Plaza Street Concept – 60' Right-of-Way

60' PLAZA STREET
CONCEPT

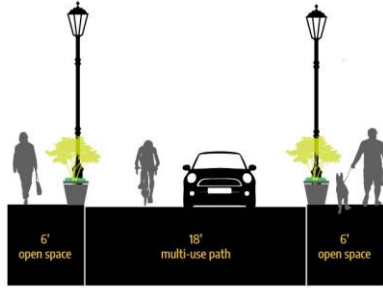


Framework, 2020

March 29, 2021 DRAFT

18C-300-8. Plaza Street Concept – 30' Right-of-Way

30' PLAZA STREET
CONCEPT



Framework, 2020

E.F. Alleys. Alleys are encouraged to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards.

F.G. Weather Protection – Easements. Weather protection or building overhangs that extend over public rights-of-way may be permitted by the Community Development Director subject to execution of an easement and requirements for maintenance by the property owner or developer.

1. Access to existing and future utilities within and under the street and boulevard must be maintained.
2. Freedom of movement of existing and future vehicular and pedestrian activity must not be restricted.
3. Weather protection depth and percentage coverage shall be consistent with LMC 18C.400.410(A) for frontage types.

| March 29, 2021 DRAFT

4. All overhead weather protection shall be placed at a height that relates to architectural features of the building and adjacent storefront weather protection, while offering effective protection from weather. Weather protection shall have a minimum vertical clearance of eight feet, measured from the sidewalk, and should not be greater than 12 feet above the pedestrian sidewalk level.

5. The slope of the weather protection feature shall allow for proper drainage and self-cleaning action of rain and wind. Materials used should be durable and require minimum maintenance.

G.H. When Required. Street Frontages shall be implemented consistent with LMC 12.09.031.

Chapter 18C.400 SITE DESIGN, BUILDINGS, AND FRONTAGE

Sections:

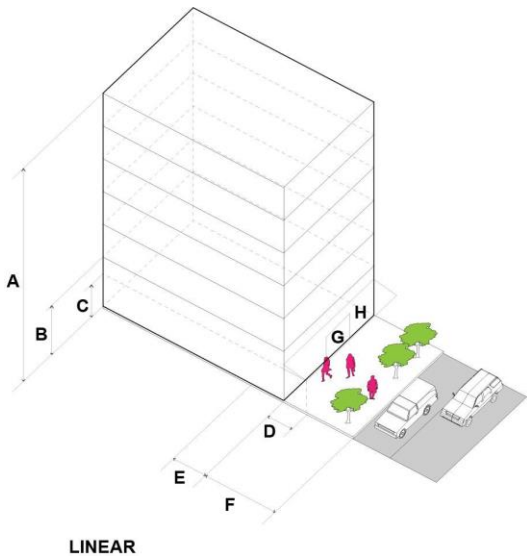
18C.400.410 Site Design, Buildings, and Frontage.

18C.400.410 Site Design, Buildings, and Frontage.

A. *Frontage Types.* The building frontage types below address the required standards for the relationship of buildings to the edge of the street and other site plan and design requirements. The permitted frontage types vary by street type as shown in LMC 18C.300.310(C). The building setback may be modified as approved by the City when necessary to expand the width of the right-of-way to accommodate the desired street design and cross section.

1. *Linear.* The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

18C.400-1. Linear Frontage Type



Framework, 2020

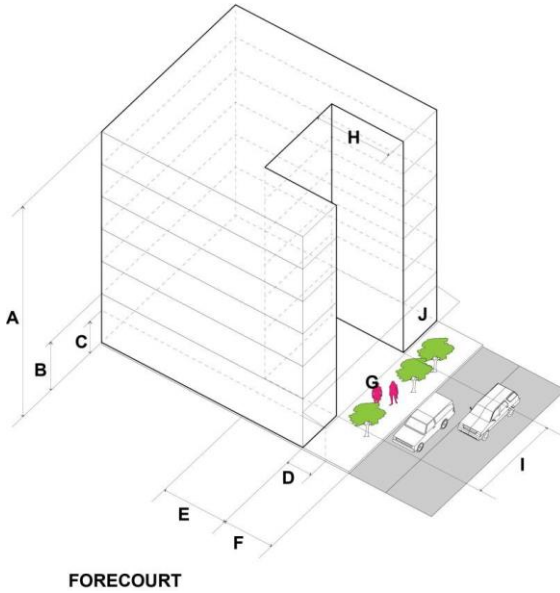
18C.400-2. Linear Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	n/a
B First Floor Minimum Height	16'	16'	n/a
C Weather Protection Height	10'	10'	n/a
D Weather Protection Minimum Depth and Linear Frontage	10' Depth Weather protection required for minimum of 60% of frontage	10' Depth Weather protection required for minimum of 60% of frontage	n/a
E Building Maximum Setback from Right-of-Way	10'	10'	n/a
F Building Setback Minimum	0'	0'	n/a
G Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

2. *Forecourt.* The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

18C.400-3. Forecourt Frontage Type



Framework, 2020

18C.400-4. Forecourt Frontage Standards

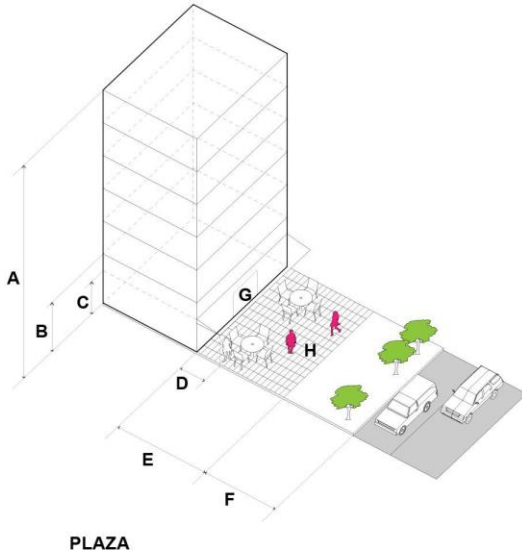
Standard	Mixed-Use Street/ <u>Plaza</u> , Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height	16'	16'	n/a
C Weather Protection Height	10'	10'	n/a
D Weather Protection Minimum Depth and Forecourt Frontage	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	15' min depth; 60% of frontage along sidewalk must have weather protection. Weather protection required over primary entrance.	n/a
E Building Maximum Setback from Right-of-Way	10'	10'	20'
F Building Setback Minimum	0'	0'	10'
G Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required
H Forecourt Depth	10' minimum;	10' minimum;	10' minimum

Standard	Mixed-Use Street/Plaza, Street	Mixed-Use Arterial	Residential Street/Green Street
	30' maximum	30' maximum	30' maximum
I Forecourt Width	10' minimum; 30' maximum	10' minimum; 30' maximum	10' minimum; 30' maximum
J Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	none

1 The maximum setback shall be up to 10 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is zero feet.

3. *Plaza*. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events. The plaza must be designed to support human activity and support a safe and inviting streetscape environment.

18C.400-5. Plaza Frontage Type



PLAZA

Framework, 2020

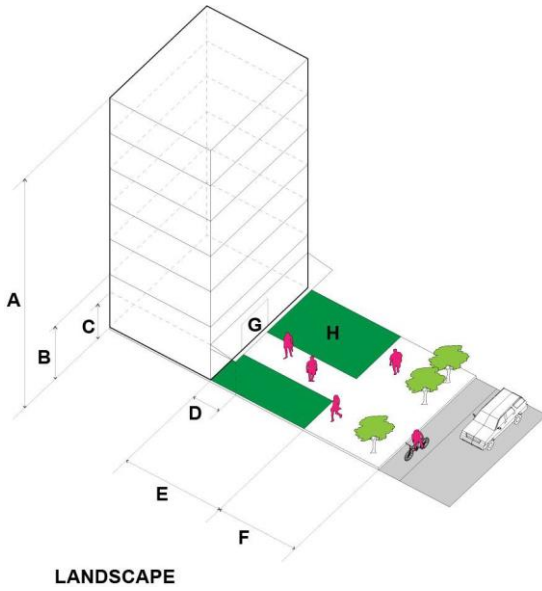
18C.400-6. Plaza Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	90'	90'	n/a
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	n/a
D Weather Protection Minimum Depth and Linear Frontage	5' min depth; 60% of frontage along sidewalk must have weather protection	5' min depth; 60% of frontage along sidewalk must have weather protection	n/a
E Building Maximum Setback from Right-of-Way	5'-25'	5'-25'	n/a
F Building Setback Minimum	5'	5'	n/a
G Front Entrance	An entrance must be located along the primary street frontage; 50% transparency required	An entrance must be located along the primary street frontage; 50% transparency required	n/a
H Plaza Design	Plaza area must be designed for human activity and include seating and other amenities	Plaza area must be designed for human activity and include seating and other amenities	n/a
I Window Transparency	A minimum 60% transparency required at street level between 30" and 8' for nonresidential uses	none	n/a

¹ The maximum setback shall be up to 25 feet where the Public Works Engineering Director has determined property may be needed to accommodate the City's planned right-of-way at full build-out. Otherwise, the maximum setback is five feet.

4. *Landscape*. The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

18C.400-7. Landscape Frontage Type



Framework, 2020

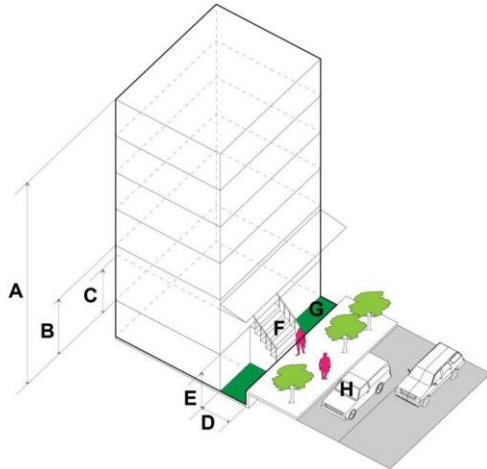
18C.400-8. Landscape Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	10'
D Weather Protection Minimum Depth and Linear Frontage	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance	5' minimum depth; required over primary entrance
E Building Maximum Setback from Right-of-Way	20'	20'	20'
F Building Setback Minimum	10'	10'	10'

Standard	Mixed-Use Street	Mixed-Use Arterial	Residential Street/Green Street
G Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
H Landscape Requirements	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

5. *Porch/Stoop/Terrace*. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

18C.400-9. Porch/Stoop/Terrace Frontage Type



PORCH/STOOP/TERRACE

Framework, 2020

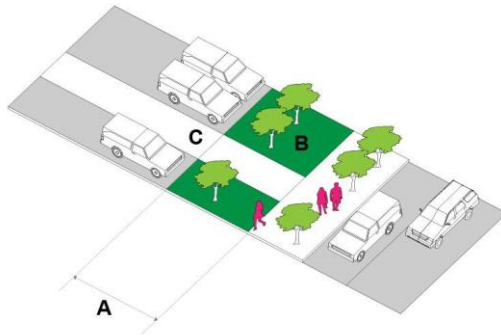
18C.400-10. Porch/Stoop/Terrace Frontage Standards

Standard	Mixed-Use Street/Plaza Street ¹	Mixed-Use Arterial	Residential Street/Green Street
A Building Height	Base Zone	Base Zone	Base Zone
B First Floor Minimum Height for depth of 30 feet from street: Commercial Uses	16'	16'	n/a
C Weather Protection Height	10'	10'	10'
D Threshold Depth	4' minimum	4' minimum	4' minimum
E Threshold Height	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'	Below-grade maximum 4'; Above-grade maximum 5'
F Front Entrance	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage	An entrance must be located along the primary street frontage
G Landscape Requirement	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.	The landscape area shall comply with the City's landscaping requirements.

¹ Prohibited on plaza street

6. *Parking*. The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

18C.400-11. Parking Frontage Type



PARKING

Framework, 2020

18C.400-12. Parking Frontage Standards

Standard	Mixed-Use Street/Plaza Street	Mixed-Use Arterial	Residential Street/Green Street
A Landscape Buffer Width	n/a	Minimum 10'; Maximum 20'	n/a
B Landscape Requirements	n/a	The landscape area shall comply with the City's landscaping requirements.	n/a
C Pedestrian Walkway	n/a	Minimum width – <u>see LMC 18C.500.520</u>	n/a

B. *Civic Uses.* Sites and buildings where the primary use is civic should be designed to reflect a public use and therefore may deviate from the frontage standards. Civic buildings and sites should be recognizable and accessible as public buildings in Station District and to reflect civic pride in the community. The following standards address the design of civic uses:

1. Civic uses are permitted on the ground level of all street types except the low-impact mixed-use streets and may be designed in accordance with any of the frontage types except parking.
2. Civic uses are not subject to the maximum setback requirements.
3. Civic uses may provide the main entry on the side of the building when necessary to the function of the site, including safety and security. A pedestrian connection shall be provided from the sidewalk to the entry.
4. The design sites and buildings for civic uses should incorporate public open space when feasible and practical and provide for a variety of activities such as public art, seating, play equipment, games, and events.

C. *Townhouse/Rowhouse Development.* The following standards apply to townhouse and rowhouse development:

1. Lot widths for townhouses and rowhouses may be less than the 50' minimum required for new parcels.
2. Site access to garages and parking area shall be consolidated with the use of alleys or a common driveway to shared parking areas.
3. Individual driveways are prohibited from any street with a street type designation and all on-site parking shall be located in the rear or in garages.

Chapter 18C.500

LANDSCAPE, OPEN SPACE, AND GREEN INFRASTRUCTURE

Sections:

- 18C.500.510** **Basic Standards.**
- 18C.500.520** **Special Landscape Treatments.**
- 18C.500.530** **Common and Open Space Standards.**
- 18C.500.540** **Green Infrastructure.**

18C.500.510 Basic Standards.

Landscaping shall be provided consistent with Chapter 18C.70 LMC, Article II, Landscaping, except as supplemented or superseded by this title.

18C.500.520 Special Landscape Treatments.

The following special landscape treatments are required or permitted along street types within the Station District:

18C.520-1. Special Landscape Treatments

Street Type	Foundations	Plazas/Courtyards	Pedestrian Walkways
Mixed-Used Street	R	P	R
Arterial Mixed-Used Street	P	P	R
Residential Street/Green Street	R	P	P

P = permitted, R= required

1 Plaza landscaping is required when the plaza frontage is selected on a street.

A. *Foundations.* Buildings shall be accented by foundation plantings. Foundation plantings may frame doors, anchor corners, or screen undesirable areas. Foundation plantings shall allow for five feet of unobstructed movement on the sidewalk. Examples of foundation plantings include raised planter boxes, containers, and similar.

B. *Plazas and Courtyards.* Where provided, consistent with frontage standards, plazas or courtyards shall meet the following standards:

1. Plazas and courtyards shall be visible and accessible to the public.
2. Minimum dimensions of plazas and courtyards are 20 feet. Courtyards may qualify as common open space per LMC 18C.500.530, Common and Open Space Standards.

3. Plazas or courtyards shall include one or more element from each category below.
 - a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material.
 - b. *Landscaping.* Trees, shrubs, trellises, flowers, or container plants. Landscaping shall be consistent with this chapter and Chapter 18A.70 LMC, Article II, Landscaping.
 - c. *Seating Area.* Benches or low seating walls. Walls or benches shall be a minimum of four feet long. Seating shall be provided at a ratio of at least one seat per 60 square feet of plaza area or open space.

C. *Pedestrian Walkways.* Pedestrian walkways are required as follows:

1. *Street to Building.* Required between a public right-of-way and building entrances where the building is set back from the street, sidewalk, or parking area.
2. *Parking.* Required between parking lots and building entrances.
3. *Mid-Block Connections.* One pedestrian walkway shall be provided between the street and the rear property line, spaced an average of every 200 feet or less of street frontage.

D. *Dimensional Standards.*

1. *Pedestrian Walkways.*

- a. *Width.* Six feet minimum, exclusive of landscaping or site furnishings, except that mid-block crossings shall have minimum walkway widths of 12 feet.
- b. *Lines of Sight.* Walkways shall be located and designed with clear sight lines for safety.
- c. *Definition.* Pedestrian walkways shall be defined through paving, landscaping, furnishings, and lighting.

2. *Design.* Pedestrian walkways shall be defined using a combination of one or more of the following techniques:

- a. *Special Paving.* Scored concrete, stained/colored concrete, concrete pavers, paving inlays, mosaics, or other special paving material. Not all portions of the pedestrian walkway are

required to be paved; however, a minimum of four feet in width of the pedestrian walkway shall provide an all-weather walking surface.

b. *Architectural Features.* Trellises, railing, low seat walls, weather protection, bollards, or other architectural features. Chain link fences are not allowed.

c. *Landscaped Edges.* A continuous, landscaped area consistent with Type II, Streetscapes, where the pedestrian walkway is from the street to building or mid-block connection, otherwise Type III, Open Space, is required, per Chapter 18A.70 LMC, Article II, Landscaping. If the walkway abuts a driveway or driving aisle on both sides, the landscaped edge shall apply to both sides.

d. *Lighting.* Pedestrian walkways shall include lighting such as pedestrian lights, bollards, and accent lighting to assist pedestrian navigation and promote a safe and comfortable walking space.

18C.500.530 Common and Open Space Standards.

The purpose of this section is to provide residents with access to usable, privately maintained and owned open space to create opportunities for active living and respite for on-site residents.

A. Common Open Space – Private.

1. Each mixed-use or residential development shall provide a common open space per dwelling unit of 100 square feet per dwelling unit.

2. For the purposes of this section, common open space means an open-air area intended for use by all residents, guests, employees or patrons of a site and may include lawns, gardens, squares, plazas, courtyards, terraces, barbecue and picnic areas, games court or multi-use recreational areas, and other types of built space.

3. Design Standards.

a. Required setback areas shall not count toward the open space requirement unless they are part of an open space that meets the other requirements of common open space.

b. Space shall have a minimum dimension of 20 feet in any direction to provide functional leisure or recreational activity. This dimension can be adjusted by the Community

Development Director based on site conditions such as topography or irregular lot geometry.

c. Space shall feature paths or walkable areas, landscaping, seating, lighting, play structures, sports courts, or other pedestrian amenities to make the area more functional and enjoyable for a range of users.

4. Common space shall be separated from ground level windows, streets, service areas and parking lots with landscaping, low-level decorative fencing (no chain link), or other treatments as approved by the Community Development Director that enhance safety and privacy for both the common open space and dwelling units.

5. The space shall be oriented to receive sunlight and = face south if possible. Open space may also face east or west, but not north, unless the Community Development Director determines that site conditions such as topography or irregular lot geometry warrant waiving this requirement. The common open space shall be designed to provide landscaping that defines the open space but permits surveillance from units and roads.

6. The space must be accessible from the dwelling units. The space must be oriented to encourage activity from local residents.

7. No more than 30 percent of the area may be covered by a structure.

8. The common open space may include multi-use storm water detention facilities if the Community Development Director determines that the facilities are designed to function as common open space by providing an enhanced natural or visually aesthetic design.

9. The common space shall be designed to ensure that the open space network addresses Crime Prevention Through Environmental Design (CPTED) principles such as security and surveillance from residential units. Common recreational spaces shall be located and arranged to allow windows to overlook them.

10. The common space shall provide adequate lighting in the open space network, but place and shield lighting so that it does not glare into housing units.

B. Private Open Space.

March 29, 2021 DRAFT

1. Each dwelling unit shall have a private open space, at a minimum of 48 square feet with a minimum width or depth of six feet.
2. For the purposes of this section, private open space includes individual decks, balconies, or patios.

C. Fee in Lieu for Portion of Open Space.

1. Within the station district planned action area, a development may pay a fee in lieu of providing common open space or a portion of the private open space to support park land purchase and improvements within the station district planned action area for urban nodal or urban linear parks identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan.

Commented [LG6]: Adapted the DT code

2. The fee in lieu is allowed for each 100 square feet of common open space not provided. No less than 50 square feet of common open space per unit shall be provided on site.

Commented [LG7]: Unlike DT code, reference legacy plan as well as station area plan so it gives a little more flex if the legacy plan is updated every six years

3. The fee in lieu is allowed for balconies or patios not provided on street-front facade. No less than 50 percent of the units shall include private open spaces.

4. The fee shall be equal to the average fair market value of the land otherwise required to be provided in on-site common or private open space.

5. Fee Payment and Use.

a. The payment shall be held in a reserve account by the City and may only be expended to fund a capital improvement for parks and recreation facility identified in the station district subarea plan or the legacy plan for parks, recreation & open space master plan.

b. The payment shall be expended in all cases within five years of collection; and

c. Any payment not so expended shall be refunded with interest to be calculated from the original date the deposit was received by the City and at the same rate applied to tax refunds pursuant to RCW 84.69.100; however, if the payment is not expended within five years due to delay attributable to the developer, the payment shall be refunded without interest.

Commented [LG8]: Unlike DT code, reference legacy plan as well as station area plan so it gives a little more flex if the legacy plan is updated every six years

18C.500.540 Green Infrastructure.

- A. Development shall implement storm water standards that incorporate low impact development (LID) principles and standards consistent with City standards in Chapter 12.11 LMC.
- B. Where on-site filtration is feasible, it shall be provided.

| **March 29, 2021 DRAFT**

C. Permeable surfaces shall be incorporated into plazas, courtyards, and pedestrian walkways, unless demonstrated infeasible to the satisfaction of the City Engineer, and shall meet City standards per Chapter 12.11 LMC.

D. Open ponds shall be designed with both natural landscape and urban access and edge treatments consistent with Chapter 18A.70 LMC, Article II.

E. Native and/or drought tolerant landscaping shall be incorporated into required landscape plans.

Chapter 18C.600 PARKING

Sections:

18C.600.610 **Parking.**

18C.600.610 Parking.

- A. *Off-Street Parking Requirements.* The following off-street parking requirements supersede the requirements in Chapter 18A.80 LMC. Uses not listed below must comply with the requirements in Chapter 18A.80 LMC.
- B.

18C-600-1. Off-Street Parking Requirements

Land Use	Vehicular Parking Requirement	Bicycle Parking Requirement
Residential	Single-family: 2 per dwelling unit Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698) Senior citizen apartments: 1 per 3 dwelling units Multifamily housing: 1.25 spaces per dwelling unit No minimum	Meet rates and standards of: Chapter 18A.80
Retail, Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

- B. *Parking Reductions or Increases.* The amount of required parking may be reduced or eliminated, or increased above the maximum, based on a site-specific parking study that demonstrates one or more of the following:

1. *Reduction Due to Shared Parking at Mixed-Use Sites and Buildings.* A shared use parking analysis for mixed-use buildings and sites that demonstrates that the anticipated peak parking demand will be less than the sum of the off-street parking requirements for specific land uses.

2. *Reduction Due to Public Parking Availability.* The availability of public parking to accommodate the parking demand generated by the site or building. The City may approve a reduction in the amount of required parking by up to 50 percent for any parking stalls that will be open and available to the public. On-street parking may be considered for the reduction; any new on-street parking provided will be counted toward the required parking availability.

3. *Reduction Due to Lower Parking Demand or Increase Based on Greater Parking Demand.* Demonstrating that anticipated parking demand will be less than the minimum parking required, or greater than the maximum allowed, shall be based on collecting local parking data for similar land uses on a typical day for a minimum of eight hours.

4. *Reduction for Housing in Proximity to Sounder Station (RCW 36.70A.620).* When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study prepared to the satisfaction of the Community Development Director or their designee:

- a. housing units that are affordable to very low-income or extremely low-income individuals.
- b. housing units that are specifically for ~~seniors or~~ people with disabilities.
- ~~c. market rate multifamily housing.~~

In determining whether to grant a parking reduction, the Community Development Director may also consider if the project is proposed in an area with a lack of access to street parking capacity, physical space impediments, or other reasons supported by evidence that would make on-street parking infeasible for the unit.

C. *Parking Location and Design.* Parking shall be located behind the [building](#) or in a [structure](#) except in locations where the parking frontage type is [permitted](#).

D. *Shared Parking.* Shared parking is encouraged to support a walkable and pedestrian-oriented Station District where people can park once and visit multiple destinations. Off-site shared parking may be authorized per the standards in Chapter [18A.80](#) LMC.

E. *Public Parking.* Public parking is permitted as a principal or accessory use in the Station District subject to the frontage and design standards.

F. *Dimensional Standards.* Parking stall and circulation design shall meet the standards of Chapter [18A.80](#) LMC.

ATTACHMENT C

SEPA ENVIRONMENTAL CHECKLIST

Lakewood Station District Subarea Planned Action

WAC 197-11-960

Purpose of checklist:

The State Environmental Policy Act (SEPA), chapter [43.21C](#) RCW, requires all governmental agencies to consider the environmental impacts of a proposal before making decisions. An environmental impact statement (EIS) must be prepared for all proposals with probable significant adverse impacts on the quality of the environment. The purpose of this checklist is to provide information to help you and the agency identify impacts from your proposal (and to reduce or avoid impacts from the proposal, if it can be done) and to help the agency decide whether an EIS is required.

Instructions for applicants:

This environmental checklist asks you to describe some basic information about your proposal. Governmental agencies use this checklist to determine whether the environmental impacts of your proposal are significant, requiring preparation of an EIS. Answer the questions briefly, with the most precise information known, or give the best description you can.

You must answer each question accurately and carefully, to the best of your knowledge. In most cases, you should be able to answer the questions from your own observations or project plans without the need to hire experts. If you really do not know the answer, or if a question does not apply to your proposal, write "do not know" or "does not apply." Complete answers to the questions now may avoid unnecessary delays later.

Some questions ask about governmental regulations, such as zoning, shoreline, and landmark designations. Answer these questions if you can. If you have problems, the governmental agencies can assist you.

The checklist questions apply to all parts of your proposal, even if you plan to do them over a period of time or on different parcels of land. Attach any additional information that will help describe your proposal or its environmental effects. The agency to which you submit this checklist may ask you to explain your answers or provide additional information reasonably related to determining if there may be significant adverse impact.

Use of checklist for nonproject proposals:

For nonproject proposals complete this checklist and the supplemental sheet for nonproject actions (Part D). The lead agency may exclude any question for the environmental elements (Part B) which they determine do not contribute meaningfully to the analysis of the proposal.

For nonproject actions, the references in the checklist to the words "project," "applicant," and "property or site" should be read as "proposal," "proposer," and "affected geographic area," respectively.

A. Background

1. Name of proposed project, if applicable:

Lakewood Station District Subarea Plan, Form-Based Code, and Planned Action

2. Name of applicant:

City of Lakewood

3. Address and phone number of applicant and contact person:

Tiffany Speir, Long Range & Strategic Planning Manager
Community Development Department
City of Lakewood
City Hall, 6000 Main St. SW
Lakewood, WA 98499
tspeir@cityoflakewood.us
253-983-7702

4. Date checklist prepared:

July 27, 2020; updated March 25, 2021

5. Agency requesting checklist:

City of Lakewood

6. Proposed timing or schedule (including phasing, if applicable):

Adoption Spring 2021

7. Do you have any plans for future additions, expansion, or further activity related to or connected with this proposal? If yes, explain.

Future site specific development is anticipated under the nonproject subarea plan and development regulations.

8. List any environmental information you know about that has been prepared, or will be prepared, directly related to this proposal.

Comprehensive Plan EIS, June 2000. The EIS contained a preferred alternative and two other alternatives, including a no action alternative and mixed-use alternative. The principal strategy of the preferred alternative was to: Protect established neighborhoods; Develop intensification within the city's spine, which stretched north along Bridgeport Way from the Lakewood Station, past the Town Center and the Colonial Center, through to the Custer Road neighborhood; Focused residential density in several neighborhoods, notably Springbrook, Tillicum, and Custer; and Increasing the employment base by converting parts of the Woodbrook Neighborhood into an industrial center. The preferred alternative provided 'development capacity' from an estimated 17,500 new residents and 12,275 new jobs.

Comprehensive Plan Supplemental EIS, 2003. There were 10 comprehensive plan amendments proposed in 2003 that would collectively redesignate numerous sections of the City of Lakewood from their existing land use and zoning designations to new designations. The majority of these amendments were relatively minor, parcel-specific

inconsistencies between the adopted future land use plan and existing or intended land uses. However, one amendment along Bridgeport Way, north of 75th Street SW, reduced high-density residential development in favor of commercial development (Wal-Mart).

Final Lakewood Downtown Subarea Plan and Draft Planned Action EIS, July 2018. The following proposals were evaluated for the Downtown to help support desired change and development: Subarea Plan, Comprehensive Plan land use, policy, and capital facility plan amendments, new hybrid form-based zoning standards, and upfront environmental review through a Planned Action consistent with RCW 43.21 c.440 and SEPA rules in WAC 197-11.

Lakewood Station District Subarea Situation Assessment – April 2020. Provides current conditions and trends across natural and built environment topics.¹

Lakewood Station District Subarea Transportation Technical Memo – October 2020. Provides transportation evaluation of the No Action and Planned Action Alternatives. See Appendix.

9. Do you know whether applications are pending for governmental approvals of other proposals directly affecting the property covered by your proposal? If yes, explain.

A portion of the study area is under consideration for a master planned mixed use development called the Lakewood Landing.

10. List any government approvals or permits that will be needed for your proposal, if known.

The non-project action requires review and recommendations by the Lakewood Planning Commission and review and approval by the Lakewood City Council. It has undergone a 60-day review by state agencies led by the Washington Department of Commerce. It has been shared with regional agencies like Puget Sound Regional Council and Pierce County as well.

11. Give brief, complete description of your proposal, including the proposed uses and the size of the project and site. There are several questions later in this checklist that ask you to describe certain aspects of your proposal. You do not need to repeat those answers on this page. (Lead agencies may modify this form to include additional specific information on project description.)

Comparison of Alternatives

Two alternatives are considered in this expanded checklist: the No Action Alternative that maintains current plans and regulations and a Planned Action. The Planned Action would expand housing and job opportunities.

The study area is an opportunity for future growth and development within Lakewood. First, there are several neighborhoods within a half mile of the Lakewood Station with a mix of multi-family and mixed residential zones that are developed at densities lower than planned or zoned. Second, with its proximity to I-5 and the Sounder regional commuter rail, the study area is a good location for employment. Third, since there are few environmental constraints, this is an ideal place to explore housing types and densities that take advantage of the proximity to high capacity transit. Fourth, the Town Center District is also only about a mile away (Exhibit 4) so there is also an opportunity to deepen connections between the two subareas.

Under the Planned Action, the City of Lakewood proposes to adopt a subarea plan for the Lakewood Station District, as well as a form-based code. As allowed under RCW 43.21.c.440, the City is developing a planned

¹ This report is available in City Council packet: <https://cityoflakewood.us/wp-content/uploads/2020/08/2020-08-10-Council-Agenda.pdf>.

action using a threshold determination process.

Following are key elements of the current plan (No Action proposal) and the Planned Action proposal.

Exhibit 1. No Action and Proposed Action Comparison

Element	No Action	Proposed Action
Land Use/Zoning	<ul style="list-style-type: none"> ▪ Existing Comprehensive Plan <ul style="list-style-type: none"> ○ Corridor Commercial ○ High Density Multi-Family ○ Neighborhood Business District ○ Open Space & Recreation ○ Public Semi-Public Institutional ▪ Existing Zoning <ul style="list-style-type: none"> ○ Commercial: C1, C2, NC2, TOC ○ MF3 ○ OSR2 ○ PI ▪ Density Range: 35 du/ac NC2 and 54 du/ac MF3 and TOC 	<ul style="list-style-type: none"> ▪ Retain Comprehensive Plan and Zoning designations, except: <ul style="list-style-type: none"> ○ Update Open Space & Recreation designation/zone. ▪ Zoning allowances: <ul style="list-style-type: none"> ○ Create new chapter in Title 18 for hybrid form-based code. Will serve functionally as an overlay zone. Address building and street typologies similar to Downtown, adapted for study area. ○ Update MF3 zone regarding incentives and standards for rowhouses/townhouses. ○ Consider Live/Work opportunities within areas where mixed use and townhomes are desired. Consider implications for home occupation types. ○ Promote Townhouses and Rowhouses at 30-40 du/ac. In form-based code, address 30 foot height and appropriate setbacks. Address minimum lot width and size allowing for townhomes and rowhouses. ▪ Land Use Assumptions: <ul style="list-style-type: none"> ○ Integrate Lakewood Landing. ○ Density Range: 35 du/ac NC2; 30-40 du/ac MF3 in study area; 54 du/ac TOC. Mixed use in proximity to station in MF3 as allowed currently.
Growth Capacity	<ul style="list-style-type: none"> ▪ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and 	<ul style="list-style-type: none"> ▪ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings.

Element	No Action	Proposed Action
	vacant lands shows a potential for up to 1,343 new dwelling units.	<ul style="list-style-type: none"> Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs. Later phases include office jobs.
Growth Assumptions	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Traffic Model: 962 Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> Transportation Model 2010-2035: <ul style="list-style-type: none"> Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ Jobs: $1,105 + 171$ Lakewood Landing retail jobs = 1,276
Housing	Zoning allows a variety of attached housing types up to 54 du/ac in TOC and MF 3 and 35 du/ac in NC 2.	<ul style="list-style-type: none"> Observed density in MF3 is nearly 12 units per acre. If redevelopable sites achieved 35 units per acre, the density would increase to about 26 units per acre zone wide or 65% of a maximum 40 units per acre. Focus on missing middle ownership: 65%-110% of area median income. Promote Townhouses and Rowhouses at 30-40 du/ac. Allow for/attract small builders as well as larger builders. Consider property assembly in area (10 acres brick & masonry plant). Opportunities for mixed use and open space.
Jobs	Service and retail are dominant sectors.	<ul style="list-style-type: none"> Encourage updated retail. Such as through façade improvement programs with existing properties and allowances for new commercial and mixed uses.
Parks and Open Space	Green streets, plazas and pocket parks, and oak tree preservation are highlights in the current Comprehensive Plan.	<ul style="list-style-type: none"> Address Open Space & Recreation designation/zone. Integrate Open Space into Larger Mixed Use Developments. Lakewood Landing: Courtyard Green streets and Alley Plaza near Occidental, within current rights of way

Element	No Action	Proposed Action
Transportation	<ul style="list-style-type: none"> ▪ Served by Pierce Transit, Intercity Transit, and Sound Transit <ul style="list-style-type: none"> ○ Souder South Capacity Expansion ○ Sound Transit Access Improvements ○ Pierce Transit Long Range Plan ▪ Limited pedestrian and bike facilities to be improved – consistent with Comp Plan Transportation Element <ul style="list-style-type: none"> ○ Improvements Sidewalk Improvements – Kendrick Drive from 111th St to 108th St SW ○ Non-Motorized Access Improvements from 2009 NMTP and ST Access Improvement Study ○ Bridgeport Way Sidewalks ○ 111th/112th St SW Sidewalk Gap ○ 111th St SW Bike Lanes ○ Real-time parking availability signage ○ Bike Parking (short and long-term) consistent with TIP 	<p>Similar improvements as No Action plus:</p> <p>Streetscape Typologies</p> <ul style="list-style-type: none"> ▪ Allocate excess right-of-way for non-motorized uses ▪ Prioritize space for active modes over on-street parking ▪ Enhanced crossings <p>Connections</p> <ul style="list-style-type: none"> ▪ Bridge across I-5 (47th Avenue SW) <p>Other</p> <ul style="list-style-type: none"> ▪ Consider street vacation (depending on citywide policy review)

Lakewood Landing

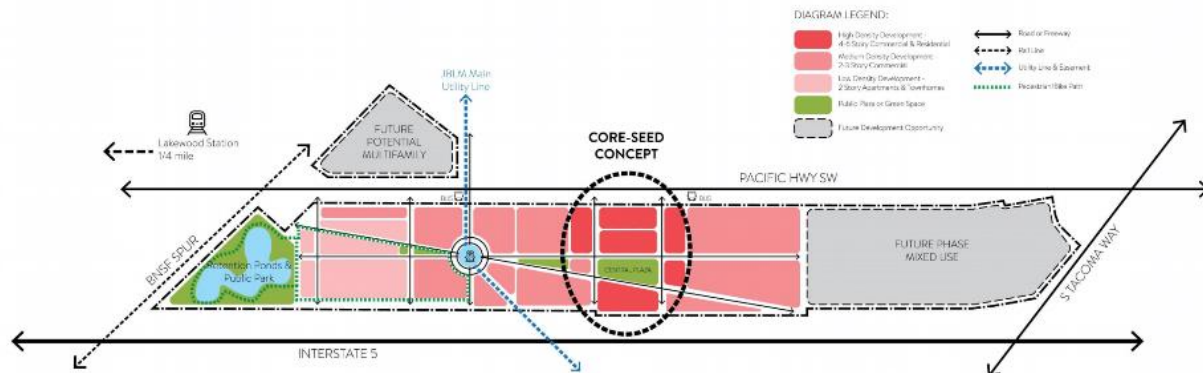
Lakewood Landing is a 30-acre site poised for mixed use and employment development. The site is adjacent to Sound Transit commuter rail station and between Pacific Highway and I-5. An initial mixed use plan with 760 units and 87,000 square feet of retail are part of Phase 1 in the Central Core. A Northwest concept, an aviation themed restaurant overlooking the McChord runway, coffee shops, a craft beer tap room, boutique shops, restaurants, and an interpretive educational park are all part of the long-term vision. Water features, carefully placed structures, and gabion walls will help to mitigate noise.

