



**COUNTY OF PRINCE WILLIAM**

5 County Complex Court, Prince William, Virginia 22192-9201  
(703) 792-6830 Metro 631-1703, Ext. 6830 FAX (703) 792-4401

Internet [www.pwcgov.org](http://www.pwcgov.org)

PLANNING  
OFFICE

Christopher M. Price, AICP  
Director of Planning

June 7, 2013

**STAFF REPORT**

Special Use Permit #PLN2012-00434, Potomac Shores Town Center (formerly known as Harbor Station Town Center)  
(Potomac Magisterial District)

**Planning Commission Public Hearing Date: June 19, 2013**

**Staff Recommendation: Approval**

**I. Background is as follows:**

- A. Request - This is a request to amend SUP #PLN2000-00079 to allow the expansion of the previously approved but not yet built Potomac Shores town center from 131.4 acres to 228.5 acres with up to 3.6 million square feet of employment and commercial building floor area and up to 1,833 dwelling units with waivers and modifications. This request is contingent upon the approval of REZ #PLN2012-00420 which would rezone the entire town center to the PMD, Planned Mixed Use zoning district.

	<b>Existing SUP</b>	<b>Amended SUP</b>
Size	131.4 acres	228.5 acres
Commercial Floor Area	992,000 sq. ft. of floor area*	3.6 million sq. ft. of floor area*
Residential Units	1,353 dwelling unit*	1,833 dwelling units*
VRE Station	Built by others	Built by applicant
VRE Parking	550 spaces	550 spaces
Library	5 acres at discretion of applicant	1 acre site with parking
Police	1,500 sq ft field office and parking	1,500 sq. ft. field office and parking

*\*The overall allowance for residential and commercial development has not changed within the master zoning area for Potomac Shores, but as proposed more of the allowance can be achieved within the town center.*

*An Equal Opportunity Employer*

- B. Location - The SUP site is located on the eastern end of the Cherry Hill peninsula adjacent to the Potomac River (see maps in Attachment A). The SUP site is identified on County maps as GPINs **8389**-14-1892 pt., -18-2218 pt., -41-3404 pt., -**42**-6195 pt., -9772, -4267 pt., -**43**-6730, -7941, -8622, -9843, -**52**-4387, -7685, -8065, -1394, -1849pt., -2672, -4232 pt., -5473, -1680, -**53**-2532, -4114, -4443, -5711, -7408, -0224, -6042, -54-0695, -60-1576 pt., -**62**-0140, -0162 -0327 pt., -1254, -5228, -5363pt., -5845 pt., -3777, -5354, -3361, -1962, -63-1137 pt., -4442.
- C. Comprehensive Plan - The site is designated REC, Regional Employment Center, SRH, Suburban Residential High and ER, Environmental Resource, and is part of the Potomac Communities Sector Plan.
- D. Zoning/Acreage - The +/-228.5-acre SUP site is zoned PMD, Planned Mixed Use District; PBD, Planned Business District; M-1, Heavy Industrial; and PMR, Planned Mixed Residential District. There is an associated rezoning (REZ #PLN2012-00420) which if approved would rezone the town center to PMD zoning only. The expanded town center would not be possible without the rezoning.
- E. Surrounding Land Uses - The subject site is surrounded by the Potomac River, CSX right-of-way, a restaurant, and vacant land to the east; Powells Creek to the north, part of the Potomac Shores Golf Course, vacant land and single family detached dwellings along Cherry Hill Road (Landbays 1 and 7); and to the south the future southern neighborhoods of Potomac Shores containing single-family detached and attached dwellings (Landbay 5), the potential marina site and future commercial development (Landbay 8).

**II. Current Situation** is as follows:

- A. Planning Office Recommendation - The Planning Office recommends approval of SUP #PLN2012-00434, Potomac Shores Town Center except for waiver #18 for easement encroachments, subject to the conditions dated June 7, 2013, found in Attachment C, based on the following reasons:
- The mixed use proposal is generally consistent with the land use planning for the area.
  - The proposal maintains the yields of commercial and residential development and provides phasing of infrastructure and commercial floor area. It also increases the opportunity to locate residential and commercial uses within the town center which will help reinforce the town center as the focal point of Potomac Shores.
  - The proposal is compatible with the adjacent uses.



- The Design Guidelines provide a detailed and cohesive understanding of the objectives of the development.
- A VRE commuter train station with associated parking is provided.

B. Planning Commission Public Hearing - A public hearing has been advertised for June 19, 2013, before the Planning Commission.

**III. Issues** in order of importance are as follows:

A. Comprehensive Plan

1. Long-Range Land Use - Is the proposed use consistent with those uses intended by the REC, Regional Employment Center, SRH, Suburban Residential High and ER, Environmental Resource designations as part of the Potomac Communities Sector Plan?
2. Level of Service (LOS) - How does the proposal address the Prince William County Policy Guide for Monetary Contributions?

B. Community Input - Have members of the community raised any issues?

C. Other Jurisdictional Comments – Have adjacent jurisdictions raised any issues?

D. Legal Uses of the Property - What uses are allowed on the property? How are legal issues resulting from Planning Commission action addressed?

E. Timing - When must the Planning Commission take action on this application?

**IV. Alternatives** beginning with the staff recommendation are as follows:

A. Recommend approval of SUP # PLN2012-00434, Potomac Shores Town Center, except for waiver # 18 for easement encroachments, subject to the conditions dated June 7, 2013, found in Attachment C.

1. Comprehensive Plan Consistency Analysis:

- a) Long-Range Land Use - The majority of the site is designated REC, Regional Employment Center on the Long-Range Land Use Map with smaller areas of SRH, Suburban Residential High and ER, Environmental Resource. The proposed town center will maintain and expand a mix of commercial and residential uses to form a cohesive, compact urban development which serves Potomac Shores with potential to serve the region.
- b) LOS - The level of service impacts related to the request would be mitigated by the conditions of the special use permit as follows:

	Per unit amount	Area	Contribution Amount
Fire and Rescue	\$0.29/sq. ft.	3.6 million square feet	\$1,044,000.00*
Water Quality	\$75 per acre	±228 acres	\$17,100.00*
<b>Total</b>			<b>\$1,061,100.00</b>

\*The amount is part of the proffered monetary for REZ #PLN2012-00420.

\*\*The LOS applicable for the residential portion of the town center will be addressed through the proffers.

2. Community Input - Notice of the application has been transmitted to adjacent property owners within 200 feet. Several citizens have called with questions but no issues were raised.
3. Other Adjacent Jurisdictions - The site is outside of the required notification area of any adjacent jurisdictions. However, the application was forwarded to the Town of Dumfries as a courtesy due to its proximity to the overall rezoning which contains the town center. The Town raised no specific concerns about the town center.
4. Legal Uses of the Property - In addition to the property's by-right uses under the PMD zoning classification, such as mixed use development, the property could be used for an expanded town center use subject to various waivers and modification and the approved SUP conditions. Legal issues resulting from Planning Commission action are appropriately addressed by the County Attorney's office.
5. Timing - The Planning Commission has until September 17, 2013, 90-days from the first public hearing date, to take action on this proposal. Approval of the SUP would meet the 90-day requirement.

B. Recommend denial of SUP # PLN2012-00434, Potomac Shores Town Center.

1. Comprehensive Plan
  - a) Long-Range Land Use - If the application is denied, the land use designation of the site would remain REC, SRH and ER within the Potomac Communities Sector Plan, and the zoning classification would remain PMD, PBD,, PMR M-1 and A-1. Additionally, the previously approved SUP allowing a town center would still be in effect.
  - b) LOS - Denial would not have any impact on the existing LOS.

2. Community Input - Notice of the application has been transmitted to adjacent property owners within 200 feet. Several citizens have called with questions but no issues were raised.
3. Other Adjacent Jurisdictions - The site is outside of the required notification area of any adjacent jurisdictions. However, the application was forwarded to the Town of Dumfries as a courtesy due to its proximity to the overall rezoning which contains the town center. The Town raised no specific concerns about the town center.
4. Legal Uses of the Property - The site could be developed with uses permitted in the PMD, PBD, PMR, and M-1 zoning districts subject to the previously approved proffers and the previously approved town center albeit smaller in area. Legal issues resulting from the Planning Commission action are appropriately addressed by the County Attorney's office.
5. Timing - The Planning Commission has until September 17, 2013, 90 days from the first public hearing date, to take action on this proposal. Denial of the SUP would meet the 90-day requirement.

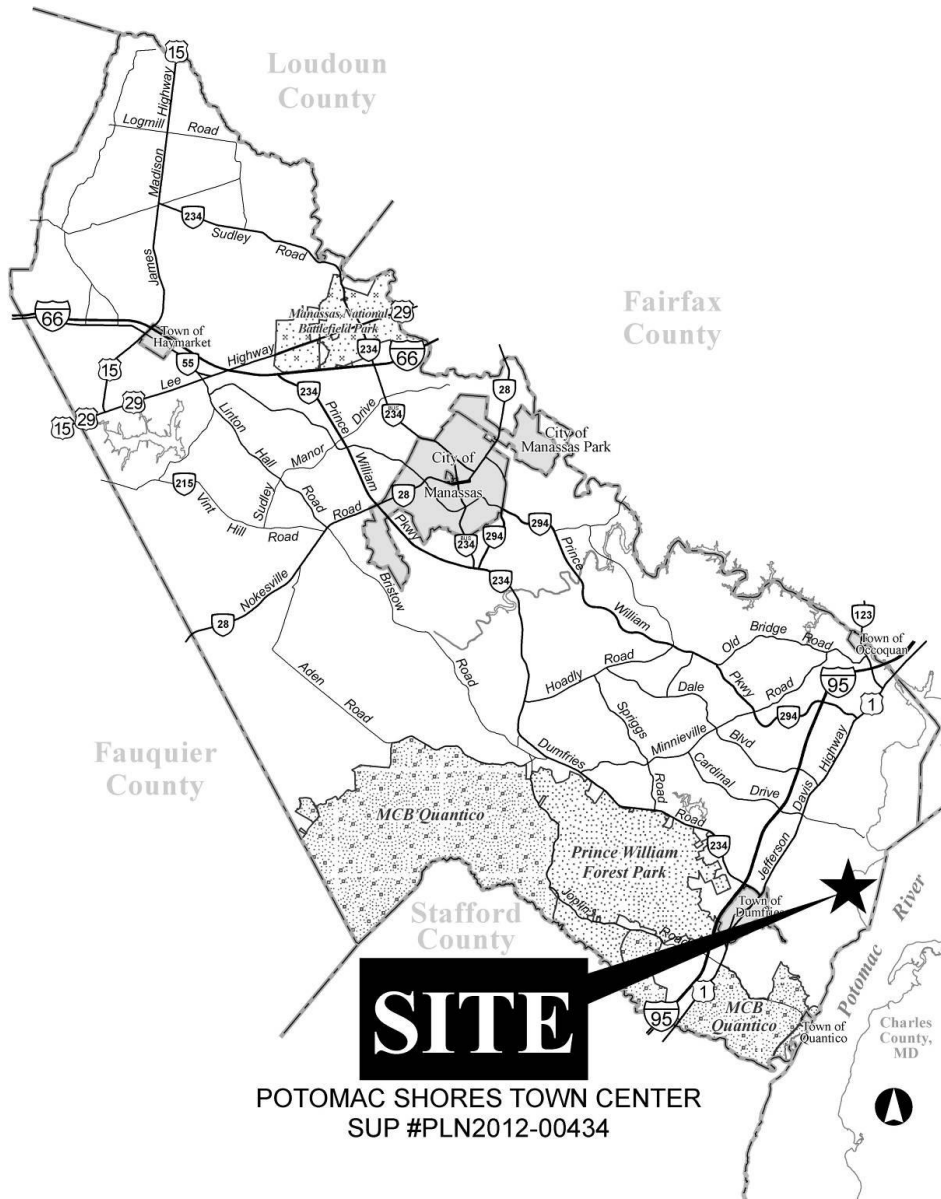
V. **Recommendation** is that the Planning Commission concurs with Alternative A and recommends approval of SUP #PLN2012-00434, Potomac Shores Town Center, except for waiver #18 which allows easement encroachments, subject to the proposed conditions dated June 7, 2013.

**Staff:** Fran Burnszynski, AICP, X6962

**Attachments**

- A. Area Maps
- B. Staff Analysis
- C. Proposed Conditions
- D. SUP Plan: Schematic Land Use Plan, Street Grid, Infrastructure, and Pedestrian Circulation Plan, Streetscape/Community Facilities Plan, Infrastructure Phasing Plan
- E. Design Guidelines
- F. Historical Commission Resolution
- G. Environmental Constraints Analysis

**Attachment A – Maps  
VICINITY MAP**



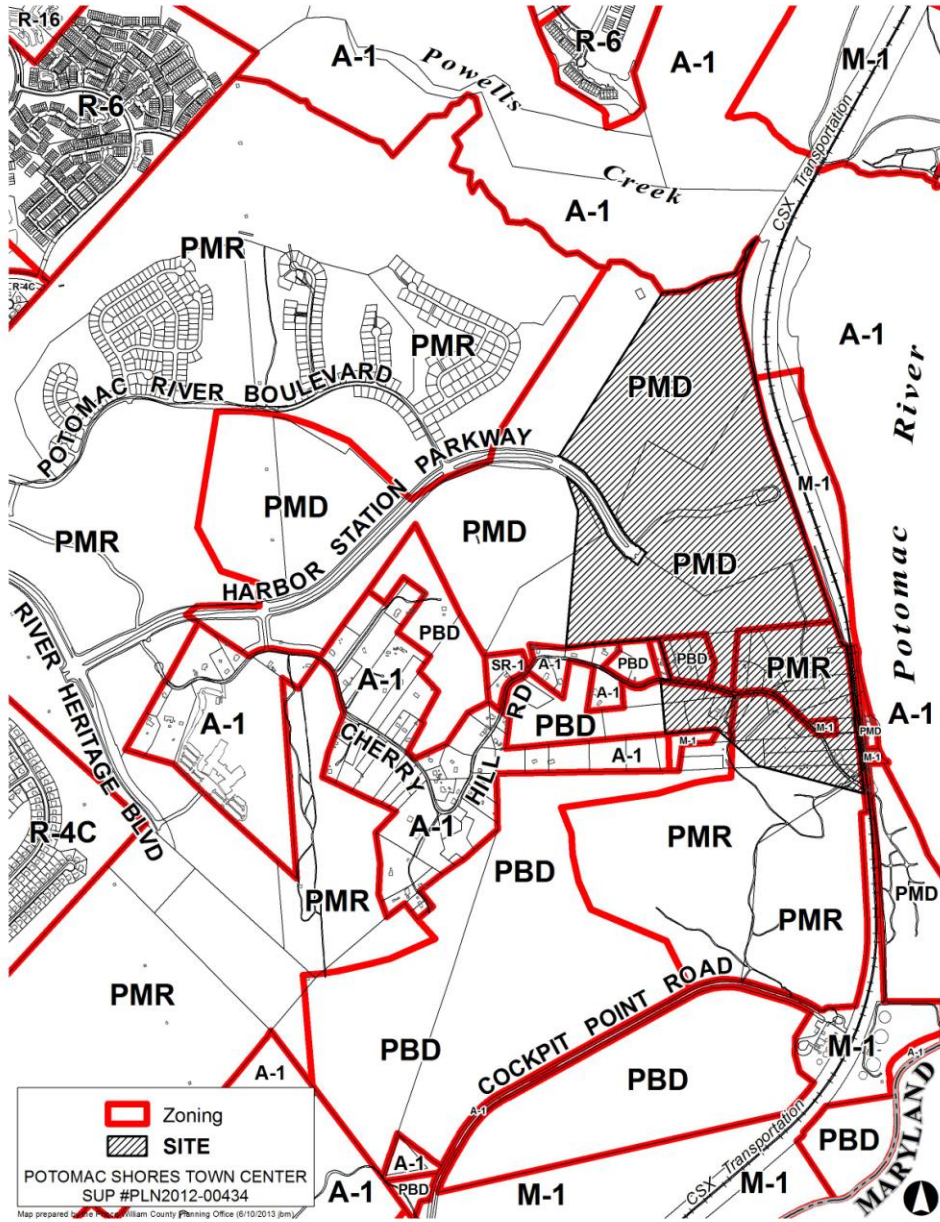
**SITE**  
POTOMAC SHORES TOWN CENTER  
SUP #PLN2012-00434

**Attachment A – Maps  
AERIAL MAP**

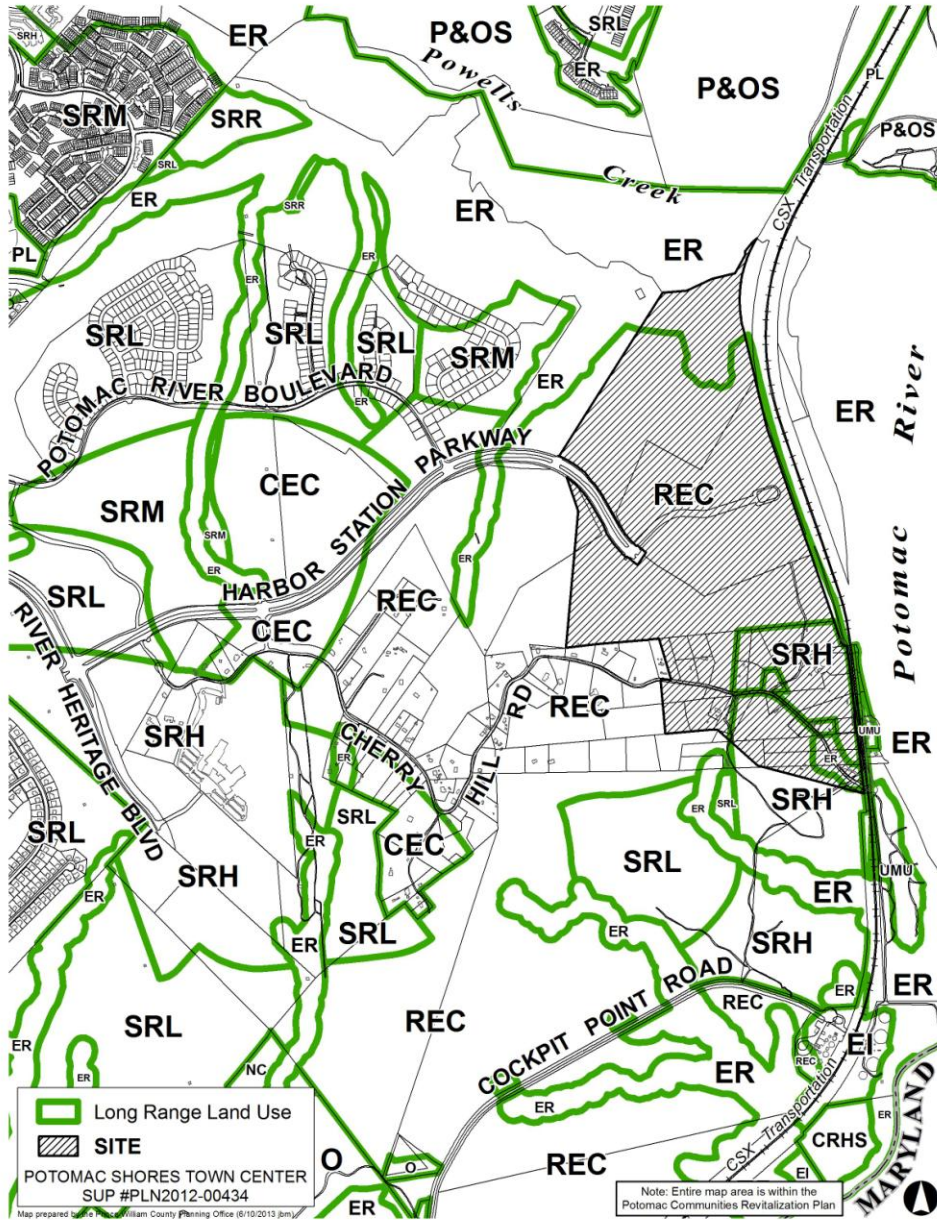




**Attachment A – Maps  
EXISTING LAND USE AND ZONING MAP**



**Attachment A – Maps  
LONG-RANGE LAND USE MAP**



**Attachment B - Staff Analysis**

<b>Part I. Summary of Comprehensive Plan Consistency</b>
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**Staff Recommendation: Approval**

The following is a summary of staff’s analysis of this special use permit request to permit the amendment of an existing SUP for town center. This analysis is based on the relevant comprehensive plan action strategies, goals, and policies. A complete analysis is provided in Part II of this report.

<u>Plan</u>	<u>Consistency</u>	<u>Reasons</u>
Long-Range Land Use	Yes	The revised special use permit plan is generally consistent with the recommended land use plan, compatible with adjacent land uses and provides residential and non-residential yields as previously approved as part of the entire rezoning area.
Community Design	Yes	The applicant has proffered to design guidelines which will help ensure a coordinated development, provides guidance about development within each block, including streetscapes; open space, signage and a network of pedestrian connections.
Cultural Resources	Yes	As part of the companion rezoning, the applicant has proffered to conduct archaeological studies and has proffered to curate cultural resource artifacts with the County. Additionally building heights have been limited within the viewshed of the Cockpit Point battery, but does not additionally restrict building heights in the town center.
Economic Development	Yes	The revised proposal continues to provide opportunities for quality employment within an expanded town center.
Environment	Yes	The site has steep slopes and sensitive soils; however, the Town Center site is the least environmentally sensitive portion of the peninsula. Conditions provide for mitigation efforts.
Fire and Rescue	Yes	The applicant continues to proffer a level of service monetary contribution. The site is within the recommended response times but the station is over capacity. Mitigation for buildings with modified bulk standards has been conditioned.



**Attachment B - Staff Analysis**

<u>Plan</u>	<u>Consistency</u>	<u>Reasons</u>
Housing	Yes	The applicant continues to proffer a monetary contribution to Housing Preservation and Development Fund.
Library	Yes	The applicant has proffered to dedicate an acre site within the town center and \$100,000 contribution has also been proffered.
Parks, Open Space and Trails	Yes	The applicant has proffered to provide a minimum of 3 open space areas within the town center, the Potomac National Heritage Scenic Trail will run along the eastern edge of the town center, a golf course is partially located in Blocks 2 and 10 and the community recreation center will be located in Block 9.
Police	Yes	The applicant has proffered and a condition has been drafted for a field station in the town center with associated parking
Potable Water	Yes	The applicant has provided an Infrastructure and Utility Plan and shall construct all on-site and off-site public water facilities.
Sanitary Sewer	Yes	The applicant has provided an Infrastructure and Utility Plan and shall construct all on-site and off-site public sewer facilities. As proffered, the applicant has provided an Infrastructure and Utility Plan and shall construct all on-site and off-site public sewer facilities.
Schools	Yes	As part of the companion rezoning, the applicant has proffered elementary and middle school sites outside the town center which they would grade and prepare for development.
Transportation	Yes	The compact layout and more dense development framework promotes transit-oriented development and walkability. As part of the companion rezoning, the applicant has proffered, a VRE station with parking, a roadway network to access the station from Route 1, a modified quadrant intersection at Route 1 and Route 234, an updated travel demand management program, a trail network and bus shelters.

**Attachment B - Staff Analysis**

<b>Part II. Comprehensive Plan Consistency Analysis</b>
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The following table summarizes the area characteristics (see maps in Attachment A):

Direction	Land Use	Long Range Future Land Use Map Designation	Zoning
North	Powells Creek	P&OS and ER	A-1
South	Vacant land planned to contain the southern neighborhoods of Potomac Shores, commercial and a potential marina	SRH, REC, ER and UMU	PMR, M-1, PBD & A-1
West	Potomac Shores Golf Course, existing single-family detached dwellings and vacant land	REC & ER	PMD, PBD, M-1 & PMR
East	Potomac River, CSX railroad, restaurant, and vacant land	UMU, ER & Charles County, Maryland	M-1, PMD, A-1 & Charles County, MD

**Long-Range Land Use Plan Analysis**

Through wise land use planning the County ensures that landowners are provided a reasonable use of their land while the County is able to judiciously use its resources to provide the services for residents and employers' needs. The Long Range Land Use Plan sets out policies and action strategies that further the County's goal of providing a land use pattern that encourages fiscally sound development and achieves a high quality living environment. In addition to delineating land use designations on the Long Range Land Use Map, the plan includes recommendations relating to ensuring adequate land for economic development opportunities, providing for a diverse housing market, protecting against the encroachment of incompatible land uses, encouraging infill development within the development area, protecting environmentally sensitive lands, promoting mixed use development where appropriate, encouraging the provision of adequate public facilities for existing and planned development, preserving valuable open space and environmental resources, encouraging higher density development near existing and future transit facilities, and utilizing the sector planning process to provide more detailed recommendations where appropriate.

This site is located in the Development Area of the County, and is designated Regional Employment Center (REC), Suburban Residential High (SRH) and Environmental Resource (ER) as part of the Potomac Communities Sector Plan. The following table summarizes the uses and densities intended within the REC, SRH and ER designations within the Sector Plan:

**Attachment B - Staff Analysis**

<b>Long-Range Land Use Plan Classification</b>	<b>Land Uses Intended</b>
<b>REC</b>	<p><b>Regional Employment Center (REC).</b> The purpose of the Regional Employment Center classification is to provide for areas located close to and/or with good access from an interstate highway where intensive regional employment uses are to be located. REC projects* should be planned and developed in a comprehensive, coordinated manner. Primary uses in the REC are mid-rise and/or high-rise office (including government offices particularly those for Prince William County agencies), research and development facilities, lodging, and mixed-use projects. Residential uses shall represent no greater than 25 percent of the total REC gross floor area of the project. Drive-in/drive-through uses are discouraged. Shared/structured parking is encouraged. The acceptable housing type within any mixed-use REC project is multifamily, at a density of 16-30 dwelling units per gross acre, less the ER, Environmental Resource designated portion of a property. Development in REC projects shall occur according to an infrastructure implementation plan submitted at the time of rezoning. The intent of this plan is to ensure that critical infrastructure for office, employment and lodging uses is developed adequately for each phase of the project. Development shall also occur according to a phasing plan that must ensure that office, employment and lodging uses are always the primary uses within the area rezoned. Office development in REC areas is encouraged to be in accordance with the <i>Illustrative Guidelines for Office Development</i>, provided as a supplement to the Community Design Plan chapter of the Comprehensive Plan and available from the Planning Office. A minimum office building height of 4-6 stories is preferred.</p>
<b>SRH</b>	<p><b>Suburban Residential High (SRH).</b> The purpose of the Suburban Residential High classification is to provide for areas of a variety of housing opportunities at the highest suburban density. The preferred housing type in this classification is multifamily (apartments and condominiums). The density range in SRH projects is</p>

\* In all instances, a “project” or “project area” is defined as the boundary of a rezoning or special use permit request.

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<b>ER</b>	<p>10-16 dwellings per acre, less the ER-, Environmental Resource-designated portion of a property.</p> <p><b>Environmental Resource (ER).</b> This classification is explained in detail within the Environment Plan. Therein are located goals, policies, action strategies, and other Plan components designed to protect the sensitive nature of the identified resources. Environmental Resources include all 100-year floodplains as determined by the Federal Emergency Management Act (FEMA), Flood Hazard Use Maps or natural 100-year floodplains as defined in the Design and Construction Standards Manual, and Resource Protection Areas (RPAs) as defined by the Chesapeake Bay Preservation Act. In addition, areas shown in an environmental constraints analysis submitted with a rezoning or special use permit application with wetlands; 25 percent or greater slopes; areas with 15 percent or greater slopes in conjunction with soils that have severe limitations; soils with a predominance of marine clays; public water supply sources; and critically erodible shorelines and stream banks are considered part of the Environmental Resource Designation.</p>
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The site is located on the eastern end of the Cherry Hill peninsula adjacent to the Potomac River. The +/-228.5 acre SUP site is zoned PMD, PBD, PMR M-1 and A-1, but the associated rezoning will rezone the entire site to PMD.

The proposed SUP would allow for an expansion of the town center from +/- 131.4 acres to +/-228.5 acres. The maximum commercial floor area yield would be 3.6 million square feet within the town center and the residential dwelling yield within the town center would be up to 1,833 dwelling units. The overall commercial and residential yields for Potomac Shores will not increase with this request but the distribution would possibly change. Specifically the associated rezoning eliminates a secured employment campus outside the town center and moves the associated development into the expanded town center. Similarly the residential development within the overall project would remain the same at 3,987 but the option to increase the residential unit within the town center is increased. Staff supports this change because it will focus more of the development potential for Potomac Shores within the expanded town center. It is believed this will help establish the mix of uses within the town center.

**Proposal's Strengths**

- Long Range Land Use Conformity – The proposal is generally consistent with the Comprehensive Plan which recommends a mixed-use project be developed on the Cherry Hill peninsula. As proposed, the +/-228.5 acres town center will contain a hotel, office, retail, and higher density residential as well as a VRE commuter rail station, strategically

## Attachment B - Staff Analysis

and conveniently located open space, portions of the golf course, a library and a community recreation center.

- Phasing – As part of the companion rezoning, the applicant has proffered a phasing schedule for the infrastructure in the town center, the VRE commuter train station and associate parking and a minimum of 165,000 square feet of non-residential building floor area prior to issuance of the building permits for 2,300 dwellings within the larger Potomac Shores development.
- Livable Community - The town center proposal is consistent with the objective of encouraging livable, planned, compact communities that promote a variety of residential opportunities.
- Mixed-use Development - The Potomac Shores Town Center proposes a unique, quality mixed-use development that is encouraged for high visibility REC locations.
- Public Facility Sites - The applicant has provided for both a library site and a police substation within the town center.
- Transit – Oriented Development - The design of the proposed town center focuses on pedestrian movement throughout the site in conjunction with mass transit opportunities involving a proposed VRE Station.

### Proposal's Weaknesses

- Phasing – The Comprehensive Plan generally looks for mixed-use developments containing employment centers to be constructed according to a phasing plan that ensures that at any one time no less than half the building floor area is nonresidential within the development. While the proposed phasing does not achieve this goal, it should be noted that the previously approved version did not either, and the previously approved version did not include the construction of a VRE commuter rail station.
- High Density in Proximity to ER – As proposed, the development has the highest residential densities closest to the Potomac River and the ER areas. Density at the low end of the residential range is recommended near ER areas.

On balance, this application is found to be consistent with the relevant components of the Long-Range Land Use Plan.

## **Attachment B - Staff Analysis**

### **Community Design Plan Analysis**

An attractive, well-designed County will attract quality development, instill civic pride, improve the visual character of the community, and create a strong, positive image of Prince William County. The Community Design Plan sets out policies and action strategies that further the County's goals of providing quality development and a quality living environment for residents, businesses and visitors, and creating livable and attractive communities. The plan includes recommendations relating to building design, site layout, circulation, signage, access to transit, landscaping and streetscaping, community open spaces, natural and cultural amenities, stormwater management, and the preservation of environmental features.

The Potomac Shores Town Center development is controlled by design guidelines, which provide specific guidance. Design guidelines were developed and approved as part of the original Southbridge SUP, and it has mechanisms for amendments, which include review and approval amendments by County staff.

The applicant has requested 38 waivers and modifications. Of these, 31 were previously approved. Staff has reviewed them and recommends approval of all the requests as they are drafted and conditioned except for Waiver #17 which allows encroachments in easements. The requests appear with the conditions in Attachment C of the staff report.

### **Proposal's Strengths**

- Site Layout – As conditioned, the site layout shall be substantially as shown on the SUP plan.
- Zoning Ordinance Requirements - Because of requirements in the Zoning Ordinance for town centers, including the preparation of design guidelines, the policies of the town center provisions will be achieved.
- Design Review Committee - Covenants and restrictions will place regulatory controls in the town center with a Design Review Committee (DRC). The Committee will be responsible for overseeing all aspects of development within the town center.
- Modifications to Design Guidelines – SUP conditions require that the town center must be developed in substantial conformance with the proposed design guidelines and that Prince William County and the DRC must be notified and must approve all modifications to the design guidelines. Prince William County must be notified of any modification or waiver from the guidelines applied for by a property owner. According to SUP conditions, deviations from or modifications to the guidelines which are not consistent with conditions of the zoning, proffers, and the special use permit cannot be approved by the Design Review Committee.
- Building Massing – The applicant has established a development framework which will provide different building heights and densities from block to block to ensure visual interest and diversity of opportunities to meet market needs.

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- Building Setbacks - The applicant is proposing to site buildings close to the sidewalks without parking or drive aisles separating the use from the adjacent sidewalk.
- Building siting and Viewsheds – As indicated in the design guidelines, buildings in Block 5 shall be oriented to maintain views of the river from interior sites.

### **Proposal's Weaknesses**

- Streetscape – The applicant has removed a detailed overall streetscape plan from the town center plans. While streetscape sections are shown in the design guidelines, it is hard to get an overall sense about the quantity of trees to be provided throughout the town center.

**On balance**, this application is found to be consistent with the relevant components of the Community Design Plan.

### **Cultural Resources Plan Analysis**

Prince William County promotes the identification, evaluation, and protection of cultural resource sites throughout the County, as well as the tourism opportunities these sites present. The Cultural Resources Plan recommends identifying, preserving, and protecting Prince William County's significant historical, archaeological, architectural, and other cultural resources—including those significant to the County's minority communities—for the benefit of all of the County's citizens and visitors. To facilitate the identification and protection of known significant properties that have cultural resource values worthy of preservation, the land use classification "County Registered Historic Site (CRHS)" is used in the Comprehensive Plan. The plan includes areas of potentially significant known but ill-defined or suspected pre-historic sites, Civil War sites, historic viewsheds, landscapes or areas of potential impact to important historic sites, and encourages the identification, preservation, protection, and maintenance of all cemeteries and/or gravesites located within the County.

Phase I archaeological studies are generally required at submission of rezoning and special use permit applications where significant prehistoric or historic sites and cemeteries are known or suspected. Phase II evaluations and treatment plan studies may also be required. Records research is required of all applicants for rezoning, special use permit, comprehensive plan amendment, and public facility review applications.

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### **Proposal's Strengths**

- **Interpretive Plan** – As part of the associated rezoning, the applicant has proffered to provide an interpretive plan that includes historical markers, a self-guided brochure for the trail system, and a small interpretative display in the town center to explain the history of the Cherry Hill peninsula.
- **Curation** – As part of the associated rezoning, the applicant has proffered to curate cultural resource artifacts found on the site with the County, which it does not otherwise wish to display. The County has further requested that all items be curated and if the applicant would like to borrow them for specific reasons arrangements could be established.
- **Cemeteries** – The applicant has proffered to preserve the cemeteries on-site in place, including the Dunnington Plantation cemetery, which is within the town center.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Cultural Resources Plan.

### **Economic Development Plan Analysis**

An enhanced, diverse nonresidential tax base creates quality jobs, allowing people to live, work and recreate in Prince William County. The Economic Development Plan sets out policies and action strategies that further the County's goal of attracting and fostering the growth of environmentally sound industries to create quality jobs and diversify the nonresidential tax base, creating a climate where citizens can live and work in Prince William County. The plan includes recommendations relating to business attraction, retention and expansion, the provision of adequate infrastructure, redevelopment of less competitive areas, telecommuting and other information-age opportunities, and recognition of tourism as an industry.

Under the previous approval, a secured campus was contemplated and approved in the area of Landbays 4 and 5. The applicant has considered this configuration but believes that expanding the town center and bringing all the noncommercial development into Landbays 8 and 9 will help create the synergy and potential necessary to foster a viable 24 hour 7 day a week town center.

### **PROPOSAL'S STRENGTHS**

- **Quality Employment Opportunities** – The special use permit plan along with the master zoning plan now allows for a more cohesive employment center within Potomac Shores in the town center that can draw major employers to the Potomac Communities area. The employment center is large enough to be used by major defense employers, should they choose to relocate to this site between Marine Corps Base – Quantico and Fort Belvoir.



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- Consolidation of Employment Uses in the Town Center – The proposal does not change the established overall potential for nonresidential building floor area in Potomac Shores, which allows up to 3.7 million square feet of commercial building floor area. However it allows for the consolidation of employment uses up to 3.6 million square feet within the town center. The remaining 100,000 square feet of nonresidential building floor area could be located in Landbay 8 just outside the town center to the southeast. This will help to create more synergy in the town center and help make the commercial floor area more transit oriented.

### **PROPOSAL'S WEAKNESSES**

- None identified.

On balance, this application is found to be consistent with the relevant components of the Economic Development Plan.

### **Environment Plan Analysis**

Prince William County has a diverse natural environment extending from sea level to mountain crest. Sound environmental protection strategies will allow the natural environment to co-exist with a vibrant, growing economy. The Environment Plan sets out policies and action strategies that further the County's goal of preserving, protecting and enhancing significant environmental resources and features. The plan includes recommendations relating to the incorporation of environmentally sensitive development techniques, improvement of air quality, identification of problematic soil issues, preservation of native vegetation, enhancement of surface and groundwater quality, limitations on impervious surfaces, and the protection of significant viewsheds.

The town center site does contain slopes exceeding 25% as well as large stands of hardwood and pine forests. Although the majority of the town center site contains loamy soils, there are portions of the site which contain marine clay soils. There are small areas of wetlands within the town center boundaries. A resource protection area is located north of the site. A large pond which drains into Powell's Creek is located directly north of the town center parcel.

Although the entire Potomac Shores site is environmentally sensitive, looking strictly at existing soils, slopes, and state and federally endangered species management, the town center site is one of the least environmentally sensitive portions of the Cherry Hill peninsula.

A bald eagle nest has been identified within the town center. It will be protected as proffered and under state and federal law.

### **Proposal's Strengths:**

- IEAMP - An Integrated Environmental Asset Management Plan (IEAMP) has been prepared and proffered for the entire Potomac Shores site including the town center area which sets standards for geotechnical study, erosion and sediment control, hazardous material disposal, stormwater management, steep slope and erosive soil mitigation, environmentally sensitive

## Attachment B - Staff Analysis

construction materials and techniques, roadway alignments, and the protection of the Powells Creek transition area.

- Marine Clay Soil Mitigation - The IEAMP submitted includes mitigation of marine clay soil areas with construction monitoring of backfill operations, roof and foundation drainage, excavations, structural infill, etc. Proffers include monthly reports by an employee of the Department of Public Works to verify the effectiveness of the measures taken. This employee will be funded by the applicant.
- Wetlands Protection – SUP conditions ensure the protection of the wetlands and intermittent streams located in the northern section of the town center site with a 100-foot conservation easement buffer from the wetlands or watercourse. If disturbance of wetlands in the southern portion of the site occurs, mitigation will be provided within the same sub-basin.
- Geotechnical Studies - At the time of site plan application, the applicant will provide a comprehensive geotechnical study. This study will specifically address structural foundation stability for marine clay soils such that potential impacts, including those to structures, can and will be mitigated.
- Contingency Plan - SUP conditions provide for a contingency plan to handle clean up and disposal of hazardous substances and other emergency situations.
- Environmentally Sensitive Materials – Three months prior to site plan submission, SUP Conditions require the applicant to illustrate the use of environmentally sensitive construction materials and techniques which may reduce environmental impacts of the development. Through the course of development, innovative erosion control techniques suggested by the Department of Public Works will be incorporated into the design of the site development plans.
- Water Quality – The proffers and conditions include a monetary contribution in the sum of \$75.00 per acre to the Board of County Supervisors for the purpose of monitoring water quality, stream restoration projects, and/or drainage improvements.

### Proposal's Weaknesses

- Tree Preservation – As previously requested and approved, stands of both hardwood and pine forests will be eliminated due to the development.
- Natural Character - As previously requested and approved, the site will be significantly graded to create an urban condition. The natural character of the site, including most notably steep slopes, will be greatly altered.
- Soil Types - The town center site contains highly erodible and highly permeable soils not conducive to development. The combination of highly erosive soils and steeply sloping terrain as well as the potential presence of marine clay soils are a major concern with the development of this site due to the high incidence of erosion and possible instability. Steep slopes and marine clay soils will require significant mitigation efforts.

## **Attachment B - Staff Analysis**

- **Impervious Surfaces** - As previously requested and approved, the town center area will contain a much higher ratio of impervious area due to the urban landscape. Runoff from these areas may have an impact on Powell's Creek and possibly the Potomac River as well as other tributaries and wetlands. SUP Conditions require the Applicant to provide a monitoring program to help insure that the post-development peak flows do not exceed those currently present.

**On balance**, this application is found to be consistent with the relevant components of the Environment Plan.

### **Fire and Rescue Plan Analysis**

Quality fire and rescue services provide a measure of security and safety that both residents and businesses have come to expect from the County. The Fire and Rescue Plan sets out policies and action strategies that further the County's goal of protecting lives, property, and the environment through timely, professional, humanitarian services essential to the health, safety, and well-being of the community. The plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility for fire and rescue facilities. The plan also includes recommendations to supplement response time and reduce risk of injury or death to County residents, establishment of educational programs, such as cardio-pulmonary resuscitation (CPR) training, automatic external defibrillators (AED), and encourages installation of additional fire protection systems—such as sprinklers, smoke detectors, and other architectural modifications.

The site is closest to the River Oaks Fire and Rescue Station (#23). It is estimated that there are approximately 3,208 responses per tactical unit at this station, which is beyond the recommended standard of 2,000 responses per unit. It is estimated that the response time for the site would be within the recommended 4.0-minute response time for fire suppression and basic life support and within the recommended 8.0-minute response time for advanced life support.

The Fire and Rescue Association is currently reviewing the number of tactical units per station, and the numbers reported here are subject to revision.

### **Proposal's Strengths**

- **Level of Service** - The applicant has proffered to provide monetary contributions for the proposed development in accordance with the policy guidelines established at the time the property was rezoned and the proffers were amended.
- **Response Times** - The site is within the recommended 4.0-minute response time for fire suppression and basic life support and within the 8.0 minute response time for advanced life support when the road network is constructed.

## **Attachment B - Staff Analysis**

- Building Bulk Standards and Mitigation – The applicant continues to propose reduced yard setbacks for dwellings and has proffered to provide mitigation as specified in the Zoning Ordinance. (Proffer #46).

