SITE DEVELOPMENT PLANS FOR ROYAL CAR WASH

TOWN OF CANANDAIGUA, ONTARIO COUNTY, NEW YORK P.N. 20182698.0001

TAX ACCOUNT NO. 070.16-004-005.1 N/F SND RESTAURANT LLC

STORMWATER
MANAGEMENT AREA

TAX ACCOUNT NO. 070.16-004-048.1 N/F ARC TPCANNYY001 LLC #5150 NORTH STREET

TAX ACCOUNT NO. 070.16-004-006.1
Royal Car Wash Canandaigua, LLC
#2586 STATE ROUTE 332

ADE A.

SAND FILTER

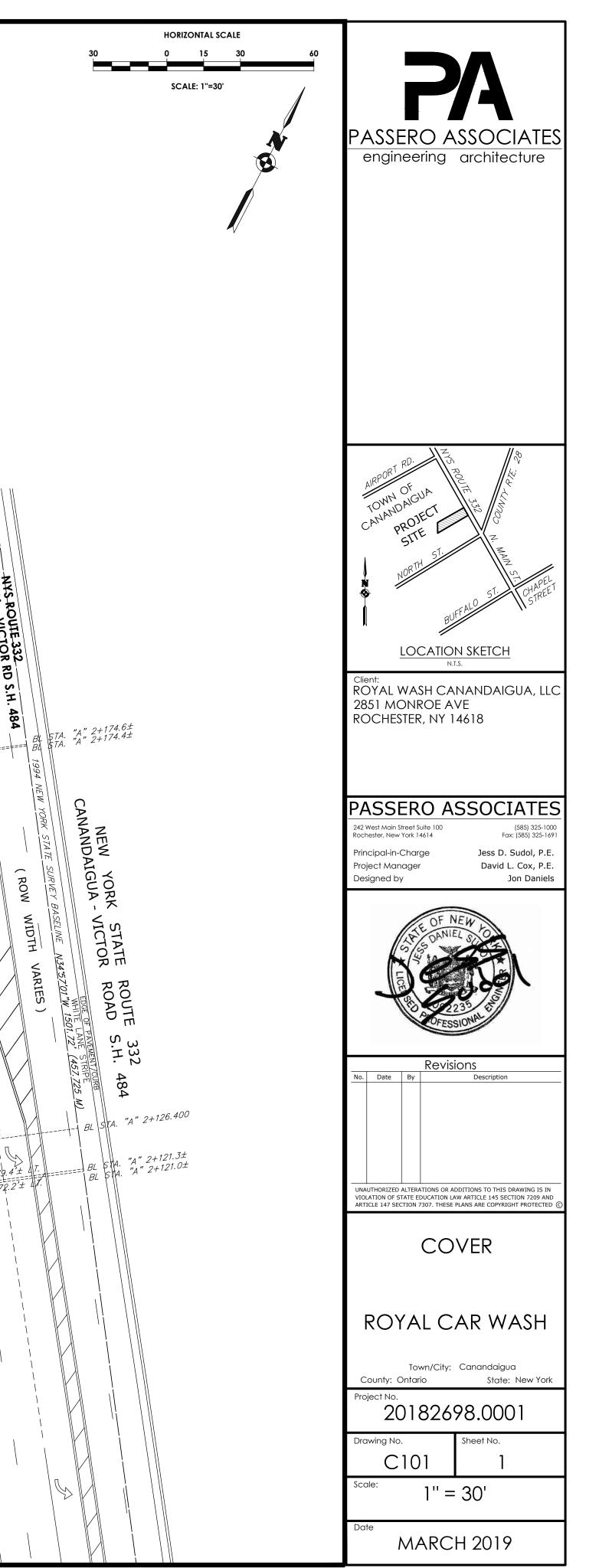
PROPOSED ROYAL

CAR WASH 4,096 S.F.