Phase I, Central Core

- 11.5 acres
- 5 over 2 construction and one level of underground parking
- 760 residential units
- 87,000 square feet of retail (about 171 employees)
- 1,474 parking spaces, including a parking field

Future phases are under consideration.

Exhibit 2. Lakewood Landing Location and Concept



Source: City of Lakewood 2020.

12. Location of the proposal. Give sufficient information for a person to understand the precise location of your proposed project, including a street address, if any, and section, township, and range, if known. If a proposal would occur over a range of area, provide the range or boundaries of the site(s). Provide a legal description, site plan, vicinity map, and topographic map, if reasonably available. While you should submit any plans required by the agency, you are not required to duplicate maps or detailed plans submitted with any permit applications related to this checklist.

The proposed LSDS subarea boundary encompasses 231.1 parcel acres, and 329.5 gross acres; see Exhibit 3. Generally, the boundary incorporates the area within a half mile of the Sounder station and associated Sound

Transit property but does not include areas southeast of I-5, since the freeway provides a significant barrier. The study area will also have added bus rapid transit serving its northern extent through Pierce Transit’s planned bus rapid transit route 2 in the Pierce Transit Destination 2040 anticipated to be operational in 2026.

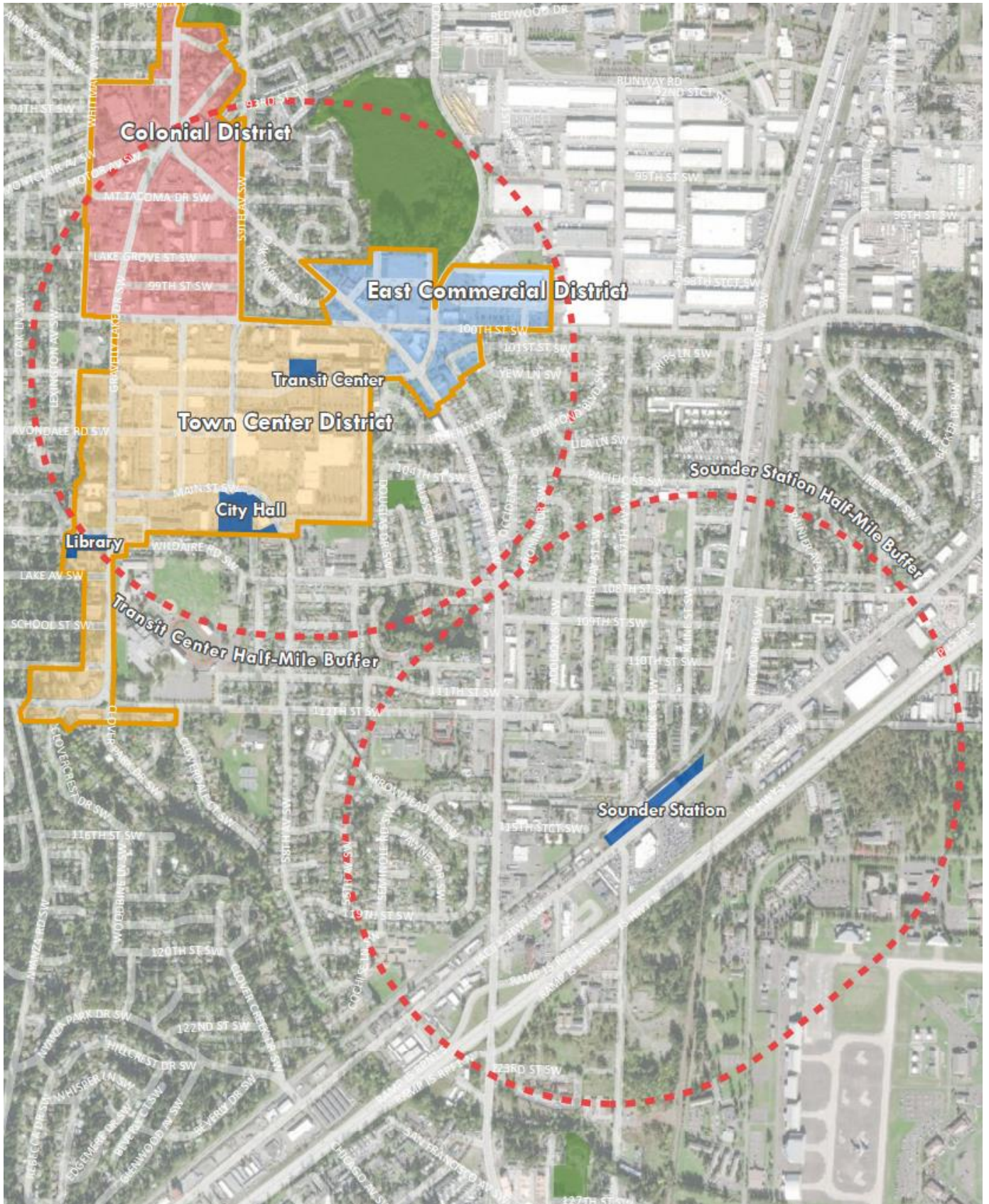
The subarea spans the area from the interchange with Bridgeport Way to the interchange with SR-512, to capture the full transportation corridor to the north and south of the station. In order to analyze transportation connections to Lakewood’s Downtown, the transportation evaluation reviews an extended area north and west of the primary subarea (Exhibit 4). The extended study area looks at ways to enhance multi-modal transportation linkages. The extended study area does not address land use and housing beyond the LSDS.

Exhibit 3. Lakewood Station District Study Area



Source: BERK, 2020; Pierce County Assessor, 2020.

Exhibit 4. Lakewood Station in Proximity to Downtown



Source: BERK, 2020; Pierce County Assessor, 2020.

B. Environmental Elements

1. EARTH

a. General description of the site:

(circle one): Flat, rolling, hilly, steep slopes, mountainous, other _____

The study area is generally flat.

b. What is the steepest slope on the site (approximate percent slope)?

Slopes are 0 to 6 percent, with a representative slope of 3%.

c. What general types of soils are found on the site (for example, clay, sand, gravel, peat, muck)? If you know the classification of agricultural soils, specify them and note any agricultural land of long-term commercial significance and whether the proposal results in removing any of these soils.

The study area is entirely mapped as 41A—Spanaway gravelly sandy loam. Properties include:

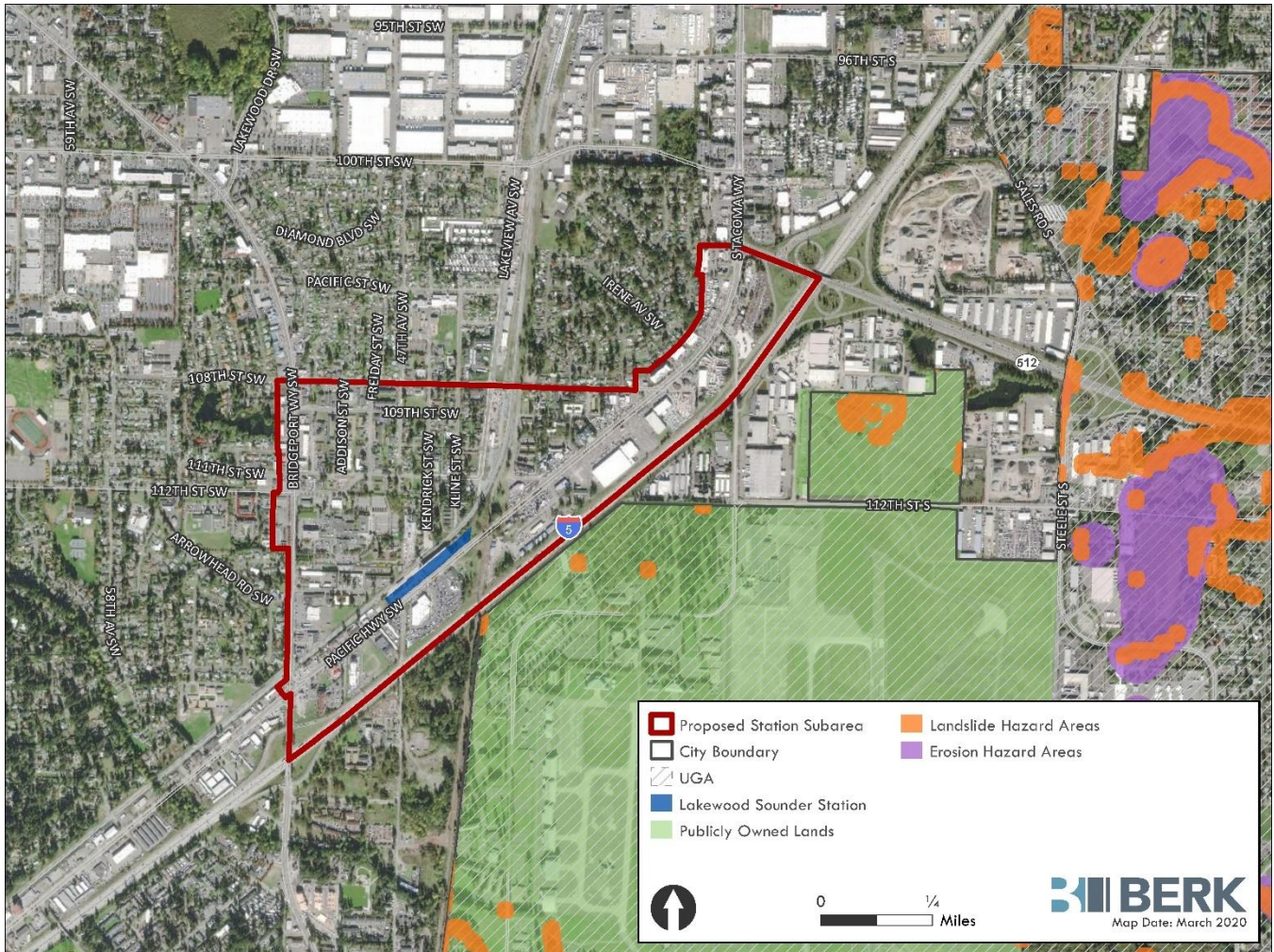
- Landform: Outwash plains
- Slope: 0 to 6%
- Depth to water table: More than 80 inches
- Hydric soil rating: No

Soil Survey Area: Pierce County Area, Washington, Survey Area Data: Version 16, Jun 4, 2020.

d. Are there surface indications or history of unstable soils in the immediate vicinity? If so, describe.

No. The City map of geologic hazards do not show liquefaction, erosion, or landslide hazard areas. See Exhibit 5.

Exhibit 5. Geologic Hazards



Source: City of Lakewood, BERK 2020.

e. Describe the purpose, type, total area, and approximate quantities and total affected area of any filling, excavation, and grading proposed. Indicate source of fill.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading.

f. Could erosion occur as a result of clearing, construction, or use? If so, generally describe.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and during construction expose soils to erosion.

g. About what percent of the site will be covered with impervious surfaces after project construction (for example, asphalt or buildings)?

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

h. Proposed measures to reduce or control erosion, or other impacts to the earth, if any:

LMC Chapter 12.10 provides regulations addressing: storm drainage, earthwork (excavation and fill), soil reports, and erosion controls.

2. AIR

a. What types of emissions to the air would result from the proposal during construction, operation, and maintenance when the project is completed? If any, generally describe and give approximate quantities if known.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites. During construction there is a potential for dust. During operation there is potential for added emissions due to vehicular travel generated by the new growth. The Planned Action would increase dwellings and jobs in the study area and have a greater local potential for air emissions. Regionally, focusing growth in and near multimodal transportation opportunities could reduce vehicle miles travelled and associated air emissions compared to development on the fringe of the urban area. (Urban Land Institute, 2010)

Puget Sound Regional Council (PSRC) has identified that its 2019-2022 transportation improvement program. The Regional TIP includes continued implementation and expansion of commuter rail service between Seattle and Lakewood, and resurfacing of Pacific Hwy and South Tacoma Way between 108th ST SW and SR 512. The Air Quality Conformity Analysis indicates that the emissions from the projects and programs in the plan are below the established daily motor vehicle emissions budgets for PM2.5 and NOX. (Puget Sound Regional Council, 2018)

b. Are there any off-site sources of emissions or odor that may affect your proposal? If so, generally describe.

The study area is located within a PM2.5 Maintenance Area, and in a former CO and Ozone maintenance area. I-5 forms the eastern border of the study area and results in air emissions to existing and future development.

c. Proposed measures to reduce or control emissions or other impacts to air, if any:

Providing for more mixed use, residential, and job opportunities in proximity to the in proximity to the Sounder Station and travel by transit is likely by a portion of residents and employees.

Application of the State Energy Code can improve energy efficiency and reduce greenhouse gas emissions. (LMC Chapter 15.05) Over time new standards will also be implemented: The 2019 Legislature adopted new standards that will increase the efficiency of these buildings and reduce emissions. The Legislature also increased efficiency standards for appliances. (Washington State Department of Ecology, 2019)

Dense landscaping along roadways can reduce air pollutants by up to 50% (Deshmukh, 2019) Green infrastructure is another source of potential air emission mitigation at a microscale (Tiwari, 2019). As part of the Subarea Plan update associated with the Planned Action, the City can promote landscaping and green infrastructure. It can also address orientation and location of residential uses in mixed use developments to reduce the potential for localized air quality effects and improve compatibility; for example residential development can be focused away from I-5 and business uses can buffer residential uses near the freeway.

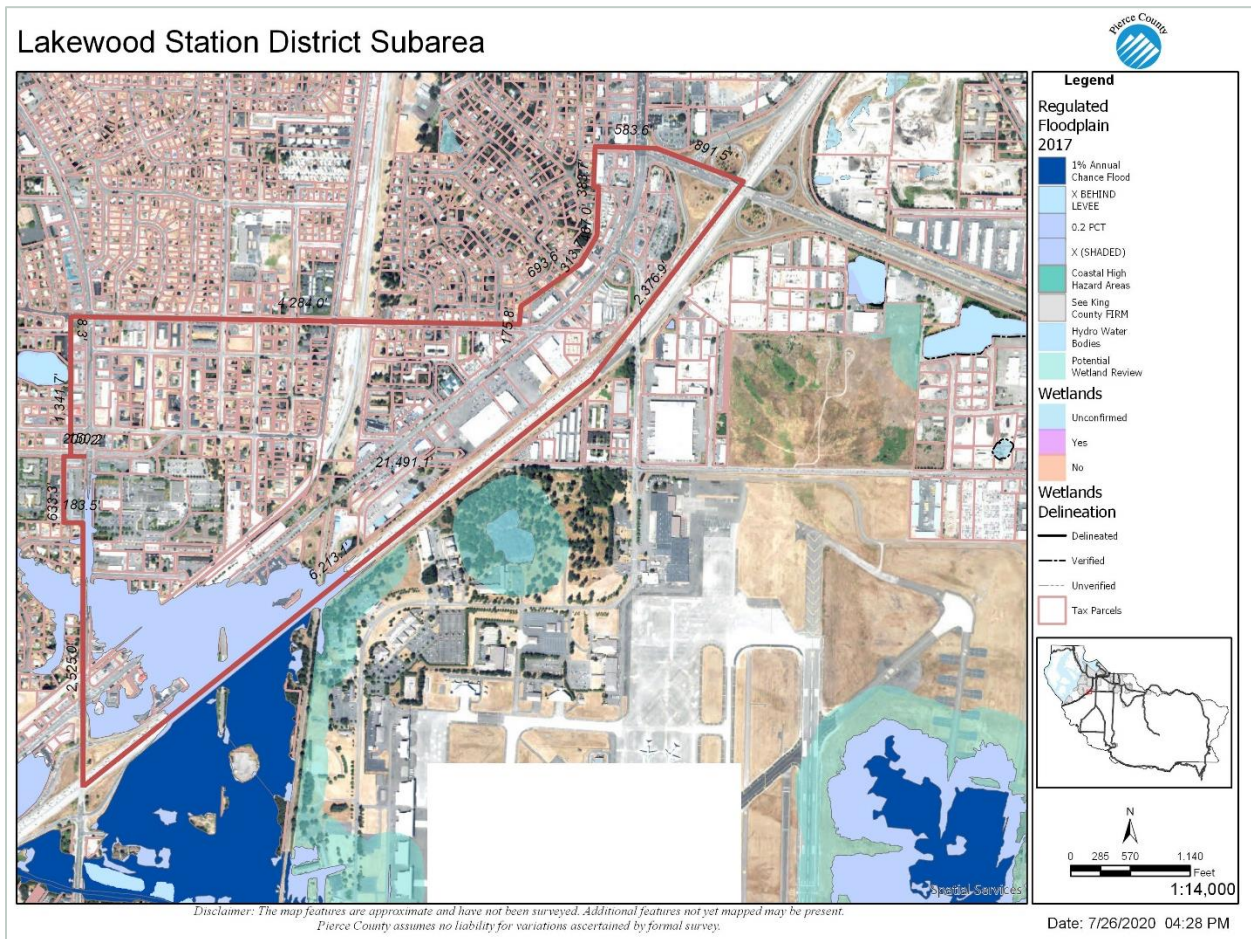
3. WATER

a. Surface Water:

1) Is there any surface water body on or in the immediate vicinity of the site (including year-round and seasonal streams, saltwater, lakes, ponds, wetlands)? If yes, describe type and provide names. If appropriate, state what stream or river it flows into.

Exhibit 6 shows the hydrology of the area. There are no significant hydrological features within the subarea. Outside and abutting the northwest corner of the subarea is Hidden Lake, which is a delineated wetland just west of Bridgeport Way. Critical area buffer requirements may affect a limited number of parcels in the subarea west of Bridgeport. Along small lakes, regulated activities are subject to a 35-foot buffer requirement. (LMC 14.154.060) Wetland protection requirements depend on habitat quality and wetland categories, and buffers could range from 40 to 225 feet.

Exhibit 6. Hydrology in the LDS and Surrounding Area



Source: Pierce County PublicGIS, 2020.

2) Will the project require any work over, in, or adjacent to (within 200 feet) the described waters? If yes, please describe and attach available plans.

Redevelopment may occur on sites near Hidden Lake but would be subject to critical area buffers.

3) Estimate the amount of fill and dredge material that would be placed in or removed from surface water or wetlands and indicate the area of the site that would be affected. Indicate the source of fill material.

Future site-specific development under either the No Action or Planned Action Alternatives may redevelop sites and require fill, excavation, or grading though none of these activities are anticipated in a wetland given lack of presence. Fill of wetlands is not proposed as part of the legislative actions associated with the subarea plan and form-based code. Any activities affecting wetland buffers would be subject to Lakewood critical area regulations.

4) Will the proposal require surface water withdrawals or diversions? Give general description, purpose, and approximate quantities if known.

No surface water withdrawals or diversions are anticipated. A public water system supplies the areas. Creeks are not located in the study area.

5) Does the proposal lie within a 100-year floodplain? If so, note location on the site plan.

See Exhibit 6. Zone X (shaded) is considered a moderate flood hazard area and is the area between the limits of the base flood and the 0.2-percent-annual-chance (or 500-year) flood. It is mapped along Bridgeport Way and between Pacific Highway and I-5 west of Lakeview Avenue SW. These are not regulated by the City's flood hazard regulations, which apply to special flood hazard areas, defined as the area that will be inundated by the flood event having a 1-percent chance of being equaled or exceeded in any given year.

6) Does the proposal involve any discharges of waste materials to surface waters? If so, describe the type of waste and anticipated volume of discharge.

No point discharges of waste material to surface waters are anticipated. All development will be subject to the City's storm water management regulations that address nonpoint runoff. (LMC Chapter 12.11)

b. Ground Water:

1) Will groundwater be withdrawn from a well for drinking water or other purposes? If so, give a general description of the well, proposed uses and approximate quantities withdrawn from the well. Will water be discharged to groundwater? Give general description, purpose, and approximate quantities if known.

No wells are proposed. The area is served by the Lakewood Water District.

2) Describe waste material that will be discharged into the ground from septic tanks or other sources, if any (for example: Domestic sewage; industrial, containing the following chemicals. . . ; agricultural; etc.). Describe the general size of the system, the number of such systems, the number of houses to be served (if applicable), or the number of animals or humans the system(s) are expected to serve.

No septic systems are proposed. The area is served by a public sewer system operated by Pierce County Planning and Public Works Sewer Utility.

c. Water runoff (including stormwater):

1) Describe the source of runoff (including storm water) and method of collection and disposal, if any (include quantities, if known). Where will this water flow? Will this water flow into other waters? If so, describe.

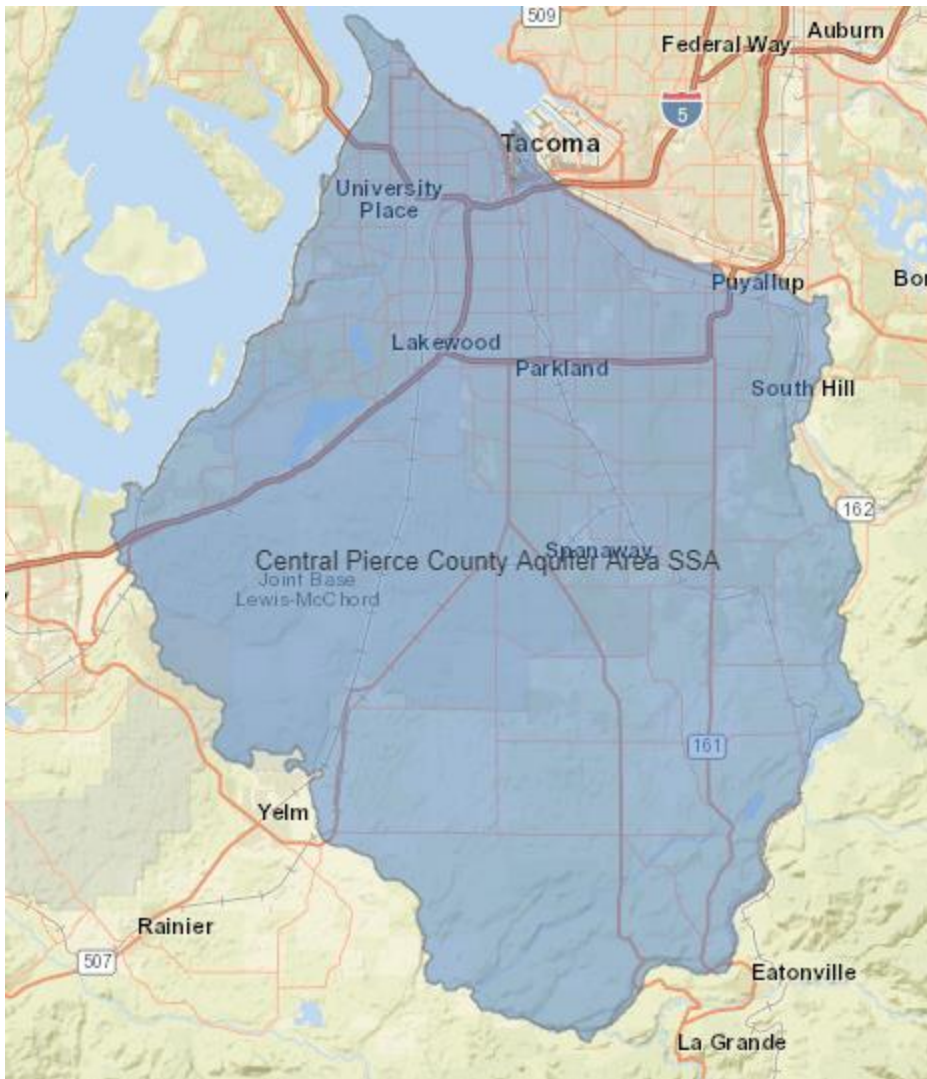
Stormwater runoff may occur though water quality and quantity is regulated, and should improve as sites developed under older regulations redevelop with newer regulations. Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11) The City

applies the Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual.

2) Could waste materials enter ground or surface waters? If so, generally describe.

The City limits land uses that could result in groundwater contamination such as storage tanks, auto repair and auto washing facilities, dry cleaners, and others within the Clover/Chambers Creek Aquifer Basin boundary that encompasses the entire study area and beyond throughout Lakewood, Tacoma, and Central Pierce County. (LMC Chapter 14.150) See Exhibit 7. See C.1 regarding stormwater management.

Exhibit 7. Sole Source Aquifer in Central Pierce County



Source: USEPA 2020.

3) Does the proposal alter or otherwise affect drainage patterns in the vicinity of the site? If so, describe.

The applicable zones allow for about 70% impervious surfaces (MF3) to 100% impervious surfaces (NC2, TOC, C1 and C2). (LMC 18A.60.030 and 18A.60.040) Future site-specific development under either the No Action or Planned Action Alternatives may replace or increase impervious surfaces consistent with zoning.

However, the Washington State Department of Ecology Stormwater Manual requires water quality treatment, including use of low impact development methods wherever feasible. (LMC Chapter 12.11)

d. Proposed measures to reduce or control surface, ground, and runoff water, and drainage pattern impacts, if any:

Application of Washington State Department of Ecology Stormwater Manual and its Engineering Standards Manual at LMC Chapter 12.11.

Stormwater management will be required of all new development whether under the No Action or Planned Action Alternatives. (LMC Chapter 12.11)

The Subarea Plan and Form-Based Code proposed by the Planned Action includes alternative street cross sections and frontage improvements that would create an opportunity for added green infrastructure and stormwater management.

4. PLANTS

a. Check the types of vegetation found on the site:

The area is developed and has ornamental landscaping.

deciduous tree: alder, maple, aspen, other

evergreen tree: fir, cedar, pine, other

shrubs

grass

pasture

crop or grain

Orchards, vineyards or other permanent crops.

wet soil plants: cattail, buttercup, bullrush, skunk cabbage, other

water plants: water lily, eelgrass, milfoil, other

other types of vegetation

b. What kind and amount of vegetation will be removed or altered?

Ornamental vegetation could be removed. However, landscaping will be required on sites.

c. List threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped in the study area. Hidden Lake abutting the study area to the west is a mapped aquatic habitat/wetlands under the State's Priority Habitats and Species program. (Washington State Department of Fish and Wildlife, 2020) It is protected by buffers. See Section B.3 above.

d. Proposed landscaping, use of native plants, or other measures to preserve or enhance vegetation on the site, if any:

Future development would implement LMC Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. When redevelopment would occur, perimeter landscaping and parking area landscaping would be

required. Additionally significant trees would be retained where they exist.

The Planned Action Alternative would improve design standards applicable to the study area and provide standards for the public realm including street trees and green infrastructure.

e. List all noxious weeds and invasive species known to be on or near the site.

Noxious weeds are not mapped in detail, but along I-5 generally it is identified as an area with Spotted Knapweed. (Pierce County Weed Board, 2019) Noxious weeds are regulated by: RCW Chapter 17.10, and WAC Chapter 16-750.

5. ANIMALS

a. List any birds and other animals which have been observed on or near the site or are known to be on or near the site.

Examples include:

✓birds: hawk, heron, eagle, songbirds, other:

✓mammals: deer, bear, elk, beaver, other: urban adapted wildlife

___fish: bass, salmon, trout, herring, shellfish, other _____

b. List any threatened and endangered species known to be on or near the site.

No threatened and endangered species are mapped near the study area. (Washington State Department of Fish and Wildlife, 2020)

c. Is the site part of a migration route? If so, explain.

All of Washington State is included in the Pacific Flyway that is traversed by migratory birds. (US Fish and Wildlife Service, 2020)

d. Proposed measures to preserve or enhance wildlife, if any:

The City maintains critical area regulations protecting fish and wildlife habitat conservation areas. Though none are mapped in the study area, if some were identified they would be protected under either the No Action or Planned Action Alternatives. (LMC Chapter 14.154)

e. List any invasive animal species known to be on or near the site.

No known invasive animal species have been identified in the study area.

6. ENERGY AND NATURAL RESOURCES

a. What kinds of energy (electric, natural gas, oil, wood stove, solar) will be used to meet the completed project's energy needs? Describe whether it will be used for heating, manufacturing, etc.

Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service. Power and natural gas are available and are anticipated to serve future growth under either the No Action or Planned Action Alternatives.

b. Would your project affect the potential use of solar energy by adjacent properties? If so, generally describe.

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones and should allow for solar energy use by adjacent uses (see LMC 18A.60.100). With the Form Based Code, heights would vary based on building type, and no building typologies would exceed current zoning and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories. Pedestrian areas in commercial zones must be designed with solar exposure in mind. (LMC 18A.70.040)

c. What kinds of energy conservation features are included in the plans of this proposal? List other proposed measures to reduce or control energy impacts, if any:

All future development would be subject to the State Energy Code. (LMC Chapter 15.05)

7. ENVIRONMENTAL HEALTH

a. Are there any environmental health hazards, including exposure to toxic chemicals, risk of fire and explosion, spill, or hazardous waste, that could occur as a result of this proposal? If so, describe.

1) Describe any known or possible contamination at the site from present or past uses.

Exhibit 8 shows sites of known contamination in the LSDS. Apart from a dry-cleaning site located north of Pacific Highway and south of St. Clare Hospital, the sites are located between Pacific Highway and I-5. Contamination from petroleum products, metals, solvents, and lead is the result of current or past activities on these sites.² Clean-up is started on all the sites except the Flying B #18 site. Sites are in various stages of the clean-up process and subject to Washington State's Model Toxics Control Act.³

² Specific information on each site and the clean-up efforts that are underway can be found at: <https://ecology.wa.gov/Spills-Cleanup/Contamination-cleanup/Cleanup-sites/Locate-contaminated-sites>.

³ RCW 70.105D.

Exhibit 8. Environmental Clean-up Sites in the LDS and Surrounding Area



Source: BERK, 2020; Pierce County Assessor, 2020.

2) Describe existing hazardous chemicals/conditions that might affect project development and design. This includes underground hazardous liquid and gas transmission pipelines located within the project area and in the vicinity.

The presence of I-5 is also a potential environmental health risk for those living in the subarea according to the Centers for Disease Control and Prevention.⁴ Those living within about 500 feet of a major highway have increased risk for exposure to carbon monoxide, nitrogen dioxide, ozone, and particulates. There are a variety of potential health impacts including increased incidences of asthma and lung disease, cardiovascular disease, cancer, and adverse birth outcomes. Children, older adults, households with low incomes, and those with underlying health conditions may be at higher risk for impacts.

⁴ <https://ephtracking.cdc.gov/showProximityToHighways.action>

3) Describe any toxic or hazardous chemicals that might be stored, used, or produced during the project's development or construction, or at any time during the operating life of the project.

Local businesses such as auto sales or service operations may store toxic or hazardous chemicals but are regulated by local development regulations and state laws.

4) Describe special emergency services that might be required.

No special emergency services are anticipated. Commercial businesses would require City review of building and fire codes, and inspections by West Pierce Fire and Rescue.

5) Proposed measures to reduce or control environmental health hazards, if any:

The State Model Toxics Control Act (MTCA) sets standards for cleanup of lower levels of contaminants that are incorporated into new development and redevelopment parcels noted to have contamination potential. The City applies relevant standards regarding hazardous materials handling in the International Fire Code and Zoning Codes. Compliance with the Lakewood Municipal Code including aquifer protection regulations also addresses hazardous materials storage and handling.

It is recommended that the Planned Action Ordinance incorporate the following mitigation measure:

- The potential for airborne toxics from high volume roads like I-5 can be mitigated through site and building design and by increasing access to transit and non-motorized transportation options. Residential uses could be located 500 feet from the centerline of I-5 at a minimum.
- Applicants for development shall conduct a site assessment to determine if contamination is present from past use.

b. Noise

1) What types of noise exist in the area which may affect your project (for example: traffic, equipment, operation, other)?

I-5 is a primary source of traffic noise, and to a lesser extent traffic on Pacific Highway. Sounder Station operations also contribute to transportation noise sources. JBLM military operations result in aircraft noise.

2) What types and levels of noise would be created by or associated with the project on a short-term or a long-term basis (for example: traffic, construction, operation, other)? Indicate what hours noise would come from the site.

New residential and employment growth could contribute traffic trips and add cumulatively to noise. However, locating new housing and jobs in proximity to the Sounder Station could allow for more transit usage.

Land use could locate in proximity to noise sources, including residential uses, exposing population to noise.

3) Proposed measures to reduce or control noise impacts, if any:

LMC Chapter 8.36 Noise Control applies to nuisances under both studied alternatives.

Under the Planned Action, the following are proposed:

- Limiting residential uses within 500 feet of I-5 centerline to address air quality concerns could also be beneficial for noise reduction.

- Within Lakewood Landing, measures to reduce noise include: water features, carefully placed structures, and gabion walls.

8. LAND AND SHORELINE USE

a. What is the current use of the site and adjacent properties? Will the proposal affect current land uses on nearby or adjacent properties? If so, describe.