### **Proposal's Weaknesses**

- Station Workload - The responses per tactical unit from the closest station are over the recommended standard.
- Building Bulk Standards and Mitigation – The applicant continues to propose building height increases within Landbays 8 and 9 (town center) but in the current proposal has not proffered mitigation. The previously approved proffers include mitigation in the form of larger elevators in taller buildings such that gurneys could be accommodated. A condition has been added to address this impact in Landbay 9.

**On balance**, this application is found to be consistent with the relevant components of the Fire and Rescue Plan.

### **Housing Plan Analysis**

Prince William County is committed to clean, safe and attractive neighborhoods for all its residents, and the elimination of neighborhood blight and substandard housing. The Housing Plan sets out policies and action strategies that further the County's goal of identifying locations and criteria for the provision of diverse housing opportunities for all segments of our population and to promote economic development. The plan includes recommendations relating to neighborhood preservation and improvement, affordable housing, special needs housing, and public/private partnerships to address housing needs.

This plan encourages provision of affordable housing units or the support of the housing trust fund by REZ applicants. A monetary contribution based on the previously approved amount is recommended toward the Housing Preservation and Development Fund. The applicant has continued that commitment.

### **Proposal's Strengths**

- Level of Service - The applicant is proffering the previously approved per unit proffer amount as escalated for inflation of \$265.68 per unit for all units over 2,500 dwelling units for the Housing Preservation and Development Fund.. The applicant has also proffered to contribute \$92,160 for the first 2,500 dwelling units built. The housing within the town center will be covered under the associated proffers.

## Attachment B - Staff Analysis

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Housing Plan.

### **Library Plan Analysis**

Access to a variety of information is a valuable service provided by the County. The Library Plan sets out policies and action strategies that further the County's goal of providing adequate library facilities and information resources to our residents. The plan includes recommendations relating to siting criteria, appropriate levels of service, and land use compatibility.

The nearest library to the proposed project area is Dumfries Neighborhood Library located at 18007 Jefferson Davis Highway, Dumfries. It is estimated that the proposed development will generate a need for additional library space and volumes of books and periodicals. The resultant capital cost expenditure needed to meet LOS for libraries based on the previously approved proffers and subject to an inflation escalator is approximately \$1,208,408.34 ( $\$255.94 \times 1,650 \text{ sfd} + 179.16 \times 850 \text{ sfa} + \$600.14 \times 139 \text{ sfa} + \$408.31 \times 1,348 \text{ mf}$ ) for the entire Potomac Shores development including the town center. The applicant has proffered to dedicate a one acre site in the town center plus bear the associated cost of site preparation, which has been deemed to be consistent with the policy guidelines.

### **Proposal's Strengths**

- **Level of Service** - The applicant has proffered to dedicate one acre of land within the town center and bear the associated cost of site preparation for a future library which is deemed to be consistent with the policy guidelines. Parking has also been proffered.
- **Monetary Contribution** – The applicant has proffered to contribute \$100,000 for the purchase of library equipment.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Library Plan.

**Attachment B - Staff Analysis**  
**Parks, Open Space and Trails Plan Analysis**

The quality of life for residents of Prince William County is linked closely to the development and management of a well-maintained system of parks, trails, and open space. Prince William County contains a diversity of park, open space, and trail resources. These parklands, open spaces, and recreational facilities play a key role in shaping both the landscape and the quality of life of Prince William County residents through the conservation of natural and cultural resources, protection of environmental quality, and provision of recreational facilities. The Parks, Open Space and Trails Plan sets out policies and action strategies that further the County’s goal of providing park lands and recreational facilities of a quantity, variety, and quality appropriate to meet the needs of the current and future residents of Prince William County. The plan includes recommendations to preserve existing protected open space, maintain high quality open space, expand the amount of protected open space within the County, and to plan and implement a comprehensive countywide network of trails.

The proposed residential development is near the following area parks and trails:

Type	Name
Neighborhood Park	Lacey Compton Park
Community Park	Neabsco Eagles Park & Ann Moncure Wall Park
Regional Park	Locust Shade Park & Veterans Memorial Park
Linear/Resource	Powells Landing
Trails	Potomac Heritage National Scenic Trail

Development of the entire Potomac Shores site as proposed will generate a minimum need for an additional 678.09 acres of parks accessible to the general public (70 acres/1,000 population), including 145.03 acres of County-owned parkland (15 acres/1,000 population), and capital cost expenditures of approximately \$6,090,265.46 based on the LOS contributions when this development was originally approved subject to an inflation escalator. The applicant has proffered to provide on-site recreational amenities for the residents of this development including a clubhouse, two swimming pools, and a tot lot. The applicant has also proffered to dedicate a total of approximately 248 acres of parks and open space to the Board of County Supervisors of which 193 acres would be specifically for open space/ passive recreation to offset the current LOS standards for parks, open space and trails. This park area, with the exception of the community recreation center, is outside the town center. Within the town center the applicant has identified several open space/plaza areas which are, a minimum of 0.75 acre in Block 2, minimum of 1.5 acres in Block 3 and minimum of 5.5 acres in Block 11.

## Attachment B - Staff Analysis

### **Proposal's Strengths**

- **Level of Service** – Within the entire Potomac Shores development, the applicant has proffered to dedicate 55 acres of land for active recreation purposes, grade the site and build nine (9) rectangular multi-purpose fields, a soft ball field, two (2) Little League fields, a comfort station and a tot lot. Two of the multi-purpose fields will be constructed with artificial turf and lit. The fields will be delivered on an accelerated schedule from the previously approved proffers. The needed parking accommodations will be addressed as the facilities are constructed. This is in lieu of a monetary contribution.
- **On-Site Active Recreation** – The applicant has proffered to construct a recreation center in Landbay 9, which is within the town center. The facility will include a minimum 10,000 square foot clubhouse with exercise/fitness and multi-purpose rooms, two (2) swimming pools, a tot lot and four (4) tennis courts.
- **Passive Recreation** – Within the entire Potomac Shores development, the applicant has proffered to dedicate 113 acres of land on the south side of Cockpit Point Road and 80 acres of land on the south side of Powells Creek for passive recreation purposes. The land adjacent to Powells Creek will contain a section of the Potomac Heritage National Scenic Trail. Additionally the applicant has agreed to establish a minimum of three open space/plaza areas distributed around the town center.
- **On-Site Trails** – Within the entire Potomac Shores development, the applicant has proffered to provide a minimum of 10 miles of on-site trails including paved and natural surface facilities. The applicant shall provide natural surface trail through the 80 acre site on the south side of Powells Creek. A critical piece of the Potomac Heritage National Scenic Trail will run along the east side of the town center. Such siting of the trail will afford panoramic views of the river within the town center.

### **Proposal's Weaknesses**

- **Trail Connection** – The applicant has not provided a direct trail connection from the 80 acres along Powells Creek to the town center. This will be important to maintain continuity along the Potomac Heritage National Scenic Trail until such time as a trail section is available off-site closer to the Potomac River and to help provide convenient access from the town center to the 80 acre site along Powells Creek.

**On balance**, this application is found to be consistent with the relevant components of the Parks, Open Space and Trails Plan.

## **Attachment B - Staff Analysis**

### **Police Plan Analysis**

Residents and businesses expect a high level of police service for their community. This service increases the sense of safety and protects community investments. The Police Plan is designed to promote Prince William County's public safety strategic goal to continue to be a safe community, reduce criminal activity, and prevent personal injury and loss of life and property, as well as to ensure effective and timely responses throughout the County. This plan encourages funding and locating future police facilities to maximize public accessibility and police visibility, as well as to permit effective, timely response to citizen needs and concerns. The plan recommends educational initiatives, such as Neighborhood and Business Watch and Crime Prevention through Environmental Design (CPTED), which encourages new development to be designed in a way that enhances crime prevention. The plan also encourages effective and reliable public safety communications linking emergency responders in the field with the Public Safety Communications Center.

### **Proposal's Strengths**

- **Field Office Space**– The applicant has proffered to provide 1,500 square feet of office space free of charge to accommodate a police field office. Parking has also been proffered.
- **Safety and Security Measures** - The Police Department has recommended the safety and security measures that should be incorporated into the design of the site.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Police Plan.

### **Potable Water Plan Analysis**

A safe, dependable drinking water source is a reasonable expectation of County residents and businesses. The Potable Water Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound drinking water system. The plan includes recommendations relating to system expansion, required connections to public water in the development area, and the use of private wells or public water in the Rural Area.

Public water is available to the subject site but requires the construction of off-site facilities. The property is within the Prince William County Service Authority service area for water. To facilitate service within Potomac Shores and the town center, a minimum 12-inch diameter water main shall be extended along Cockpit Point Road through the subject development site to close a loop. The loop closure is required for reliability of service to the proposed site and will be provided at the cost of the applicant.



## **Attachment B - Staff Analysis**

Additional water main construction may be required in order to provide required fire protection hydrant coverage and fire flows.

### **Proposal's Strengths**

- **Water Connection** - The applicant is required to comply with Zoning Ordinance Section 32-250.74, which mandates connection of the site to public water service. The associated proffers require the applicant to design and construct all on-site and off-site water utility improvements necessary to develop the subject use.
- **Water Tank Site** – To serve the entire development including the town center, the applicant has maintained a proffer dedicating 1.5 acres of land as shown on the Infrastructure/Utility Plan for the siting of a water tank to serve this area. This is sited outside the town center in Landbay 2.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Potable Water Plan.

## **Sanitary Sewer Plan Analysis**

Appropriate wastewater and sanitary facilities provide needed public health and environmental protections. The Sewer Plan sets out policies and action strategies that further the County's goal of providing an economically and environmentally sound sanitary and stormwater sewer system. The plan includes recommendations relating to system expansion, required connections to public sewer in the development area, and the use of either private or public sewer systems in locations classified as Semi-Rural Residential (SRR), as well as the Rural Area.

Public sewer is available to the subject site but requires the construction of off-site facilities. The property will be within the Prince William County Service Authority service area for sewer service. Sanitary sewer easements necessary to serve the proposed development shall be provided through the overall site. These easements shall be addressed as part of the request and the associated rezoning shall constitute the Public Facility Review for that sewer main. It should also be noted that the proposed redistribution of development including the enlargement of the town center may impact the sewage pumping stations proposed for the peninsula development. Design for these facilities must be evaluated to ensure adequacy. The impact of the pumpovers must also be examined to prevent overloading the existing gravity sewers and downstream facilities.

## Attachment B - Staff Analysis

### Proposal's Strengths

- Sewer Connection - The applicant is required to comply with Zoning Ordinance Section 32-250.75, which mandates connection of the site to public sewer service. The associated proffers require the applicant to design and construct all on-site and off-site sewer utility improvements necessary to develop the subject use.
- Infrastructure Plan – The infrastructure/utility plan is proffered.

### Proposal's Weaknesses

- None identified.

On balance, this application is found to be consistent with the relevant components of the Sanitary Sewer Plan.

### Schools Plan Analysis

A high-quality education system serves not only the students and their families, but the entire community by attracting employers who value educational opportunities for their employees. The Schools Plan sets out policies and action strategies that further the County's goal of providing quality public education to our school-aged population. The plan includes recommendations relating to facility size and location, siting criteria, compatible uses, and community use of school facilities.

The Schools Plan establishes level of service (LOS) standards for schools countywide, based on available or projected program capabilities. The Schools Plan recommends that proposed developments offset impacts on County schools by providing land for future schools and/or monetary contributions for construction of schools.

The proposed residential development is near the following schools:

School	Capacity	Enrollment
Swan's Creek Elementary School	770 (85.6%)	659
Potomac Middle School	1,233 (88.6%)	1,093
Potomac High School	1655 (96.6%)	1,598

Based on figures generated on June, 2012.

Student generation for the entire Potomac Shores site including the town center is 893-1,196 students for elementary school, 386 - 614 students for middle school, and 493 - 821 students for high school for a total range of 1,772 – 2,631 students.

## Attachment B - Staff Analysis

The resultant capital cost expenditure needed to meet current LOS for schools is \$17,257,498.16 ( $\$6,641.73 \times 1,650 \text{ sfd} + \$1,951.57 \times 850 \text{ sfa} + \$9,039.60 \times 139 \text{ sfa} + \$2,509.87 \times 1,348 \text{ mf}$ ) for the entire Potomac Shores development including the town center. The applicant has proffered two school sites and site preparation in lieu of a monetary contribution which has been deemed to be in accordance with the Policy Guidelines. The school sites are outside the town center in Landbay 2

The following projects will likely provide relief for school capacity in this part of the County.

- A 7-classroom addition is planned at River Oaks Elementary School, for completion September 2014.
- An 11-classroom addition is under construction at Potomac Middle School, for completion September 2013.
- A 9-classroom addition is planned at Rippon Middle School, for completion September 2016.
- A 30-classroom addition is under construction at Potomac High School, for completion September 2013.
- An elementary school is planned in the Potomac Shores area to open September 2017.
- A middle school is planned in the Potomac Shores area to open September 2020.

### **Proposal's Strengths**

- Level of Service - The applicant has proffered to dedicate 21 acres for an elementary school and 54 acres for a middle school. The applicant has also proffered to grade the sites when requested by the School Board in preparation for impending school development. The school board has determined this to be sufficient mitigation in lieu of a monetary contribution because one of the previously approved elementary school sites is being changed to a middle school site. This increased the overall acreage from 37 acres to 75 acres. The contribution is deemed to be consistent despite eliminating age restrictions on about 400 dwelling units.
- County-Wide School Capacity - County-wide, the various categories of schools are operating within overall enrollment capacities.
- Individual School Capacity - The school capacities for this area are currently within capacity. New schools and expanded schools, which are being built in this area, will continue to provide relief.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Schools Plan.

## Attachment B - Staff Analysis

### Transportation Plan Analysis

By providing a multi-modal approach to traffic circulation Prince William County promotes the safe and efficient movement of goods and people throughout the County and surrounding jurisdictions. The Transportation Plan sets out policies and action strategies that further the County's goal of providing a complete, safe and efficient multi-modal circulation system that includes sufficient capacity to meet the demands placed upon it. The plan includes recommendations relating to improving existing service levels, increasing capacity of the existing system, minimizing negative impacts to environmental and cultural resources, supporting targeted industries and major activity centers, exploring innovative funding mechanisms, promoting regional approaches to transportation issues, and promoting transit opportunities. At a minimum, projects should include strategies that result in a level of service (LOS) of "D" or better on all roadways, the dedication of planned rights-of-way, the provision of pedestrian pathways, and access to mass transit and other commuter facilities as appropriate.

Potomac Shores Parkway (which was previously named Harbor Station Parkway and Potomac Parkway) will provide the most direct access to the property including the town center from Route 1, directly across from Dumfries Road. Access to the site is also provided by River Heritage Boulevard, as well as River Ridge Boulevard and Wayside Drive to the north. The Applicant also proposes access through Potomac River Drive on the northern portion of the site. During the previous rezoning applications, traffic impact analyses (TIA) were provided based on the large amounts of traffic generated by the site. Because the Applicant is proposing changes to roadway layouts, intersection access at the main entrance to the site (Potomac Shores Parkway/Route 1), and because the applicant is changing the location (and in some instances the sizes/number) of uses, a revised TIA was required by the Design and Construction Standards Manual (DCSM). The applicant provided one TIA for the internal roadway network which included the town center and one TIA for the roadway network external to the site. Both TIAs were completed by Gorove/Slade and dated March 30, 2012. Updated TIAs dated January 8, 2013 and March 12, 2013 were provided by Gorove/Slade. The External Network TIA was required as the applicant proposes to change the proffers that provided an interchange at the intersection of Dumfries Road/Potomac Shores Parkway and Route 1 to provide a two quadrant intersection instead.

Several transportation modifications have been requested by the applicant and reviewed and approved by the County Department of Transportation. They are located within the conditions in Attachment C of the staff report.

### Proposal's Strengths

- Trip Reduction - The town center concept proposes that trips will be reduced and that local residents will walk to facilities rather than drive.
- Walkability - Employment and retail uses within the town center are designed to be provided within walking distance to all residents.

## **Attachment B - Staff Analysis**

- Transit-oriented Development - The town center design promotes the concept of transit-oriented development with higher density development located in close proximity to public transit. The proposed VRE rail facilities will be pedestrian accessible.
- LOS Improvements – The applicant has proffered to build a VRE Commuter Rail Station, associated parking, and access improvements for the station to Route 1. The cost of these community serving improvements exceed the inflation adjusted monetary contribution for the development.
- Transportation Demand Management (TDM) – The applicant has proffered to adhere to the updated transportation demand management program. Conditions have been drafted as well.
- Phasing Plan – The applicant has proffered to a road phasing plan within the town center. This will facilitate the development of the town center generally to be staged going north to south.
- Triggers for Transportation Improvements – The applicant has proffered with the associated rezoning triggers for the construction of Potomac Shores Parkway to US Route 1 and the US Route 1/Route 234 modified quadrant intersection that meet with the approval of the Transportation Department.
- Bus Shelters – The applicant has proffered to provide several bus shelters throughout the development when bus service becomes available to the site. A condition has also been drafted to address the provision of bus shelter facilities within the town center.
- Pedestrian Connections – The applicant has shown and proffered to a comprehensive network of sidewalks and trails throughout the development. A condition has also been drafted which specifies that pedestrian connections shall be provided through Block 3 and 4 to ensure the ease of pedestrian movement in the town center.
- Bike Storage – A condition has been drafted to address the provision of bike racks and lockers in connection with the provision of VRE parking.

### **Proposal's Weaknesses**

- None identified.

**On balance**, this application is found to be consistent with the relevant components of the Transportation Plan.

## **Attachment B - Staff Analysis**

### **Materially Relevant Issues**

This section of the report is intended to identify issues raised during the review of the proposal that are not directly related to the policies, goals, or action strategies of the Comprehensive Plan, but that are materially relevant to the County's responsibilities in considering land use issues. The materially relevant issues in this case are as follows:

- None identified.

### **Minimum Design Criteria**

Staff and other agencies that have reviewed the proposal noted the following minimum design criteria. The development proposal will be reviewed for compliance with all minimum standards at the time that the applicant submits detailed site development information prior to the issuance of construction permits. The listing of these issues is provided to ensure that these concerns are a part of the development record. Such issues are more appropriately addressed during the site plan review.

1. In order to ensure that all measures are taken to reduce the opportunity for crime to occur, Crime Prevention Through Environmental Design (CPTED) techniques should be used when designing the Town Center including provisions for appropriate lighting levels, common observation points, and the location and design of recreational and parking facilities.
2. The Service Authority will require appropriately sized easements for water and sewer utilities in accordance with the PWCSA Utility Standards Manual. These easements are not to be overlain with electric, phone and television cables so as to insure the Authority's ability to maintain the water and sewer mains in an unhindered manner.
3. Adequate access to water and sewer mains in the rights of way of streets must be provided.
4. Corrosion protection may be required for water and sewer mains.
5. Landscaping over water and sewer easements must comply with PWSCA Utility Standards Manual requirements and limitations.
6. Fencing and walls will not be permitted across or on PWCSA water and sewer easements.

## Attachment B - Staff Analysis

### Agency Comments

The following agencies have reviewed the proposal and their comments have been summarized in relevant comprehensive plan chapters of this report. Individual comments are in the case file in the Office of Planning:

Planning Office, Case Manager  
DPW- Environmental Services  
Service Authority  
Library  
Schools  
Police  
Department of Transportation  
Department of Parks and Recreation  
VDOT  
Zoning  
Proffer Administrator  
Housing  
VRE  
PRTC

**Attachment C**  
**Conditions dated June 7, 2013**

**PROPOSED CONDITIONS**

Owner/Applicant: Harbor Station Communities LLC

Special Use Permit: SUP #PLN2012-00434

Prince William County GPINs: 8389-14-1892 pt., -18-2218 pt., -41-3404 pt., -42-6195 pt., -9772,-4267 pt., -43-6730, -7941, -8622, -9843, -52-4387, -7685, -8065, -1394, -1849pt., -2672, -4232 pt., -5473, -1680, -53-2532, -4114, -4443, -5711, -7408, -0224, -6042, -54-0695, -60-1576 pt., -62-0140, -0162 -0327 pt., -1254, -5228, -5363pt., -5845 pt., -3777, -5354, -3361, -1962, -63-1137 pt., -4442

Special Use Permit Area: +/-228.5 acres

Zoning: PMD, Planned Mixed District (subject to associated rezoning)

Magisterial District: Potomac

Date: June 7, 2013

**CONDITIONS**

**Special Use Permit #PLN2000-00079, Southbridge Town Center**  
**January 9, 2004**

The following conditions, to be met by the Applicant and its successors and assigns, supersede the conditions associated with the special use permit #PLN200-00079 and are intended to offset the impacts of the proposal and to render the application consistent with the applicable chapters of the 1998-2008 Comprehensive Plan and the ~~Cherry Hill Sector~~ Potomac Communities Revitalization Plan:

If the conditions of this special use permit or the special use permit plan (the "SUP plan") are in conflict with the Zoning Ordinance, and/or the Design and Construction Standards Manual (DCSM), the more restrictive standards shall apply, except as specifically allowed by this special use permit.

Implementation – The Applicant shall file a site/subdivision or public improvement plan for a use or road/infrastructure on the property within the town center within two (2) years of final approval of this special use permit by the Board of County Supervisors<sup>1</sup> and shall construct said improvement or occupy said use within five (5) years of the date of the final approval of such plan. Construction/occupancy of such improvement/use within said five year period shall vest the Applicant's right to thereafter construct and use/occupy all other improvements/uses permitted in the town center in any sequence, provided it is in accordance with the phasing schedule set forth herein.

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Site Development

Plans and studies – The property shall be developed in substantial conformance as determined by the Prince William County Planning Office ("Planning Office") with the following plans and studies:

1. Plans and studies - The Property shall be developed in substantial conformance with the plans listed below. Minor modifications shall be permitted at the time of site/subdivision plan. Should



**Attachment C**  
**Conditions dated June 7, 2013**

review and comment by any County, State or Federal agency require more substantial modifications, such changes shall be coordinated with the Planning Director, or his designee.

- a. Potomac Shores Town Center Special Use Permit Plan - Schematic Land Use Plan/Development Code, sheet 2 of 5, dated May 2012, last revised May 30, 2013, prepared by Urban Engineering Ltd. ("Site Development/Schematic Land Use Plan")
- b. Potomac Shores Town Center Special Use Permit Plan - Street Grid, Infrastructure & Pedestrian Circulation Plan, sheet 3 of 5, dated May 2012, last revised May 30, 2013, prepared by Urban Engineering Ltd. ("Street Grid, Infrastructure & Pedestrian Connections Plan")
- c. Potomac Shores Town Center Special Use Permit Plan – Streetscape/Community Facilities Plan, sheet 4 of 5, dated May 2012, last revised May 30, 2013, prepared by Urban Engineering Ltd. ("Streetscape & Community Facilities Plan")
- d. Potomac Shores Town Center Special User Permit - Infrastructure Phasing Plan, sheet 5 of 5, dated May 2012, last revised May 30, 2013, prepared by Urban Engineering, Ltd. ("Infrastructure Phasing Plan").
- e. Potomac Shores Transportation Demand Management (TDM) Guidelines prepared by Gorove/Slade and dated April 25, 2013 ("TDM Guidelines").
- f. Potomac Shores Town Center Parking Assessment, prepared by Gorove Slade Associates, Inc. and dated June 11, 2012. ("Parking Assessment").
- g. Traffic Impact Study Potomac Shores Internal Network, prepared by Gorove/Slade and dated March 12, 2013 ("Internal TIA").

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- a. ~~Special Use Permit Plat, dated January 20, 2000. Prepared by Dewberry & Davis. ("SUP Plat")~~
- b. ~~Town Center Special Use Permit Slopes and Resource Protection Areas, dated January 20, 2000 revised through October 12, 2000, prepared by LDR International, Inc. ("Town Center SUP Slopes and RPAs")~~
- e. ~~Town Center Special Use Permit Site Development / Schematic Land Use Plan, dated January 20, 2000 revised through October 12, 2000, prepared by LDR International, Inc. ("Town Center SUP Site Development / Schematic Land Use Plan")~~
- d. ~~Town Center Special Use Permit Street Grid and Infrastructure, dated January 20, 2000 revised through October 12, 2000, prepared by LDR International, Inc. ("Town Center SUP Street Grid and Infrastructure")~~
- e. ~~Town Center Special Use Permit Streetscape and Landscape / Community Facilities, dated January 20, 2000 revised through October 12, 2000, prepared by~~

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**Attachment C**  
**Conditions dated June 7, 2013**

LDR International, Inc. (“Town Center SUP Streetscape and Landscape/  
Community Facilities”)

- f. ~~Town Center Special Use Permit Phasing Plan~~, dated January 20, 2000 revised through October 12, 2000, prepared by LDR International, Inc. (“Town Center SUP Phasing Plan”)
- g. ~~Traffic Impact Assessment and Transportation Plan for Southbridge Town Center~~ prepared by Patton, Harris and Rust, PC, dated December 1999. (“Town Center Traffic Impact Assessment and Transportation Plan”)
- h. ~~Recommended Minimum Off Street Parking Supply Ratios for the Southbridge Town Center of the Parking Impact Analysis for Southbridge Town Center~~, prepared by Patton Harris Rust & Associates, PC, dated December 1999. (“Parking Impact Analysis”)

2. ~~Design Guidelines and Development Code - The Property shall be developed and used in substantial conformance with the "Potomac Shores Town Center Design Guidelines", prepared by Hart/Howerton and dated January 15<sup>th</sup>, 2013, revised April 26<sup>th</sup>, 2013 ("Town Center Design Guidelines"). These Design Guidelines may be modified in accordance with the provisions set forth in the Town Center Design Guidelines. A Design Review Committee ("DRC") will be established to implement the Design Guidelines and said committee shall consist of a minimum of three members which shall include a representative of the owner, and/or an architect, and/or a landscape architect and/or an engineer. The DRC shall be responsible for the review and approval of all plans for development on the Property to ensure that such plans are in conformance with the design concepts and details set forth in the Design Guidelines. Minor modifications to the Design Guidelines may be made at the time of final site/subdivision plan provided a copy of the modification is submitted to the County prior for record-keeping and possible comment if identified as more substantial. More substantial modifications to the Design Guidelines may be approved by the DRC provided the overall concept and intent of the Design Guidelines is met. Final DRC approval of such substantial modifications is subject to approval of the Planning Office as follows: Prior to final approval by the DRC, each amendment, revision or waiver to the Design Guidelines shall be forwarded to the Prince William County Planning Director, or his designee, who shall review and determine consistency. Approval of requested changes which are consistent shall not be unreasonably withheld.~~

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2. ~~Design Guidelines and Development Code - The property shall be developed and used in substantial conformance as determined by the Planning Office with the Southbridge Town Center Design Guidelines and Development Code, prepared by LDR International, Inc., dated January 20, 2000, revised through August 28, 2000. (“Town Center Design Guidelines”) These Design Guidelines may be modified in accordance with the provisions set forth in said guidelines and with approval from the Planning Office.~~

3. ~~Signage - Signage in the Town Center shall be as provided for in the Town Center Design Guidelines. The signage standards contained in these Design Guidelines are mandatory minimum regulations which can only be waived with the approval of the Planning Office.~~

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4.

3. Waivers and Modifications - The Town Center may be developed utilizing waivers and modifications of the standards set forth in the Zoning Ordinance, Design and Construction Standards Manual and other applicable County regulations and ordinances and as set forth in the Potomac Shores Town Center Waivers and Modifications ("Town Center Waivers and Modifications"), attached hereto and incorporated herein by reference, the Town Center Design Guidelines (including the signage modifications contained therein pursuant to Section 250.23 of the Zoning Ordinance) and/or additional waivers and modifications as may be approved by the County. All applicable waivers/modifications shall be included on any site/subdivision plan submittals.

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~~Waivers and Modifications - The Town Center may be developed utilizing waivers and modifications of the standards set forth in the Zoning Ordinance, Design and Construction Standards Manual and other applicable County regulations and ordinances as set forth in the Southbridge Town Center Waivers and Modifications ("Town Center Waivers and Modifications"), attached hereto and incorporated herein by reference. All waivers and modifications shall be subject to the accompanying justification text. All waivers / modifications shall be included on any site / subdivision plan submittals.~~

4.4 Gates for refuse storage areas shall remain closed when refuse containers are not being accessed.

Phasing & Block Development

65.

There shall be no freestanding retail uses in Blocks 7 and 8. The foregoing shall not preclude secondary retail as otherwise permitted in the O(H) district.

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6. The infrastructure for the Town Center (roads, water and sewer) shall be developed in the order shown on the Infrastructure Phasing Plan and in accordance with the phasing schedule set forth below. With the exception of Potomac Shores Parkway and Dunnington Place, all roads in the Town Center shown on the Street Grid, Infrastructure & Pedestrian Connections Plan shall be constructed to their ultimate design standard, in accordance with the approved modified street sections, at the time of their initial construction, or as otherwise modified/approved by Prince William County Transportation. The Applicant may modify the phasing schedule as shown on the Infrastructure Phasing Plan (i.e., construct an infrastructure phase or phases prior to the next scheduled phase) if needed to accommodate employment, retail, commercial or other non-residential uses in the Town Center.

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INFRASTRUCTURE PHASING SCHEDULE

Phase	Residential Building Permit Within Potomac Shores
1	1,000
2	1,500
3	2,000
4	2,500
5	3,000

For purposes of implementing this phasing schedule, the improvements required in each phase shall be plan approved and bonded prior to the issuance of the first building permit for a residential unit located within the Potomac Shores project (i.e., the property subject to REZ #PLN2012-00420) identified for that phase. Once the improvements are approved, the Applicant shall commence construction of said improvements and thereafter diligently work to complete construction.

The Town Center shall be developed in substantial conformance as determined by the Planning Office with the phasing schedule and parameters contained in Town Center SUP Phasing Plan. The following conditions apply to phasing and block development (streets, unless otherwise noted, are identified as shown in the Town Center Traffic Impact Assessment and Transportation Plan):

Mixed Use Development. Town Center Blocks D,E,F, and G shall be developed so that each block face contains a mixture of residential and nonresidential uses.

Ground floor Uses—Except for the two multifamily residential buildings at the corner of Street B and Potomac Parkway, buildings fronting on Streets B, I, and J of Blocks D, E, F, and G shall incorporate at least fifty percent (50%) of the ground floor gross floor area (gfa) as nonresidential uses. These ground floor areas however, may contain lobbies and other access areas intended to serve residential uses above the ground floor. For these types of mixed use buildings, ground floor entries and / or lobbies serving upper level residential shall not exceed 20% of the ground floor gross area (gfa).

Mixed Use Buildings—With the approval of the Planning Office, additional mixed use buildings and blocks within the Town Center may be developed in addition to those provided for on the Town Center SUP Site Development / Schematic Land Use Plan.

Phase 1 as shown on the Town Center SUP Phasing Plan must be constructed or under construction at the time the VRE Station opens.

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~~No more than 500 residential units within the Town Center can be constructed prior to the completion of a minimum of 300,000 gsf of nonresidential development including 100,000 gsf of office and employment uses.~~

~~No more than 1,000 residential units within the Town Center can be constructed prior to completion of a minimum of 600,000 gsf of nonresidential development including 300,000 gsf of office and employment uses.~~

7. ~~Notwithstanding the phasing schedule set forth in Condition #6 above, the following shall apply:~~

a. ~~Phase 1, Phase 2 and Phase 3 infrastructure improvements (as shown on the Infrastructure Phasing Plan) shall be constructed and open for use, subject to reasonable traffic control measures, prior to the time the Virginia Railway Express (VRE) commuter rail station at the Property opens for use; provided, however, Phase 3 infrastructure improvements may include a half section of the ultimate Potomac Shores Parkway until such time that traffic dictates a full section. The ultimate access to the VRE commuter parking facilities shall be provided via a public street (Road #3) as shown on the Street Grid, Infrastructure & Pedestrian Connections Plan. Notwithstanding the above, should the County, VDOT and other affected parties agree in the future that Road #3 can be a private street, then the Applicant may build Road #3 as a private road without the need for an amendment to this SUP.~~

b. ~~At the time a final site/subdivision plan is approved that generates the earlier of the following amounts of traffic on the section of River Heritage Boulevard immediately north of its intersection with Potomac Shores Parkway: (i) more than 25,000 total vehicles per day cumulatively, or (ii) more than 3,500 total vehicles (two-way traffic) cumulatively during the AM or PM peak hour, the Applicant shall obtain plan approval, post a bond and execute a performance agreement for the construction of either River Heritage Road or that portion of Road #13 (identified for construction in Phase 5 on the Infrastructure Phasing Plan) in order to provide a road connection through the Town Center to the southern portion of the Potomac Shores community.~~

8. ~~The Applicant shall file a site plan within two (2) years of the final approval of the Town Center SUP and thereafter pursue its approval in good faith and with due diligence for the golf course clubhouse, to be located in Block 10.~~

79. Boundary Line Adjustment – The boundary line between two contiguous ~~land bays/~~ blocks located within the Town Center may be adjusted provided such adjustment does not increase the size of either ~~land bay/block~~ by more than ten percent (10%) of the area of the larger ~~land bay/~~ block.

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8. ~~Block Face~~— Each block face shall maintain a consistent setback or building line from the street. ~~Bay windows, awnings, canopies, cornices, porches, roof overhangs, signs, recessed entryways or other secondary building elements may vary from this building line; however, the primary building face must remain consistent along each street edge.~~

10. ~~FAR and density tabulations - At the time each site plan is submitted for development in the Town Center, a cumulative total of all land use activities in the Town Center (i.e., residential, non-residential FAR and density tabulations) shall be provided as a part of each such site plan submission.~~

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11. ~~Open Space – Notwithstanding Modification #19 as set forth in the Town Center Waivers and Modifications, a minimum of five percent (5%) open space shall be provided in each Block within the Town Center.~~

Transportation

12. ~~Traffic Operations Study - At the time of issuance of occupancy permits for fifty percent (50%) and of seventy-five percent (75%) of the cumulative residential or nonresidential development, respectively, targeted in the Town Center, the Applicant shall provide traffic operations studies to examine the traffic counts and flows, intersection capacity, queuing analysis and accident history within the Town Center. Transportation Demand Management strategies in addition to operational modifications such as striping, signage, signalization, sight distance and additional lanes, warranted by this study and requested and approved by the Prince William County Department of Transportation ("Department of Transportation"), shall be undertaken by the Applicant. Operational modifications shall be limited to those permitted within available right of way.~~

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13. ~~On-Street Parking - The control and enforcement of on-street parking on all private streets shall be the responsibility of the Transportation Management Association ("TMA"), the applicable homeowners associations and/or business associations created by the Applicant; such control may include, but shall not be limited to, implementation of parking permits, stickers, meters, fines, any combination thereof or other techniques to be established by the TMA and/or associations(s).~~

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14. ~~Temporary Parking - With the approval of the County, any undeveloped Block may be used for parking until developed. All such interim parking shall be subject to site plan approval and such parking shall be paved. No such interim surface parking lot shall be used for longer than seven (7) years from the date of its construction, but such use may be extended by the County.~~

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15. ~~Minimum Required Parking - The minimum off-street parking for the various land uses within the Town Center shall be provided in accordance with the Parking Assessment. The minimum parking spaces provided for commercial uses pursuant to the Parking Assessment will be available for shared use. This shall not preclude designations of these spaces for specific uses at certain times during any twenty-four (24) hour period.~~

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At the time of issuance of an occupancy permit for seventy five percent (75%) of the cumulative residential or nonresidential development targeted in the Town Center, the Applicant shall provide a comprehensive review of parking utilization, operations, and requirements within the Town Center. Any modifications, such as striping, signage, parking space use restrictions and additional parking, warranted by this study and approved by the Prince William County Department of Transportation shall, at the request of the County or VDOT in the case of parking on State-maintained roads, be provided by the Applicant.

16. Parking Impact Analysis-Differing Land Uses - If the land uses developed in the Town Center differ from those analyzed in the Parking Assessment, the Applicant shall provide a revised analysis and additional parking shall be provided as required by said analysis. In addition, any operational modifications, limited to striping, signage, and parking space use restrictions, warranted by this study and approved by the County and VDOT shall be provided by the Applicant at the request of the County or VDOT (in the case of parking on State-maintained roads).

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17. VRE Station Parking - The Applicant shall provide a maximum of 550 parking spaces for the non-exclusive priority use by VRE commuter rail patrons in Block 3, Block 4 and/or Block 5, as said Blocks are identified on the Site Development/Schematic Land Use Plan. Said parking spaces shall be provided no later than at the time the VRE station is open for use. Said parking spaces shall be reserved exclusively for VRE patrons between 5:00 a.m. and 9:00 a.m. Monday through Friday, or as otherwise agreed to by VRE and the Applicant. After 9:00 a.m., the spaces shall be made available to others. Said parking may be provided in surface and/or structured facilities. The permanent location of any such facilities shall be within a ¼ mile (1,320') radius of the entrance to the pedestrian bridge providing access to the elevator/stair tower. Temporary surface parking lots must be located such that a minimum of fifty percent (50%) of the parking spaces located within any such lot shall be within a ¼ mile (1,320') radius of the entrance to the pedestrian bridge providing access to the elevator/stair tower, and the balance of the spaces shall be located adjacent to the spaces located within the ¼ mile radius. The Applicant shall have the right to relocate said parking from time to time as the Property develops so long as the number of parking spaces provided remains available throughout the relocation process. The Applicant shall provide VRE with a minimum of two (2) months advance notice of any such relocation and in no event shall any given parking lot be relocated more than one time during any six (6) month period. All parking provided in surface parking lots shall be paved. The Applicant shall be responsible for the maintenance of all VRE parking provided in surface lots. At the time VRE parking is provided in structured parking lots, said structured parking facilities shall be conveyed to the County, VRE or other public entity with the Applicant reserving the right to construct additional parking spaces within said structure for use, control and ownership by the Applicant or other private entity. In addition to the parking spaces provided as referenced herein above, one (1) additional exclusive designated space shall be provided for VRE employees. All VRE parking shall be provided at no cost to VRE patrons, unless otherwise agreed to by VRE. The Applicant shall be responsible for the control/management of the VRE parking. The specific controls shall be determined in consultation with VRE and the County. Said parking management may be assigned to

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the Transportation Management Association to be established as set forth in Condition #19 or another association established for the community. The Applicant shall provide directional/"pathfinder" signage for both the temporary and permanent VRE parking facilities within the community to direct drivers, bicyclists and pedestrians to the VRE station, as approved by the County, VDOT and VRE.

18. Bicycle Racks/Lockers. The Applicant shall provide bicycle rack(s) to accommodate a minimum of ten (10) bicycles and shall provide a minimum of ten (10) bicycle lockers in connection with the VRE parking. The bicycle racks shall be provided at the time VRE parking is initially provided, whether in surface or structured parking facilities. The bicycle lockers shall be provided at the time the permanent VRE parking is provided in a parking structure. The bicycle racks and bicycle lockers shall be located with the VRE parking spaces or in another location convenient to the VRE station as determined by the Applicant in consultation with the Department of Transportation and VRE. Maintenance of the bicycle lockers and bicycle racks shall be the responsibility of the entity responsible for the maintenance of the VRE parking spaces.

19. Transportation Demand Management Program

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a. The Applicant shall implement a transportation demand management program (the "Program") in connection with the development of the Property. Said Program shall be developed utilizing the strategies set forth in the TDM Guidelines, attached hereto and incorporated herein by reference. Examples of TDM strategies as set forth in the Guidelines to be considered for implementation include, but shall not be limited to, telework and similar alternative work arrangements, bike/walk programs, on-site amenities to reduce vehicle trips, transit benefits program, feeder bus connections to area destinations and the VRE station, flexible work schedules to reduce peak hour vehicle trips, on-site rideshare program, managed parking and a transportation coordinator.

b. At the time the Potomac Shores VRE station is operational or a site plan is approved for the 500,001<sup>st</sup> square foot of office gross floor area ("gfa") on the Property, whichever occurs first, a Transportation Management Association ("TMA") shall be established for the Property, said TMA to be funded by the Applicant or one or more of the property owner associations established as a part of the community, and the Program shall be developed and implemented as appropriate. A copy of the Program shall be submitted to the County prior to the approval of the site plan reflecting the 500,001<sup>st</sup> square foot of office gfa.

c. The TMA shall consist of a minimum of five (5) and a maximum of seven (7) board members. Prince William County, at its option, may appoint one (1) board member.

d. The TMA shall be responsible for monitoring the effectiveness of the TDM program and shall submit reports to the Prince William County Department of



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Transportation every five (5) years. In the event a report shows that the amount of traffic does not achieve the projected traffic reductions for the amount of development, the Program shall be modified in consultation with a qualified traffic engineer and the County in order to achieve the reductions assumed in the TIA. In the event the Program is modified to address the need for additional traffic reduction, the monitoring and reporting program shall also be adjusted as agreed to by the County.

e. It is the intent of this condition that the Program is flexible over time to respond to the evolving transportation related circumstances of the Property, the community and the region as well as to technologies and/or other improvements that may occur, all of which may impact travel behavior and the transportation network. Accordingly, the Program, including the specific parameters identified above, may be modified and amended from time to time, subject to the approval of the Department of Transportation.

20. Bus Shelters – If requested, the Applicant shall construct a minimum of four (4) bus shelters within the Town Center. The location of said shelters shall be determined in consultation with the Potomac and Rappahannock Transportation Commission (PRTC). The Applicant's obligation to construct each bus shelter shall be triggered upon receipt of a written request from the PRTC, or its successor, at the time service is to be provided in the area. The bus shelters located within the Town Center shall be in addition to the obligation set forth in the companion rezoning #PLN2012-00420.

21. Handicapped Parking - Handicapped parking spaces shall be provided in accordance with DCSM and ADA requirements, including number and location of spaces. If requested by the County at the time a site plan is filed for the VRE station facilities, a minimum of three (3) handicapped parking spaces shall be provided proximate to the entrance to the pedestrian bridge providing access to the elevator/stair tower.