GEORGIAN MOTEL SIGN

ONE WAY SIGN CONC. WALK

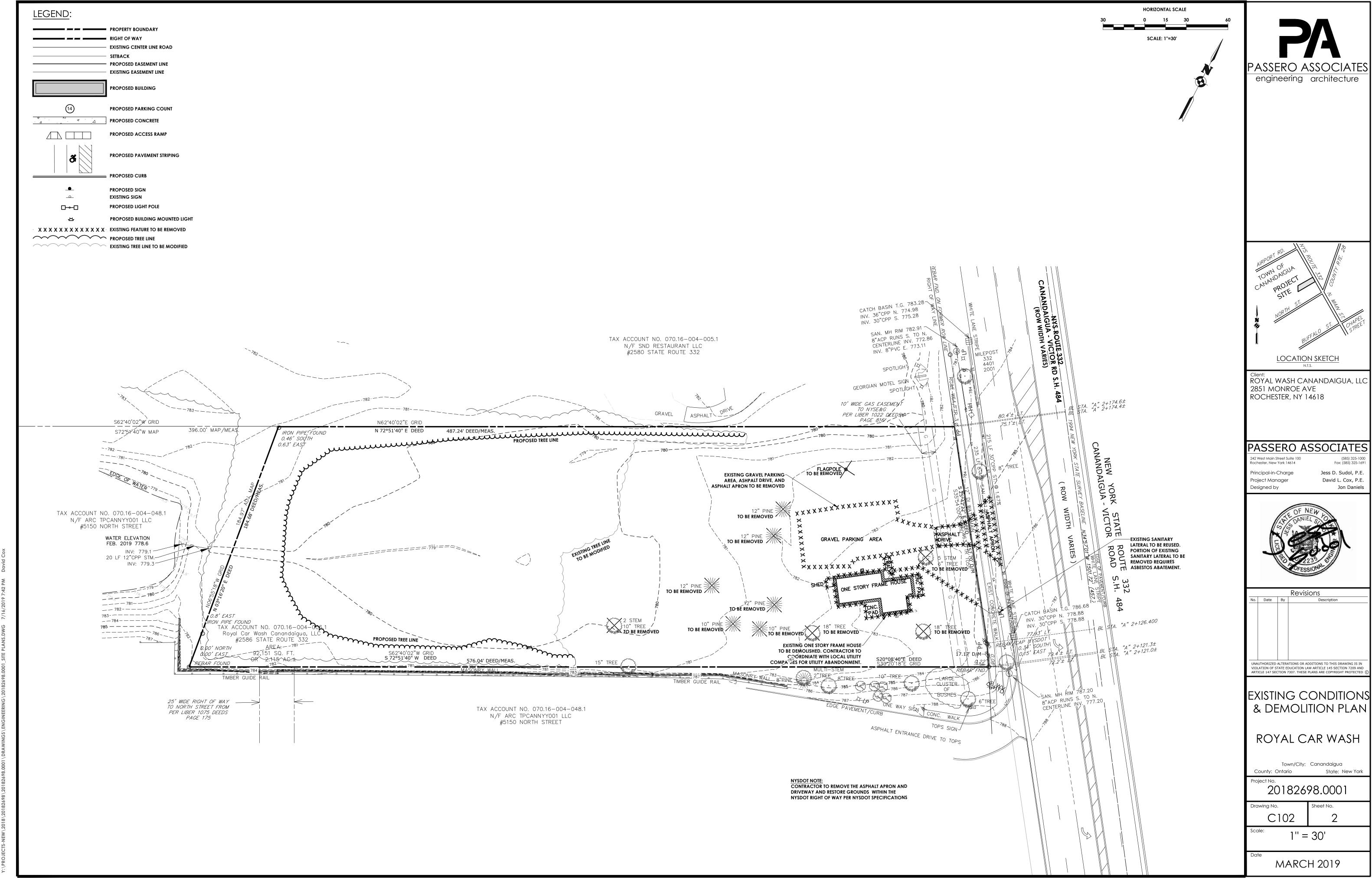
ASPHALT ENTRANCE DRIVE TO TOPS

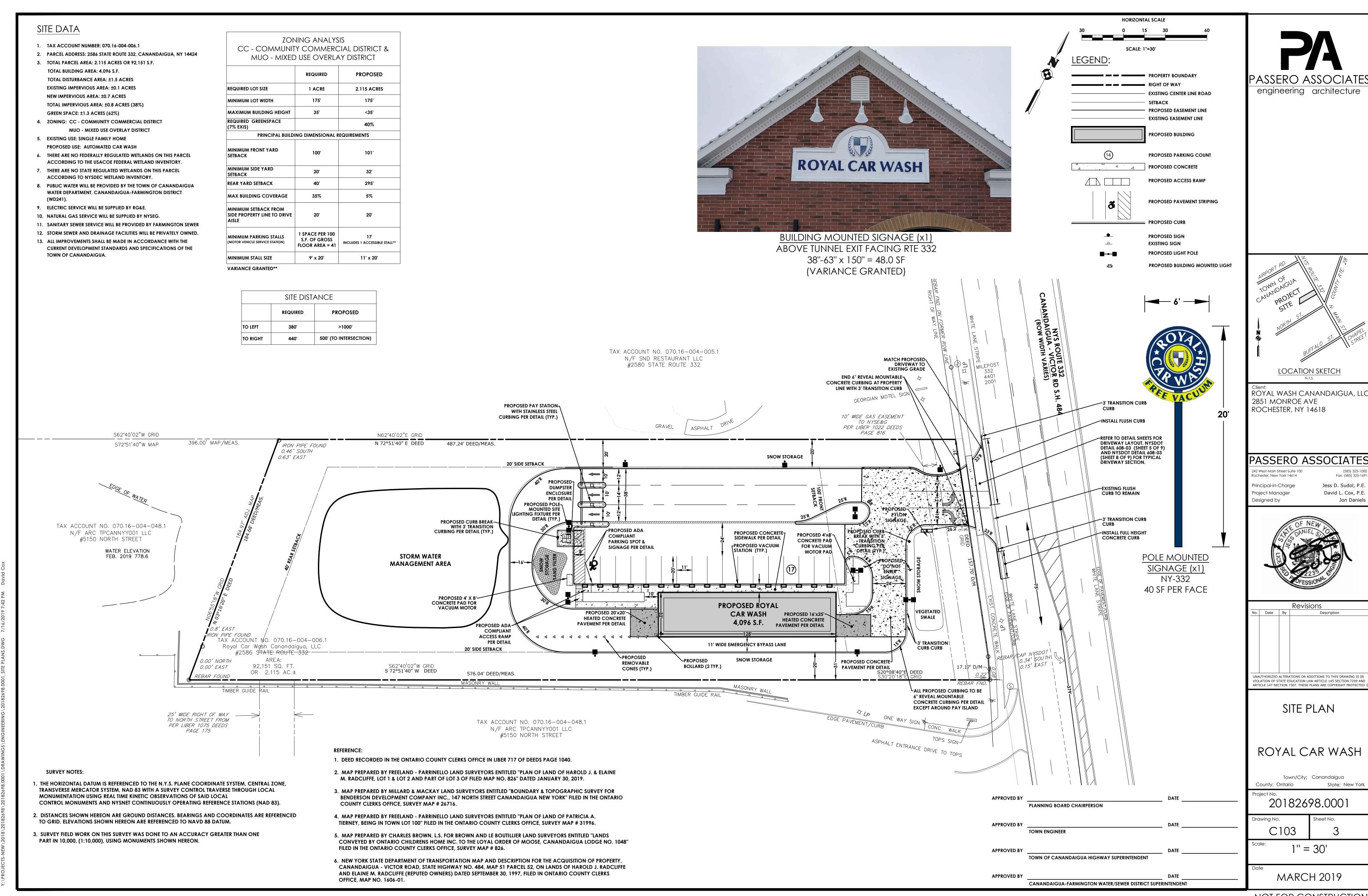


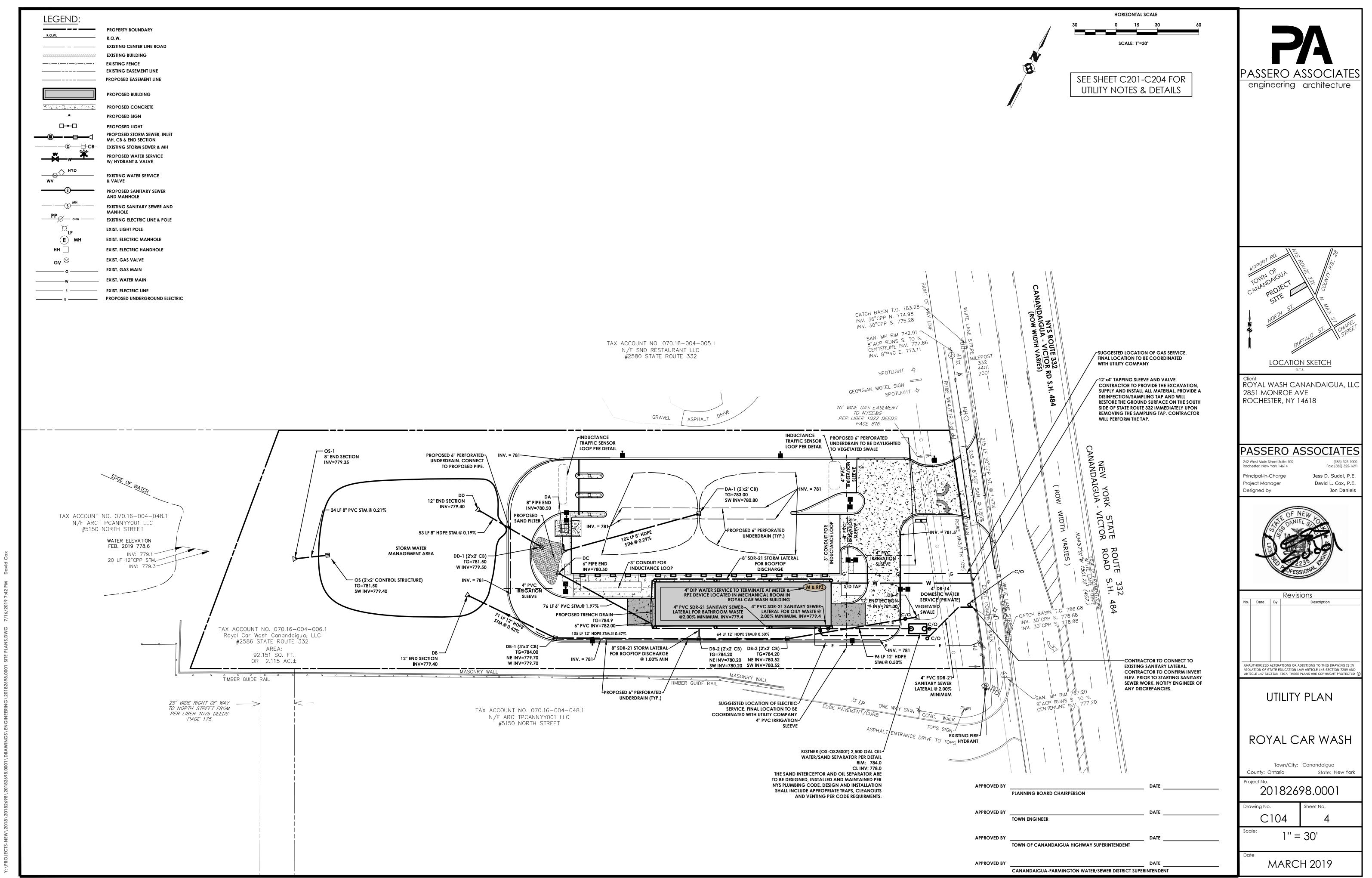
NYSDOT DESIGNER CONTACT STATEMENT

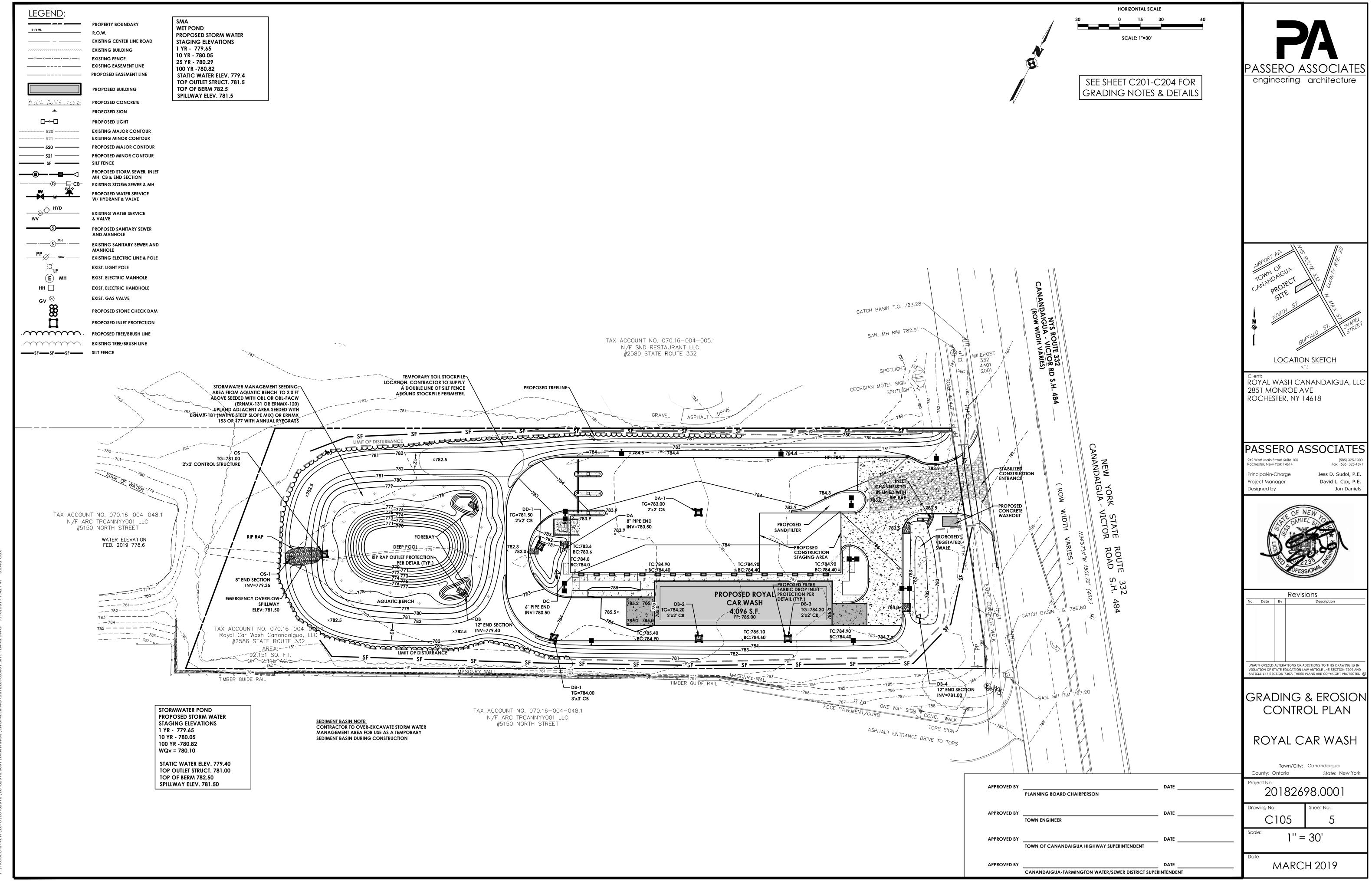
TAX ACCOUNT NO. 070.16-004-048.1 N/F ARC TPCANNYY001 LLC #5150 NORTH STREET

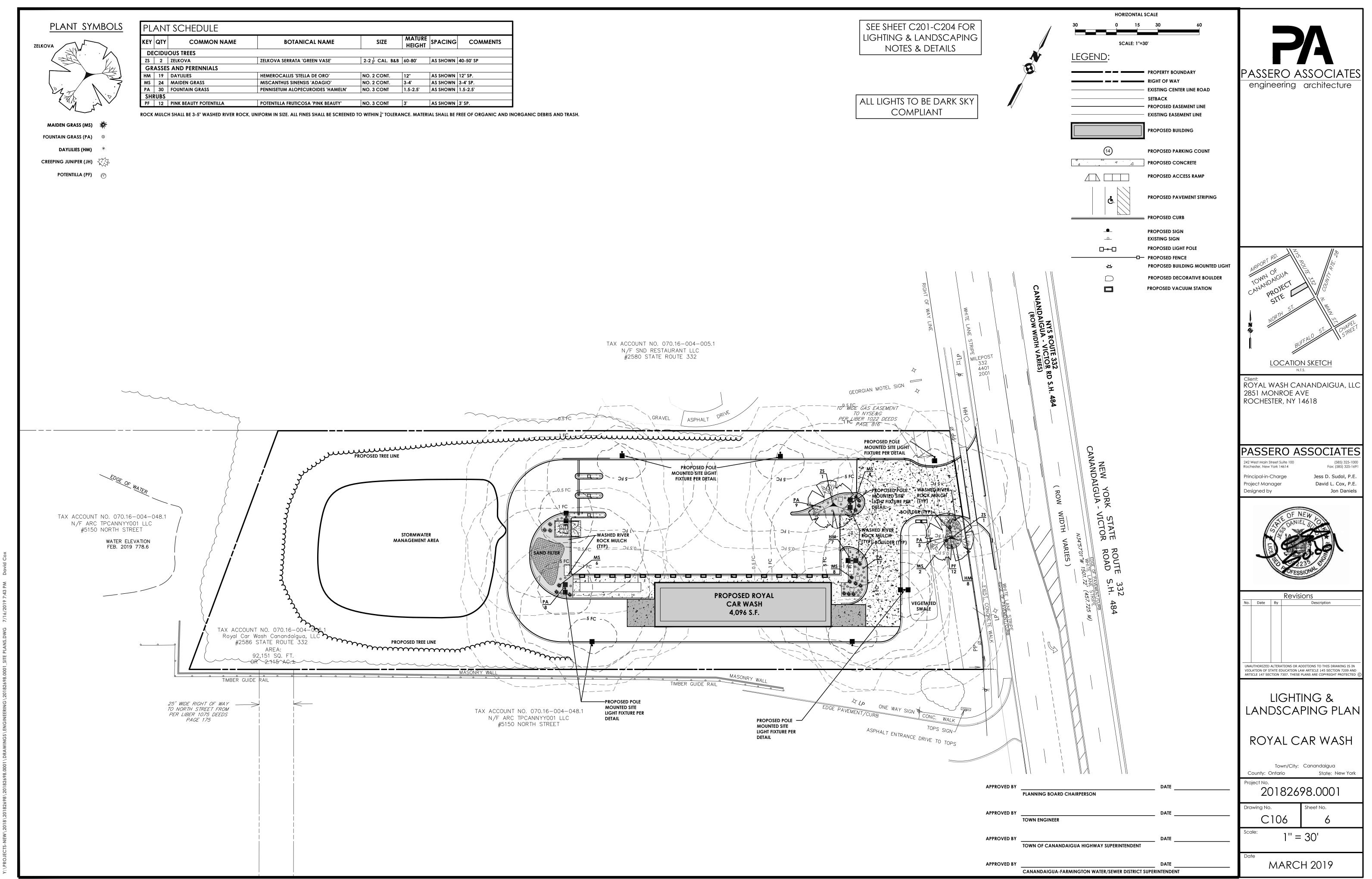
PASSERO ASSOCIATES IS RESPONSIBLE FOR THE DESIGN OF THIS PROJECT. DAVID COX, P.E. IS THE DESIGNER AND IS FAMILIAR WITH NYSDOT STANDARDS AND REQUIREMENTS AND SHALL BE CONTACTED AT (585) 325-1000 TO RESOLVE ISSUES OR PROBLEMS DURING CONSTRUCTION. ALL REVISIONS, INCLUDING REVISIONS NECESSARY DUE TO FIELD CONDITIONS, SHALL BE APPROVED BY THE NYSDOT.











- INSTALL STABILIZED CONSTRUCTION ENTRANCE.
- INSTALL POND AREAS.
- CONSTRUCT DRAINAGE SWALES ALONG PROPERTY LINES AS SHOWN.
- CLEAR AND GRUB THE PROJECT IMPROVEMENTS AREAS. STRIP TOPSOIL AND TRUCK EXCESS OFF-SITE.
- GRADE IMPROVEMENTS AREAS WITHIN THE PROJECT SITE. AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR MORE THAN 7 DAYS WILL BE STABILIZED WITH A TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST DISTURBANCE.
- 8. CONSTRUCT DROP INLET PROTECTION DEVICES AFTER STORM SEWER INSTALLATION AS SHOWN ON THIS
- REPLACE TOPSOIL AND FINE GRADE.
- 10. HYDRO-SEED ALL DISTURBED AREAS WITHIN 7 DAYS AFTER FINAL GRADING, CONTRACTOR IS RESPONSIBLE TO RESEED IF GRADING IS UNSATISFACTORY
- 11. UPON APPROVAL OF THE TOWN, REMOVE ALL TEMPORARY SILTATION CONTROLS.
- 12. SLOPES SHALL NOT EXCEED 1' VERTICAL TO 3' HORIZONTAL MAX. MAINTAIN 1:4 WHERE POSSIBLE 13. MINIMUM OF 6" OF TOPSOIL IS TO BE PLACED ON ALL GRASS AREAS.
- 14. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BASED UPON ACTUAL FIELD CONDITIONS AOBE. CONTRACTOR SHALL PROVIDE FOR THIS COST IN HIS CONTRACT.
- 15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SILTATION AND EROSION CONTROL MEASURES FROM INSTALLATION THROUGH MAINTENANCE AND REMOVAL AFTER REVEGETATION HAS BEEN ESTABLISHED. 14 ALL END SECTIONS WILL BE PROVIDED WITH RIP-RAP APRONS.
- 17. ALL EROSION AND SEDIMENT CONTROL METHODS WILL BE DESIGNED AND INSTALLED IN ACCORDANCE WITH THE LATEST EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL
- 18. ALL EROSION CONTROL MEASURES WILL BE REMOVED AFTER APPROVED BY QUALIFIED PROFESSIONAL.

AT THE VERY MINIMUM, EROSION CONTROL SHALL BE AS SHOWN ON THIS PLAN. EROSION CONTROL MAY CONSIST OF SEDIMENT TRAPS AND/OR ENVIRONMENTAL FENCES. THE CONTRACTOR AND THE DEVELOPER SHALL BE RESPONSIBLE FOR THE INTEGRITY. MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES UNTIL NO LONGER DEEMED NECESSARY BY THE TOWN OR IT'S REPRESENTATIVE. THE CONTRACTOR SHALL MAINTAIN THE STORM SEWER SYSTEM UNTIL THE PROJECT IS DEVELOPED AND APPROVED BY THE TOWN AND

ALL EROSION CONTROL MEASURES SHALL BE MAINTAINED IN GOOD WORKING ORDER. THE OWNER SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE STONE FILL. CORRECTIVE ACTIONS. AS IDENTIFIED BY THE DEVELOPER'S ONSITE REPRESENTATIVE OR A TOWN REPRESENTATIVE. SHALL BE INITIATED WITHIN 24 HOURS OF BEING REPORTED. THE TOWN MAY REVIEW THE PROJECT SITE AT ANY TIME. REVIEW OF EROSION CONTROL MEASURES BY THE TOWN DOES NOT RELIEVE THE DEVELOPER OF HIS OBLIGATIONS TO EXECUTE RESPONSIBLE STANDARD MANAGEMENT PRACTICES

CONSTRUCTION SEQUENCE FOR STEEP SLOPE STABILIZATION/ JUTE MESH INSTALLATION FOR SLOPES OF 1:3 OR GREATER:

- STRIP TOPSOIL ON SLOPE.
- GRADE SLOPE PER PLAN. CONTRACTOR SHALL TAKE CARE TO NOT OVER EXCAVATE EMBANKMENT. REPLACE TOPSOIL.
- TRACK TOPSOIL WITH A BULLDOZER IN A DIRECTION PERPENDICULAR TO THE PROPOSED CONTOUR LINES.
- IMMEDIATELY SEED AND FERTILIZE TOPSOIL PER PLAN. IMMEDIATELY APPLY JUTE MESH BY UNROLLING THE PRODUCT DOWN THE SLOPE. DO NOT STRETCH THE
- MESH, ALLOWING IT TO FULLY CONTACT THE SOIL SECURE THE JUTE MESH WITH STAPLES (11 GAUGE - 8"x1"x8") 24" APART THOUGH OUT THE MATTING
- (APPROXIMATELY 200 STAPLES PER 100 SY). STAPLES SHOULD BE DRIVEN FLUSH WITH THE GROUND. SECURE BEGINNING AND END OF ROLL BY ANCHORING THE MATTING INTO 6" DEEP SLOTS CUT INTO THE SOIL. THEN STAPLE MATTING AT CHECK SLOTS.

PUBLIC WATER SERVICE LINE NOTES (4" & GREATER)

- 1. WATER SERVICE LINES (LATERALS) SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE
- REGULATION AND SPECIFICATIONS OF THE CITY OF CANANDAIGUA. WATER SERVICE LINES SHALL HAVE A MINIMUM OF FIVE FEET OF COVER FROM FINISHED GRADE IN LAWN AREAS AND SIX FEET COVER FROM FINISHED GRADE IN
- PAVED AREAS 3. WATER SERVICE LINES SHALL BE SEPARATED AT LEAST TEN FEET, MEASURED FROM THE
- **OUTSIDE OF THE PIPES, FROM SEWER MAINS OR SEPTIC SYSTEMS** WATER SERVICE LINES SHALL BE AS IDENTIFIED AS:

DESCRIPTION	SIZE	MATERIAL (a)	TYPE (b)
CITY PORTION = FROM THE WATER MAIN TO AND INCLUDING THE CONTROL VALVE ON THE ROW/PROPERTY/EASEMENT LINE	4"	DR-14	СМВ
PRIVATE PORTION = FROM THE CONTROL VALVE TO THE RPZ	4"	DR-14	СМВ

(a) ACCEPTABLE MATERIAL IS *POLY WRAPPED CLASS 52 CEMENT MORTAR LINED DUCTILE **IRON PIPE OR DR-14**

(b) SERVICE TYPES INCLUDE:DOMESTIC = DS, FIRE = FS, OR COMBINED = CMB

- 5. THE PUBLIC PORTION OF THE WATER SERVICE LINE SHALL BE INSTALLED PRIOR TO THE PRIVATE PORTION OF THE SERVICE LINE.
- 6. WATER METER(S) TO BE LOCATED ON THE INTERIOR OF EXTERIOR WALL(S) IMMEDIATELY UPON SERVICE ENTRANCE INTO THE BUILDING(S). RESIDENTIAL SERVICES: A BY-PAS: ASSEMBLY IS NOT REQUIRED AROUND THE INSTALLATION OF & INCH THROUGH 2-INCH
- METERS. NON-RESIDENTIAL SERVICES: THE INSTALLATION OF AN 1½ INCH METER OR LARGER REQUIRES A BY-PASS ASSEMBLY AROUND THE METER. 7. PUBLIC WATER LINES SHALL BE PRESSURE TESTED TO 150 PSI OR 150% OF STATIC LINE PRESSURE, WHICHEVER IS
- GREATER, IN ACCORDANCE WITH THE LATEST SPECIFICATIONS OF THE CANANDAIGUA-FARMINGTON WATER DEPT. A REPRESENTATIVE MUST WITNESS THIS
- DISINFECTED BY USING THE CONTINUOUS FEED METHOD ACCORDING TO AWWA STANDARD SPECIFICATIONS. AFTER FLUSHING AND DISINFECTING THE SERVICE LINE, WATER SAMPLES SHALL BE COLLECTED BY THE CONTRACTOR. APPROVAL AND NOTIFICATION BY THE HEALTH DEPARTMENT MUST BE RECEIVED BEFORE THE LATERAL IS PLACED IN SERVICE. 8. PRIVATE WATER LINES SHALL BE:
- PRESSURE TESTED TO 150 PSI IN ACCORDANCE WITH THE LATEST SPECIFICATIONS OF THE CANANDAIGUA-FARMINGTON WATER DEPT. A REPRESENTATIVE MUST
- DISINFECTED BY USING THE CONTINUOUS FEED METHOD ACCORDING TO AWWA STANDARD SPECIFICATIONS. AFTER FLUSHING AND DISINFECTING THE SERVICE LINE, WATER SAMPLES SHALL BE COLLECTED BY THE CONTRACTOR AND SUBMITTED TO A THIRD PARTY TESTING COMPANY.

FARMINGTON WATER MAIN NOTES

PROVISIONS FOR MEETING THE FLUSHING AND TESTING WATER SHALL BE MADE. WATER USED MUST BE PURCHASED BY THE DEVELOPER.

- WATER PIPING SHALL BE FLUSHED AND TESTED IN CONFORMANCE WITH THE LATEST REVISION OF ANSI/AWWA C600 STANDARD FOR DUCTILE-IRON PIPE, C605 FOR PVC PIPE, OR EQUIVALENT OF C600 AND/OR C605 FOR PE PIPE.
- THE PROPOSED WORKS MUST CONFORM TO LATEST REVISION OF ANSI/AWWA C651 STANDARD, TABLET METHOD EXCEPTED. FOLLOWING FLUSHING AND TESTING, THE ENGINEER SHALL OVERSEE COLLECTION OF 2 CONSECUTIVE DAYS OF AN APPROPRIATE NUMBER OF BACTERIOLOGICAL SAMPLES FOR TOTAL AND FECAL COLIFORM AND FOR STANDARD BACTERIAL PLATE COUNT AFTER THE FIELD FREE CHLORINE RESIDUAL IS LESS THAN 1.5 PPM AND THE SAMPLING POINTS HAVE BEEN DECONTAMINATED, PRIOR TO SAMPLING, THE ENGINEER SHALL COORDINATE THE APPROPRIATE NUMBER AND LOCATIONS OF SAMPLES TO BE COLLECTED WITH THE COUNTY OR STATE HEALTH DEPARTMENT HAVING JURISDICTION.
- AFTER FLUSHING OF THE NEWLY DISINFECTED MAIN AND IN ACCORDANCE WITH THE SECTION ABOVE, THE CONTRACTOR SHALL OBTAIN SAMPLES OF WATER AND SUBMIT THEM TO A LABORATORY APPROVED BY THE NEW YORK STATE DEPARTMENT OF HEALTH. UPON THE RECEIPT OF A SATISFACTORY LABORATORY REPORT, THIS INFORMATION TOGETHER WITH THE DESIGN ENGINEER'S CERTIFICATE OF CONSTRUCTION SHALL BE SUBMITTED TO THE NEW YORK STATE DEPARTMENT OF HEALTH FOR APPROVAL, UPON RECEIPT OF THE APPROVAL OF COMPLETED WORK FROM THE HEALTH DEPARTMENT, THE WATER SYSTEM SHALL BE CONSIDERED COMPLETE AND MAY BE ACCEPTED FOR SERVICE
- IN ANY AREAS WHERE SATISFACTORY RESULTS OF APPLIED TESTS CANNOT BE OBTAINED, THE DEFECTIVE PORTION OF THE SYSTEM SHALL BE LOCATED AND REPLACED WITH NEW
- THAT PORTION OF THE SYSTEM SHALL THEN BE RETESTED UNTIL SATISFACTORY RESULTS ARE OBTAINED. USE OF REPAIR CLAMPS WILL NOT BE PERMITTED BY THE TOWN.