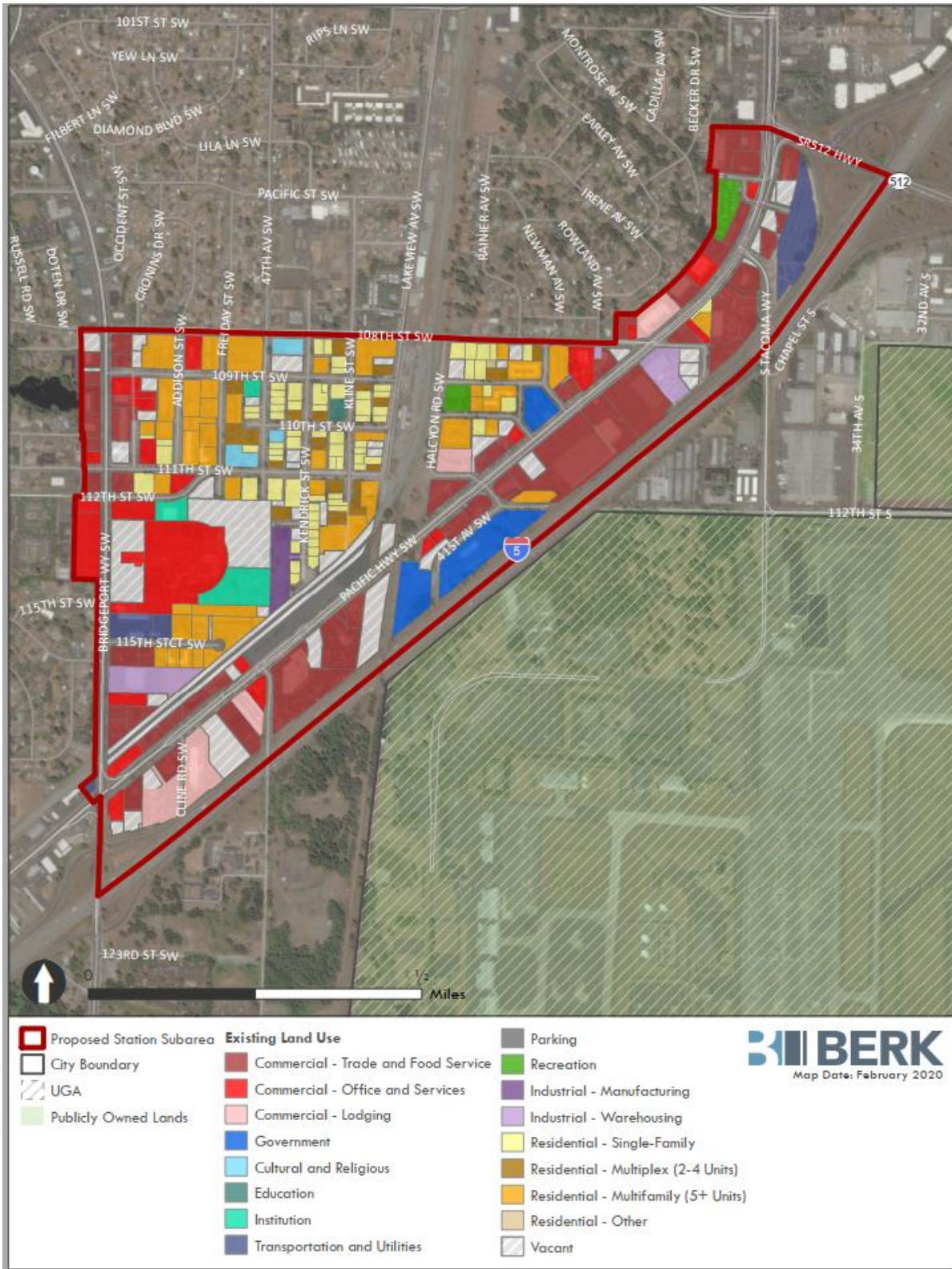
Most of the study area is in commercial use, followed by multifamily and single family residential combined, and vacant lands. See Exhibit 9 and Exhibit 10.

Exhibit 9. Current Land Use and Acres

Category	Acres	Share
Commercial - Food Service	8.7	4%
Commercial - Lodging	9.0	4%
Commercial - Office	4.5	2%
Commercial - Retail/Wholesale Trade	45.5	22%
Commercial - Service	10.5	5%
Cultural and Religious	1.9	1%
Education	0.5	0%
Government	9.0	4%
Industrial - Manufacturing	2.5	1%
Industrial - Warehousing	5.7	3%
Institution	13.9	7%
Parking	3.5	2%
Recreation*	2.7	1%
Residential - Multifamily (5+ Units)	29.4	14%
Residential - Multiplex (2-4 Units)	5.3	3%
Residential - Other	0.1	<1%
Residential - Single-Family	15.6	7%
Transportation and Utilities*	6.9	3%
Vacant	35.7	17%
Grand Total	211.0	100%

This includes a RV park at Halcyon that could be reclassified as multifamily. At 109th is Water District Well and should be Utility though it provides open space values.

Exhibit 10. Existing Land Use (Draft)



Source: BERK, 2020; Pierce County Assessor, 2020.

b. Has the project site been used as working farmlands or working forest lands? If so, describe. How much agricultural or forest land of long-term commercial significance will be converted to other uses as a result of the proposal, if any? If resource lands have not been designated, how many acres in farmland or forest land tax status will be converted to nonfarm or nonforest use?

The site is urban and not used for agriculture. The area is not designated as a resource land of long-term significance.

1) Will the proposal affect or be affected by surrounding working farm or forest land normal business operations, such as oversize equipment access, the application of pesticides, tilling, and harvesting? If so, how:

Not applicable. The study area is in the city limits and contains and is surrounded by urban lands.

c. Describe any structures on the site.

The sites contain residential and commercial structures. Commercial structures apart from the hospital and lodging are generally one or two stories in height. The hospital is 2-3 stories in height. The Candlewood Hotel is 3 stories. The Sounder Station Garage extends 4 stories. Residential structures vary in height but are generally less than three stories. See Exhibit 11.

Exhibit 11. Description of Residential Structures in Study Area

Type of Residential Structures	Sum of Units
1 1/2 Story Fin	14
1 Story	68
2 Story	4
Addon Only Res	0
Apartment <= 3 Stories	20
Detached Garage	0
Duplex 1 1/2 Story	2
Duplex One Story	20
Duplex Two Story	10
Home For the Elderly	5
Mixed Retail w/ Res Units	15
Multiple - Residential	662
Triplex One Story	3
Triplex Two Story	3
RV Park: 1 Apartment, 11 Mobile Homes & 26 RV Spaces	38
Grand Total	864

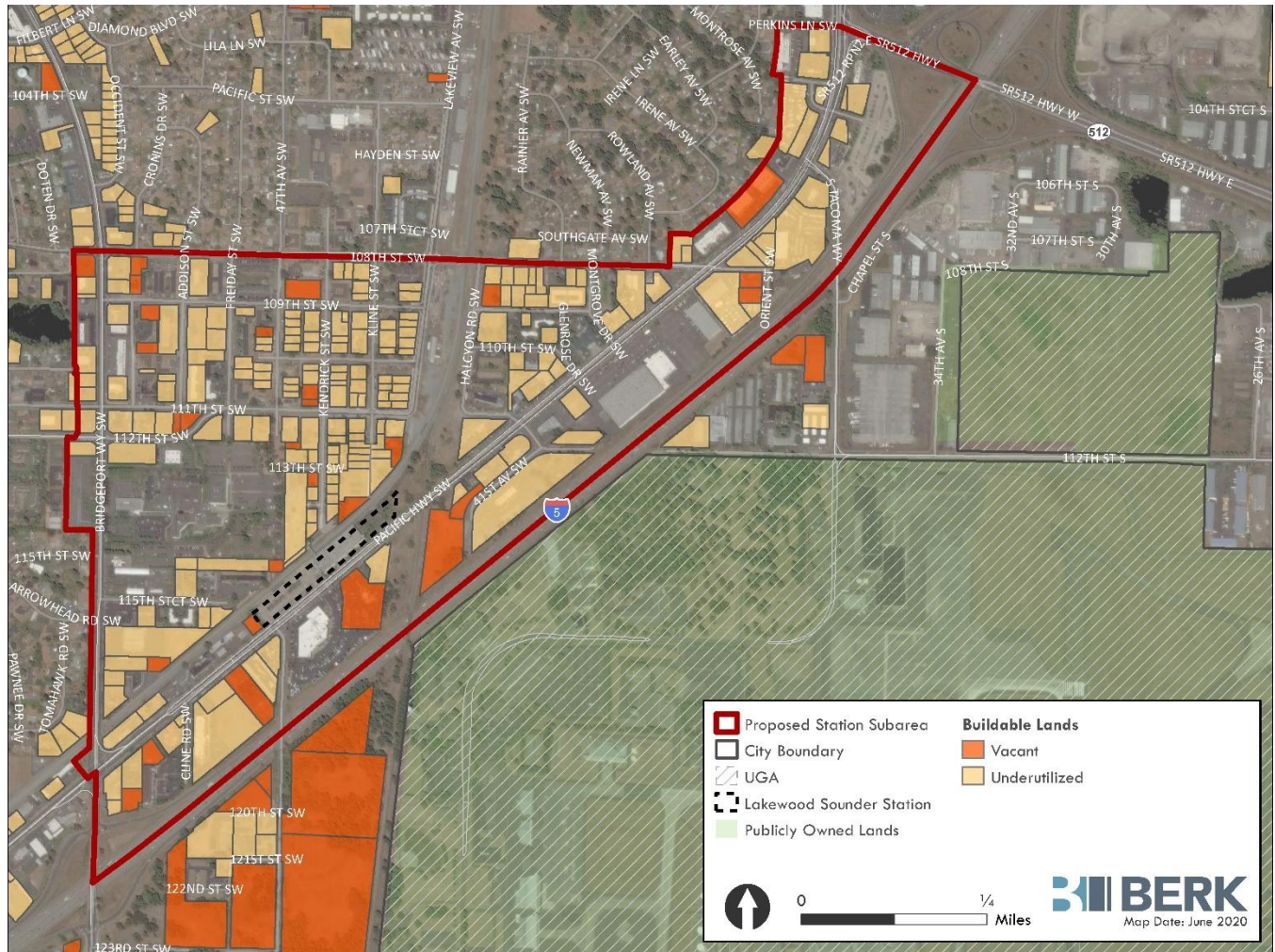
Source: Pierce County Assessor, BERK 2020.

d. Will any structures be demolished? If so, what?

Structures may be demolished and replaced by new residential and commercial development.

Sites most likely to change were identified by a 2014 Buildable Lands Report prepared by Pierce County in conjunction with cities. See Exhibit 12.

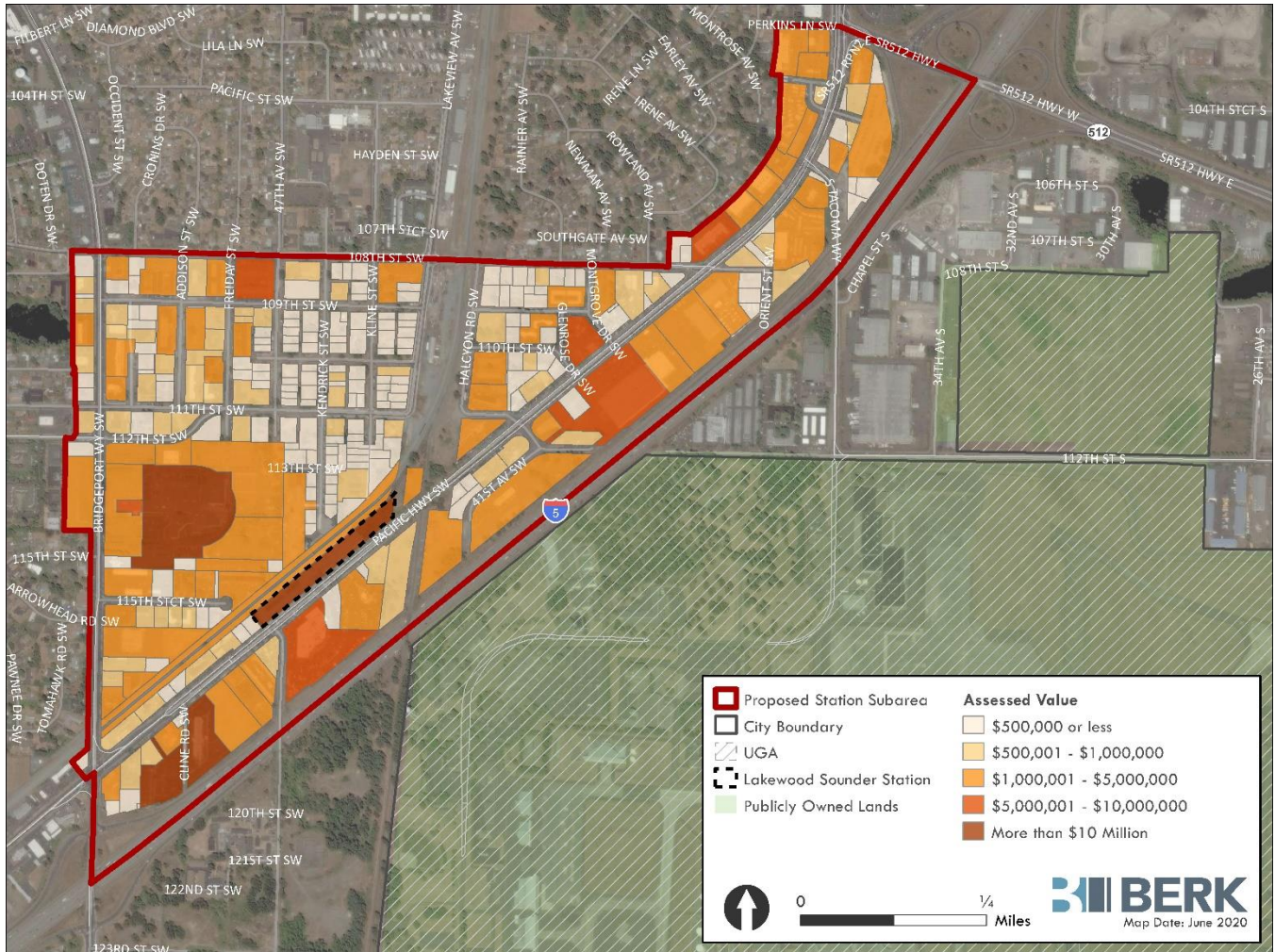
Exhibit 12. Buildable Lands: Vacant and Underutilized Properties



Source: Pierce County 2014, BERK 2020.

While the buildable lands data relies on older 2010 information published in 2014, the information is still relatively intact reviewing building to land values. Where land values are relatively equal or higher than building values, changes to land use are more likely. See Exhibit 13.

Exhibit 13. Building to Land Values



Source: Pierce County 2014, BERK 2020.

e. What is the current zoning classification of the site?

Most of the study area is zoned Transit-Oriented Commercial (TOC), Public Institutional, Multifamily 3 (MF3) and Commercial 2 (C2). See Exhibit 14. See also 12.a below.

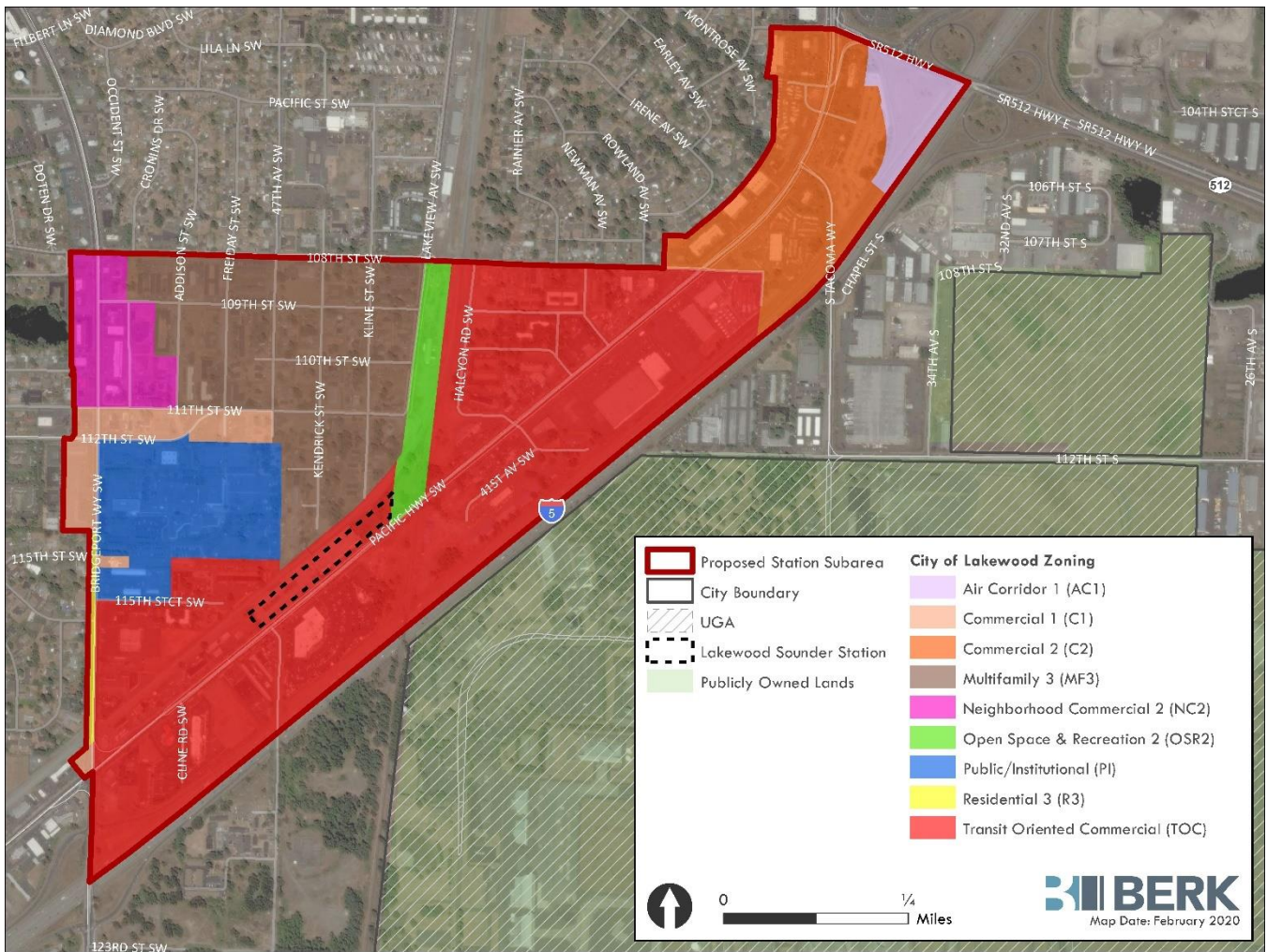
Exhibit 14. Current Zoning Acres

Zone District	Acres
Air Corridor 1	0.3
Commercial 1	7.9
Commercial 2	25.1
Multifamily 3	41.1
Neighborhood Commercial 2	10.6
Open Space & Recreation 2	0.6

Zone District	Acres
Public Institutional	46.0
Transit Oriented Commercial	99.5
Grand Total	231.1

Source: City of Lakewood, BERK 2020.

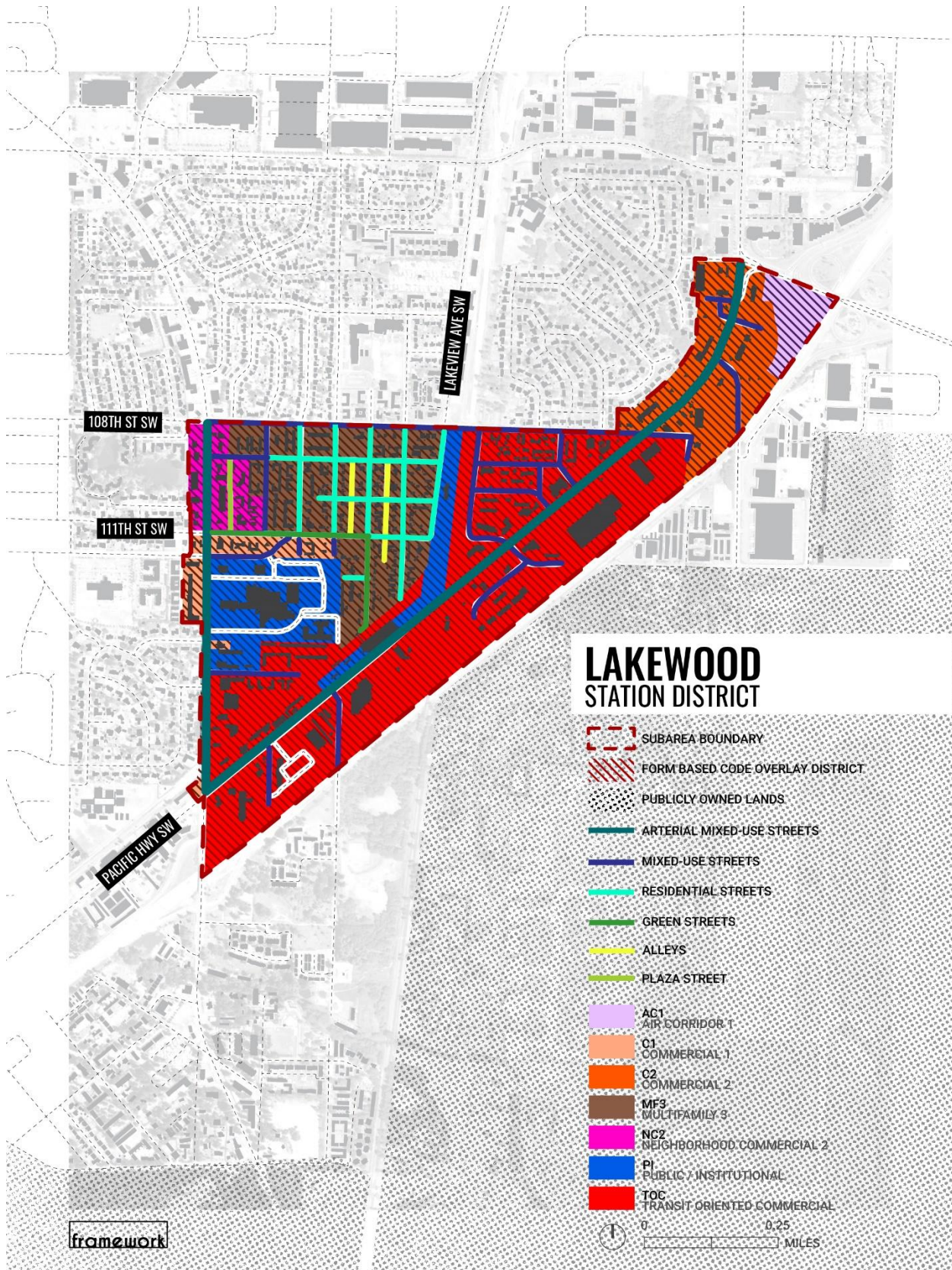
Exhibit 15. Current Zoning Map



Source: City of Lakewood, BERK 2020.

Under the Planned Action, typologies for buildings would be allowed based on a regulating plan that varies street typologies. See Exhibit 16 for the Station District Regulating Plan. With the Station District Form Based Code, building typologies would be defined; see Exhibit 17. A townhouse/rowhouse style building typology is added compared to Downtown.

Exhibit 16. Draft Planned Action Regulating Plan – Station District



Source: Framework 2021.

Exhibit 17. Form-Based Code Frontage Types – Station District

Street type	Sidewalk width	Linear	Forecourt	Plaza	Landscape	Porch /Stoop /Terrace	Parking
Mixed-Use Street	10' minimum	P	P	P/R1	P	X	X
Arterial Mixed-Use Street	As determined by Public Works	P	P	P	P	P	P
Residential Street	8' minimum	X	P	X	P	P	X
Green Street	8' minimum	X	P	X	P	P	X

P=permitted, X=prohibited, R=required

1 Required when on a corner lot.

Description

Linear. The linear building frontage has zero setback from the street edge and is the primary frontage type on retail streets and is also appropriate for land uses such as townhouses and row houses.

Forecourt. The forecourt building frontage type has an open area at the entrance along the street edge. This building type is applicable to a wide range of land use types and mixed-use development.

Plaza. The plaza frontage type includes a pedestrian-oriented plaza between the building and the street edge. The plaza frontage type is applicable to retail and dining uses and can support activities such as outdoor dining, public art displays, seating, entertainment, and events.

Landscape. The landscape frontage has landscaping between the building and street edge. The landscape frontage is not permitted on retail streets and is appropriate for office and residential uses particularly when on the ground floor.

Porch/Stoop/Terrace. This frontage type has the building set back from the street edge to accommodate a porch, stoop, or terrace to serve as the primary access to the building. This frontage type is applicable primarily to residential and nonretail commercial uses and is not permitted on retail streets.

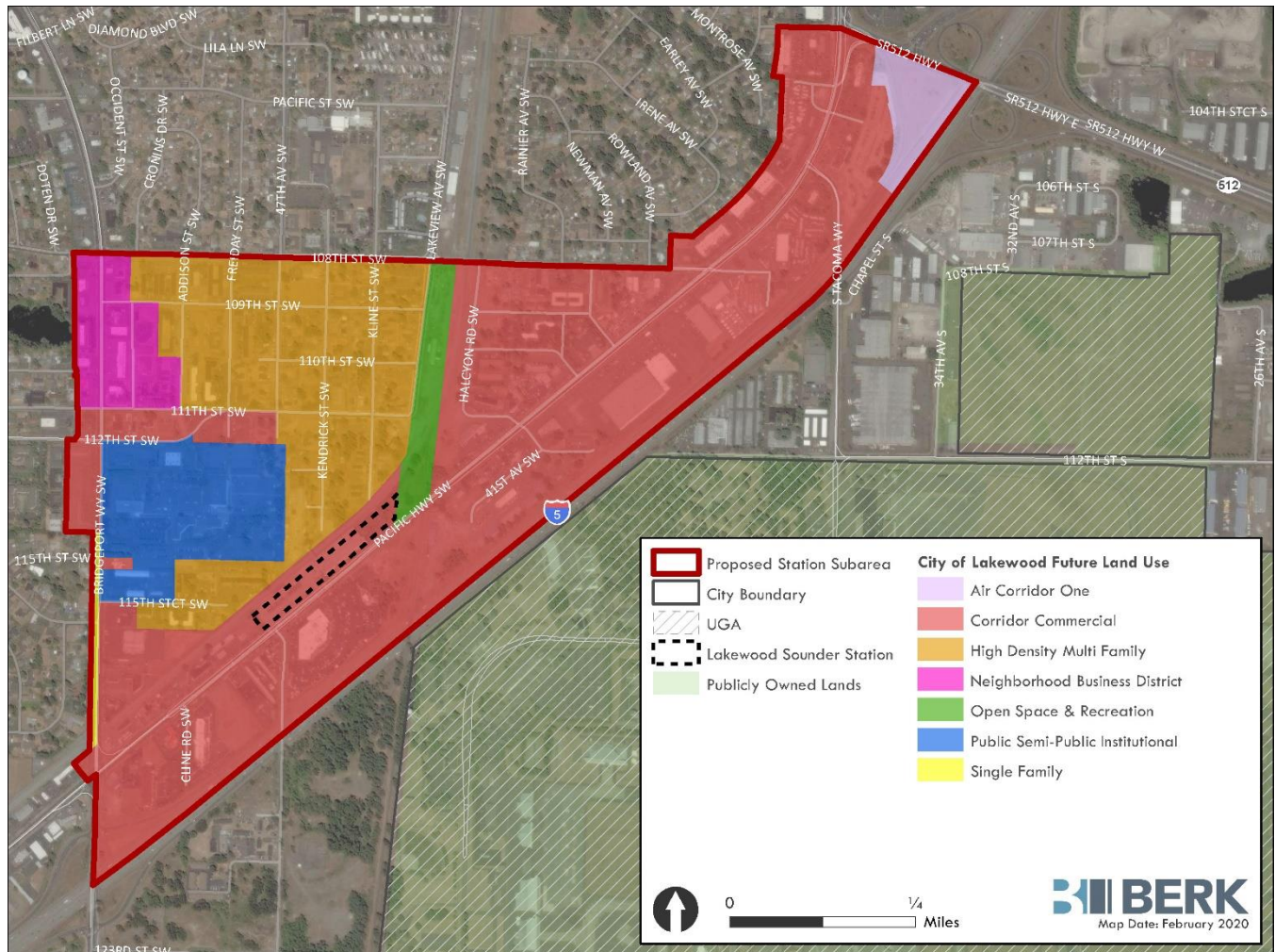
Parking. The parking frontage type is only permitted on arterial mixed-use streets and is the only frontage type where parking is allowed between the building and the street edge. Landscape and trees are required in the landscaped area between the street edge and the parking area.

The proposed code encourages the use of alleys to provide secondary access to properties, de-emphasize parking lots, and to promote continuous building frontages. Alleys shall meet Public Works engineering standards. Along Occidental, a special designation identifies acceptable development frontages similar to Mixed Use Streets.

f. What is the current comprehensive plan designation of the site?

Most of the study area is designated as Corridor Commercial, or High Density Multi Family. There are smaller areas of Neighborhood Business District and Public Semi-Public Institutional. See Exhibit 18. See also 12.a below.

Exhibit 18. Current Comprehensive Plan



Source: City of Lakewood, BERK 2020.

g. If applicable, what is the current shoreline master program designation of the site?

There are no waterbodies or designated shorelines in the study area.

h. Has any part of the site been classified as a critical area by the city or county? If so, specify.

The area is considered an aquifer recharge area and contains a 500-year floodplain. There are no mapped wetlands or streams or geologic hazards. See Sections B1 and B3. There are no mapped threatened and endangered plants or animals; see Sections B.4 and B.5.

i. Approximately how many people would reside or work in the completed project?

Growth capacity ranges from 1,345 to 1,780 dwellings. There would be at least 1,190 to 1,280 jobs rounded, with a few thousand commercial jobs likely in later phases of the Lakewood Landing site. The lower range represents the No Action Alternative and the Planned Action represents the higher range. Growth assumptions are similar to growth capacity though slightly lower to test a little less than maximum capacity (e.g. midrange density in amended MF3 zone). See Exhibit 19.

Population associated with 1,722 households under the Planned Action would equal about 4,047 persons assume

2.35 persons per household. Under the No Action, the expected population would equal 2,261.

Exhibit 19. Study Area Growth Capacity and Assumptions

	No Action Alternative	Planned Action Alternative
Growth Capacity	<ul style="list-style-type: none"> ▪ The 2014 Buildable Lands Report indicated a low capacity of 258 new dwelling units and 1,186 jobs. <ul style="list-style-type: none"> ○ Application of 2014 Buildable Lands Report zone-based assumptions to underutilized and vacant lands shows a potential for up to 1,343 new dwelling units. 	<ul style="list-style-type: none"> ▪ If achieving up to 40 units per acre in the MF3 zoned area and 54 units per acre in mixed use zones, capacity for residential would be about 1,019 new units. Lakewood Landing would result in another 760 planned units or a total of 1,779 dwellings. ▪ Job capacity would equal 1,186 jobs, plus Lakewood Landing jobs. Early phase includes retail jobs (approximately 171). Later phases will be determined but could include several thousand office jobs.
Growth Assumptions	<ul style="list-style-type: none"> ▪ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Traffic Model: 962 ○ Jobs Distributed per Traffic Model: 1,193 	<ul style="list-style-type: none"> ▪ Transportation Model 2010-2035: <ul style="list-style-type: none"> ○ Households Distributed per Buildable Land Capacity Shares illustrating middle density distribution across MF3 zone (35 units per acre average): $962 + 760 = 1,722$ ○ Jobs: 1,105 plus 171 Lakewood Landing = 1,276

j. Approximately how many people would the completed project displace?

The study area has portions considered at moderate and high displacement risk. (Puget Sound Regional Council, 2019)⁵ Residents and businesses may be priced out of the area as new development occurs.

k. Proposed measures to avoid or reduce displacement impacts, if any:

There is sufficient growth capacity to replace existing job space as sites redevelop. The City can promote development standards that allow for commercial space in mixed use areas to maintain affordable rents (e.g. adjust parking ratios for businesses less than a certain size similar to Downtown).

There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the area to its multi-family tax exemption for affordable housing in

⁵ According to PSRC, displacement risk is a composite of indicators representing five elements of neighborhood displacement risks: socio-demographics, transportation qualities, neighborhood characteristics, housing, and civic engagement.

L. Proposed measures to ensure the proposal is compatible with existing and projected land uses and plans, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, rowhouses/townhouses (e.g. 35 feet focusing on building types such as duplexes, triplexes or rowhouses in the MF3 zone would be less disruptive for existing single family residents that are located in pockets of the zone.

m. Proposed measures to reduce or control impacts to agricultural and forest lands of long-term commercial significance, if any:

None required.

9. HOUSING

a. Approximately how many units would be provided, if any? Indicate whether high, middle, or low-income housing.

Currently there are 864 dwellings; see Exhibit 11. Growth capacity ranges from 1,345 to 1,780 dwellings. See Exhibit 19.

b. Approximately how many units, if any, would be eliminated? Indicate whether high, middle, or low-income housing.

About 159 of the total permanent dwellings and RVs could be displaced due to redevelopment on underutilized lands. The Oakknoll Trailer Park is on 1.7 acres and would not meet all the criteria of LMC 18A.40.110 due to its acreage. It may not meet space per unit, open space, and other standards.

Exhibit 20. Units and Potential for Displacement

Unit Type	Units on Vacant and Underutilized Land
Comm Land With SFR	1
Duplex 2 Units	14
Fourplex 4 Units	3
Multi Fam Apts 5 Units Or More	26
Other Residential	1
Single Family Dwelling	71
Triplex 3 Units	5
Grand Total	121
Grand Total With RV and Mobile Homes	159

Source: Pierce County Assessor, BERK 2020.

c. Proposed measures to reduce or control housing impacts, if any:

The draft subarea plan includes strategies to reduce displacement effects. There is sufficient capacity to replace existing housing as the sites redevelop. The City does provide inclusionary density bonuses, development standard flexibility and fee reductions for affordable housing in Chapter 18A.90 Housing Incentives Program. The City could also implement RCW 36.70A.545 which allows for an increased density bonus for affordable housing located on property owned by a religious organization. The City’s program in 18A.90 could be adapted to this recent state law, which requires that 100% of the units be affordable.

The intent of the Planned Action is to create more incentives for missing middle ownership housing such as townhomes/rowhouses. The City could add the study area to its multi-family tax exemption for affordable housing in LMC Chapter 3.64 Tax Incentive Urban Use Center Development.

Based on recent state laws, tiny homes and recreational vehicles must be allowed as a primary residence. (See ESSB 5183 (2019)). RCW 35.21.684, allows for tiny homes in manufactured home parks, and allows local jurisdictions more flexibility to authorize them in other zones). The City will implement strategies in the Subarea Plan largely through the Form-Based Code, but will also determine implementation through its code docketing process.