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22. Emergency Vehicles - Emergency vehicles must be afforded appropriate access throughout the Town Center, the final location and design of which shall be determined at the time of site plan.

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23. Prohibited On-Street Parking – The covenants on the Property shall prohibit the overnight on-street parking of service vehicles, recreational vehicles, boats, trailers, and heavy trucks within the Town Center. This shall not preclude parking and storage of construction vehicles during development of the Town Center.

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24. Town Center Streets - Streets within the Town Center, which shall be public or private as shown on the Street Grid, Infrastructure & Pedestrian Connections Plan, shall be designed in substantial conformance with the Modified Street Sections as set forth in the Town Center Design Guidelines (Appendix B), the Town Center Waivers and Modifications (as applicable) or the Urban Street Standards that may be incorporated into the DCSM.

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~~25. Pedestrian Connectivity (Blocks 3 and 4) – The Applicant shall incorporate internal pedestrian connections in Blocks 3 and 4 in the area generally bounded by Roads 2, 3 and 6 (as such Roads are shown on the Street Grid, Infrastructure & Pedestrian Plan). Such connections may be provided as a walkway/sidewalk or a sidewalk provided with a street that is designed to a standard set forth in Appendix B of the Town Center Design Guidelines.~~

~~-Transportation~~

~~9. Transportation Impact Analysis – The development shall conform to and not exceed the land use density, lane widths, vpd volumes and trip generation rates, and lane configurations documented in the approved TIA dated December 1999 or the lane widths documented in Town Center Design Guidelines if different from the TIA. The development shall meet the minimum performance requirements outlined in the TIA.~~

~~Prior to development in the Town Center exceeding the residential or nonresidential levels analyzed in the TIA and / or failing to meet Level of Service D or better, the Applicant shall submit to the Department of Public Works, Transportation Division, an amended Traffic Impact Assessment, unless the Department of Public Works determines that the revised TIA is not necessary. This assessment shall reflect an adjustment in the residential or nonresidential development for purposes of confirming the adequacy of the Town Center road network. The Applicant shall be responsible for any improvements required to meet level of service requirements to serve the additional development.~~

~~10. FAR and density tabulations – At the time of site plan submission for each section or phase of the Town Center, a cumulative total of all land use activities in the Town Center (i.e. residential, non-residential FAR and density tabulations) shall be provided to the Planning Office.~~

~~11. Traffic Operations Study – At the time of issuance of occupancy permits for fifty percent (50%) and of seventy five percent (75%) of the cumulative residential or nonresidential development, respectively, targeted in the Town Center, the Applicant shall provide traffic operations studies to examine the traffic counts and flows, intersection capacity, queuing analysis and accident history within the Town Center. Operational modifications such as striping, signage, signalization, and sight distance, warranted by this study and requested and approved by the Department of Public Works, shall be undertaken by the Applicant. This shall not apply to lane widening or construction that requires additional right of way.~~

~~12. On Street Parking – The control and enforcement of on-street parking on all private streets shall be the responsibility of applicable homeowners associations and/or business associations created by the Applicant; such control may include, but shall not be limited to, implementation of parking permits, stickers, meters, fines, any combination thereof or other techniques to be established by the associations(s).~~

~~13. Temporary Parking – With the approval of the Planning Office, any undeveloped land bay may be used for parking until developed. No temporary surface parking lot shall be~~

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~~used on a lot for longer than seven (7) years from the date of its construction, but such use may be extended by the Planning Office.~~

~~14. Maximum Surface Parking—The maximum off-street surface parking within the Town Center shall be as shown on the Town Center SUP Site Development / Schematic Land Use Plan. Notwithstanding the foregoing, the Applicant, with the approval of the Planning Office may provide additional structured parking on areas illustrated for surface parking.~~

~~15. Minimum Required Parking—The minimum off-street parking for the various land uses within the Town Center shall be provided in accordance with Table 14 of the Parking Impact Analysis incorporated herein by reference. The minimum parking spaces provided for commercial uses pursuant to the Parking Impact Analysis will be available for shared use. This shall not preclude designations of these spaces for specific uses at certain times during any twenty-four (24) hour period.~~

~~At the time of issuance of an occupancy permit for seventy-five percent (75%) of the cumulative residential or nonresidential development targeted in the Town Center, the Applicant shall provide a comprehensive review of parking utilization, operations, and requirements within the Town Center. Any operational modifications, limited to striping, signage, and parking space use restrictions, warranted by this study and approved by the Department of Public Works shall, at the request of the County or VDOT in the case of parking on State-maintained roads, be undertaken by the Applicant.~~

~~a. VRE Station Parking—~~

~~250-space parking lot—The Applicant shall construct a minimum 250-space parking facility / lot within Block G of the Town Center for use by VRE patrons. This lot will be constructed and ready for use at the time the VRE station opens. The Applicant or its successors shall retain ownership, and the Town Center Owners Association shall maintain the parking facility / lot.~~

~~300-space parking lot—The Applicant shall construct 300 additional spaces within a Park and Ride / VRE station Commuter Parking Lot at the time demand on an average daily basis reaches 80% of initial parking capacity for an entire month. The lot will be located in a portion of Land Bay SN-1a and/or SN-1b, as close as reasonably possible to the VRE Station.~~

~~In the event these additional spaces are constructed as part of a deck within the Town Center on the site indicated for the 250-space parking lot, the Applicant shall provide as necessary during the course of deck construction. The Applicant shall coordinate phasing and location of the facility / lots with the County and PRTC. The Town Center Owners Association will maintain the commuter parking lot.~~

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~~A portion of these parking spaces may be made available to Town Center residents and visitors during evenings and weekends, however, if VRE schedules change so that VRE parking demand increases during these hours, first priority will be given to accommodating VRE patron use of the parking facilities.~~

~~At no time shall VRE patrons be charged for use of these parking facilities.~~

- ~~16. Parking Impact Analysis Differing Land Uses. If the land uses in the Town Center as developed differ from those analyzed in the Parking Impact Analysis, the Applicant shall provide a revised analysis. Any operational modifications, limited to striping, signage, and parking space use restrictions, warranted by this study and approved by the Department of Public Works shall, at the request of the County or VDOT in the case of parking on State maintained roads, be undertaken by the Applicant.~~
- ~~17. Transportation Demand Management Program. At the time of issuance of an occupancy permit for fifty percent (50%) of the cumulative residential or nonresidential development targeted in the Town Center, the Applicant shall provide a Transportation Demand Management Program ("TDMP") operations study for the Town Center, acceptable to the Department of Public Works. This study must be approved by the Department of Public Works. Any operational modifications excluding the provision or construction of additional or widened lanes or right of way, warranted by this study shall, at the request of the County and / or VDOT, be undertaken by the Applicant.~~
- ~~18. Handicapped Parking. Handicapped parking spaces shall be provided per the ADA requirements, including number and location of spaces.~~
- ~~19. Emergency Vehicles. Emergency vehicles must be afforded appropriate access throughout the Town Center, to every parcel and every structure.~~
20. The covenants on the Property shall prohibit the overnight on-street parking of service vehicles, recreational vehicles, boats, trailers, and heavy trucks within the Town Center. This shall not preclude parking and storage of construction vehicles during development of the Town Center.
21. Drainage easement If required by VDOT at final site plan, a perpetual maintenance / drainage easement shall be provided on public streets with the right of way line coinciding with the back of curb, provided such easement does not change the street grid, street widths, sidewalks, or setbacks.
- ~~22. Town Center Streets. All streets within the Town Center shall be private, except for Potomac Parkway and the street(s) or portions(s) of streets that will provide the ultimate access to the proposed VRE station. Public and private streets shall be designed in substantial accordance as determined by the Planning Office with the standards contained~~

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~~in the Town Center Design Guidelines. The Applicant shall bond the completion of construction of the private streets serving the non-residential portion of the Town Center as determined by Prince William County. Continual public access shall be provided and maintained to the public library and to the police substation. Public access may be by means of private streets with appropriate public access easements.~~

~~23. Access to VRE Station Continual public access to the proposed VRE Station shall be provided and maintained by the Applicant or its successor. This access shall be as proffered in Rezoning PLN2000-00077, Southbridge Phase IIB-VII dated December 20, 2000 ("Rezoning PLN2000-00077") and shall include the indicated construction access road.~~

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~~24. Street Access Upon the sale of any parcel or lot within the Town Center, the Applicant shall prepare and provide to the Purchaser at settlement individual property plats to clearly indicate communal ownership of on-street parking spaces and public access to all pedestrian and bike trails and the future library site. Promotional materials shall advise prospective owners of property within the Town Center of Town Center Design Guidelines, the private ownership of Town Center private streets, and the reduced road and parking requirements allowed because of the Transportation Demand Management Program and Parking Impact Analysis.~~

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~~25. Potomac Parkway Design Speed A forty five (45) mile per hour design speed for Potomac Parkway (Street Type "A" in the Town Center Guidelines) will be considered for approval by Prince William County if the Applicant can provide, at the time of site plan, traffic-calming measures acceptable to the County and VDOT.~~

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~~26. Emergency Vehicle / Truck Access Route The following shall be the required route for WB-40 type vehicles within the Town Center: from Potomac Parkway to Street B, to Street I, to Street A and from there to Potomac Parkway, with all streets identified as shown in the Traffic Impact Assessment and Transportation Plan.~~

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Environmental

~~27. IEAMP. The Integrated Environmental Asset Management Plan ("IEAMP"), including the geotechnical study/evaluation as proffered in REZ PLN 20002012-00077-00420 shall apply to the Town Center area within REZ PLN 2000-00077.~~

~~28. Best Management Practices ("BMP") Master Plan – The approved Harbor Station BMP Master Plan (Prince William County Plans #03-00576 & #04-00334), prepared by christopher consultants (collectively "BMP Master Plan") shall be updated at the time each final site/subdivision plan is submitted for the Property.~~

~~Best Management Practices ("BMP") Master Plan – The Applicant shall provide a comprehensive BMP Master Plan to address pollution and sediment removal, protection of wetlands and water quality on the Town Center parcel, within Powell's Creek, and for those wetlands which are not Resource Protection Areas but feed directly into the pond on the north~~

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~~side of the site. This plan shall include multiple layers of siltation controls and bioengineered controls for the property including but not limited to the vicinity of the Wetland / Natural Space area, as required by the Department of Public Works. This plan shall be coordinated with the BMP plan for the other property which is the subject of REZ PLN2000-00077 since the urbanized Town Center may need to utilize off site BMP credits to show compliance with existing regulations. The BMP Master Plan must be provided prior to site plan approval.~~

~~2928. Wetlands Protection - In connection with the disturbance of any wetlands in Blocks 6 and 7, the Applicant shall locate mitigation on-site within the same hydrologic unit code, if this is acceptable to the Corps of engineers and Department of Environmental Quality. If it is not possible to locate mitigation on-site, the mitigation can occur off-site, but shall be located within the same or adjacent hydrologic unit code.~~

~~The Applicant shall protect wetlands and intermittent streams in the northern section of the Town Center within Block C with a conservation easement. This easement shall be a 100 foot buffer from the wetlands or watercourse acceptable to the Department of Public Works. Reasonable efforts will be made to preserve the wetlands in the Southern portion of the site in Block H. However, if the southern wetlands are disturbed as part of the Town Center construction, the Applicant shall locate the mitigation on site within the same sub-basin if this is acceptable to the Corps of Engineers. If it is not possible to locate the mitigation on site, the mitigation can occur off site, but must still be located within the same sub-basin.~~

~~3029. Geotechnical Study - At the time of site plan application, the Applicant shall provide a comprehensive geotechnical study acceptable to the Department of Public Works which will specifically address structural foundation stability for Marine Clay soils such that potential impacts, including those to structures, can and will be mitigated.~~

~~30. Natural Open Space – Natural open space shall be provided within the Town Center as referenced in the companion rezoning #PLN2012-00420 and as shown on the proffered Preservation/Restoration Area Exhibit.~~

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~~31. Water Quality Monitoring –~~

~~The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$75.00 per acre as proffered in REZ #PLN2012-00420 to be used toward water quality monitoring, stream restoration projects and/or drainage improvements. Said contribution shall be paid prior to and as a condition of each site/subdivision plan approval on the Property and shall be based on the total acreage reflected on each such plan. This obligation is intended to be coordinated with the companion rezoning #PLN2012-00420 in order to avoid duplicate payment.~~

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~~At the time of site plan approval, the Applicant shall provide a \$75 / acre fee toward or a water quality monitoring program as proffered in REZ PLN 2000-00077. Prior to site plan approval, the Applicant must demonstrate that post-development peak flows will not exceed those generating prior to development.~~

~~32. Low Impact Development - The Applicant shall utilize, to the extent reasonably feasible, low impact development ("LID") techniques in combination with traditional stormwater~~

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management techniques to manage stormwater runoff in the Town Center. LID techniques may include, but shall not be limited to, bio-retention facilities on-site infiltration, porous pavement, cisterns/rain barrels, bio-infiltration systems and grass swales. The locations (including Block 11) and specific techniques to be utilized, where feasible, shall be determined at the time of site/subdivision plan review for the Property, in consultation with the Department of Public Works, Environmental Services Division.

- ~~3233.~~ Utilities - Utilities such as gas, electric, phone, and cable shall be located in order to provide adequate green space for street tree planting in accordance DCSM requirements.
- ~~3334.~~ Tree Preservation - ~~The applicant is responsible for undertaking efforts to preserve specimen trees within the Town Center that are in good health and form. A tree preservation plan shall be submitted at the time of site plan submission which illustrates the portions of the Town Center containing tree save areas in accordance with the DCSM. This plan is subject to the approval of Prince William County~~ Tree preservation shall be provided as referenced in the proffers associated with REZ #PLN2012-00420.
- ~~34.~~ Limits of Disturbance - ~~The limits of disturbance drawing illustrating tree save and wetlands protection areas must be approved by Prince William County prior to site plan submission. Approved limits of disturbance shall be adhered to except for minor adjustments needed for normal engineering purposes. Minor adjustments must be approved by the County prior to site plan approval.~~
- ~~35.~~ Tree Planting - At the time of site plan approval, the Applicant shall provide a typical design for tree pits which ~~is consistent with Architectural Graphic Standards, 9<sup>th</sup> Edition, 1998 Cumulative Supplement~~ have adequate soil volumes and quality to sustain the long-term health and growth of plantings, especially trees, or equivalent acceptable to the Department of Public Works. Adequate soil volumes and quality, Additionally drainage, surface protection and root channels must be provided ~~based on the standard referenced above according to DCSM standards.~~
- ~~36.~~ Environmentally Sensitive Construction Materials and Techniques - Three (3) months prior to the initial site plan submission, the Applicant ~~will present research concerning the use of environmentally sensitive construction materials and techniques specific to the Town Center. At that time, the Applicant~~ shall meet with the Department of Public Works to develop a plan identifying areas and instances where ~~these~~ LEED, LID, Energy Star and other sustainability techniques will be implemented. Innovative erosion control techniques suggested by Public Works will be incorporated into the design of the site development plans.

Service Authority

- ~~37.~~ Utility Easements - The Applicant will provide easements for water and sewer utilities in accordance with the Prince William County Service Authority (PWCSA) Utility

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Standards ~~Manual~~. These easements shall not be overlain with electric, phone and television cables or gas pipelines except to allow perpendicular or other crossings as approved by the PWCSA, so as to enable the Service Authority to safely maintain and repair the water and sewer mains.

38. Utility Access - Adequate access to water and sewer mains in the right-of-way of streets must be provided. ~~Stray currents and corrosion protection shall be provided if deemed necessary by the Service Authority~~ Installation shall be according to PWCSA requirements.

**Parks and Open Space**

39. Trails – The Applicant shall provide trails in the Town Center as shown on the Street Grid, Infrastructure & Pedestrian Connections Plan. A segment of the Potomac Heritage National Scenic Trail ("PHNST") shall be located within the Town Center as shown on the Street Grid, Infrastructure & Pedestrian Connections Plan. Said trail segment will use sidewalks/walkways constructed as part of the Town Center and will use the Class I trail constructed along River Heritage Road.

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40. Golf Course – A golf course, or portion(s) thereof, and accessory uses may be located and constructed within Blocks 1, 2 and 10 provided all required permits are obtained from the applicable governmental agencies.

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41. Canoe Facility - In the event a hotel is constructed in Block 2, the Applicant may construct a canoe launch facility in connection with such use subject to the issuance of the necessary permits.

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42. Open Space/Plazas - Landscaped park areas, open space and plazas shall be provided in the Town Center in accordance with the Town Center Design Guidelines, to include a minimum 0.75 acre area in Block 2, a minimum of 1.5 acres in Block 3 and a minimum of 5.5 acres in Block 11.

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43. Building Setback - A minimum twenty-five foot (25') setback shall be provided between the eastern Property boundary and any building (excluding VRE station facilities/headhouse) in those portions of Blocks #2, #3 and #5 where the PHNST is located, as the PHNST is shown on the Street Grid, Infrastructure & Pedestrian Connections Plan. Said area shall be developed with the PHNST and a combination of landscaping and/or hardscape features to create an attractive amenity within the Town Center.

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39.

~~Bike Trail and Access – The Applicant shall provide a minimum 10 foot wide bike/ pedestrian trail along the eastern portion of the site as indicated in the Town Center Design Guidelines. In the event the design for the Potomac National Heritage Trail is located through the Town Center, the Applicant shall provide a trail easement of an appropriate width at a location agreeable to both the Applicant and the County.~~



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~~40. Golf Course – A golf course or portion (s) of a golf course may be located and constructed within the OCR area of Block C provided all required permits, including a Provisional Use Permit are obtained from all required governmental agencies.~~

Public Safety

~~44. Police Substation – As proffered in REZ PLN ~~2000~~2012-0007700420, the Applicant shall make available, with no rent charge, to the Prince William County Police Department (“Police Department”) approximately 1,500 square feet of street level tenant space with drywalled offices, interview room, two restrooms, storage area and conference room or as otherwise approved by the Police Department. This substation will be located in a central location in the Town Center or may be located elsewhere in the town center as approved by the Police Department. All operational expenses (i.e., real estate taxes, common area fees, etc.) shall be the responsibility of the Applicant or an owners association established for the development.~~

~~45. The Applicant shall provide a maximum of six (6) parking spaces reserved for the exclusive use by the Police Department and said spaces shall be marked/signed accordingly. The parking spaces may not be located at the front of the building in which the tenant space is located but will be located in the general area. At least three (3) such parking spaces shall be located within five hundred feet (500') of the field office space. The final location of such parking spaces shall be determined by the Applicant in consultation with the Police Department.~~

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~~Reserved parking spaces for 10-12 police vehicles shall also be provided. This tenant space will be made available at such time as development of 100,000 gsf nonresidential and 1,100 units of residential development in the Town Center is complete. Final plans for the space shall be coordinated with the Police Department.~~

~~46. The "field office" space and associated parking spaces shall be shown on the final site plan for the building in which it is located and shall be provided no later than at the time a building permit is issued for the 500,000<sup>th</sup> square foot of nonresidential space in the Town Center.~~

~~47. The police "field office" improvements shall be in accordance with the Design Guidelines provided, however, that the Police Department shall be exempt from paying any fees due to it being subject to the Association.~~

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~~48. The Applicant shall coordinate with Prince William County at the time of site plan to facilitate the accommodation of radio signal augmentation. Such coordination may include the Applicant providing conduit or design accommodations to permit public safety radio antenna system communications, except that any equipment shall be provided by others.~~

~~42-49 Crime Prevention - The Applicant shall undertake ~~the following~~ Crime Prevention Through Environmental Design (“CPTED”) measures to reduce the opportunity for crime and to enhance the quality of life, including:~~

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- a. Lighting: At the time of site plan submission, the Applicant shall submit a lighting plan taking into consideration the concepts of CPTED and the design and concept of the Town Center to the ~~Police Department for review and approval~~Director of Planning or his designee.

~~Cut off fixtures shall be used with diffusers to focus light where needed and to minimize light trespass. Landscaping shall not interfere with any security lighting at the time of planting or as they mature.~~

~~The following standards for outdoor lighting levels shall be incorporated into the lighting plan:~~

- ~~a. — Parking and pedestrian areas in single family detached residential areas — 1 foot candle minimum~~
- ~~b. — Single family attached and multifamily areas — 2 foot candle minimum~~
- ~~c. — Commercial areas — 2 foot candle minimum, 3-5 foot candle preferred~~
- ~~d. — Street lighting — .5 foot candle minimum~~

- b. Common Observation: Tree branches in parking areas shall be trimmed such that no branch hangs lower than ~~six-four (4)~~ (4) feet above the ground. Shrubbery shall not be allowed to grow above ~~36-42~~ 36-42 in height. ~~Screening of parking areas / facilities shall be done so that landscaping leaves room for common observation points intended for police and security personnel.~~

- c. Underground parking / parking decks: The use of metal halide light fixtures is suggested. The building material shall be light in color to reflect the lighting. Pavement shall be scored to prevent trespassing by skaters/skate borders also improving vehicle traction. The facilities shall also have panic/communication stations strategically located throughout. Open stairwells shall be used since they minimize areas of concealment.

- d. Recreation Facilities: ~~These areas shall be placed in well observed / high activity areas where they can be observed by persons in their normal course of activity.~~

~~43.~~ 50 Sprinklers - Fire sprinkler systems shall be installed in all buildings exceeding 45 feet in height in conformance with National Fire Protection Association (NFPA) standards.

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~~44.51~~ Contingency Plan - Prior to site plan approval, a contingency plan shall be submitted to the Fire Marshal's Office which specifically addresses the firefighting cleanup responsibilities for the Town Center in accordance with Section 32:~~280.35~~700.51(7)-(7) of the Zoning Ordinance.

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452. Alternative Fire Protection Measures - The Applicant shall provide alternative fire protection measures to compensate for requested modifications and waivers that may reduce the effectiveness of fire and rescue operations or increase the risk of fire, when the Fire Marshal's Office determines that such waivers and/or modifications create conditions that may adversely affect fire and life safety in the Town Center.

53 Elevators – Each “high rise” residential housing type building on the Property shall include at least one elevator large enough to accommodate a standard ambulance size gurney lying flat plus emergency personnel. The obligation created hereby shall be reflected with a note on all final site/subdivision plans that include any “high rise” building on the Property.

46-54 Public Uses – Public uses such as the library and police substation shall not be required to belong to any Town Center association and shall not be required to contribute towards the maintenance of private streets within the Town Center.

Library

55. The Applicant shall dedicate, and convey in fee simple, to the Prince William Board of County Supervisors, at no cost to the County, a pad site containing approximately one (1) acre of land for use as a library (“Library Site”), said site being located within Block 3, as said Block is shown on the Land Use Plan. The Library Site shall be available for dedication no later than at the time the abutting streets are constructed or a building permit is issued for the 500<sup>th</sup> residential unit in the Town Center, whichever occurs first; provided however, (i) the site shall not be dedicated until such time that said dedication is requested in writing by the County and the County has funding and is in a position to move forward with the construction of the library; and (ii) any improvements to be constructed on the Library Site shall be in accordance with the Town Center Design Guidelines; however, the Library shall be exempt from any fees due as a result of it being a member of and subject to the Association. Within thirty (30) days of receipt of a written request from the County in accordance with the above, the Applicant shall commence and pursue in good faith and with due diligence, the preparation of documents necessary to convey the property to the County. Prior to the conveyance of the Library Site to the County, the Applicant shall have the right to use the property for passive recreation/open space purposes.

56 Prior to conveying the Library Site to the County and subject to obtaining all County and/or other governmental permits and approvals, the Applicant shall provide, at no cost to the County, the following improvements for the Library Site: (i) site grading in accordance with standards and specifications provided by Prince William County, (ii) the extension of public water and sanitary sewer to a point along the boundary of the Library Site to allow a connection of said utilities (excluding connection costs/tap fees), and (iii) storm water management (either on-site and/or off-site) for the Library Site (“Library Site Improvements”).

57. If requested by the County at the time the site plan is filed for the Library Site Improvements, up to fifty (50) spaces within the Town Center shall be made available for the non-exclusive use of library patrons, the final number and location of which shall be coordinated with the Department of Library Services.

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58. In lieu of dedicating the site as referenced in Condition #41 above, and with the consent of the Planning Director, or his designee, the Applicant may pursue an alternative arrangement to provide library space and associated parking in an alternative location within the Town Center. Said alternative arrangement shall be determined in consultation with the Library Services Department and shall be finalized prior to the release of the Library Site for private use. In the event this alternative is selected, the Library Site may be used for any use otherwise permitted by the zoning.

59 The Applicant shall make a monetary contribution to the Prince William Board of County Supervisors in the amount of \$100,000 to be used for the purchase of equipment for the library. Said contribution shall be paid at the time the Library Site is dedicated to the County or when an alternative arrangement is finalized pursuant to Condition #44 above. This obligation is intended to be coordinated with the companion rezoning #PLN2012-00420 in order to avoid duplicate payment.

Telecommunications

4760. Telecommunication Towers – Given the urban nature of the Town Center, telecommunication towers and / or monopoles are not permitted, except through a special use permit and the use of stealth technology.

Implementation

48.61 Implementation of Special Use Permit - ~~At the time of commencement of construction for the Route 1 / Route 234 Interchange, the Applicant shall submit site and subdivision plans for the entire street grid network. The Applicant shall diligently pursue the approvals of the street grid network plans.~~

At the time of each site plan submission, the Applicant shall submit a cumulative tally to the County indicating the percentages of residential and nonresidential development constructed and commencement of uses.

General

4962. Associations – For the Town Center, there ~~shall may~~ be ~~no~~ more than ~~two one (21)~~ homeowners and / or business associations, plus one (1) overall umbrella association. The umbrella association shall have the responsibility and authority to manage the overall town center community to ensure the umbrella association can respond to changing circumstances. The Associations shall be subject to the SUP conditions and underlying REZ PLN ~~2000~~2012-0007700420.

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63 Temporary Activity Permits (TAPs) - The associated waiver request for TAPs does not relieve the applicant from obtaining necessary permits, such as health or building permits. Furthermore any event expected to draw more than 500 participants in a day shall be required to consult with the police department about safety and traffic control measures.

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**Request for Modification or Waiver of Specific Requirements of the Subdivision Ordinance, Zoning Ordinance, or DCSM as it Relates to a Town Center Special ~~User Use~~ Permit Application, ~~January 9, 2004~~ Pursuant to Section 32-280.34**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
1. Modification to allow submission of Town Center schematic land use plan and exhibits at a scale of 1" = 200'.	Sections 32-700.21 (7), <del>32-700-50 and Section 32-280.35</del> <del>(1), 32-700.51 and 32-280.30</del>	The size and area of the proposed Town Center does not lend itself to 1" = 100' without creating matching sheet lines. <b>APPROVE</b>
2. Modification to allow submission of Town Center plat and other required plans and exhibits with scale other than 1" = 100', and to exceed maximum sheet size of 24" x 36"	<del>Section 32-700.50</del> <del>Section 32-280.35,</del> <del>32-700.51 and 32-280.30</del>	The size and area of the proposed "Town Center does not lend itself to 1" = 100' and 24" x 36" sheets without creating match lines. <b>APPROVE</b>
<del>3. Modification to allow submission of plans with contour intervals greater than five feet.</del>	<del>Section 32-700.50</del> <del>Section 32-700-21</del>	<del>The size and area of the proposed Town Center does not lend itself to five foot contour intervals without making plans difficult to read.</del>
<del>3.</del> <u>4.</u> Modification to omit <del>private streets, roadway alignments, alleys, shared access driveways, delivery and emergency vehicle access and loading-, easements, right of way widths, sidewalks, paths and trails, speed limits and design speeds, lengths, curb radius, paving width and on- and off-street parking characteristics</del> from the Town Center street grid plan; said information to be provided at the time of site plan filing for respective land bays (blocks).	Section <del>32-280.35</del> <del>32-700.51</del> (1)	Planning and design for Town Center has not reached this level of detail and will be clarified with the detailed engineering associated with the site plan filing. <u>Major streets and associated sidewalks are shown on the Street Grid Plan.</u> <b>APPROVE</b>
<u>4.</u> <del>5.</del> Waiver of internal buffers between different uses to be replaced and implemented by the <del>landscaping and buffer set forth within the proffers,</del> conditions and the Design	Sections <del>250.30;</del> <del>250.31, 32, 33</del> <del>32-250.30,</del> <del>32-250.31 and 32.250.32;</del> DCSM	The planning for the Town Center and associated Design Guidelines blend the proposed land uses as well as the surrounding uses. This effort provides more appropriate standards consistent

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<p><del>Guidelines. Applicant will provide, at time of site plan, information, including sidewalks, paths, wall, fencing, street standard and range of landscaping details, for the area of Town Center Blocks E and G within the specific site plan along the easterly boundary of these blocks westerly to include the most easterly street and northerly until such street turns northwesterly.</del></p>	<p>Sections 802.11 and 802.12</p>	<p>with the desired design for this urban area, and the adjacent uses. <b>APPROVE</b></p>
<p><u>5.</u> <del>6.</del> Waiver of use regulations for parking of all uses to reduce the number of required spaces and to provide for shared parking arrangements consistent with the <u>Potomac Shores Town Center Design Guidelines and the Parking Impact Analysis for Southbridge Town Center Assessment.</u></p>	<p>Sections 32-250.10, <del>32-280.23; 32-280.51(2)(a), (3); 32-280.61(3); 32-280.71(3); 32-280.81(3); 32-280.41(3); 32-280.61; 32-400.16; 32-280.41(5), 32-280.51(3), 32-280.61(3), 32-280.71(3), 32-400.24.</del></p>	<p>Town Center concept is based upon <del>multiple use of shared parking walk to</del> opportunities which reduce the need for parking spaces. <del>Also, the Design Guidelines provide for parking which is deemed necessary for a successful project. Details for the Town Center parking are set forth in the Potomac Shores Town Center Parking Assessment.</del> <u>Approved 4/24/13 (WAI2013-00157)</u></p>
<p><u>6.</u> <del>7.</del> Modification to allow parking as a principal use on a subdivided lot(s). This modification shall also allow temporary surface parking as provided in the Town Center Conditions.</p>	<p><del>Sections 32-280.41, 32-280.51 and 32-250.10</del> <u>Section 32-280.31</u></p>	<p>This would allow for the provision of parking structures to service multiple users <del>and on</del> a separate subdivided lot thereby providing more efficient use of parking and resources to provide parking. This modification would allow efficient use of land, limit need to provide structured parking until development requires structured parking <del>and the provided.</del> <u>There is a Condition that</u> provides limitations to deal with concerns that temporary parking could remain in place indefinitely. <b>APPROVE</b></p>
<p><del>8.</del> Modification of DCSM requirement for roadway sections with respect to <del>right of way width, design speed, lane width, maximum grade, centerline radius, off-street parking requirements</del></p>	<p><del>DCSM Sections 650.00; 602.03; 602.08; 610.01; 602.13; 601.08; 603.11 602.17, 18,</del></p>	<p><del>This would allow for the Urban Design Guidelines to dictate specifics of the roadway and sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and</del></p>

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Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
<del>and curb returns and sidewalk/trail width, location and materials.</del>	19, 20, 21, 22, 23	consistent with the TIA. Private roads are to be designed and constructed to County standards unless otherwise approved by DPW.
9. Modification to allow SFD units on private streets designed as set forth above (#7 and #8).	Section 32-280.71(2)(a)	These may be locations within the Town Center where SFD units will front on private streets and/or may not front on public streets, while supporting the Town Center concept.
7. <del>10.</del> Waiver to allow certain OCR land bays to contain less than 50% residential net floor area, but no less than 45% residential in the aggregate and to provide the option to have ground level residential, to have retail and/or office uses with no residential uses within the same building. Modification to eliminate the requirement that 50% of the aggregate nfa in a mixed use Block be residential and that residential shall only be located on the 2nd and higher floors of buildings.	Section 32-280.41(1)(f) 32-280.41(3)	Allows for some land bays (blocks) to be exclusively non-residential as is often the case in Town Centers. The Applicant proposed a condition which would preclude block faces from being solely residential or solely nonresidential in the critical Town Center areas in Blocks d, D, F and G and providing for mixed use buildings in other areas. This would provide for the mixed use nature needed for the Town Center while allowing development to occur in areas less visible and important to the heart of the Town Center's vibrancy without the code's restrictions and better allow the marketing of the Town Center as a whole. <u>core Blocks in the Town Center will be developed with a mix of residential and nonresidential uses to create an active center but single buildings may not include a mix of uses.</u> <b>APPROVE</b>
8. <del>11.</del> Modification to reduce the setback and yard requirements for multifamily, single family detached and single family attached residential uses per the Development Code.	Sections 32-280.51(2)(a), 32-280.61(2) and 32-280.71(2)	Provides flexibility consistent with Urban Design Guidelines <del>and in order to</del> provide the "feel" of a Town Center. <b>APPROVE</b>
9. Modification to allow a 2-story façade rather than a 3-story façade and a minimum height of 25' for multifamily buildings and townhouses.	Sections 280.51(2)(b) and 280.61(2)(d)	Provides flexibility to create an environment that simulates the character of Tidewater settlements such as Norfolk, VA and Edenton, NC. <b>APPROVE</b>
10. Modification to allow 1-story visible	Section 280.71(2)(b)	Single family detached homes are limited



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<u>from the street for single family detached dwellings.</u>		<u>to Block 1, which is located at the edge of the Town Center. One-story golf course villas provide an attractive housing alternative for the community and provide an appropriate transition between the higher density toward the Town Center core and the lower density residential neighborhoods surrounding the Town Center.</u> <b>APPROVE</b>
<u>11. <del>12.</del> Modification to have no minimum number of courtyards and plaza areas with each multi-family building.</u>	Section 32-280.51(2)(d)	Provides flexibility to arrange multifamily units in a more urban fashion and to combine and coordinate amenities such as courtyards and plaza areas. <del>Every multifamily structure will have a courtyard or plaza or will share a courtyard or plaza with another building throughout the Town Center.</del> <b>APPROVE</b>
<u>12. <del>13.</del> Modification to allow minimum lot widths of less than 50-40 feet for SFD units.</u>	Section 32-280.71(2)(c)	<del>Allows for more urban design of residential areas consistent with Town Center approach. Narrow lot width would accommodate the golf villa option in Block 1.</del> <b>APPROVE</b>
<u>13. Modification to reduce the minimum height of nonresidential buildings from 30' to 15'</u>	Section 280.41.4(a)	Provides flexibility to create pedestrian oriented retail streets that simulate the character of Tidewater settlements such as Norfolk, VA and Edenton, NC. <b>APPROVE</b>
<u>14. Modification to reduce the building separation for townhouses from 20' to 15'.</u>	Section 280.61.2(e)	Consistent with the more urban character of the Town Center. <b>APPROVE</b>
<u>15. Modified housing type development standards for a Duplex unit.</u>	Section 32-305.6.E	See attached duplex housing development standards. The Town Center development standards do not specifically address duplex development standards and the PMR standards are more appropriate for more typical suburban development. <b>APPROVE</b>
<u>16. <del>14.</del> Waiver of the 15-foot wide landscaped area around public use sites. As provided in the Design Guidelines, such public uses</u>	Section <del>32-</del> <u>201-1932-201.18</u>	Provides flexibility to integrate public uses into development design rather than separate out such uses. The Design Guidelines provide for the protection of

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<del>(utilities) are to be located to the rear or side of buildings or screened/enclosed.</del>		the <del>viewscape-view-scape</del> intended by the Code requirement and establish requirements and standards which are tailored to the Town Center concept. <b>APPROVE</b>
17. <del>Waiver to modify the standard to allow for permanent structures or unauthorized obstructions and encroachments within easements as well as to allow for drainage easements to be located within buffer areas.</del>	<del>DCSM 702.08(F)</del> <del>DCSM 702.08(I)</del>	<del>This modification allows for structures customarily seen in a Town Center, such as: decorative street lighting, informative signage, street furniture, etc., to be placed within easements.</del> <b>DENY</b>
18. <del>15.</del> Waiver request to allow mobile or modular offices for construction or sales offices to be located on <del>site a parcel for which do there may not have be an approved site plan or building permit and to not comply.</del>	Section 32-210.11( <del>1</del> ) and ( <del>2</del> ); <del>Section 32-210.12(5)12</del>	The dense nature of the Town Center and the <del>land bay-Block</del> configuration may require that such facilities be located nearby but not on the specific site which has a final site or subdivision plan where <del>the construction is being done</del> <u>has commenced</u> . This is typical of development in more urban areas. <b>APPROVE</b>
19. <del>16.</del> Modification of the Zoning Ordinance and DCSM requirements for tree canopy and open space to allow calculations based upon the entire Town Center site instead of each individual site plan.	Sections 32-250.40 and <del>250.44; and</del> DCSM <del>Sections</del> 802.20, 21, 30 and 31	Due to the nature of the overall project and the agreements to not disturb particular areas, and due to the nature of the Town Center, it is not reasonable to require these standards on each site plan. <b>APPROVE</b>
20. <del>17.</del> Waiver of additional setback requirements for structures which exceed the height permitted in the district.	Section 32-400.03(5)	The design for the Town Center is specifically to avoid suburban type setback and create a more urban design. These setback requirements make it more difficult for individual structures to relate to one another. <b>APPROVE</b>
21. <del>18.</del> Request to modify or waive various restrictions and limitations on temporary commercial activities to allow for a vibrant Town Center <del>as set forth in the justification.</del>  18. <del>—————</del> (Continued)	<del>Sections 32-210.01(1)(d), (i), (j); (2)(a), (b), (e), (f); (3)(a), (b), (e), (d); 32-210.02(1)(a), (2), (5), (6) Sections 210.01(1)(a), (b), (d), (i), (j), 210.01(2)(a), (b),</del>	The waivers and modifications are intended to remove limitations on the number, duration, location and separation of the temporary commercial activities which are expected to help provide the atmosphere needed to achieve and maintain a vibrant Town Center. <del>E.g., §32-210.01(1) modification to allow roadside stands in excess of 30 days. (d)</del>

**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
	<p><u>(c), (f), 210.01(3)(a), (b), (c), (d), 210.01(4)(a), (b), 210.02(1)(a), 210.02(2), 210.02(5) and 210.02(6)</u></p>	<p>Modification to provide that safe and adequate access may be non-vehicular and that the no "disruption" of normal traffic flow requirement does not apply as may be part of private street closing. (i) Waiver on limitation on number of permits allowed per year and duration (j) Waiver of parking requirement as this would involve private streets, possibly closed within a Town Center. Providing additional parking is not in keeping with Town Center concept. Parking along the private streets is to be in the control of the developer and/or associations as applicable. (2)(a) Waivers and modifications of provisions requested in (1) sought here as made applicable by (2)(a) for same reasons. (b) Waiver sought to not limit sponsorship of circuses or carnivals, so that clear associations and/or developer may be a sponsor. (c) Waiver sought from on-site/adjacent site parking as providing additional parking is not in keeping with Town Center concept. Parking along the private streets is to be in the control of the developer and/or associations as applicable. (f) Waiver sought so the number of permits is not limited. (3)(a) Waiver sought to not limit duration or require separation of such activities (b) Waivers and modifications requested in (1) sought here as made applicable by (3)(b) for same reasons (c) Waiver sought to not limit number of permits. (d) Waiver sought on parking as provision of additional parking is not in keeping with Town Center concept. Parking on private streets is to be within the control of the developer or association, applicable. 32 210.02(a)(a) Waiver sought to remove requirement of separation of events/activities. (2) Waiver to allow more than one active temporary commercial activity permit to be on a lot at one time. (5) Waiver to clarify that no setback will be required from private street, even if providing public access.</p>

**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
		<del>(6) Waiver of limitation that no permit will issue until 21 days after expiration of temporary permit on adjacent lot.</del> <b>APPROVE</b>
<del>22.</del> <u>19.</u> Modify requirement for only one association to allow more than one association.	Section <del>32-280.31(6)(a)</del> <u>32-280.31(7)(a)</u>	It is standard practice for such a project to have more than one, and the nature of residential versus commercial uses argue for more than one association. <del>A condition has been added limiting the number of associations for the Town Center to two (2) homeowners and/or business associations, plus one (1) over, umbrella association.</del> <b>APPROVE</b>
<del>20.</del> <u>20.</u> <del>Waiver of timeframe for implementing the special use permit for the Town Center.</del>	Section <del>32-700.55(1)</del>	<del>The complexity of this project and the requirement for numerous infrastructure improvements prior to the implementation of the Town Center.</del>
<del>21.</del> <u>21.</u> <del>Modification of requirement to delineate stormwater management areas on-site and BMP facilities for the Town Center SUP.</del>	Section <del>32-700.50(3)(b)(xiii)</del>	<del>This information has not been determined because of the need for greater engineering detail and is dependent upon the proposed SUP configuration. The information will be available at the time of site plan submission.</del>
<del>23.</del> <u>22.</u> <del>Modification to allow CCR's to be amended in accordance with standard practice during developer control of project provisions specified therein.</del>	Section <del>32-280.31(6)(j)</del> <u>32-280.31(7)(j)</u>	While the SUP conditions control the significant development parameters, the developer will need the ability to implement changes and the flexibility to amend the CCR to respond to particular situations during the development of the site. <b>APPROVE</b>
<del>24.</del> <u>23.</u> <del>Modification to eliminate the requirement to include items in the Design Guidelines required by Section 280.35 which are not part of the submitted and approved guidelines.</del> Design Guidelines.	Section <del>32-280.36;</del> <u>32-280.35</u>	The required elements of the Design Guidelines should be tailored to address each Town Center circumstance – including only those elements which are relevant to the proposal. Applicant and consultants have <del>worked with County staff and Centerbrook I</del> <u>preparing the prepared</u> Design Guidelines and Development Code <del>acceptable to</del> <u>appropriate to this Town Center.</u> We do not want an issue raised that a technicality or detail may have not been