STANDARD WATER MAIN EXTENSION NOTES:

- THE WATER MAIN SHALL BE DISINFECTED EQUAL TO AWWA STANDARD FOR DISINFECTING WATER MAINS DESIGNATION C651. FOLLOWING DISINFECTION, THE WATER MAIN SHALL BE FLUSHED UNTIL THE CHLORINE CONCENTRATION IN THE WATER LEAVING THE MAIN IS NO HIGHER THAN THAT GENERALLY PREVAILING IN THE SYSTEM. THE SAMPLING POINT(S) MUST BE DECONTAMINATED BY FLAMING. ALL WATER MAIN FITTINGS NOT RECEIVING 24-HOUR CHLORINE DISINFECTION CONTACT TIME MUST BE SWAB DISINFECTED 30 MINUTES PRIOR TO INSTALLATION. THE SAMPLING POINT(S) MUST BE DECONTAMINATED BY FLAMING. FIRE HYDRANTS ARE NOT ACCEPTABLE SAMPLING POINTS. SAMPLING WILL NOT BE PERFORMED PRIOR TO RECEIPT FROM A NEW YORK STATE LICENSED OR REGISTERED DESIGN PROFESSIONAL (ENGINEER, ARCHITECT OR LAND SURVEYOR WITH A SPECIAL EXEMPTION LINDER SECTION 7208(n) OF THE EDUCATION LAW) CERTIFYING THAT THE WATER SUPPLY IMPROVEMENTS, TESTING AND DISINFECTION PROCEDURES WERE COMPLETED IN ACCORDANCE WITH THE APPROVED PLANS, REPORTS, SPECIFICATIONS AND ANY APPROVED AMENDMENTS. SAMPLES SHALL BE COLLECTED FOR FREE CHLORINE RESIDUAL, TOTAL AND FECAL COLIFORM AND 24-HOUR BACTERIAL PLATE COUNT AND TAKEN TO A NYS CERTIFIED LABORATORY AND RESULST PROVIDED TO NYSDOH. THE WATER MAIN SHALL NOT BE PLACED INTO SERVICE UNTIL SO AUTHORIZED BY THE NYS DEPARTMENT OF HEALTH.
- MINIMUM VERTICAL SEPARATION BETWEEN WATER MAINS AND SEWER LINES SHALL BE 18 INCHES MEASURED FROM THE OUTSIDE OF THE PIPE AT THE POINT OF CROSSING. ONE FULL STANDARD LAYING LENGTH OF WATER MAIN SHALL BE CENTERED UNDER OR OVER THE SEWER SO THAT BOTH JOINTS WILL BE AS FAR FROM THE SEWER AS POSSIBLE, IN ADDITION, WHEN THE WATER MAIN PASSES UNDER A SEWER. ADEQUATE STRUCTURAL SUPPORT (COMPACTED SELECTED FILL) SHALL BE PROVIDED FOR THE SEWER TO PREVENT EXCESSIVE DEFECTION OF JOINTS AND SETTLING OF THE SEWER ON THE WATER MAIN. MINIMUM HORIZONTAL SEPARATION BETWEEN PARALLEL WATER MAINS AND SEWER PIPES (INCLUDING MANHOLES AND VAULTS) SHALL BE 10 FEET MEASURED FROM THE OUTSIDE OF THE PIPES, MANHOLES OR VAULTS.
- WHEN INSTALLING FIRE HYDRANTS, SHOULD GROUND WATER BE ENCOUNTERED WITHIN SEVEN (7) FEET OF E FINISHED GRADE, FIRE HYDRANT WEEP HOLES (DRAINS) SHALL BE PLUGGED.
- THE WATER MAIN SHALL BE PRESSURE/LEAKAGE TESTED IN ACCORDANCE WITH THE MINIMUM REQUIREMENTS OF THE AWWA STANDARD C600 (LATEST REVISION) OR IN ACCORDANCE WITH MORE STRINGENT REQUIREMENTS IMPOSED BY THE SUPPLIER OF WATER.

ARCHITECTURAL COORDINATION NOTE:

THE CONTRACTOR IS RESPONSIBLE TO COORDINATE ALL UTILITY CONNECTIONS WITH MECHANICAL/ARCHITECTURAL DRAWINGS FOR LOCATION, PENETRATION, AND SIZES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WITH ALL SUBCONTRACTORS FOR PROPER UTILITY CONNECTIONS. ANY DISCREPANCIES SHALL BE IMMEDIATELY DESCRIBED TO ENGINEER AND ARCHITECT.

ELECTRIC & GAS SERVICE COORDINATION NOTES: 1. ELECTRIC AND PLUMBING CONTRACTORS SHALL COORDINATE WITH EACH OTHER AND DIRECTLY WITH ROCHESTER GAS & ELECTRIC (RG&E) ELECTRIC AND GAS DEPARTMENTS FOR METER AND

- SERVICE LOCATIONS PRIOR TO INSTALLATION. 2. ELECTRIC AND PLUMBING CONTRACTORS SHALL ALLOW FOR THE NECESSARY PROVISIONS WHEN INSTALLING METERS AND SERVICES IN ACCORDANCE WITH RG&E DESIGN AND INSTALLATION
- 3. ELECTRIC AND PLUMBING CONTRACTORS SHALL DIRECTLY COORDINATE WITH EACH OTHER AND DIRECTLY WITH RG&E FOR ELECTRIC AND GAS START-UP.

LANDSCAPING NOTES:

- ALL PLANTS SHALL MEET OR EXCEED THE MINIMUM REQUIREMENTS AS NOTED IN THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK BY AMERICAN NURSERY AND LANDSCAPE **ASSOCIATION**
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIS OWN QUANTITY TAKEOFF. CONTRACTOR SHALL STAKEOUT PLANTINGS FOR OWNER'S APPROVAL PRIOR TO BEGINNING WORK.
- THE CONTRACTOR IS HEREBY NOTIFIED THAT UNDERGROUND UTILITIES EXIST AND HE SHOULD CALL FOR A COMPLETE UTILITY STAKEOUT BEFORE COMMENCING WORK.
- SHOULD LOCATION OF TREES BE WITHIN 5' OF UNDERGROUND UTILITIES, RELOCATE SAID TREES TO MIN. OF 5' FROM BALL TO UTILITIES. SHOULD LOCATIONS OF TREES BE WITHIN 20' OF OVERHEAD WIRES, RELOCATE SAID TREES TO MINIMUM OF
- 20' FROM WIRES.
- PLANTING BACKFILL MIXTURE
- 4 PARTS TOPSOIL; 1 PART PEAT MOSS, 10 LBS. 5-0-5 PLANTING FERTILIZER THOROUGHLY MIXED PER CUBIC YARD.
- STAKE TREES IMMEDIATELY AFTER PLANTING.
- PROVIDE BLACK HARD RUBBER EDGING AT ALL PLANTING AREAS. 10. ALL SEEDED AREAS SHALL RECEIVE 10:0:10 COMMERCIAL FERTILIZER APPLIED AT A RATE OF 20LBS./1000 S E NO FERTILIZER CONTAINING PHOSPHORUS IS PERMITTED ON SITE
- 11. ALL NEWLY SEEDED AREA SHALL RECEIVE STRAW AT A RATE OF 2000 LBS/ACRE OR HYDROSEED METHOD MAY BE UTILIZED
- 12. FINE GRADE & HAND RAKE SOIL IN AREAS TO RECEIVE NEW SEED
- 13. ALL AREAS TO BE SEEDED SHALL RECEIVE 6" DEPTH OF TOPSOIL. 14. ANY PROPOSED DEVIATION TO THIS PLAN MUST FIRST BE REVIEWED AND APPROVED BY THE PLANNING
- BOARD AND TOWN ENGINEER, PRIOR TO THE INSTALLATION OF THOSE PROPOSED LANDSCAPE CHANGES. 15. RED MAPLE, BLACK TUPELO, BIRCH, OAK AND ZELKOVA ARE CONSIDERED FALL PLANTING HAZARD. CONTRACTOR SHALL TAKE SPECIAL CARE WHEN PLANTING AND WATERING THESE TREES.
- . NO PHOSPHOROUS SHALL BE USED AT PLANTING TIME UNLESS SOIL TESTING HAS BEEN COMPLETED AND TESTED BY A HORTICULTURAL TESTING LAB AND THE SOIL TESTS SPECIFICALLY INDICATE A PHOSPHOROUS DEFICIENCY THAT IS HARMFUL, OR WILL PREVENT NEW LAWNS AND PLANTINGS FROM ESTABLISHING
- 17. IF SOIL TESTS INDICATE A PHOSPHOROUS DEFICIENCY THAT WILL IMPACT PLANT AND LAWN ESTABLISHMENT, PHOSPHOROUS SHALL BE APPLIED AT THE MINIMUM RECOMMENDED LEVEL PRESCRIBED IN THE SOIL TEST FOLLOWING ALL MYSIDEC REQUIREMENTS

SEEDING NOTE

TO PROVIDE THE FOLLOWING SEED MIXTURES DURING CONSTRUCTION:

TEMPORARY SEEDING: RYEGRASS (ANNUAL OR PERENNIAL) @ 120 LBS. PER ACRE OR 0.7 LBS. PER 1000 SQUARE FEET.

65% KENTUCKY BLUEGRASS BLEND @ 85LBS. PER ACRES OR 2.0-2.6 LBS. PER 1000 SQUARE FEET 20% PERENNIAL RYEGRASS @ 26-35 LBS. PER ACRE OR 0.6-0.8 LBS. PER 1000 SQUARE FEET 15% FINE FESCUE @ 19-26 LBS. PER ACRE OR 0.4-0.6 LBS. PER 1000 SQUARE FEET

FOR ALL SEEDING & STABILIZATION MEASURES IT IS THE RESPONSIBILITY OF THE OWNER & DEVELOPER TO ENSURE THAT FINAL STABILIZATION OCCURS AS REQUIRED BY THE NYSDEC. AREAS WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES FOR MORE THAN 7 DAYS WILL BE STABILIZED WITH A TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST DISTURBANCE. ONCE CONSTRUCTION ACTIVITY CEASES PERMANENTLY IN AN AREA, THAT AREA WILL BE STABILIZED WITH PERMANENT SEED AND MULCH. AFTER THE ENTIRE SITE IS STABILIZED, THE ACCUMULATED SEDIMENT WILL BE REMOVED FROM THE SEDIMENT BASIN(s). **UTILITY NOTES:**

- 1. PRIOR TO THE START OF UTILITY INSTALLATION THE CONTRACTOR AND SUBCONTRACTOR IS RESPONSIBLE FOR COORDINATION OF ALL UTILITY CONNECTIONS WITH MECHANICAL/ARCHITECTURAL DRAWINGS FOR INCLUDING BUT NOT LIMITED TO VERTICAL AND HORIZONTAL LOCATION, PENETRATIONS, AND SIZES, THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION TO PROCEED WITH UTILITY INSTALLATION BY THE OWNERS ONSITE REPRESENTATIVE UPON COMPLETION OF COORDINATION WITH CONTRACTORS, AND
- 2. THE DEVELOPER AND HIS/HER CONTRACTOR IS RESPONSIBLE FOR COORDINATING GAS, ELECTRICAL, CABLE, TELEPHONE AND ANY OTHER UTILITIES NOT SPECIFICALLY SHOWN WITHIN THIS PLAN SET WITH APPROPRIATE AGENCY, PASSERO ASSOCIATES ASSUMES NO RESPONSIBILITY FOR THE DESIGN OR PERFORMANCE OF UTILITIES NOT SPECIFICALLY SHOWN WITHIN THIS PLAN SET.
- 3. PRIOR TO THE START OF UTILITY INSTALLATION THE CONTRACTOR SHALL LOCATE ALL EXISTING UTILITIES VERTICALLY AND HORIZONTALLY AND COORDINATE WITH EXISTING UTILITIES SHOWN ON THE PLANS AND REPORT ANY DISCREPANCIES TO THE DESIGN ENGINEER. THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION TO PROCEED WITH UTILITY INSTALLATION BY THE OWNERS ONSITE REPRESENTATIVE UPON COMPLETION EXISTING UTILITY VERIFICATION.
- 4. THRUST BLOCKS ON THE WATERMAIN ARE REQUIRED AT BENDS, TEES OR PLUGS. SEE DETAIL SHEETS FOR THRUST BLOCK DETAILS.

STORM NOTES

- STORM SEWERS AND APPURTENANCES SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE LATEST **REGULATIONS OF THE MUNICIPALITY**
- 2. PROPOSED STORM SEWER LATERAL MATERIAL: PVC SDR-35 6" MIN. SIZE & SHALL BE LAID AT A MINIMUM GRADE OF 1/4" PER FT. **ADS HDPE 12" MIN**
- FOUNDATION DRAINS SHALL BE CONNECTED TO STORM WATER SYSTEM VIA SUMP PUMPS. DOWNSPOUTS SHALL BE CONNECTED TO STORM SEWER WHERE APPLICABLE, WHERE NOTED ON THE PLANS DOWNSPOUTS SHALL DISCHARGE TO SPLASH BLOCKS.
- 4. UPON COMPLETION OF SYSTEM INSTALLATION, THE MAIN SEWER SYSTEM AND LEADS TO STRUCTURES SHALL BE FLUSHED AND LAMPED TO THE SATISFACTION OF THE MUNICIPALITY.

DEMOLITION NOTES

- 1. MILL ASPHALT PER PLAN AND STOCKPILE FOR RE-USE.
- FOR FULL DEPTH PAVEMENT EXCAVATION; THE TOP OF THE EXISTING CURB IS TO REMAIN AND SHALL HAVE A REVEAL OF 6" AT THE COMPLETION OF CONSTRUCTION. EXISTING SUITABLE SUB-BASE ENCOUNTERED DURING EXCAVATION SHALL BE STOCKPILED FOR RE-USE. EXCESS MATERIALS OR UNSUITABLE MATERIAL SHALL BE HAULED OFF-SITE.
- PREPARE AND PLACE A SUB-BASE COURSE COMPOSED OF A BLEND OF THE EXISTING SUB-BASE (FROM STOCKPILE), ASPHALT MILLINGS (FROM STOCKPILE) AND IF NEEDED, ADDITIONAL STONE AGGREGATE TO OBTAIN THE SPECIFIC DEPTH OF AND THE PLACEMENT OF 1" OF CRUSHED STONE FOR FINE GRADING PURPOSES.
- 4. PAVE ENTIRE PARKING AREA WITH TYPE 7F TOP COURSE ASPHALT.
- 5. UPON COMPLETION, ALL PAVEMENT AREAS SHALL BE SMOOTH, ALL CURB SHALL HAVE 6" REVEAL, WATER SHALL DRAIN WELL INTO EXISTING CATCH BASINS (NO PONDING) ALL HANDICAP RAMPS SHALL BE FLUSH WITH NEW PAVEMENT AND SHALL MEET ALL ADA REGULATIONS.

ONTARIO COUNTY SANITARY LATERAL NOTES

- ALL SANITARY SEWER CONSTRUCTION AND/OR IMPROVEMENTS SHALL BE IN ACCORDANCE WITH THE MOST RECENT STANDARDS AND SPECIFICATIONS OF THE CANANDAIGUA LAKE COUNTY SEWER DISTRICT, N.Y.S. DEPARTMENT OF ENVIRONMENTAL CONSERVATION, N.Y.S. DEPARTMENT OF HEALTH, THE LATEST EDITION OF <u>RECOMMENDED STANDARDS FOR WASTEWATER FACILITIES</u> AND ANY OTHER AGENCIES HAVING JURISDICTION.
- NO SANITARY SEWER RELATED WORK MAY BE PERFORMED WITHOUT FIRST OBTAINING A WRITTEN PERMIT FROM THE CANANDAIGUA LAKE COUNTY SEWER DISTRICT.
- DISTRICT PERSONNEL SHALL BE NOTIFIED A MINIMUM OF 48 HOURS PRIOR TO
- **BEGINNING ANY SANITARY SEWER-RELATED WORK.** THE CONTRACTOR SHALL LOCATE, MARK AND PRESERVE ANY RIGHT OF WAY
- MONUMENTS OR SURVEY CONTROL IN THE AREA OF CONSTRUCTION. UTILITY LOCATIONS SHOWN ARE APPROXIMATE ONLY. THE CONTACTOR SHALL DETERMINE EXACT LOCATION OF UTILITIES, EXCAVATING TO EXPOSE THE UTILITY, IF NECESSARY IN THE AREA OF CONSTRUCTION, BEFORE COMMENCING CONSTRUCTION
- CONTACT U.F.P.O. AT 1-800-962-7962 AT LEAST 72 HOURS PRIOR TO BEGINNING WORL 6. LATERALS SHALL BE MIN. 4" DIA. SDR-21 WITH ELASTOMERIC JOINTS; FOR COMMERCIAL ESTABLISHMENTS, LATERALS ARE TO BE 6" DIA. SDR -21. MINIMUM DEPTH OF BURIAL IS FOUR FEET, CLEANOUTS SHALL BE INSTALLED WITHIN 30 INCHES OF THE OUTSIDE FACE OF BUILDINGS AT ALL CHANGES IN HORIZONTAL ALIGNMENT AT THE RIGHT OF WAY
- OR EASEMENT LINE, AND AT SPACING NOT TO EXCEED 90 FEET. SEWER MAINS AND LATERALS SHALL BE LOCATED A MINIMUM HORIZONTAL DISTANCE OF TEN FEET FROM ANY EXISTING OR PROPOSED WATERMAIN (AS MEASURED FROM THE OUTSIDE OF THE SEWER/LATERAL TO THE OUTSIDE OF THE WATERMAIN). IN CASES WHERE THE MAIN OR LATERAL CROSSES A WATERMAIN, THE MINIMUM VERTICAL SEPERATION SHALL BE EIGHTEEN INCHES (MEASURED OUT-TO-OUT). THE CROSSING SHALL BE ARRANGED SO THAT THE SEWER JOINTS WILL BE EQUIDISTANT AND AS FAR AS POSSIBLE FROM THE WATERMAIN JOINTS.
- 8. THE CONTRACTOR SHALL PROVIDE THE DISTRICT WITH SHOP DRAWINGS AND MATERIAL SPECIFICATIONS THAT HAVE BEEN PRE-APPROVED BY THE DESIGN ENGINEER BEFORE A PERMIT WILL BE ISSUED.
- 9. THE CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH OSHA REQUIREMENTS IN ALL ASPECTS OF CONSTRUCTION.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SANITARY FLOWS AT ALL TIMES BY METHODS ACCEPTABLE TO THE DISTRICT. 11. FLOOR DRAINS IN BASEMENTS OR GARAGES ARE TO BE CONNECTED TO THE SANITARY
- SEWER. FLOOR DRAINS DO NOT INCLUDE FOUNDATION OR FOOTER DRAINS INSTALLED TO INTERCEPT UNCONTAMINATED GROUNDWATER. ALL DISCHARGES TO THE SANITARY SEWER MUST COMPLY WITH EFFLUENT LIMITS OF THE ONTARIO COUNTY SEWER USE LAW FOUNDATION AND FOOTER DRAINS SHALL BE CONSTRUCTED IN A MANNER THAT PROHIBITS GROUNDWATER FROM DRAINING INTO THE SANITARY SEWER PIPE CRADLE
- . LATERAL CONNECTIONS REQUIRING OPENINGS IN ASBESTOS CEMENT PIPE WILL BE DESIGNED, INSPECTED AND CERTIFIED BY THE DESIGN ENGINEER OR REPRESENTATIVE
- 13. ANY EXCAVATION NOT BACKFILLED BY THE END OF THE WORKDAY SHALL BE FENCED, BARRICADED AND LIGHTED FOR SAFETY AND PROTECTION OF THE PUBLIC.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF EXISTING SANITARY MAINS, STRUCTURES AND APPURTENANCES, IF ANY, NEEDED TO COMPLETE THE WORK.