10. AESTHETICS

a. What is the tallest height of any proposed structure(s), not including antennas; what is the principal exterior building material(s) proposed?

Heights range by zone from 60 to 90 feet but are lower at boundaries with lower intensity residential zones (see LMC 18A.60.100). With the Form Based Code, building typologies would be similar to those applied in Downtown. Heights would vary based on building type, and no building typologies would exceed current zoning standards and may be lower. For example, a townhouse/rowhouse style building would have a height more like three stories.

Exhibit 21. Current Height by Zone

	MF3	NC2	TOC	CBD	C1	C2
Building height	80 feet	60 feet	90 feet	90 feet	60 feet	60 feet

Source: Lakewood Municipal Code, Title 18, BERK 2020.

b. What views in the immediate vicinity would be altered or obstructed?

There are no protected views in the study area. Greater building heights could result in view of Mount Rainier.

c. Proposed measures to reduce or control aesthetic impacts, if any:

The No Action Alternative would implement Chapter 18A.70 Community Design, Landscaping, and Tree Preservation. The Planned Action would implement similar landscaping and tree presentation standards and custom design standards for the buildings and public realm in a hybrid form-based code.

Under the Planned Action, promoting MF3 building heights around three stories/35 feet, and focusing on building types such as duplexes, triplexes or rowhouses is less disruptive for existing single family residents.

11. LIGHT AND GLARE

a. What type of light or glare will the proposal produce? What time of day would it mainly occur?

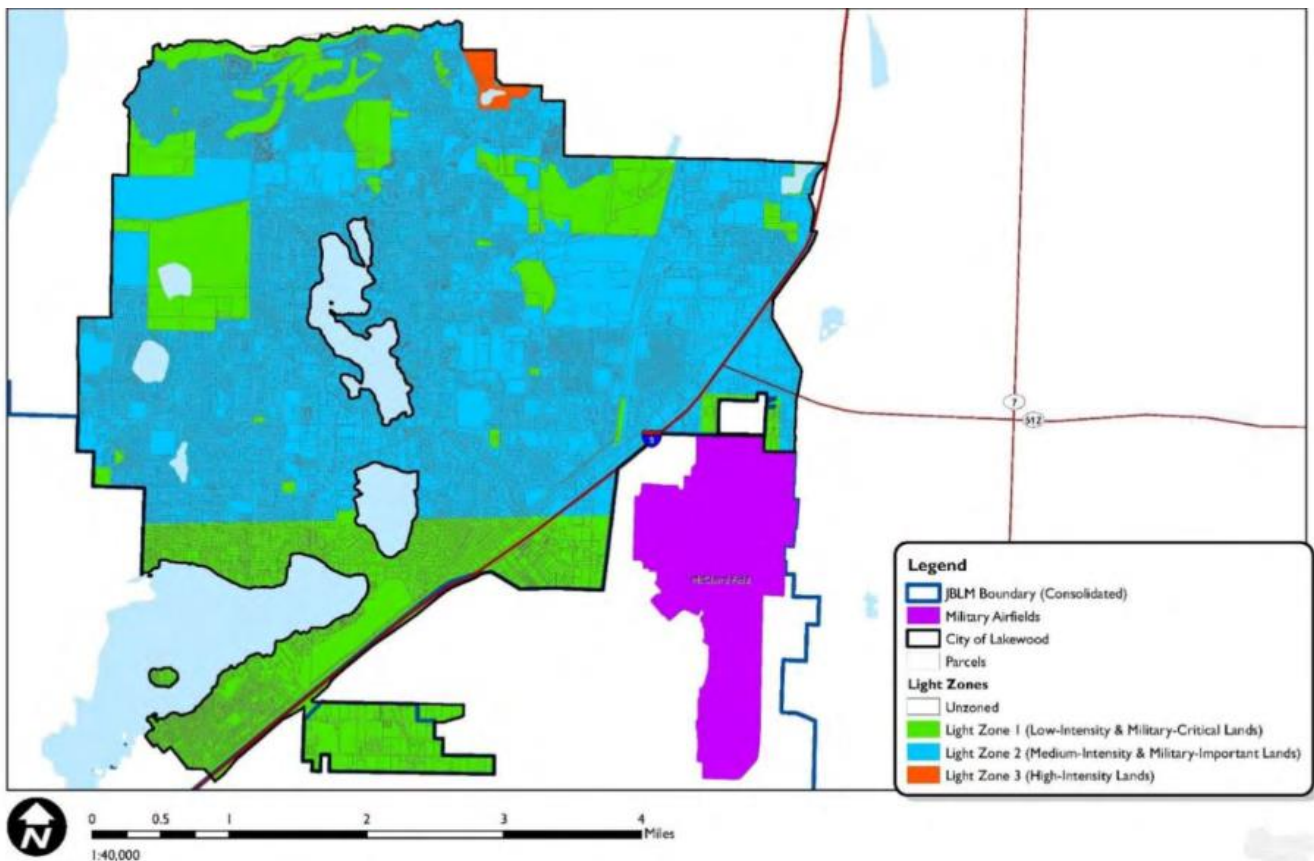
Future development allowed in the study area would add interior and exterior lights to buildings and parking lots to address night time visibility.

b. Could light or glare from the finished project be a safety hazard or interfere with views?

Impacts are not anticipated from typical residential and commercial uses, which are regulated by City lighting standards.

The subarea is subject to lighting standards and requirements to prevent interference with aircraft operation at JBLM.⁶ The northern portion of the subarea, near the interchange with SR-512 is part of Light Zone 2 and the rest of the subarea is in Light Zone 1 (Exhibit 22). Light Zone 1 is applicable to low-intensity land uses with low levels of exterior lighting at night. Light Zone 2 applies to medium intensity uses with model levels of exterior lighting such as residential, mixed-use, and commercial areas. Lighting standards are also intended to reduce light pollution, conserve energy, and provide safety and security. Generally, the code requires lighting to be shielded to prevent light shining above the luminary and to prevent light spill over on to adjacent properties.

Exhibit 22. Lakewood Light Zones



Source: Lakewood Municipal Code, 18A.60.095

⁶ 18A.60.095

c. What existing off-site sources of light or glare may affect your proposal?

Highways and streets abutting the study area like I-5 produce light.

d. Proposed measures to reduce or control light and glare impacts, if any:

LMC 18A.60.095 Outdoor Lighting Code provides lighting standards to reduce light and glare and to avoid interference with JBLM operations.

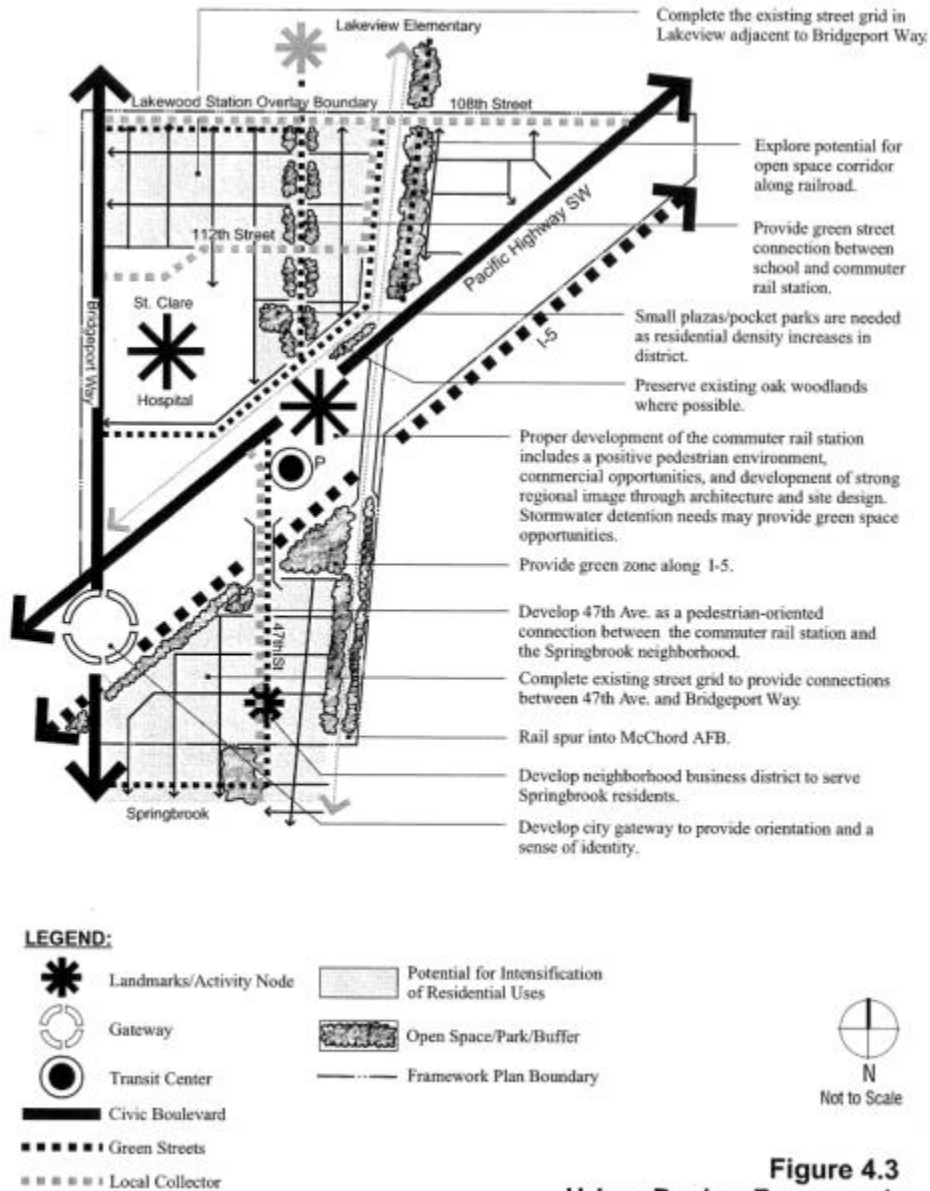
12. RECREATION

a. What designated and informal recreational opportunities are in the immediate vicinity?

Early conceptions of the subarea included the development of a linear park along the railway tracks on land now owned by Sound Transit. See Exhibit 23.

The Open Space designation/zone may be modified to be in a Public/Institutional designation and zone as the railway tracks where the land is owned by Sound Transit and not likely to become a dedicated off-street open space. However, there are opportunities for linear parks/trails.

Exhibit 23. Lakewood Station District Subarea Concept



SOURCE: EDAW, Inc. 1999

Source: Lakewood Comprehensive Plan, 2014.

b. Would the proposed project displace any existing recreational uses? If so, describe.

No parks exist or would be displaced. The area is beyond a 10-minute walk of parks per the City’s Legacy Parks Plan 2020 (Figure 3-14).⁷ The City’s plan calls for a park in or near the Lakewood Station District. With planned sidewalk improvements (see B.1.4) and street frontage improvements there is an opportunity to provide for linear parks.

The Planned Action proposes onsite open space standards for multifamily and commercial uses similar to the

⁷ See: <https://cityoflakewood.us/wp-content/uploads/2020/05/FINAL-Legacy-Plan-2020-w-pg-numbers-flattened-1.pdf>.

Downtown. The Subarea Plan includes the following public space concepts:

- Lakewood Landing will include a landscaped public plaza in its first phase to support community gathering and provide urban open space.
- A linear park along Kendrick Street south of 111th Avenue SW that would bring green space to the heart of the district's residential area. This would better define the pedestrian connection between Lakewood Station and the Lakeview Hope Academy and facilitate the use of the playground there as a neighborhood park as envisioned in the *Legacy Plan 2020*.
- Green corridors with enhanced landscaping in areas with planned pedestrian and bicycle improvements such as 111th /112th Avenue SW.
- A pocket of open space in the alley near Occidental Street.
- A linear park connecting Lakewood Station to the Springbrook Area through improvements along 47th Avenue as envisioned in the *Legacy Plan 2020*.

Public and semi-private open spaces will help to balance the intensity of land use in the LSDS. This could include the development of plazas, pocket parks, courtyards, or other landscaped open spaces that would provide opportunities for gathering and passive recreation.

c. Proposed measures to reduce or control impacts on recreation, including recreation opportunities to be provided by the project or applicant, if any:

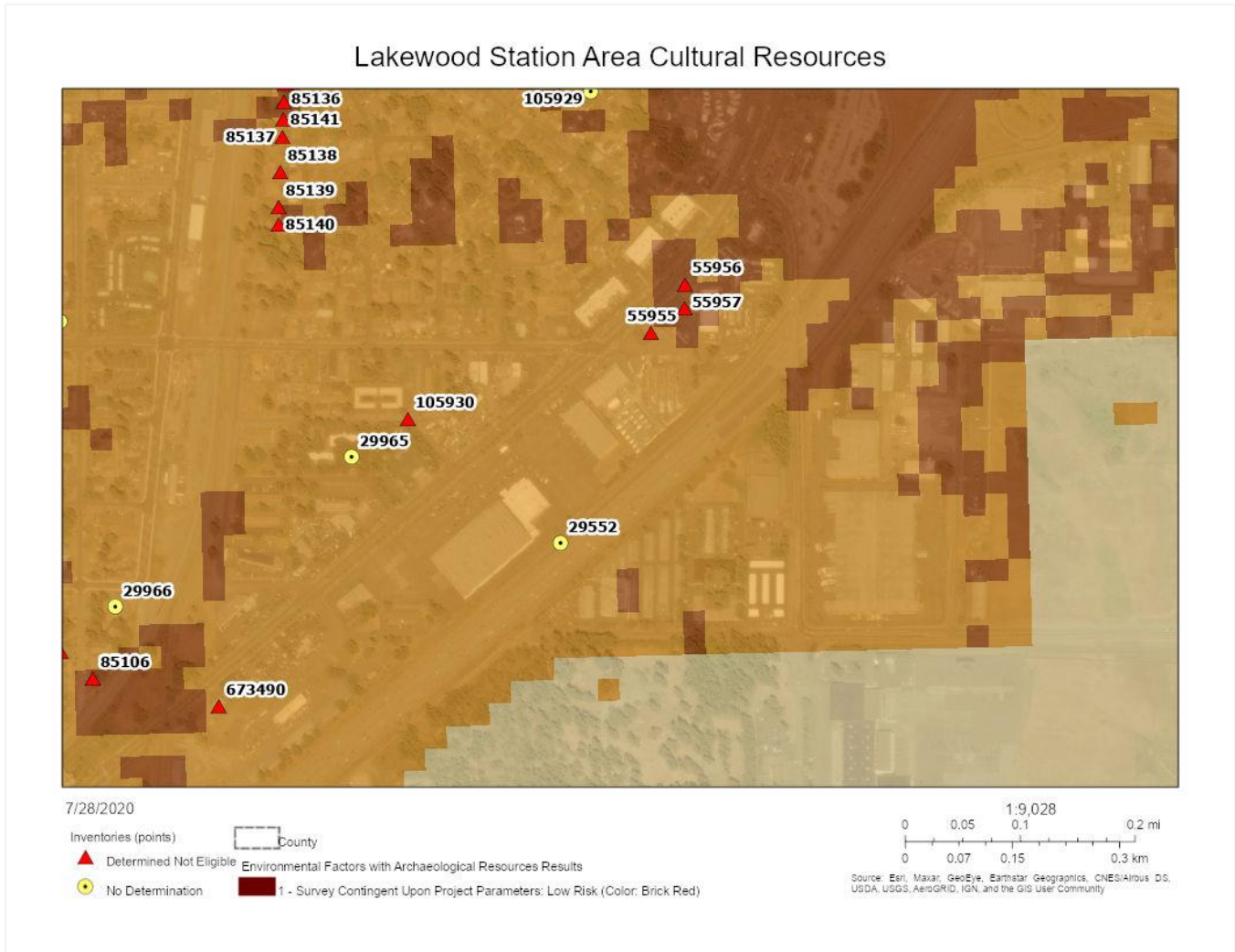
Under the Planned Action there would be application of common and private open space standards. Green Streets and other public spaces listed in B.12.b above are identified for enhanced pedestrian and landscaping experience. The form-based code and subarea plan include cross sections for streets that fit into current rights of way.

13. HISTORIC AND CULTURAL PRESERVATION

a. Are there any buildings, structures, or sites, located on or near the site that are over 45 years old listed in or eligible for listing in national, state, or local preservation registers? If so, specifically describe.

No structures are determined as eligible, and some are undetermined according to the Department of Archaeology and Historic Preservation (DAHP). See Exhibit 24. There are several properties that have structures that are over 45 years old. See Exhibit 25.

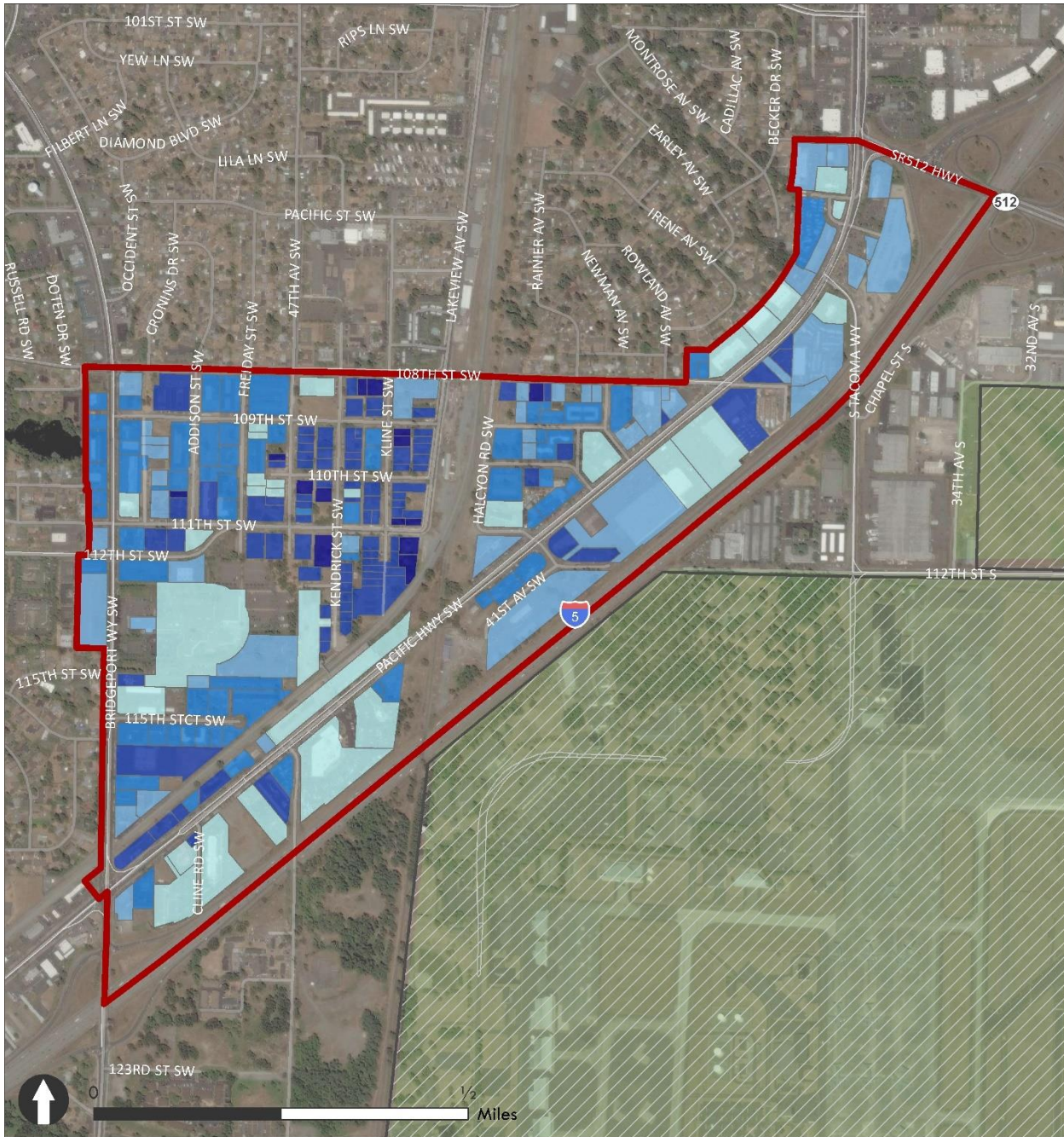
Exhibit 24. Department of Archaeology and Historic Preservation Historic and Cultural Resources



Source: Department of Archaeology and Historic Preservation, WISAARD, 2020.
 Additional Legend:

- 1 - Survey Contingent Upon Project Parameters: Low Risk (Color: Brick Red) ■
- 2 - Survey Contingent Upon Project Parameters: Moderately Low Risk (Color: Burnt Orange) ■
- 3 - Survey Recommended: Moderate Risk (Color: Orange) ■
- 4 - Survey Highly Advised: High Risk (Color: Pale Yellow) ■
- 5 - Survey Highly Advised: Very High Risk (Color: Brightest Yellow/Canary Yellow) ■

Exhibit 25. Age of Buildings



BERK
Map Date: February 2020

Source: Pierce County Assessor, BERK 2020.

b. Are there any landmarks, features, or other evidence of Indian or historic use or occupation? This may include human burials or old cemeteries. Are there any material evidence, artifacts, or areas of cultural importance on or near the site? Please list any professional studies conducted at the site to identify such resources.

There are areas of low to moderate risk of finding cultural resources per Exhibit 24.

c. Describe the methods used to assess the potential impacts to cultural and historic resources on or near the project site. Examples include consultation with tribes and the department of archeology and historic preservation, archaeological surveys, historic maps, GIS data, etc.

Methods include review of Pierce County Assessor Records, DAHP WISAARD program, and Lakewood's local register review.

d. Proposed measures to avoid, minimize, or compensate for loss, changes to, and disturbance to resources. Please include plans for the above and any permits that may be required.

Washington State has a number of laws that oversee the protection and proper excavation of archaeological sites (RCW 27.53, WAC 25-48), human remains (RCW 27.44), and historic cemeteries or graves (RCW 68.60). The Governor's Executive Order 05-05 requires state agencies to integrate DAHP, the Governor's Office of Indian Affairs, and concerned tribes into their capital project planning process. This executive order affects any capital construction projects and any land acquisitions for purposes of capital construction not undergoing Section 106 review under the National Historic Preservation Act of 1966.

Under RCW 27.53, DAHP regulates the treatment of archaeological sites on both public and private lands and has the authority to require specific treatment of archaeological resources. All precontact resources or sites are protected, regardless of their significance or eligibility for local, state, or national registers. Historic archaeological resources or sites are protected unless DAHP has made a determination of "not-eligible" for listing on the state and national registers.

The City applies Lakewood Municipal Code Chapter 2.48 Protection and Preservation of Landmarks.

It is proposed that the following mitigation measures be made part of the Planned Action Ordinance in order to be considered Planned Action. This would include:

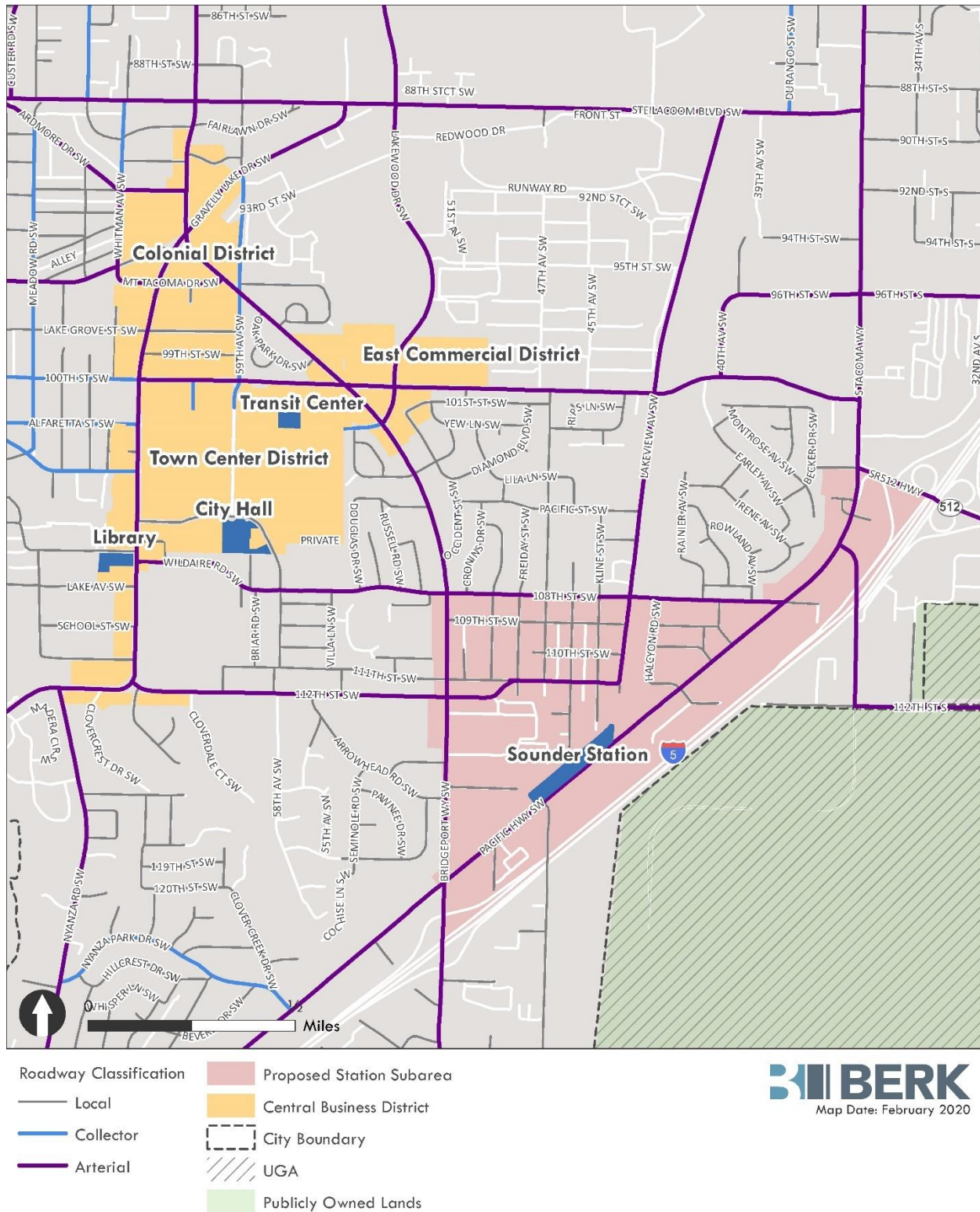
- Include Inadvertent Human Remains Discovery Language recommended by DAHP as a condition of project approval.

14. TRANSPORTATION

a. Identify public streets and highways serving the site or affected geographic area and describe proposed access to the existing street system. Show on site plans, if any.

Major roadways in the subarea include Pacific Highway, 108th Street SW, and Bridgeport Way. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to 1-5 ramps and the Lakewood Station within the subarea. See Exhibit 26.

Exhibit 26. Road Classifications in Study Area and Vicinity

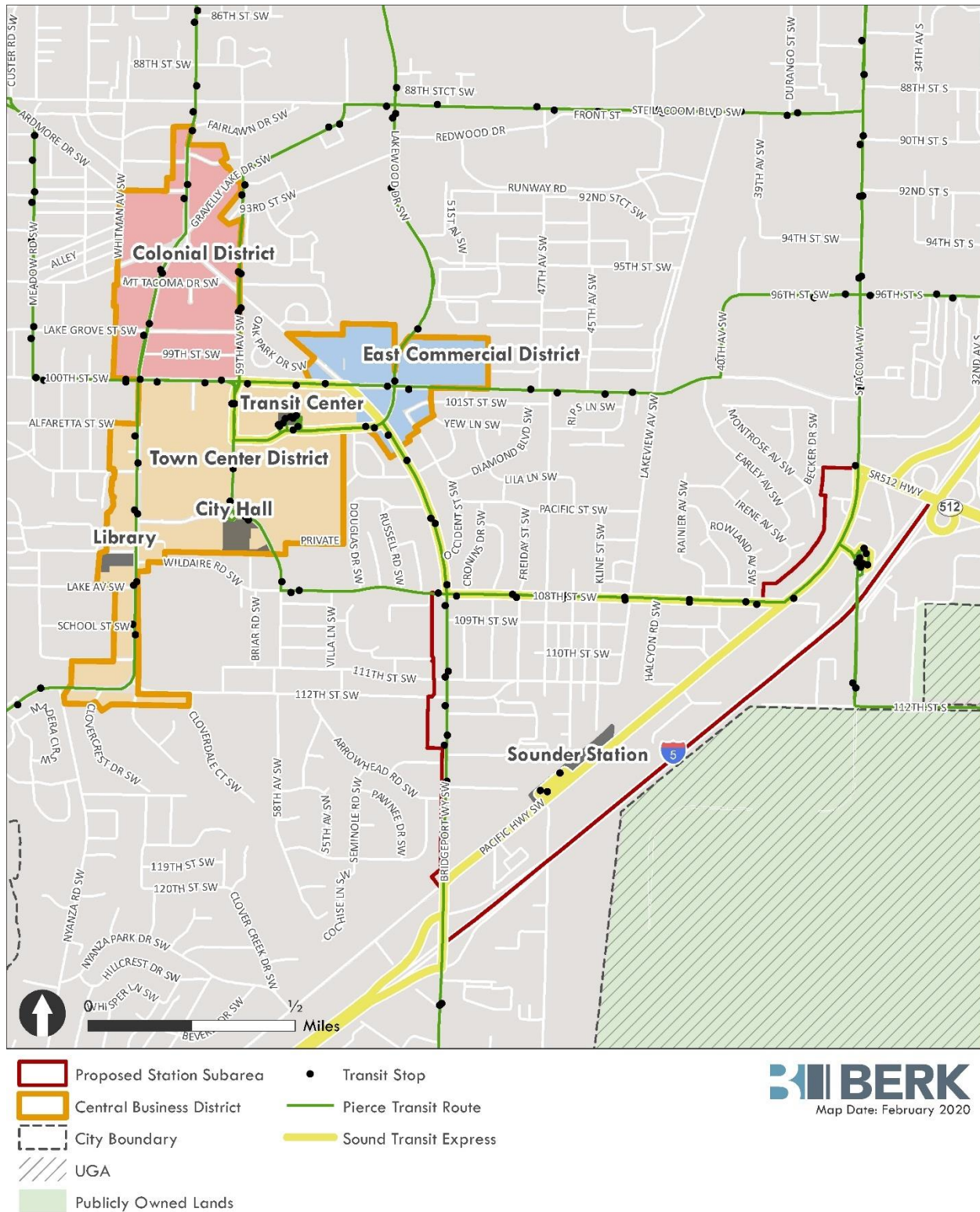


Source: City of Lakewood, Fehr & Peers 2020.

b. Is the site or affected geographic area currently served by public transit? If so, generally describe. If not, what is the approximate distance to the nearest transit stop?

Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

Exhibit 27. Transit Service in Study Area and Vicinity



Source: Pierce Transit, City of Lakewood, Fehr & Peers 2020.

c. How many additional parking spaces would the completed project or non-project proposal have? How many would the project or proposal eliminate?

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital’s campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Under the No Action Alternative, future uses must comply with the requirements in Chapter 18A.80 LMC. The Planned Action would follow parking standards for residential, retail, and office uses, and otherwise follow LMC Chapter 18A.80 for others.

Exhibit 28. Station District Proposed Specific Parking Standards

LAND USE	PARKING REQUIREMENT: STAFF RECOMMENDATIONS	BICYCLE PARKING
Residential	Single-family: 2 per dwelling unit Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698) Senior citizen apartments 1 per 3 dwelling units* Multifamily housing: 1.25 spaces per dwelling unit* *A process is included to prepare parking study to reduce further near station.	Meet rates and standards of: Chapter 18A.80
Retail. Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500’ or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

Source: Lakewood Municipal Code, draft 18C Form Based Code.

City staff recommendations are included in the table above. The Planning Commission has recommended no minimum parking for senior citizen or multifamily housing, and proponents would consider parking demand and market forces to determine spaces.

d. Will the proposal require any new or improvements to existing roads, streets, pedestrian, bicycle or state transportation facilities, not including driveways? If so, generally describe (indicate whether public or private).

Planned Improvements

The Six-Year Comprehensive Transportation Improvement Program (TIP) for 2021-2026 was approved by the Lakewood City Council in June 2020. This document outlines short and long term road projects, including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays, throughout the city. The following projects within the LSDS are identified in the TIP as long term improvements:

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study.
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities.
- 112th/111th – Bridgeport to Kendrick – sidewalks, bicycle facilities, street lighting.
- 108th – Bridgeport Way to Pacific Highway – roadway patching and overlay.

Sound Transit and Pierce Transit have also identified improvements for the LSDS in their plans. The Sound Transit System Access Fund awarded funds in 2019 to support non-motorized access to Lakewood Station. These include a variety of improvements to support pedestrian and bicycle access on 11th Street SW and 112th Street SW. Pierce Transit’s *Destination 2040* plan identifies two new Bus Rapid Transit (BRT) routes that will bring additional transit service to the subarea. A new line in 2026 would connect to Downtown Tacoma. In 2030 a BRT line would replace an existing transit line connecting Lakewood to Puyallup’s South Hill.

Specific Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the subarea. Exhibit 29 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right-of-way acquisition or widening, rather only changes to signal timing. The exception is intersection of Pacific Highway and Sharondale Street, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better.

Exhibit 29. Proposed Mitigation Measures

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
4	Pacific Highway / Sharondale Street			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			

ID	Intersection	No Action	Alt 1	Alt 1 Mitigated
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.	D/54	E/78	D/49
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

With implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030.

Street Standards

Street frontage standards are part of the proposed Title 18C Station District Development Code.

e. Will the project or proposal use (or occur in the immediate vicinity of) water, rail, or air transportation? If so, generally describe.

All of Lakewood, including the subarea, is within the Lakewood Military Coordination & Notice Area (MCNA).⁸ Jurisdictions within the MCNA coordinate with JBLM prior to the approval of zoning and Comprehensive Plan amendments. Current zoning has already been subject to MCNA review, but the City notifies JBLM of all land use and building permits, subdivisions, and site plans to provide opportunity for comment.

Parts of Lakewood are also subject to airport compatible land use restrictions. The subarea is not within the most restrictive aircraft safety zones, but is within the Inner Horizontal Surface imaginary surface area for the safe operation of aircraft around JBLM.⁹ JBLM reviews proposed development to determine if the use is prohibited or could interfere with pilot vision, communication, radar, or other elements of safe operation. Typically, uses that produce steam, dust, glare that may impair visual operation, or those that attract birds, are prohibited.