**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
		included that is referenced in the <del>Code</del> when we have worked hard to accommodate staff and Centerbrook concerns and ultimately come up with an acceptable set of <u>Zoning Ordinance</u> once the Design Guidelines are approved. <b>APPROVE</b>
<p><del>24. — Waiver of Section 250.20 et seq. of Zoning Ordinance and applicable sections of the Design and Construction Standards Manual to the extent are in conflict with the standards and parameters for signage as set forth in the Design Guidelines.</del></p> <p>24. — (Continued)</p>	<p><del>Sections 32-250.20 et seq.</del></p>	<p>The Town Center Design Guidelines incorporate a signage plan appropriate for the Town Center which would permit consistent and compatible signage for the Town Center. Certain signs, such as tenant logo signs, will be an important part of street level retail. Information kiosks and signs perpendicular to building facades which would extend into required setbacks are an important visual feature of an urban environment. (E.g. <del>32-250.22(2), 32-250.24(1)(a)(i), 32-250.21(2)</del>) The signs in the Town Center must be reviewed and approved by a Design Review Committee, which will provide guidance for consistent and compatible signage in the Town Center. Size would be determined by the Design Guidelines, not <del>§32-250.22(6)</del>. E.g., Sections <del>32-250.24(1)(a)(i), (1)(b)(i); (ii), (iv), (v), (2) and (4)</del> would be superseded to extent those sections would not allow what the Design Guidelines would allow. Section <del>250.25</del> would be waived to extent it is clarified that sign permits would be issuable when this waiver is satisfied, i.e., where the proposed sign would satisfy the Design Guidelines which would conflict with the Zoning Ordinance or DCSM.</p>
<p>25. Modification request to postpone submission of recreation from the schematic land use plan until the time of site plan submission of individual land bays.</p>	<p>Section <del>32-280.35(232-700.51(2))</del></p>	<p>At this time, it is not possible to know precisely how much recreation area and facilities will be provided. This information will be available at time of site plan submission. <b>APPROVE</b></p>
<p>26. Modification request to postpone phasing of <u>infrastructure development</u> and other</p>	<p>Section <del>32-280.35(3)(432-</del></p>	<p>An initial <u>infrastructure</u> phasing plan for the Town Center has been submitted. However, it is not possible at this point in</p>

**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
improvements until the time of site plan submission.	<a href="#">700.51(3)</a>	time to precisely predict when <del>infrastructure improvements development</del> will be provided except on a site plan by site plan basis. <del>Please note the transportation phasing plan provides for road improvements required to service each phase.</del> <b>APPROVE</b>
<del>27. Modification request to postpone identification of specific stormwater management areas and/or stormwater management strategies until the initial site plan submission for the Town Center.</del>	<del>Section 32-280.35(3) and Section 32-700.50(3)(xiii)</del>	<del>The overall stormwater management strategy will be decided upon with the submission of the initial site plan for the Town Center. The stormwater management strategy and specific improvements will be consistent with the Urban design Guidelines and the IEAMP.</del>
<del>27. 28.</del> Modification request to allow submission of contingency plan to address responsibilities for clean-up, containment and notification until time of site plan submission.	<del>Section 32-280.35(732-700.51(7))</del>	At the time of site plan submission, the actual uses and responsible parties will be known and can be clearly identified. Similarly the specific uses will determine the type of contingencies which need to be planned for and called out in the plan. Also, the IEAMP provides for a contingency plan. <b>APPROVE</b>
<del>29. Waiver of requirement to establish architectural standards beyond those approved in the Design Guidelines.</del>	<del>Section 32-280.31(6)(b)</del>	<del>The architectural standards applicable to the Town Center are set forth in the Design Guidelines and Development Code. The architectural guidelines included in the adopted Design Guidelines will govern the architectural practices and issues associated with the Town Center. Reference to or creation of another set of architectural standards is confusing and raises questions as to who/what is the ultimate authority.</del>
<del>30. Waiver of Planned Development District requirements in lieu of the specific provisions and Design Guidelines for the Town Center SUP.</del>	<del>Sections 32-280.01 through 280.24, and Part 405</del>	<del>This will serve to clarify that the Town Center SUP Zoning Ordinance provisions and associated SUP supersede the general requirements for Planned Development Districts. In effect, this states for the record that the Design Guidelines and conditions of the SUP take precedent over the Planned Development District regulations so long as the SUP is in effect.</del>

**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
<p><u>28.</u> <del>34.</del> Modification to allow heights greater than permitted in <u>RM-O(H) &amp; UHDR</u> Land Bays as provided in the Design Guidelines and Development Code.</p>	<p><u>Section 32-280.41(4)(a) and Section 32-280.51(2)(b)</u></p>	<p>In order to accommodate the County's intent to include <del>a-</del><u>buildings with greater number of residential dwelling units heights and densities</u> in the Town Center, buildings will need to be taller than otherwise allowed by this Zoning Ordinance provision. <del>Heights would be allowed as provided in the Southbridge Town Center Design Guidelines and Development Code,</del> as approved with the SUP. <b>APPROVE</b></p>
<p><u>29.</u> <u>Modification of DCSM requirement for roadway sections with respect to right-of-way width, design speed, lane width, maximum grade, centerline radius, off-street parking requirements, intersection curb returns, requirements for curbing and sidewalk/trail width, intersection design, entrance spacing, turn lanes, channelization, location and materials.</u></p>	<p><u>DCSM Sections 650.00, 602.04, 602.07, 610.01, 602.14, 601.08, 603.11, 602.15, 16, 18 and 22</u></p>	<p><u>This would allow for the SUP Plan – Modified Street Sections and Design Guidelines to dictate specifics of the roadway and sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and consistent with the TIA. Private roads are to be designed and constructed to modified standards shown on the SUP Plan or as may be otherwise approved by Department of Transportation. Allow alternative pavement on private streets to highlight areas. Design Speed Waiver Approved 12/07/12 (WAI #2013-00058)</u></p>
<p><u>30.</u> <u>Waiver of the minimum travel way widths as set forth in the DCSM, the geometry of angled and parallel parking in one and two way drive aisles, as well as to allow for angled parking within a two way drive aisle.</u></p>	<p><u>DCSM 600-46 DCSM 650.06 DCSM 600-71 DCSM 610.06(R)</u></p>	<p><u>This would allow for the Urban Design Guidelines to dictate specifics of the roadway and sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and consistent with the TIA. Private roads are to be designed and constructed to County standards unless otherwise approved by the Department of Transportation. Approved with PWC Plan #05-00640 on 11/08/12 (WAI #2013-00036)</u></p>
<p><u>31.</u> <u>Waiver of various standards associated with the DCSM TS-1 and CI-1 plates in order to allow for a modified I' gutter pan, as well as the limitations for on-street parking.</u></p>	<p><u>DCSM 650.06 DCSM 650.08</u></p>	<p><u>This would allow for the Urban Design Guidelines to dictate specifics of the roadway and sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and consistent with the TIA. Private roads are</u></p>

**Attachment C**  
**SUP Conditions dated June 7, 2013**

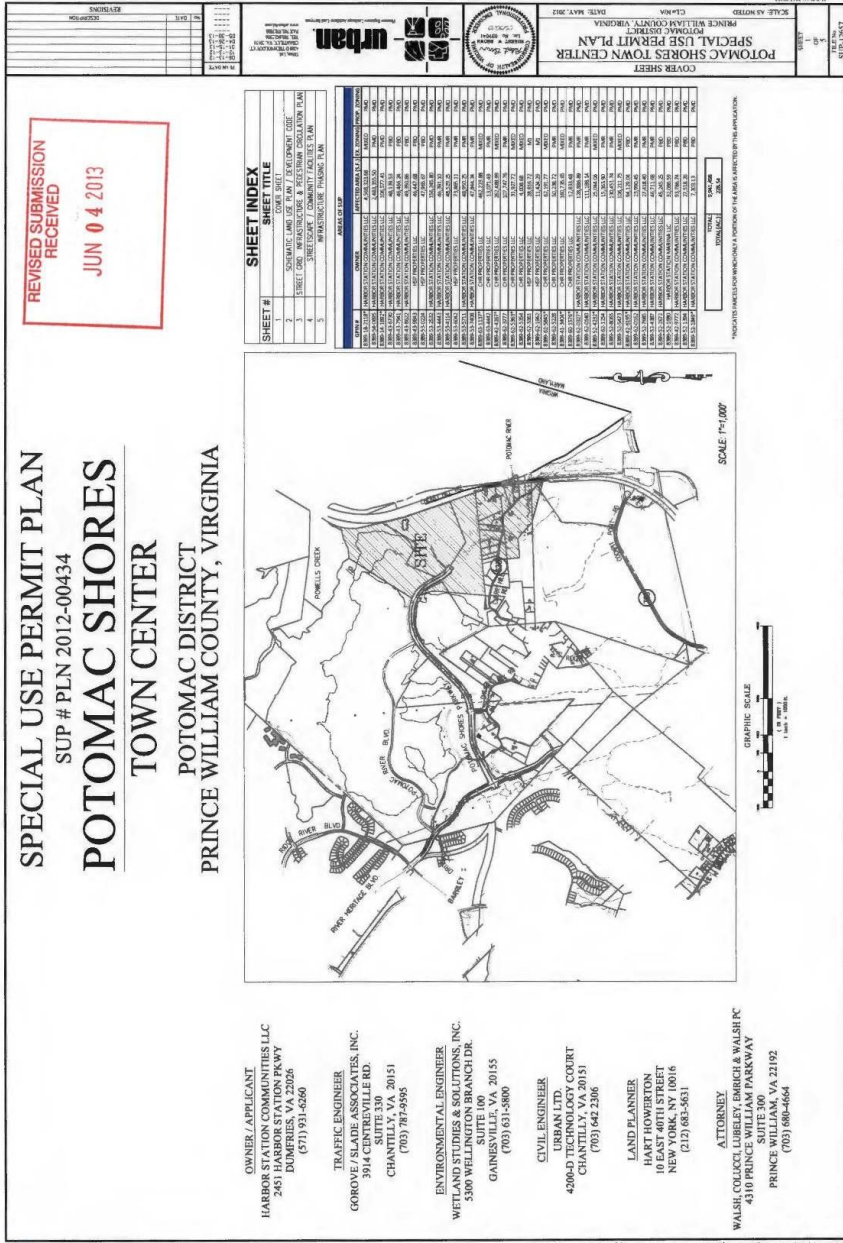
Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
		<p><u>to be designed and constructed to County standards unless otherwise approved by the Department of Transportation.</u> <b><u>Modified 1' Gutter Pan Waiver</u></b> <b><u>Approved 12/07/12 (WAI #2013-00057)</u></b></p>
<p><u>32. Waiver of the minimum curb return radius of 35' to be reduced to 25'.</u></p>	<p><u>DCSM 602.07(I)</u></p>	<p><u>This would allow for better pedestrian movements at each intersection. Where on-street parking is present, while the curb will be physically constructed with a 25-foot radius, the on-street parking has been held back sufficiently from the intersection as to afford an unobstructed 35-foot radius.</u> <b><u>Approved with PWC Plan #05-00640 on 11/08/12 (WAI #2013-00038)</u></b></p>
<p><u>33. Waiver to eliminate the minimum entrance cross-over distance of 200'.</u></p>	<p><u>DCSM 602.07</u></p>	<p><u>Based on the location and intent of the entrance to the hotel, the geometry of the "loop" will allow for a more desirable traffic flow condition and will not affect the intent of the minimum entrance cross-over distance requirement.</u> <b><u>Approved with PWC Plan #05-00640 on 11/08/12 (WAI #2013-00038)</u></b></p>
<p><u>34. Waiver to the street pavement design and privately maintained travelway pavement redesign requirements to allow for a modified pavement section that will include a brick or concrete unit paver section within the Town Center.</u></p>	<p><u>DCSM 602.10</u> <u>DCSM 602.11</u> <u>DCSM 602.15</u> <u>DCSM 650.01</u></p>	<p><u>This waiver would allow for the Urban Design Guidelines to dictate the specifics of the pavement/sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and consistent with the T.I.A. This affords the applicant the ability to create special places within the Town Center.</u> <b><u>Approved 12/07/12 (WAI #2013-0059)</u></b></p>
<p><u>35. Waiver to the requirement to construct/install street lights in accordance with the standards set forth in the DCSM.</u></p>	<p><u>DCSM 602.13</u> <u>DCSM 650.54</u></p>	<p><u>This waiver would allow flexibility for the applicant to construct/install street lights in line with materials that would be aesthetically in conformance with the intent of the remainder of the Town Center without compromising the intent of the details provided by the DCSM.</u> <b><u>Approved 12/07/12 (WAI #2013-00060)</u></b></p>



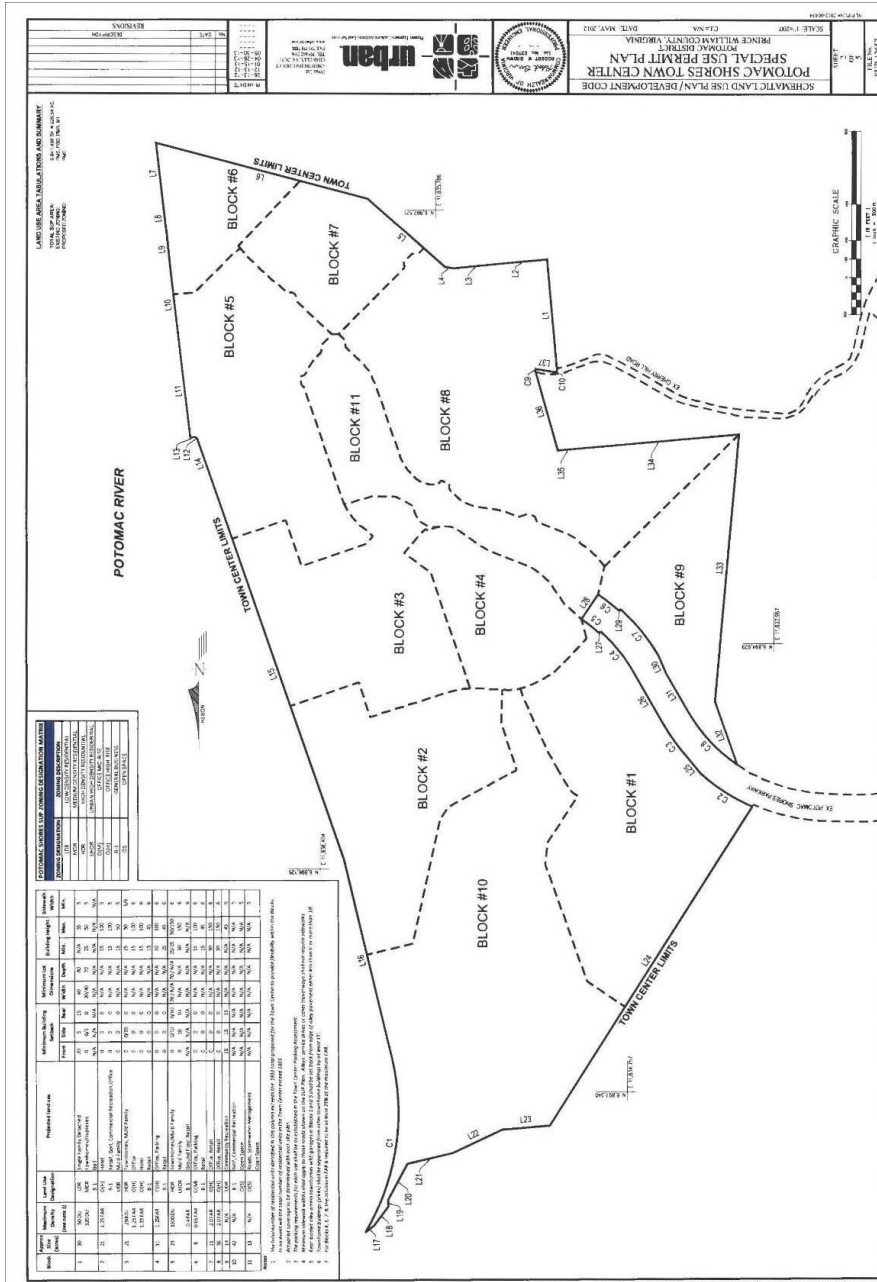
**Attachment C**  
**SUP Conditions dated June 7, 2013**

Requested Modification or Waiver	Citation	Applicant's Justification for Request and Staff Recommendation
<p><u>36. Waiver to the requirements associated with the street name signs and privately maintained travelway signs to be more in line with the materials and aesthetics of the remainder of the Town Center.</u></p>	<p><u>DCSM 604.06</u> <u>DCSM 640.08</u> <u>DCSM 650.45</u> <u>DCSM 650.48</u> <u>DCSM 650.49</u></p>	<p><u>This waiver would allow flexibility for the applicant to construct/install street signs in line with materials that would be aesthetically in conformance with the intent of the remainder of the Town Center without compromising the intent of the details provided by the DCSM.</u> <b><u>Approved 12/07/12 (WAI #2013-00061)</u></b></p>
<p><u>37. Waiver to the requirement to construct guardrails in accordance with VDOT specifications to create a guardrail more consistent in material and aesthetics to the remainder of the Town Center in lieu of the standard VDOT GR-2.</u></p>	<p><u>DCSM 602.12</u> <u>DCSM 603.15</u></p>	<p><u>This waiver would allow flexibility for the applicant to construct guardrails in line with materials that would be aesthetically in conformance with the intent of the remainder of the Town Center without compromising the intent of the GR-2 detail provided by VDOT</u> <b><u>Approved with PWC Plan #05-00640 on 11/08/12 (WAI #2013-00037)</u></b></p>
<p><u>38. Waiver to the general sidewalk/trail requirements; specifically the requirement to provide sidewalks or trails within the street right-of-way of all proposed streets within urban section streets, to design and construct trails in accordance with Detail 650.31, and the requirements associated with the maximum slope of trails (maximum proposed slope to be 10%).</u></p>	<p><u>DCSM 602.18</u> <u>DCSM 602.22</u> <u>DCSM 650.31</u></p>	<p><u>This waiver would allow for the Urban Design Guidelines to dictate the specifics of the roadway and sidewalk/trail design consistent with the intent and desired flexibility of the Town Center and consistent with the T.I.A. Private roads will be designed and constructed to County standards unless otherwise approved by Prince William County.</u> <b><u>Approved with PWC Plan #05-00640 on 11/08/12 (WAI #2013-00035)</u></b></p>

**Attachment D**  
**SUP Plan dated June 23, 2012 and revised through May 30, 2013**



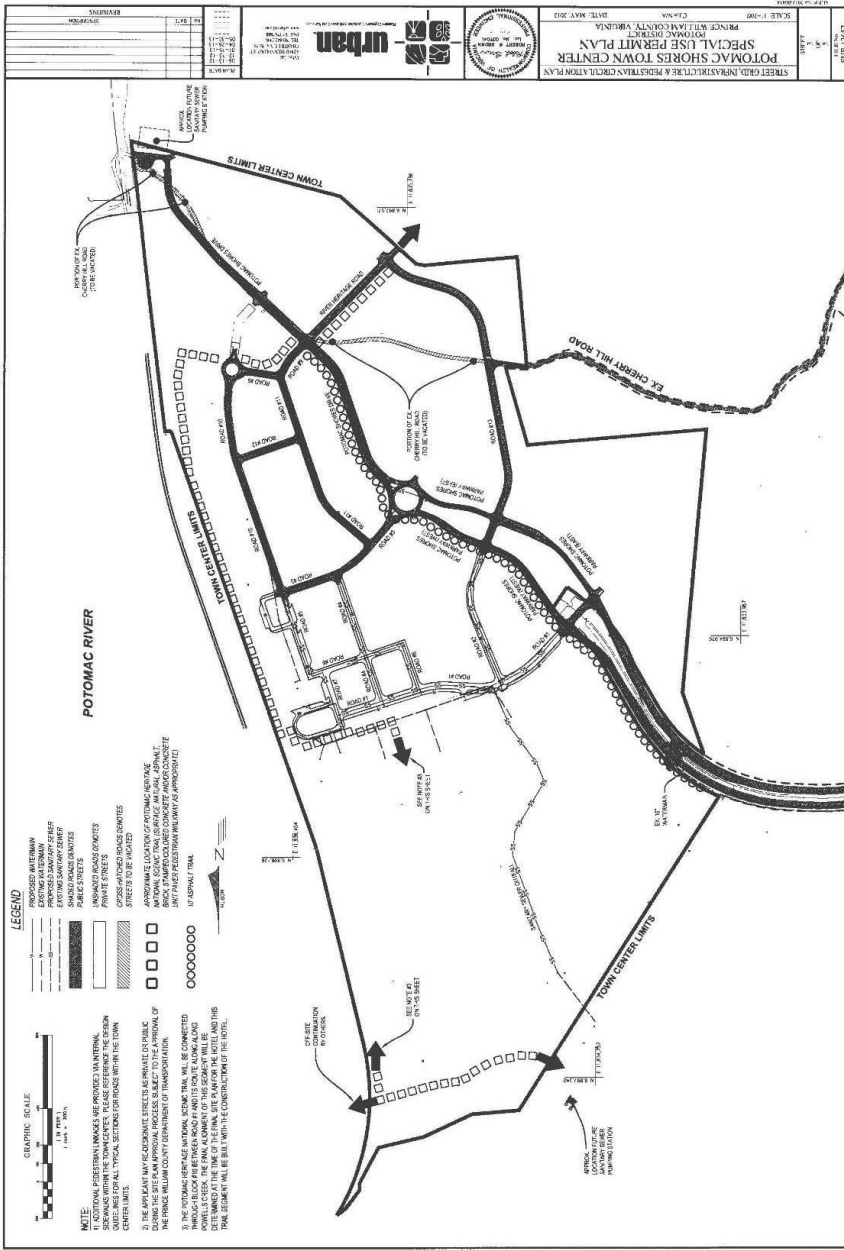
# Attachment D Schematic Land Use Plan/ Development Code dated June 23, 2012 and revised through May 30, 2013



# Attachment D

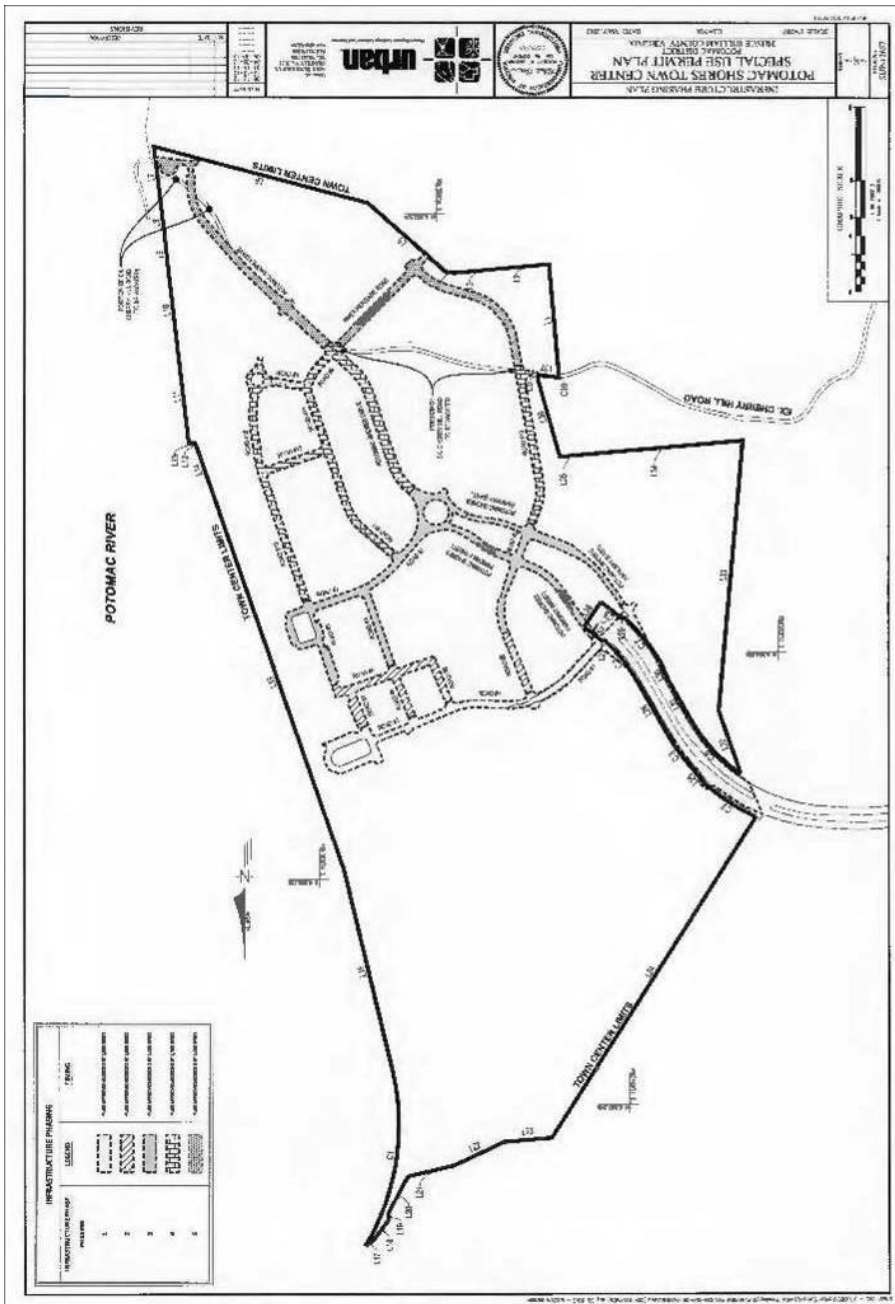
## Street Grid, Infrastructure & Pedestrian Circulation Plan

dated June 23, 2012 and revised through May 30, 2013





**Attachment D**  
**Infrastructure Phasing Plan**  
 dated June 23, 2012 and revised through May 30, 2013





**Attachment E  
Design Guidelines**

**POTOMAC SHORES**

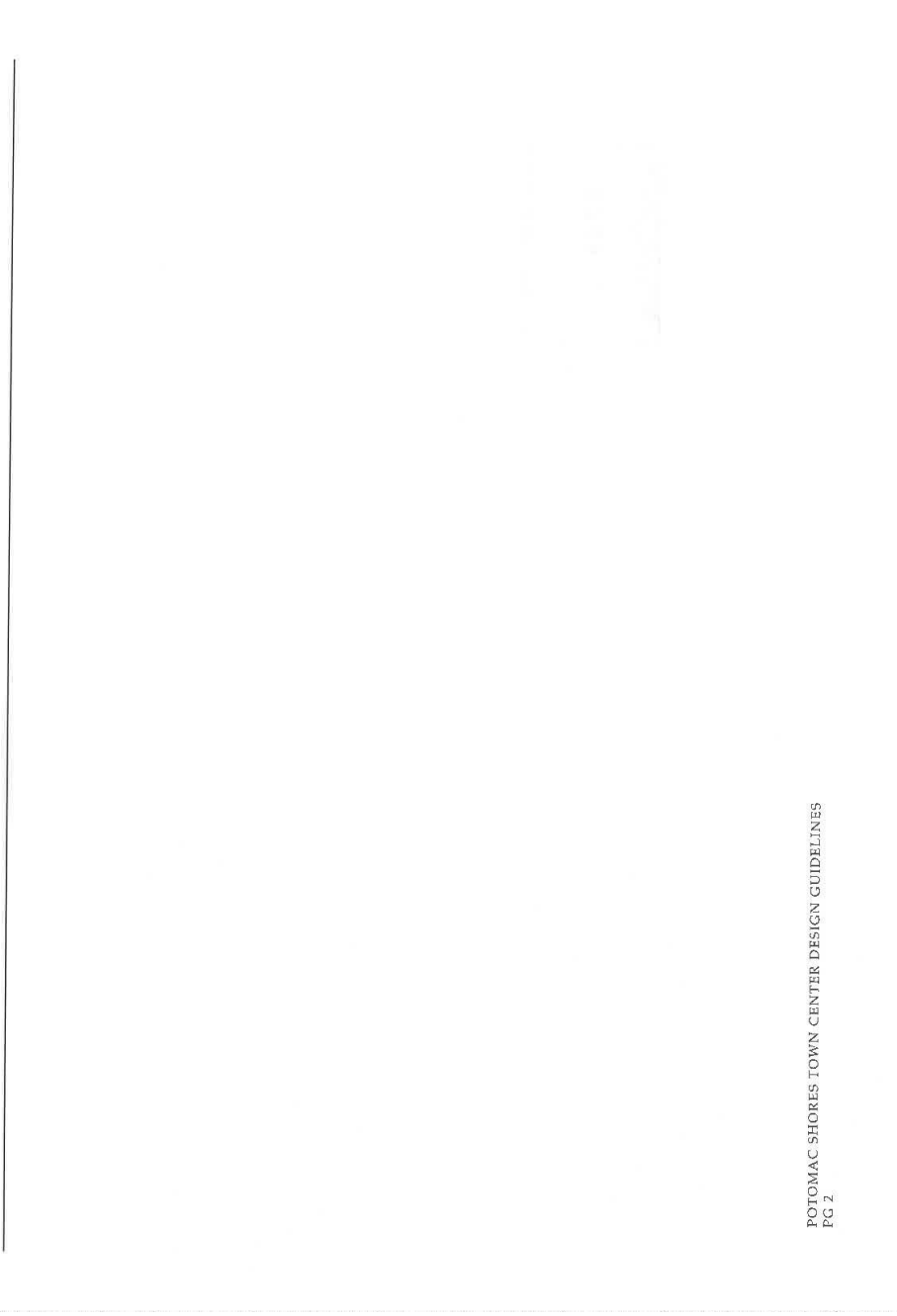
REVISED SUBMISSION  
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MAY 03 2013

**TOWN CENTER  
DESIGN GUIDELINES**

*January 15th 2013  
revised April 26th, 2013*



**Attachment E**  
**Design Guidelines**



POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 2



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**Appendix B: Street Sections**

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# 1.0 INTRODUCTION

POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 4

## 1.0 INTRODUCTION

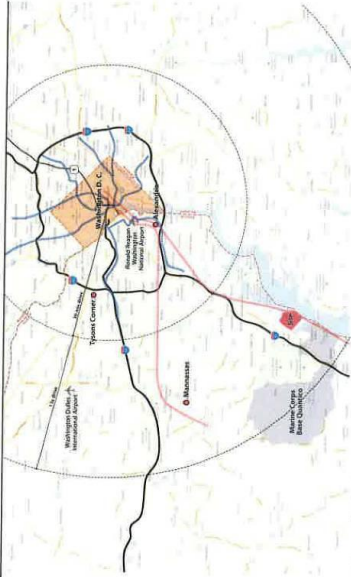
The Town Center is the anchoring element of the Potomac Shores community. This community has been planned as a waterfront multi-use development located along the Potomac River, 25 miles south of Washington DC. The Town Center Master Plan consists of approximately 228 acres within the Potomac Shores community. The Town Center is established through an approved Special Use Permit (SUP) within the PMD zoned property.

### 1.1 LOCATION

With direct access to the proposed VRE Commuter Station, Interstate 95, and Route 1/ Route 234 Interchange, the Town Center is strategically located within the region. Proximity to Potomac Mills, Leesylvania State Park, Prince William Forest Park, historic battlefields, and two major airports are significant attractions for future residents, businesses, and property owners. Few, if any, comparable sites remain in the D.C. Metropolitan Area with the equivalent size, access, natural beauty, and strategic location.

### 1.2 PHILOSOPHY

The Design Guidelines and Development Code for the Potomac Shores Town Center is a document that integrates and organizes the development of human habitats, workplaces, streetscapes, and public spaces. This code presents the urban regulations that reach beyond merely aesthetic and architectural principles. The code promotes rich physical, social, and economic value by defining a consistent and uniformly-high level of quality throughout the Town Center. The vitality of this new town will be symbolized by the well-coordinated streetscapes that are compact, consistent, beautiful, and walkable, framed by complementary



Map of Washington D.C.



Potomac Shores - Illustrative Master Plan

## 1.0 INTRODUCTION

architectural styles, and integrated with natural systems on a human scale. The organizing principles of these guidelines are not new. Instead they represent a return to the principles of urbanism found in Tidewater coastal towns and the Mid-Atlantic region, with cultural, climatic, and physical particularities.

The Town Center has been organized into discrete planning units. These planning units may also be referred to as neighborhoods, blocks and streetscapes as the case may be. The blocks serve as the principal physical areas around which to structure most architectural and site design elements. They promote the quality, compatibility, and cohesion envisioned for all development in the Town Center.

### 1.3 DESIGN REVIEW COMMITTEE

The Design Guidelines and Development Code fulfill the requirements of Prince William County's Town Center special use permit requirements and, more importantly, provides the necessary and sufficient urban regulations to guide and assist builders, architects, and designers in preparing coherent and appropriate development plans. In addition to the County's statutory plan review process, the Design Review Committee (DRC) provides for a heightened design review. To ensure the integrity of the Town Center, the DRC process is established in addition to separate covenants creating a Potomac Shores Community Association.

### 1.4 COMPREHENSIVE PLANNING OBJECTIVES

Integrating both a regional planning and site-specific perspective, the Town Center plan builds on years of study by both Prince William County and the master developer. Potomac Shores is rooted in the underlying objectives and initiatives of the County's Comprehensive Plan and Cherry Hill Sector Plan.

The overriding philosophy for the Town Center incorporates these commonly-held goals and objectives:

- Create a Town Center that is both a home and destination for County residents, workers, shoppers, and visitors.
- Create sufficient density of residential, commercial, retail and resort uses to drive a balanced and economically vibrant community, and achieve long term sustainable growth.
- Develop a Town Center using Traditional Neighborhood Development (TND) in harmony with Transit-Oriented Development (TOD) that is tailored to the specific characteristics of the site and adjoining communities.
- Create compact, close-knit, human-scaled neighborhoods located within a short walk of the commercial core. The core of the Town Center is a 'live/work/play' fully functioning community.
- Enhance expectations for public and private development using standards that maintain, and promote the environmental integrity and enjoyment of the Potomac River and Powell's Creek.
- Promote the use of transit and shared parking facilities in and around the Town Center commercial core to increase transportation efficiency and reduce unnecessary impervious areas along the waterfront.
- Ensure that the Town Center's Town Center Illustrative Plan included with the special use permit application requirements will help guide the master planning process on a project-by-project basis.

## 1.0 INTRODUCTION

### 1.5 GUIDING PRINCIPLES FOR URBAN DESIGN

Throughout the project, both the master developer and the individual site developer/builder shall meet the following guiding principles:

- Each streetscape and block shall promote well-integrated and comfortable pedestrian movement between buildings as well as offer a memorable, safe, and attractive experience.
- The Town Center's architectural and urban design elements shall make a positive enduring contribution to the project's image, viability, and sustainability.
- The soil volumes and quality within landscaped areas of the Town Center will be of the quality to sustain long term health and growth of plantings, especially trees.
- All aspects of the Town Center shall promote high standards of urban design and architectural quality, seeking a careful balance amongst variety, consistency, and contrast within the community.
- The Town Center shall become a unified, compact, integrated Transit Oriented Development with a special identity and sense of place as illustrated by the street and block pattern in the SUP and the Town Center Illustrative Plan. To fulfill this concept, buildings and uses at Potomac Shores can be varied while being both visually and functionally complementary and compatible to the overall design theme of the Town Center. The aesthetic of the Town Center shall evolve from the tradition of Tidewater architecture and planning, with suitable building orientations, building materials, graphic identity systems, landscaping, and a common set of site and building elements.
- The functional relationships of Town Center uses shall be reinforced by the development of pedestrian interconnections and coordinated vehicular circulation, parking, utility corridors, and physical connections between buildings.



*Palmetto Bluff, SC*



*Annapolis, MD*

## 1.0 INTRODUCTION

### 1.6 THE TOWN CENTER IN CONTEXT - MASTER ZONING PROGRAM FOR POTOMAC SHORES

With the Town Center as its nucleus, the overall community master plan for Potomac Shores includes an array of land uses that, ultimately, will augment the character of this core development. The Potomac Shores Master Zoning Plan, shown in Figure 1.1, serves as a framework guiding future development of the community. The Master Zoning Plan embraces a program for orderly and environmentally sound growth by establishing a distinctive mix of land uses and public spaces within the context of the County's urban growth management objectives to accommodate the range of approved land uses. The Potomac Shores community is organized into distinct neighborhoods or "land bays." With the Special Use Permit (SUP) application, one neighborhood or land bay uses the Town Center's street system to create eleven separate "block" and "use group" designations.

#### 1.6.1 TOWN CENTER PLAN

An important planning goal is to promote synergy and compatibility among the residential, business, recreational, cultural, and social activities. Streetscapes and blocks within the Town Center will display cohesiveness in landscape and urban design. Design themes will be coordinated with the larger Potomac Shores community, and Town Center development will be integrated with adjoining neighborhoods. This includes a system of connected sidewalks and trails that link the neighborhoods to the river frontage, open space amenities, trails, and passive recreational features, as well as to the recreational and cultural amenities.

Consistent architectural, landscape, streetscape, and transportation concepts must be carried from one block and streetscape to another. Design styles, colors, materials, and architectural excellence will be

POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
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the unifying elements in ensuring the compatibility between each of Potomac Shores's neighborhoods and the Town Center. The vision for both quality and variety will stem from adherence to architectural and landscape design guidelines, restrictive covenants, and community land use regulations.

The Potomac Shores Community Master Plan's underlying strength is its ecologically-sound urban design strategy. Furthermore, carefully established natural open space created by preservation and restoration will accent the built environment. Please see page 5 for the Open Space Illustrative Plan illustrating this in detail. The Town Center design creates an urban riverfront destination for residents and visitors while providing appropriate transitions and linkages to recreational uses and natural open space to the north and potential development to the south. The Town Center Illustrative Plan graphic on page 12 contains elements that illustrate the design intent, although actual development as it occurs may vary.

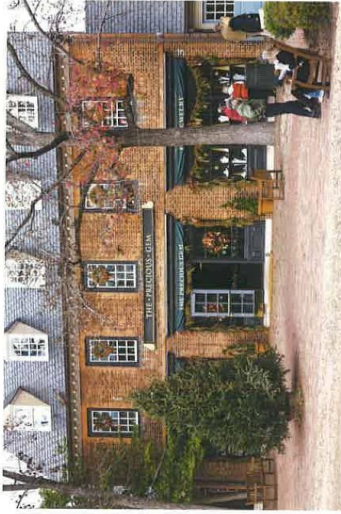
#### *Main Street & Gateways*

At its northern gateway, Potomac Shores Parkway is a scenic landscaped street that leads to the Town Center. The Town Center is the commercial core for the new community, where civic and commercial development are organized around a vibrant retail district. From the parkway, two landscaped gateway streets take residents and visitors further into the Town Center commercial core, terminating in two landscaped greens. The commercial core is bounded by the hotel to the north, and the VRE station to the south. The connecting street between the two is the main retail street of the Town Center.

Echoing the traditional street form and function of Prince William County's historic towns such as Occoquan and Manassas, the interior Town Center street system is well organized and scaled to promote traffic calming, on-street parking, and pedestrian-friendly streetscapes. The use of the traditional grid street pattern will allow the Town Center to grow



1.0 INTRODUCTION



*Williamsburg, VA*



*Annapolis, MD*



*Town Center Rendering*

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## 1.0 INTRODUCTION

flexibly over time, with north/south streets extended to the south to link additional uses to the transit-oriented core.

### 1.6.2 TOWN CENTER LAND USES

Using the scenic background of the Potomac River and Powell's Creek, the Town Center will be an integral part of the surrounding community. A mix of upscale residential neighborhoods, corporate offices, retail shopping, recreation, and cultural and community facilities will provide a desirable setting for people to live, work and play. Civic facilities, the VRE train station, a hotel meeting center, and a golf clubhouse for the Jack Nicklaus Signature Golf Course are proposed amenities within the Town Center. Corporate and high-technology offices interested in taking full advantage of the Washington Metropolitan Area's economy and work force are anticipated tenants of the Town Center.

#### *Downtown Retail Concept*

The Town Center street grid will support an attractive neighborhood retail shopping experience, could include either horizontal or vertical mixed-uses, and is designed to accommodate both pedestrian and vehicular traffic. The traditional neighborhood development concept invokes an architectural and streetscape scale that makes all uses compatible with each other; be they retail, commercial, office, residential, and/or destination activity centers. At the same time, the hotel meeting center, golf course, VRE train station, riverside amenities, and the adjacency of Powell's Creek and Potomac River will enhance the destination appeal of the Town Center and attract visitors from a much broader regional base.

#### *Residential Neighborhoods*

The Town Center's residential neighborhoods are planned for a mix of detached and attached dwellings, townhomes, mid-rise and/or high-rise POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
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multifamily. The residential neighborhoods that comprise the Town Center blocks are within walking distance of the commercial core. The northerly residential area adjacent to Potomac Shores Parkway will serve as a transition from single family residential, golf, and open space uses to more urban portions of the Town Center.

With excellent views to the Potomac River, Powell's Creek and adjacent woodland settings, the residential blocks will have a strong sense of place, placing emphasis on well-composed streetscapes, landscapes, neighborhood parks, and open space.

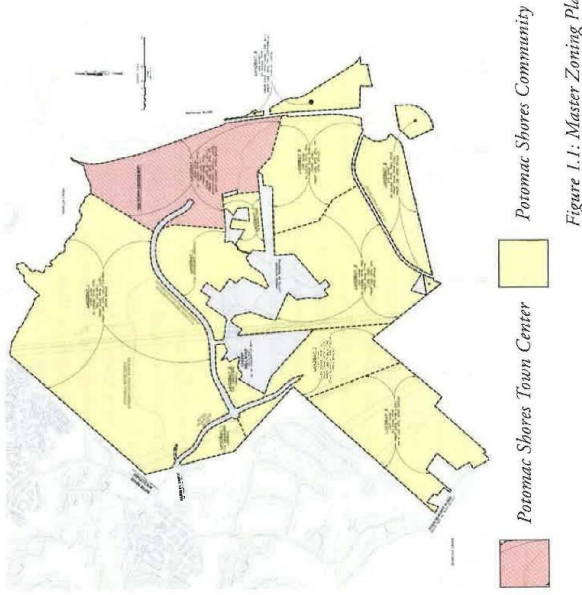


Figure 1.1: Master Zoning Plan



## 1.0 INTRODUCTION

### 1.6.3 SPECIAL USE PERMIT (SUP) LAND USES

The Town Center Land Use Summary Table below outlines the range and mix of the Town Center's anticipated uses and densities. The Town Center will include up to 1,833 residential units. The projected Town Center retail, commercial, office, and recreational, activities may include as much as 3.7 million square feet of gross floor area. These estimated yields will change based on actual market demand and final building layout and design.