RENOVATION PROJECTS ONLY:

- 15. EXISTING LATERALS TO BE DISCONNECTED MUST BE PERMANENTLY PLUGGED OR CAPPED AT THE EASEMENT OR RIGHT OF WAY LINE UNDER THE DIRECTION OF CANANDAIGUA LAKE COUNTY SEWER DISTRICT PERSONNEL. THE LOCATION OF THE
- PLUG OR CAP SHALL BE RECORDED FOR AS-BUILT DRAWING PURPOSES PRIOR TO DEMOLISHING AN EXISTING BUILDING, THE CONTRACTOR SHALL EXCAVATE, DISCONNECT, AND ABANDON THE LATERAL FROM THE BUILDING TO THE POINT OF DISCONNECTION (APPROXIMATELY 30' FROM THE EXISTING BUILDING) PER DISTRICT STANDARDS. A TEMPORARY PLUG SHALL BE INSTALLED IN THE REMAINING PORTION OF
- THE EXISTING SANITARY LATERAL UNTIL IT IS TESTED AND TELEVISED. IN ORDER TO DETERMINE WHETHER AN EXISTING SANITARY LATERAL IS ACCEPTABLE FOR CONNECTION TO A NEW BUILDING, THE LATERAL SHALL BE TELEVISED IN THE PRESENCE OF DISTRICT PERSONNEL AT THE OWNER'S EXPENSE.

18. IF AN EXISTING LATERAL IS FOUND TO BE ACCEPTABLE AND MEET THE MINIMUM

- DISTRICT REQUIREMENTS. IT SHALL BE TEMPORARILY RE-PLUGGED AND BACKFILLED WIT A WITNESS STAKE IN PLACE, UNTIL CONNECTION TO THE NEW BUILDING CAN TAKE 19. IF A NEW SANITARY LATERAL IS REQUIRED, THE EXISTING LATERAL MUST BE EXCAVATED
- REMOVED AND CAPPED AT THE EASEMENT OR RIGHT OF WAY LINE IN ACCORDANCE WITH DISTRICT REQUIREMENTS.

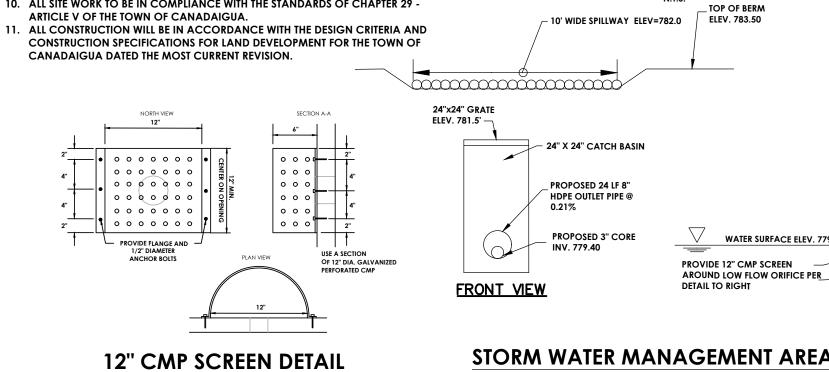
SEWER USE LAW:

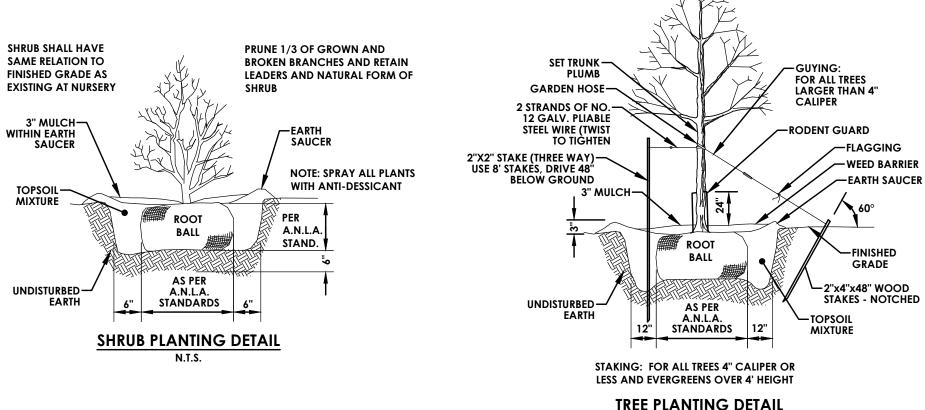
FLOOR DRAINS, IF CONSTRUCTED, SHALL BE CONNECTED TO THE SANITARY/COMBINATION SEWER. FLOOR DRAINS DO NOT INCLUDE FOUNDATION/FOOTER DRAINS.

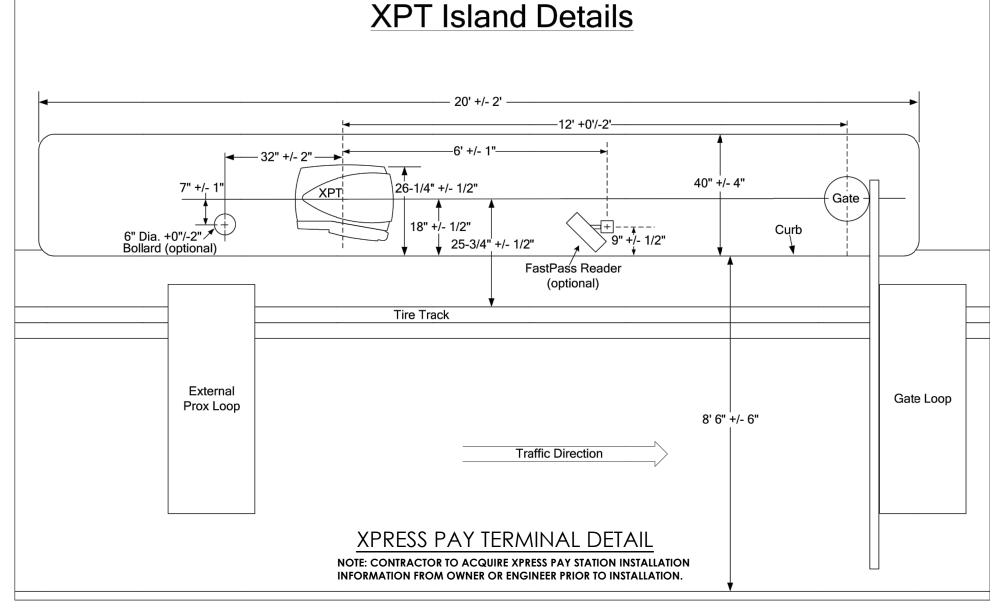
NOTE: ALL DISCHARGES TO THE SANITARY SEWER MUST COMPLY WITH THE LOCAL SEWER USE

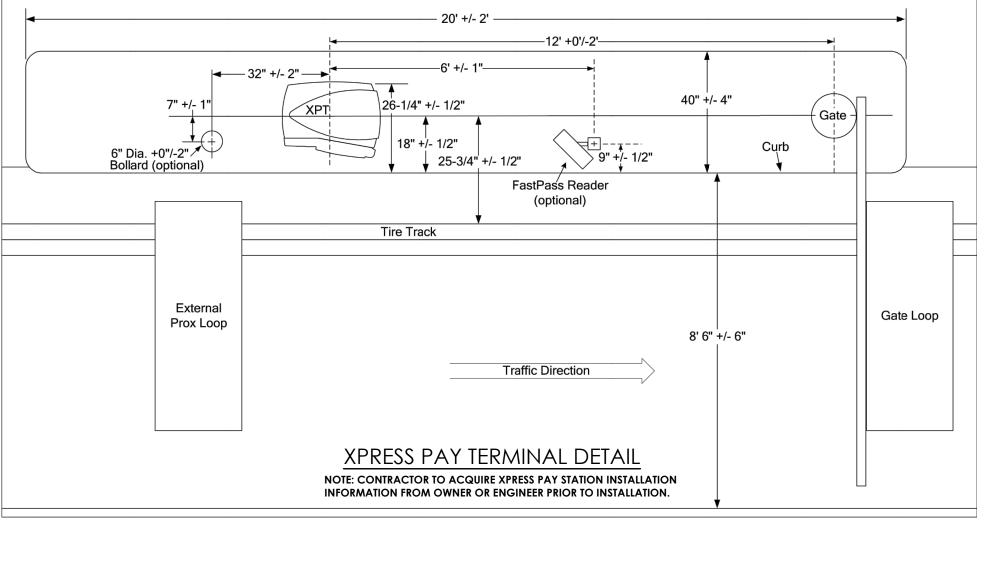
GENERAL NOTES

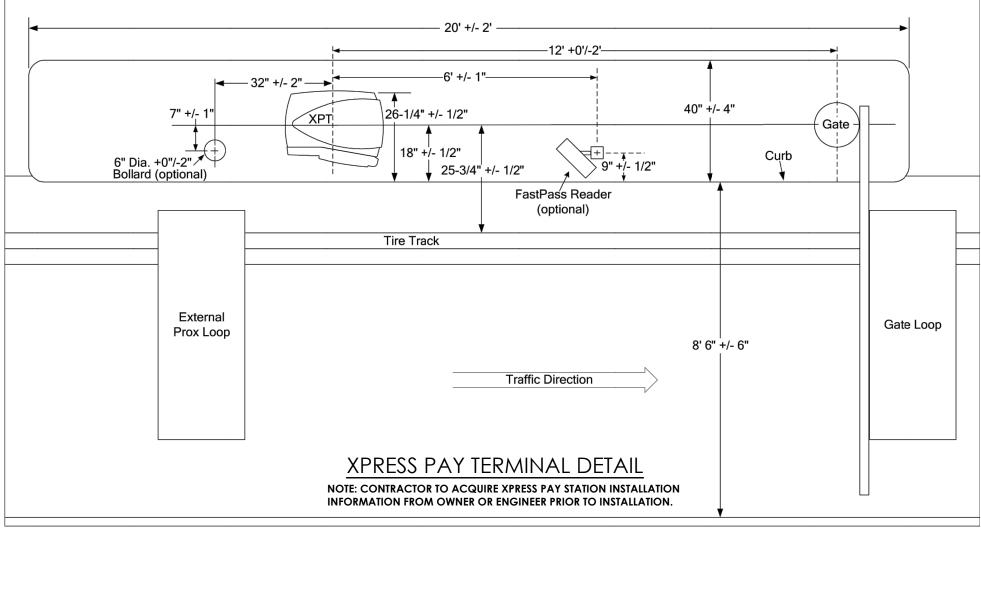
- 1. THE SANITARY WASTE DISPOSAL SYSTEM SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS OF THE NEW YORK STATE DEPARTMENT OF HEALTH AND THE CANANDAIGUA-FARMINGTON SEWER DISTRICT.
- WATER SERVICE WILL BE INSTALLED IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE CANANDAIGUA-FARMINGTON WATER DEPT THE BUILDING CONSTRUCTION TO BE IN COMPLIANCE WITH THE NEW YORK STATE
- 4. ANY COST RELATED TO THE RELOCATION OF ANY UTILITIES NECESSITATED BY THIS PROJECT SHALL BE THE RESPONSIBILITY OF THE OWNER OR THOSE REQUESTING THE **RELOCATION OF THE UTILITY**
- 5. ANY AND ALL CULVERT LOCATIONS, SIZES OR TYPES, TO BE APPROVED BY THE TOWN SUPERINTENDENT OF HIGHWAYS. ALL PROPERTY CORNERS TO BE MARKED WITH IRON PINS.
- THIS SUBDIVISION SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF THE COUNTY MONUMENTATION LAW. 8. THE SUBDIVISION IS LESS THAN 1500 FEET FROM THE NEAREST CONTROL MONUMENT AND IS THEREFORE TIED INTO THE MONUMENTATION NETWORK
- 9. THIS SUBDIVISION SHALL COMPLY WITH ANY COUNTY OR STATE AGENCIES HAVING JURISDICTION. 10. ALL SITE WORK TO BE IN COMPLIANCE WITH THE STANDARDS OF CHAPTER 29
- ARTICLE V OF THE TOWN OF CANADAIGUA.

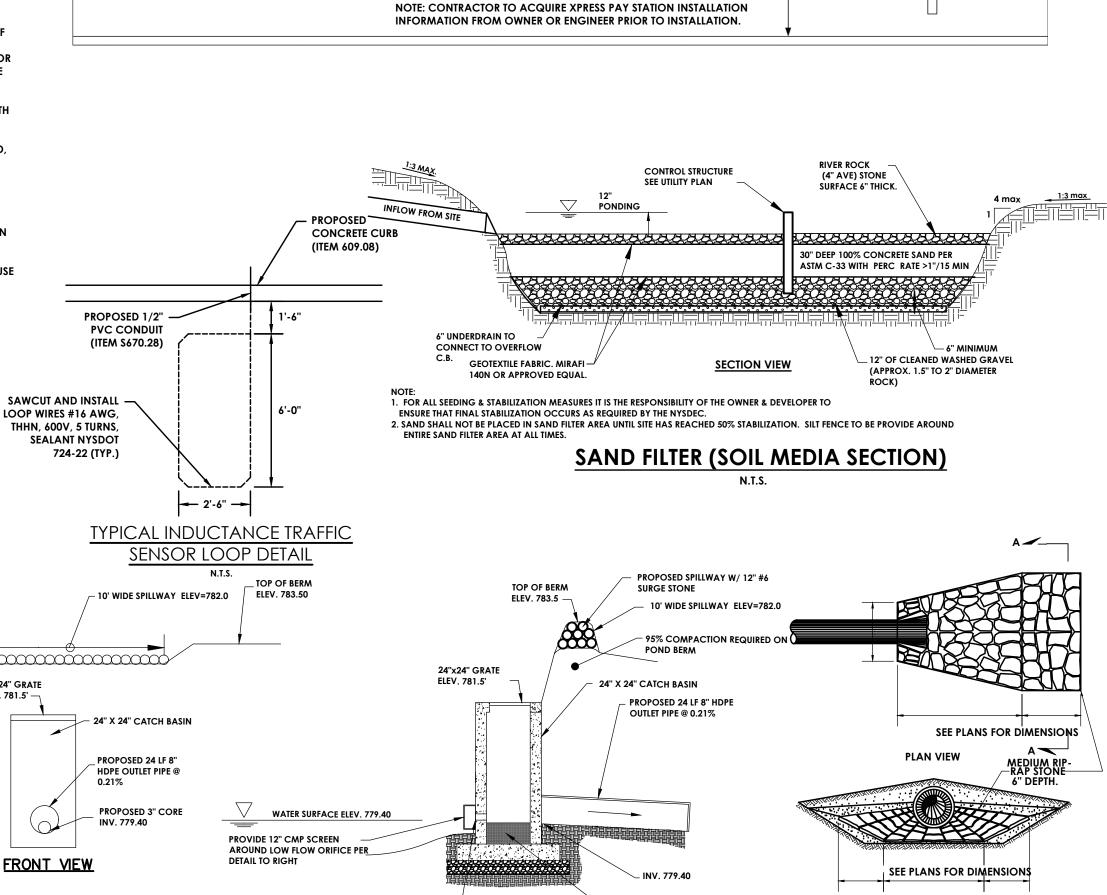












_FILL SUMP WITH 3500 PSI

CONCRETE

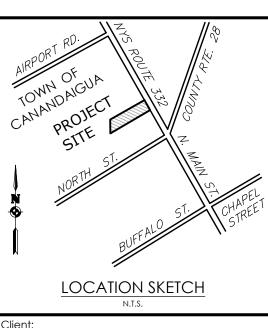
- PROPOSED 3" CORE

INV. 779.40

SECTION VIEW



If you excavate anywhere in **New York State.** except NYC or Long Island, call i-Notice = www.DigSafelyNewYork.com



ROYAL WASH CANANDAIGUA, LL 2851 MONROE AVE ROCHESTER, NY 14618

PASSERO ASSOCIATES

Principal-in-Charge Project Manager Designed by

Jess D. Sudol, P.E. David L. Cox, P.E. Jon Daniels



Revisions

INAUTHORIZED ALTERATIONS OR ADDITIONS TO THIS DRAWING IS IN VIOLATION OF STATE EDUCATION LAW ARTICLE 145 SECTION 7209 AN ARTICLE 147 SECTION 7307. THESE PLANS ARE COPYRIGHT PROTECTED