The subarea is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

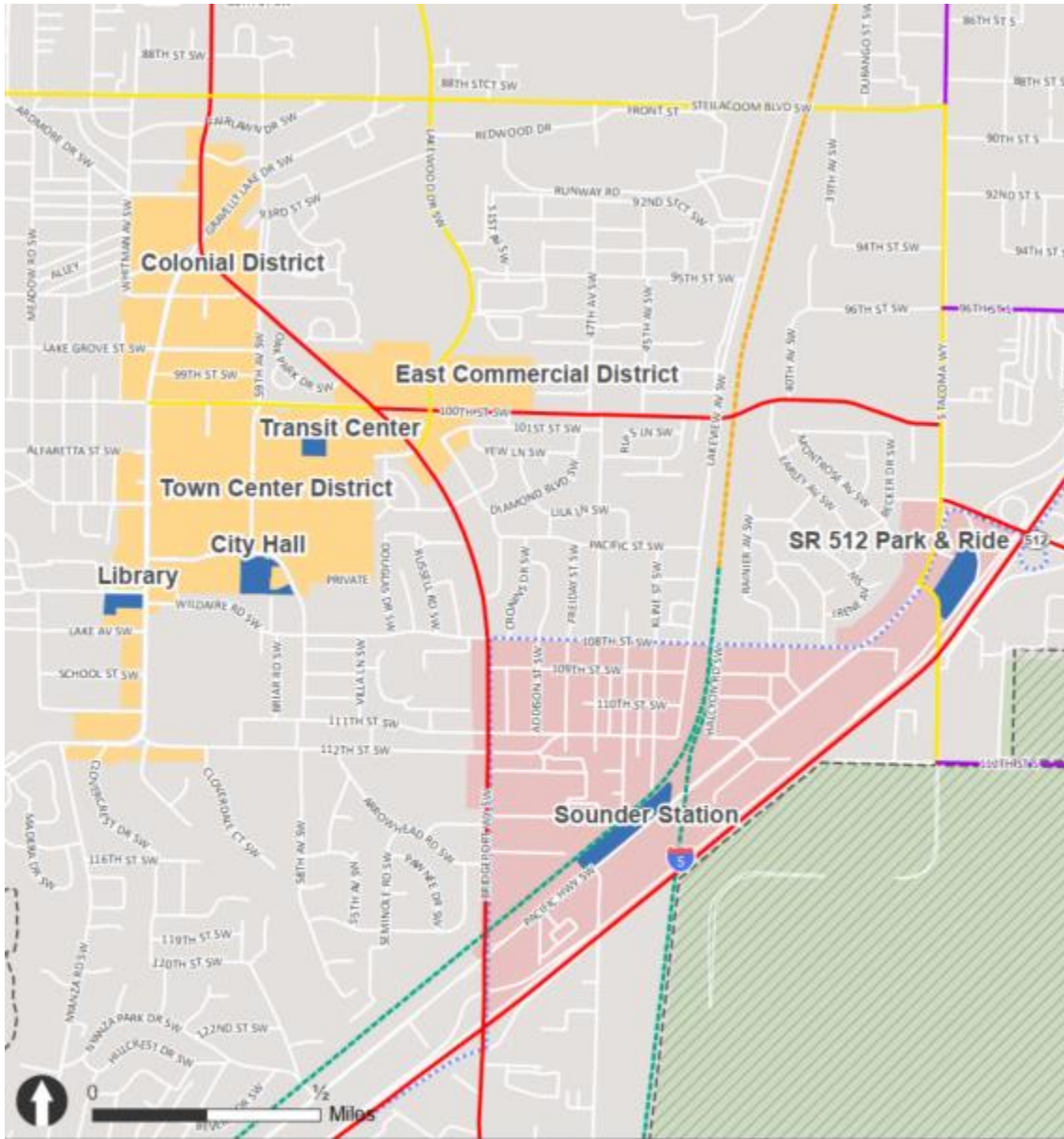
The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the City. Designation as a major truck street helps Lakewood's Public Works Transportation division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated Way. Bridgeport Way and South Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 update, as shown in Exhibit 30.¹⁰ In addition to the designated routes, 108th Street SW serves as a de facto freight route for trucks heading northbound on I-5 due to the limited height of the railroad bridge crossing I-5 at JBLM. These trucks exit the freeway at Bridgeport, take 108th to Pacific Highway, and then reenter I-5 at SR-512.

⁸ 18A.10.135.6

⁹ 18A.10.135.10B

¹⁰ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 30. Freight and Good Transportation System Routes



Source: WSDOT, 2019; Fehr & Peers, 2020; City of Lakewood, 2020.

f. How many vehicular trips per day would be generated by the completed project or proposal? If known, indicate when peak volumes would occur and what percentage of the volume would be trucks (such as commercial and nonpassenger vehicles). What data or transportation models were used to make these estimates?

The net increase between Planned Action and Existing Conditions is 1,950 vehicle trips. Total rounded vehicle trips by scenario tested include:

- Existing – 2,530
- No Action – 3,810
- Planned Action – 4,480

g. Will the proposal interfere with, affect or be affected by the movement of agricultural and forest products on roads or streets in the area? If so, generally describe.

This is not applicable. There are no areas in use for agricultural and forest products.

h. Proposed measures to reduce or control transportation impacts, if any:

See “d” above for planned improvements.

Other mitigation includes:

- Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip reduction plans and work toward meeting their mode share targets through internal programs and monitoring.
- The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

15. PUBLIC SERVICES

a. Would the project result in an increased need for public services (for example: fire protection, police protection, public transit, health care, schools, other)? If so, generally describe.

Emergency Services

Emergency response services in Lakewood are provided by West Pierce Fire and Rescue and the Lakewood Police Department. St. Clare hospital also supports a range of medical services including a 24 hour emergency room.

Future development would add new residents and employees that could increase the demand for emergency services.

Comprehensive Plan Policy P-5.1 establishes police response time objectives:

PS-5.1: Provide police protection with a three-minute response time for life-threatening emergencies (Priority 1), a six-minute response time for crimes in progress or just completed (Priority 2), and a routine/non-emergency response time of 20 minutes (Priority 3).

The status of response times is unknown.

The department 101 commissioned officers that service the City, and in 2016 responded to 49,569 calls for service. (City of Lakewood, 2020) With this information, an effective level of service can be calculated, resulting in about 1.72 officers per 1,000 residents.

Exhibit 31. Police Services Effective Level of Services Standards

Year	Population	Officers	Officers Per 1,000 Residents
2020	60,030	101	1.68

Source: (City of Lakewood, 2020)

The demand for the number of police officers under the Planned Action is anticipated to be 6.8, and under the No Action Alternative 3.8 officers.

Lakewood has adopted policies setting level of service standards for West Pierce Fire and Rescue (WPFR):

- PS-1.1: Maintain a Washington Surveying and Rating Bureau (or successor agency) rating of ISO Class 3 or better; and
- PS-4.2: Provide a four-minute initial time standard for EMS calls.

WPFR has met the PS-1.1 LOS standard with a class 3 WSRB through 2020.¹¹

WPFR has a turnout time for all incidents of 110 seconds (less than 2 minutes) and a response time of 6 minutes for a first arriving unit with a “first responder” or higher. In 2017, the goal of a 110 second response time was met 70% of the time. The goal of arrival of a unit with first responder or higher level capability at an emergency medical incident was met 83% of the time.¹²

A common effective level of service standard is to look at fire response personal per 1,000 capita. This helps compare service capabilities over-time and across jurisdictions. Fire suppression personnel are often trained in emergency medical services, and there is overlap in the number of full time equivalents (FTEs) for each activity.

Based on the expected population of the No Action Alternative, there would be a demand for 3.44 firefighters, and 6.15 under the Planned Action.

Exhibit 32. Fire Services Effective Level of Services Standards

Year	District Population	Firefighters / EMT/Medics	Firefighters Per 1,000 Residents
2020	100,000	152 Firefighters/EMTs	1.52

Source: [WPFR Budget 2020](#).

There is no level of service for the St. Clare Hospital. Relevant Comprehensive Plan policies include:

¹¹ See: <https://www.westpierce.org/west-pierce-fire-rescue-maintains-class-3-rating/>.

¹² See: <https://www.westpierce.org/wp-content/uploads/Annual-Report-2017-with-52.33-report.pdf>.

PS-14.7 Encourage the location of medical clinics and services near transit facilities.

Public Schools

Future residents would increase the demand for public schools. Public school services are provided by the Clover Park School District (CPSD). The study area is primarily served by Lakeview Hope Academy Elementary School and to a small degree by Four Heroes Elementary School. Lochburn and Hudtloff Middle Schools also serve the study area. Clover Park High School serves the entire study area.¹³

To estimate student generation, it is also possible to consider the number of households in the district in relation to the number of students. The number of households in the Clover Park School district is 29,700 as of 2019 based on State of Washington Office of Financial Management (OFM) small area estimates. There are 13,789 students in the district as of 2019. Thus, the effective student per household ratio is 0.46.

Under the Planned Action, there would be 792 students in new households, and under the No Action Alternative there would be a potential for 447 new students.

b. Proposed measures to reduce or control direct impacts on public services, if any.

The growth will occur through 2035 and beyond and would contribute taxes to help support services; growth will be incrementally absorbed.

The City addresses public service levels of service in its Capital Facilities Plan Element. The element is updated periodically to ensure that proposed growth and change can be served.

16. UTILITIES

a. Circle utilities currently available at the site:

electricity, natural gas, water, refuse service, telephone, sanitary sewer, septic system, other ___

All power, water, and sewer services are available. Lakeview Light and Power provides electrical service to the subarea and Puget Sound Energy provides natural gas service.

Water service is provided by Lakewood Water District. District policy requires the developer to pay for system improvements related to new development. Depending on the location and intensity of new development in the subarea this may include water main upgrades or line extensions to provide additional capacity or fire flow. Coordination of water line upgrades with the District's 50-year repair and replacement program could provide cost and timing efficiency for redevelopment projects in the subarea. Fire flow requirements would be applied at the time of building and construction permits. See Appendix A.

Wastewater service in Lakewood is provided by Pierce County Public Works. See Appendix A. Most of the sewer pipes were installed in the 1980's (some later) and are primarily 30 inch diameter PVC pipes, which have a 100-year lifetime.

Pierce County coordinates quarterly with the City of Lakewood to discuss upcoming and future projects. A Sewer Improvement Plan was adopted in September 2019, addressing capital facility planning from 2020-2040 and identifying funding for the next six years of capital facility improvements. There are no listed projects within or near the subarea identified in this plan.

¹³ See: <http://apps.schoolslocator.com/index.html?districtCode=49275>.

The most recent system plan is the 2010 Unified Sewer Plan, adopted in 2012. Pierce County is scheduled to begin an update to this plan in 2020. The plan accounted for zoning densities in place today, e.g. up to 54 units per acre. Development that occurs at a higher intensity than the existing conditions may require wastewater system capacity upgrades. New development or redevelopment at the levels envisioned by the future land use plan will require larger collection lines. Update of the Unified Sewer Plan provides an opportunity to plan for future development in the LSDS.

The Lakewood Landing Project would have densities of 66 units per acre if considering the Phase 1 site area of 11.5 acres, but would have densities at 25 units per acre across the entire 30 acre site. Based on the whole site area, densities would be consistent with the TOC zone that has a maximum of 54 units per acre. Coordination regarding the phasing of development and needs for sewer service improvements would assist in development implementation.

b. Describe the utilities that are proposed for the project, the utility providing the service, and the general construction activities on the site or in the immediate vicinity which might be needed.

Potential strategies for water and sewer include coordination with service providers by each Planned Action Applicant consistent with LMC 18A.60.140 Concurrency:

- Water mains may need to be upgraded for capacity or fire flow to support redevelopment. This may be done efficiently and at lower cost if strategic upgrades are coordinated with the Lakewood Water District's 50-year repair and replacement plan.
- Participation in the update of Pierce County's Unified Sewer Plan will help to identify future capacity challenges, needed upgrades to collection lines, and funding options for replacement.

C. Signature

The above answers are true and complete to the best of my knowledge. I understand that the lead agency is relying on them to make its decision.

Signature 

Name of signee: Lisa Grueter, AICP

Position and Agency/Organization: Principal, BERK Consulting

Date Submitted: July 27, 2020; updated March 25, 2021.

D. SUPPLEMENTAL SHEET FOR NONPROJECT ACTIONS

(do not use this sheet for project actions)

Because these questions are very general, it may be helpful to read them in conjunction with the list of the elements of the environment.

When answering these questions, be aware of the extent the proposal, or the types of activities likely to result from the proposal, would affect the item at a greater intensity or at a faster rate than if the proposal were not implemented. Respond briefly and in general terms.

1. How would the proposal be likely to increase discharge to water; emissions to air; production, storage, or release of toxic or hazardous substances; or production of noise?

See B.2, B.3, and B.7.

Proposed measures to avoid or reduce such increases are:

See B.2, B.3, and B.7.

2. How would the proposal be likely to affect plants, animals, fish, or marine life?

See B.4 and B.5.

Proposed measures to protect or conserve plants, animals, fish, or marine life are:

See B.4 and B.5.

3. How would the proposal be likely to deplete energy or natural resources?

See B.7.

Proposed measures to protect or conserve energy and natural resources are:

See B.7.

4. How would the proposal be likely to use or affect environmentally sensitive areas or areas designated (or eligible or under study) for governmental protection; such as parks, wilderness, wild and scenic rivers, threatened or endangered species habitat, historic or cultural sites, wetlands, flood plains, or prime farmlands?

See B.3, B.4, B.5, B.12, and B.13.

Proposed measures to protect such resources or to avoid or reduce impacts are:

See B.3, B.4, B.5, B.12, and B.13.

5. How would the proposal be likely to affect land and shoreline use, including whether it would allow or encourage land or shoreline uses incompatible with existing plans?

See B.8 regarding land use.

Proposed measures to avoid or reduce shoreline and land use impacts are:

The integration of the LSDS Subarea Plan may require minor consistency edits to the Comprehensive Plan. For example, replacement of the urban design diagram shown in Exhibit 23 and references from the Comprehensive Plan to the Subarea Plan in the Land Use Element, Transportation Element, and Capital Facilities Element as appropriate.

6. How would the proposal be likely to increase demands on transportation or public services and utilities?

See B.14 and B.15.

Proposed measures to reduce or respond to such demand(s) are:

See B.14 and B.15.

7. Identify, if possible, whether the proposal may conflict with local, state, or federal laws or requirements for the protection of the environment.

The area has limited natural environment features, but all development will be required to address critical area and water quality regulations.

Transportation

EXISTING FEATURES

This section presents a multimodal transportation evaluation of current conditions in the Lakewood Station District Subarea (LSDS). Existing transportation conditions are documented for pedestrians, bicycles, automobiles, freight, transit, and parking.

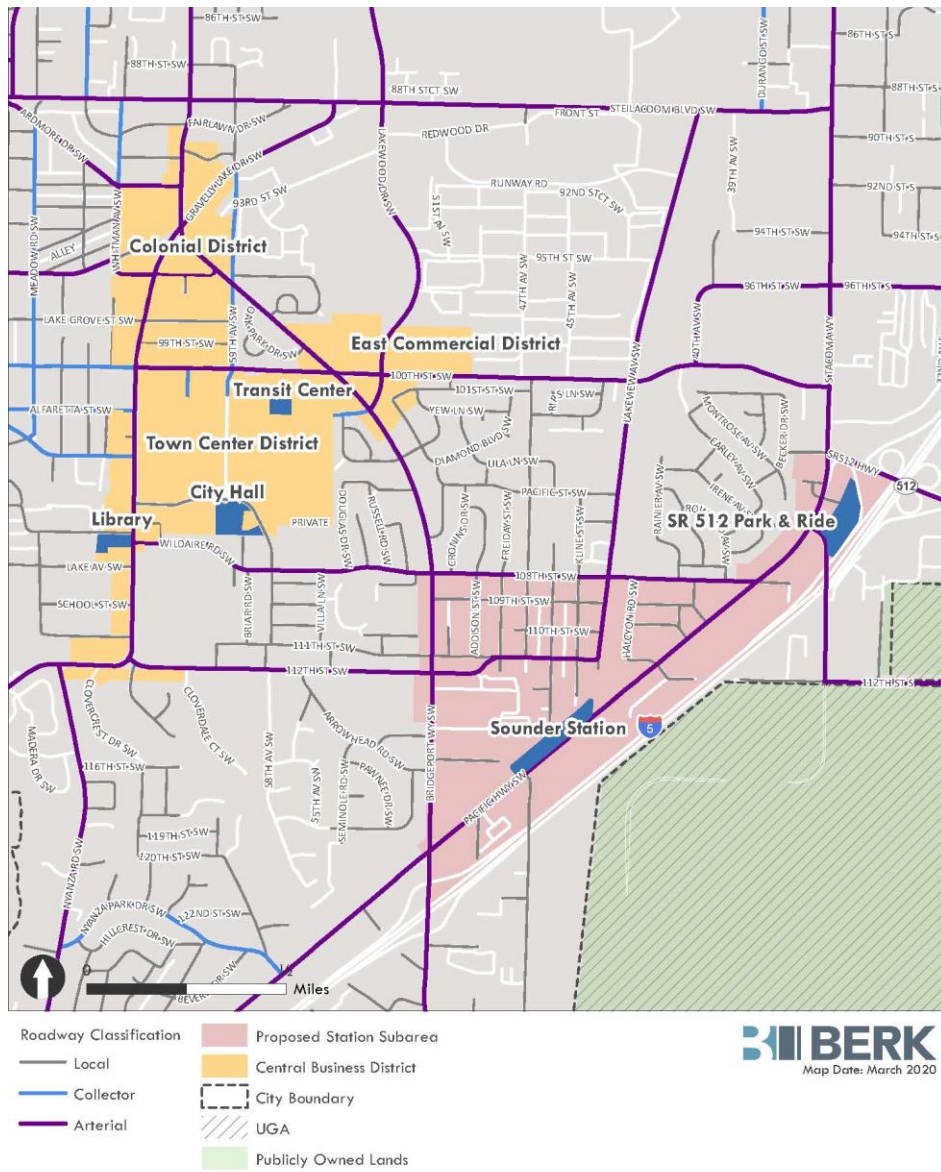
The study area is a predominantly auto-oriented environment, however the area also includes two important regional transit facilities: Lakewood Station and the SR-512 Park & Ride. Major roadways in the study area include Pacific Highway, 108th Street SW, and Bridgeport Way SW. These are classified as either principal or minor arterials with 35 mph posted speed limits. Pacific Highway provides a north-south connection between Tacoma and Lakewood, with access to I-5 ramps and Lakewood Station within the study area. **Exhibit 1** shows transportation connections in and near the LSDS.

The study area contains two key regional transportation facilities: Lakewood Station and SR 512 Park & Ride. Lakewood Station is a focal point of many regional commuter trips including service to downtown Seattle via the Sounder train in addition to bus service to local and regional cities operated by Sound Transit and Intercity Transit. The SR 512 Park & Ride provides bus connections for local and regional commuters to Lakewood Town Center, Tacoma, Puyallup, SeaTac, Dupont, and Olympia and communities throughout the South Sound. Pierce Transit, Sound Transit, and Intercity Transit provide bus service to the SR 512 Park & Ride via Pacific Highway and I-5, Bridgeport Way, and 108th Street SW. Both Lakewood Station and SR 512 Park & Ride provide vehicle parking for transit users.

The study area is bisected by an existing rail line on which Sounder and freight operate. While the rail line is an important regional transit and freight corridor, it has also created an environment with few roadway crossing opportunities which can affect vehicle operations and bicycle and pedestrian mobility.

Roadway Network

Exhibit 1. Transportation Features in the Extended Study Area



Source: Fehr & Peers, 2020; Pierce County, 2020.

Parking

There is very little on-street parking on arterials in the study area, but on-street parking exists on several local streets. Commercial uses tend to have dedicated parking lots adjacent to their buildings, frequently buffering the building from the street. St Clare Hospital provides off-street parking at multiple lots on the hospital’s campus. Parking intended for transit users is provided at the Lakewood Station garage (600 spaces) and at the SR 512 Park & Ride (493 stalls). During the daytime, the SR 512 Park & Ride is at about 90 percent capacity. Bicycle parking is provided at Lakewood Station garage via bicycle racks and lockers.

Transit Network

The Lakewood Station District Subarea is served by Pierce Transit, Intercity Transit, and Sound Transit, and includes Lakewood Station and the SR 512 Park & Ride. **Exhibit 2** shows routes serving the area and their associated headways, and Exhibit 3 shows transit pathways and bus stops. Lakewood Station serves six bus routes, providing connections to Tacoma, Puyallup, Olympia, Lacey, and Seattle. The SR 512 Park & Ride serves three bus routes providing connections to Tacoma, Puyallup, and Sea-Tac Airport. Bus stops are present along many of the study area’s arterials including Bridgeport Way, 108th Street SW, and Pacific Highway SW. Bus routes run at 15 to 30 minute headways (frequencies) throughout the peak periods, with routes 592 (DuPont-Seattle) and 612 (Olympia-Tacoma Dome Station) running most frequently. Off-peak headways vary substantially, ranging between 12 and 120 minutes.

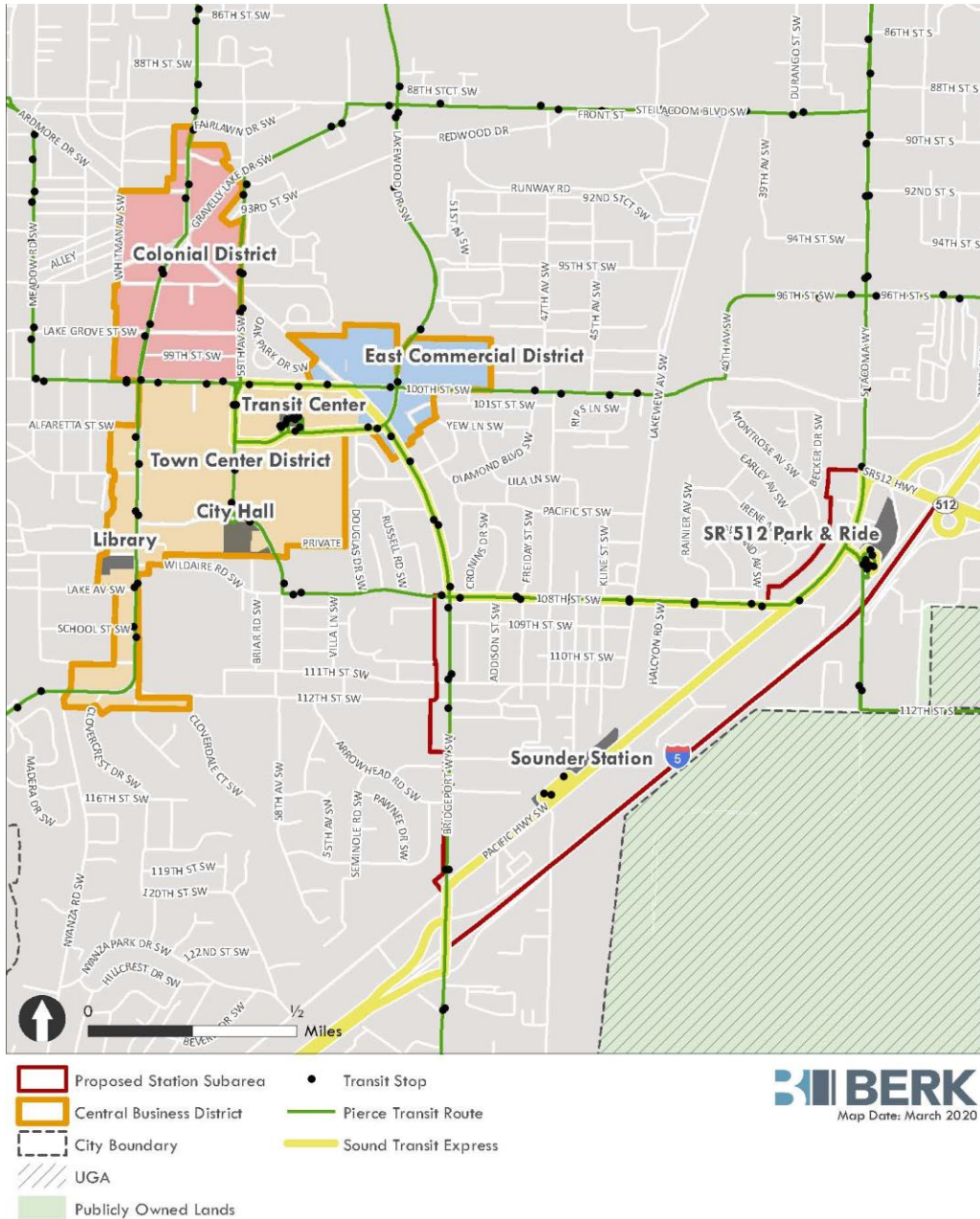
Sound Transit’s commuter train, the Sounder, has a stop at Lakewood Station in the southeast portion of the study area. The Sounder provides a regional transit connection between Lakewood, Tacoma, and Seattle during AM and PM commute times.

Exhibit 2. Existing Bus Routes.

ROUTE	DESTINATIONS	PEAK HEADWAY (IN MINUTES)	OFF-PEAK HEADWAY (IN MINUTES)
Sounder	Lakewood - Seattle	20	20-45
3	Lakewood to Downtown Tacoma	30	30
4	Lakewood to South Hill Mall	30	30
574	Lakewood to Sea-Tac Airport	30	30
580	Lakewood to Puyallup Station/South Hill Park and Ride	20	20-40
592	Dupont to Seattle	15	12-15
594	Lakewood to Seattle	20	20-30
612	Olympia to Tacoma Dome Station	15	40-120
620	Olympia to Tacoma Mall	30	60-90

Source: Pierce Transit, 2020; Intercity Transit, 2020; and Sound Transit, 2020.

Exhibit 3. Transit in the Extended Study Area



Source: Fehr & Peairs, 2020; Sound Transit; Pierce Transit; Intercity Transit

Non-Motorized Network

Marked bicycle lanes are located on Pacific Highway from Lakewood Station south to Gravelly Lake Drive SW and north from Sharondale Street SW to the South Tacoma Way/SR 512 interchange. Bicycle lanes are also located on 108th Street SW from Bridgeport Way to Pacific Highway. While sidewalks are generally located on all major streets in the project area, the residential neighborhood north of

Pacific Highway lacks sidewalks on most roadways. Due to the train tracks and lack of dedicated facilities, direct non-motorized connection from neighborhoods to Pacific Highway is limited. As a result, accessing Lakewood Station via bicycle or walking can also be challenging. Crossing opportunities are limited to the Lakewood Station pedestrian walkway, Bridgeport Way, and 108th Street SW. Although there is sidewalk infrastructure on Pacific Highway and Bridgeport Way, these roadways are high volume and high speed within the study area. This creates a more uncomfortable pedestrian experience that may discourage non-motorized use on these roadways.

Freight Network

The City's 2016 Comprehensive Plan identifies designated truck routes for freight as a transportation goal. Designated major truck streets are primary routes for goods movement throughout the city. Designation as a major truck street helps Lakewood's Public Works Transportation Division determine street design, traffic management plans, and pavement improvement projects that allow and facilitate the movement of larger vehicles along the designated street. Bridgeport Way and S Tacoma Way are designated as truck routes in WSDOT's Freight and Good Transportation System (FGTS) 2019 Update.¹ Exhibit 4 shows the FGTS routes that run through the study area.

¹ <https://www.wsdot.wa.gov/sites/default/files/2006/02/13/washington-freight-and-goods-transportation-system-2019.pdf>

Exhibit 4. FGTS Routes within the Study Area



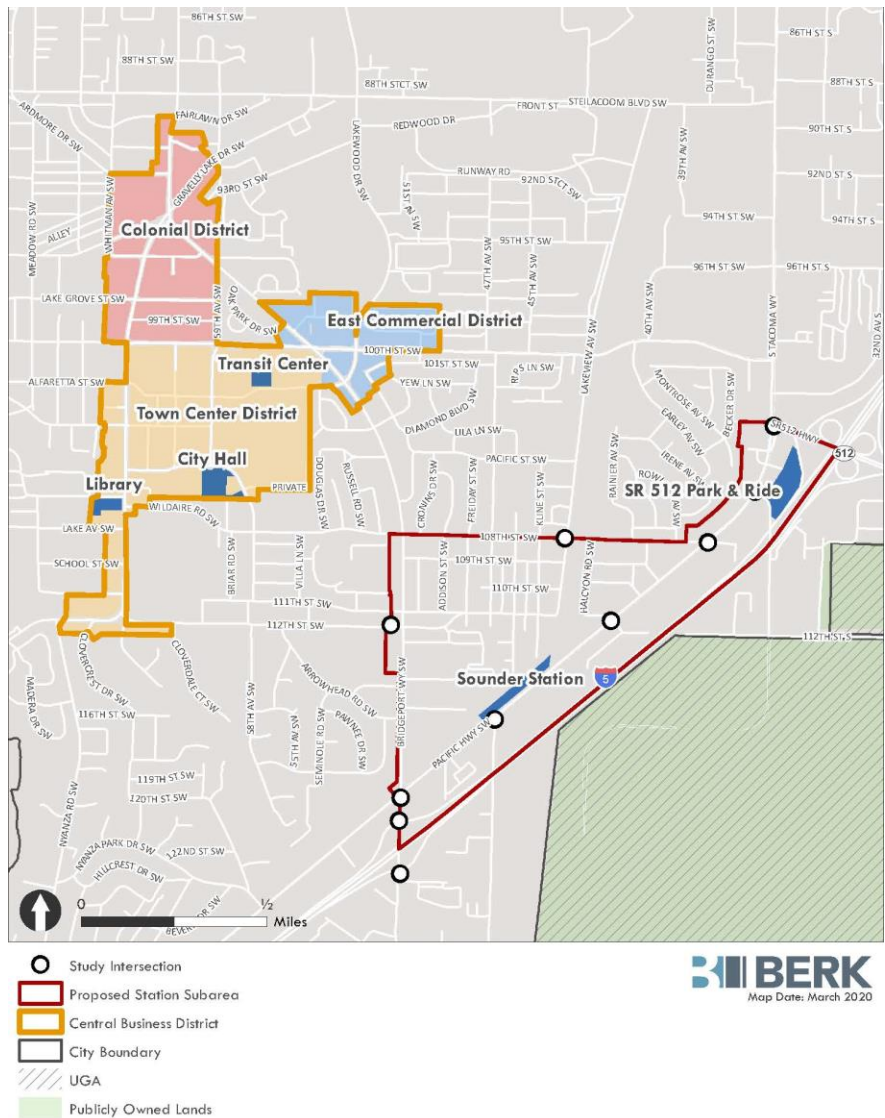
Source: WSDOT, 2019, Fehr & Peers, 2020.

Traffic Operations

Study Intersections

Within the project study area, traffic operations at ten locations were analyzed, as shown in **Exhibit 5**. At Lakewood Station, the garage entrance and north and south bus driveways were also analyzed, bringing the total number of study intersections to 12. These intersections are located on key roadway connections, including Pacific Highway, Bridgeport Way SW, and 108th Street SW and are most likely to be affected by potential land use changes.

Exhibit 5. Study Area Intersections



Source: Fehr & Peers, 2020.

Intersection Level of Service

The City uses PM peak hour average delay to evaluate traffic operations level of service (LOS) at its intersections. Traffic operations were analyzed using the Synchro software package. The Synchro network reflects the study area’s existing roadway network including segment and intersection geometry, signal timings, and recent traffic counts (2018-2020). For signalized, roundabout, and all-way stop controlled intersections, the LOS is based on the average delay for all approaches. For minor street stop controlled intersections, the LOS is based on the movement with the highest delay. **Exhibit 6** summarizes the LOS and delay thresholds specified in the Sixth Edition of the Highway Capacity Manual (HCM), which is a standard methodology for measuring intersection performance.

The Transportation Element designates level of service guidelines for the city’s arterial streets and intersections. Within the study area, that City sets a standard of LOS D during the weekday PM peak hour at all arterial street intersections. However, according to Policy T-20.5, the City may allow minor street stop-controlled intersections to operate below that LOS standard if those instances are thoroughly analyzed from an operational and safety perspective.

Exhibit 6. LOS/Delay Thresholds for Signalized and Unsignalized Intersections.

LOS	SIGNALIZED INTERSECTIONS (DELAY IN SECONDS)	UNSIGNALIZED INTERSECTIONS (DELAY IN SECONDS)
A	≤ 10	≤ 10
B	> 10 to 20	> 10 to 15
C	> 20 to 35	> 15 to 25
D	> 35 to 55	> 25 to 35
E	> 55 to 80	> 35 to 50
F	> 80	> 50

Source: Highway Capacity Manual (Transportation Research Board, 2017).

This study considers 12 intersections, 11 of which are signalized. **Exhibit 7** summarizes the existing intersection LOS at the study intersections. The level of service analysis suggests that automobiles generally move through the study area with acceptable levels of delay during the PM peak period. All study intersections operate at the City’s LOS D standard or better, although some approaches may operate with higher delay. Most intersections operate at LOS C or higher, which represents stable conditions with moderate congestion levels for an urban area. Three intersections - South Tacoma Way & SR 512, Pacific Highway & 108th Street, Pacific Highway & Bridgeport Way all operate at LOS D during the PM peak period, which indicates traffic conditions are approaching unstable flow.

As mentioned above, these are intersections that are affected by regional travel patterns, such as afternoon commute congestion stemming from I-5. According to local stakeholders, during certain congestion events on I-5 roadway users may be using Pacific Highway to bypass interstate traffic.

Exhibit 7. Existing PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/35
2	Pacific Highway / South Tacoma Way ¹	Signal	C/21
3	Pacific Highway / 108 th Street ¹	Signal	D/37
4	Pacific Highway / Sharondale Road	TWSC ²	C/23
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/6
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/12
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/18
8	Pacific Highway / Bridgeport Way	Signal	D/41
9	108 th Street / Lakeview Drive	Signal	B/14
10	Bridgeport Way / 112 th Street	Signal	C/27
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	B/16
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	B/18

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

FUTURE PLANS

Transportation Improvements

This section describes existing local area plans and planned improvements to the transportation network. Some long-range plans identify strategies for the development of the subarea, and others provide specific roadway improvements such as bicycle and pedestrian facilities. Future transportation improvements will be incorporated into the No Action and Planned Action alternatives developed as part of this project.

Lakewood Comprehensive Plan (2019)

Last revised in 2019, the Comprehensive Plan articulates the City’s vision for its future for the next 20 years. The City envisions the Lakewood Station District Subarea as a combination of transit oriented commercial, neighborhood commercial, public and institutional land, and a mix of multifamily land uses. LSDS is designated as a Regional Growth Center under Puget Sound Regional Council’s VISION 2040.

Transportation Element (2019)

The transportation element of the Comprehensive Plan provides policy direction to address local and regional mobility. The transportation element acknowledges the increase of traffic congestion within Lakewood and seeks to mitigate it by developing a balanced multimodal system that effectively moves people, goods, and services without compromising community character. The Plan specifically strategizes for the incorporation of non-motorized facilities, enhanced illumination, and other pedestrian amenities into new development designs.

Six-Year Transportation Improvement Program: 2020-2025

The Six-Year comprehensive Transportation Improvement Program (TIP) for 2020-2025 was approved by the Lakewood City Council in June 2020. This document outlines short- and long-term road projects – including the addition of new sidewalks, curb, gutter, stormwater improvements and road overlays – throughout the city. The following projects are identified in the TIP.