**Town Center Land Use Summary Table**

Residential	± 1,833 Units
Retail / Commercial / Office	± 3,340,000 Sq. Ft.
Recreational Center	± 10,000 Sq. Ft.
Hotel / Golf Clubhouse <sup>1</sup>	± 350,000 Sq. Ft.
<b>Non-Residential Sub-Total</b>	<b>± 3,700,000 Sq. Ft.</b>

### 1.6.4 TOWN CENTER BLOCKS

#### *Block Diagram and Street Grid*

The framework for the Town Center Master Plan is a series of eleven blocks arranged in a street grid that enhances pedestrian activity with the potential for integrating development of additional parcels south of the retail core.

### 1.6.5 BLOCK & USE GROUPS

The Development Code Table summarizes the eleven Town Center blocks in terms of SUP land bay designation, approximate block size, planned land use, and opportunities for development mix and densities. Those blocks that are currently scheduled to contain structured parking are

referenced for the purpose of projecting the number of parking spaces to satisfy the demands of the parking impact analysis for the overall Town Center. The residential development yields represent the recommended densities necessary to provide primary market support for the planned Town Center convenience and neighborhood retail development with the understanding that residential development within the Town Center is capped at a maximum of 1,833 units. The exact boundary acreage of individual blocks may be adjusted at the time of site plan submission in order to accommodate market demands, engineering, environmental, geotechnical or reasonable design considerations.

### 1.6.6 ILLUSTRATIVE PLAN

For purposes of visualizing how the Town Center might develop under the Block Diagram and associated Development Code, an Illustrative Plan is provided to reflect one of many possible alternative development scenarios for the Town Center. The Illustrative Plan is not intended to establish specific building footprints, internal block configurations or the ultimate mix of uses within a block, but rather, the Illustrative Plan is intended to portray a vision of how the Town Center might evolve through implementation of the various controlling documents. Similarly, the various photos and renderings contained in the Design Guidelines portray this general vision and are not intended to reflect the actual design, buildings, materials or uses to be developed in the Town Center.

Flexibility is key to the successful implementation of the Town Center within the overall context of the established parameters to help ensure that the Town Center can evolve over time into a vibrant mixed use community where residents, employees and visitors can interact and enjoy a variety of opportunities and activities afforded by a uniquely designed and located urban community.

**Attachment E**  
**Design Guidelines**

**1.0 INTRODUCTION**



*Figure 1.2: Town Center Illustrative Plan / Block Diagram.*

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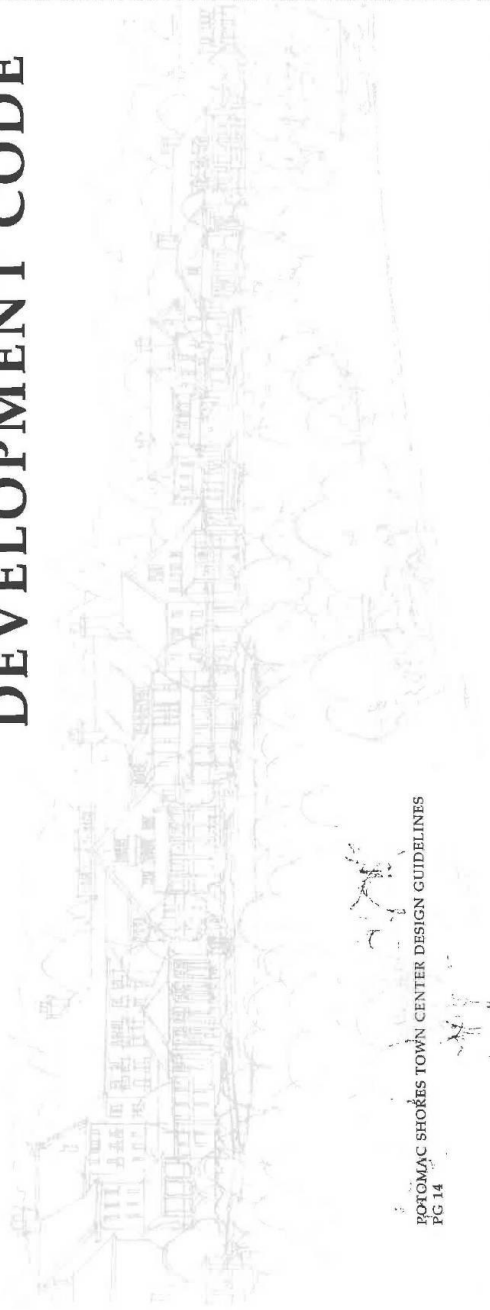
**Attachment E  
Design Guidelines**

**1.0 INTRODUCTION**



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**2.0 ARCHITECTURAL &  
DEVELOPMENT CODE**



POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
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## **2.0 ARCHITECTURAL & DEVELOPMENT CODE**

As indicated on the Town Center Illustrative Plan, Potomac Shores Town Center will incorporate a range of land uses. The riverfront commercial and hospitality uses and the VRE Station are planned to be the focus of the commercial core. Residential development is a part of the mix of uses, and will provide the critical mass necessary to enliven the core and enhance the viability of commercial development. Each building design will implement the Town Center streetscape and architectural objectives specified in the Design Guidelines.

### **2.1 GUIDING PRINCIPLES**

The vision for the architecture and urban design components for the Town Center shall embody the following:

- The Town Center shall be rich and varied, composed of complementary architectural styles and materials, resulting in an eclectic composition that characterizes towns in the Tidewater region.
- The look of the Town Center shall evolve from architectural styles, building orientations, materials, graphic identity systems, landscaping, and a common set of site and building elements. Covered porches, overhangs, and additional shading devices should provide a casual, waterfront atmosphere.
- Buildings shall be developed with entrances and frontage on a grid of traditional streets that “bind” the town together. Main building facades will face the street.
- Buildings and other structures shall be set at or close to, the street edge, with a sidewalk. Parking and drive aisles shall not be permitted between the buildings that primarily contain retail uses and the public street.
- Building heights and uses in the town center shall be varied in order to create a vibrant, integrated, urban core.
- Traditional urban architectural elements, such as awnings, benches, streetlights, bollards, and signage are encouraged, and shall be

compatible and coordinated with the architectural theme.

- Within the Town Center (Block 3) architectural expression of buildings four stories or lower should reflect the individual building’s use while still adhering to the principles of Tidewater architecture.
- In the blocks surrounding the core (Blocks 4-8) transition in scale from the Town Center; architectural expression should accommodate the larger masses of mid-rise office and residential uses, while still harmonizing with the language of the Town Center.

To fulfill these concepts as well as the functional and environmental quality of the project, the DRC intends to consistently apply the following design and development precepts throughout the Town Center.

### **2.2 GENERAL DESIGN FUNDAMENTALS**

This section describes the architectural standards that contribute to the overall design excellence of the Town Center. The building form, silhouette, orientation, and material standards reinforce the urban design concept. The architectural design fundamentals ensure compatibility amongst all architectural elements and maintain a high level of quality. Furthermore, it is a primary objective to provide for functional, flexible, and cost effective buildings which are responsive to market considerations and varying tenant requirements.

Architects and urban designers of individual properties in the Town Center are encouraged to produce an eclectic composition. Inspired by the rich layering achieved in many waterfront towns and cities in the Mid-Atlantic, such as Norfolk, VA, Annapolis, MD, and Edenton, NC. However, certain design fundamentals must be adhered to from building to building to allow the Town Center to be consistent with the dominant themes expressed herein.

## 2.0 ARCHITECTURAL & DEVELOPMENT CODE

### 2.2.1 SCALE

The intended intimacy of the Town Center and its pedestrian-scaled building treatments are to provide definitions of space, interest, and articulation along the ground floor facades.

The perceived scale of a building, as seen from a specific vantage point, may be controlled through careful siting and architectural treatment at the ground floor level. The scale and visual impact of a building may also be controlled by breaking larger volumes into smaller components and by careful architectural detailing.

### 2.2.2 ORDER

The use of symmetry, axial relationships, hierarchies of scale between blocks, and the expression of entry are a few tools that architects should employ to create or reinforce visual order.

### 2.2.3 BALANCE

Balance is achieved when the desired focal point of a composition is apparent. Balance is readily achieved in a symmetrical building when the focal point is located at its center. Where a building composition is asymmetrical, balance may be achieved through the use of rhythm and repetition, often combined with localized symmetry or a discontinuity in the typical patterns at the focal point itself.

### 2.2.4 RHYTHM

Rhythm is fundamental to the architectural integrity of the Town Center. At its essence, rhythm is the consistent repetition of a building's architectural components. The orderly repetition of similar architectural

elements such as columns, doors, and windows contribute to the balance and order perceived in a building or group of buildings. Once a rhythmic pattern has been established, an interruption in that pattern becomes a focal point.

Within the Town Center, the regular spacing of similarly sized openings within individual buildings and blocks of buildings establish rhythmic patterns. Rhythms established at the ground floor level are often repeated at the upper levels. Variations in rhythm are established to provide acute visual interest in specific instances such as in roof lines visible from the river and the Town Center's main streets.

### 2.2.5 PROPORTION

Proportional relationships demonstrated in building elevations will include consistent window width-to-height ratios, consistent roof slopes at related roof lines and uniformity in the percentage of void (window and door) area to overall area of the building elevations. Scale relationships between the Town Center blocks and their adjacent dependencies also support the established system.

## 2.3 BUILDING DESIGN FUNDAMENTALS

- Where appropriate, buildings designed in the Tidewater style, are encouraged to incorporate brick or other indigenous natural materials.
- For buildings designed in the Tidewater style, special attention should be paid to the facade composition of gable ends, where appropriate openings should be centered on gable. In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- For residential buildings, landscaped courtyards and/or lawns are encouraged between building entrances and street edge.

## 2.0 ARCHITECTURAL & DEVELOPMENT CODE

- Garages, and surface lots shall be screened, or placed at the rear of buildings.
- Environmental sustainable principles should be incorporated into building design.
- Dormers, chimneys, and any other items extending above the roofline should be of the same color and materials used in the building's façade.
- Planting and/or hedges appropriate to the urban environment shall be employed along frontages of Potomac Shores Pkwy and Potomac Shores Drive.
- Roof penetrations, such as vents and stacks, wherever possible, shall be placed on rear roof slopes, or in the case of flat-roof buildings, screened from street-level views with parapets and other screening devices.
- Exterior building lighting is to be coordinated among buildings.

### 2.4 EXTERIOR TREATMENT AND MASSING

The appearance of unified, harmonious building exteriors addressed within this section will portray the design excellence and building quality embodied throughout the Town Center. The following section addresses elements of a buildings exterior treatment.

#### 2.4.1 FACADE MATERIALS

The exterior building skin shall be composed of one dominant facing material and one or two additional materials as accents. Natural building materials shall be used as dominant facing materials, including natural and manufactured stone, brick, wood, and wood-like materials such as cementitious fiber board. Material selections shall be submitted to the Design Review Committee along with plans and specifications, for review and approval.

#### 2.4.2 COLOR

One dominant color for the building elevation and two accent colors will be permitted. Black and bright primary colors are not recommended for the dominant material. Recommended dominant colors and tones are as follows:

- Stone: Warm and cool tones.
- Glass: Warm and cool tones, semi-reflective glass, except for highly reflective, mirrored, bronze, and/or gold glass.
- Pre-cast concrete and stone: Warm and cool tones.
- Metal: Warm and cool brushed and neutral tones.
- Wood: Warm and cool, neutral tones.

The color of all proposed materials shall be indicated on each plan submission for review and approval. Buildings within each block shall have compatible, coordinated color schemes. The DRC will approve proposed color schemes on a case-by-case basis.

## 2.0 ARCHITECTURAL & DEVELOPMENT CODE

### 2.4.3 ROOF TREATMENTS

Mechanical equipment and roof color shall be compatible and integrated with the building design. Visible roof mounted equipment is prohibited. If necessary, the equipment shall be masked behind screening materials. All roof-mounted mechanical equipment shall be screened and painted to blend into the approved roofing color. Equipment shall be arranged in an orderly, clustered manner, grouped behind one parapet screen. At a minimum, a single continuous parapet wall without louvers or penetrations shall screen, or hide entirely, all roof mechanical equipment from ground views. Antennas, disks, solar panels, etc., if required by building occupants shall be grouped in an orderly manner behind the mechanical screen referenced above unless specifically approved by the DRC.

### 2.4.4 ARCHITECTURAL LIGHTING

Except for hotels and the restaurants, flood lighting of building exteriors shall be prohibited without the approval of the Design Review Committee. An illumination impact plan may be required by the DRC prior to building approval.

### 2.4.5 GROUND FLOOR FACADES

Where appropriate, the design of buildings shall provide a transition from the upper floors to the pedestrian level by the use of materials, building articulation and level of detailing. When seen from the boulevards and streets a transition zone shall add interest at the vehicular and pedestrian scale, thereby reinforcing the urban street quality of the street and system.

### 2.4.6 SITE & BUILDING INTEGRATION

Potomac Shores Town Center is a fully-integrated Transit Oriented Development with a strong emphasis placed on coordinated pedestrian linkages. Therefore, it is important that equal attention be paid to pedestrian and vehicular circulation throughout the project. An integrated multi-use development provides greater opportunities to share facilities and infrastructure systems, thereby creating greater convenience and economies for users.

### 2.4.7 UTILITIES

Utility and service integration must be considered in the development of all parcels within subdivided blocks to allow the most effective connection to the infrastructure systems. These considerations shall include the installation of sleeves for future extension of piping, shared, and oversized utility connections between buildings and shared entry courts or service areas. Ground mounted equipment shall be screened.

### 2.5 BLOCK BY BLOCK DEVELOPMENT

Buildings internal to the Town Center are organized by general use groups within the ten major blocks. The planned uses are within the Town Center's Block Diagram. Within many of the blocks, multiple use groups are permitted to promote integrated, multi-use design principles.

As depicted on the Town Center Illustrative Plan, each use group will have a specific targeted mix of density and orientation of uses. This section provides the guiding principles for each use group as well as the general architectural guidelines. The Potomac Shores Town Center Development Code Table provides a matrix tabulation of the land use performance requirements - density, setback, height, coverage and parking standards - for block development while the ensuing text



## 2.0 ARCHITECTURAL & DEVELOPMENT CODE

provides block-by-block guidelines for the designated Land Use. With these standards serving as a point of departure for the architectural review process, the DRC shall be responsible for final decisions related to architectural design.

The following pages and Development Code Table define on a block-by-block basis specific parameters to proposed development within the Town Center Special Use Permit area. Please note that the sequencing of the block numbers does not follow a proposed phasing of the development.

Parks and usable public open spaces are planned into an integrated network of open spaces, with specific areas allocated to open space—refer to the Open Space Illustrative Plan on page 65, and preliminary conceptual plans on page 66 for specific details.



*Perry Cabin, MD*



*Figure 2.1: Block Diagram*

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2.0 ARCHITECTURAL & DEVELOPMENT CODE

POTOMAC SHORES TOWN CENTER DEVELOPMENT CODE TABLE

Block	Approx. Size (acres)	Maximum Density (see note 1)	Land Use Designation	Projected land use	Minimum Building Setback			Minimum Lot Dimensions		Building Height		Sidewalk Width	
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.
1	30	50 DU 320 DU	LDR MDR B-1	Single family detached Townhomes/Duplexes Golf	20	5	15	40	80	N/A	35	5	5
2	21	1.25FAR	O(H) B-1 UDR	Hotel Retail, Golf, Commercial Recreation, Office Multi-Family	0	0/5	0	20/40	70	N/A	N/A	N/A	N/A
3	21	250DU 1.25FAR	O(H) O(H) O(H) B-1	Townhomes, Multi Family Office Hotel Retail	0	0	0	N/A	N/A	15	100	5	5
4	11	1.25FAR	O(H) B-1	Office, Parking Retail	0	0	0	N/A	N/A	30	100	6	6
5	23	1500 0.4FAR	HDR UHDR B-1	Townhomes/Multi Family Multi Family Ground Floor retail	0	0/10	0/10	20 / N/A	70 / N/A	25/25	50/150	6	6
6	8	0.65FAR	O(M)	Office, Parking Retail	0	0	0	N/A	N/A	15	100	6	6
7	11	2.0FAR	O(H)	Office, Retail	0	0	0	N/A	N/A	30	150	6	6
8	36	2.0FAR	O(H)	Office, Retail	0	0	0	N/A	N/A	30	150	6	6
9	14	N/A	UDR	Community Recreation	15	15	15	N/A	N/A	N/A	45	5	5
10	42	N/A	B-1	Golf, Commercial Recreation Open Space	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
11	13	N/A	O(S)	Roads, Stormwater Management Open Space	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A	5

Notes

- The total number of units identified in this column exceeds the 1833 total proposed to the town center to provide flexibility within the blocks. In no event will the total number of residential units in the town center exceed 1833.
- Actual lot coverage to be determined with each site plan
- The parking requirements for each use shall be as established in the Town Center Parking Assessment
- Minimum sidewalk widths shall apply to those roads shown on the SUP plan. Alleys, service drives or other travel ways shall not require sidewalks.
- Rear-loaded alley access townhomes in blocks 1 and 5 with garages shall be set back from edge of alley pavement either less than 6' or more than 18'
- Townhome buildings (sticks) shall be separated from other townhome buildings by at least 15'.
- For blocks 4,5,7,8 the minimum FAR is required to be at least 25% of the maximum FAR

## 2.1 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 1

### Urban Framework

Block 1 is the lowest density residential neighborhood within the Town Center, with the potential for single family detached, semi-detached, and attached homes. The block establishes a pattern of increasing density between the Golf Course and the Town Center core. Driveways shall be required for all freestanding houses, but must be designed in a way that minimizes the impact of parked cars on private lots. Townhouses may provide parking in the ground floor of the building ("tuck under"), in attached garages, in detached garages and in driveways behind the house.

### Street Network

Block 1 is primarily a network of narrow neighborhood streets. An entry street provides direct access from Potomac Shores Parkway and a main spine connects to the Town Center. Private alleys provide access for all rear-loaded townhouses. The street and block pattern is aligned to enhance view corridors towards golf. A golf cart path will cross through the neighborhood to access the driving range from the Golf Clubhouse. The grid shown in the block plan is for illustrative purposes only; actual street grid will be determined in the final site plan submittal.

### Building Characteristics

- Buildings are encouraged to incorporate brick or other indigenous natural materials.
- Stoops, porches, stairs, bay windows, etc are desired features and permitted encroachments in the front setback.
- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Dormers, chimneys, and any other items extending above the roofline

should be of the same color and materials used in the building's façade.

- Landscaped courtyards and/or lawns are encouraged between building entrances and street edge.
- Except for single family detached buildings, garages shall be at the rear of buildings. Surface parking lots shall be screened.
- Townhomes with included garages shall have a setback that is either less than 5' or greater than 18' from edge of alley pavement. Additionally, townhomes should have a minimum 10' setback between sticks and other townhouse buildings.

### Public Spaces

A minimum 1 acre of landscaped public space which may include natural area restoration, trails, pocket parks shall be incorporated into the block system. The majority of the perimeter fronts on a golf course. Block 1 also connects to a trail system leading into the Town Center core, and the surrounding open space.



Keyplan



Block Plan Illustrative

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Block	Approx. Size (acres)	Max. Density	Land Use Designation	Projected Land Use			Min. Building Setbacks			Min. Lot Dims			Building Height			Sidewalk Width								
				Single Family Detached	Townhomes/Duplexes	Golf	Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.	Min.	Max.	Min.	Max.					
1	30	50 DU 320 DU	LDR MDR B-1	0	5	15	0	0/5	NA	NA	NA	20/40	40	80	NA	NA	NA	35	50	NA	5	5	NA	NA

Development Code Table

## 2.2 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 2

### Urban Framework

Block 2 is designated for hospitality and recreational uses. The hotel is intended to be a signature building in the Town Center, maximizing river views. Recreational activities centered around the spa and tennis courts will establish this area as a major destination for residents, their guests, and visitors to Potomac Shores. Residential uses, such as detached single family homes and attached multifamily may be included, and may be managed by the Hotel. Vertically integrated mixed-use as well as temporary 'pop-up' retail may also be integrated into the areas around the Town Green.

Surface lots may be used to provide for the particular needs of the golf and hotel facility provided screen walls and/or hedges are designed to minimize views of cars from the public realm. Hedges and walls should be designed to be complementary to the buildings, and uses they serve.

### Street Network

Block 2 is accessed by an entry street off Potomac Shores Pkwy, creating a transition to slower moving traffic and a pedestrian-oriented streetscape. Block 2 will also have internal circulation within the resort area, providing access to the resort cottages as well as to the proposed canoe launching area on Powell's Creek to the north.

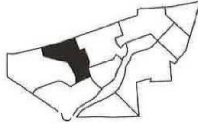
### Building Characteristics

Buildings in Block 2 should be designed for specific uses that will help enliven the Town Center as a unique riverfront destination. The architecture of the Hotel should evoke the casual elegance characteristic of the Tidewater region and traditional waterfront resort communities.

- Buildings are encouraged to incorporate brick or other indigenous natural materials.
- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Dormers, chimneys, and any other items extending above the roofline should be of the same color and materials used in the building's façade.

### Open Space

Block 2 open space includes the golf course at the northern edge, the existing cemetery to be preserved in the Hotel grounds, the Town Green, and the adjacent square to the west of the Town Green that may include a building for assembly and events such as weddings.



Keyplan



Block Plan Illustrative

Block	Approx. Size (acres)	Max. Density	Land Use Designation	Projected Land Use	Min Building Setbacks			Min Lot Dims		Building Height		Sidewalk Width	
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.
2	21	1.25 FAR	O(H) B-1 UDR	Hotel Retail, Golf, Commercial Recreation, Office Multifamily	0	0	0	NA	NA	15	100	5	5

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Development Code Table



## 2.3 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 3

### Urban Framework

Block 3 is the heart of the Town Center and provides opportunities for a mix of activities. Around the Town Green and the train station, it will accommodate several commercial and residential uses. The Town Green to the north and the VRE station to the south anchor the bluff side of the block. Pedestrian-oriented retail streets in the heart of the block evoke the casual atmosphere of established Tidewater settlements such as Norfolk, VA, Annapolis, MD, and Edenton, NC. Entertainment uses such as a movie theater are planned for this block, as well as a potential grocery store midway in the block on the western side. Attached residential uses is planned to be located on the northern end of the block along the Hotel entry road.

The massing, intensity, and pattern of development is intended to activate the streets in proximity to the hotel meeting center and provide for pedestrian-oriented streetscapes while at the same time provide for appropriate vehicular circulation to the VRE station and associated parking.

Pedestrian access in the form of well-detailed pathways are provided to circulate from parking areas to the streets. These access ways should be detailed like the sidewalks along the streets to provide a consistent pedestrian experience. Between individual buildings, pathways may be constructed to provide mid-block pedestrian circulation and access to the street from parking areas.

### Street Network

Block 3 has two points of access, the hotel entry road and the road accessing the train station. Two linkage streets run parallel to each other in a north/south direction. These streets extend through the southern portion of the Town Center and are designed to link the Town Center with adjacent properties.

Service access for Block 3 should be made via the north-south streets and screened from view. Parking should be generally located in the center of the block, screened from view from public streets and private streets leading to the hotel or VRE area with buildings and/or appropriate screen walls and/or landscaping. On-street parking, both angled and parallel should also be available.

### Building Characteristics

Buildings and structures in block 3 are eclectic in style, and span a mix of uses from residential and commercial to public-use buildings like the VRE head house. Architecture should be reflective of the Tidewater region and demonstrate this mix with varied facade and 'storefront' designs.



Keyplan



Block Plan Illustrative

Block	Approx. Size (acres)	Max. Density	Land Use Designation	Preferred Land Use			Min Building Setbacks			Building Height		Sidewalk Width								
				Townhome/Multifamily	Office	Hotel	Remil	Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.				
3	21	250 DU 1.25 FAR	HDR O(H) O(H) B-1	0	0	0	0	0	0	0	0	0	0	0	25	50	5/6	6	6	6

Development Code Table

### 2.3 ARCHITECTURAL & DEVELOPMENT CODE

- Buildings are encouraged to incorporate brick or other indigenous natural materials.
- Special attention should be paid to the facade composition of gable ends, where appropriate openings should be centered on gable.
- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Roof penetrations, such as vents and stacks, wherever possible, shall be placed on rear roof slopes, or in the case of flat-roof buildings, screened from street-level views with parapets and other screening devices.
- Exterior building lighting is to be coordinated among buildings.

#### **Open Space**

Public space in Block 3 includes the train station square; and a network of wide sidewalks to accommodate informal pop-up activities. The train station square shall be designed to encourage public use and enjoyment of the space and should ensure flexibility, and be able to accommodate performances. The train station square is also the terminus of public promenade which stretches south along the bluff through block 5 to block 6. Landscape architecture should be in harmony with the architectural expression of the surrounding buildings.

The space directly in front of the VRE head house should also be designed for multiple functions. It will be the drop-off area for the commuter train, but also may serve as overflow area for events.

The public realm should be inspired by first-rate shopping streets and include ample sidewalks, appropriate furnishings, high-quality lighting, and appropriate landscaping.

POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 24



*Rendering or Photo*



*Rendering or Photo*

## 2.4 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 4

### Urban Framework

Block 4 is composed of employment offices within walking distance to Town Center retail and residential areas. Adjacency to the Town Center commercial core and to Potomac Shores Parkway make this an appropriate location for mid-rise office buildings desiring a great frontage and address within a campus-like arrangement. Responding to the scale of the surrounding environment, the office buildings are low to mid-rise. Buildings along the northern edge fronting on the Hotel entry road should be lower and more pedestrian in scale than development at the southern end of the block.

Between individual buildings, pathways may be constructed to provide mid-block pedestrian circulation and access to the street from parking areas. The impact of parked cars on the public realm should be minimized through screening wherever parking is directly adjacent to a street. Block 4 will likely be phased, with early development relying on surface parking lots and subsequent higher density uses necessitating multi-level parking garages.

### Street Network

Block 4 is bounded to the west by Potomac Shores Parkway; to the north by the Hotel entry road. The block will have an internal street system to allow for direct connections to retail and Potomac Shores Parkway. Location of internal streets and/or alleys will be determined based on final development plans.

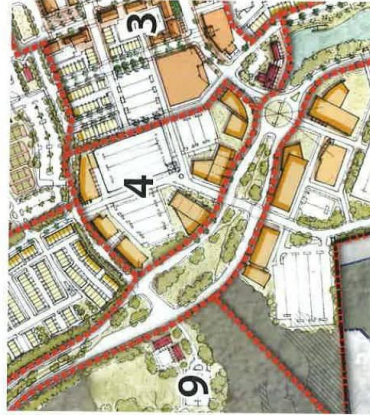
### Building Characteristics

Building height and massing should reflect the transitional scale of the overall town plan.

- Planting and/or hedges appropriate to the urban environment shall be employed along frontages of Potomac Shores parking.
- Roof penetrations, such as vents and stacks, wherever possible, shall be placed on rear roof slopes, or in the case of flat-roof buildings, screened from street-level views with parapets and other screening devices.
- Landscaped courtyards and/or lawns are encouraged between building entrances and street edge. Parking or drive aisles are not permitted between the front of a building and the street.
- Exterior building lighting is to be coordinated among buildings.
- For additional notes, please refer to the building characteristics section on blocks 7 and 8.

### Open Space

Block 4 open space consists of the landscape buffer along Potomac Shores



Block Plan Illustrative

PG 25

Block	Approx. size (acres)	Max Density	Land Use Designation	Projected Land Use	Min Building Setbacks			Min Lot Dims		Building Height		Sidewalk Width	
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.
4	11	1.25 FAR	OH-E-1	Office/Parking Retail	0	0	0	NA	NA	30	100	6	6

Development Code Table



## 2.5 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 5

### Urban Framework

Block 5 is a waterfront parcel composed of townhomes, mid-rise, and high-rise multifamily, with parking serving the VRE and surrounding retail. Retail uses may be included at ground level. The neighborhood is bounded by the scenic Potomac River to the east and a park system to the west, providing views in either direction. Its proximity to the retail center, the marina, a waterfront promenade, a park and trail system, and public transportation create an ideal residential setting.

Block 5 consists of high-density multi-family residential buildings. Building lots may be wide or narrow (along the frontage), but should present a continuous facade with minimal front yards. Between individual buildings, a pathway may be constructed to provide mid-block pedestrian circulation and access to the street from parking areas. Within 150' of the bluff edge, building masses over 40 feet in height should be oriented generally perpendicular to the shoreline and may not occupy more than 50% of the bluff frontage. The impact of parked cars on the public realm will be minimized through the requirement that all off-street parking must occur in the rear of the lot, generally screened from street level.

### Street Network

Block 5 contains a circuit of public streets bordered on the east by a linear promenade park along the river's edge. The streets are pedestrian-focused, highly landscaped, multi-purpose environments safely mixing together people, bicycles, cars, stormwater management and utility infrastructure.

### Building Characteristics

High rise multifamily to 150' in height in block 5 should be executed in a language that is complementary to the informal Tidewater character, while recognizing the unique material and massing issues present in this type of construction.

- Roof penetrations, such as vents and stacks, wherever possible, shall be placed on rear roof slopes, or in the case of flat-roof buildings, screened from street-level views with parapets and other screening devices.
- Landscaped courtyards and/or lawns are encouraged between building entrances and street edge.
- Exterior building lighting is to be coordinated among buildings.

### Public Spaces

A network of interconnected open spaces and neighborhood parks provides residents with venues for active and passive recreation with specific areas allocated to open space- refer to the Open Space Illustrative Plan on page 65. This includes a linear landscaped area adjacent to the east side of Potomac Shores Rd. Block 5 has direct access to the community pond to the west and the promenade on the Potomac River frontage to the east.



Block Plan Illustrative

Block	Approx. Size (acres)	Max. Density	Land Use Designation	Projected Land Use	Mir. Building Setbacks			Min Lot Dims			Building Height			Sidewalk Width		
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.	Min.	Max.	
5	23	1500 DU 0.4 PARK	HDR UHDR B-1	Townhomes/Multifamily Multifamily Ground Floor Retail	0	0/10	10	10	20/NA	70/NA	25/25	50/150	6	6	6	6

Development Code Table



## 2.6 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 6

### Urban Framework

Block 6 is planned as a low-rise, area that anchors the southern edge of the Town Center integrating multiple uses with commercial and office development along the waterfront. The block has a small retail/entertainment area called "Rivershore District" to serve Potomac Shores residents, their guests, and visitors. The block provides access to the proposed marina, as well as the existing restaurant to the north.

Ground floor commercial and retail space will be located along Potomac Shores Dr. The massing and intensity of development along this street is intended to foster successful retail by promoting activity along the street and retail-supporting office above. This block provides space for several low-density buildings. Lot sizes may vary. Front setbacks in this block should be minimal - buildings should be placed immediately at the back of the sidewalk. Parking must be provided behind buildings along Potomac Shores Dr and on-street.

### Street Network

Potomac Shores Dr provides access to Block 6, and links to existing roads that connect over the train tracks to the existing restaurant on the water and a potential future marina.

### Building Characteristics

Buildings in Block 6 are low-density buildings (commercial retail with commercial office space above).

- Buildings are encouraged to incorporate brick or other indigenous natural materials.
- Special attention should be paid to the facade composition of gable ends, where appropriate openings should be centered on gable.

- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Roof penetrations, such as vents and stacks, wherever possible, shall be placed on rear roof slopes, or in the case of flat-roof buildings, screened from street-level views with parapets and other screening devices.
- Exterior building lighting is to be coordinated among buildings.

### Open Spaces

The open space network of Block 6 connects to and expands upon the series of open spaces and neighborhood parks along the waterfront edge of Block 5 and is the terminus of the promenade. For specific areas allocated to open space - refer to the Open Space Illustrative Plan on page 65. In block 6 this includes a linear landscaped area adjacent to the east side of Potomac Shores Rd.



Keyplan



Block Plan Illustrative

PG 27

Block	Approx. Size (acres)	Max Density	Land Use Designation	Projected Land Use	Min Building Setbacks			Min Lot Dims		Building Height		Sidewalk Width	
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.
6	8	65 PAR	O(M) B-1	Office, Parking Retail	0	0	0	NA	NA	25	100	45	6

Development Code Table

## 2.7 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 7

### Urban Framework

Block 7 is an employment center composed of higher density office buildings at the crossroads between Potomac Shores Dr and River Heritage Rd. The transition to higher intensity development should continue with office buildings in this block. Between individual buildings, pathways may be constructed to provide mid-block pedestrian circulation and access to the street from parking areas. Typical buildings in this block will be 6-8 stories.

Block 7 will likely be phased, with early development relying on surface parking lots and subsequent higher density uses necessitating multi-level parking garages.

### Street Network

Block 7 is bounded by River Heritage Rd to the north and to the east by Potomac Shores Dr. Both streets provide access to structured parking behind the office buildings in Block 7.

### Building Characteristics

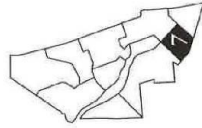
Building height and massing should reflect the "middle-ground" of the overall town plan.

- Planting and/or hedges appropriate to the urban environment shall be employed along frontages of parking.
- Landsaped courtyards and/or lawns are encouraged between building entrances and street edges, ground floor entrances should be located on street and clearly marked. Parking or drive aisles are not permitted between the front of a building and the street.
- Exterior building lighting is to be coordinated among buildings.

- Material should vary, with use of brick, wood and metal encouraged.
- Roofs are encouraged to have strong articulation: big overhangs, brackets, stepped back upper level floors, double height spaces, etc.
- Fenestration should be generous, and screening devices and operable windows are encouraged.
- Massing should vary and integrate with residential uses nearby.

### Open Space

Block 7 open space consists of a landscape buffer along the southern edge of the block.



Block	Approx. Size (acres)	Max. Density	Land Use Designation	Projected Land Use	Min. Building Setbacks				Min. Lot, Driv. Building Footprint		Min. Sidewalk Width		
					Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.
7	11	2.0 FAR	O/H	Office, Retail	0	0	0	NA	NA	30	150	6	6

Development Code Table

PG 28

## 2.8 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 8

### Urban Framework

Block 8 is an employment center composed of higher density office buildings at the crossroads between Potomac Shores Dr and River Heritage Rd. The transition to higher intensity development should continue with office buildings in this block. Between individual buildings, pathways may be constructed to provide mid-block pedestrian circulation and access to the street from parking areas. Typical buildings in this block will be 6-8 stories, however, buildings which do not serve as a primary use may be a minimum height of 15'.

Block 8 will likely be phased, with early development relying on surface parking lots and subsequent higher density uses necessitating multi-level parking garages.

### Street Network

Block 8 is bounded to the northeast by Potomac Shores Parkway and to the south by River Heritage Rd. Both streets provide access to surface parking behind the office buildings in Block 8.

### Building Characteristics

Building height and massing should reflect the "middle ground" of the overall town plan.

- Planting and/or hedges appropriate to the urban environment shall be employed along frontages of parking.
- Landscaped courtyards and/or lawns are encouraged between building entrances and street edge; ground floor entrances should be located on street and clearly marked.
- Exterior building lighting is to be coordinated among buildings.

- Material should vary, with use of brick, wood and metal encouraged.
- Rooflines should vary and a combination of flat and sloped roofs are encouraged.
- Roofs should have strong articulation: big overhangs, brackets, stepped back upper level floors, double height spaces, etc.
- Fenestration should be generous, and screening devices and operable windows are encouraged.
- Massing should vary and integrate with residential uses nearby.

### Open Space

Block 8 open space consists of the landscape buffer along Potomac Shores Parkway and preserved open space west of the block.



Keyplan



Block Plan Illustrative

Block	Average Size (acres)	Max. Density	Land Use Designation	Projected Land Use			Min. Building Setbacks			Min. Lot Dims			Building Height			Sidewalk Width		
				Office/Retail	Other	Open	Front	Side	Rear	Width	Depth	Min.	Max.	Min.	Max.	Min.	Max.	
8	36	2.0 FAR	O(H)	0	0	0	0	0	0	NA	NA	30	150	6				

Development Code Table



## 2.9 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 9

### Urban Framework

Block 9 serves as a Recreational Center for Potomac Shores. The block is entirely dedicated to active and passive recreation space surrounded by golf, and is a private use under the designation of residential.

### Open Space

Block 9 is surrounded by open space including the golf course to the west, and forest to the north and south.

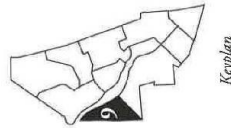
### Street Network

Block 9 is accessed directly off Potomac Shores Parkway at the western end of the Hotel entry road.

### Building Characteristics

Buildings in Block 9 should be consistent with the Tidewater character, evoking the casual elegance of the region, and following the language established in the resort core.

- Where appropriate, buildings designed in the Tidewater style, are encouraged to incorporate brick or other indigenous natural materials.
- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Dormers, chimneys, and any other items extending above the roofline should be of the same color and materials used in the building's façade.



Block	Approx. Size (acres)	Max Density	Land Use Designation	Projected Land Use			Min. Building Setbacks			Min. Lot Dims.			Building Height/ Sidewalk Width		
				Front	Side	Rear	Front	Side	Rear	Depth	Width	Min.	Max.	Min.	Max.
9	14	NA	UDR	15	15	15	NA	NA	NA	NA	NA	NA	45	5	

Development Code Table

PG 30

## 2.10 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 10

### Urban Framework

Block 10 includes a portion of the golf course and is dedicated to golf and natural open space uses, the golf clubhouse, and associated parking areas. Buildings in Block 10 are limited to golf support, waterfront activity structures such as the canoe launch.

### Open Space

Block 10 includes and is surrounded by open space: the Potomac River to the east, Powell's Creek to the north, the golf course to the west, and the hotel grounds to the south.

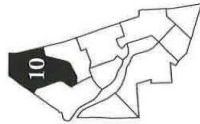
### Street Network

Block 10 is accessed off Potomac Shores Parkway directly off the Hotel entry road. It will also include internal circulation paths providing access to the canoe launching area on Powell's Creek.

### Building Characteristics

Buildings in Block 9 should be consistent with the Tidewater character, evoking the casual elegance of the region, and following the language established in the resort core.

- Buildings are encouraged to incorporate brick or other indigenous natural materials.
- In pitched roof buildings, long expanses of unarticulated roofs are discouraged, designs should incorporate dormers, hip roofs, cupolas, gables, and other architectural features.
- Dormers, chimneys, and any other items extending above the roofline should be of the same color and materials used in the building's façade.



Keplan



Block Plan Illustrative  
PG 31

Block	Approx. Size (acres)	Max Density	Land Use Designation	Projected Land Use		Min Building Setbacks		Min Lot Dimns		Building Height		Sidewalk Width	
				Golf, Commercial Recreation	Open Space	Front	Side	Rear	Width	Depth	Min.	Max.	Min.
10	42	NA	B-1 OS	NA	NA	NA	NA	NA	NA	NA	NA	NA	5

Development Code Table

2.11 ARCHITECTURAL & DEVELOPMENT CODE - BLOCK 11

**Urban Framework**

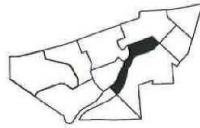
Block 11 is an open space area that includes the landscaped Potomac Shores Parkway median and roadway and a large community pond also serving as a stormwater retention basin. The Parkway landscape and pond will also serve as usable open space for the surrounding blocks. The installation of public art or entry monumentation may be included within this block.

LEGEND

1	Walking Path
2	Lawn
4	Boat Dock



Central Park Plan



Keyplan



Block Plan Illustrative

**Open Space**

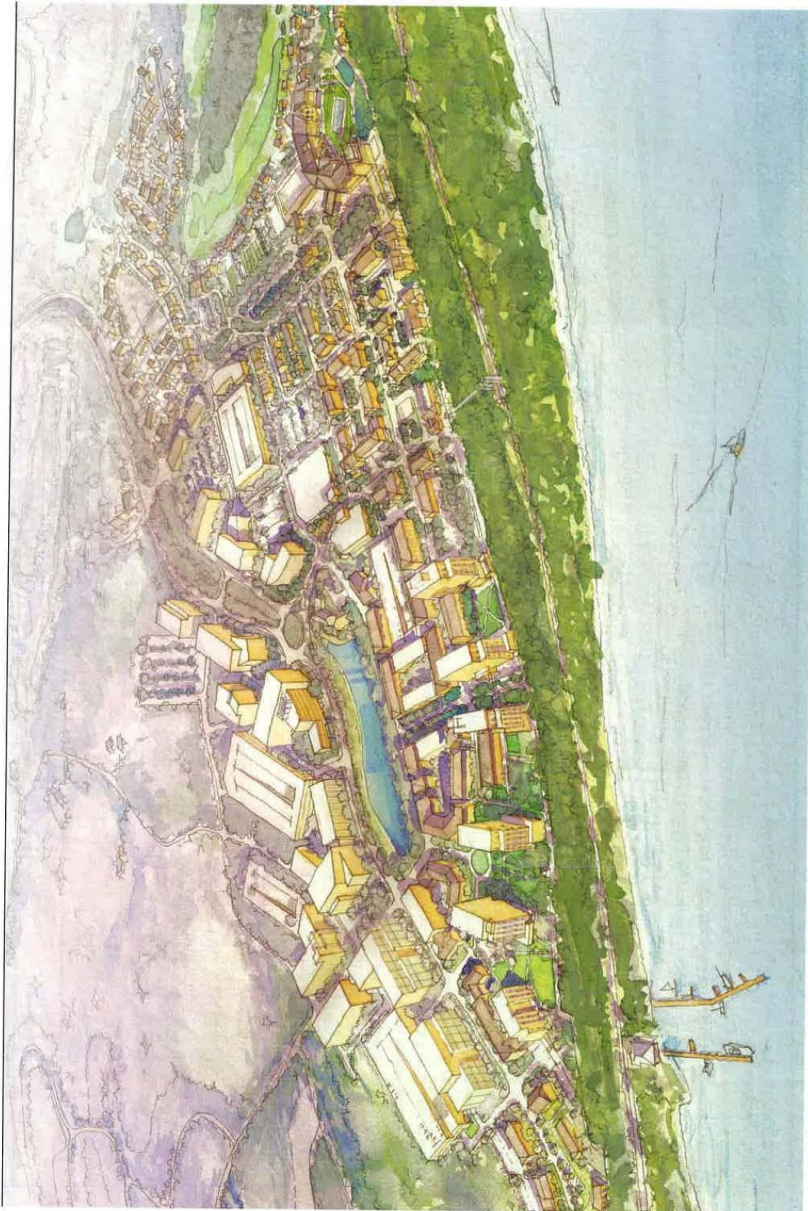
Upon arrival in the Town Center, in lieu of a 50 foot buffer external to the Potomac Shores Parkway, where there are no residential uses adjacent to the street, a wide median (average width of 100') shall be incorporated into the streetscape to enhance the arrival experience and to help reduce travel speeds as visitors approach the traffic circle, which will serve to disperse traffic to various destinations in and around the Town Center. To the south of the traffic circle, a large pond may incorporate both wet and dry portions, and will serve as a buffer between the office campus and the more residential and retail components of the Town Center. The pond feature will be bounded by linear walking parks on both sides, providing a pleasant and usable park for residents, visitors, and office tenants of the Town Center.