NOTES

ROYAL CAR WASH Town/City: Canandaigua

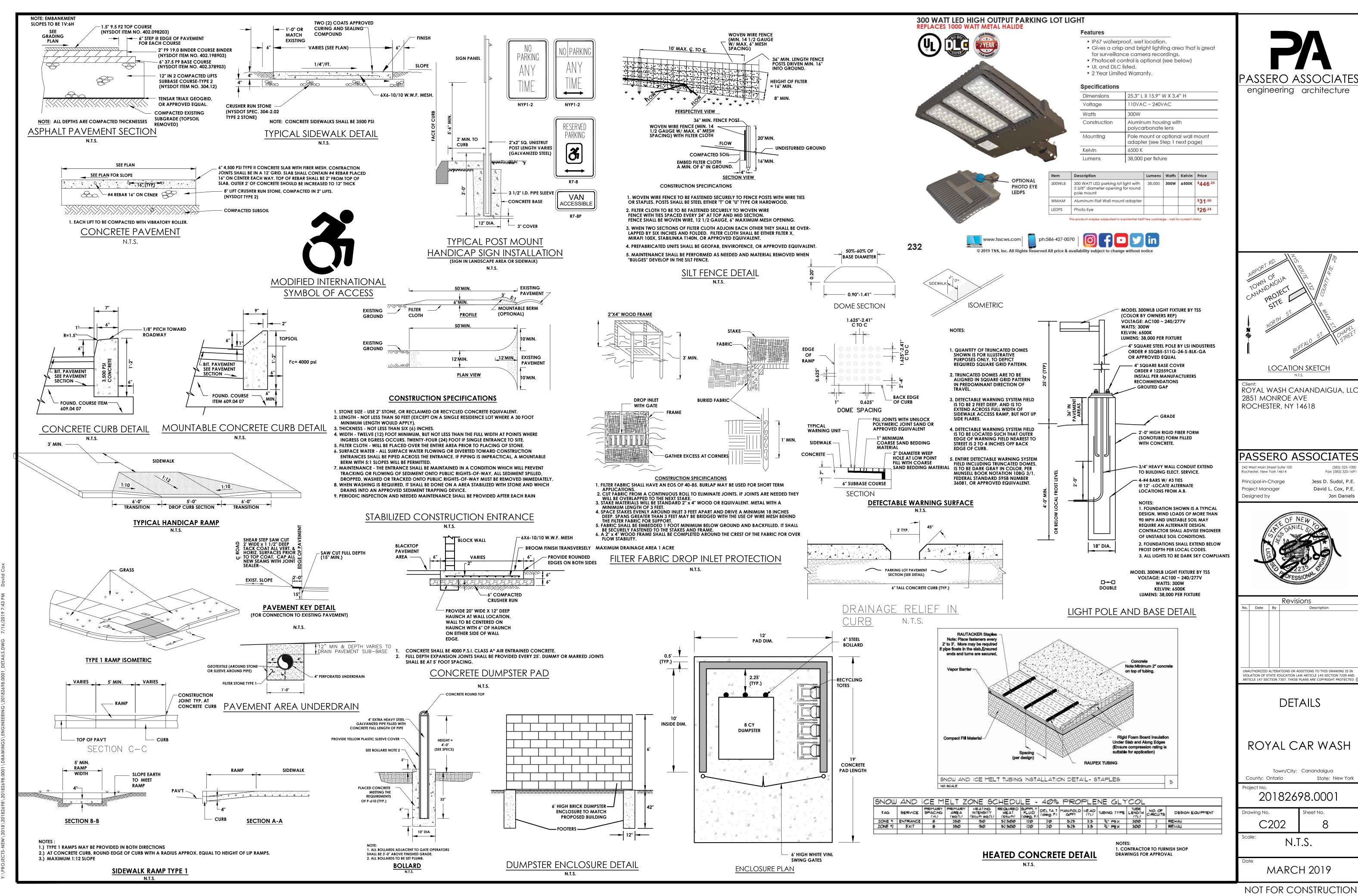
County: Ontario 20182698.000

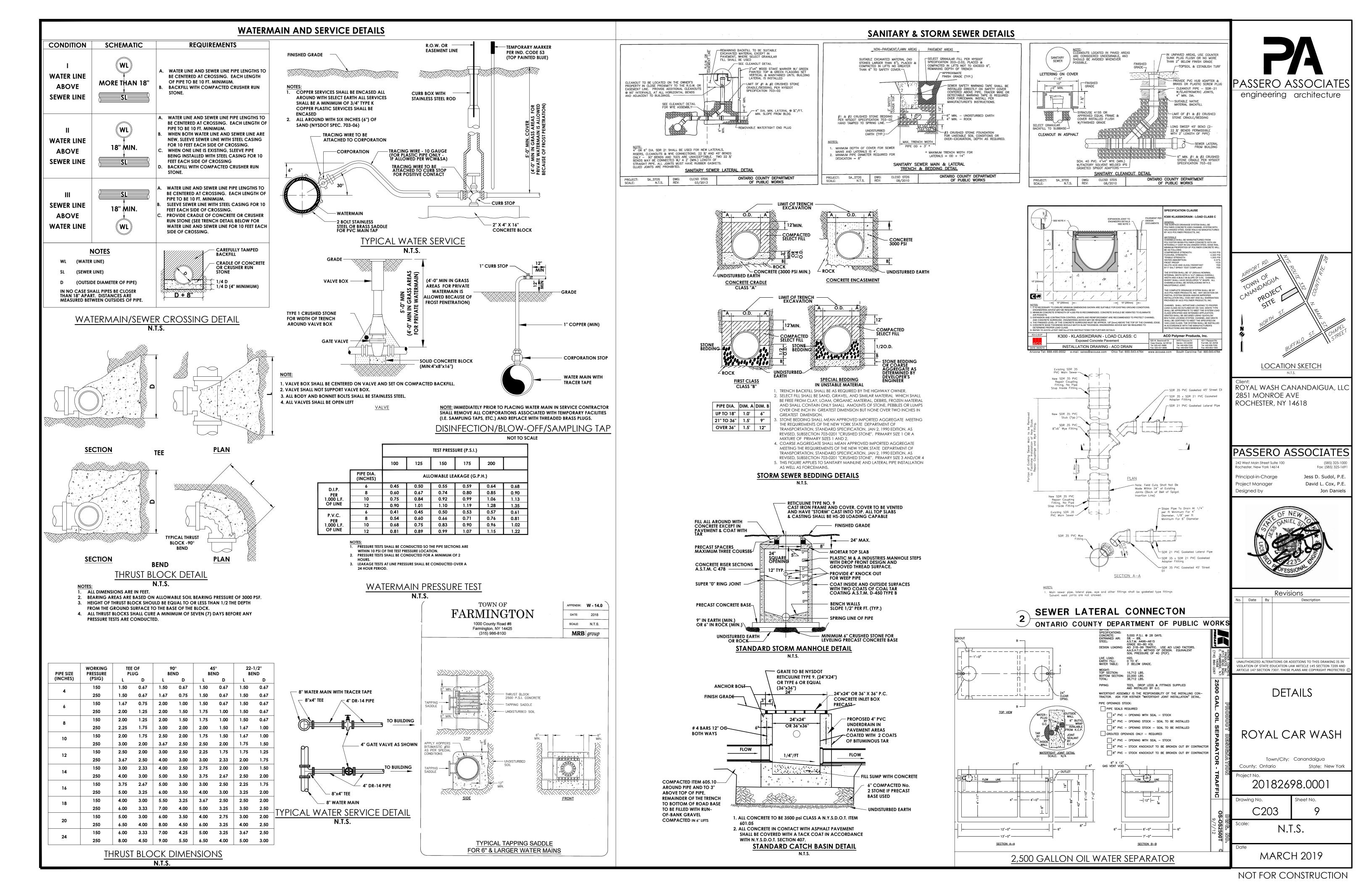
C20

SECTION "A-A"

STONE RIP-RAP DETAIL

MARCH 2019





NYSDOT STANDARD GENERAL PLAN NOTES

- ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
- 2. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES.
- 3. MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE NEW YORK STATE RIGHT-OF-WAY.
- 4. MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THE CURRENT NATIONAL MUTCD WITH NYS SUPPLEMENT, SECTION 619 OF THE CURRENT NYSDOT STANDARD SPECIFICATIONS, THESE PLANS AND AS ORDERED BY THE ASSISTANT RESIDENT ENGINEER, ON A NYSDOT CONSTRUCTION PROJECT, MAINTENANCE AND PROTECTION OF TRAFFIC MUST COMPLY WITH THESE PLANS AND BE IN ACCORDANCE WITH THE NYSDOT CONTRACT DOCUMENTS AS DEEMED NECESSARY BY THE NYS ENGINEER-IN-CHARGE
- 5. NOTIFY THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION'S ASSISTANT RESIDENT ENGINEER AT THE APPROPRIATE NUMBER, AS NOTED BELOW, FIVE (5) WORK DAYS PRIOR TO WORKING WITHIN THE STATE RIGHT- OF-WAY.

GENESEE CO. - DAN STAHLEY (585) 343-0502

LIVINGSTON CO. - ADAM KISIAH (585) 346-3036 MONROE CO. WEST OF GENESEE RIVER - DARRELL FAZIO (585) 352-3471

MONROE CO. EAST OF GENESEE RIVER - KEVIN KIESER (585) 586-4514

ONTARIO CO. - GREG TROST (585) 396-4955

ORLEANS CO. - ANDREW SHICK (585) 589-6655 WAYNE CO. - JEFF JONES (315) 332-4000

WYOMING CO. - KEN BITTNER (585) 786-3310 THE PERMITTEE IS ADVISED THAT THE NYSDOT IS NOW REVIEWING ALL LANE CLOSURES ON THE PRIORITY NETWORK FOR IMPACT ON TRAFFIC OPERATIONS IN REAL-TIME. THE PRIORITY NETWORK CONSISTS OF INTERSTATES 390, 490 AND 590 AND STATE ROUTES 390, 590, 104 (FROM NY 390 TO WAYNE COUNTY), US 20, NY 63 (FROM STEUBEN COUNTY TO 1 90) AND NY 77 IN GENESEE COUNTY. THE CLOSURE WILL BE EVALUATED IN THE LIGHT OF WHAT IS HAPPENING AT THAT TIME ON THE HIGHWAY INCIDENTS

CONFLICTING WORK ZONES OR OTHER UNPLANNED EVENTS THAT RENDER THE HIGHWAY UNAVAILABLE FOR THE CLOSURE MAY RESULT IN A SHORT-TERM DISAPPROVAL OF THE CLOSURE UNTIL THE SITUATION HAS BEEN RESOLVED. EVALUATION PROCEDURES ARE AVAILABLE FOR REVIEW AT THE REGIONAL TRAFFIC OPERATIONS CENTER, 1155 SCOTTSVILLE ROAD, ROCHESTER, NY. 6. NOTIFY THE NYSDOT SIGNAL MAINTENANCE FACILITY AT (585) 753-7793 5 DAYS PRIOR TO

WORKING WITHIN 350' OF A SIGNALIZED INTERSECTION. NOTIFY DIG SAFELY NEW YORK 2

- WORK DAYS PRIOR TO DIGGING, DRILLING OR BLASTING AT 811 FOR A UTILITY STAKE-OUT. 7. ALL MATERIALS USED WITHIN THE STATE RIGHT-OF-WAY MUST COMPLY WITH THE CURRENT NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION'S STANDARD SHEETS.
- 8. QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 401 OF THE STANDARD SPECIFICATIONS. ALL ASPHALT PRODUCED AS PART OF SECTION 401 WILL BE PAID AT A FINAL QUANTITY ADJUSTMENT FACTOR OF 1.0. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
- 9. NO NIGHT WORK SHALL BE ALLOWED UNLESS APPROVED PRIOR TO START OF PROJECT. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC MAY BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
- 10. HAZARDOUS WASTE NOTIFICATION THE PERMITTEE ACCEPTS THE RIGHT-OF-WAY OF THE STATE HIGHWAY IN ITS "AS IS" CONDITION. THE DEPARTMENT OF TRANSPORTATION MAKES NO REPRESENTATION AS TO THE ABSENCE OF UNDERGROUND TANKS. STRUCTURES. FEATURES OR SIMILAR IMPEDIMENTS TO THE COMPLETION OF THE WORK PERMITTED HEREUNDER. SHOULD PERMITTEE FIND SOME PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS TO ITS WORK. THE DEPARTMENT OF TRANSPORTATION SHALL HAVE NO OBLIGATION TO CURE. REMOVE. REMEDY OR OTHERWISE DEAL WITH SUCH PREVIOUSLY UNKNOWN UNDERGROUND IMPEDIMENTS. THE PERMITTEE IS REQUIRED TO REMOVE, MODIFY OR OTHERWISE DEAL WITH SUCH UNDERGROUND TANKS, STRUCTURES, FEATURES OR IMPEDIMENTS IN A MANNER WHICH MEETS ACCEPTABLE ENGINEERING PRACTICE AND IS APPROVED BY THE DEPARTMENT OF TRANSPORTATION.
- 11. ADA COMPLIANCE ALL WORK ON PEDESTRIAN FACILITIES SHALL BE COMPLIANT WITH THE AMERICANS WITH DISABILITIES ACT (ADA). 05/2018

HIGHWAY RESTORATION DETAIL NOTES

- 1. HMA ITEMS ARE BASED ON PG 64-22 BINDER, ESALS LESS THAN 30,000,000. TACK COAT IS REQUIRED BETWEEN ALL LIFTS OF ASPHALT. USE OF POLYPHOSPHORIC ACID (PPA) TO MODIFY THE PG BINDER PROPERTIES IS PROHIBITED. THIS PROHIBITION ALSO APPLIES TO THE USE OF PPA AS A CROSS-LINKING AGENT FOR POLYMER MODIFICATION.
- 2. BUTT JOINTS, CREATED BY A FULL-DEPTH SAWCUT, SHALL BE USED BETWEEN NEW AND EXISTING PAVEMENTS. THE ASPHALT TOP COURSE SHALL BE LAID SUCH THAT IT UNIFORMLY OVERLAPS THE ADJACENT COLD MAT BY 2"-3". THE THICKNESS OF THE OVERLAP MATERIAL SHALL BE 1/4 THE COMPACTED THICKNESS OF THE COURSE, SO AS TO RESULT IN A SMOOTH AND WELL COMPACTED JOINT AFTER ROLLING. BROADCASTING OF THE OVERLAP MATERIAL ONTO THE LANE IS NOT ALLOWED. IF THE OVERLAP IS EXCESSIVE THE EXCESS MATERIAL SHALL BE TRIMMED OFF SO THAT THE MATERIAL ALONG THE JOINT IS UNIFORM. THE COARSE PARTICLES OF AGGREGATE IN THE OVERLAP MATERIAL SHALL BE REMOVED AND WASTED IF DEEMED NECESSARY BY THE INSPECTOR. ASPHALT PAVEMENT JOINT ADHESIVE SHALL BE USED ON VERTICAL FACES FOR ALL OF THE JOINTS ON THE SURFACE COURSE SUCH AS LONGITUDINAL, TRANSVERSE AND OTHER JOINTS **BUTTING AGAINST THE NEW ASPHALT PAVEMENT.**

3. DESIGN PGB CONTENT SELECTION

THE PRODUCER SELECTS THE DESIGN PGB CONTENT AT THE BINDER CONTENT THAT RESULTS IN A COMPACTED DENSITY OF 96.5% GMM AT THE DESIGN NUMBER OF GYRATIONS (NDESIGN), UNDER NO CIRCUMSTANCES SHALL THE PERFORMANCI GRADED BINDER CONTENT IN THE HMA MIXTURE BELESS THAN 5.8% FOR A 9.5 DESIGN. 5.2% FOR A 12.5 DESIGN. 4.5% FOR A 19.0 DESIGN. 4.2% FOR A 25.0 DESIGN, OR 3.7% FOR A 37.5 DESIGN. ALL VOLUMETRIC AND MECHANICAL PROPERTIES ARE CHECKED AT THIS PGB CONTENT TO ENSURE THAT ALL REQUIREMENTS ARE MET.

- 4. PERFORM ALL WARRANTY WORK IN ACCORDANCE WITH MATERIALS PROCEDURE (MP) 402-01, WARRANTY REQUIREMENTS FOR HOT MIX ASPHALT (HMA) TOP
- 5. AS PER STANDARD SPECIFICATION SECTION 401-4.01 CERTIFIED PRODUCTION, PRODUCTION LESS THAN 500 TONS, AND HIGHWAY PERMIT PRODUCTION, PRODUCTION MEETING THE SPECIFICATION REQUIREMENTS WILL BE ASSIGNED A QAF OF 1.00. PRODUCTION FAILING TO MEET THE SPECIFICATION REQUIREMENTS WILL BE SUBJECT TO EVALUATION ACCORDING TO SECTION 401-4.03, **EVALUATION OF SUBLOTS REPRESENTED BY 0.85 QAF.**

NYSDOT STANDARD WORK ZONE TRAFFIC CONTROL NOTES

- 1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS. 2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
- 3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER. IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.

1. THE CONTRACTOR SHALL MAINTAIN A MINIMUM 500' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY,

UNLESS OTHERWISE APPROVED BY THE ENGINEER. 2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

- 1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
- 2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
- 3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
- 4. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE RAMPS, AND ONE-WAY STREETS. IN CASES WHERE LANE RESTRICTIONS REDUCE THE TRAVEL LANE TO ONE LANE, SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE ACTIVE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- 5. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
- 6. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.

NYR9-12 MAY BE USED IN PLACE OF NYR9-11

CHANNELIZING DEVICES 1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS

2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.

2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'.

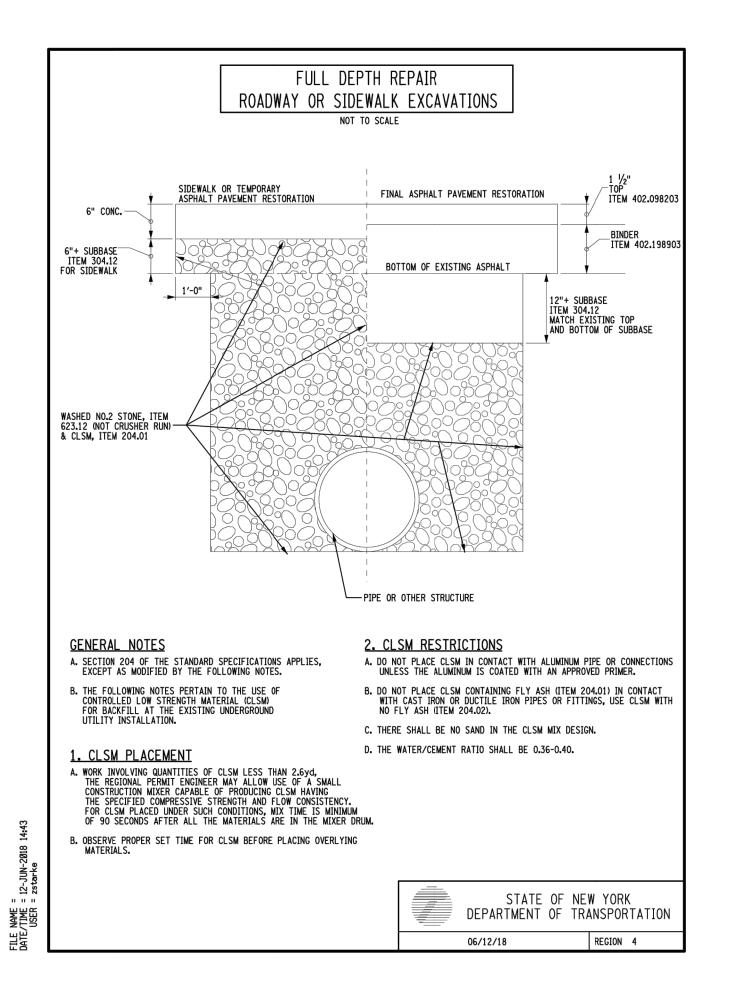
THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10 2. THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS

BARRIER/SHADOW VEHICLES 1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES"

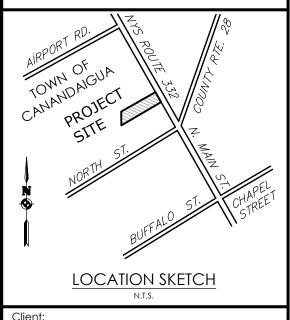
IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.