- Lakewood Station Non-Motorized Access Improvements – sidewalks and street lighting per the Non-Motorized Plan and Sound Transit Access Improvement Study
- Kendrick Street SW from 111th Street SW to 108th Street SW – sidewalks, street lighting, bicycle facilities
- 111th Street SW and 112th Street SW bicycle and pedestrian improvements
- 47th Avenue SW bridge improvements supporting non-motorized mobility

Additional projects included in the TIP are discussed in the No Action alternative section.

Sound Transit System Access Fund 2019

The Sound Transit Board of Directors awarded System Access Funds in September 2019 for various non-motorized improvements intended to facilitate connection to transit services. Some of these projects have also been included on the City's Transportation Improvement Program. Among these projects are proposed improvements on 111th Street SW and 112th Street SW in Lakewood. These improvements include curb, gutter, sidewalks, bicycle lanes, street lighting, pavement overlay, and associated storm drainage on both sides of these two streets between Bridgeport Way SW and Kendrick Street SW. Kendrick Street SW provides direct access to the Lakewood Sounder Station via a pedestrian bridge. Although this project will not have a direct impact on traffic operations at the study intersections, it represents a substantial improvement to the non-motorized network within the study area.

Destination 2040: Pierce Transit Long Range Plan Update

Destination 2040 is Pierce Transit's first Long Range Plan. This plan was originally adopted in 2016 and is currently being updated with the most recent available draft released in February 2020. Under this plan, two new Bus Rapid Transit (BRT) routes are being proposed that include services within the study area in 2026 and in 2030.

- **Downtown Tacoma to Lakewood BRT Line:** This BRT line would run from Downtown Tacoma, through the South Tacoma Sounder Station and along South Tacoma Way/Pacific Highway to the SR 512 Park & Ride. The line would then run along 108th Street SW toward the Lakewood Mall Transit Center. It is anticipated that this line would begin service in 2026.

- **Lakewood to South Hill BRT Line:** This BRT line would replace the existing Route 4, which currently runs along South Tacoma Way south of the SR 512 Park & Ride before running east/west along 112th Street SW towards the South Hill Mall Transit Center in Puyallup. It is anticipated that this line would begin service in 2030.

KEY FINDINGS AND PLANNING IMPLICATIONS

- All intersections operate at LOS D or better in current conditions.
- Planning for multimodal improvements connecting to Lakewood Station is programmed for implementation within the next 6 years.
- The area is well-served by transit with two existing hubs at Lakewood Station and the SR 512 Park & Ride. Both Sound Transit and Pierce Transit are enhancing transit connectivity and mobility at Lakewood Station over the next decade.

ALTERNATIVES EVALUATION

No Action Alternative

The No Action Alternative serves as the baseline for the impact analysis of the Planned Action Alternative. It represents the operation of the transportation system if no zoning or network changes were made in the Study Area. However, some growth would continue to occur under the No Action Alternative. The travel model used to forecast vehicle volumes for the No Action Alternative is consistent with the Planned Action model developed for the Lakewood Downtown Plan EIS.

This section summarizes analysis results and environmental impacts of the No Action Alternative. Specifically, the following definitions are used to identify impacts under the No Action Alternative:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection.
- **Transit:** average vehicle delay below LOS D at a study intersection through which transit routes travel.

Pedestrian, bicycle, and parking impacts are discussed qualitatively. As defined above, this EIS identifies impacts if future transportation operations under the No Action Alternative are not expected to meet the City's adopted level of service standards.

Intersection Level of Service

Exhibit 8 summarizes the average vehicle delay for each study intersection. By 2035, traffic volumes would increase due to the land use growth that would occur within the Study Area itself as well as citywide and regional growth not associated with the Study Area. Therefore, delay at individual intersections is also expected to increase although changes to the future land use are moderate. Of the 12 study intersections, six are expected to drop by at least one LOS grade compared to existing conditions, though only one intersection is expected to operate below the City's LOS D standard.

The Pacific Highway/Sharondale Road intersection is anticipated to operate at LOS F, which is below the City standard. It should be noted that the City has a policy stating that it may allow minor street stop-

controlled intersections (such as this intersection) to operate below the LOS D standard if they are thoroughly analyzed from an operational and safety perspective.

Exhibit 8. 2035 No Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/50
2	Pacific Highway / South Tacoma Way ¹	Signal	D/41
3	Pacific Highway / 108 th Street ¹	Signal	D/46
4	Pacific Highway / Sharondale Road	TWSC ²	F/>100
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/14
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	B/19
8	Pacific Highway / Bridgeport Way	Signal	D/54
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/35
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	D/41
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/29

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

Vehicle queuing analysis was completed at select locations to determine potential impacts to state highways. Although the overall intersection LOS meets the City standard, queuing at Bridgeport Way & the I-5 southbound off-ramp is expected to extend from the southbound off-ramp onto I-5, which could impact traffic flow, including buses, on I-5 near this interchange.

Transit

No transit impacts are expected under the No Action Alternative.

Pedestrian and Bicycle

Under the No Action Alternative, the pedestrian and bicycle network would remain similar to its current state. The City plans to implement sidewalk projects on 47th Avenue SW, 111th and 112th Streets SW, Kendrick Street SW, and 115th Street SW. The City also plans to implement bicycle facility projects on 47th Avenue SW, 108th Street SW, 111th and 112th Streets SW, and Kendrick Street SW. Many of these projects combine pedestrian and bicycle improvements as part of the same project and may also include improvements to roadway lighting. The 47th Avenue SW project enhances non-motorized connectivity over the existing I-5 bridge between Pacific Highway SW and Lakewood Station to the neighborhood on the opposite side of I-5.

Development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations so no significant adverse impacts to pedestrians or bicycles are identified under the No Action Alternative. A full list of known planned pedestrian and bicycle projects is below.

Pedestrian Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Sidewalks
- 111th & 112th Street SW - Bridgeport Way to Kendrick Street SW – Construct sidewalks to fill gaps in pedestrian network
- 112th Street SW from Gravelly Lake Drive SW to Bridgeport Way SW – New Sidewalks
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Sidewalks
- 115th Court SW from Bridgeport Way SW to Kendrick Pedestrian Bridge – New Sidewalks and pedestrian lighting

Bike Improvements

- 47th Avenue SW from 121st Street SW to Pacific Highway – New Bike Lanes
- 108th Street SW from Bridgeport Way SW to Lakeview Avenue SW – New Bike Lanes
- 111th Street SW Bike Lanes – Bridgeport Way SW to 112th Street SW
- 112th Street from Gravelly Lake Drive SW to Bridgeport Way SW – New Bike Lanes
- Kendrick Street SW from 111th Street SW to 108th Street SW – New Bike Facilities

Parking

Off-street parking is expected to be provided for residential, retail and other uses throughout the Study Area. Residences in the study area generally have their own off-street parking, allowing the roadway right-of-way to be reallocated to other modes as needed. New developments in the Study Area would be required to meet the City's parking requirements, so no adverse impacts to parking are expected.

Safety

Traffic volumes in the Study Area are projected to increase by 2035. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase. Therefore, no safety impacts are identified under the No Action Alternative. In fact, some of the projects planned that improve or construct new pedestrian and bicycle facilities may result in a safety benefit.

Planned Action Alternative

One Planned Action Alternative was developed which incorporates an increase in residential and retail land use at the Lakewood Landing and within the study area as compared to the No Action Alternative. These land use changes would result in an increase in the number of trips made within the Study Area; vehicle trips are expected to increase by 700 compared to the No Action Alternative. This section summarizes analysis results and environmental impacts of the Planned Action Alternative. Specifically, the following definitions are used to identify intersection impacts:

- **Auto and Freight:** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.
- **Transit (applicable to study intersections through which transit routes travel):** average vehicle delay below LOS D at a study intersection that operated acceptably under the No Action Alternative; or an increase in delay of at least 5 seconds at a study intersection already expected to operate below LOS D under the No Action Alternative.

Pedestrian, bicycle, safety, and parking impacts are discussed qualitatively in comparison to the No Action Alternative. An impact is defined if a project would preclude or fail to implement a City-identified bicycle or pedestrian improvement.

Intersection Level of Service

Exhibit 9 summarizes level of service and average vehicle delay for each study intersection. The Planned Action Alternative’s proposed land use growth would result in increased volumes compared to the No Action Alternative. There were no major changes to roadway or intersection capacity between the No Action and Planned Action scenarios. As defined above, impacts are evaluated in comparison to the No Action Alternative. The following significant auto and freight impacts are expected under the Planned Action Alternative.

- Intersection 4 – Pacific Highway / Sharondale Road SW – Increase in delay, LOS F to LOS F
- Intersection 8 – Pacific Highway / Bridgeport Way – from LOS D to LOS E
- Intersection 11 – Bridgeport Way / SB I-5 Ramp – from LOS D to LOS E

Because bus routes pass through all of the aforementioned intersections, these intersections are also considered as significant transit impacts. Potential measures to mitigate the impacts on these intersections are presented in the Mitigation Measures section.

Exhibit 9. 2035 Planned Action PM Peak Hour Intersection Level of Service and Delay.

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
1	South Tacoma Way / SR 512 ¹	Signal	D/52
2	Pacific Highway / South Tacoma Way ¹	Signal	D/42
3	Pacific Highway / 108 th Street ¹	Signal	D/48
4	Pacific Highway / Sharondale Road SW	TWSC	F/>100

ID	INTERSECTION	TRAFFIC CONTROL	LOS/DELAY
5	Pacific Highway / Sounder Station Garage Entrance ¹	Signal	A/7
6	Pacific Highway / Sounder Station North Transit Access ¹	Signal	B/16
7	Pacific Highway / Sounder Station South Transit Access ¹	Signal	C/21
8	Pacific Highway / Bridgeport Way	Signal	E/78
9	108 th Street / Lakeview Drive	Signal	C/24
10	Bridgeport Way / 112 th Street	Signal	D/41
11	Bridgeport Way / SB I-5 Ramp ¹	Signal	E/60
12	Bridgeport Way / NB I-5 Ramp ¹	Signal	C/31

1. This intersection required the use of HCM 2000 methodology, due to non-standard traffic signal phasing.

2. TWSC = Two way stop control

Source: Fehr & Peers, 2020.

Vehicle Queuing

In the Planned Action Alternative, queuing is expected to extend onto I-5 at Bridgeport Way and the I-5 southbound off-ramp, and at Bridgeport Way and the I-5 northbound off-ramp. Queuing at the southbound off-ramp was identified as an impact under the No Action Alternative, with the 95th percentile queue length expected to increase substantially with the Planned Action Alternative. Queuing on the I-5 northbound off-ramp was not expected to be an issue under the No Action Alternative, but substantial queuing is expected under the Planned Action Alternative. Therefore, queuing impacts to I-5 are expected under the Planned Action Alternative.

Pedestrian and Bicycle

The pedestrian and bicycle environment will be similar to the No Action Alternative. However, several local roadways are expected to be redesigned to better accommodate walking and bicycling through the implementation of street typology design proposed as part of this project. These typologies will reallocate right of way on residential roadways to sidewalks, bicycle facilities, and other improvements that enhance the non-motorized experience in these areas. Because of these additional benefits of the Planned Action Alternative and because development is expected to meet the City design standards related to bicycle and pedestrian facility accommodations, no significant adverse impacts to pedestrians and bicycles are identified under the Planned Action Alternative.

Parking

Select residential streets in the study area have been identified to be redesigned to better accommodate non-motorized travel through application of street typologies. As part of this effort, on-street parking may be removed or changed to other modes and uses. Many households in this area currently have off-

street parking, and parking demand may be lower in the future given the study area’s proximity to transit facilities. New developments in the study area would be required to meet the City’s parking requirements. Therefore, no significant impacts to parking are identified under the Planned Action Alternative.

The City requires bicycle parking in LMC 18A.80.030 for commercial and multifamily uses.

Safety

Traffic volumes in the Study Area are projected to increase under the Planned Action Alternative compared to the No Action Alternative. With higher volumes, there is potential for an increased number of collisions. However, there is no indication that collision rates at intersections or along segments would increase meaningfully compared to the No Action Alternative. Bicycle and pedestrian improvements may improve roadway safety conditions for the Planned Action Alternative. Therefore, no significant adverse impacts to safety are identified under the Planned Action Alternative.

Summary of Impacts

Exhibit 10 summarizes the significant impacts for each alternative.

Exhibit 10. Summary of Transportation Impacts

TYPE OF IMPACT	NO ACTION	PLANNED ACTION
Auto and Freight	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Transit	1 intersection and I-5 SB off-ramp queuing impact	3 intersections and I-5 NB and SB off-ramp queuing
Pedestrian	None	None
Bicycle	None	None
Parking	None	None
Safety	None	None

Source: Fehr & Peers, 2020.

Mitigation Measures

This section identifies a range of potential mitigation strategies that could be implemented to help reduce the significance of the adverse impacts identified for four intersections in the Planned Action alternative.

Managing demand for auto travel is an important part of mitigating the traffic congestion impacts. Transportation demand management (TDM) strategies could include subsidies or discounts for non-auto travel, education and assistance to help travelers identify non-auto commute options, rideshare and ridematch promotion, and local incentive and reward programs.

Washington state Commute Trip Reduction (CTR) law focuses on employers with 100 or more employees whose shifts begin during the typical AM commute. This law requires employers to develop commute trip

reduction plans and work toward meeting their mode share targets through internal programs and monitoring.

The City of Lakewood included policies aimed at managing auto travel demand in its Comprehensive Plan. The policies call for the City to encourage and assist employers who are not affected by the CTR law to offer TDM programs on a voluntary basis, encourage large employers to offer flexible or compressed work schedules to reduce localized congestion, and implement a public awareness and educational program to promote transportation demand management (TDM) strategies.

The potential TDM strategies discussed here could be implemented regardless of which alternative is selected. With a robust TDM program in place, it is expected that actual trip generation in the Study Area could be lowered beyond the levels analyzed in the impacts section of this EIS.

Other Proposed Mitigation Measures

The City could make signal timing improvements to increase the efficiency of impacted intersections and roadways in the Study Area. Exhibit 11 describes potential improvements to the impacted study intersections. Improvements generally involve changes that will not require right of way acquisition or widening, rather only changes to signal timing. The exception is intersection 4, which is currently all-way stop controlled, where a signal could improve operations if the volumes meet a signal warrant. With the proposed mitigation measures all intersections are expected to operate at LOS D or better. The City is planning to include improvements at the intersections of Bridgeport Way with the I-5 NB/SB ramps in their upcoming Transportation Improvement Program. While queuing at these locations may still occur, the improvements are expected to mitigate the effects of the Planned Action Alternative.

Exhibit 11. Proposed Mitigation Measures

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
4	Pacific Highway / Sharondale Road			
	Install signal using existing channelization and signal length consistent with adjacent intersections	F>100	F>100	A/4
8	Pacific Highway / Bridgeport Way			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way	D/54	E/78	D/49

ID	INTERSECTION	NO ACTION	ALT 1	ALT 1 MITIGATED
	from 108th to the NB I-5 Ramp. Change signal phasing for the northbound right turn to include an overlap with the eastbound left turn phase.			
11	Bridgeport Way / SB I-5 Ramp			
	Signal timing revisions, including optimizing cycle lengths and coordination offsets on Bridgeport Way from 108th to the NB I-5 Ramp.	D/41	E/60	D/52

Source: Fehr & Peers, 2020.

Therefore, with implementation of the potential signal timing mitigation measures outlined in the previous section and future improvements at the Bridgeport Way/I-5 ramps, the magnitude of the impacts could be mitigated such that auto, freight, and transit are not significantly impacted.

Development in the study area would provide information about its expected trips and will provide a traffic study where applicable per LMC 12.09.028. This analysis can affirm the mitigation approach and phasing, such as signal timing at Bridgeport Way/I-5 on and off ramps.

Encouraging bike usage would reduce transportation demand on local roads and state highways. The City could continue applying its required bicycle parking standards by use in 18A.80.030. The City could consider alternative standards or incentives in developments in the planned action or form-based code.

ATTACHMENT D

Amendments to LMC Title 18A

18A.10.140 Establishment of subareas.

Per RCW 36.70A.08 0(2), in order to plan for and regulate the use of land and structures in a manner which recognizes that residential neighborhoods and business areas within Lakewood vary one from another in desired character, subareas may be established as optional elements of the comprehensive plan and implementing zoning regulations may be adopted as a title of the Lakewood Municipal Code (LMC). Subarea plans are implemented in part through the adoption of use, development, performance, or procedural regulations specific to the subarea or to a portion or portions of the subarea. Regulations which are specific to a subarea or portions of a subarea are located in the title of the LMC concerning the subarea.

The following subareas and subarea plans are established:

Name	Symbol	Code Title
Downtown Subarea Plan	DSAP	18B

~~This section is reserved Lakewood Station District, Reserved, LDS, Reserved, 18C~~

18A.20.208 Applicability – Nonconformities.

A. This article shall apply to legally existing nonconformities, except the following items, which shall be governed by the standards set forth in the chapters identified below:

~~1. 1. Nonconforming uses and structures within the Downtown Subarea Plan, as governed in LMC Title 18B.~~

~~2. 2. Nonconforming uses and structures within the Lakewood Station District Subarea, as governed in LMC Title 18C.~~

~~3. 3. Nonconforming signs as defined in Chapter 18A.100 LMC, Signs.~~

~~4. 4. Nonconforming wireless service facilities as defined in Chapter 18A.95 LMC, Wireless Service Facilities.~~

~~5. 5. Nonconforming mobile home parks as defined in LMC 18A.40.110(C), Manufactured Home Parks. Manufactured and mobile home parks which were legally approved prior to the effective date of this title may continue to exist; provided, that the density of the park does not increase over the number of dwelling units legally existing on the effective date of this title. Manufactured home sites within legally nonconforming manufactured home parks may continue to be used; provided, that the placement of newer manufactured homes does not result in encroachment of the dwelling beyond the lot space boundaries or into the right-of-way and fire code requirements for structure spacing are met.~~

ATTACHMENT E

Comprehensive Plan Text Amendments

The following sections of the City's Comprehensive Plan's text may be amended to reflect the adoption of the Lakewood Station District Subarea Plan, Form-Based Development Code and Planned Action via the 2021 Comprehensive Plan amendment cycle, which is scheduled to be completed and adopted by the City Council no later than December 31, 2021:

Sections 1.4.1, 1.4.3, 1.6.7, 2.3.5, 2.3.7, and 2.3.16, LU-17.3, Sections 3.3.3, 3.3.5, 4.1, 4.2, 4.4, 4.5, 4.5.2, 5.2.1, 5.2.7, 5.3.1, Goal UD-9, ED-5, and T-13, Sections 11.3.3 and 11.3.11, and Transportation Implementation Strategies.

ATTACHMENT F

Planning Commission public hearing comments and City responses:

Public Comment	City Response
<p>Coral Clark @ 11/18/20 public hearing and written comments: Parents and family members live at/near intersection of 109th and Kendrick St. Concern that ROW as depicted on LSDS maps runs through parent’s property. Request that Kline St. or 47th St. be converted to a “green street” instead of Kendrick St.</p>	<p>Comments acknowledged. Information regarding existing ROW widths near 109th and Kendrick St. provided to commenters and Planning Commission.</p>
<p>Jeremy Long @ 11/18/20 public hearing and written comments: Concerned about effects of LSDS Plan implementation on 10917 Kendrick St. Concurs with Coral Clark’s comments and recommendation that only sidewalks be added to green streets in order to reduce amount of ROW needed for improvements.</p>	<p>Comments acknowledged. Information regarding existing ROW widths near 109th and Kendrick St. provided to commenters and Planning Commission.</p>
<p>Mike Brandstetter @ 11/18/20 public hearing: Questioning whether intersection improvements list correct streets since Halcyon does not intersect with Pacific Highway. Stated need for subsidized housing to avoid displacement of current area residents.</p>	<p>Halcyon reference corrected to Sharondale Rd.</p>
Pierce County Planning and Public Works and Lakewood Water District Written Comments	
<p>Draft LSDS Plan page 3: is the 54 units per acre identified for TOC zone referring just to residential development within the zone?</p>	<p>Comment acknowledged. 54 units per acre refers to residential and mixed use projects.</p>
<p>Draft LSDS Plan page 4: A 20% reduction factor was applied to gross parcel area to account for storm and parking. An additional 20% reduction factor was applied to account for critical areas in general. Based on County GIS there appears to be a flood plain in the South TOC zoned area, so these reductions percentages seem valid. Does the City agree with this? If not what does the City use? I know that the City is hoping that the parking reduction will be less over time. Does the City have an anticipated timeline on when the parking reduction will start taking place?</p>	<p>Comment acknowledged. Parking reduction will happen over time and market demand and technological changes affect vehicle usage.</p>
<p>Draft LSDS Plan page 5: Diagram at bottom of page 5 depicting sewer line as JBLM line incorrect – it is a PC Sewer line. What is image depicting over utility easement?</p>	<p>Comment acknowledged and image removed.</p>
<p>Draft LSDS Plan page 7: Area on 1999 Station District Map south of I-5 Owned by the County and managed by OCE department (former road shop and gravel pit). Currently being marketed for distribution center use.</p>	<p>Comment acknowledged. City working with the County on sale of property.</p>
<p>Draft LSDS Plan page 11: Utilities and Public Services phrase “Sponsoring the upfront costs of infrastructure investment may be a strategy to support new development in the LSDS.”</p>	<p>Comment acknowledged. City works regularly with the County regarding utility capacity needs and improvements.</p>

<p>What does this mean? Lakewood would sponsor or is this utility sponsoring improvements? Pierce County currently does upgrades in a "just in time" model. Because of this we will need the City of Lakewood's help to determine when and where development will take place. Once we are able to determine when and where projects will be needed we will have to evaluate our finances and see if we can build these projects, if there will have to be some partnering between the City and County, or if some developers will have to build projects (if timelines hold). Also see Page 57.</p>	
<p>Draft LSDS Plan page 54:</p> <p>Wastewater service in Lakewood is provided by Pierce County Planning and Public Works. Most of the sewer pipes shown in Exhibit 24 were installed in the 1980s (some later) and are primarily 308-inch diameter PVC pipes, which have a 100-year lifetime. Larger interceptors are typically made of concrete, which has a service life of 50 to 100 years. Generally, the sewer infrastructure is considered in good condition with plenty of remaining service life and no current need for large scale line replacements or upgrades.</p>	<p>Comment acknowledged and edits incorporated.</p>
<p>Draft LSDS Plan page 57 and 60:</p> <p>"Work with public service and utility providers to ensure that capital improvement plans include the projects necessary to support development in the LSDS subarea."</p> <p>Does Lakewood believe that utilities should be upgrading their system ahead of developments? Can the City of Lakewood provide exhibits of where they believe that development will take place, in 5-6 year increments is preferred, so that utilities can evaluate what improvements are needed in this 20 year period being discussed. Once we are able to evaluate this the Sewer Division will be able to add it to our financial model and see if meeting the City's timeline is possible or if there needs to be some partnership between the City and County or Developer to build the project if the timelines do hold.</p>	<p>Comment acknowledged. City coordinating with County sewer utility regarding Unified Sewer Plan update and other sewer utility planning. City has notified County regarding anticipated capacity increase needs related to Lakewood Landing.</p>

ATTACHMENT G



STATE OF WASHINGTON

DEPARTMENT OF COMMERCE

1011 Plum Street SE • PO Box 42525 • Olympia, Washington 98504-2525 • (360) 725-4000
www.commerce.wa.gov

February 1, 2021

Donne Daniels, Chair
Lakewood Planning Commission
c/o Tiffany Speir, Long Range & Strategic Planning Manager
City of Lakewood
6000 Main Street SW
Lakewood, Washington 98499

RE: Lakewood Station District Subarea (LSDS) Plan and Hybrid Form-Based Development Code

Dear Mr. Daniels:

The proposed amendments to the Lakewood Station District Subarea (LSDS) Plan and Hybrid Form-Based Development Code were received by Growth Management Services on January 14, 2021, and given material identification number 2021-S-2258. In addition to providing notice pursuant to RCW 36.70A.106, this was also a deliverable in our grant program to increase residential building capacity. We appreciate that Lakewood took the opportunity to use this grant to carry out this subarea plan. We especially like the following:

- There are a number of incentives to encourage the development of multifamily housing to make highest and best use of land in the plan area, and allow many people to live near the station area. The Housing Incentive Program applies in this station area, specifically providing density bonuses, flexibility in development standards and fee reductions for affordable housing development. The flexibility in parking requirements near transit are also helpful to reduce development costs. The policies to help investors identify and navigate affordable housing financing programs will also help encourage and support investment.
- The plan addresses the potential displacement of current residents in the plan area through policies related to preservation of existing housing and redevelopment. For example, retaining the mobile home park for tiny homes on wheels is a good idea. There are few places for tiny homes to be legally located in a community, and they can provide an effective form of housing at very low cost to residents.

Donne Daniels, Chair

February 1, 2021

Page 2

- The policy to connect this area to the downtown with enhanced multimodal green streets is an efficient way to provide green space in linear parks, and provides safe and pleasant bicycle and pedestrian access to the station, and supports human health and sustainability goals. However, it is unclear how these green streets connect to a wider multimodal network across the city. You may want to consider through future planning and capital improvements how the citywide multimodal network and other infrastructure will support the subarea plan. We appreciate recent policy additions to the plan to consider tools such as development and latecomer agreements, and fee assessment districts to address infrastructure gaps.

We have a suggestion for strengthening your plan. It also appears that drive-through facilities are permitted, which may be inappropriate in an area where Lakewood is investing so much in supporting the public realm for bicycle and pedestrian transportation, and the region is investing in public transportation.

Congratulations to you and your staff for the good work these amendments represent. If you have any questions or concerns about our comments or any other growth management issues, please contact me at 360.259-5216. We extend our continued support to the City of Lakewood in achieving the goals of growth management.

Sincerely,



Anne Aurelia Fritzel
Senior Planner
Growth Management Services

AAF:lw

cc: Tiffany Speir, Esq., CPM, Long Range/Strategic Planning Manager, city of Lakewood
David Andersen, AICP, Managing Director, Growth Management Services
Steve Roberge, Deputy Managing Director, Growth Management Services
Ben Serr, AICP, Eastern Region Manager, Growth Management Services
Laura Hodgson, Associate Housing Planner, Growth Management Services

ATTACHMENT H



**PLANNING COMMISSION
REGULAR MEETING MINUTES
February 3, 2021
Zoom Meeting
6000 Main Street SW
Lakewood, WA 98499**

Call to Order

Mr. Don Daniels, Chair called the ZOOM meeting to order at 6:30 p.m.

Roll Call

Planning Commission Members Present: Don Daniels, Chair; Paul Wagemann, James Guerrero, Ryan Pearson, Connie Coleman-Lacadie, Phillip Combs

Planning Commission Members Excused: Nancy Hudson-Echols

Staff Present: Dave Bugher, Assistant City Manager for Development Services; Tiffany Speir, Long Range & Strategic Planning Manager

City Council Liaison: Paul Bocchi

Approval of Minutes

MOTION: To approve the January 20, 2021 meeting minutes as drafted.

SECONDED. PASSED 6 - 0.

Agenda Updates

At the request of Commissioner Guerrero, the presentation on parking was moved to occur prior to the action on the proposed Lakewood Station District Plan and Development Code.

Public Comments

This meeting was held virtually to comply with Governor Inslee's Emergency Proclamations 20-28 and its addendums. Citizens were encouraged to virtually attend and to provide written comments prior to the meeting. No public comments were received.

Public Hearings

None

New Business

Parking Requirements in Lakewood

Commissioner Guerrero provided a presentation to the Commission about current parking requirements in Lakewood's municipal code, analyses and predictions of how many parking spaces may be needed in the near future given costs and changes to transportation over time.

Unfinished Business

Lakewood Station District Subarea (LSDS) Plan and Hybrid Form-Based Code Update

Ms. Tiffany Speir provided responses to Commission questions raised on January 20 regarding parking requirements and housing affordability and the plan to create a Green Street along Occidental within the subarea boundaries. Ms. Lisa Grueter, BERK Consulting, was also present to answer questions.

The Planning Commission discussed the updated draft LSDS Plan and Development Code. Members Wagemann voiced their concerns over affordable housing and preventing displacement of current residents of the subarea as redevelopment occurs. They also voiced concerns regarding the costs for developers to provide parking versus housing affordability and how many parking spaces would actually be used.

MOTION: To recommend adoption of the Lakewood Station District Subarea Plan and Hybrid Form-Based Code as included in proposed Resolution 2020-01.

SECONDED. DISCUSSION.

MOTION TO AMEND: To amend the proposed LSDS Development Code as follows:

18C.600.610 Parking.

A. *Off-Street Parking Requirements.* The following off-street parking requirements supersede the requirements in Chapter 18A.80 LMC. Uses not listed below must comply with the requirements in Chapter 18A.80 LMC.

B.

18C-600-1. Off-Street Parking Requirements

Land Use	Vehicular Parking Requirement	Bicycle Parking Requirement
Residential	Single-family: 2 per dwelling unit Accessory dwelling: 1 per dwelling unit, provided that no additional parking is required when located within one-quarter mile of the Sounder Station. (RCW 36.70A.698) Senior citizen apartments: No minimum 1 per 3 dwelling units Multifamily housing: 1.25 spaces per dwelling unit No minimum	Meet rates and standards of: Chapter 18A.80
Retail, Services, Restaurants	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Office	2 per 1,000 GSF minimum; 3 per 1,000 GSF maximum	Meet rates and standards of: Chapter 18A.80
Street level retail 3,000sq.ft. or less per business	None where there is available public parking within 500' or abutting on-street parking designed to serve street level retail	Meet rates and standards of: Chapter 18A.80

B. *Parking Reductions or Increases.* The amount of required parking may be reduced or eliminated, or increased above the maximum, based on a site-specific parking study that demonstrates one or more of the following:

- Reduction Due to Shared Parking at Mixed-Use Sites and Buildings.* A shared use parking analysis for mixed-use buildings and sites that demonstrates that the anticipated peak parking demand will be less than the sum of the off-street parking requirements for specific land uses.
- Reduction Due to Public Parking Availability.* The availability of public parking to accommodate the parking demand generated by the site or building. The City may approve a reduction in the amount of required parking by up to 50 percent for any parking stalls that will be open and available to the public. On-street parking may be considered for the reduction; any new on-street parking provided will be counted toward the required parking availability.

3. *Reduction Due to Lower Parking Demand or Increase Based on Greater Parking Demand.* Demonstrating that anticipated parking demand will be less than the minimum parking required, or greater than the maximum allowed, shall be based on collecting local parking data for similar land uses on a typical day for a minimum of eight hours.

4. *Reduction for Housing in Proximity to Sounder Station (RCW 36.70A.620).* When located within one-quarter mile of the Sounder Station, an applicant may apply for an exception allowing minimum parking requirements to be reduced at least to one parking space per bedroom or 0.75 space per unit, as justified through a parking study prepared to the satisfaction of the Community Development Director or their designee:

- a. housing units that are affordable to very low-income or extremely low-income individuals.
- b. housing units that are specifically for ~~seniors or~~ people with disabilities.
- ~~c. market rate multifamily housing.~~

In determining whether to grant a parking reduction, the Community Development Director may also consider if the project is proposed in an area with a lack of access to street parking capacity, physical space impediments, or other reasons supported by evidence that would make on-street parking infeasible for the unit.

C. *Parking Location and Design.* Parking shall be located behind the [building](#) or in a [structure](#) except in locations where the parking frontage type is [permitted](#).

D. *Shared Parking.* Shared parking is encouraged to support a walkable and pedestrian-oriented Station District where people can park once and visit multiple destinations. Off-site shared parking may be authorized per the standards in Chapter [18A.80](#) LMC.

E. *Public Parking.* Public parking is permitted as a principal or accessory use in the Station District subject to the frontage and design standards.

F. *Dimensional Standards.* Parking stall and circulation design shall meet the standards of Chapter [18A.80](#) LMC.

SECONDED. MOTION TO AMEND PASSED 5-0 with Commissioner Combs abstaining.

ACTION ON ORIGINAL MOTION:

To recommend adoption of the Lakewood Station District Subarea Plan and Hybrid Form-Based Code as included in proposed Resolution 2020-01 and amended, provided a new finding of fact is also added explaining the Planning Commission's concern regarding housing affordability needs and risk of displacement in the subarea.

PASSED 5-0 with Commissioner Combs abstaining.

The City Council would begin consideration of the LSDS Plan, Development Code and SEPA Planned Action on March 8.

Report from Council Liaison

Councilmember Bocchi updated the Commission on the ongoing search for a new City Councilmember following Councilmember Simpson’s resignation at the end of 2020. Commissioner Guerrero offered to present his information on parking in Lakewood to the City Council; Councilmember Bocchi state he would bring the offer to the Council.

Reports from Commission Members and Staff

Future Agenda Topics

February 17: Annual Housing Report and introduction of proposed 2021 Comprehensive Plan amendment 2021-01, a new Energy & Climate Change Chapter
March 3: 2021 Comprehensive Plan Amendments Review

Regional Planning Land Use Updates


None

Other

None

Next Regular Meeting: The next regular meeting would be held on March 3, 2021.

Meeting Adjourned at 7:55 p.m.



Don Daniels, Chair
Planning Commission 02/17/2021



Karen Devereaux, Recording Secretary
Planning Commission 02/17/2021

REQUEST FOR COUNCIL ACTION

DATE ACTION IS REQUESTED: April 5, 2021	TITLE: Amending Lakewood Municipal Code Chapter 9.06 related to Controlled Substances.	TYPE OF ACTION: <input checked="" type="checkbox"/> ORDINANCE NO. 750 <input type="checkbox"/> RESOLUTION NO. <input type="checkbox"/> MOTION NO. <input type="checkbox"/> OTHER
REVIEW: March 22, 2021	ATTACHMENTS: Draft Ordinance	

SUBMITTED BY: Heidi Ann Wachter, City Attorney

RECOMMENDATION: It is recommended that the City Council adopt an Ordinance amending the Lakewood Municipal Code Chapter 9.06 related to Controlled Substances.

DISCUSSION: The Washington State Supreme Court issued a ruling on February 25, 2021 that rendered the state’s current drug possession law unconstitutional. The state statute does not require intent; possession itself is sufficient to support arrest and ultimately conviction. The ruling left the state without a drug possession law unless and until a remedy is legislated.

Currently, arrests for drug possession are not being made because there is no law in effect upon which to base such arrest. Previously, such arrests would be referred to the Pierce County Prosecuting Attorney because possession of any controlled substance other than marijuana has been a felony; there has previously been no misdemeanor possession of heroin, methamphetamine or other controlled substance.


County prosecutors are reviewing previous cases to reverse convictions under the previous law. Even with new law, individuals convicted under the previous law are eligible for relief from any conviction. Any new law will not be retroactive to previous arrests. The City had no authority to file charges under the previous law, only the County. While the City may have made the arrest, any further hold was the result of County action.

The State Legislature has introduced SB 5468 to remedy the constitutional defect identified by the Washington State Supreme Court. The bill is a simple fix in that it adds the word “knowingly” to state law, which adds the intent element missing from the previous law. As of today, the bill has yet to be scheduled for a hearing.