Block	Approx. Size (Acres)	Max. Density	Land Use Designation	Projected Land Use	Min. Building Setbacks		Min. Lot Dimensions		Building Height		Sidewalk Width	
					Front	Side	Rear	Min.	Max.	Min.	Max.	Min.
11	13	NA	OS	Roads, Stormwater Management Open Space	NA	NA	NA	NA	NA	NA	NA	5

Development Code Table



**Attachment E**  
**Design Guidelines**



PG 33

### **3.0 STREETSCAPE GUIDELINES**



POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG. 34



### 3.0 STREETSCAPE GUIDELINES

#### 3.1 MAJOR TRANSPORTATION SYSTEMS

The planned vehicular circulation system for Potomac Shores Town Center uses both public and private transportation systems to support the coordinated development of individual parcels. Three categories of streets are the backbone of the Town Center street grid: entry streets, linkage streets, and connector streets.

**Entry Streets:** Two entry streets run generally perpendicular to Potomac Shores Parkway and provide access from it into the Town Center. Each should be designed to announce arrival at the Town Center. Each of these, one public and one private, are considered major gateways to the Town Center core. Gateway streets shall be designed as transitions from the higher speed parkway to lower speed, pedestrian-oriented streets within the Town Center. As such, parking may be restricted along segments leading to Potomac Shores Parkway. Street sections may also change in such areas so as to accommodate necessary turn lanes.

**Linkage Streets:** Two streets link the Town Center core to adjacent parcels to the south and west. Linkage streets also provide the opportunity for future pedestrian and bicycle access between the Town Center and adjacent parcels.

**Connector Streets:** Three streets service the Town Center by connecting gateway streets to each other. Two provide for internal trips between the residential and mixed use areas and the other provides for service access within the mixed use area.

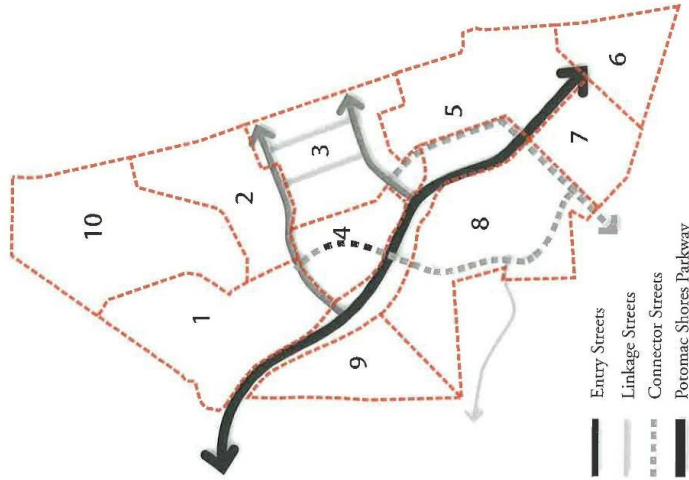


Fig. 3.1: Major Transportation Systems

### 3.0 STREETSCAPE GUIDELINES

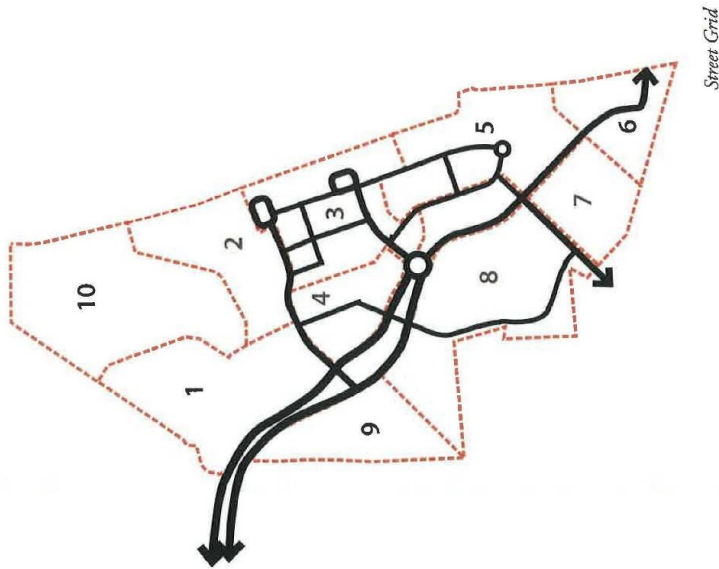
There are six specific aspects of circulation and streetscape improvements within the Town Center:

1. Town Center Public Streets
2. Private Streets & Alleys/Travelways
3. Motor & Pedestrian Courts
4. Street & Hardscape Furnishings
5. Parking Facilities
6. Public Transportation

#### 3.1.1 TOWN CENTER STREETS

Potomac Shores Parkway is part of the spine road serving the entire Potomac Shores community and the primary access road leading to the Town Center. It shall be dedicated for public use and designed to ensure capacity for adequate circulation among the range of planned land uses.

Internal Town Center streets are generally designed to include two driving lanes (two-way) and parking on one or two sides of the street. These tree-lined streets will provide an attractive address for individual buildings and convenient on-street parking. All building sites shall have access to either public or private streets or alley/travelways at the locations designated on final site plans and subdivision plats. Reasonable variations in the exact location of pedestrian and vehicular access points may be permitted, subject to the DRC concept/preliminary site plan approval and the County site plan review/approval process.



POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 36

### 3.0 STREETScape GUIDELINES

#### 3.1.2 STREET GUIDELINES

The Town Center's street guidelines are divided into two sections: Town Center Public Streets and Private Streets and Alleys/Travelways. The Illustrative exhibit shown at right identifies public and private roads.

#### 3.2 TOWN CENTER PUBLIC/PRIVATE STREETS

There are four typical street types in the Town Center:

The guidelines for the Town Center public streets are summarized in the following pages, and are intended to address the character of the streetscape. Refer to the Special Use Permit application package for additional and more specific design parameters.

The general intent for street design within the Town Center is to minimize street widths while effectively moving traffic.

The streets and sidewalks will be designed for traffic calming; features may include: Special paving materials, raised crosswalks, minimal curb radii, and separated sidewalk depressions.

#### LEGEND

- Public Streets
- Private Streets

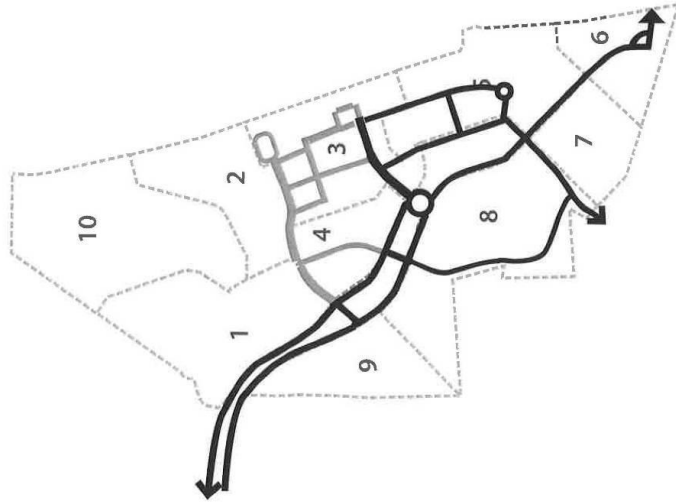


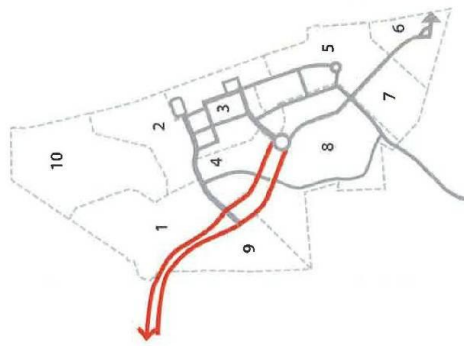
Fig. 3.2: Public / Private Streets  
PG 37

### 3.0 STREETSCAPE GUIDELINES

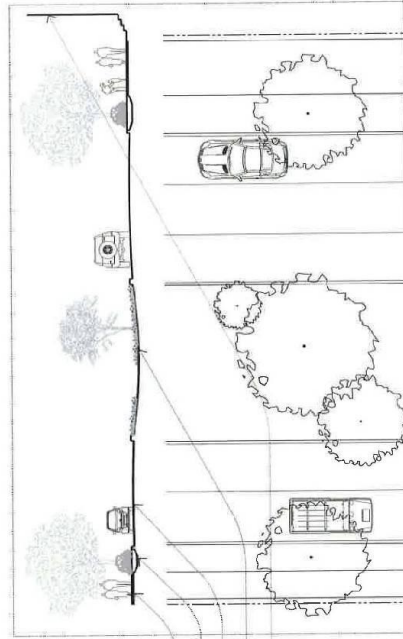
#### 3.2.1: POTOMAC SHORES PARKWAY

Potomac Shores Parkway is a public parkway with two-way traffic. This street type provides the principal vehicular access from Interstate 95 and Route 1. It is also part of the spine road that serves the entire Potomac Shores community.

- Lanes/Median: Four to six travel lanes with additional turn lanes at intersections, median width varies.
- Street locations and geometry shall comply with specifications shown on the Town Center SUP Plan.
- Bikeway: Where used, bikeways shall have a minimum width of ten (10) feet within public right-of-way.
- Sidewalks: On both sides of road. Bikeways may be installed in lieu of sidewalks, but sidewalks will not be substituted for bikeways.
- Street Lighting: Lighting will follow VDOT requirements



Potomac Shores Parkway  
POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 38



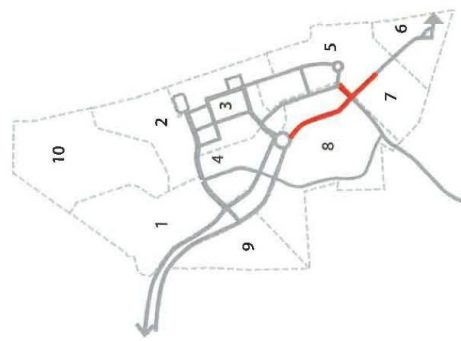
Potomac Shores Parkway Typical Section (ROW varies), NIS  
See appendix B for street sections and dimensions

### 3.0 STREETSCAPE GUIDELINES

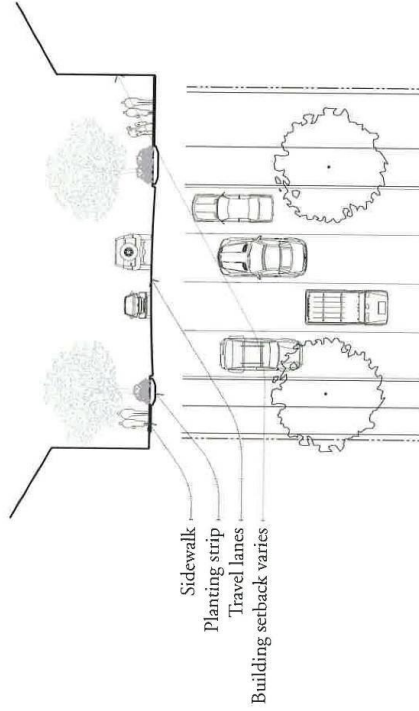
#### 3.2.2: FOUR-LANE ROAD

4-lane streets provide vehicular access to the Town Center and link to the Riverside District. These streets are meant to transition from the arterial streets to the local streets and are characterized by slowing vehicular speeds and transition to a pedestrian-oriented streetscapes.

- Street locations and geometry shall comply with specifications shown on the Town Center SUP Plan.
- Planting Zone: Any planting or streetscape element inside the public right-of-way will be subject to VDOT and PWC review and approval at the time of site plan submission.
- Tree Planting: Planting of trees will not interfere with utilities or will the planting of trees affect sight distance requirements.
- Sidewalks: On both sides of road. Bikeways may be installed in lieu of sidewalks.
- Street Lighting: Lighting will follow VDOT requirements



Collector Streets



4-Lane Collector Street Typical Section (ROW varies) NTS  
See appendix B for street sections and dimensions

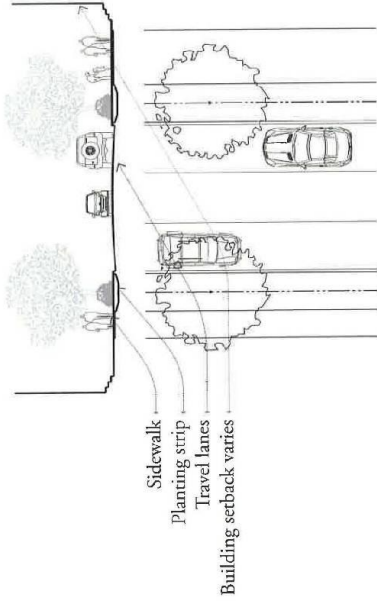
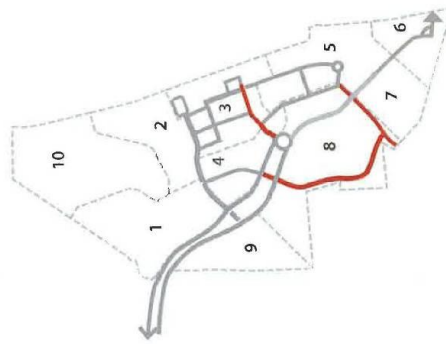
### 3.0 STREETScape GUIDELINES

- Street locations and geometry shall comply with specifications shown on the Town Center SUP Plan.

#### 3.2.3: 3-LANE ROAD

Three lane streets serve as connectors from higher volume to lower volume areas. The Cherry Hill Connector serves to connect existing Cherry Hill Rd. into the Town Center.

- Planting Zone: Any planting or streetscape element inside the public right-of-way will be subject to VDOT and PWC review and approval at the time of site plan submission.
- Tree Planting: Planting of trees will not interfere with utilities or will the planting of trees affect sight distance requirements.
- Street Lighting: Lighting will follow VDOT requirements.
- Road design may include 3 travel lanes (2/1) or 2 travel lanes w/ continuous turn lane.
- Sidewalks: On both sides of road. Bikeways may be installed in lieu of sidewalks.
- May have additional lanes for parking



3-Lane Connector Street  
POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 40

3-Lane Collector Street Typical Section (ROW, NTS)  
See appendix B for street sections and dimensions



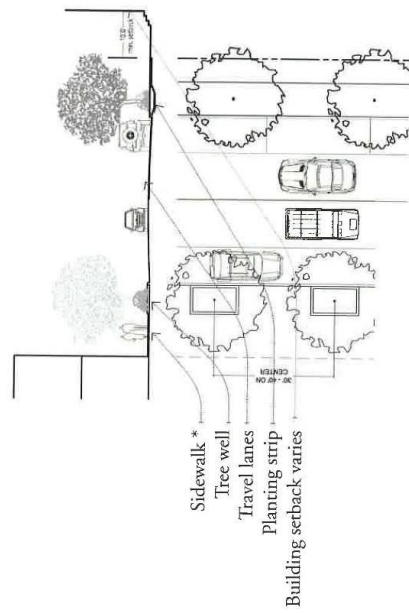
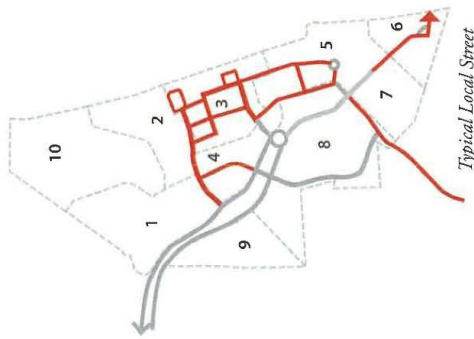
### 3.0 STREETSCAPE GUIDELINES

#### 3.2.4: TYPICAL 2 LANE ROAD

Typical streets within the Town Center and surrounding residential areas have two moving lanes with parallel parking on either side. Depending on adjacent uses, diagonal parking may be designed on one side of the street. Sidewalks, crosswalks, low speed traffic, and tree planting all contribute to a pedestrian-friendly environment. This is the most typical street, and many variations on the design will occur throughout the Town Center. Please refer to the Civil SUP submittal for specific location designs. Depending on their location and purpose, streets may be public or private. Private streets may include brick paving, and enhanced landscape and street tree planting.

Street locations and geometry shall comply with specifications shown on the Town Center SUP Plan.

- Planting Zone: Any planting or streetscape element inside the public right-of-way will be subject to VDOT and PWC review and approval at the time of site plan submission.
- Tree Planting: Planting of trees will not interfere with utilities or will the planting of trees affect sight distance requirements.
- Street Lighting: Lighting will follow VDOT requirements
- Sidewalks: On both sides of road. Bikeways may be installed in lieu of sidewalks.
- \*Sidewalk conditions, including width, treatment, etc vary according to adjacent use: resort, retail, commercial office, residential.



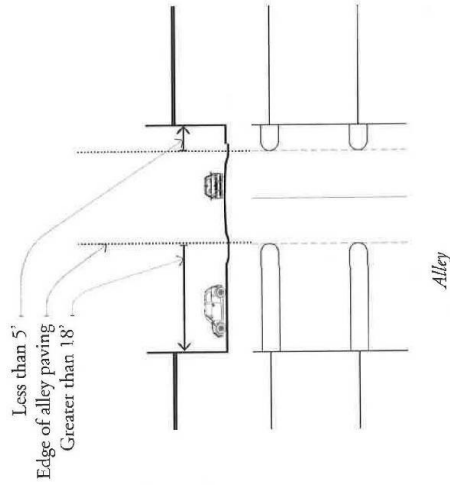
Local Street Typical Section (ROW varies) NTS  
See appendix B for street sections and dimensions

### 3.0 STREETSCAPE GUIDELINES

#### 3.2.6: ALLEY

A system of alleys and travelways may be constructed within the interior of Town Center. They should be generally parallel to the orthogonal street system and provide internal block access for residents, customers, and service. Alley/travelways may provide access between buildings and associated parking, separate but related uses within the same block, or access to rear loaded residential uses. They can be constructed within the interior of individual building sites in order to connect the proposed building(s), uses, motor courts, surface parking lots, and/or parking structures to a street in accordance with the following standards:

- Street to conform to Prince William County standards for pavement design.
- Private street locations and geometry shall be in accordance with final engineering and site plan.
- Paving materials to match those of adjacent streets, provided that specialty pavers are permitted subject to approval of the DRC.
- Building and garage setbacks in residential areas should be either <5' or >18' to allow for off street parking
- In the event that entry to units is from alley, a dedicated sidewalk of 5' independent of the roadway shall be provided for pedestrian use.





### 3.0 STREETScape GUIDELINES

#### 3.3 MOTOR COURTS

While the development implies that typical pedestrian/vehicular interface occurs at the street, certain uses within the Town Center (such as the hotel, and VRE Station) may dictate specialized access arrangements. These buildings may require multi-purpose motor and pedestrian courts that combine both (1) a "plaza" or "courtyard"-like entrance from the adjacent street, and (2) a visitor or handicap drop-off area at the building's entry or its porte-cochère. The DRC shall review final design solutions for each motor/pedestrian court for scope, placement, materials, and configuration.

A limited number of short-term visitor/delivery/handicap parking spaces may be provided adjacent to the motor court, which shall be designed as dual-purpose pavements to ensure safe and attractive pedestrian use. A drop-off area shall be integrated with the motor court in close proximity to the building entryway. Structured parking may be accessed directly from motor court areas. Motor courts shall not be used for through traffic or long-term parking.

Materials shall indicate to motorists that they have entered a pedestrian area. One or more materials may be used as the dominant material for motor court paving, including:

- Integrally-colored concrete with special accent bands.
- Precast concrete unit pavers.
- Rustic terrazzo (integral exposed aggregate concrete).
- Clear or stress finished concrete, with special jointing patterns and accent bands of another material.

- Stone and/or brick pavers
- Imprinted paving patterns, such as stamped concrete.
- Asphalt

The color of vehicular/pedestrian court paving shall complement and be a part of both its immediate streetscape and the overall family of colors within the Town Center. Accent colors that directly relate to the individual building lobby or skin materials shall provide the linkage between each motor court and its immediately visible surroundings. Bituminous paving, unless imprinted, shall not be used as the dominant material.

Header curbs, varied paving treatment, or another method of separating vehicular travelways from pedestrians shall be used at the edge of all motor court vehicular driving surfaces. Header curbs, if used, shall be constructed of concrete or stone. Bituminous curbs are not permitted.

### 3.0 STREETSCAPE GUIDELINES



*Rendering or Photo*

#### 3.4 PARKING FACILITIES

The Potomac Shores Town Center, which is rich in variety of design, use, and function, requires a parking solution that is also varied. The Town Center includes on-street parking, structured parking, and surface parking areas. This combination of parking options creates an authentic urban area that is varied and eclectic.

Both structured and surface parking shall be designed so that they are screened from the street-level, pedestrian experience. No surface parking area will be readily visible along public streets or private streets leading to the hotel or the YRE/Town Commons with the exception of the handicap and short-term parking provided in the VRE motor court. Surface parking may only be provided along the other streets if it is landscaped and designed in a way that screens it from the street-level, pedestrian experience.

#### 3.4.1 SURFACE PARKING AREAS

For residential uses, surface parking is encouraged in rear yards. Where surface parking fronts the street, screening must be provided.

Access locations from the street to the surface parking lot, landscaping, screen wall design and sidewalk locations must be approved by the DRC. Chapter 4 highlights additional landscaping requirements. Interior parking shall provide adequate turnaround areas for emergency and delivery vehicles. Individual off-site parking spaces within commercial areas shall not have direct individual access to any public street other than for parallel parking.

All surface parking lots shall have interior and perimeter planting areas



*Rendering or Photo*

POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
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### 3.0 STREETScape GUIDELINES

and quantities of trees following Design and Construction Standards Manual and as otherwise modified/waived. Unless otherwise modified by the DRC, there shall be a 10-foot minimum buffer between the rear of buildings and the surface parking. Within this buffer there shall be a minimum 5-foot landscape planting buffer adjacent to the building with a sidewalk (5-foot minimum width) adjacent to the parking area. In the event that Block 8 is developed to be inward facing or partially inward facing, the entrance to commercial uses do not require a 5-foot landscape planting buffer between the building and surface parking.

Where applicable, at-grade parking areas, as well as medians, will be graded to ensure the preservation of existing vegetation as close as possible to the edge of the parking lot.

#### 3.4.2 PARKING STRUCTURES

Freestanding and integrated parking decks will be employed at strategic locations throughout the Town Center to accommodate the parking needs of the mixed uses. Parking structures, like other buildings within the Town Center, shall be thoughtfully designed within context of other buildings within the Town Center commercial core. Their design and siting shall respond to pedestrian and vehicular circulation patterns and to site utility connections. The DRC and Prince William County must approve access locations from the street to structured parking.

Parking structures shall be designed to blend into adjacent architecture and appear as inconspicuous as possible. Screening of a parking structure may be accomplished by either lining it with residential and/or nonresidential uses along the block or street face, applying ornamental building treatments, rooflines, gradually sloping earth berms, new plantings, and/or preservation of existing trees, where possible. Freestanding parking structures shall reflect exterior architectural elements, including color, materials, and landscaping that is compatible with the surrounding structures. Exposed portions of integrated garages

shall be complementary in materials and color to adjoining buildings.

Parking structures shall not front public streets or private streets leading to the hotel or the VRE Town Commons area unless that exposed parking structure façade is above the second story and is designed and architecturally treated to resemble the façade of an occupied building. Entrances to integrated garages along these streets are permitted.

When the placement of a freestanding parking structure creates a space between itself and the immediately adjacent building(s) it is intended to serve, a 20-foot minimum separation between the structures shall be provided (allowing for perpendicular connections). A 6-foot wide sidewalk shall be provided within the separation. A 12-foot wide landscape/planting area shall be provided within and along the length of the 20-foot wide setback.

#### 3.4.3 ELEVATORS

Elevators shall be provided in all structures with more than three levels and shall be visible for safety and located for convenient access into pedestrian circulation systems.

#### 3.4.4 RAMPS

All parking garage facades should have a horizontal composition, where ramps occur on perimeter of the building, diagonal lines should be concealed.

#### 3.4.5 GRAPHIC SYSTEM

A common Graphic System shall be used at each level within continuous parking structures to simplify orientation for ingress and egress.

### 3.0 STREETSCAPE GUIDELINES

#### 3.4.6 ACCESSIBILITY

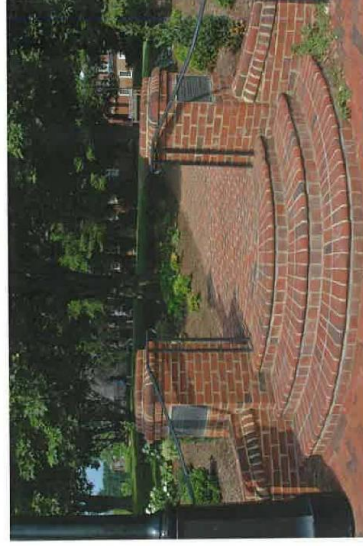
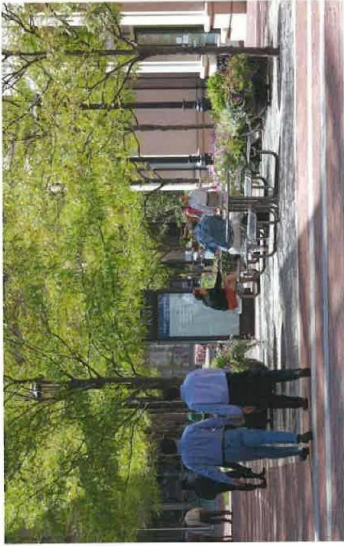
All requirements for barrier-free handicap access including the guidelines contained in the Americans with Disabilities Act (ADA), shall be observed.

#### 3.4.7 TEMPORARY SURFACE PARKING

Temporary surface parking areas may be located in any block. These temporary surface parking areas must be hard surfaced, i.e. asphalt. If a temporary surface parking area is to remain for more than twenty-four (24) months, then there will be a higher standard of screening for any edge butting completed portions of the Town Center per the DRC. The screening may include multiple beds with clustered evergreen trees and shrubs, and other landscape elements. DRC approval is required for all temporary surface parking areas. Paving requirement does not apply to parking for construction staging.

#### 3.5 PUBLIC TRANSPORTATION

Bus stops and locations for other forms of public ground transportation transfer points may be located within the Town Center in locations to be determined in final site plans and must be approved by the County and in consultation with the PRTC. The bus stop shelters may be specified and coordinated with the character of the adjacent architecture and streetscape furnishings.



### 3.0 STREETSCAPE GUIDELINES

#### 3.6 PEDESTRIAN CIRCULATION

The Town Center is planned as a pedestrian-friendly environment favoring traditional neighborhood design principles in which residents, the workforce, and visitors have equal access to amenities, goods, and services.

##### 3.6.1 OPEN SPACE

Incorporating passive park features and urban landscaped spaces, open space will establish the center of outdoor activity for residents, shoppers, tourists, and the town's workforce. Pedestrian access to the commons and riverfront views will be aligned with important lines of sight within the Town Center. Complementary landscaping will be organized to accentuate, not block, views of major buildings and public spaces from street level. The public access riverfront extending from the Town to the southern retail district may be accented with low concrete, brick or stone site walls, and a specialty asphalt paver to provide a distinctive character to the promenade.

##### 3.6.2 SIDEWALKS

Sidewalks shall be provided in the Town Center along all streets. Private walks shall be provided by individual developers to connect to the existing/proposed sidewalks, motor courts, buildings, and civic spaces. Sidewalks along streets within residential blocks shall be a minimum of five (5) feet wide and shall generally match the public sidewalks in scoring patterns and materials. Residential entry areas shall be connected to the commercial area sidewalks, which shall be designed to appropriate, but varying widths. Covered walkways may be provided by individual developers at building entries. Connections shall also be provided to

buildings and their parking lots or structures. Bituminous paving shall not be used in sidewalk areas. Traditional brick pattern sidewalks may be designated for certain streets that are to have brick vehicular paving.

##### 3.6.3 CROSSWALKS

Street crosswalks shall be provided across all public streets, private vehicular access ways and service drives located between surface parking areas and building entrances. Crosswalks shall provide both clues to the pedestrian and driver to promote traffic calming. Crosswalks shall be at least eight (8) feet wide and located at all major crossing points on the Town Center streets, access drives, motor courts, service drives, and parking lots. They may shall be denoted by contrasting paving materials such as vehicular brick or stamped concrete, or appropriate white elastomeric paint striping. For consistency, a common vocabulary of complementary crosswalk materials and colors will be adopted and implemented by the DRC.

##### 3.6.4 TRAILS

Trails and related forms of pedestrian access will be provided as appropriate within the Town Center through public spaces and common areas, forming a circulation system connected to the traditional public infrastructure sidewalks.

### 3.0 STREETSCAPE GUIDELINES

#### 3.7 LIGHTING

A coordinated hierarchy of lighting sources and intensities is required to maximize the quality of the nighttime environment in the Town Center. This hierarchy shall respond to the relative building heights and architectural features. A white-colored light source shall provide an appropriate foot candle level of illumination that satisfies industry standards. Lighting design criteria shall be developed and implemented by the DRC:

- Town Center Streets
- Private Streets, Alleys, and Access Drives
- Natural Open Spaces and Trails
- Motor/Pedestrian Courts and Plazas
- Structured Parking

Streets, alleys and access ways within the Town Center shall be illuminated using a controlled light distribution pattern. A lighting plan shall be prepared with the public improvements plan.

In residential areas and areas immediately adjacent to these areas, minimal, unobtrusive lighting is recommended. Use of shielded fixtures is required, and where necessary, lighting should be downshielded to eliminate glare. Generally, uplighting of trees is discouraged. Building and area lighting should be designed to minimize unnecessary illumination of neighbouring uses.

Pedestrian and landscape areas shall have lighting in pedestrian plazas, pedestrian connections, and seating areas to provide reasonable levels of nighttime visibility. Accent lighting may be used at special locations such as stairs, entry signage, and sculpture.

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#### 3.8 STREET & HARDSCAPE FURNISHINGS

This section focuses on creating appropriate streetscape and site furnishings with similar color, scale, and materials to visually unify the Town Center. The palette of materials and color for all streetscape and site elements must be approved by the DRC.

A variety of coordinated street furnishings or hardscape improvements are strongly encouraged throughout the Town Center. The final selection of the palette of materials will be made by the DRC prior to commencement of construction of first phase public improvements. The following general guidelines shall apply:

- Bollards may be used in a number of applications, provided that they are consistent with adjacent landscape space and building scale. Concrete and metal are acceptable materials. No fiberglass or wood materials shall be used.
- Where used, pole-mounted lighting sources shall have a controlled cutoff reflector and shall be coordinated with and complementary to public street lighting. Light sources shall produce a white light. Colored lenses will be generally prohibited. Lighting shall be positioned and/or screened to minimize light spillover to adjacent sites and communities.
- Sculpture may be used but shall be appropriate in scale, form and texture. It shall not produce noise (other than that can be generated from moving water) or contain flashing or moving lights. Fountains and water features may be appropriate in pedestrian/motor court and plaza areas.



### 3.0 STREETSCAPE GUIDELINES

#### 3.8.1 FENCING AND SITE WALLS

Fencing is permitted at residential areas and corporate office building sites, but will be discouraged within retail areas. In general, use of walls, hedges and shrub massings are preferred screening solutions. All fencing shall be reviewed on a case-by-case basis and is at the sole discretion of the DRC. Iron fences in a traditional picket style are allowed, and may be combined with a low stone or brick wall. Chain link and wood fencing is not allowed. Metal mesh fencing may be allowed to screen service areas not visible from public areas.

Brick, lace brick, stucco and stone walls are permitted with DRC approval for use on individual commercial, retail, and office lot frontages. Multi-family applications, brick walls no higher than 3.5 feet can be used in front yard areas to help define the street edge when residential units are pulled back from the street, when site grading requires use of a retaining wall, or when motor courts are used as in the case of the proposed hotel site. Brick, stucco or stone walls may be incorporated into gateway features and other areas of special emphasis. In instances where permanent enclosure or screening is necessary, screen walls complementary to the building architecture may be used. Wall height shall be as determined by the DRC.

#### 3.8.2 BUS STOPS

The location of any bus facilities, including parking, or private property shall be approved by the DRC.

#### 3.8.3 BENCHES, TABLES, & CHAIRS

Benches, tables, and chairs shall be provided within the Town Center's public and quasi-public spaces, at minimum one bench per block in blocks 2,3,4,5 where non residential uses front the street. Color and style shall be complementary to and coordinated with the building and

hardscape. Benches and other related furnishings shall be located outside of the public right-of-way unless approved by the County and shall be maintained by the Potomac Shores Community Association.

#### 3.8.4 BICYCLE RACKS

Bicycle racks will be provided at one per parking structure, one at the VRE station, and one per block alternating sides along the eastern most road in blocks 2,3,5. Where bike racks are to be installed in residential areas, they shall be sited to minimize their appearance from the public streetscape.

Racks will be located outside of the public right-of-way unless approved by the County and shall be maintained by the Potomac Shores Community Association. Bike racks shall be metal with clean, minimal designs.

#### 3.8.5 TRASH RECEPTACLES

Trash receptacles will be provided in public plaza areas and other sections of the Town Center where people will congregate. Approved materials are metal and metal with wood accents. To the extent feasible, trash receptacle colors and materials shall complement and be coordinated with adjacent street furnishings.

### 3.0 STREETSCAPE GUIDELINES

#### 3.8.6 PARKING METERS

Metered parking may occur along designated streets in retail areas. Meters shall use a consistent design throughout the district, subject to future approval.

The DRC reserves the right to review and approve all street and hardscape furnishings.

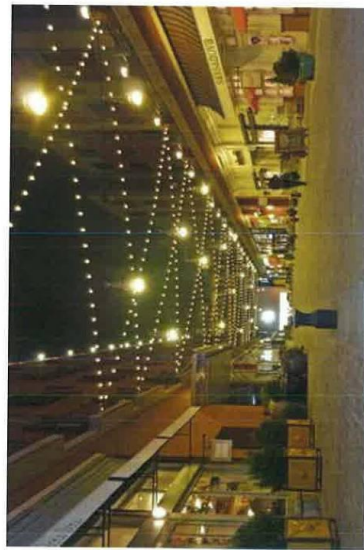
#### 3.9 SIGNAGE

All signage within the Town Center SUP shall be in accordance with the Potomac Shores Signage Guidelines.





3.0 STREETScape GUIDELINES



## 4.0 LANDSCAPE GUIDELINES



## 4.0 LANDSCAPE GUIDELINES

### 4.1 LANDSCAPE VISION

A distinguishing feature of Potomac Shores is its planned streetscape and landscape development program. The master streetscape and landscape themes for the public elements, consisting of the entry gateway landscape treatments and tree-lined streets, are to be implemented by the Town Center developer in conjunction with the Town Center public improvements. The development of each building site shall complement the architecture and streetscape character with landscape designs appropriate in scale, context, and character that reinforce the community landscape character.

The purpose of this section is to define the expectations and qualities of the landscape. The areas of landscape responsibility for individual property owners are defined herein as well as in the CC&Rs. Design submittals must include a landscape plan. The landscape plan shall be reviewed and approved by the DRC.

### 4.2 COMMON LANDSCAPE AREAS

The Town Center's Common Landscape Areas will consist of plazas, courtyards, passive open space and natural areas that will be incorporated into a final site design by the Developer. These common landscape areas shall be maintained by the Community Association. These areas fall into three categories:

1. **Street Tree Planting:** Improvements within street rights-of-way will be installed by the individual Builder/Lot Developer in accordance with an approved Landscape Plan provided by the Developer and approved by the DRC. Street trees shall be of an approved species and be planted at quantities specified in section 4.8. The soil volumes and quality within landscaped areas of the Town Center shall be of the quality to sustain long term health and growth of plantings, especially trees.

2. **Landscape Areas:** These areas include the peripheral areas of the Town Center that are primarily Resource Protection Areas and consist of wooded areas and grassy slopes. Enhancements to the Natural Open Space will be provided by the developer and maintenance will be provided through the Potomac Shores Community Association.

### 4.3 TRANSITION AREAS

Transitional landscape areas consist of the areas internal to individual blocks and streetscapes, including utility easements and parking lots that are located between the buildings and the common landscape areas. This transitional area landscape shall be implemented by the Developers of individual neighborhoods, or Blocks, and shall be compatible with surrounding landscapes and streetscapes to provide a unified setting for the Town Center. Landscape improvements shall complement the building architecture and overall streetscape in all visible public areas. Transition areas within the individual lots where individualized landscape solutions may apply are limited to non-visible rear and side yards.

## 4.0 LANDSCAPE GUIDELINES

### 4.4 LANDSCAPE CLASSIFICATION

The following guidelines shall apply to the major categories of landscape materials in order to create the desired visual harmony among the individual development sites. Temporary surface parking areas are exempt from the Design Guideline's landscaping requirements. The Approved Plant List contained within these Guidelines based on Prince William County's Tree Selection Guide. All Plant materials selections shall be subject to review by the DRC's landscape architect representative. Species with chronic diseases, pest problems, and/or structural weaknesses are not permitted. The following categories are addressed:

1. Canopy & Street Trees (minimum 3" caliper size at planting)
2. Ornamental Trees
3. Evergreen Trees
4. Deciduous Shrubs
5. Evergreen Shrubs
6. Ground Cover/Perennials
7. Turf
8. Plant Material for Restoration and Habitat Enhancement Actions

### 4.4.1 CANOPY & STREET TREES

Regular street tree plantings consisting of large scale deciduous trees, and based on an approved Street Tree Plan by the Developer will provide an overall character to the Town Center. Individual Lot Owners/Developers shall install street trees in accordance with the Approved Plan; street tree size, species and layout along with other streetscape improvements and plantings are subject to DRC review and approval.

Tree planting islands within surface parking lots shall meet County minimum coverage standards. Large canopy trees shall also be used generally within the landscape areas of corporate office sites, parks and civic and open space areas to extend a shade canopy throughout the Town Center.

### 4.4.2 ORNAMENTAL TREES

Ornamental trees that normally grow from 12 to 25 feet tall at maturity shall be liberally planted for accents and visual emphasis. The flowering habits of ornamental understory trees are very effective when used on corporate office lawns or in the vicinity of Town Center buildings where space is adequate.

Ornamental trees are most effective with a dark background provided by architecture or evergreen trees. Multiple or single stem, small-scale trees may be used in small-scale pedestrian locations where space is limited and an intimate feeling is appropriate. Minimum preferred size: 3 to 4 inch caliper.

### 4.4.3 EVERGREEN TREES

It is recommended that evergreen trees be selected from the Prince William County's Tree Selection Guide. Minimum preferred size: 7 to 9 feet height.

Evergreen trees shall be used at strategic locations within the Town Center as appropriate and can be used to enhance "winter" seasonal interest, to screen objectionable views of service areas and parking structures, as a backdrop for ornamental trees, and to block northwesterly winter winds.

## 4.0 LANDSCAPE GUIDELINES

### 4.4.4 DECIDUOUS SHRUBS

Deciduous shrubs shall be used as accents throughout the Town Center to create seasonal color interest. Deciduous shrubs can be maintained with a "natural" growth habit or may be used as formal hedges or informal massings according to context. Minimum preferred size: 3 to 4 feet height.

### 4.4.5 EVERGREEN SHRUBS

These shall be selected from varieties of winter hardy shrubs commonly found in the mid-Atlantic regional area and as provided in the Approved Plan List based on Prince William County's plant selection guidelines. They may be used where a low-level screen or hedge is desired at parking lots, sidewalks or as mass plantings in the landscape; they may also be used as effective groundcovers on slopes. Minimum preferred size: 18 to 24 inch height and spread.

### 4.4.6 GROUNDCOVERS / PERENNIALS

These shall be used where appropriate within tree pits and in shrub beds, as appropriate and selected from the Approved Plant List within these Guidelines.

### 4.4.7 TURF

Within the first phase Development Area, lawns and turf within the streetscape and landscape areas on lots may be either sodded or seeded. If seeded, lawns shall be irrigated until they are established. In later phases, per the DRC, sodded lawns shall be required. A standard turf shall be established for the entire project and may be modified by the DRC. Where used, sod shall meet the certification requirements of the Virginia Association of Turf Growers.

### 4.4.8 PLANT MATERIALS FOR RESTORATION AND HABITAT ENHANCEMENT ACTIONS

In addition to plantings adjacent to the developed portions of the Town Center, areas of temporary disturbance such as interior development road rights-of-way, and stormwater treatment facilities including riparian zones may be restored with native vegetation to benefit wildlife, improve water quality and enhance the natural setting.