2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES, AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK AREA (ROLL

3. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.



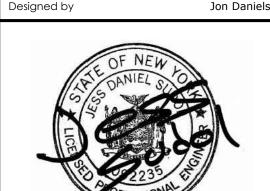


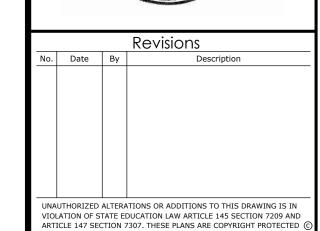


ROYAL WASH CANANDAIGUA, LL 2851 MONROE AVE ROCHESTER, NY 14618

PASSERO ASSOCIATES ochester, New York 14614 Fax: (585) 325-16

Principal-in-Charge Jess D. Sudol, P.E. Project Manager David L. Cox, P.E. Designed by





DETAILS

ROYAL CAR WASH

Town/City: Canandaigua

State: New York County: Ontario 20182698.000

MARCH 2019

NYSDOT REGION 4 UTILITY PERMIT NOTES

- ROAD PAVEMENT, SHOULDER AND DRAINAGE ARE NOT TO BE DISTURBED OR UNDERMINED UNLESS INDICATED OTHERWISE IN THE PLANS.
- SUITABLE NON-FROZEN, NON SATURATED BACKFILL TO BE USED AND PLACED IN 6" COMPACTED LIFTS.
- PAVEMENT TO BE MAINTAINED UNTIL A PERMANENT REPAIR CAN BE MADE.
- PAVEMENT TO BE REPLACED IN KIND OR AS SPECIFIED ON ATTACHED PLANS.
- ALL DISTURBED GRASS AREAS WITHIN THE R.O.W. ARE TO BE FINE GRADED, 4" TOPSOIL PLACED AND TURF ESTABLISHED.

NOTE

ROAD PLATES SHALL NOT BE USED DURING THE ASNOW AND ICE SEASON® (NOVEMBER 1ST THROUGH APRIL 1ST). DURING THE MONTHS OF OCTOBER AND APRIL, CALL THE WORK AREA JURISDICTIONAL N.Y.S.D.O.T. TRANSPORTATION MAINTENANCE RESIDENCY WITH PLATE LOCATIONS. ROAD PLATES SHALL BE PINNED AND RAMPED. A "STEEL PLATE AHEAD" SIGN (W8-24) SHALL BE INSTALLED IN ADVANCE OF THE PLATE IN ACCORDANCE WITH TABLE NY6H-3 ON STANDARD SHEET 619-11. IF THE SURFACE OF THE PLATE IS 1" OR GREATER ABOVE/BELOW THE SURROUNDING PAVEMENT, A "BUMP" SIGN (W8-1) SHALL BE INSTALLED BETWEEN THE PLATE AND "STEEL PLATE AHEAD" SIGN IN ACCORDANCE WITH TABLE NY6H-3 ON STANDARD SHEET 619-11 AND A BARREL WITH A TYPE A FLASHER SHALL BE PLACED ON THE SHOULDER AT THE

WINTER ASPHALT PAVEMENT AND SHOULDER RESTORATION

EXCAVATION BACKFILL SHALL BE WITH NO. 2 STONE (NOT CRUSHER RUN), ITEM 623.12, PLACED TO SIX INCHES (6") BELOW THE EXISTING PAVEMENT SURFACE. THE REMAINING SIX INCHES (6") SHALL HAVE CONCRETE PLACED TO MATCH EXISTING ADJACENT PAVEMENT GRADE AND SHALL HAVE A GOOD, SMOOTH, NON-POLISHED RIDING SURFACE.

IN THE SPRING, WHEN ASPHALT PLANTS OPEN, THE CONCRETE AND STONE SHALL BE REMOVED TO THE EXISTING BOTTOM OF THE SUBBASE IN ORDER TO PERFORM FINAL RESTORATION PER PREVIOUSLY APPROVED N.Y.S.D.O.T. PAVEMENT RESTORATION **PROCEDURES**

TEST PITS

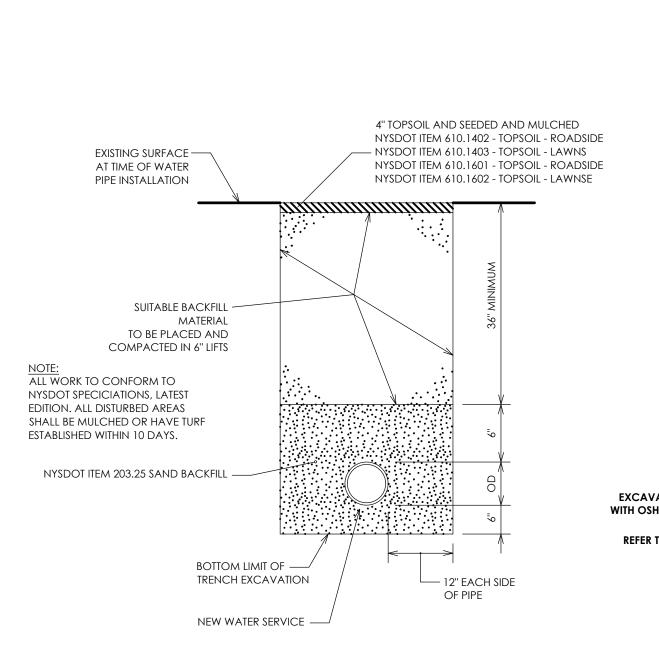
ALL TEST PITS IN THE PAVEMENT SHALL BE COMPLETED BY AN 18" SQUARE/ROUND VACUUM EXCAVATION.

EXCAVATION SUPPORT SYSTEM REQUIREMENTS

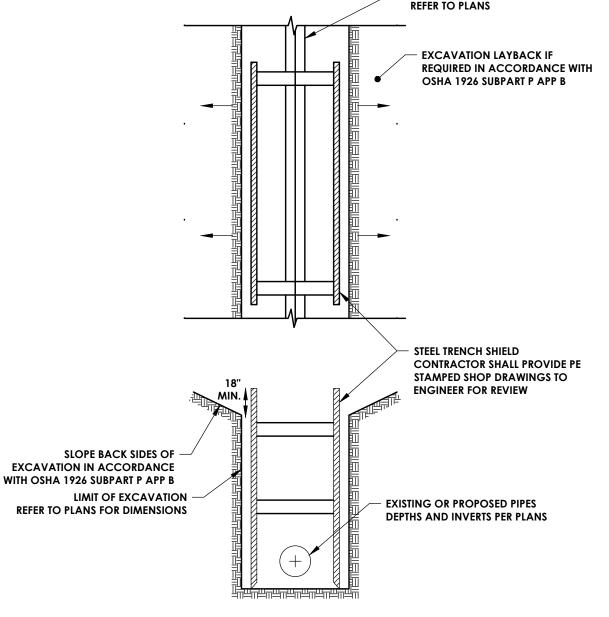
ANY EXCAVATION 5 FEET OR MORE IN DEPTH SHALL UTILIZE A SHIELDS AND SHORING OR SHEETING SYSTEM WHICH PROVIDES DIRECT CONTACT AND SUPPORT OF THE EXCAVATION SIDES. THE SYSTEM SHALL ACCOMMODATE ANY ASSOCIATED SURCHARGE LOADS AND SHALL BE SUBMITTED TO NYSDOT'S REGIONAL GEOTECHNICAL ENGINEER FOR REVIEW AND APPROVAL PRIOR TO PERMIT ISSUANCE.

A SLOPING (LAYBACK) OPTION WILL NOT BE ALLOWED.

SHIELDS AND SHORING OR SHEETING SYSTEM REQUIREMENTS DO NOT PERTAIN TO TEST PITS WHICH ARE 18" OR LESS IN DIAMETER.



NYSDOT ROW LAWN RESTORATION DETAIL



TYPICAL TRENCH EXCAVATION DETAIL

EXCAVATION NOTE: EXCAVATIONS WITH

EXISTING OR PROPOSED PIPE

THE NEW YORK STATE RIGHT-OF-WAY

SHALL NOT BE LEFT OPEN OVER NIGHT

 \bigvee \bigvee \bigvee \bigvee \bigvee \bigvee \bigvee \bigvee \bigvee \bigvee **PLAN VIEW** CONSTRUCTION SPECIFICATIONS ALL TEMPORARY SWALES SHALL HAVE UNINTERRUPTED POSITIVE GRADE TO AN OUTLET DIVERTED RUNOFF FROM A DISTURBED AREA SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE DIVERTED RUNOFF FROM AN UNDISTURBED AREA SHALL OUTLET DIRECTLY INTO AN UNDISTURBED STABILIZED AREA A 4. ALL TREES, BRUSH, STUMPS, OBSTRUCTIONS, AND OTHER OBJECTIONABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF SO AS NOT TO INTERFERE WITH THE PROPER FUNCTIONING OF THE SWALE.

5. THE SWALE SHALL BE EXCAVATED OR SHAPED TO LINE, GRADE, AND CROSS SECTION AS REQUIRED TO MEET THE CRITERIA SPECIFIED HEREIN AND BE FREE OF BANK PROJECTIONS OR OTHER IRREGULARITIES WHICH WILL IMPE NORMAL FLOW.
FILLS SHALL BE COMPACTED BY EARTH MOVING EQUIPMENT. ALL EARTH REMOVED AND NOT NEEDED FOR CONSTRUCTION SHALL BE PLACED SO THAT IT WILL NOT INTERFERE WITH STABILIZATION SHALL BE AS PER THE FLOW CHANNEL STABILIZATION CHART BELOW: CHANNEL GRADE A(5 AC. OR LESS) 0.5-3.0% SEED AND STRAW MULCH SEED AND STRAW MULCH 3.1-5.0% SEED AND STRAW MULCH SEED USING JUTE OR EXCELSIOR 5.1-8.0% SEED WITH JUTE OR EXCELSIOR, SOD LINED WITH 4-8" RIP-RAP OR RECYCLED CONCRETE EQUIVALENT 8.1-20.% LINED WITH 4-8" RIP-RAP 9. PERIODIC INSPECTION AND REQUIRED MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT 10' X 10' WIDE HEAVY DUTY PLASTIC LINING **CONCRETE WASH OUT AREA**

STABILIZED CONSTRUCTION ENTRANCE

—EXISTING GROUNI

-SLOPE 2:1 OR FLATTER

STORAGE AREA-

SLOPE 2:1 OR-

OUTLET AS REQUIRED SE

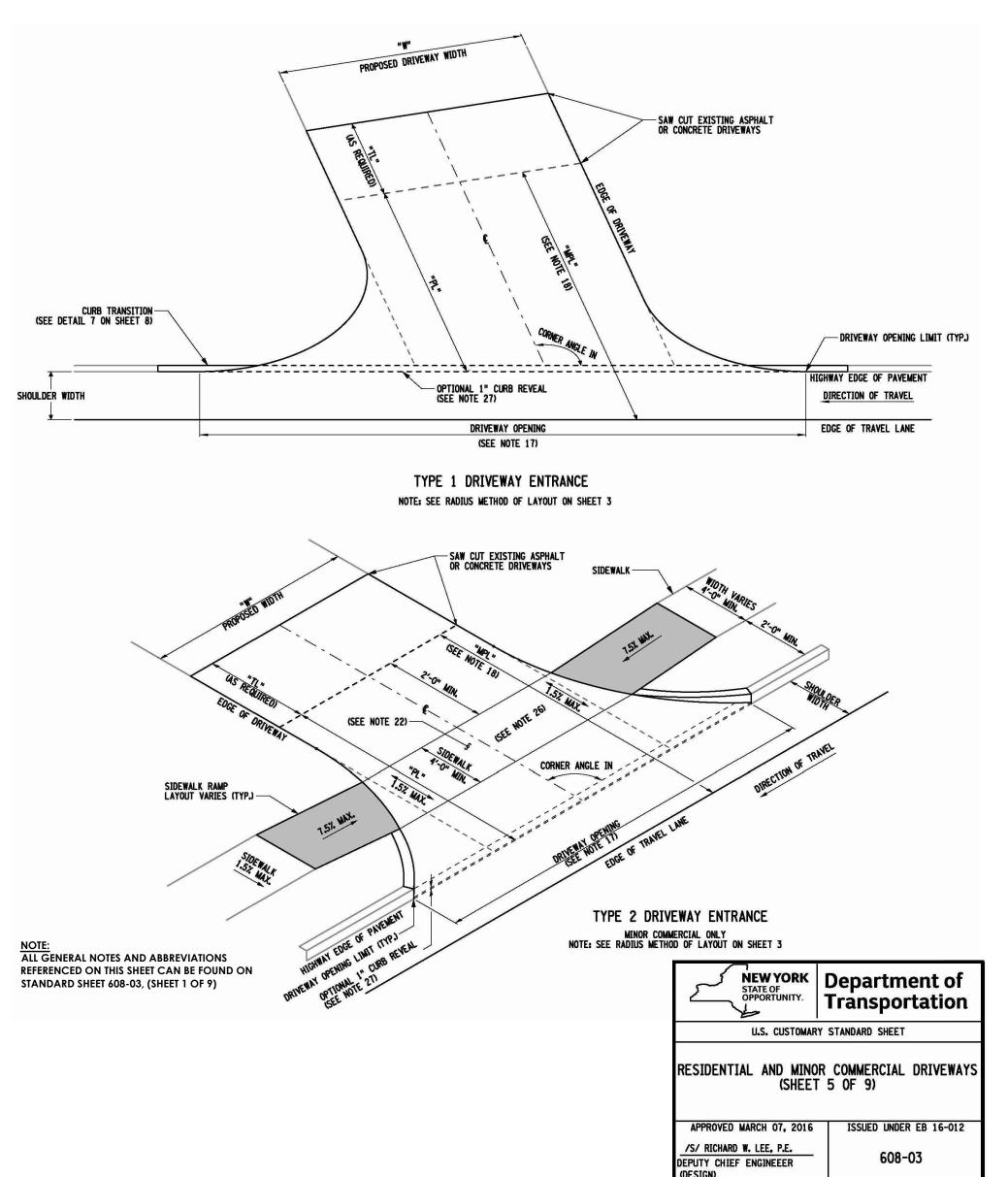
D MIN. LEVEL

CROSS SECTION

POSITIVE DRAINAGE: 0.5% OR STEEPER DEPENDENT ON TOPOGRAPH

R-4 (03/15)

NOT FOR CONSTRUCTION



CONTINUOUS CONCRETE
BACKING TO TOP OF PAVEMENT

PRECAST CONCRETE, STONE, AND GRANITE CURB WITH FLEXIBLE PAVEMENT BEDDING AND BACKFILL

STATE OF NEW YORK

DEPARTMENT OF TRANSPORTATION

609-03

U.S. CUSTOMARY STANDARD SHEET

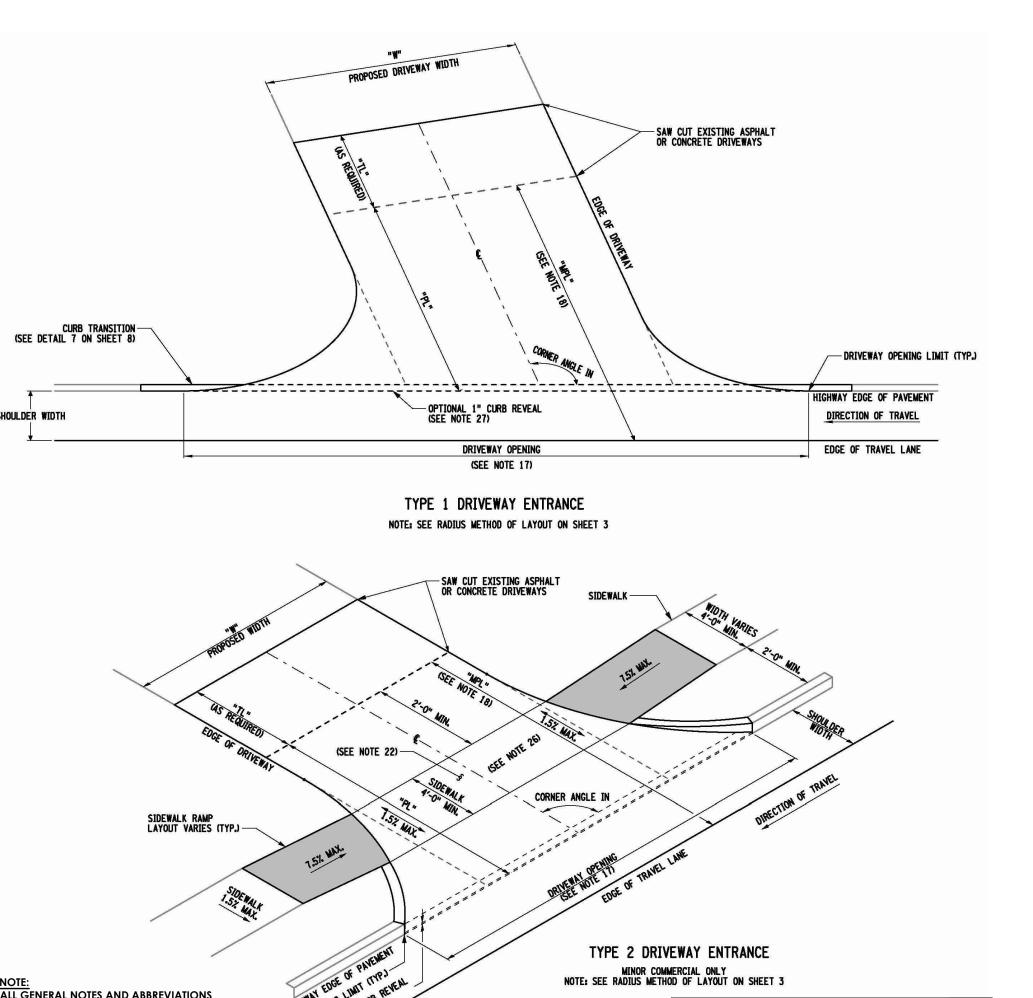
CONCRETE CURB.