<https://app.leg.wa.gov/billsummary?BillNumber=5468&Year=2021&Initiative=false>

ALTERNATIVE(S): The City could wait for the legislature to enact new law.

FISCAL IMPACT: None.

Heidi Ann Wachter Prepared by _____ Department Director	 _____ City Manager Review
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ORDINANCE NO. 750

AN ORDINANCE of the City Council of the City of Lakewood, Washington, amending Chapter 9.06 of the Lakewood Municipal Code relating to Controlled Substances.

WHEREAS, the City of Lakewood finds that laws prohibiting possession of illegal drugs are a valuable tool to law enforcement in preserving public safety in the City; and

WHEREAS, the Supreme Court of the State of Washington has declared illegal and rendered useless the state law prohibiting possession of illegal drugs due to the deficiency in the state law of requiring the element of intent; and

WHEREAS, the state legislature has failed to immediately remedy this deficiency; and

WHEREAS, the City of Lakewood has the authority to adopt laws to protect public safety to the extent such laws are not in conflict with State or Federal law; and

WHEREAS, loss of the State law hinders effective law enforcement in the City of Lakewood;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF LAKEWOOD, WASHINGTON, DO ORDAIN as Follows:

Section 1. That Chapter 9.06 of the Lakewood Municipal Code is amended to add a new section, 9.06.005 Possession of controlled substance, to read as follows:

Section 9.06.005 Definitions

(1) RCW 69.50.101(e) is adopted by reference. "Controlled substance" means a drug, substance, or immediate precursor included in Schedules I through V as set forth in federal or state laws, or federal or commission rules.

Section 2. That Chapter 9.06 of the Lakewood Municipal Code is amended to add a new section, 9.06.045 Possession of controlled substance, to read as follows:

Section 9.06.045 Possession of controlled substance

(1) It is unlawful for any person to knowingly possess a controlled substance unless the substance was obtained directly from, or pursuant to, a valid prescription or order of a practitioner while acting in the course of his or her professional practice, or except as otherwise authorized by this chapter.

(2) Any person who violates this section is guilty of a gross misdemeanor punishable pursuant to LMC 9.06.080.

(3)(a) The possession, by a person twenty-one years of age or older, of useable marijuana, marijuana concentrates, or marijuana-infused products in amounts that do not exceed those set forth in RCW 69.50.360(3) is not a violation of this section.

(b) The possession of marijuana, useable marijuana, marijuana concentrates, and marijuana-infused products being physically transported or delivered within the state, in amounts not exceeding those that may be established under RCW 69.50.385(3), by a licensed employee of a common carrier when performing the duties authorized in accordance with RCW 69.50.382 and 69.50.385, is not a violation of this section.

(4)(a) The delivery by a person twenty-one years of age or older to one or more persons twenty-one years of age or older, during a single twenty-four hour period, for noncommercial purposes and not conditioned upon or done in connection with the provision or receipt of financial consideration, of any of the following marijuana products, is not a violation of this section:

(i) One-half ounce of useable marijuana;

(ii) Eight ounces of marijuana-infused product in solid form;

(iii) Thirty-six ounces of marijuana-infused product in liquid form; or

(iv) Three and one-half grams of marijuana concentrates.

(b) The act of delivering marijuana or a marijuana product as authorized under this subsection (4) must meet one of the following requirements:

(i) The delivery must be done in a location outside of the view of general public and in a nonpublic place; or

(ii) The marijuana or marijuana product must be in the original packaging as purchased from the marijuana retailer.

(5) No person under twenty-one years of age may knowingly possess, manufacture, sell, or distribute marijuana, marijuana-infused products, or marijuana concentrates, regardless of THC concentration. This does not include qualifying patients with a valid authorization.

(6) The possession by a qualifying patient or designated provider of marijuana concentrates, useable marijuana, marijuana-infused products, or plants in accordance with chapter 69.51A RCW is not a violation of this section.

Section 3. Severability. If any portion of this Ordinance or its application to any person or circumstances is held invalid, the remainder of the Ordinance or the application of the provision to other persons or circumstances shall not be affected.

Section 4. Effective Date. That this Ordinance shall be in full force and effect thirty (30) days after publication of the Ordinance Summary.

ADOPTED by the City Council this 5th day of April, 2021.

CITY OF LAKEWOOD

Attest:

Don Anderson, Mayor

Briana Schumacher, City Clerk


Approved as to Form:

Heidi Ann Wachter, City Attorney



TO: Mayor and City Council

FROM: Mary Dodsworth, Parks, Recreation and Community Services Director

THROUGH: John Caulfield, City Manager 

DATE: April 5, 2021

SUBJECT: City / CPSD Cooperative Use Agreement

ATTACHMENTS: DRAFT Cooperative Use Agreement

Summary: City of Lakewood (City) and Clover Park School District (CPSD) representatives met to discuss cooperative use of City and CPSD facilities with an intent to draft a cooperative use agreement. A draft document that addresses cooperative use along with how we will communicate or mitigate impacts due to local, regional or national issues is provided for your review and discussion.

Process: From April - June, 2019 representatives from the City and CPSD met to discuss a cooperative use agreement. Representatives included City and CPSD programming staff, leadership staff and Council and School Board members. The group discussed the purpose of the agreement, current issues, needs and concerns and future meeting topics. We reviewed other cooperative use agreements between districts and agencies. We noted that both agencies are protective of their facilities and that developing trust and supporting ongoing communication are keys to a successful partnership. All agreed that we have a positive working relationship and that memorializing current arrangements as well as providing updates based on changes in use and access to publically owned facilities was a good idea. This agreement would be used to educate representatives from each agency as well as the community regarding public use and accessibility of City and CPSD facilities.

The purpose and intentions of the agreement are:

- Efficient use of public resources
- Aligns w/current plans and policies
- Improves local or student services
- Provides more opportunities for community members (taxpayers)
- Creates a safe and healthy district and community
- Creates equity throughout the district and community

Various topics were discussed, clarified and included in the draft document:

- Scheduling
- Priorities
- Access and Supervision
- Impacts to staff, buildings and fields
- Responsibility and Liability
- Fees and Cost Sharing

The Lakewood City Council and the CPSD Board reviewed and discussed a draft document at a February 3, 2020 joint meeting. Questions were answered and the document was ready to move forward. And then the Covid pandemic occurred and each agency was responding to constantly changing health, safety and operational issues. Recently agency representatives came back together to clarify information in the draft document and included language that addresses how we will communicate or mitigate impacts due to local, regional or national issues, mandates or guidelines.

Staff will be at the April 5th Council meeting to provide an update and answer any questions.

**COOPERATIVE USE AGREEMENT
CLOVER PARK SCHOOL DISTRICT AND THE CITY OF LAKEWOOD
INTERLOCAL AGREEMENT**

This Interlocal Agreement, made this _____ day of _____, 20 __, is by and between Clover Park School District No 400, a Washington municipal corporation (hereinafter referred to as the "District"), and the City of Lakewood, a Washington municipal corporation (hereinafter referred to as the "City"), for the purpose of efficient use of public resources, improving local or student services and providing more opportunities for the community members they serve.

WHEREAS, The District recognizes that public school facilities are community facilities that should be utilized to their maximum potential for the benefit of all community members, the Board of Directors further recognizes that the primary purpose of these facilities is the educational program of the Clover Park School District. Within the guidelines specified by law, the superintendent or designee is authorized to develop administrative regulations and rental fees as applicable to administer the non-commercial use of school district facilities at times they are not in use by school district programs.

WHEREAS, The City has a mission of creating a healthy and vibrant community where opportunities for all are available through established programs, services, events and partnerships; and, within the guidelines specified by law, the City Manager or designee is authorized to implement City Council approved facility use policies and fees as applicable to use City facilities at times they are not in use for City purposes.

WHEREAS, the District and the City have shared facilities and resources for many years and have a positive history of cooperative use along with a desire to be effective and efficient stewards of public resources and assets; and,

WHEREAS, the District and the City have a mutual interest in supporting the needs of students, youth and families living in the City of Lakewood and surrounding areas; and,

WHEREAS, the District and the City are mutually interested in supporting programs and services at facilities located in and around the City of Lakewood; and

NOW, THEREFORE, pursuant to Chapter 39.34 "Interlocal Cooperation Act" of the Revised Code of Washington, the City and District agree as follows:

I. Purpose

- A. The City and District acknowledge that the intent of this Agreement is to address the community use of District and City facilities.
- B. This Agreement addresses the City and the District's desire to memorialize this partnership, clarify expectations and ensure consistency and continuity for current and future agency representatives.

II. General Provisions

- A. This Agreement shall be continuous, with procedures for modification to meet the needs of the District and City.

- B. The execution and modification of this Agreement must be authorized by the Clover Park School Board or designee and the Lakewood City Council or designee.
- C. The District and City shall act in good faith to implement the terms of this Agreement.
- D. Access to District and City facilities is subject to change based on local, state or national issues or disasters. The reopening of City or District facilities will follow local, state or national regulations, guidelines and best practices to ensure a healthy and safe environment.

III. Scheduling

A. Schedule: Clover Park School District

- i. The District shall create and maintain the master schedule of facility use.
- ii. The District will utilize their own current scheduling and reservation systems for scheduling their facilities. All efforts will be made to inform each other of any changes to the scheduling processes.
- iii. The District reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other uncontrollable circumstances. Efforts will be made to provide adequate notice as to not disrupt program continuity.

B. Schedule: City of Lakewood

- i. The City shall create and maintain the master schedule for City and District use of City Facilities.
- ii. The City will utilize their own current scheduling and reservation systems for scheduling their facilities. All efforts will be made to inform each other of any changes to the scheduling processes.
- iii. The City reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other uncontrollable circumstances. Efforts will be made to provide adequate notice as to not disrupt program continuity.

IV. Access and Priority Use

A. Access: Clover Park School District

- i. The schedule of available times for community use of District facilities, which are not in conflict with District use, shall be:
September-June (academic year)
Monday through Friday 6:00 P.M. to 8:30 P.M. or 30 min after school programs have ended and
Saturday and -as scheduled
June - August (summer months)
Monday through Friday 8:00 A.M. to 5:00 P.M.
Saturday as scheduled
- ii. Consideration will be given for special events.

B. Access: City of Lakewood

- i. The schedule of available times for community use of City park facilities
Monday – Sunday – 7:00 A.M. to dusk or 9:00 P.M. whichever is earlier.
The schedule of available times for community use of City Hall
Monday – Friday – 8:30 A.M. to 5:00 p.m.
Saturday – Sunday – as scheduled

C. Priority Use: Clover Park School District

- i. Clover Park School District owned athletic fields and facility priority shall be given to District sponsored programs. District programs and activities shall have the right to preempt other users upon giving advance notice, except in extraordinary circumstances when advance notice is not possible. The District reserves the right to add or remove facility options to accommodate district mission and vision.
- ii. Clover Park School District facility use is prioritized in the following order:
 - a. District sponsored school-related activities and events. District sponsored educational or interscholastic activities limited to student and school related functions will only incur charges if custodial or other personnel are required.
 - b. Non-Profit Organizations, Community Parks and Recreation, Youth-Oriented Community Service Groups. Community organizations promoting youth programs, local youth parks and recreation groups and affiliated organizations (PTSA/PTA, booster clubs, city, Girl/Boy Scouts, Boys & Girls Club, etc.) may be charged for incurred expenses for use of facilities, playfields, custodial overtime and/or personnel fees.
 - c. Fee-Based Non-Profit Organizations, Non-Community Parks and Recreation, Adult Service Groups, Religious and For-Profit/Commercial Groups. Meetings of religious, fee-based non-profit, and profit-making organizations or corporations whose gatherings are open to the public for which adequate facilities are not otherwise readily available in the community shall incur charges as required by the district fee schedule.
- iii. The District agrees to give the City second priority of District owned facilities with the exception of the following that will become available on a case by case basis, following regular scheduling policies and procedures:
 - a. Lakes High School; not included
 - b. Clover Park High School; not included
 - c. Harry Lang Stadium; not included
 - d. Thomas Middle School facility use available after the following dates:
 - 1. Gymnasium available after August 31, 2021
 - 2. Outdoor fields available after August 31, 2022
- iv. The City may request district facility use:
 - a. Elementary Schools ten (10) school days after the start of school
 - b. Middle Schools
 - 1. Fall activities after August 15
 - 2. Winter activities after October 15
 - 3. Late Winter activities after December 15
 - 4. Spring activities after February 15
- v. Requests are limited as follows; two (2) weekdays Monday through Thursday at four (4) or less elementary schools and two (2) or less middle school facilities per season. Friday and Saturday use scheduled as needed for games at one (1) middle school and one (1) elementary school per weekend. Requests outside of these dates will be given the same priority as traditional users. Consideration will be given to special requests.
- vi. In the event it is determined that facilities are not fully utilized there may be adjustments in allocation the following year.

D. Priority Use City of Lakewood

- i. The District agrees that first priority for the use of the City owned athletic fields and facilities shall be given to City managed programs. City programs and activities shall have the right to preempt other users upon giving advance notice, except in extraordinary circumstances when advance notice is not possible.

Use of City public facilities shall have the following priority:

- 1. Programs, governmental functions, recreational activities or special events sponsored by the City of Lakewood will have first priority in scheduling.
 - 2. Lakewood non-profit organizations, Lakewood community groups, or Clover Park School District will have the second priority.
 - 3. Residents of the Lakewood area, employees and employers based in the Lakewood area, may reserve facilities and parks for group or personal use and will have third priority.
 - 4. Nonprofit groups, organizations and individuals who reside outside of the Lakewood area will have fourth priority.
- ii. In conjunction CITY facility use is prioritized in the following order:
 - a. City use, government functions, and sponsored events, including recreational and community programs provided by the City.
 - b. District use and District sponsored programs provided for District staff and students.
 - c. All other uses as determined and prioritized by the City.
 - iii. The City agrees to give the District second priority of City owned facilities with the exception of the following that will become available on a case by case basis, following regular scheduling policies and procedures:
 - a. Colonial Plaza, Motor Ave SW
 - b. Pavilion at Fort Steilacoom Park, 9107 Angle Ln SW
 - iv. The District may request dates for meetings and special events in October for the following scheduling period of January 1 – December 31. In November available space for all use is on a space available basis. Every effort will be made to accommodate existing established schedules. Sports fields may be unavailable December – the end of February annually for field repair and maintenance.

V. Supervision:

- A. Agencies acknowledge that the community has invested significant resources in City and District facilities and that proper use, management, security and supervision are required to reduce negative impacts and potential overuse of facilities. Joint use should not create additional work or burden for agency employees. An agreed upon joint agency code of conduct for staff, volunteers and participants will be adhered to when using City or District facilities. The City and District reserves the right to cancel or postpone any activity due to conflicts, disregard of policies, or other unacceptable or controllable circumstances.

IV: Fees and Cost Sharing

- A. Generally, facilities will be available at no cost unless additional equipment, staffing, access or operational support is needed due to use outside of established hours or because new regulations, guidelines or changes in operations are required to meet local, state or national issues or mandates. Each agency may request reimbursement for additional support needed to

ensure a healthy and safe environment. Examples include scheduled use taking place during a weekend or after established business hours could require additional staff to supervise or secure buildings or additional labor could be needed to clean and sanitize an area to meet Dept. of Health guidelines.

- B. Registration and use fees will be retained by the collecting and managing agency.
- C. Fee would be charged for direct costs incurred as a result of a particular activity, such as when a given use results in non-scheduled labor costs or other direct costs attributed to a specific use of a facility, or when in the view of the facility owner a facility was left unkempt or damaged. In this case, the City and District agree to reimburse one another for their share of expenses upon written invoice for direct costs that are a consequence of facility use. Fees for indirect costs shall not be reimbursed.
 - i. "Direct Costs" are those costs that are incurred directly such as the cost of labor, an object or specific service.
 - ii. "Indirect Costs" are those costs that cannot be readily and specifically attributed to a particular use, such as depreciation, normal "wear and tear" of facilities, overhead or administrative expenses.

V: Term, Renewal and Termination

- A. Agreement shall be from _____ through and including _____. At the end of this three (3) year period, the Agreement shall automatically renew with (3) one-year renewal terms existing on this contract, unless any Party gives the others written notice of its intent not to renew the Agreement at least ninety (90) days before the expiration of this Agreement.

VI: Capital Improvements

- A. Joint Projects will be addressed in a separate agreement.

VII: Responsibility and Liability

- A. The District shall indemnify and hold the City and its agents, employees, and/or officers, harmless from and shall process and defend at its own expense any and all claims, demands, suits, at law or equity, actions, penalties, losses, damages, or costs, of whatsoever kind or nature, brought against the City arising out of, in connection with, or incident to the execution of this Agreement and/or the District's performance or failure to perform any aspect of this Agreement; provided, however, that if such claims are caused by or result from the concurrent negligence of the City, its agents, employees, and/or officers, this indemnity provision shall be valid and enforceable only to the extent of the negligence of the District; and provided further, that nothing herein shall require the District to hold harmless or defend the City, its agents, employees and/or officers from any claims arising from the sole negligence of the City, its agents, employees, and/or officers. No liability shall attach to the City by reason of entering into this Agreement except as expressly provided herein.
- B. The City shall indemnify and hold the District and its agents, employees, and/or officers, harmless from and shall process and defend at its own expense any and all claims, demands, suits, at law or equity, actions, penalties, losses, damages, or costs, of whatsoever kind or nature, brought against the District arising out of, in connection with, or incident to the execution of this Agreement and/or the City's performance or failure to perform any aspect of this Agreement; provided, however, that if such claims are caused by or result from the concurrent negligence of the District, its agents, employees, and/or officers, this indemnity provision shall be valid and enforceable only to the extent of the negligence of the City; and provided further, that nothing

herein shall require the City to hold harmless or defend the District, its agents, employees and/or officers from any claims arising from the sole negligence of the District, its agents, employees, and/or officers. No liability shall attach to the District by reason of entering into this Agreement except as expressly provided herein.

VIII: Conflict Resolution

- A. If either party believes that the other party is not fulfilling the performance obligations established by this Agreement, that party shall give written notice of its complaint to the other party. The party receiving the complaint shall, within ten (10) calendar days, correct the situation and confirm the correction in writing or reject the complaint while explaining the mitigating circumstances and why a remedy cannot be achieved.
- B. If the City and District representatives are unable to resolve the complaint, the District's Director of Operations & Maintenance and the City's Parks and Recreation Director agree to meet to resolve the complaints. If they are unable to resolve the complaints, the issue shall be referred to the District Superintendent and the City Manager for resolution.

IN WITNESS WHEREOF the parties hereto have executed this Agreement as of the day and year first above written.

CLOVER PARK SCHOOL DISTRICT

CITY OF LAKEWOOD

By: _____
Superintendent

By: _____
John Caulfield, City Manager

Attest: _____
City Clerk, Briana Schumacher

Attest: _____
Heidi Wachter, City Attorney



SOUTH SOUND HOUSING AFFORDABILITY PARTNERS

Shaping Pierce County's Future by Creating Access to Affordable and Attainable Housing

March 26, 2021

Dear City Manager Caulfield,

As you know, for the past two years we have been working on an exciting initiative to create more housing that is accessible, affordable, and attainable for all Pierce County resident – the South Sound Housing Affordability Partners (SSHA³P). We are very pleased to send you a draft Intergovernmental Agreement (IGA) for your city's review and comment.

The IGA will create a new formal collaboration among Pierce County cities, towns, tribes and the county to work together on housing issues and projects. We know that housing needs and solutions vary from one community to the next, so we have worked to create a structure that can meet the unique needs of each of our communities. The IGA spells out the purpose, proposed structure, and framework for operating this new collaborative structure.

We want to thank Lakewood for working with us to get to this point. Mayor Anderson has been involved in the meetings and discussions that have shaped this attached draft agreement.

Our hope is to have governments decide whether they want to participate in SSHA³P (and if so, approve the intergovernmental agreement) by the end of June of this year so that we can get to work creating this structure in the second half of 2021. In order to meet that schedule we are **asking all interested governments to provide us with any comments or questions on the draft IGA by April 23**. That will provide enough time to make final changes to the document and send it back to you for final review and approval.

We are sending a similar letter to all Mayors, Council Chairs/Deputy Mayors/Mayor Pro Tems, and City Managers, Administrators or Clerks in the county. We will keep you informed about the reactions we receive from those governments. Based on discussions we've been having we are hopefully that many governments across the county will join SSHA³P.

Please send any comments or questions about the IGA to either Bryan Schmid at bryan.schmid@piercecountywa.gov or John Howell at john@cedarrivergroup.com.

Sincerely

Members of the SSHA³P Steering Committee

Mayor Don Anderson
City of Lakewood

Mayor Nancy Backus
City of Auburn

Council Member Annette Bryan
Puyallup Tribe

Executive Bruce Dammeier
Pierce County

Mayor Julie Door
City of Puyallup

Mayor Kit Kuhn
City of Gig Harbor

Council Member Ryan Mello
Pierce County

Mayor Josh Penner
City of Orting

Mayor Kim Roscoe
City of Fife

Mayor Victoria Woodards
City of Tacoma

**Intergovernmental Agreement for SSHA³P
South Sound Housing Affordability Partners**

(Effective Date: **Date of Execution**)

This Intergovernmental Agreement ("Agreement") is entered into by and between the Cities of **??**, Pierce County (each municipal corporations and political subdivisions of the State of Washington), and the Puyallup Tribe of Indians, a **federally-recognized Indian tribe** (the Tribe) Individually each member of SSHA³P is a "Party" and collectively, the "Parties". This Agreement relates to the creation and operation of a joint undertaking among the Parties hereto to be known as the South Sound Housing Affordability Partners ("SSHA³P"). This Agreement is made pursuant to the Intergovernmental Cooperation Act, chapter 39.34 RCW, and has been authorized by the legislative body of each government.

WHEREAS, the recent increases in the cost to purchase or rent housing in Pierce County has resulted in more than one-third of all households in the county considered cost-burdened, paying more than 30% of their income for housing costs; and

WHEREAS, in the past decade increases in Pierce County median home sales prices (108%) and rental rates (78%) have dramatically outpaced the increases in median household incomes (10%); and

WHEREAS, the recent global pandemic has placed new pressures on the housing market, with the economic downturn resulting in many households unable to afford rent or mortgage payments, anticipated increases in the number of evictions and foreclosures (including among those small rental property owners who have not been receiving rental payments), and at the same time increased demand for purchasing homes due to historically low interest rates and the large increase in the number of those now working remotely; and

WHEREAS, the Parties have a common goal to ensure the availability of housing that meets the needs of residents and workers at all income levels in Pierce County; and

WHEREAS, the Parties share a common goal to eliminate housing inequities for all races, ethnicities and other vulnerable or marginalized communities and geographic locations in Pierce County; and

WHEREAS, the Parties wish to provide a sound base of housing policies and programs in Pierce County and to complement the efforts of local governments and existing public and private organizations to address housing needs in Pierce County; and

WHEREAS, the Parties wish to act cooperatively to formulate policies that create access to affordable/attainable housing policies and strategies that address housing stability, ensure an ample housing supply that remains affordable for all income levels, provide incentives for affordable/attainable housing for lower and middle income households, foster efforts to preserve and provide access to affordable/attainable housing by combining public funding with private-sector resources, and support implementation of the goals of the Washington State (the "State") Growth Management Act, related countywide planning policies, and other local policies and programs relating to providing access to affordable and attainable housing, and to do so efficiently and expeditiously; and

WHEREAS, the Parties have determined that the most efficient and effective way for the Parties to address access to affordable and attainable housing needs in Pierce County is through cooperative action and pooling their resources to create a larger scale of activities than any one Party could accomplish on their own, achieve economies of scale, and leverage other public and private resources; and

WHEREAS, the intent of this cooperative undertaking is not to duplicate efforts of non-profit corporations and other entities already providing affordable/attainable-housing-related services; and

WHEREAS, a cooperative work plan with a primary focus on the production and preservation of affordable/attainable housing, is needed because the lack of access to affordable/attainable housing is one of the key contributors to homelessness;

NOW, THEREFORE, in consideration of the mutual promises, benefits and covenants contained in this Agreement and other valuable consideration, the sufficiency of which is acknowledged, the Parties agree to the above Recitals and the following terms and conditions:

1. DEFINITIONS. The following terms used in this Agreement are defined as follows:

a. *Administering Agency* - One or more of the Parties that will provide administrative support services on behalf of SSHA³P. The role of the Administering Agency will be to serve as the fiscal agent for SSHA³P, in accordance with the requirements of chapter 39.34 RCW, and provide space and administrative support for the SSHA³P staff. These roles could be provided by one Party or the roles could be split and provided by two Parties.

b. *Advisory Board* - A board consisting of 15 to 20 community members and/or key stakeholders appointed by the Executive Board to provide advice and recommendations to the Executive Board on policy needs related to housing stability, program design and development, land and/or funding resource allocation for enhancing access to affordable/attainable housing projects, and other immediate affordable/attainable housing needs, and to provide public education and community outreach support.

c. *Area of Activity* - The area in Pierce County where SSHA³P is expected to conduct its work, and where future expansion of SSHA³P membership could occur, as further described in Exhibit A.

d. *Executive Board* - The governing board for SSHA³P comprised of members as provided herein, which will act on behalf of all Parties and as may be in the best interest of SSHA³P in order to carry out the purposes of this Agreement.

e. *SSHA³P Fund* - A special fund established by the Administering Agency, as authorized by RCW 39.34.030, to be known as the "Operating Fund of the SSHA³P Executive Board". Within the SSHA³P Fund may be established various accounts and subaccounts including but not limited to a "SSHA³P Operating Account" and a "Housing Capital Funds Account".

i. *SSHA³P Operating Account* - An account created by the Administering Agency for the receipt of contributions from the Parties and other private or public entities, for the purpose of paying for the operations of and supporting the SSHA³P annual budget.

ii. *Housing Capital Funds Account* - An account established by the Administering Agency within the SSHA³P Fund for the purpose of administering the voluntary contributions of Parties, or other public or private entities, to affordable/attainable housing projects and programs, to be known as the SSHA³P Housing Capital Funds Account. The amounts in the Housing Capital Funds Account will be held and disbursed by the

Administering Agency.

iii. *Individual Account(s)* - One or more subaccounts established within the Housing Capital Funds Account for each Party to this Agreement that chooses to contribute to the Housing Capital Funds Account. Contributions to an Individual Account from a Party may be released only upon, and in accordance with, the written direction of that Party.

f. *SSHA^{3P} Executive Manager* - The managing lead staff for SSHA^{3P}, reporting to the Executive Board, responsible for carrying out the SSHA^{3P} annual work plan and other terms of this Agreement.

g. *SSHA^{3P} Staff* - The full-time and part-time staff hired or assigned with the responsibility for carrying out the SSHA^{3P} work plan, under the direction of the SSHA^{3P} Executive Manager.

2. PURPOSE. All Parties to this Agreement have responsibility for local, regional and tribal planning for the provision of housing affordable to residents that work and/or live in Pierce County. The Parties agree to act cooperatively to formulate housing policies that address access to affordable/attainable housing, housing stability and to foster efforts to preserve and create affordable/attainable housing by combining public funding or other resources with private-sector resources. In order to accomplish the purposes of this Agreement, the Parties agree to create a joint and cooperative undertaking responsible for administering the activities described herein. The cooperative undertaking among the Parties will be called the South Sound Housing Affordability Partners ("SSHA^{3P}"). The Parties further intend that this Agreement serve as a framework for all participating municipalities within the broader SSHA^{3P} Area of Activity to do the aforementioned work. The Parties encourage other cities and tribes and other public and private entities in Pierce County within the SSHA^{3P} Area of Activity to join the Parties in this endeavor.

3. STRUCTURE; CREATION OF JOINT BOARD. By executing this Agreement, the Parties hereto create a joint board pursuant to RCW 39.34.030(4) to govern SSHA^{3P}. The joint board created by this Agreement will be called the "Executive Board" and will be composed of members as provided for in Section 5.a of this Agreement. The Joint Board created in this Agreement is not a separate legal or administrative entity within the meaning of RCW 39.34.030(3). The Executive Board will act on behalf of all Parties and as may be in

the best interest of SSHA³P in order to carry out the purposes of this Agreement. The Executive Board is not authorized to own assets or have employees.

4. RESPONSIBILITIES AND AUTHORITY. In order to carry out the purposes of SSHA³P and this Agreement, the Executive Board will have the responsibility and authority to:

a. Create and implement an annual work plan approved by the Executive Board to guide the work of SSHA³P Staff. The work plan will include activities that may benefit multiple participating jurisdictions or individual jurisdictions. The work plan activities will be consistent with the purpose described in this Agreement.

b. Make decisions regarding the allocation of public and private funding deposited into the SSHA³P Housing Capital Funds Account for the purpose of enhancing access to affordable/attainable housing.

c. Direct the Administering Agency to enter into agreements with third parties for the use and application of public and private funds contributed by individual Parties to the SSHA³P Fund, including the Housing Capital Funds Account established in Section 14, under terms and conditions as may be agreed by the individual contributing Parties, as further described in Section 14. At least annually, report to the Parties on the geographic distribution of all housing capital funds as recommended by the SSHA³P Executive Board.

d. Guide the SSHA³P Staff in the performance of the following duties:

- i. Provide recommendations to the Parties regarding local and regional policies to improve access to affordable/attainable housing. SSHA³P Staff may assist the Parties in developing strategies and programs to achieve State Growth Management Act housing goals, growth targets, local Comprehensive Plan Housing Element goals, and local housing strategy plans, as appropriate.
- ii. SSHA³P Staff may provide technical assistance as requested to any Party considering adoption of land use incentives or requirements, or programs to enhance access to affordable/attainable housing. SSHA³P Staff may research model programs, develop

draft legislation, prepare briefing materials and make presentations to planning commissions and councils on request by a Party. SSHA³P Staff may assist Parties in developing strategies and programs to implement county-wide affordable/attainable housing policies to meet State Growth Management Act objectives, growth targets, local Comprehensive Plan Housing Element goals, and local housing strategy plans for an equitable and rational distribution of low- and moderate-income housing.

- iii. Facilitate cooperation between the private and public sector with regard to the provision of and access to affordable/attainable housing. SSHA³P Staff may communicate directly with private developers, financial institutions, non-profit corporations and other public entities to assist in the implementation of affordable/attainable housing projects. SSHA³P Staff may work directly with any Party to provide technical assistance with regard to the public funding of affordable/attainable housing projects and the implementation of affordable/attainable housing regulatory agreements for private developments.
- iv. SSHA³P Staff may also identify public and private sites that are available for affordable/attainable housing and develop housing alternatives for such sites.
- v. After the Housing Capital Fund is created, develop standard regulatory agreements acceptable to private and public financial institutions to facilitate the availability of funding for private and public projects containing affordable/attainable housing, and other documents and procedures as appropriate.
- vi. Provide other technical advice to any Party upon request and to enter into agreements to provide technical assistance to other public entities on a reimbursable basis.
- vii. Provide support and educational activities and to monitor legislative and regulatory activities and proposed policy changes related to enhancing access

to affordable/ attainable housing at the county, state and federal levels. Ensure that the Parties to this Agreement have a strong Pierce County voice in federal, state and regional decision making regarding affordable/attainable housing programs, policy and funding.

- viii. After the Housing Capital Fund is created, develop and carry out procedures for monitoring of affordable/attainable units and to administer direct service housing programs on behalf of any Party. Such direct service housing programs may include but are not limited to relocation assistance programs, rent voucher and/or deposit loan programs.
- ix. Work directly with other public and private entities for the development of affordable/attainable housing policies and encourage the provision and preservation of affordable/attainable housing.
- x. Work with the Advisory Board appointed under this Agreement and Pierce County agencies and organizations working on regional housing stability and access to affordable and attainable housing, identify trends and promising practices, and mobilize those agencies and organizations to support Pierce County positions in regional, state and federal decision-making forums.
- xi. Support public and private fundraising efforts of public and private entities (including non-profit corporations) to raise funds to carry out the purposes of this Agreement. Enter into agreements with those entities for the use of such funds in order to carry out the purposes of this Agreement.
- xii. As directed by the Executive Board, accept public and private funding, invest those funds in accordance with State law, and take other appropriate and necessary action to carry out the purposes of this Agreement.

5. EXECUTIVE BOARD.

a. Membership. SSHA³P will be governed by an Executive Board composed of individuals who have authority to make decisions on the Board on behalf of their governments.

- For cities/towns: Members will either be Mayors or, as designated by the Mayor, Council members or a City Manager/Administrator.
- For tribes: Members will be the Tribal Council Chair or a member of the Tribal Council as designated by the Chair.
- For the county: Members will be the County Executive, and the County Council Chair (or their designee).

Each Party will have one vote on the Executive Board.

Municipalities, local governments, tribes and public agencies within the Area of Activity may become Parties to this Agreement with representation on the Executive Board described in this Section 5, upon satisfaction of the requirements of Section 20.

The Executive Board will administer this cooperative undertaking under the terms of this Agreement and under any procedures adopted by the Executive Board.

b. Officers. The Chair and Vice Chair of the Executive Board will be elected by the members of the Board from the Board membership. The Chair, or in their absence the Vice Chair, will preside over all meetings of the Executive Board; and will, in the absence of an Executive Manager provided for in Section 6 of this Agreement, process issues, organize meetings and provide for administrative support as required by the Executive Board. The Chair will serve a one-year term, but may be reelected by the Executive Board for one additional term.

c. Alternate Member. Each member of the Executive Board may designate one alternate member who will serve in the place of the member on the Executive Board during the member's absence from a meeting. Notice of the designation will be provided to the Chair of the Executive Board. An alternate member may only vote on behalf of the Party if the Chair has received written notice of the alternate's appointment. It is not intended that alternates will serve on the Board in an ongoing capacity.

d. Powers. It is intended that the Executive Board will serve as a decision-making body for SSHA³P. The Executive Board will have the power to (1) develop and approve an annual budget and annual work plan for SSHA³P and oversee the management of the SSHA³P Fund; (2) adopt procedures for the administration of

SSHA³P (including securing any necessary legal counsel as needed) and for the conduct of meetings; (3) make recommendations to the Parties concerning planning, policy, programs and the funding of projects to enhance access to affordable/attainable housing; (4) establish policies for the expenditure of SSHA³P budgeted items; (5) work with the designated Administering Agency (see Section 6) to carry out the terms of this Agreement; (6) establish a special fund with the Administering Agency as authorized by RCW 39.34.030, to be known as the "Operating Fund of the SSHA³P Executive Board" ("SSHA³P Fund") within which Fund may be established various accounts and subaccounts including but not limited to a "SSHA³P Operating Account" (into which will be deposited funding for the SSHA³P operating budget) and a "Housing Capital Funds Account"; (7) hold regular meetings on those dates and at those places as the Executive Board may designate; (8) direct the Administering Agency to enter into contracts and agreements for the provision of personnel and other necessary services to SSHA³P, including accounting and legal services and the purchase of insurance, and authorize the Administering Agency to execute such other contracts, agreements or other legal documents necessary with public and private entities for SSHA³P to carry out its purposes; (9) establish the responsibilities for the SSHA³P Executive Manager, direct and oversee the Administering Agency in the hiring process and performance review for that position and direct and oversee the activities of the SSHA³P Executive Manager; (10) create a recruitment process and appoint members of the Advisory Board; (11) assist with incorporating and/or work with a non-profit corporation to accept grants, donations and other funds on behalf of SSHA³P and direct the Administering Agency to enter into contracts or other agreements with such agency for the use of those funds to carry out the purposes of this Agreement; and (12) take whatever other action is necessary to carry out the purposes of this Agreement.