## 4.0 LANDSCAPE GUIDELINES

### 4.5 APPROVED PLANT LIST

At Potomac Shores, landscape plays a key role, blending together all the built elements of the community to create the "look" and "feel" of the place. Both native and adapted ornamental plantings may be used provided they are suited to their context--- with appropriate scale, character and cultural tolerances.

The Approved Plant List provided is a typical list of plants acceptable to the DRC. The list is intended as a general guideline to assist Builders or Owners and their consultants with species selections:

- The Approved Plant List shall act as a general guideline, but not as an absolute restriction of what plants can and cannot be utilized, as there are other species and cultivars that may be suited for use at Potomac Shores.
- Proposed plants not on the Approved List shall be reviewed on a case by case basis by the DRC, and may be approved provided they do not conflict with the overall landscape character of the community.
- Also refer to the Prohibited Plant List provided in these Guidelines.

### CANOPY AND STREET TREES

- \* More suited to parks, landscape buffers and open space areas due to large space requirements
- + Suitable use as street trees per Prince William County
- ^ Suitable use for interior parking lot trees per Prince William County

Acer rubrum cultivars/Red Maple +  
 Fagus sylvatica/European Beech\*  
 Gymnocladus dioica/Kentucky Coffeetree^  
 Liriodendron tulipifera/Tulip Poplar\*  
 Platanus acerifolia 'Bloodgood' and 'Columbia'/London Plane tree+^  
 Quercus falcata /Southern Red Oak^  
 Quercus palustris/Pin Oak+^  
 Quercus palustris 'Sovereign'/'Sovereign' Pin Oak+^  
 Quercus phellos/Willow Oak+^  
 Quercus phellos 'Hightower'/'Hightower' Willow Oak+^  
 Quercus lyrata/Overcup Oak+  
 Quercus lyrata 'Highbeam'/'Highbeam Overcup Oak+  
 Quercus nuttallii 'Highpoint'/'Highpoint' Nuttall Oak+  
 Quercus rubra/Northern Red Oak+^  
 Sophora japonica/Japanese Pagoda Tree+^  
 Tilia americana/American Linden\*  
 Tilia cordata 'Greenspire'+^/Lirtleleaf Linden  
 Tilia cordata 'Chancellor'/Lirtleleaf Linden+^  
 Tilia tomentosa/Silver Linden+^  
 Ulmus americana 'Princeton', 'Valley Forge', New Harmony/American Elm+^  
 Zelkova serrata 'Green Vase'/Japanese Zelkova+^

4.0 LANDSCAPE GUIDELINES

DECIDUOUS ORNAMENTAL TREES

Acer griseum/Paperbark Maple  
 Amelanchier canadensis/Canada Serviceberry  
 Amelanchier laevis/Allegany Serviceberry  
 Betula nigra 'Heritage' and 'Duraheart'/River Birch  
 Carpinus caroliniana/Ironwood  
 Cercis canadensis/Eastern Redbud  
 Chionanthus virginicus/Fringe-tree  
 Cladrastis kentuckea/Yellowwood  
 Cornus controversa/Giant Dogwood\*  
 Cornus kousa/Kousa Dogwood  
 Cornus florida Rutgers Hybrids/Flowering Dogwood  
 Cornus Venus/Hybrid Dogwood  
 Halesia tetraptera/Silvertbell  
 Magnolia Soulangiana/Saucer Magnolia  
 Nyssa sylvatica/Black Tupelo  
 Ostrya virginiana/Hopthornbeam+  
 Parrotia persica 'Vanessa'/Persian Parrotia  
 Stewartia pseudocamellia/Japanese stewartia  
 Styrax japonicum/Japanese Snowbell  
 Syringa reticulata 'Ivory Silk'/Japanese Lilac  
 Zelkova serrata 'City Sprite'/ 'City Sprite' Zelkova

\*Focal specimen  
 +Hedge or screen

Cupressocyparis leylandii 'Emerald Isle'/Leyland cypress+  
 Ilex aquifolium cultivars/English Holly\*+  
 Ilex x aquipernyi/Hybrid Holly\*+  
 Ilex x attenuata 'Fosteri'/Foster's Holly\*+  
 Ilex x meserveae 'China Boy' and 'China Girl'/Meserve Hybrid  
 Holly\*+  
 Ilex opaca/American Holly\*+  
 Magnolia grandiflora cultivars 'Edith Bogue' and 'Bracken's Brown  
 Beauty'/Southern Magnolia\*  
 Magnolia 'Alta'/Dwarf cultivar Southern Magnolia  
 Magnolia 'Little Gem'/ 'Little Gem' Magnolia+  
 Magnolia virginiana 'Moon Glow' /Sweetbay Magnolia  
 Osmanthus americanus/Devilwood\*+  
 Pinus nigra/Austrian Pine\*+

LARGE EVERGREEN TREES



#### 4.0 LANDSCAPE GUIDELINES

##### EVERGREEN SHRUBS

- \* more suited to parks and natural open space areas
- + potential use as a hedge or screen
- Buxus sempervirens/ Boxwood
- Camellia japonica/Japanese Camellia (note: hardy cultivars for fall, winter and spring bloom)
- Cephalotaxus harringtonia 'Prostrata'/'Prostrate Japanese Plum Yew
- Chamaecyparis obtusa dwarf forms/Dwarf Hinoki Falsecypress
- Chamaecyparis pisifera 'Golden Mop' Dwarf Sawara Falsecypress
- Ilex crenata 'Soft Touch'/'Japanese Holly
- Ilex glabra 'Compacta' and 'Shamrock'/'Inkberry Holly'+
- Ilex 'Red Beauty'/'Hybrid Holly
- Myrica pennsylvanica/Northern Bayberry\*
- Leucothoe fontanesiana/Drooping Leucothoe
- Pieris japonica/Japanese Andromeda
- Prunus laurocerasus/Cherry Laurel+
- Rhododendron hybrids and cultivars (note: contact the local American Rhododendron Society for suggested cultivars)
- Sarcococca hookeriana var. humilis/Sarcococca
- Skinimia japonica/Japanese Skinimia
- Viburnum x Pragensis/Prague Viburnum
- Viburnum thyrifolium Dart's Duke/'Leatherleaf Viburnum'

##### DECIDUOUS SHRUBS

- Calycanthus floridus/Sweetbush\*
- Caryopteris x clandonensis/Blue Mist shrub
- Clethra alnifolia 'Hummingbird'/'Dwarf Summersweet
- Daphne caucasica/Daphne
- Forsythia x intermedia 'Spring Glory'/'Forsythia \*
- Fothergilla gardenii/'Dwarf Fothergilla'
- Hamelis virginiana/'Witch Hazel'
- Hydrangea macrophylla dwarf cultivars/'Dwarf Bigleaf Hydrangea
- Hydrangea quercifolia dwarf cultivars/'Dwarf Oakleaf Hydrangea 'Pee Wee' and 'Syke's Dwarf'
- Hypericum prolificum/Shrubby St. John's Wort
- Illicium parvifolium/Anise\*
- Itea virginica/Virginia Sweetspire\*
- Ilex virginica 'Little Henry'/'Dwarf Virginia Sweetspire
- Kerria japonica/Japanese Kerria
- Lagerstroemia indica - intermediate scale cultivars /Crapemyrtle
- Potentilla fruticosa/Shrubby Cinquefoil
- Rhododendron arborescens/Sweet Azalea\*
- Rhododendron viscosum/Swamp Azalea\*
- Rhododendrons - Encore Azaleas, 'Nova Zembla', 'P.J.M.', 'Roseum Elegans', 'Ingrid Mehlquist', 'Johanna'
- Rhus aromatica 'Gro-Low'/'Dwarf Fragrant Sumac
- Roses - Knockout Series, 'Living Easy' and 'Baby Love'
- Spiraea x b. 'Limemound'
- Syringa Palibin/'Korean Lilac
- Viburnum nudum/'Withered Viburnum'
- Viburnum dentatum 'Blue Muffin'/'Arrowwood Viburnum'
- Viburnum opulus 'Nanum'/'Nanum'
- Viburnum plicatum var. tomentosum 'Summer Snowflake'

## 4.0 LANDSCAPE GUIDELINES

### PERENNIALS

Acanthus spinosus/Acanthus  
 Agapanthus/Nile Lily  
 Anemone x hybrida/Japanese anemone  
 Anemone tomentosa 'Robustissima'/Japanese Anemone  
 Aruncus/Goats beard  
 Aster spp./Aster  
 Astilbe  
 Ceratostigma plumbaginoides/Plumbago  
 Chelone glabra/White Turtlehead  
 Eupatorium dubium dwarf form/Dwarf Eastern Joe-Pye Weed  
 Geranium sanguineum/Cranesbill  
 Hellebores  
 Iberis sempervirens / Candyruft  
 Liriope muscari/Lilytuft (keep away from natural areas)  
 Liriope spicata/Lilytuft (keep away from natural areas)  
 Ophiopogon/Dwarf Mondo Grass  
 Perovskia atriplicifolia/Russian Sage  
 Persicaria virginiana 'Var.'/Tovara  
 Polystichum acrostichoides/Christmas Fern  
 Rudbeckia hirta/Black-eyed Susan  
 Solidago canadensis/Canada Goldenrod  
 Hosta spp./Hosta

### ORNAMENTAL GRASSES

Carex morrowii  
 Pennisetum alopecuroides/Fountain Grass  
 Pennisetum alopecuroides cultivars 'Hamelin', 'Burgundy Bunny',  
 'Katie Rose', and 'Little Bunny' (not P. 'Mouly')  
 Heterolepis sporobolus/Prairie Dropseed

### VINES

Bignonia capreolata 'Dragon Lady' and 'Tangerine Beauty'/Cross  
 Vine  
 Campsis radicans 'Madame Galen'/Trumpet Creeper  
 Clematis cultivars Jackmannii, The President, Polish Spirit, Nelly  
 Moser/Clematis  
 Gelsemium sempervirens 'Margarita'/Carolina Jessamine  
 Lonicera heckrottii/Everblooming Honeysuckle  
 Lonicera sempervirens 'John Clayton'/Trumpet Honeysuckle  
 Vitis labrusca/Fox Grape  
 Parthenocissus quinquefolia/Virginia Creeper  
 Passiflora lutea/Yellow Passionflower  
 Passiflora incarnata/Purple Passionflower  
 Rose 'New Dawn', 'Dortmund' and 'William Baffin'/Climbing  
 Roses  
 Wisteria frutescens 'Amethyst Falls'/American Wisteria

#### 4.0 LANDSCAPE GUIDELINES

##### GROUNDCOVERS

Abelia grandiflora 'Compacta' and 'Little Richard'/Glossy Abelia (compact forms)  
 Carex morrowii 'Goldband' and 'Variegata'/Morrow Sedge  
 Carex plantaginea/Plantainleaf Sedge  
 Cotoneaster salicifolius and C. 'Scarlet Leader'/Creeping Willowleaf Cotoneaster  
 Dryopteris erythrosora/Japanese Shield Fern  
 Epimedium x perralchicum 'Frohnléiten'/Epimedium  
 Epimedium versicolor 'Sulphureum'/Epimedium  
 Helleborus foetidus/Bearsfoot Hellebore  
 Helleborus orientalis and hybrids/Lenten Rose  
 Hypericum calycinum /St. John's Wort  
 Liriope muscari cultivars/Blue Liriope (Note: potentially invasive, keep away from natural areas)  
 Liriope spicata/Lilyturf (Note: potentially invasive, keep away from natural areas)  
 Lonicera nitida/Box Honeysuckle  
 Ophiopogon japonicus/Mondo Grass  
 Pachysandra terminalis/Japanese Spurge (note: Potentially invasive, keep away from natural areas)  
 Rhododendron – Dwarf North Tisbury hybrids 'Joseph Hill' and 'Michael Hill'  
 Rhododendron kiusianum 'Komo Kulshan'/Dwarf Azalea  
 Sarcococca hookeriana var. humilis/Dwarf Sarcococca  
 Vinca minor/Dwarf Periwinkle (note: Potentially invasive, keep away from natural areas)

##### LAWN GRASSES

\*Note: Sod shall meet quality certification requirements of the Virginia Association of Turf Growers.

Kentucky Bluegrass Blends (50% Kentucky bluegrass, 10% Red Fescue, 40% Annual Rye or similar)  
 Zoysiagrass, 'Meyer' and 'Zorro' cultivars

##### BULBS

Anemone blanda 'White Splendor'/Grecian Windflower  
 Chionodoxa/Glory of the Snow  
 Galanthus/Snow drop  
 Hyacinthoides/Spanish Bluebells  
 Ipheion uniflora/Starflower  
 Iris sibirica/Siberian Iris  
 Iris ensata/Japanese Iris  
 Leucojum/Snowflake  
 Muscari/Grape Hyacinth  
 Narcissus/Daffodils  
 Scilla sibirica/Wood Squill

## 4.0 LANDSCAPE GUIDELINES

### PROHIBITED PLANT LIST

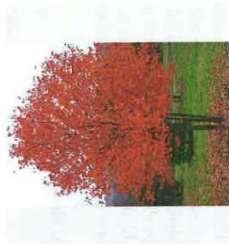
The following list of invasive species are prohibited at Potomac Shores. As information is constantly changing, this list may be updated by the DRC from time to time. \* The list is derived from the State of Virginia Department of Conservation and Recreation 2009 edition and the Prince William County DCSM Chapter 8 Table I-3.

Ailanthus altissima/Tree of Heaven  
 Acer platanoides/Norway maple  
 Ajuga reptans/Bugleweed  
 Akebia quinata/Five-leaf akebia  
 Albizia julibrissin/Mimosa  
 Ampelopsis brevipedunculata/Porcelain Berry  
 Arundo donax/Giant reed  
 Bambusa, Phyllostachys and Pseudosasa spp./Bamboo  
 Berberis thunbergii/Japanese barberry  
 Broussonetia papyrifera/Paper Mulberry  
 Celastrus orbiculata/Oriental bittersweet  
 Elaeagnus angustifolia/Russian Olive  
 Elaeagnus pungens/Thorny elaeagnus  
 Eragrostis curculula/Weeping lovegrass  
 Euphorbia esula/Leafy spurge  
 Euonymus alata/ Burning bush  
 Euonymus fortunei/Creeping Euonymus  
 Festuca elatior/Tall Fescue  
 Foeniculum vulgare/Fennel  
 Glechoma hederacea/Gill-over-the-ground  
 Hedera helix/English ivy  
 Hemerocallis fulva/Common Daylily

Humulus japonicus/Japanese hops  
 Ipomoea coccinea/Red morning-glory  
 Ipomoea hederacea/Ivey-leaved morning glory  
 Ipomoea purpurea/Common morning-glory  
 Iris pseudacorus/Yellow flag  
 Lespedeza bicolor/Shrubby Lespedeza  
 Lespedeza cuneata/Chinese Lespedeza  
 Ligustrum (all species)/Privet  
 Ligustrum sinense/Chinese privet  
 Lonicera fragrantissima/Sweet breath of spring  
 Lonicera x bella/Bell's honeysuckle  
 Lonicera japonica/Japanese honeysuckle  
 Lonicera maackii/Amur honeysuckle  
 Lonicera tatarica/Tartarian honeysuckle  
 Lonicera xylosteum/Bush honeysuckle  
 Lysimachia clethroides/giisebecj Kiisestrufe  
 Lysimachia nummularia/Moneywort  
 Lythrum salicaria/Purple loosestrife  
 Melia azedarach/China-berry  
 Miscanthus sinensis/Silver grass  
 Morus spp./ Mulberry  
 Nandina domestica/Nandina  
 Paulownia tomentosa/Princess tree  
 Pennisetum alopecuroides 'Moudry'/Fountain Grass  
 Phalaris arundinacea/Ribbon Grass  
 Phyllostachys aurea/Golden bamboo  
 Poa compressa/Canada bluegrass  
 Poa trivialis/Rough bluegrass  
 Polygonum cuspidatum/Japanese Knotweed  
 Populus alba/White poplar  
 Pueraria Montana/Kudzu vine

4.0 LANDSCAPE GUIDELINES

- Pyracantha coccinea/Scarlet firethorn
- Pyracantha angustifolia/Narrow-leaf Firethorn
- Quercus acutissima/Sawtooth Oak
- Ranunculus ficaria/Lesser Celandine
- Rubus phoenicolasius/Wineberry
- Rosa multiflora/Multiflora rose
- Spiraea japonica/Japanese spiraea
- Triadica sebifera/Tallowtree
- Ulmus pumila/Siberian elm
- Viburnum Linden viburnum dilatatum/
- Vinca major/Periwinkle
- Vitex rotundifolia/Beach vitex
- Wisteria floribunda/Japanese wisteria
- Wisteria sinensis/Chinese wisteria



'Red Sunset' Red Maple



London Plane Tree



Northern Red Oak



Japanese Pagoda Tree



'Greenspire' Littleleaf Linden



'Valley Forge' American Elm

## 4.0 LANDSCAPE GUIDELINES

program for landscape areas within their site. The program shall include the following and be coordinated with the maintenance of common areas:

- Prune trees and shrubs as necessary to maintain an attractive shape; remove dead branches and provide clearance for vehicles and pedestrians.
- Replace dead and disfigured plant material immediately with plants of equal size except in environmentally-sensitive areas at the discretion of the DRC. Fertilize, mulch, water, and weed plant beds. Apply insecticides and fungicides as necessary to maintain plant vigor and appearance.
- Lawns shall be watered, mowed, and maintained in a dense, weed-free condition. Lawns shall be edged adjacent to paved surfaces.
- Remove fallen leaves, branches, paper, and other debris from lawn, shrub, and pavement areas.

### 4.8 PLANT QUANTITIES

The following Guidelines establish typical plant quantities. Because parcels will vary in size, physical character and restrictions, more or less plant material than specified below may be required, subject to decision by the DRC. For the purposes of this section, green area is defined as disturbed landscape area that is not built-upon, is not hardscape, has not been restored to original condition, and has not been naturalized.

#### 1. Shade Trees:

- a. Approximately 1 per 40 linear foot of building road frontage (excluding driveways and clear zones at intersections) along public roadways and major private streets (planted 35 to 45 feet on-center).

PG 63

### 4.6 ADDITIONAL REQUIREMENTS

#### 4.6.1 IRRIGATION

An automated irrigation system may be installed by the developer in the landscape easement areas. Individual building site owners shall install, operate and maintain similar systems in landscape areas within their site where required, as approved by the DRC.

#### 4.6.2 PLANTING PRACTICE

All plants shall be nursery grown in accordance with the highest standards of horticultural practices and shall be grown under climatic conditions similar to those of the site for at least two years. Plants shall be typical of their species or variety and shall have normal growth habits. Plants shall be free of disease and shall have healthy, well-developed root systems. All plants shall conform to the American Standard for Nursery Stock as published by the American Association of Nurserymen.

#### 4.6.3 EDGING MATERIAL

Planting beds should be defined from lawn areas. The following materials are acceptable edge treatments: spade edges, steel (black), aluminum, concrete, stone, and brick.

### 4.7 MAINTENANCE & COVENANTS

The Potomac Shores Community Association will maintain common areas described in the Declaration of Protective Covenants. Individual owners shall be responsible for executing a landscape maintenance

#### 4.0 LANDSCAPE GUIDELINES

- b. 1 tree per lot (single family detached). Minimum size 2-2.5 inch caliper.
- c. 1 per 10 surface parking spaces, refer to section 3.4.1 for further information.
- d. Spacing of street trees shall be coordinated with the striped on-street parking spaces where feasible. This spacing allows vehicle passengers to open doors without hitting a tree and thus minimizes the damage inflicted upon trees from car doors.

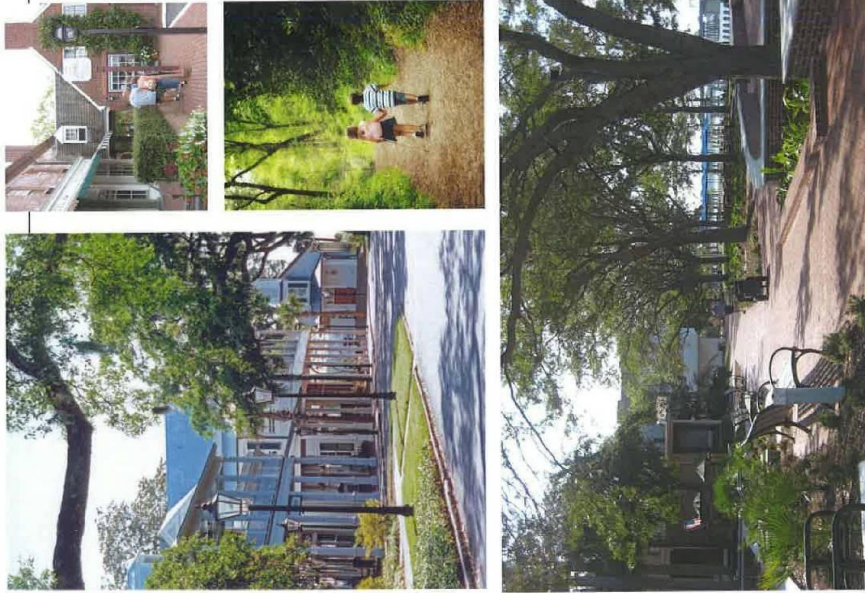
#### 2. Ground Plane Planting:

A minimum of fifteen (15) percent of the green area of a commercial, office, retail, business, industrial parcel, or lot shall consist of planting beds with shrubs, flowers, or groundcover. For building parcels with large undisturbed areas, this requirement will be based on the disturbed area.

For single-family attached and detached units, a minimum of ten (10) percent of the green area of a lot shall consist of planting beds with flowering trees, shrubs, flowers, or groundcover (for lots with large, undisturbed wooded areas, this requirement will be based on the disturbed area). For multi-family units, a minimum of ten (10) percent of the green area of a lot or parcel shall consist of planting beds with shrubs, flowers, or groundcover.

#### 3. Evergreen and Ornamental Trees:

Two flowering or evergreen trees may be substituted for each shade tree up to thirty (30) percent of the required number, if approved by the DRC.





**Attachment E  
Design Guidelines**

**4.0 LANDSCAPE GUIDELINES**

**4.9. OPEN SPACE PLAN**



*Town Center Open Space Plan* PG 65

4.0 LANDSCAPE GUIDELINES



A. Dunnington Entry Landscape Plan

4.10. PRELIMINARY LANDSCAPE CONCEPTS

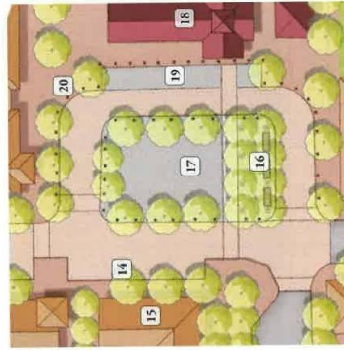
The conceptual layouts shown may evolve through implementation of the various controlling documents, and are intended to illustrate design intention for key public areas of the Town Center.

LEGEND		LEGEND	
1	Entry Monument	11	Event Lawn
2	10' Brick Path	12	Floored Drop Off
3	Vehicular Brick Paving with Brick Cross Walk	13	Retail Street
4	Elm Alley	14	Convenience Parking
5	Cherry Tree Bosque	15	Retail
6	Swamp White Oak Alley	16	Bosque with Bench Seating
7	Chapel and Garden	17	Paved Plaza
8	Convenience Parking	18	VRE Head House
9	Pinxton Elms around Town Green	19	Granite paver Train Station Drop-Off
10	Bluff Promenade	20	Bollards

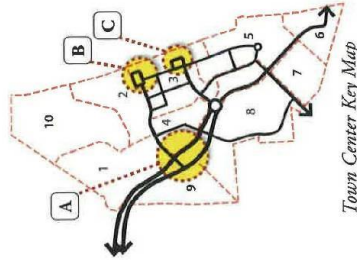
POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PC 66



B. Town Green

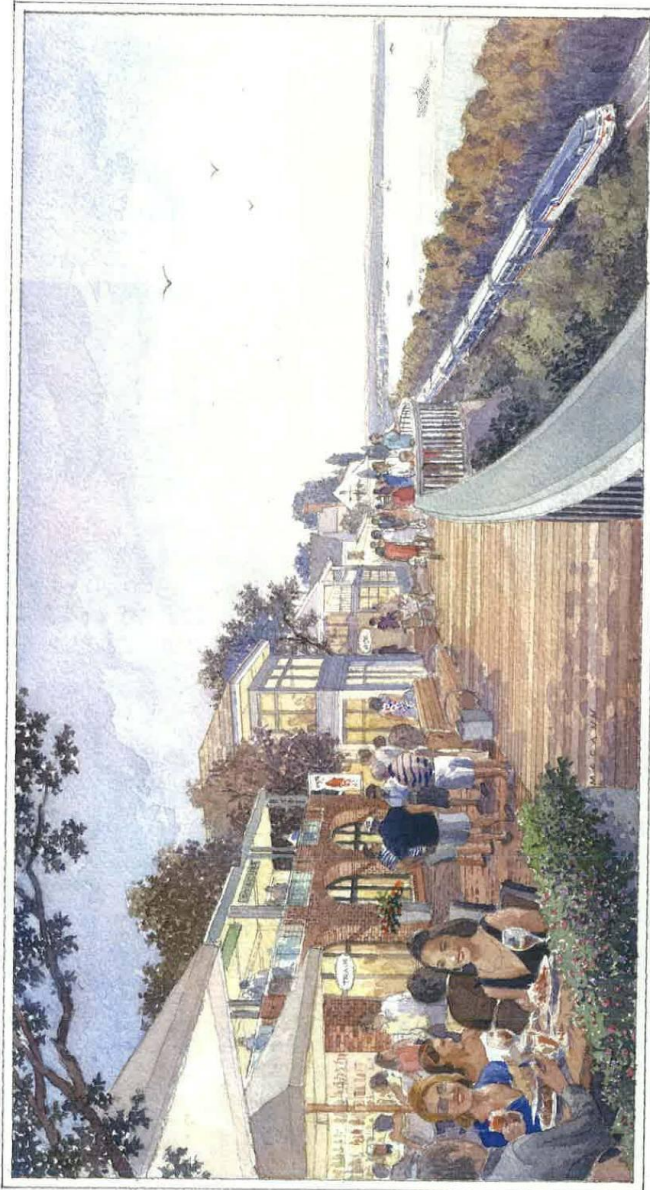


C. VRE Square



Town Center Key Map

4.0 LANDSCAPE GUIDELINES



*Train Station Promenade, Artist Rendering*

PG 67



## **5.0 POLICIES AND PROCEDURES**



POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 68

## 5.0 POLICIES AND PROCEDURES

### 5.1 DESIGN REVIEW POLICIES & PROCEDURES

#### 5.1.1 INTENT

The Intent of the Design Review is to ensure that buildings and landscapes are consistent with the Design Guidelines. The Design Review process will be administered by the Design Review Committee (the "DRC"). All review comments need to be incorporated into the subsequent documents before approval can take place. No construction should commence without completing all Design Reviews and without written approval from the DRC.

Applicant should complete the DRC review prior to applying to Prince William County for all applicable building and use permits.

#### 5.1.2 DESIGN REVIEW PROCESS

Each and every building design is required to complete sequentially the following:

- Preliminary Design
- Construction Document Design
- Design Changes to Approved Construction Documents
- Request for Final Inspection

Application forms are contained within this document for each type of submission. Each submission must be accompanied by the required information as specified in the application instructions and these

Guidelines. Applicant is to submit 5 to-scale sets of plans to the DRC two (2) weeks prior to the scheduled review. Owners may provide a master set of documents for multiple buildings as one submittal. Design Reviews may be informal written communications or alternatively a meeting may be requested with the Applicant. Design Review comments shall be documented as written minutes and/or redlines to drawings. All conditions for approval shall be met before proceeding to subsequent reviews.

The DRC may perform periodic reviews during construction to verify compliance with the approved construction documents. An optional final inspection at the end of construction may be performed by the DRC to verify consistency with the approved construction documents.

#### 5.1.3 STEPS IN DESIGN REVIEW PROCESS

A separate application for each step of the process is provided. Applicant should bring all documents required for review at the time of review.

- I. Preliminary Design Review
- II. Construction Documents Review and Changes to Approved Construction Documents (if applicable)
- III. Request for Final Inspection

#### 5.1.4 OWNER REVIEW PROCESS

Owners may submit a master set of drawings with multiple floor plans and material selections under a single review process. Following approval of this master set, these designs will be considered 'pre-approved' by the DRC. A separate site plan review process will occur subsequently once the house is assigned to a lot. This site plan review process will have a turnaround process of 2 business days.

## 5.0 POLICIES AND PROCEDURES

### 5.1.5 VARIANCES TO DESIGN GUIDELINES

The DRC has full authority to consider and grant variance requests at its discretion. Any decision of the DRC regarding whether to grant or deny a variance request shall not be binding or establish precedent for future variance decisions.

### 5.1.6 AMENDMENTS TO THE DESIGN GUIDELINES

Amendments, revisions, and waivers to the Potomac Shores Design Guidelines and Development Code must be approved by a majority vote of the full membership of the DRC, with documentation provided to ensure that the design intent of the amendments and revisions can be clearly understood and implemented. This includes the preparation of architectural, landscape, and engineering standards and criteria, as appropriate, to be included in the revised document. No amendment, revision or waiver shall be approved which is inconsistent with the conditions of the special use permit, or inconsistent with the overall design concepts and intent of the Town Center.

Prior to final approval by the DRC, each amendment, revision or waiver to the Design Guidelines shall be forwarded to Prince William County Planning Director, or his designee, who shall have twenty (20) business days from receipt of such amendment, revision or waiver to notify the DRC in writing that he has determined said amendment, revision or waiver to be substantially inconsistent with the Design Guidelines or special use permit. The Planning Director's written determination shall include specific references to those portions of the Design Guidelines or special use permit conditions which are the basis for such determination. The DRC shall not approve any such amendment, revisions or waiver found to be inconsistent by the Planning Director. Failure of the Planning Director to provide such written notice shall automatically

mean that he has determined such amendment, revision or waiver to be substantially consistent with the Design Guidelines and the conditions of the special use permit and the DRC shall be entitled to either approve or not approve said amendment, revision or waiver at its sole discretion.











**B. CONSTRUCTION DOCUMENTS REVIEW APPLICATION & CHANGES  
TO APPROVED CONSTRUCTION DOCUMENTS (2/4)**

SUBMITTAL REQUIREMENTS CONTINUED \_\_\_\_\_ Products and Materials list (see reverse)

*Landscape Submittal Requirements:*

\_\_\_\_\_ Fences, Site Walls, Gates, and Arbors (Elevation Details)  
Minimum Scale: 1 1/2" = 1'

\_\_\_\_\_ Landscape Plan  
Minimum Scale: 1/8" = 1', showing:

- Location
- Location and coverage of irrigation system, if any
- Species
- Planting species, size, spacing. Note: Symbols should be drawn to reflect the stated spacing indicated on the Plant Schedule.
- Driveways, walkways
- Landscape areas
- Hardscape areas
- Fences with dimensions, heights, materials
- Location and type of irrigation system, if applicable
- Fences, Low Walls, Gates, and Arbors - Layout, materials, elevations
- Pool and pool enclosures
- AC equipment, utilities, and meters - Size, location, screening material
- Existing Trees
- Mulch type, size, color, and location
- Soil preparation specification including type and depth of amendment

POTOMAC SHORES TOWN CENTER DESIGN GUIDELINES  
PG 74

**Attachment E**  
**Design Guidelines**

**B. CONSTRUCTION DOCUMENTS REVIEW APPLICATION & CHANGES  
TO APPROVED CONSTRUCTION DOCUMENTS (3/4)**

PRODUCT AND MATERIAL SAMPLES: Please include photographs or cut sheets from manufacturers catalogs of proposed windows, doors, lighting fixtures, etc. Also identify all materials and colors for the elements listed below. If any of the material or colors differ from what is shown on the approved exterior materials and color list, samples must be provided or identified on construction drawings.

EXTERIOR ELEMENT	MATERIAL COLOR / FINISH	MANUFACTURER / DESCRIPTION
Balcony/Porch/Loggia		
Balcony/Porch/Loggia Ceilings		
Balcony/Porch/Loggia Floorings		
Balcony/Porch/Loggia Railings		
Brick		
Chimney		
Columns		
Door-Entry		
Door-Other in Public View		
Door-Other		
Down Spouts		
Driveway		
Entry Walk		
Fascia		
Fences		
Garage Door(s)		
Garden Walls		
Gate(s)		
Gutters		
Lighting		
Planters		
Pool Deck		
Pool Enclosure		

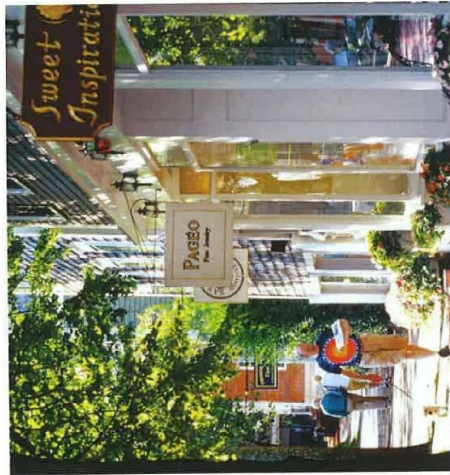
**Attachment E  
Design Guidelines**

**B. CONSTRUCTION DOCUMENTS REVIEW APPLICATION & CHANGES  
TO APPROVED CONSTRUCTION DOCUMENTS (4/4)**

PRODUCT AND MATERIAL SAMPLES: Please include photographs or cut sheets from manufacturers catalogs of proposed windows, doors, lighting fixtures, etc. Also identify all materials and colors for the elements listed below. If any of the material or colors differ from what is shown on the approved exterior materials and color list, samples must be provided or identified on construction drawings.

EXTERIOR ELEMENT	MATERIAL COLOR / FINISH	MANUFACTURER / DESCRIPTION
Roofing		
Screens		
Shutters		
Siding		
Stucco		
Walls		
Windows		
Window Trim		

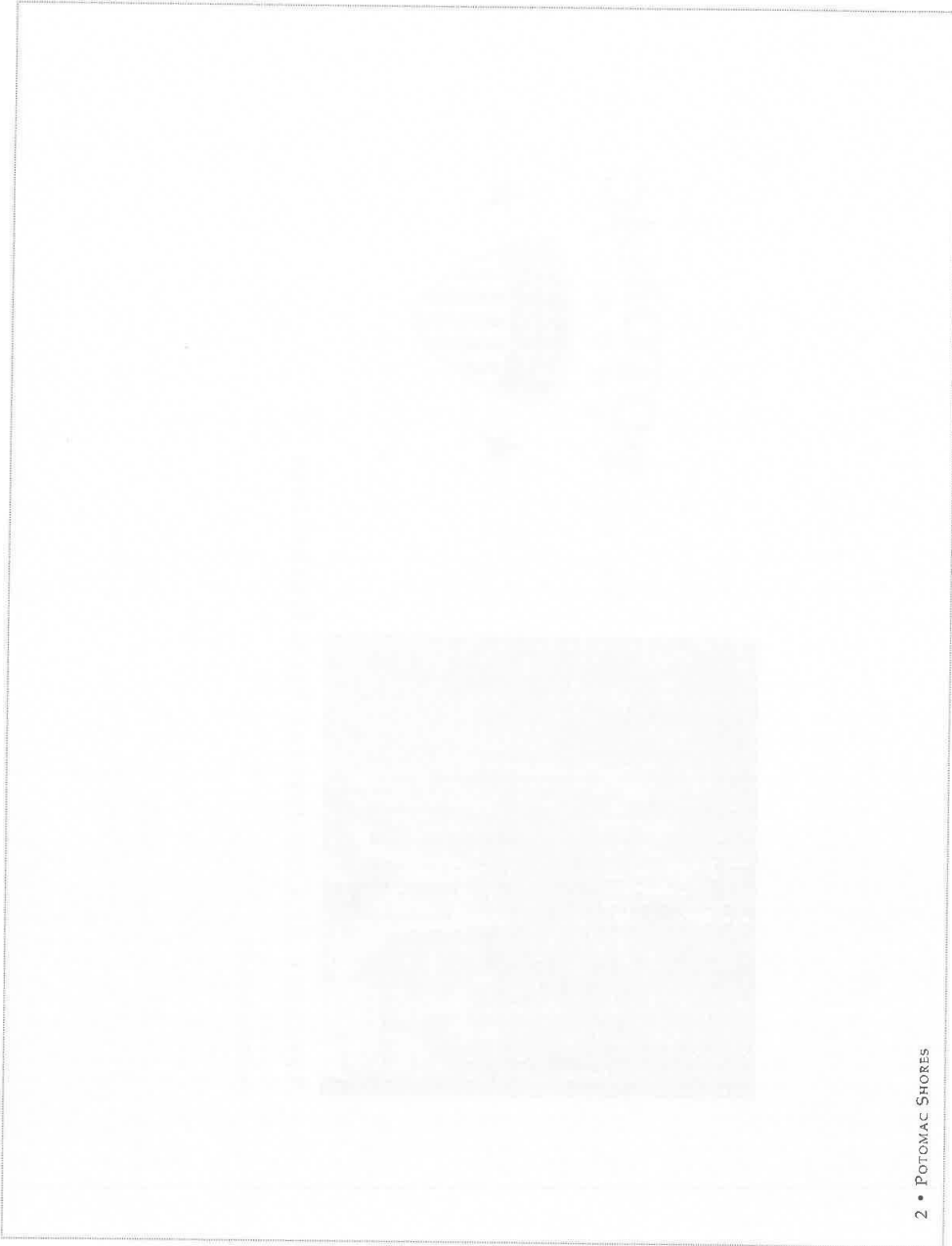
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Design Guidelines**



**APPENDIX A: COMMERCIAL SIGN GUIDELINES**

REVISED 4.26.13

**Attachment E**  
**Design Guidelines**



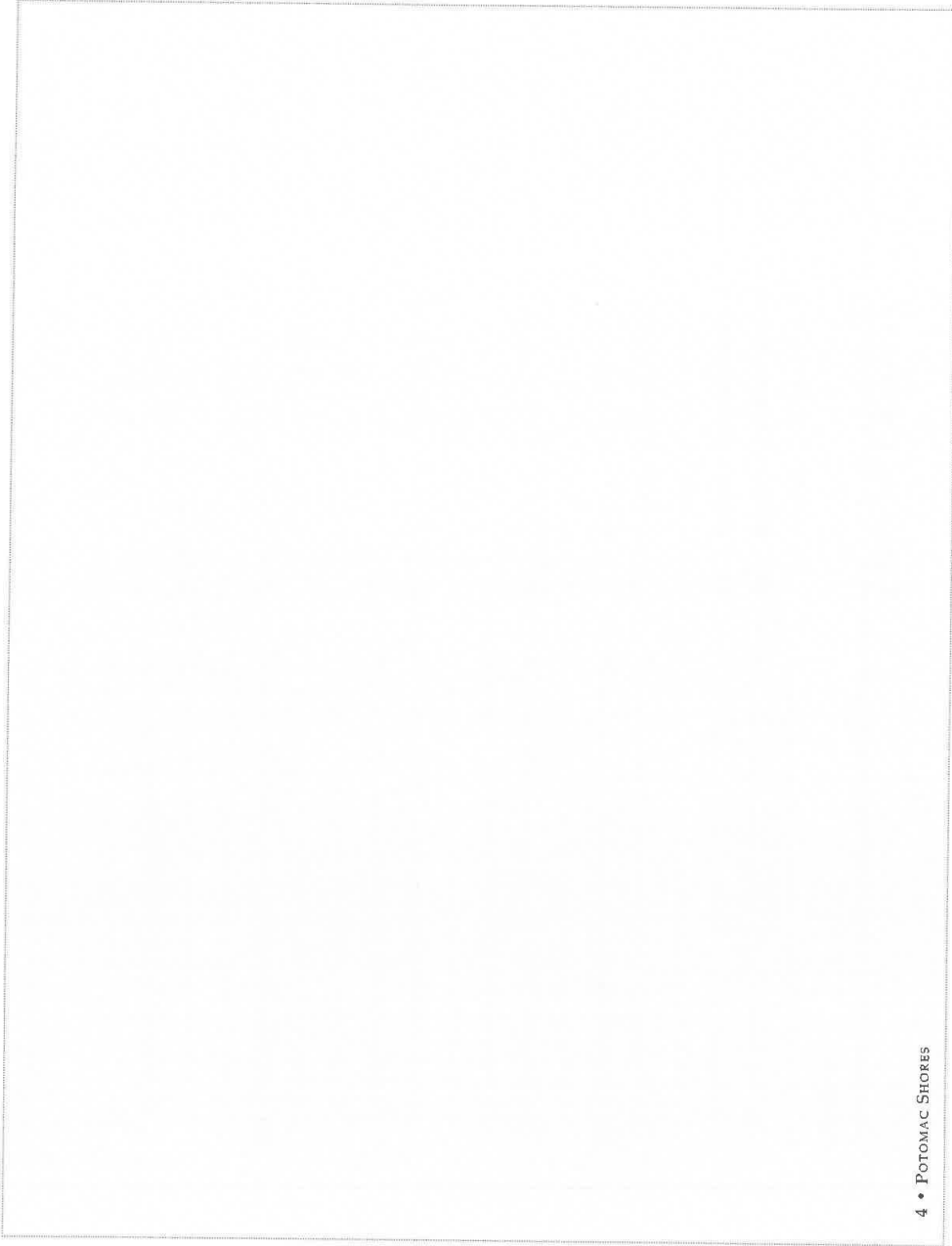
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**Attachment E**  
**Design Guidelines**



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# 1.0 INTRODUCTION

- 1.1 Why Brand?
- 1.2 Purpose
- 1.3 How To Use These Guidelines
- 1.4 Objectives

## 1.0 INTRODUCTION

### 1.1 WHY BRAND?

A compelling brand blends the magic and the messages that attract—and retain—potential residents, employees, tenants and visitors, connecting with them on a deep, emotional level that turns them into lifelong enthusiasts. A well-defined brand is not only a key component of a community's success long-term, but also a critical element in establishing consistency in its physical design. It's an invaluable storytelling device implemented in major resort and master-planned communities worldwide.