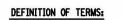
CURB AND GUTTER, AND HOT MIX ASPHALT CURB

APPROVED SEPTEMBER 19, 2008 | ISSUED UNDER EB 08-036

/S/ DANIEL D'ANGELO, P.E.

DEPUTY CHIEF ENGINEER (DESIGN)





 $\mbox{DRIVEWAY}$ - EVERY ENTRANCE OR EXIT USED BY VEHICULAR TRAFFIC TO AND FROM LANDS OR BUILDINGS ABUTTING A HIGHWAY.

RESIDENTIAL DRIVEWAY - A DRIVEWAY SERVING FOUR OR FEWER PRIVATE HOMES OR AN APARTMENT BUILDING FOR FOUR OR FEWER FAMILY UNITS. COMMERCIAL DRIVEWAY - A DRIVEWAY SERVING A COMMERCIAL ESTABLISHMENT, INDUSTRY, GOVERNMENTAL OR EDUCATIONAL INSTITUTION, PRIVATE UTILITY, HOSPITAL, CHURCH, APARTMENT BUILDING, OR OTHER COMPARABLE TRAFFIC GENERATOR.

MAJOR COMMERCIAL DRIVEWAY - ANY COMMERCIAL DRIVEWAY WHERE THE ACTUAL OR ANTICIPATED TRAFFIC VOLUME ON A TYPICAL DAY IS DEFINED BY THE DRIVEWAY POLICY AS DEFINED IN THE HIGHWAY DESIGN MANUAL (HDM) CHAPTER 5 APPENDIX 5A.

MINOR COMMERCIAL DRIVEWAY - ANY COMMERCIAL DRIVEWAY WHERE THE ACTUAL OR ANTICIPATED TRAFFIC VOLUMES ON A TYPICAL DAY ARE LESS THAN THE VALUES STIPULATED FOR A MAJOR COMMERCIAL DRIVEWAY. FIELD ENTRANCE - A DRIVEWAY SERVING A FARMYARD, CULTIVATED OR UNCULTIVATED FIELD, TIMBERLAND, OR UNDESCRIPTION OF LAND NOT USED FOR INDUSTRIAL, COMMERCIAL, OR

URBAN / RURAL - THE AREA CHARACTER BASED ON NYSDOT HIGHWAY DESIGN MANUAL CHAPTER 2, SECTION 2.4.

DRIVEWAY OFFSET - THE DISTANCE IN FEET MEASURED FROM THE INSIDE EDGE OF THE OUTERMOST TRAVEL LANE, OR TURNING LANE, TO THE HIGHWAY EDGE OF PAVEMENT. THE DISTANCE IS EQUAL TO THE WIDTH OF THE OUTERMOST LANE AND THE WIDTH OF THE PAVED SHOULDER, OR CURB OFFSET.

HIGHWAY EDGE OF PAVEMENT - THE OUTSIDE EDGE OF THE PAVED HIGHWAY SURFACE. SHOULDER WIDTH - THE WIDTH IN FEET OF PAVED SHOULDER INCLUDING A PARKING LANE, BIKE LANE, CURB OFFSET, OR OTHER PAVED AREA OUTSIDE OF THE TRAVEL LANE.

MINIMUM PAYING LIMIT (MPL) - THE MINIMUM DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY FROM THE OUTSIDE EDGE OF THE OUTERMOST TRAVEL LANE THAT A DRIVEWAY MUST BE PAYED (INCLUDES THE SHOULDER WIDTH).

PAVEMENT LENGTH (PL) - THE DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY FROM THE HIGHWAY EDGE OF PAVEMENT TO THE END OF PROPOSED DRIVEWAY PAVEMENT. TRANSITION LENGTH (TL) - THE DISTANCE IN FEET MEASURED ALONG THE CENTERLINE OF A DRIVEWAY BEYOND THE DRIVEWAY PAVEMENT LENGTH (PL) TO THE END OF PROPOSED DRIVEWAY WORK. THE TRANSITION LENGTH (TL) IS TYPICALLY USED FOR GRADING, LAYOUT, OR TRANSITION REASONS. THE TRANSITION LENGTH (TL) ONLY APPLIES TO DRIVEWAYS THAT ARE UNPAYED.

BUFFER ZONE - A PHYSICAL DISTANCE SEPARATING THE PEDESTRIAN ACCESS ROUTE AND THE VEHICLE TRAVELED WAY. THE BUFFER ZONE BUFFERS PEDESTRIANS FROM TRAFFIC AND PROVIDES SPACE FOR SNOW STORAGE, UTILITIES, PLANTS, AND OTHER STREET APPURTENANCES. THE BUFFER ZONE MAY BE PLANTED OR PAVED.

SHARED-USE-PATH (SUP) - A BICYCLE AND PEDESTRIAN FACILITY, TYPICALLY WITHIN TH RIGHT-OF-WAY, SEPARATED FROM MOTORIZED VEHICULAR TRAFFIC BY A BUFFER ZONE OF BARRIER. REFER TO HIGHWAY DESIGN MANUAL CHAPTER 17 AND AASHTO GUIDE FOR THI DEVELOPMENT OF BICYCLE FACILITIES FOR GUIDANCE ON BUFFER ZONE WIDTH AND SEPARATION OF SHARED USE PATHS FROM ROADWAYS.

SIDEWALK - A SMOOTH, STABLE AND SLIP RESISTANT EXTERIOR PATHWAY INTENDED FOR PEDESTRIAN USE ALONG A VEHICULAR WAY SEPARATED WITH A CURB OFFSET.

HMA - HOT MIX ASPHALT

PCC - PORTLAND CEMENT CONCRETE

SIDEWALK CROSS SLOPE - SEE NOTE 12

ELEMENT

GENERAL NOTES FOR DRIVEWAY STANDARD SHEETS:

- 1. THE DRIVEWAY STANDARD SHEETS APPLY TO FIELD ENTRANCES, RESIDENTIAL DRIVEWAYS AND MINOR COMMERCIAL DRIVEWAYS. FIELD ENTRANCES AND RESIDENTIAL DRIVEWAYS ACCOMMODATE AN AASHTO PASSENGER CAR DESIGN VEHICLE. MINOR COMMERCIAL DRIVEWAYS ACCOMMODATE AN AASHTO SINGLE UNIT TRICK DESIGN VEHICLE
- DRIVEWAY WORK PERFORMED OFF THE RIGHT-OF-WAY REQUIRES AN EASEMENT OR A DRIVEWAY RELEASE. A DRIVEWAY RELOCATION WILL REQUIRE A TEMPORARY EASEMENT MAP.
- 3. IF COMMERCIAL PROPERTY DEVELOPMENT PLANS INVOLVE NEW OR MODIFIED ACCESS TO A STATE HIGHWAY A COMMERCIAL HIGHWAY WORK PERMIT APPLICATION (FORM PERM 33-COM) MUST BE FILLED OUT AND SUBMITTED TO THE REGIONAL PERMIT COORDINATOR.
- 4. SEE THE DRIVEWAY TABLE IN THE CONTRACT PLANS FOR SPECIFIC DRIVEWAY LOCATIONS, WIDTHS ("W"), CORNER ANGLES, LENGTHS ("L"), MATERIAL, AND ENTRANCE TYPE.
- 5. DETECTABLE WARNING SURFACES SHALL BE PROVIDED WHERE THE PEDESTRIAN ACCESS ROUTE CROSSES DRIVEWAYS WITH SIGNAL, YIELD OR STOP CONTROL. DETECTABLE WARNING SURFACES SHALL NOT BE PROVIDED AT CROSSINGS OF UNCONTROLLED DRIVEWAY APRONS.
- 6. THE TAPER METHOD IS GENERALLY NOT RECOMMENDED FOR DRIVEWAYS WITH A DRIVEWAY OFFSET LESS THAN 16 FEET, UNLESS IT CAN BE FIELD VERIFIED THAT THE DRIVEWAY ENTRANCE WIDTH WILL ACCOMMODATE THE VEHICLES THAT USE THE DRIVEWAY ON A REGULAR BASIS.
- 7. TYPE 3 AND TYPE 4 DRIVEWAY ENTRANCES CAN BE USED WITHOUT CURB IF A TAPER STYLE ENTRANCE BETTER MATCHES THE HIGHWAY CORRIDOR AESTHETICS OR SPECIFIC SITE CONDITIONS THAN A RADIUS STYLE ENTRANCE.

9. UP TO 9" OF PCC MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.

- 8. UP TO 10" OF HMA MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.
- 10. UP TO 12" OF SUBBASE MAY BE REQUIRED FOR HEAVY TRUCKS PER CONTRACT DOCUMENTS.
- 11. THE DETAILS SHOW THE PAVEMENT LENGTH ("PL") EXTENDING TO THE MINIMUM PAVING LIMIT ("MPL"). HOWEVER, THE "PL" CAN EXTEND BEYOND THE "MPL" AS SPECIFIED IN THE CONTRACT DOCUMENTS.
- 12. A DRIVEWAY TIP-UP SECTION SHOULD EXTEND TO A LOGICAL TERMINI (EXAMPLE: SIDEWALK EDGE, WHERE THE DRIVEWAY GRADE MATCHES EXISTING GROUND, OR LAYOUT POINT). FOR REFERENCE, A REASONABLE LENGTH FOR TAPERING THE TIP-UP SECTION BACK TO THE EDGE OF DRIVEWAY IS 3 TO 4 TIMES THE LENGTH OF CURB DROP. THE TIP-UP SECTION IS NOT PART OF THE DRIVEWAY OPENING WIDTH. REFER TO NYSDOT STANDARD SHEET 609-02 "MISCELLANEOUS CURB DETAILS" FOR THE CURB TRANSITION.
- TO DETERMINE THE LIMITS OF SHOULDER RECONSTRUCTION, REFER TO THE DRIVEWAY OPENING TABLES ON SHEET 4 FOR NO SHOULDER (O' OFFSET).
- 14. FOR PCC SHOULDERS, SEE STANDARD SHEET 502-02 FOR LONGITUDINAL JOINT TIE DETAILS.
- 15. DIMENSIONS AND ANGLES MAY BE INTERPOLATED FOR VALUES OTHER THAN THOSE SHOWN IN THE TABLES.
- 16. THE SHOULDER PAVEMENT THICKNESSES SHOWN ARE DEFAULT VALUES UNLESS OTHERWISE SHOWN IN THE PLANS. MATERIALS SHALL BE AS SPECIFIED IN THE CONTRACT DOCUMENTS.

17. WHERE THERE ARE CONSTRAINTS THAT PREVENT THE CONSTRUCTION OF THE DRIVEWAY OPENING USING EITHER OF THE LAYOUT METHODS, THE ENGINEER MAY SPECIFY A SMALL CORNER CURB RADIUS OF 2' (OR A "1/2 BULL NOSE" CURB ALONG LOW SPEED HIGHWAYS), PROVIDED THE DRIVEWAY OPENING MEETS THE REQUIREMENTS OF THE "DRIVEWAY OPENING" TABLES ON SHEET 4.

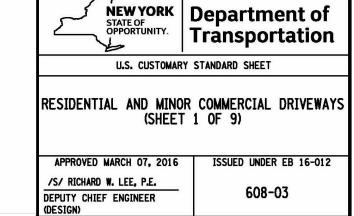
- 18. FOR RESIDENTIAL DRIVEWAYS, THE MINIMUM PAVING LIMIT SHALL BE 10' FROM THE OUTSIDE EDGE OF TRAVEL LANE OR 2' BEHIND ANY SIDEWALK, IF PRESENT, WHICHEVER IS GREATER, FOR MINOR COMMERCIAL DRIVEWAYS, THE MINIMUM PAVING LIMIT SHALL BE 30' FROM THE OUTSIDE EDGE OF TRAVEL LANE, OR 2' BEHIND ANY SIDEWALK, IF PRESENT, OR EXTEND TO THE RIGHT-OF-WAY LINE, WHICHEVER IS GREATER. THE PAVING LIMIT MAY EXTEND BEYOND THE MINIMUM PAVING LIMIT FOR NEW DRIVEWAYS AND TO TRANSITION TO EXISTING PAVED DRIVEWAYS. THE PAVING LIMIT WILL BE NOTED IN THE DRIVEWAY TABLE OF THE CONTRACT PI ANS.
- 19. FOR GRADING AND CONSTRUCTION REQUIREMENTS OF TRANSITIONS FROM PLACED HMA TO EXISTING HMA DRIVEWAYS, REFER TO DETAIL 9 "TIE-IN TO EXISTING DRIVEWAYS" ON SHEET 9, AND TABLE 3 "DRIVEWAY MATERIALS AND THICKNESS" ON SHEET 2.
- FOR PCC DRIVEWAYS, REFER TO THE 502 SERIES STANDARD SHEETS FOR METAL REINFORCEMENT, JOINT TIES, SAWING AND SEALING, ETC.
- 21. A 5' MINIMUM BUFFER ZONE SHALL BE USED UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

SITE CONDITIONS (SIDEWALK / CURB):

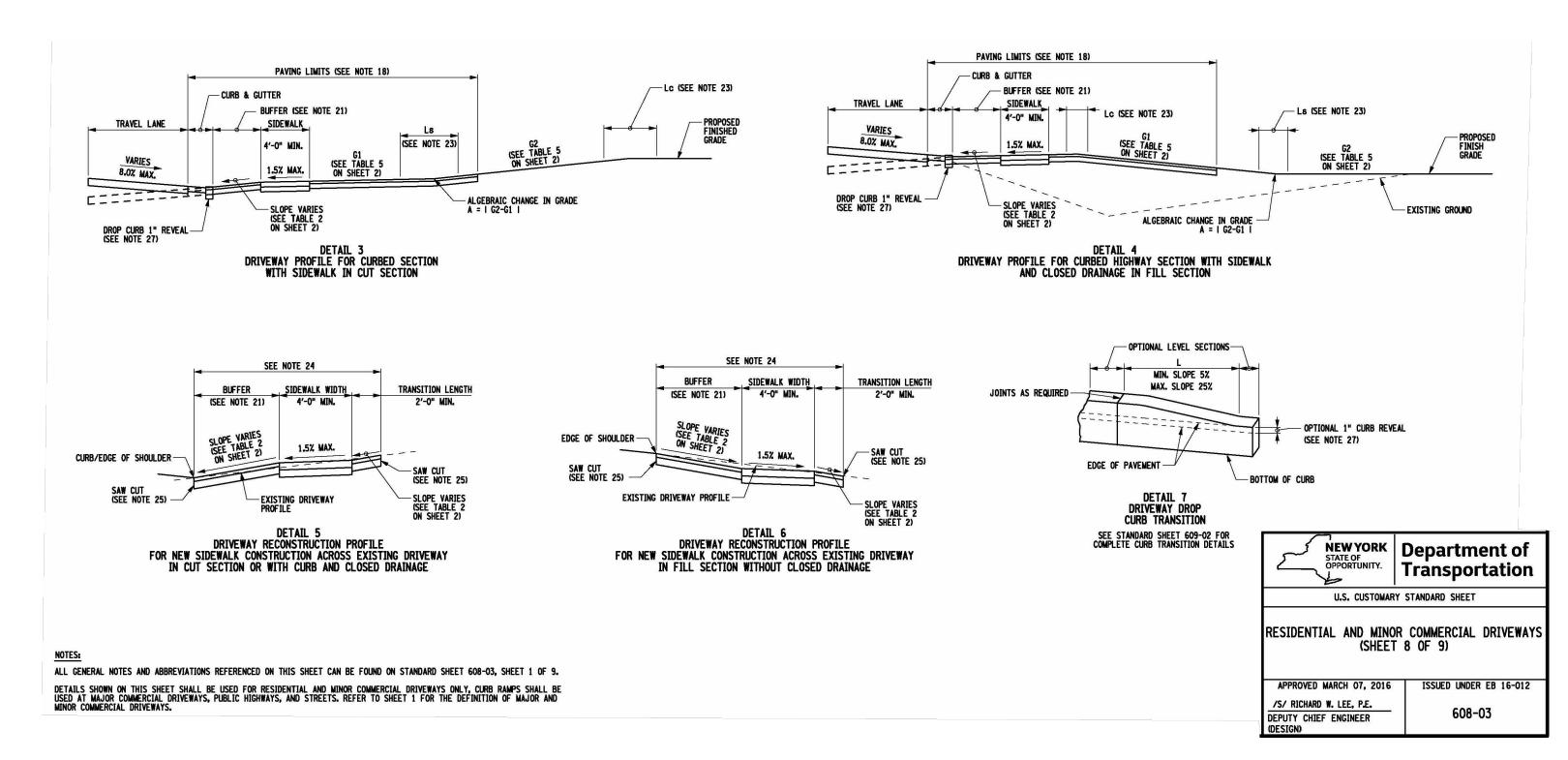
- 22. ANY PCC SIDEWALK WHICH CROSSES A DRIVEWAY SHALL HAVE A MINIMUM THICKNESS OF 6" AND INCLUDE STEEL MESH REINFORCEMENT WITH 3" OF TOP COVER.
- 23. FOR GRADE CHANGES REFER TO THE DRIVEWAY PROFILES ON SHEET 8. VERTICAL CURVES ARE RECOMMENDED TO CONNECT TANGENTS. SEE TABLE 5 'MINIMUM LENGTH OF VERTICAL CURVE' ON SHEET 2 FOR TYPICAL VERTICAL CURVE LENGTHS "L".
- 24. WHERE THE EXISTING GRADE OF THE DRIVEWAY PROFILE IS LESS THAN OR EQUAL TO 2%, MATCH THE CROSS SLOPE OF THE SIDEWALK TO THE EXISTING DRIVEWAY PROFILE GRADE.
- WHERE THE EXISTING GRADE OF THE DRIVEWAY PROFILE EXCEEDS 2% SAWCUT THE DRIVEWAY AND RECONSTRUCT A MINIMUM OF 2' ON BOTH SIDES OF THE SIDEWALK, TO TRANSITION FROM THE EXISTING GRADE OF THE DRIVEWAY PROFILE TO THE SIDEWALK CROSS SLOPE.
- 26. TO PREVENT DRIVEWAY GRADES FROM EXCEEDING THE VALUES IN TABLE 2 'MAXIMUM DRIVEWAY SLOPE' ON SHEET 2, IT MAY BE NECESSARY TO DEPRESS THE SIDEWALK ACROSS THE DRIVEWAY. SIDEWALK RAMPS SHALL HAVE THE LEAST RUNNING SLOPE POSSIBLE, WITH A MAXIMUM DESIGN AND LAYOUT SLOPE OF 7.5%. THE RUNNING SLOPE FOR WORK ACCEPTANCE SHALL BE A MAXIMUM OF 8.3%. WHERE EXISTING CONDITIONS DO NOT ALLOW THE CONSTRUCTION OF A SIDEWALK RAMP AT 8.3% OR LESS RUNNING SLOPE, THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-1" FOR DESIGN AND LAYOUT. THE RAMP LENGTH SHALL NOT BE REQUIRED TO EXCEED 15'-0" FOR WORK ACCEPTANCE.
- WHERE DRAINAGE IS CARRIED ALONG THE CURB, CONSTRUCT THE DRIVEWAY WITH A SHORT UPGRADE TO PREVENT RUNOFF FROM PONDING AT THE DRIVEWAY ENTRANCE (FLAT DRIVEWAY) OR RUNNING DOWN THE DRIVEWAY (DOWNHILL DRIVEWAY SLOPE). IF CONDITIONS MAKE THE ADDITION OF A SHORT UPGRADE IMPRACTICAL, USE I" CURB REVEAL AND CONTINUE CURB ACROSS THE DRIVEWAY OPENING. TYPICALLY, CURB REVEAL WILL NOT BE CONSTRUCTED IN RURAL AREAS. IF CURB REVEAL IS SPECIFIED FOR A SPECIFIC DRIVEWAY, IT WILL BE NOTED IN THE DRIVEWAY TABLE OF THE CONTRACT PLANS IN THE 'COMMENTS' COLUMN.