6. ADMINISTRATION AND OVERSIGHT. The Executive Board will have final decision-making authority on behalf of SSHA³P over all legislative and administrative matters within the scope of this Agreement. The Executive Board may delegate responsibility for general oversight of the operations of SSHA³P to an Executive Manager. The SSHA³P Executive Manager will submit quarterly budget performance and progress reports on the status of the work program elements to the Executive Board and annually to the legislative body of each Party. The reports and their contents will be in a form acceptable to the Executive Board.

Pierce County will be the SSHA³P Administering Agency serving as the fiscal agent (in accordance with the requirements of chapter

39.34 RCW) and providing administrative support services. To change the Administering Agency, the Executive Board will, by two-thirds supermajority vote, designate one or more of the Parties to serve as the Administering Agency. Each Party that serves in the capacity of Administering Agency hereby agrees to comply with the terms of this Agreement applicable to the Administering Agency in order to permit SSHA³P to carry out its purposes. SSHA³P will be staffed with personnel funded by the Parties and/or independent contractors contracting with the Administering Agency on behalf of SSHA³P.

Any Party providing personnel to SSHA³P will remain solely responsible for the continued payment of all compensation and benefits to those personnel as well as for any worker's compensation claims. All Parties will cooperate fully in assisting SSHA³P to provide the services authorized in this Agreement.

7. MEETINGS OF EXECUTIVE BOARD.

a. Frequency. The Executive Board will meet as often as it deems necessary, but not less often than quarterly.

b. Quorum. A quorum at any meeting of the Executive Board will consist of the Board members (or alternates) who represent a simple majority of the Board's membership. Executive Board members (or alternates) may participate in any meeting by phone and/or video conferencing for all purposes, including but not limited to voting and establishing a quorum.

c. Action. No action may be taken except at a meeting where a quorum exists. Action by the Executive Board requires an affirmative vote by a majority of the Board's membership, provided however, that a supermajority (two-thirds of Board members) will be required to change the Administering Agency, or to modify the proposed contribution methodology for dues and assessments (see Section 15). Official action by the Executive Board may be conducted by motion, resolution, declaration, or such means as determined to be necessary by the Executive Board in their procedures for conducting meetings (referred to in 5.d above), provided however, changing the Administering Agency or Agencies shall be completed by resolution. All official action of the Executive Board shall be recorded in its official minutes. Each Executive Board member, or his or her alternate, will have one vote on any proposed action; except that shared member voting shall be in accordance with the Executive Board's approval under Section 5 (a). Proxy voting will not be allowed. To the extent applicable to meetings of the Executive Board, the Executive Board will comply

with applicable requirements of the Washington State Open Public Meetings Act (chapter 42.30 RCW).

8. ADVISORY BOARD.

a. Purpose and Role. The Executive Board will create an Advisory Board to provide advice and recommendations on the work of SSHAP. Specific objectives shall be to advise the Executive Board regarding: assessment of affordable/attainable housing needs for different populations in Pierce County; the development of policies and/or funding resources to meet those needs; the priorities for allocation of future SSHAP capital resources to create or preserve access to affordable/attainable housing; input on program design and development related to housing affordability and stability; suggested approaches to federal, state and regional legislative advocacy; and providing support for public education and community outreach activities. The Advisory Board will be created before the end of the first year of SSHAP operations (with the start of the year defined as the hiring of the SSHAP Executive Manager).

b. Membership. The Advisory Board will consist of not more than twenty (20) and not less than fifteen (15) community members and/or key stakeholders. The Executive Board will appoint members to the Advisory Board. Appointed members must have knowledge and understanding of affordable/attainable housing and be committed to the furtherance of improving access to affordable/attainable housing for all in Pierce County. Membership should reflect a geographic diversity of Pierce County cities, towns, tribes and unincorporated areas. Membership will reflect one-third consumers of affordable/attainable housing (rental and ownership housing), one-third developers, designers, planners of affordable/attainable housing, and one-third advocates and advisors who work on land use and housing issues.

c. Terms. Appointments will be for a three-year term with service limited to a total of two consecutive terms. When the Advisory Board is created, the initial terms of members shall be staggered as follows:

- One-third shall be appointed for a one-year term
- One-third shall be appointed for a two-year term
- One-third shall be appointed for a three-year term

The membership categories of the Advisory Board, described in section 8.b above, will be evenly distributed across the three initial terms.

d. Recruitment and Termination. The Executive Board will adopt procedures for the recruiting, convening and administration of the Advisory Board. A member may be removed from the Advisory Board by the Executive Board with or without cause on a majority vote of membership of the Executive Board.

e. Officers. When the Advisory Board has been duly created, the members shall elect a Chair and Vice Chair, who will be responsible for working with the SSHA^{3P} Executive Manager to set agendas and facilitate meetings.

f. Conflict of Interest. No member of the Advisory Board may vote on a Board recommendation if the result would financially benefit an agency or organization of which the Member is a decision-making officer, or is in a position of authority or responsibility. It is the responsibility of each Board Member to declare potential conflicts of interest. The Chair shall be responsible for mediating differences of opinion on these matters as they may arise.

9. MEETINGS OF ADVISORY BOARD.

a. Frequency. The Advisory Board will meet as often as it deems necessary, but not less often than quarterly.

b. Quorum. A quorum at any meeting of the Advisory Board will consist of the Board members who represent a simple majority of the Board's membership. Advisory Board members may participate in any meeting by phone and/or video conferencing for all purposes, including but not limited to voting and establishing a quorum.

c. Action. No action may be taken except at a meeting where a quorum exists. Action by the Advisory Board requires an affirmative vote by a majority of those members attending a Board meeting where a quorum exists. Official action by the Advisory Board may be conducted by motion, resolution, declaration, or other means as determined to be necessary by the Advisory Board. Proxy voting is not allowed. To the extent applicable to meetings of the Advisory Board, the Advisory Board will comply with applicable requirements of the Washington State Open Public Meetings Act (chapter 42.30 RCW).

10. AGREEMENT DURATION. This Agreement will be in full force and effect for a period commencing as provided in Section 34, and ending December 31, 2023. This Agreement will automatically renew

for successive two-year terms, unless sooner terminated under the provisions of this Agreement.

11. TERMINATION OF AGREEMENT. This Agreement may be terminated at any time by affirmative vote of a majority of the legislative bodies of the Parties to this Agreement.

Upon termination of this Agreement, all property acquired during the life of the Agreement will be disposed of in the following manner:

a. all property contributed without charge by any Party will revert to the contributing Party;

b. all property purchased on behalf of SSHA³P after the effective date of this Agreement will be distributed to the Parties based on each Party's pro rata contribution to the overall budget at the time the property was purchased;

c. except as provided in Subsection d. below, all unexpended or reserve funds will be distributed to the Parties based on each Party's pro rata contribution to the overall budget in effect at the time the Agreement is terminated;

d. all uncommitted monies in the Housing Capital Funds Account will be distributed to the Parties by remitting the total uncommitted amounts remaining in each Party's Individual Account (as described in Section 14) to that Party; and

e. each Party that has funded a project through the Housing Capital Funds Account will be substituted for the Administering Agency with regard to all project financing documents, and the Administering Agency will be released from further obligations with respect thereto.

12. WITHDRAWAL. If a Party wishes to withdraw from participation in this Agreement, it may do so after the initial term if it first gives six months advance written notice to the Executive Board of its intention to withdraw, which notice will be due on or before June 30, 2022. If notice of an intent to withdraw is timely received, withdrawal will be effective as of 11:59 p.m. on December 31, 2023. The initial members of SSHA³P agree they will participate for at least through the first full term of the collaboration.

After December 31, 2023, an opportunity is provided once per year for a Party to withdraw from this Agreement. Under this provision, advance written notice must be provided to the Executive Board on

or before June 30, of its intention to withdraw from participation in the Agreement, effective as of 11:59 p.m. on December 31st of that current year.

Any Party withdrawing from this Agreement will remain legally and financially responsible for any obligation incurred by the Party pursuant to the terms of this Agreement, including but not limited to any project financing documents executed by the Administering Agency on behalf of that Party, during the time the withdrawing Party was a member of SSHA³P. A notice of withdrawal may be revoked by the Party that submitted the notice only by approval of a majority of the Executive Board to accept the revocation. Any Party that provides written notice of its intent to withdraw may continue to vote on all matters before the Executive Board prior to the effective date of its withdrawal, except the budget and work plan for the following year.

13. BUDGET. The fiscal year for SSHA³P will be January 1 to December 31 of any year. The first year budget and anticipated second year budget for SSHA³P are attached to this Agreement as Exhibit B and incorporated herein by this reference. Approval of this Agreement by the legislative body of each Party includes approval of the SSHA³P budgets through the first full term of the agreement. For subsequent years, on or before July 1st of each year, a recommended operating budget for SSHA³P for the following budget year will be prepared, reviewed and recommended by the Executive Board and transmitted to each Party. The recommended operating budget will contain an itemization of all categories of budgeted expenses and will contain an itemization of the amount of each Party's contribution, including in-kind services, toward that budget. Contributions to the SSHA³P annual budget may also come from other public or private entities. The Executive Board will approve acceptance of those contributions.

a. No recommended budget will become effective until approved by the legislative body of each Party and adopted by the Executive Board, except for SSHA³P's first full term (2021, 2022 and 2023). Once the legislative body of each Party has approved its contribution to SSHA³P, either separately or through its budget process, and the SSHA³P budget has been adopted by the Executive Board, each Party will be obligated to make whatever contribution(s) is budgeted for that Party. If a Party does not approve the budget in a timely manner, the Executive Board may adopt the annual budget and work plan with a two-thirds majority vote.

b. Each Party's contribution(s) will be transmitted to SSHA³P on an annual basis no later than the end of the first quarter of each calendar year, unless otherwise provided in the budget document. Each Party will specify the amount of its contribution to be deposited by the Administering Agency into the SSHA³P Operating Account, as well as the amount, *if any*, to be deposited into the Party's Individual Account within the SSHA³P Housing Capital Funds Account established by Section 14.

c. If any Party is delinquent by more than three months in the payment of its approved operating budget contribution to the SSHA³P Operating Account, that Party will not be entitled to vote on matters before the Executive Board until the delinquency has been paid. A Party's decision not to contribute funds to its Individual Account, or its delinquency in funding the same will not affect that Party's voting rights on the Executive Board.

14. HOUSING CAPITAL FUNDS ACCOUNT ESTABLISHED; PROCEDURES FOR ADMINISTERING HOUSING CAPITAL FUNDS ACCOUNT CONTRIBUTIONS. If authorized by resolution of the Executive Board, the Administering Agency may establish and maintain an account within the SSHA³P Fund for the purpose of administering the contributions of Parties, or other public or private entities, to affordable/attainable housing projects and programs, to be known as the SSHA³P Housing Capital Funds Account. The amounts in the Housing Capital Funds Account will be held and disbursed by the Administering Agency as described in this Agreement.

a. Within the Housing Capital Funds Account, a sub-account will be established for each Party to this Agreement that chooses to contribute to the Housing Capital Funds Account. Such sub-accounts are collectively referred to as the "Individual Accounts." Contributions to an Individual Account from a Party may be released only upon, and in accordance with, the written direction of that Party.

b. A subaccount will be established within each Individual Account for each project or program funded in whole or part by a Party from its Individual Account. Such sub-accounts are referred to as "Project Accounts."

c. The Administering Agency will maintain records sufficient to separately track the deposits, withdrawals and interest earnings, within each Individual Account and each Project Account, and will provide quarterly reports to all Parties as to the status of funds in each Individual Account and Project Account. The Administering Agency's responsibilities under this Section may

be delegated to the SSHA³P Executive Manager to the extent consistent with applicable law and as the Administering Agency and Executive Board may agree.

d. Funds transmitted to SSHA³P for deposit in the Housing Capital Funds Account will be held by the Administering Agency on behalf of SSHA³P until directed for application by the contributing Party (as described in 14.a. above). Uncommitted funds in an Individual Account will be remitted to the contributing Party within 45 days of receipt of written request from the Party signed by its chief executive officer, or upon the Party's withdrawal from SSHA³P in accordance with the terms of Section 12, or on the dissolution of SSHA³P per Section 11.

e. Funds held by the Administering Agency on behalf of SSHA³P will be subject to laws applicable to public funds, including but not limited to limitations on investments for public funds as provided in State law and the State constitutional limitations on the gifting of public funds. Investment earnings will be credited to each Individual Account and Subaccount on a pro rata basis.

f. The Executive Board will develop standard forms of approvals and agreements to be used in the application of funds contributed to the Housing Capital Funds Account (collectively referred to as "project financing documents"), consistent with the following goals and procedures:

- i. The SSHA³P Executive Board will authorize the application of specified amounts of Individual Account monies to projects or programs consistent with the purpose of the SSHA³P Capital Fund, and will authorize and direct the SSHA³P Executive Manager, and the Administering Agency to take such actions as necessary to accomplish this. At least two weeks prior to the Executive Board action they will notify the individual parties whose funds will be used for such projects or programs of the various terms to accompany their authorizations, and will at a minimum include terms related to:
 1. amount of funds allocated;
 2. project description, including minimum affordability requirements, if any;
 3. nature of the funding commitment (loan, grant, or other);
 4. in the event the funding commitment is provided as a loan, the general repayment terms including

- but not limited to the term of the loan and applicable, interest rate(s); and
5. a general description of the security interests, if any, to be recorded in favor of the Party.
- ii. For each individual housing project or program to be funded through the Housing Capital Funds Account, the developer, owner or administrator of such project or program (generally referred to as the "developer"), will enter into the necessary set of agreements (project financing documents) with the Administering Agency. The project financing documents will specify that the Administering Agency is acting as administering agency on behalf of each Party that is providing Housing Capital Funds Account funding to the project or program pursuant to this Agreement and RCW 39.34.030(4), and will identify each participating funding Party. The project financing documents will be consistent with general forms of agreement approved by the Executive Board and the terms and conditions approved by the legislative bodies of the funding Parties. To the extent permitted by law, the project financing agreements will incorporate all terms and conditions relative to the use and (if applicable) repayment of such funds, and provide for transfer of the Housing Capital Funds Account monies from the funding Parties to the developer.
 - iii. Funding contributions to, and repayments (if any) from, specific projects and programs will be recorded by the Administering Agency, including recording of monies deposited and withdrawn from each Party's Individual Account and Project Accounts. Repayments (if any) under any project financing agreement will be applied *pro rata* to the funding Parties' Individual Accounts based on the ratio of funding contributed to the project by each Party, unless the funding Parties otherwise agree.
 - iv. Unless the Parties funding a project or program through the Housing Capital Funds Account otherwise agree, a default, termination or other action against a developer or other third party may be declared only after securing approval in writing of the chief executive officers of funding Parties which together have contributed not less than 51%

of the total SSHA³P member funding to the project. The Administering Agency will provide reasonable notice and information regarding the status of projects as necessary to each funding Party's chief executive officer to allow them to make a timely decision in this regard. Funding Parties not consenting to such default, termination or other action will be bound by the decision of the majority. All funding Parties will be obligated on a pro rata basis (based on their contributions to the project) to pay to the Administering Agency its reasonable costs and expenses incurred as a result of declaring a default, terminating an agreement or taking other action against a developer or other third party. Any funds recovered through such enforcement proceedings will be allocated to the funding Parties Individual Accounts pro rata based on their respective funding contributions to a project.

- v. The Executive Board may from time to time authorize the Administering Agency to administer housing project agreements entered into before the effective date of this Agreement, upon terms consistent with this Agreement and subject to the consent of the Administering Agency and the Parties to such agreements.

15. DUES, ASSESSMENTS AND BUDGET AMENDMENTS. Contributions to the SSHA³P Operating fund will be based on groupings of like-sized cities (based on population), or whatever contribution methodology is approved by a two-thirds majority of the Executive Board. Funding for the activities of SSHA³P will be provided solely through the budgetary process. Unless otherwise specified in this Agreement, no dues, charges or assessments will be imposed or required of the Parties except upon unanimous vote of the membership of the Executive Board and ratification by the legislative body of each Party subject to the dues, charges or assessments. An approved budget (the overall revenues and expenditures) will not be modified until approved by the legislative body of each Party and finally adopted by the Executive Board. If a Party agrees to totally fund an additional task to the work program, not currently approved in the budget, the task may be added to the work plan and the budget amended to reflect the funding of the total cost of such task by the requesting Party, upon approval by a majority of the membership of the Executive Board without approval by the individual Parties. Notwithstanding

the foregoing, contributions by a Party to its Individual Account within the Housing Capital Funds Account will be solely within the discretion of that Party and are not considered "dues or assessments."

16. PUBLIC RECORDS REQUESTS. Records of SSHA³P will be held by the Administering Agency, in addition to any records the Parties themselves may retain concerning their participation in SSHA³P. These records may include meeting agendas, meeting summaries, reports, plans, budgets, and other documents.

When a Party receives a request for records that may be held by the Administering Agency, the Party will refer the request to its own Executive Board member. The Party's Executive Board member will then work with the Administering Agency to gather all responsive records. The Party receiving the request should work with its own public records staff (if any) to describe the request with sufficient specificity to allow the Administrative Agency to identify and provide the records being sought. If any clarification of the request is required, the Party receiving the request will obtain that clarification from the requester. All communication with the requester will be by the Party to whom the request was submitted.

In consultation with the Administering Agency, it will be the responsibility of the Party to whom the request for records was made to estimate the time it will need to produce responsive records, determine which records are responsive to the request, and address any redactions permitted by law. The Party that receives the request will bear the responsibility of complying with the Public Records Act in relation to its public records.

Nothing in this Section is intended to require a Party to collect or produce records that are not prepared, owned, used, or retained by that Party as contemplated by the Public Records Act.

17. INDEMNIFICATION AND HOLD HARMLESS.

a. Each Party will indemnify, defend and hold other Parties (including without limitation the Party serving as, and acting in its capacity as, SSHA³P's Administering Agency), their officers, officials, agents and volunteers harmless from any and all claims, injuries, damages, losses or suits including attorney fees, arising out of that Party's wrongful acts or omissions in connection with the performance of its obligations under this Agreement, except to the extent the injuries or damages are caused by another Party. If there is any recovery under this Section, the

Party responsible for any wrongful acts or omissions will pay any judgment or lien arising from the acts or omissions, including all costs and other Parties' reasonable attorney's fees. If more than one Party is held to be at fault, the obligation to indemnify and to pay costs and attorney's fees, will be only to the extent of the percentage of fault allocated to each respective Party by a final judgment of the court.

b. If a court of competent jurisdiction determine that this Agreement is subject to RCW 4.24.115, then, in the event of liability for damages arising out of bodily injury to persons or damages to property caused by or resulting from the concurrent negligence of a Party (including without limitation the Party serving as, and acting in its capacity as, SSHA³P's Administering Agency), its officers, officials, employees, and volunteers, the Party's liability hereunder will be only to the extent of the Party's negligence. It is further specifically and expressly understood that the indemnification provided herein constitutes the Party's waiver of immunity under Industrial Insurance Title 51 RCW, solely for the purpose of this indemnification. This waiver has been mutually negotiated by the Parties. The provisions of this Section will survive the expiration or termination of this Agreement.

c. Each Party (including without limitation the Party serving as, and acting in its capacity as, SSHA³P's Administering Agency) will give the other Parties proper notice of any claim or suit coming within the purview of these indemnities.

18. INSURANCE. The Executive Board, SSHA³P Executive Manager and the Administering Agency will take such steps as are reasonably practicable to minimize the liability of the Parties associated with their participation in this Agreement, including but not limited to the utilization of sound business practice. The Executive Board will determine which, if any, insurance policies may be reasonably practicably acquired to cover the operations of SSHA³P and the activities of the Parties pursuant to this Agreement (which may include general liability, errors and omissions, fiduciary, crime and fidelity insurance), and will direct the acquisition of same.

19. AMENDMENTS. Any amendments to this Agreement must be in writing. This Agreement may be amended upon approval of at least two-thirds of the legislative bodies of all Parties to this Agreement, evidenced by the authorized signatures of those Parties as of the effective date of the amendment. However, any amendment to this Agreement affecting the terms and conditions of membership

on the Executive Board, powers of the Executive Board, voting rights of Executive Board members, Party contribution responsibilities, hold harmless and indemnification requirements, provisions regarding duration, termination or withdrawal, or the conditions of this Section will require consent of the legislative authorities of all Parties. This Section will not be construed to require amendment of this Agreement for the addition of a new Party contemplated under Section 20 or for any related revision to Executive Board membership authorized in Section 5(a) or Advisory Board membership in Section 8(b). The Executive Board will review this Agreement, no less than every five years, to determine if any amendments or modifications are needed.

20. ADDITIONAL PARTIES. Municipalities, local governments, tribes, and public agencies within the SSHA³P Area of Activity may, on execution of the Agreement and approval of the budget and work plan by its legislative body, become a Party to this Agreement on affirmative vote of at least fifty percent of the Executive Board. The Executive Board will determine by a vote of at least fifty percent of its membership what, if any, funding obligations the additional Party will commit to as a condition of becoming a Party to this Agreement.

21. SEVERABILITY. The invalidity of any clause, sentence, paragraph, subdivision, section or portion of this Agreement, will not affect the validity of the remaining provisions of the Agreement.

22. CONFLICT RESOLUTION. Whenever any dispute arises between the Parties or between a Party or Parties, the Executive Board, or the Administering Agency (referred to collectively in this Section as the "Parties") under this Agreement which is not resolved by routine meetings or communications, the parties agree to seek resolution of such dispute in good faith by meeting, as soon as feasible. The meeting will include the Chair of the Executive Board, the SSHA³P Executive Manager, and the representative(s) of the Parties involved in the dispute. If the parties do not come to an agreement on the dispute, any party may pursue mediation through a process to be mutually agreed to in good faith between the parties within 30 days, which may include binding or nonbinding decisions or recommendations. The mediator(s) will be individuals skilled in the legal and business aspects of the subject matter of this Agreement. The parties to the dispute will share equally the costs of mediation and assume their own costs.

23. SURVIVABILITY. Notwithstanding any provision in this Agreement to the contrary, the provisions of Section 10 (Agreement

Duration), Section 11 (Termination of Agreement), Section 12 (Withdrawal), Section 16 (Public Records Requests), and Section 17 (Indemnification and Hold Harmless) will remain operative and in full force and effect, regardless of the withdrawal or termination of any Party or the termination of this Agreement.

24. WAIVER. No term or provision of this Agreement will be deemed waived and no breach excused unless that waiver or consent is in writing and signed by the Party claimed to have waived or consented.

25. SUBSEQUENT BREACH. Waiver of any default will not be deemed a waiver of any subsequent default. Waiver of breach of any provision of this Agreement will not be deemed to be a waiver of any other or subsequent breach and will not be construed to be a modification of the terms of the Agreement unless stated to be such through written approval of all Parties.

26. NOTICE. Any notice to the Executive Board will be in writing and will be addressed to the Chair of the Executive Board and to the SSHA^{3P} Executive Manager. In the absence of a SSHA^{3P} Executive Manager, notice will be given to the chief executive officer of the Administering Agency. Any notice to an Officer or Party will be sent, to the address specified by the chief executive officer of the Party.

27. ASSIGNMENT. No Party may sell, transfer or assign any of its rights or benefits under this Agreement without Executive Board approval.

28. APPLICABLE LAW AND VENUE. This Agreement and any rights, remedies, or obligations provided for in this Agreement will be governed, construed, and enforced in accordance with the substantive and procedural laws of the State. The Parties agree that the venue for any legal action under this Agreement is in the County in which a project is located, provided that the venue for any legal action against Pierce County may be filed in accordance with RCW 36.01.050.

29. RETAINED RESPONSIBILITY AND AUTHORITY. Parties retain the responsibility and authority for managing and maintaining their own internal systems and programs related to affordable/attainable housing activities.

30. INDEPENDENT CONTRACTORS. Each Party to this Agreement is an independent contractor with respect to the subject matter of this Agreement. No joint venture or partnership is formed as a

result of this Agreement.

Nothing in this Agreement will make any employee of one Party an employee of another party for any purpose, including, but not limited to, for withholding of taxes, payment of benefits, worker's compensation pursuant to Title 51 RCW, or any other rights or privileges accorded by virtue of their employment. No Party assumes any responsibility for the payment of any compensation, fees, wages, benefits or taxes to or on behalf of any other Party's employees. No employees or agents of one Party will be deemed, or represent themselves to be, employees of another Party.

31. NO THIRD PARTY BENEFICIARIES. This Agreement is for the benefit of the Parties only, and no third party will have any rights under this agreement.

32. NONDISCRIMINATION. The Parties will comply with the nondiscrimination requirements of applicable federal, state and local statutes and regulations.

33. COUNTERPARTS. This Agreement may be signed in counterparts and, if so signed, will be deemed one integrated Agreement.

34. FILING AND EFFECTIVE DATE; PRIOR AGREEMENTS. This Agreement will become effective (Date of Execution), subject to its approval by the legislative bodies of all jurisdictions who are members of SSHA^{3P} as of (Date of Execution), and, pursuant to RCW 39.34.040, upon recording this Agreement or posting this Agreement on a Party's web site or other electronically retrievable public source. Although this Agreement may be approved and signed by a Party after the Agreement's effective date, all acts consistent with the authority of this Agreement that occur on or after , (Date of Execution), are hereby ratified and affirmed, and the terms of this Agreement will be deemed to have applied.

(Signature Page Follows)

Approved and executed this _____ day of _____, 2019.

Name of Party: _____ Approved as to form

By: _____ City Attorney
Its: _____

Exhibit A - SSHA³P Area of Activity (Map)



NOTE: Per Steering Committee recommendation Area of Activity should be the entire county. Final IGA will include clearer map.

NOTE: The following budgets are draft and won't be finalized until the final number of member governments are identified. The numbers may also change depending the amount of outside revenue raised.

The following provides draft budgets for years 1 (6 months of operations) 2 and 3. There are two scenarios: one in which ten governments participate and the other in which 14 governments participate. If a government is not listed on the draft budgets the annual dues can be estimated based on the dues for another government with a similar population.

DRAFT BUDGET PROPOSAL (10 Governments)

YEAR 1 (2021) - 1 FTE

Funding for 6 Months Operations

Annual Expense Budget

Full-time manager with salary/taxes/benefits	\$	154,000
Administrative & policy support with salary/taxes/benefits (3 months)	\$	-
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	178,000
<i>10% Overhead/Administration Fee</i>	\$	<i>17,800</i>
TOTAL	\$	195,800
Cost for 6 months of Operations:	\$	97,900

Revenue Budget

Outside sources* (Philanthropy, aligned organizations)	\$	7,500
Office Space (in-kind donation) [Pro-rated]	\$	6,000
Shared among participating governments (see below)	\$	84,400
TOTAL	\$	97,900

Shares for participating governments:

	Population (OFM 2020 est)	% of Population	Grouped by Population Size
Auburn	9,980	1%	\$ 1,750
Fife	10,200	1%	\$ 1,750
Gig Harbor	11,240	1%	\$ 1,750
Lakewood	60,030	8%	\$ 8,000
Puyallup	42,700	5%	\$ 6,000
Puyallup Tribe	6,000	1%	\$ 1,000
Steilacoom	6,505	1%	\$ 1,000
Sumner	10,360	1%	\$ 1,750
Tacoma	213,300	27%	\$ 20,000
Unincorporated Pierce Co	426,200	54%	\$ 42,500
TOTAL	796,515		\$ 85,500

DRAFT STRAW PROPOSAL (10.26.20)

YEAR 2 (2022) - 1.5 FTE

10 Governments Providing Full Year of Operations Funding

Annual Expense Budget

Full-time manager with salary/taxes/benefits/inflation for 2nd year	\$	159,390
Administrative & policy support with salary/taxes/benefits/inflation 2nd yr	\$	67,636
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	251,026
<i>10% Overhead/Administration Fee</i>	<i>\$</i>	<i>25,103</i>
TOTAL	\$	276,128

Revenue Budget

Outside sources (Philanthropy, aligned organizations)	\$	15,000
Office Space (in-kind donation)	\$	12,000
Shared among participating governments (see below)	\$	249,128
TOTAL	\$	276,128

Shares for participating governments:

	Population (OFM 2020 est)	% of Population	Grouped by Population Size
Auburn	9,980	1%	\$ 4,500
Fife	10,200	1%	\$ 4,500
Gig Harbor	11,240	1%	\$ 4,500
Lakewood	60,030	8%	\$ 20,000
Puyallup	42,700	5%	\$ 15,000
Puyallup Tribe	6,000	1%	\$ 3,000
Steilacoom	6,505	1%	\$ 3,000
Sumner	10,360	1%	\$ 4,500
Tacoma	213,300	27%	\$ 65,000
Unincorporated Pierce Co	426,200	54%	\$ 130,000
TOTAL	796,515		\$ 254,000

YEAR 3 (2023) - 1.5 FTE
10 Governments Providing Full Year of Operations Funding

Annual Expense Budget

Full-time manager with salary/taxes/benefits/inflation for 3rd year	\$	164,969
Administrative & policy support with salary/taxes/benefits/inflation 3rd yr	\$	70,003
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	258,972
<i>10% Overhead/Administration Fee</i>	<i>\$</i>	<i>25,897</i>
TOTAL	\$	284,869

Revenue Budget

Outside sources (Philanthropy, aligned organizations)	\$	15,000
Office Space (in-kind donation)	\$	12,000
Shared among participating governments (see below)	\$	257,869
TOTAL	\$	284,869

Shares for participating governments:

	Population (OFM 2020 est)	% of Population	Grouped by Population Size
Auburn	9,980	1%	\$ 4,658
Fife	10,200	1%	\$ 4,658
Gig Harbor	11,240	1%	\$ 4,658
Lakewood	60,030	8%	\$ 20,700
Puyallup	42,700	5%	\$ 15,525
Puyallup Tribe	6,000	1%	\$ 3,105
Steilacoom	6,505	1%	\$ 3,105
Sumner	10,360	1%	\$ 4,658
Tacoma	213,300	27%	\$ 67,275
Unincorporated Pierce Co	426,200	54%	\$ 134,550
TOTAL	796,515		\$ 262,890

DRAFT BUDGET PROPOSAL (14 Governments)
YEAR 1 (2021) - 1 FTE
Funding for 6 Months Operations

Annual Expense Budget

Full-time manager with salary/taxes/benefits	\$	154,000
Administrative & policy support with salary/taxes/benefits (3 months)	\$	-
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	178,000
<i>10% Overhead/Administration Fee</i>	<i>\$</i>	<i>17,800</i>
TOTAL	\$	195,800
Cost for 6 months of Operations:	\$	97,900

Revenue Budget

Outside sources* (Philanthropy, aligned organizations)	\$	7,500
Office Space (in-kind donation) [Pro-rated]	\$	6,000
Shared among participating governments (see below)	\$	84,400
TOTAL	\$	97,900

Shares for participating governments:

	Population (OFM 2020 est)	% of Population	Grouped by Population Size
Auburn	9,980	1%	\$ 1,500
Bonney Lake	21,390	2%	\$ 3,000
Edgewood	12,070	1%	\$ 1,500
Fife	10,200	1%	\$ 1,500
Gig Harbor	11,240	1%	\$ 1,500
Lakewood	60,030	7%	\$ 7,000
Orting	8,635	1%	\$ 1,500
Puyallup	42,700	5%	\$ 5,000
Puyallup Tribe	6,000	1%	\$ 500
Steilacoom	6,505	1%	\$ 500
Sumner	10,360	1%	\$ 1,500
Tacoma	213,300	24%	\$ 18,500
University Place	33,310	4%	\$ 5,000
Unincorporated Pierce Co	426,200	49%	\$ 37,500
TOTAL	871,920		\$ 86,000

YEAR 2 (2022) - 1.5 FTE
14 Governments Providing Full Year of Operations Funding

Annual Expense Budget

Full-time manager with salary/taxes/benefits/inflation for 2nd year	\$	159,390
Administrative & policy support with salary/taxes/benefits/inflation for 2nd year	\$	67,636
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	251,026
<i>10% Overhead/Administration Fee</i>	<i>\$</i>	<i>25,103</i>
TOTAL	\$	276,128

Revenue Budget

Outside sources (Philanthropy, aligned organizations)	\$	15,000
Office Space (in-kind donation)	\$	12,000
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TOTAL	\$	276,128

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Fife	10,200	1%	\$ 4,000
Gig Harbor	11,240	1%	\$ 4,000
Lakewood	60,030	7%	\$ 18,000
Orting	8,635	1%	\$ 4,000
Puyallup	42,700	5%	\$ 11,000
Puyallup Tribe	6,000	1%	\$ 2,000
Steilacoom	6,505	1%	\$ 2,000
Sumner	10,360	1%	\$ 4,000
Tacoma	213,300	24%	\$ 58,000
University Place	33,310	4%	\$ 11,000
Unincorporated Pierce Co	426,200	49%	\$ 118,000
TOTAL	871,920		\$ 251,000

YEAR 3 (2023) - 1.5 FTE
14 Governments Providing Full Year of Operations Funding

Annual Expense Budget

Full-time manager with salary/taxes/benefits/inflation for 3rd year	\$	164,969
Administrative & policy support with salary/taxes/benefits/inflation for 3rd year	\$	70,003
Expenses - travel, phone, postage	\$	12,000
Space	\$	12,000
SubTotal	\$	258,972
<i>10% Overhead/Administration Fee</i>	<i>\$</i>	<i>25,897</i>
TOTAL	\$	284,869

Revenue Budget

Outside sources (Philanthropy, aligned organizations)	\$	15,000
Office Space (in-kind donation)	\$	12,000
Shared among participating governments (see below)	\$	257,869
TOTAL	\$	284,869

Shares for participating governments:

	Population (OFM 2020 est)	% of Population	Grouped by population size
Auburn	9,980	1%	\$ 4,140
Bonney Lake	21,390	2%	\$ 7,245
Edgewood	12,070	1%	\$ 4,140
Fife	10,200	1%	\$ 4,140
Gig Harbor	11,240	1%	\$ 4,140
Lakewood	60,030	7%	\$ 18,630
Orting	8,635	1%	\$ 4,140
Puyallup	42,700	5%	\$ 11,385
Puyallup Tribe	6,000	1%	\$ 2,070
Steilacoom	6,505	1%	\$ 2,070
Sumner	10,360	1%	\$ 4,140
Tacoma	213,300	24%	\$ 60,030
University Place	33,310	4%	\$ 11,385
Unincorporated Pierce Co	426,200	49%	\$ 122,130
TOTAL	871,920		\$ 259,785