- 28. THE ENGINEER MAY INTERCHANGE TYPE 1, TYPE 3 AND TYPE 4 RESIDENTIAL DRIVEWAYS TO BETTER MATCH THE EXISTING ENTRANCE TYPES ALONG THE HIGHWAY CORRIDOR WHILE CONSIDERING AVAILABLE SPACE, CONSTRUCTABILITY, SAFETY, AND FUNCTIONALITY. THE DRIVEWAY TYPE SHALL COMPLY WITH TABLE 4 'DRIVEWAY ENTRANCE TYPE SELECTION' ON SHEFT 2.
- FOR A ONE-WAY DRIVEWAY ENTRANCE OR EXIT, THE DRIVEWAY ENTRANCE WIDENING IS ONLY NECESSARY ON ONE SIDE OF THE DRIVEWAY TO ACCOMMODATE THE SHARPER TURNING MOVEMENT. ONE-WAY DRIVEWAYS WILL BE IDENTIFIED ON THE DRIVEWAY DRIVE WAYS OF THE CONTRACT PLANS UNDER 'COMMENTS'. FOR CURBED HIGHWAYS, A SMALL CORNER CURB RADIUS OF 2' (OR '1/2 BULLNOSE' CURB ALONG LOW SPEED HIGHWAYS) SHALL BE CONSTRUCTED TO ELIMINATE A SHARP CORNER BEND IN THE CURB LINE (WHICH IS SAFER FOR SNOWPLOW OPERATIONS).

- 31. FOR DRIVEWAY MATERIAL REQUIREMENTS, USE TABLE 3 'DRIVEWAY MATERIALS AND THICKNESS' ON SHEET 2.







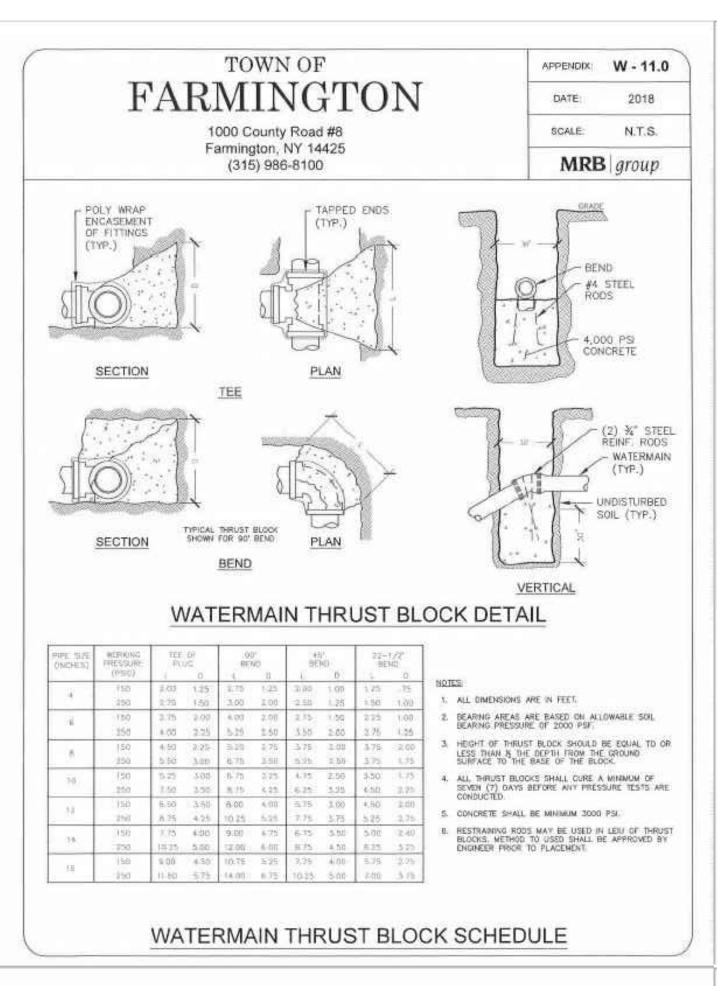


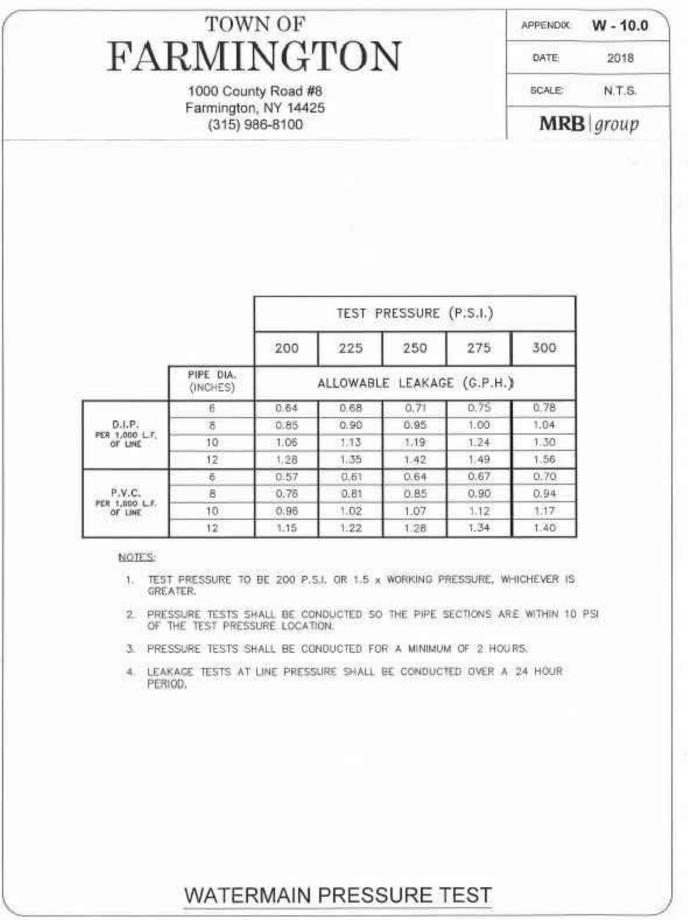
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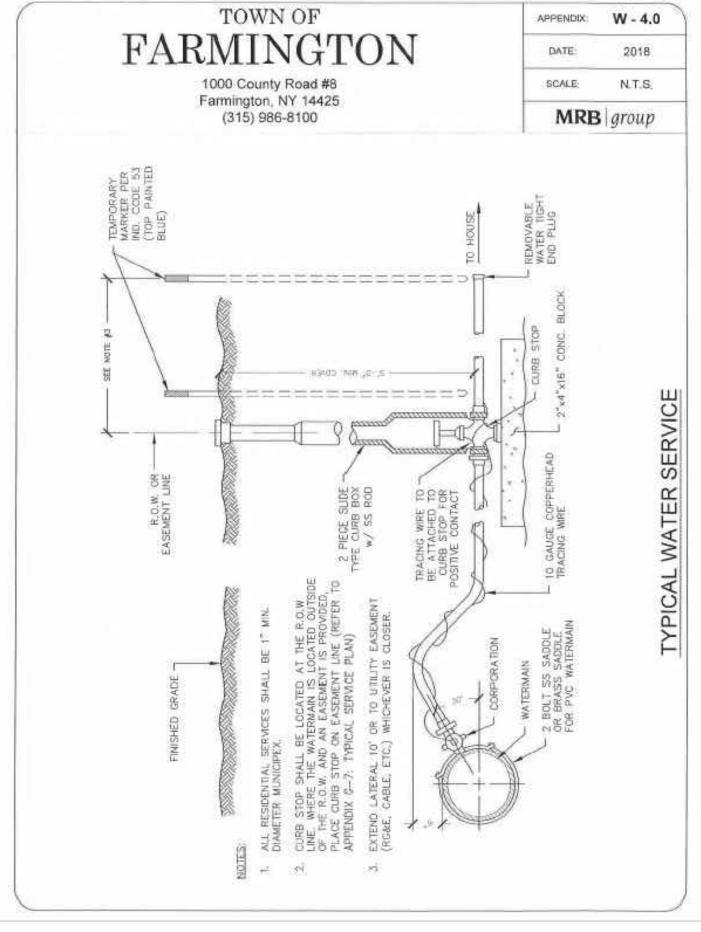
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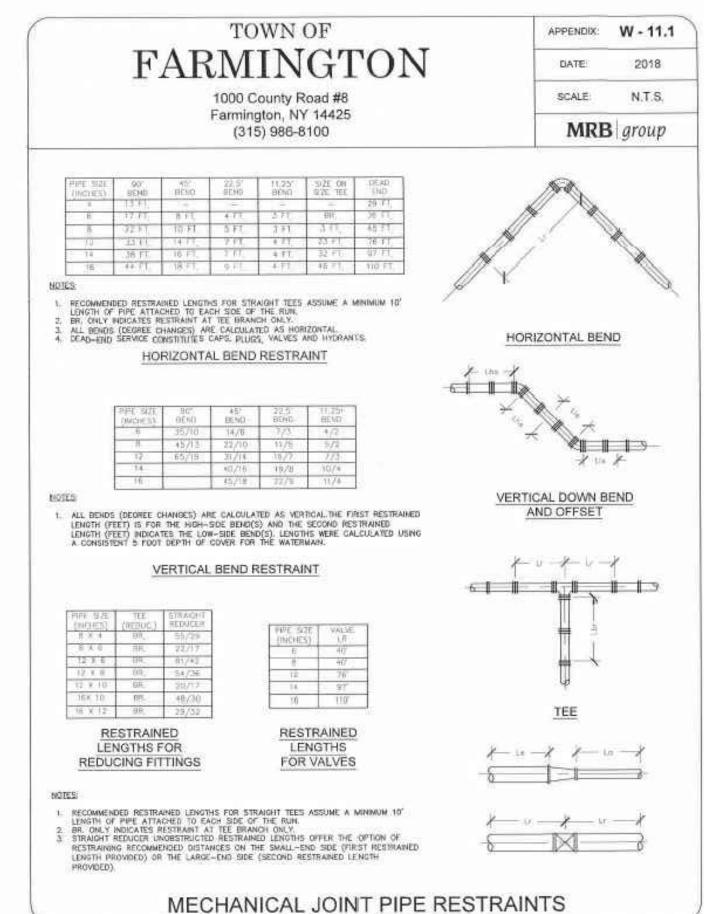
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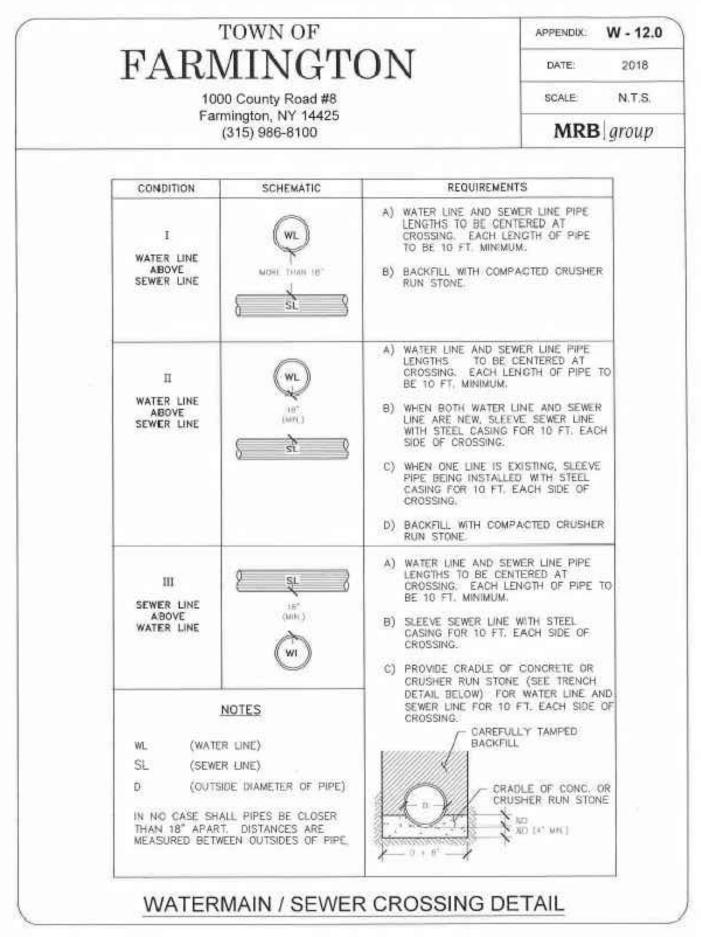
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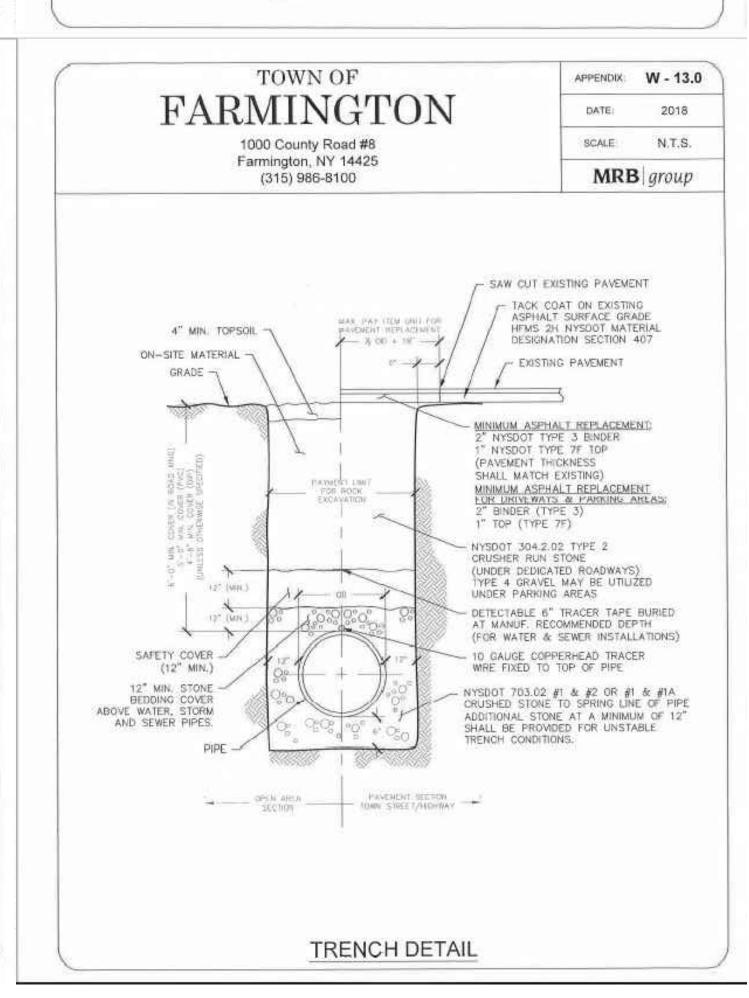




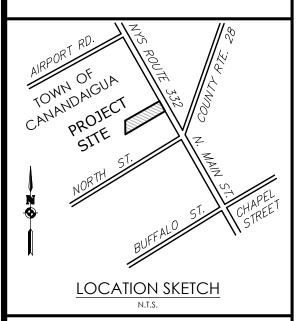




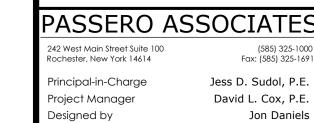


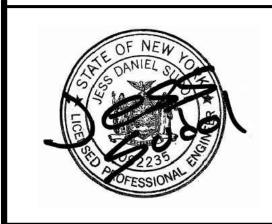


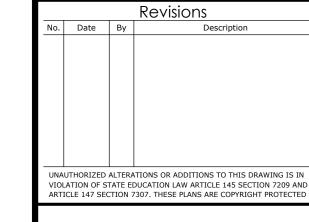




ROYAL WASH CANANDAIGUA, LL 2851 MONROE AVE ROCHESTER, NY 14618







DETAILS

ROYAL CAR WASH

Town/City: Canandaigua
County: Ontario State: New Y

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