Signage is no different. It's really a complex marketing discipline applied to a three-dimensional space—in this case, a 2,000-acre peninsula on the Potomac River. More than just a navigational solution, a unified signage program enhances the user experience while complementing the environment. When done right, it becomes another medium for telling the Potomac Shores story.

### 1.2 PURPOSE

The purpose of these Commercial Sign Guidelines is to establish a coordinated system that has a unified overall appearance throughout Potomac Shores. Signage must also be reflective of the community's identity as a new transit-oriented riverfront retreat that embraces the Tidewater traditions—a Destination for a Lifetime. A carefully planned system serves to create a physically functional and aesthetically pleasing environment within Potomac Shores that integrates individual site needs with project-wide design goals.

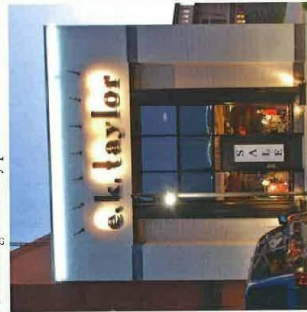


**1.3 How To Use These Guidelines**

These commercial sign guidelines apply to all commercial signs located within Potomac Shores. The guidelines are supplemental to the Prince William County sign regulations. This framework will provide direction to applicant developers and property owners and provide assurance to Prince William County that Potomac Shores commercial signs will be in general conformance with the intent of the County sign regulations and the approved Community Plan.

Sign standards within Potomac Shores, as defined within these guidelines, may be in some cases more restrictive with regard to appropriate sign type, scale, materials, lighting or location, in order to achieve overall design objectives of the community.

Commercial signs within Potomac Shores shall require a separate review and approval process by the Potomac Shores Design Review Committee ("DRC") as described in Chapter 5.0 of these guidelines, in addition to obtaining a County permit.



**1.4 OBJECTIVES**

The purpose of guidelines is to control signs in a manner that will protect property values, enhance the character of Potomac Shores and protect the public safety and welfare, while allowing reasonable identification and advertising by professional, commercial and industrial establishments.

Design standards informing and guiding issues such as appropriate sign scale, materials, lighting and location will help to ensure that signs are complementary to the resort vision of Potomac Shores, where respect for regional and historic traditions serve as a foundation for contemporary architecture, landscape and community design.

A distinct family of signs is envisioned for Potomac Shores:

- that would provide a visually cohesive setting;
- that would provide different sign standards by individual districts within the Town Center, as described and illustrated in these guidelines;
- that would allow variation and individual design approaches as described and illustrated in these guidelines.

Signs shall meet the following general objectives of the Potomac Shores community:

- A strong signage presence with clean, sophisticated graphic design that provides connectivity and relevance to public space, further establishing Potomac Shores as a progressive, attractive and convenient place to live, work and play.

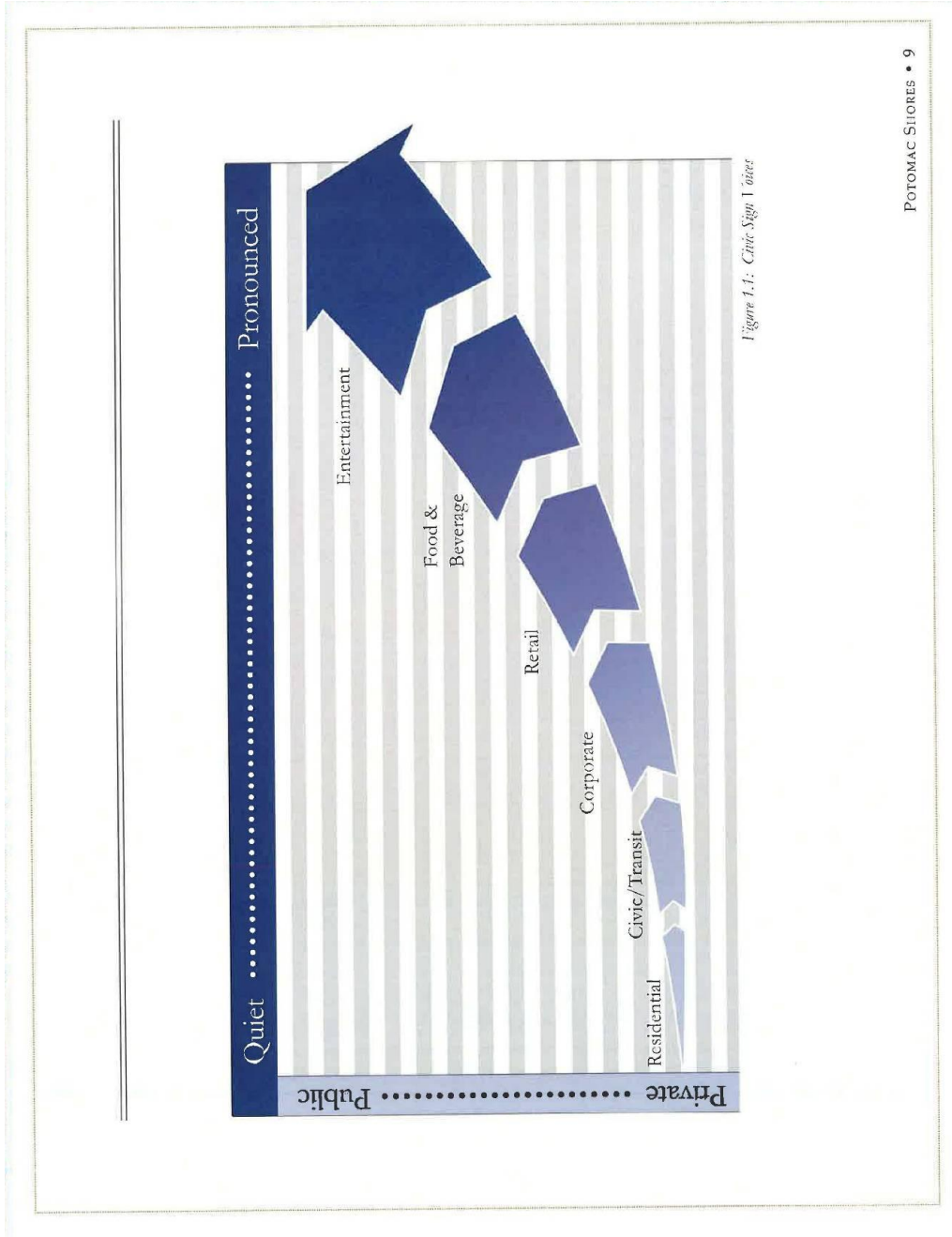


## 1.0 INTRODUCTION

- All civic, residential and commercial signage should complement the architecture, creating a lively atmosphere and visually enriching the fabric of the overall community.
- Signs should work with other street amenities to establish the character of the street on which they front while clearly identifying businesses, encouraging window shopping and enhancing the pedestrian experience.
- A cohesive family of municipal and civic signs should provide a quiet street presence that unifies the community and that becomes the constant backdrop to the widely varying commercial spectrum of signage.
- There should be several coexisting sign “voices” with different volumes from “subtle” to “loud” that provide variation and visual interest. Accordingly, civic signs should be composed and relatively “quiet” while commercial, hospitality, retail and entertainment signs may be more “lively”. Refer to Figure 1.1: Civic Sign Voices.

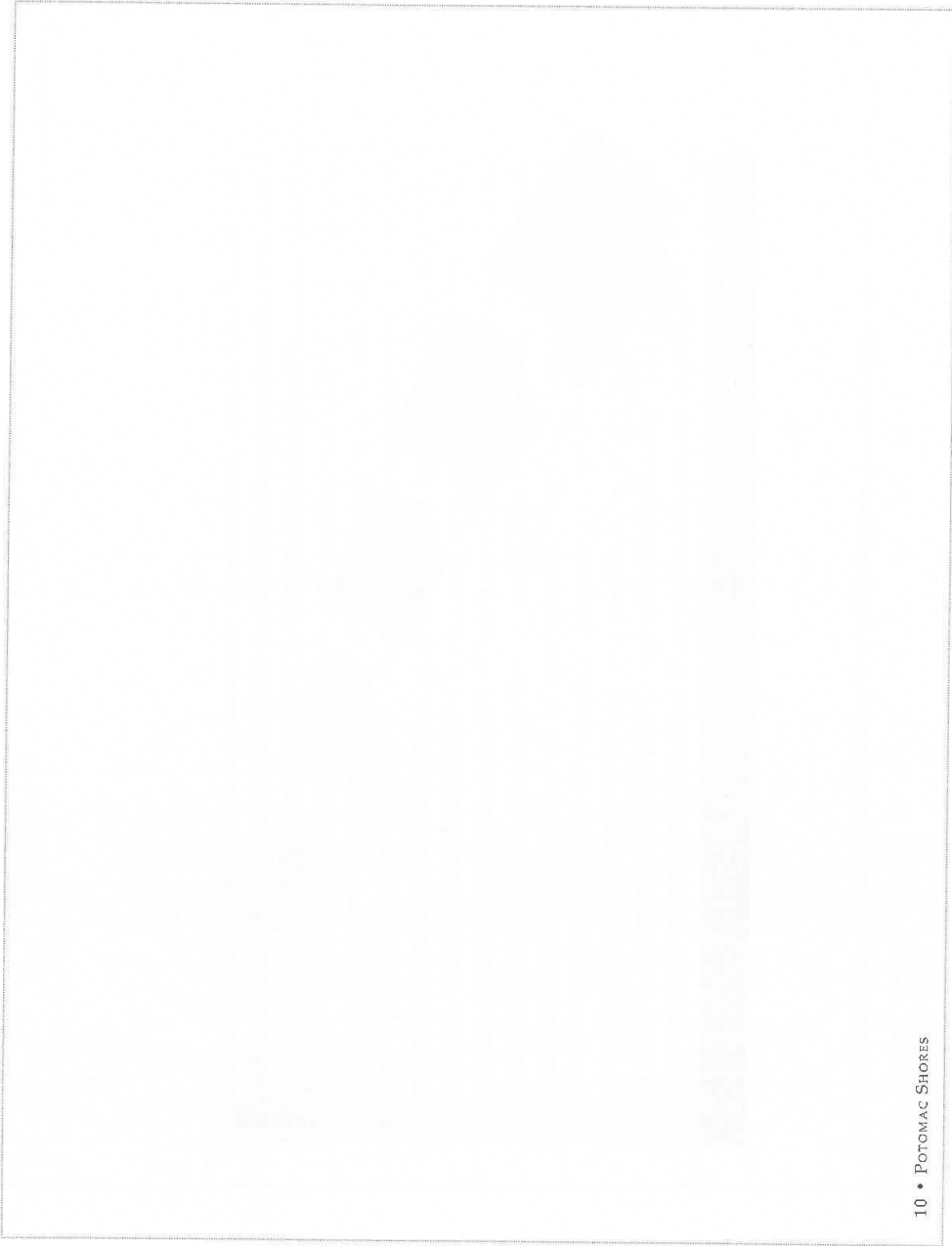


**Attachment E  
Design Guidelines**





**Attachment E**  
**Design Guidelines**



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## **2.0 THE DISTRICTS**

- 2.1 Introduction
- 2.2 District Overview
- 2.3 Tidewater and Marketplace Districts
- 2.4 Woods District
- 2.5 Lake and Riverbluff Districts
- 2.6 Waterfront District
- 2.7 Sign Opportunities
- 2.8 Sign Standards by District

## 2.0 THE DISTRICTS

### 2.1 INTRODUCTION

Tidewater is more than a place and more than an era. It is a relaxed, distinctly regional way of life that exemplifies the quintessential Virginia experience.

As the first true master-planned destination along the Potomac River, Potomac Shores will recapture the ambiance and design aesthetic of historic Tidewater settlements founded on Virginia's Coastal Plain more than 300 years ago. This was a time of magnificent manners, gracious hospitality and prosperity on the Potomac's waterways. It was a time of social gatherings and cultural pursuits. And so it will be again.

Though Potomac Shores is large in scale, it will feel intimate and walkable, so neighbors can mingle with one another. Streetscapes will be spacious and beautifully maintained. Homes, lush grounds and gardens will reflect Tidewater traditions reinterpreted in a more contemporary manner. The Potomac Shores signage program will be another important way to establish the Tidewater the character of the community and distinguish it within each district.

Within the Town Center area designated by the approved Community Plan, there are 6 Districts as illustrated in Figure 3.1: Potomac Shores District Map.

All signs shall generally adhere to design principles as described in Chapter Three - General Guidelines. Representative images of appropriate signs are provided throughout these guidelines and in the Section 5, Examples & Definitions. These images are not exhaustive, however, as there are many possibilities for individual design solutions.

### 2.2 DISTRICT OVERVIEW

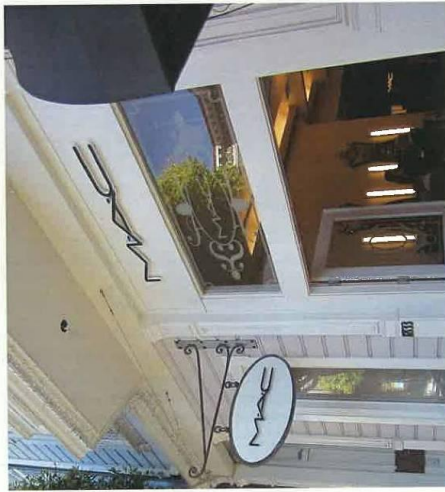
Each district will have its own distinctive identity, reflective of its physical character, natural features and/or purpose. Signage will fit within the unique themes of the individual districts, while remaining complementary throughout the community.

- **The Marketplace District (Marketplace):** A vibrant, pedestrian-friendly gathering place for residents, Tidewater guests, local visitors and tourists alike. Located next door to the historic Tidewater District, it will teem with shopping, dining, entertainment and events.
- **Tidewater District (Tidewater):** Comprises a hotel and amenities, Jack Nicklaus Signature Golf Course, transit station and high-end retail that serves the Tidewater guest. The Tidewater's character will be evocative of a Virginian plantation estate, transformed into a grand Tidewater hotel overlooking the Potomac River.
- **The Woods District (Residential):** A lush, forested environment that conveys its tranquil location among the peninsula's pristine highlands. Walking distance from the heart of Potomac Shores, the Woods will be host to medium-density residential neighborhoods.
- **The Lake District (Business):** Captures the spirit of its grand, landscaped boulevard and refreshing water features. A wellspring for inspiration and innovation, the Lake will welcome the Corporate Campus, an eclectic mix of office buildings and the creation of a new exciting employment center.
- **The Riverbluff District (Residential):** An artful residential destination with breathtaking views. Named for its blufftop

**Attachment E  
Design Guidelines**

promontory setting, the Riverbluff will feature urban high-rises, contemporary condos, perhaps artist's lofts and studios, plus a lively promenade overlooking the Potomac River.

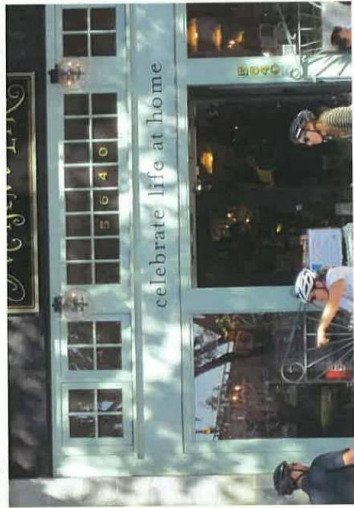
- **The Waterfront District (Waterfront):** Home to the marina and marina village, The Waterfront will be a charming world of boats, fishing, seafood broils and adventure, designed to give people the sense of living during the days when generations of watermen made their living off the shores of the peninsula.



*Tidewater District*



*Waterfront District*



*Marketplace District*

# Attachment E Design Guidelines

## 2.0 THE DISTRICTS

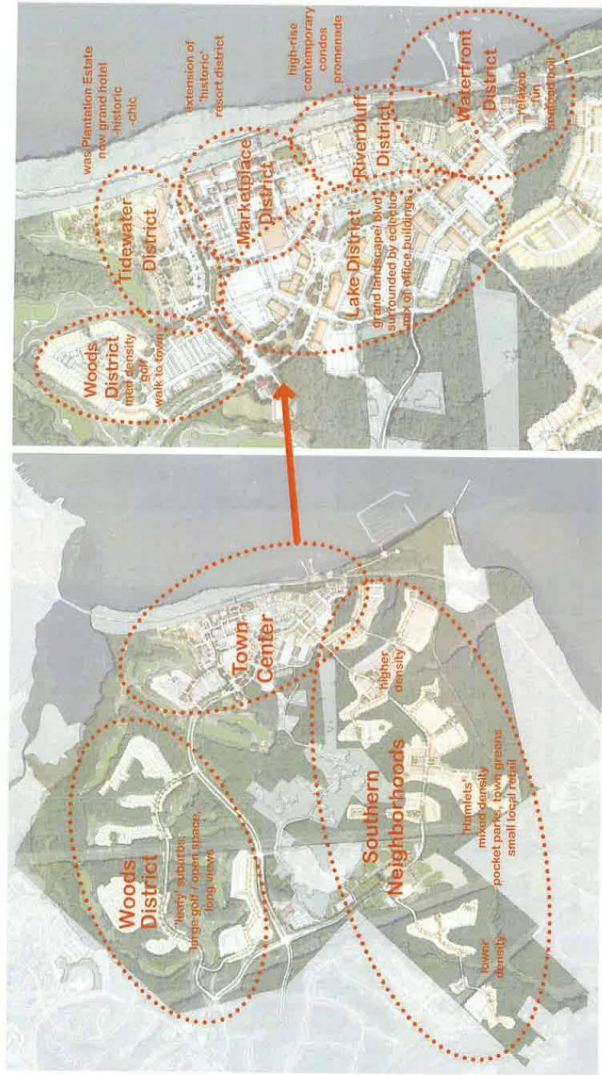


Figure 3.1: Potomac Shores District Map

*Note: A map showing final boundaries of the six Districts will be made available by the DRC. The plan shown above is based on conceptual thinking that may change in future plans.*



### 2.3 TIDEWATER AND MARKETPLACE DISTRICTS

These districts are strongly related in character and share the following elements:

- **Land use:** Conceived as an historic plantation estate and original settlement transformed into a grand hotel, buildings dominate over landscapes and are visually prominent.
- **Architecture:** Expressing Tidewater vernacular traditions with simple, composed volumes and generous porches of painted brick and wood siding. Colors are predominantly white with dark, rich color accents such as green-black.
- **Streetscape:** Based on a gridded block system, the streetscape has historic brick sidewalks and streets with traditional lantern streetlights. The Tidewater District will have its own distinct street furnishings, paving materials and signs. The Marketplace District will share a common architectural sensibility, but will introduce a broader range of details moving away from the Tidewater District.
- **Sign character/Sign types:** Based on historic precedents and design principles (Signs should not be actual replicas of historic signs, however.) Designs should emphasize simplicity and craftsmanship. Signs shall use an appropriate scale to complement the historic street scale. Contemporary and “edgy” designs/materials and neon or theatrical lighting are generally not allowed.

Greater sign variety is encouraged within the Marketplace District. Signs should generally be more traditional close to the Tidewater District and may transition to a more contemporary design direction south of the Train Station.

### 2.4 WOODS DISTRICT

The Woods District is conceived as an area closely tied to the Tidewater District, that developed at a later time. It's character is determined by the site's topography, views and wooded setting. Buildings are more contemporary, although based on Tidewater design principles. Signage should be more traditional and should be consistent with the architecture.



*The Tidewater and Marketplace Districts will have a rich pedestrian character and shall use more traditional signs.*

## 2.0 THE DISTRICTS

### 2.5 LAKE AND RIVERBLUFF DISTRICTS

The Lake and Riverbluff Districts share common elements:

- **Land use:** based on a vernacular tied to the site's topography rather than a gridded street pattern.
- **Architecture:** nestled into a forested setting where landscape dominates over built form; buildings are more contemporary, although based on Tidewater design principles.
- **Signage:** contemporary with a "classic" sensibility.
  - o May use contemporary design details, materials and lighting.
  - o Scales appropriate to larger scale urban environments, buildings and streets; aimed at vehicular as well as pedestrian legibility.



*Lake and Riverbluff District signs are contemporary but with a "classic" sensibility and suited to larger scale urban environments.*



### 2.6 WATERFRONT DISTRICT

Conceived as a district that offers a more relaxed atmosphere appealing to younger user groups, the Waterfront is distinct from the other districts:

- **Land Use:** Vernacular based on the site's topography, merging into a gridded retail street environment.
- **Architecture:** Contemporary but also "edgy", hip/fun.
- **Streetscape:** urban, lively, using an eclectic mix of details, with an animated night-time scene.
- **Sign character/Sign types:** Edgy, fun, innovative, varied, "loud" is acceptable, not quiet/classic/traditional.



*Waterfront District signs are innovative and lively*





2.7 SIGN OPPORTUNITIES

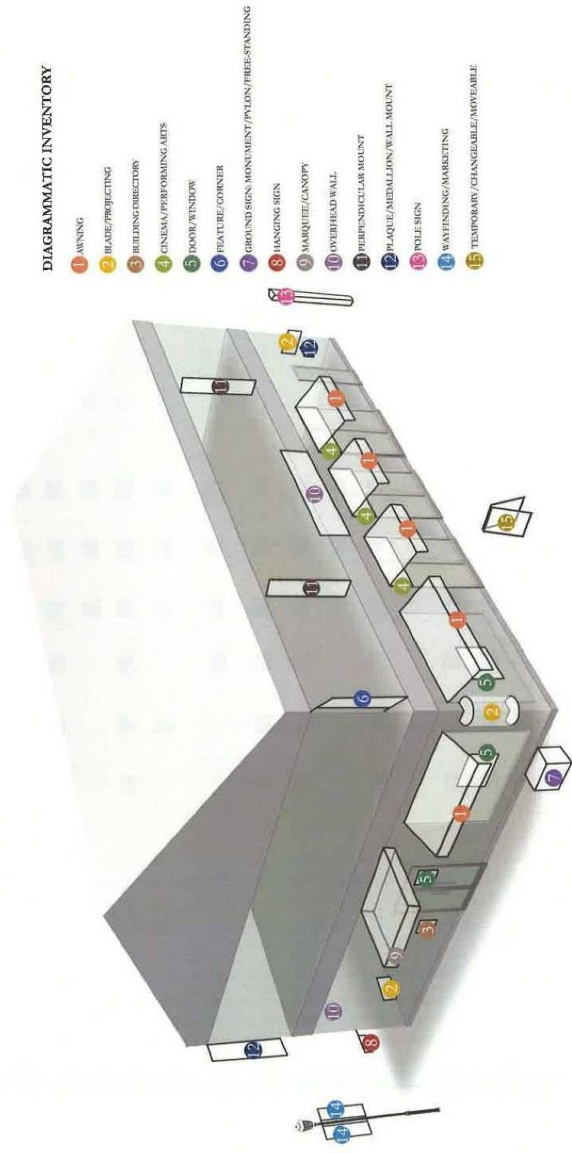


Figure 4.3: Sign Opportunities  
Note: Signs are generally limited to 1st and 2nd story locations

## 2.0 THE DISTRICTS

### 2.8 SIGN STANDARDS BY DISTRICT

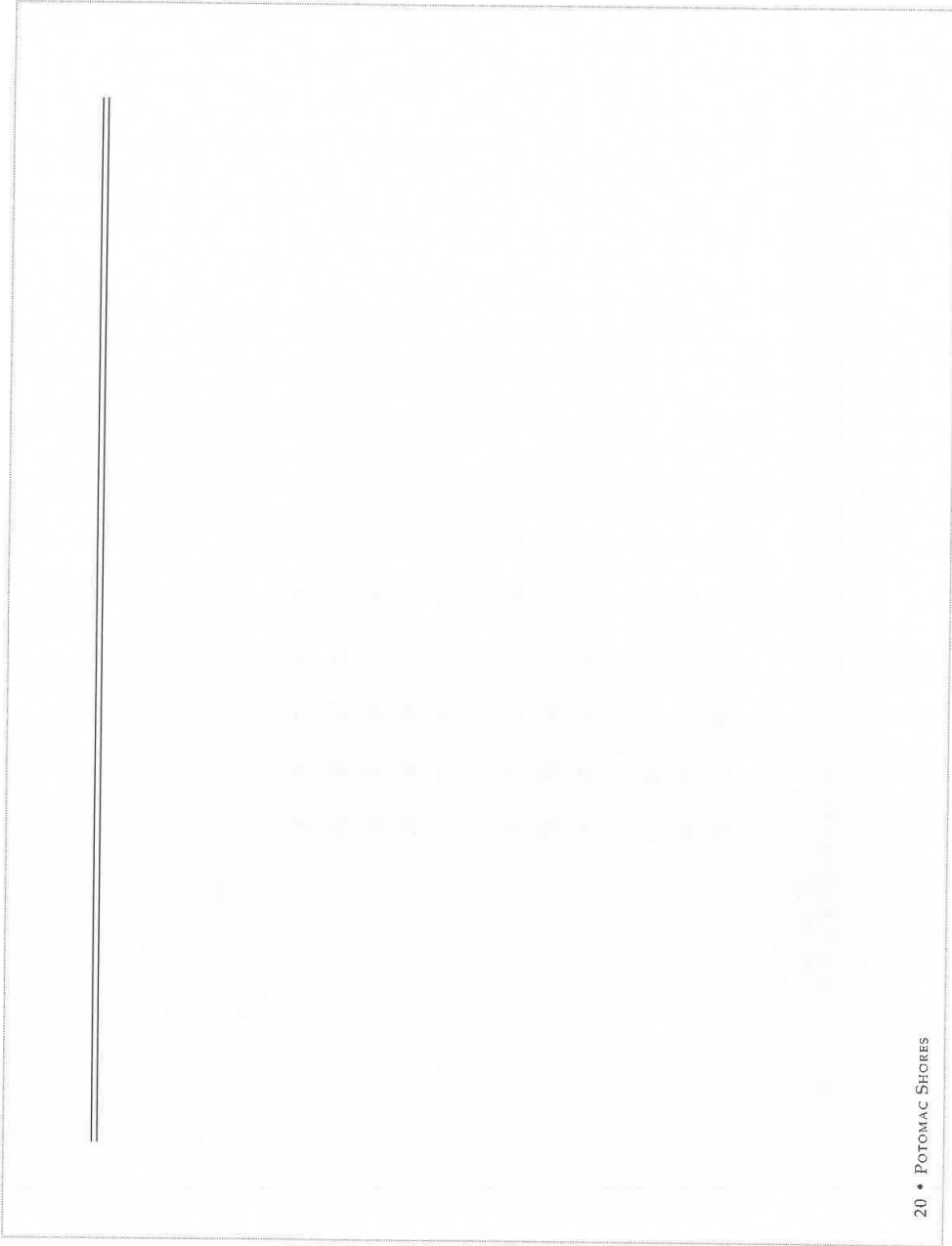
SIGN TYPE	Telephone/Utilities		Woods		Lake		Riverbank/Waterfront		Shoreline		Maximum Size for Sign	Lighting (A)
	District	Distance	District	Distance	District	Distance	District	Distance	District	Distance		
<b>I. PERMANENT SIGNS</b>												
1 Awning Signs	■		■		■		■		■		80% of building frontage on street	external
2 Blade/Projecting Signs	■		■		■		■		■		25 s.f.	internal/ external
3 Building Directory Signs	■		■		■		■		■		20 s.f.	internal/ external
4 Cinema/Performing Arts											60 s.f.	internal/ external
5 Door/Window Signs	■		■		■		■		■		10% of door/window area	internal
6 Feature/Corner Signs											40 s.f.	internal/ external
7 Ground/Monument/ Free-standing Signs	■		■		■		■		■		60 s.f.	internal/ external
8 Hanging Signs	■		■		■		■		■		8 s.f. excluding bracket	internal/ external
9 Marquee/Canopy Signs											80% of marquee/ canopy frontage or 30 s.f. (whichever is lesser)	internal/ external
10 Overhead Wall Signs											25% of building frontage on street or 30 s.f. (whichever is lesser)	internal/ external
11 Plaque/Medallion/Wall Mount Sign	■		■		■		■		■		6 s.f.	external
12 Pole Signs											20 s.f.	internal/ external
13 Neighborhood Monuments	■		■		■		■		■		400 s.f.	internal/ external
14 Wayfinding/Marketing Signs	■		■		■		■		■		12 s.f.	internal

# Attachment E Design Guidelines

SIGN TYPE	Tallpoles/Minipoles		Woods		Lake		Rivers/Inlet/Waterfront		Signs, Max. Height		Minimum Size for Sign	Lighting (5)
	District	District	District	District	District	District	District	District	District	District		
<b>TEMPORARY SIGNS</b>												
<b>A. Temporary Construction Signs</b>												
1. Builder Sign	■	■	■	■	■	■	■	■	■	7 feet	32 s.f.	not allowed
2. Construction Barricade Sign	■	■	■	■	■	■	■	■	■	7 feet	150 s.f.	not allowed
3. Construction/Project Sign	■	■	■	■	■	■	■	■	■	7 feet	12 s.f.	not allowed
<b>B. Temporary Realty Offering Signs</b>												
1. Lot Marker/Survey Stakes	■	■	■	■	■	■	■	■	■	4 feet	3 s.f.	not allowed
2. Realty Offering signs-commercial use	■	■	■	■	■	■	■	■	■	20 feet	32 s.f.	not allowed
3. Realty Offering signs-residential use	■	■	■	■	■	■	■	■	■	4 feet	3 s.f. (8)	not allowed
<b>C. Temporary Marketing Events Signs</b>												
4. A-Frame Signs	■	■	■	■	■	■	■	■	■	3.5 feet	6 s.f.	not allowed
1. Banners	■	■	■	■	■	■	■	■	■	varies <sup>(1)</sup>	50 s.f. 4 feet max. projection	not allowed
2. Community Event Signs	■	■	■	■	■	■	■	■	■	(3)	32 s.f.	not allowed
3. Political Campaign Signs	■	■	■	■	■	■	■	■	■	4 feet	3 s.f.	not allowed
4. Wayfinding/Marketing Signs	■	■	■	■	■	■	■	■	■	16 feet	32 s.f.	not allowed

(1) DRC to evaluate on case-by-case basis  
 (2) 8 feet minimum clearance over sidewalk, 15 feet over pavement  
 (3) 12 feet minimum clearance over sidewalk, 15 feet over pavement  
 (4) A community standard post shall be used according to these guidelines  
 (5) Internally illuminated signs may only be considered with individually illuminated letters and should utilize LED technology. All internally illuminated signs may be approved on a case-by-case basis  
 Note: Sign types allowed within Districts are denoted with the symbol

**Attachment E  
Design Guidelines**



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## **3.0 GENERAL GUIDELINES**

- 3.1 Introduction
- 3.2 General Guidelines
- 3.3 General Sign Types and Definitions
- 3.4 Sign Materials and Design

### 3.0 GENERAL GUIDELINES

#### 3.1 INTRODUCTION

These guidelines are intended to provide a comprehensive framework for commercial signs at Potomac Shores. A variety of commercial signage will occur within the community. Commercial signs may reflect business, tenant or building names, or advertise a product or activity.

All signs, as defined by these guidelines, shall be subject to a discretionary aesthetic appropriateness review by the Potomac Shores DRC (Design Review Committee) in order that signs are consistent and in harmony with the Potomac Shores design vision. The DRC shall use the graphics and language in these guidelines to make a determination of appropriateness on a case-by-case basis.

The Tenant/Property Owner, at its sole expense and effort, should pursue any municipal approvals and/or variances necessary to install the sign package approved by the DRC. DRC aesthetic design approval does not guarantee the success of any municipal approval or variance application. Refer to Chapter 5: Review and Approval Process for information regarding the design review process.

#### 3.2 GENERAL GUIDELINES

The following general guidelines apply to signage:



*If the sign is a freestanding monument (as shown in this image), measure length times height of monument. If sign letters are attached directly to a building wall, measure length times height of letters.*

1. Signs shall comply with all applicable building codes and County regulations.

2. Signs shall be properly maintained. Exposed surfaces shall be clean and paint shall not be flaked. Signs shall be sound, kept in a vertical, upright position at all times, defective parts shall be replaced and landscaping shall be maintained in good condition.

3. Signs shall not create confusion with traffic lights, create a traffic or fire hazard, impair visibility at any street intersection or driveway entrance, or create conditions adversely affecting public safety.

4. Signs standards are generally as set forth in Figure 4.4 Sign Standards by District. If variations from these standards are needed, the Applicant shall seek written approval from the DRC and County, if applicable, on a case-by-case basis.

#### 3.2.1 Lighting Guidelines

Unless otherwise specified by these guidelines, all signs may be illuminated. The following lighting guidelines shall apply:

1. Lighting shall be evaluated for appropriateness by the DRC according to context. Lighting is to complement the District character, streetscape, architecture and proposed use. Generally, sign lighting shall apply to non-residential uses only.

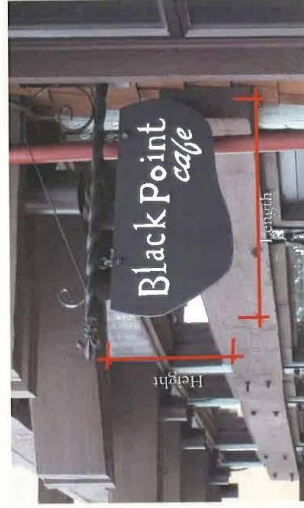
2. Use of neon lighting shall be restricted to the Waterfront District and/or entertainment/hospitality uses within other districts as deemed appropriate by the DRC.

3. Moving and flashing lights are generally not allowed. Artistic/creative specialty signs with LCD or LED informational displays, such as for cinema/performance arts, are allowed but require the





Measure sign framework



Atypical dimensions (multiply the two largest dimensions)

special approval by the DRC.

4. Illuminated signs shall not create a nuisance to nearby residential uses. Therefore, illuminated signs shall be located a minimum distance of fifty (50) feet from a residential use or residential property boundary, and shall be evaluated by the DRC on a case-by-case basis according to adjacent land uses conditions.
5. Illuminated signs shall be screened, shaded or shielded so as to not produce glare or shine directly on a residential use.
6. Backlighting of individual letters is allowed. Internal lighting and inverse illumination are strongly discouraged. Cabinet construction "light boxes" (where there is a singular form with internal lighting) are prohibited.
7. Hours of lighting operation are limited according to each District and shall be determined by the DRC. Hours of operation are subject to change by the DRC from time to time. To support a lively street life well into the evening, exterior lighting of storefronts, interior displays and signs within retail and entertainment districts may be encouraged, with required hours of operation controlled by time clock.

**3.2.2 Allowable Building Wall Signage and Sign Measurement**

Identity signs are to be placed on the primary façade of a building. Where buildings have more than one primary façade (such as corner locations) and where there is a visible secondary entry, additional signs may be allowed by the DRC. Total signage on a given building side will be generally limited to 10% of the façade area for commercial identity signage.

Door and window signs and temporary signs are not calculated as part



### 3.0 GENERAL GUIDELINES

of the allowable building wall signage. Refer to 4.4 Sign Standards by District for maximum allowable sign area and maximum allowable mounting height for each sign type.

Sign area shall be measured in one of two ways:

- a) by multiplying the length and height of the sign panel framework (in the case of freestanding signs independent of buildings).
  - b) by multiplying the length of a lettered or numbered message, name or statement by the height of the lettered or numbered message. If an advertisement exceeds a single line, the dimensions of each line are added for a total. This would apply to signs on awnings and building frontages.
- Only one face of a double-faced sign shall be counted in calculating sign area, provided sign faces do not have greater than a 45 degree angle measured from the apex.
  - Logos and icons are counted in the calculation of signage area by multiplying the height by the width of the logo or icon.
  - Specialty signs with atypical dimensions shall be calculated by multiplying the two largest dimensions.



*Community signs such as traffic regulatory, street name and directional signs will be developed for the community but are not governed by these guidelines.*

- Artwork, such as building paintings/murals not intended for advertising and/or announcements are not included in calculations of building signage unless the painting/mural displays corporate logos or icons.

- Hanging brackets are not included in calculations.

#### 3.2.3 Sign Location

Signs are generally limited to first and second story locations. Signs mounted projecting from a building frontage should maintain minimum clearances of eight (8) feet height above sidewalks and fifteen (15) feet above paving. Signs mounted on the roof shall not project above the ridge/line. Signs shall not extend beyond the curb line.

#### 3.2.4 Sign Types

**These guidelines do not govern the following sign types:**

- building design
- community signs (such as community identity, facility signs and trail signs)
- display or improvement not defined herein as a sign.
- gravestones
- major public roadway signs (traffic regulatory signs, street names, directional signs). Signs on private roads shall be designed as part of community sign master plan.

- official and government signs
  - religious plaques
  - signs which are not intended to be viewed from any street, road, highway, public or pedestrian private way;
- These guidelines govern the following sign types:
- awning signs
  - banner signs
  - building tenant directory
  - business identification signs
  - canopy/marquee signs
  - cinema/performing arts sign
  - display of street numbers
  - door/window signs
  - electronic (or digital/dynamic) signs, computer-driven LED, LCD and plasma displays
  - flags
  - graphic/logo signs
  - ground/monument signs
- home business signs
  - motor vehicle fuel station
  - murals
  - overhead wall signs
  - plaque/medallion/wall mount signs
  - professional name plates
  - projecting/hanging signs
  - roof signs
  - sandwich board signs
  - specialty/creative signs, including animated LED/LCD displays
  - storefront sign
  - temporary signs, including banners, bunting and flags
  - tenant directory signs
  - temporary and permanent way finding/marketing signs
- The following sign types have restricted use:
- Flags are allowed, but are restricted to an aggregate maximum size not to exceed the total s.f. of signage allowed per building. Patriotic flags are to be displayed according to State and County regulations or standards and are limited to three (3) in number, one of which may be the corporate flag. Large scale flags and/or freestanding in-ground flag poles may be allowed by the DRC on a case-by-case basis.

### 3.0 GENERAL GUIDELINES

Building mounted flag poles are preferred.

- Collection Boxes shall be located on private property and entirely screened from public view.
- Home business signs and professional name plates are generally **not** allowed in residential districts but may be allowed in higher density residential (townhomes) and mixed use districts.
- Personal name signs are allowed only in residential districts.
- Motor vehicle fuel station signs are allowed as monument or pole signs. Canopy signs are **not** allowed, although exceptions may be made by the DRC on a case-by-case basis.
- Multi-tenant building directories are allowed.
- Stacked large scale off-site multi-tenant monument signs are **not** allowed.



*Professional name plates*



*Multi-tenant building directories*

The following sign types are **not** allowed

- billboards
- electronic message center (lampbank control by programmable computer).
- flashing and/or physically moving signs (except as noted)
- inflatable objects, noise emitting or fan-operated attention-getting devices (such as person standing at intersection waving sign).
- mall entrance sign
- mobile sign (stationary trailer or vehicles)
- multi-tenant building signs (other than directory sign).
- roof signs with free-standing support structures not tied to architecture or projecting above ridge/line.
- snipe signs (signs attached to utility poles, walls, or other places for advertisement)
- water tower signs

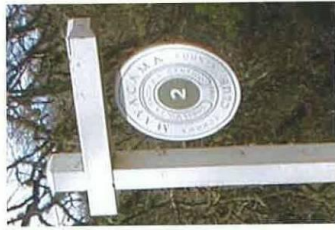
**Temporary Signs**

Temporary signs include construction signs, realty offering signs, marketing/event signs, sandwich boards and sales promotional window signs. No temporary sign shall require a County sign permit, but DRC review and approval is required.

- All temporary signs may be single or double-faced.
- Signs may be placed perpendicular, parallel or angled to the roadway.
- Temporary signs that intrude into rights-of-way shall have written approval from the DRC.
- Signs to be located within public rights-of-way require written approval from the Virginia Department of Transportation.
- Construction Signs shall be removed two days after end of construction or building occupancy.
- Realty Signs shall be removed two days after closing.
- Banner Signs, Community Event Signs require special DRC approval and shall be removed two days after the event.
- Political campaign signs may only be displayed on private property with the property owner's consent and shall be removed two days after the election for which they were intended.



*Realty offering*



*Lot marker/survey stake*



*Sandwich board*



*Temporary sales promotion*



### 3.0 GENERAL GUIDELINES

#### 3.2.5 Sign Spacing

Minimum spacing – the minimum spacing between signs is as follows:

- Ten (10) feet between arcade signs
- Ten (10) feet between blade signs
- Eight (8) feet between building signs
- Four (4) feet between awning signs
- Ten (10) feet between neon signs parallel to rights-of-way
- Twenty-four (24) feet between neon signs perpendicular to rights-of-way



*Required minimum spacing between building signs and awnings shall be per the guidelines*



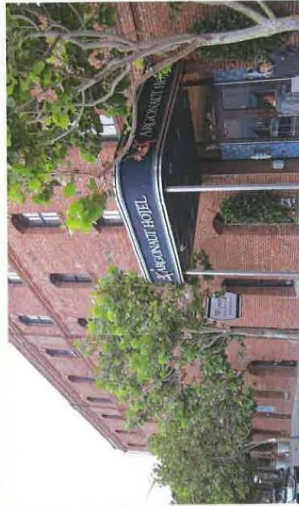
*Repetitive signs that are spaced too closely together (not allowed)*

### 3.3 GENERAL SIGN TYPES AND DEFINITIONS

The following are general sign types that may be used at Potomac Shores, appropriate to context and the standards within these Guidelines.

**Type 1 – Awning Sign:** Awning signs can be used as a primary or secondary sign opportunity. Their strength lies in repetition, as awnings often run the full length of a storefront or building facade. Graphics may be applied to the primary surface or the valance surface of the awning. Graphics may include the tenant's logo or typographic signature, services offered, or even just color and pattern.

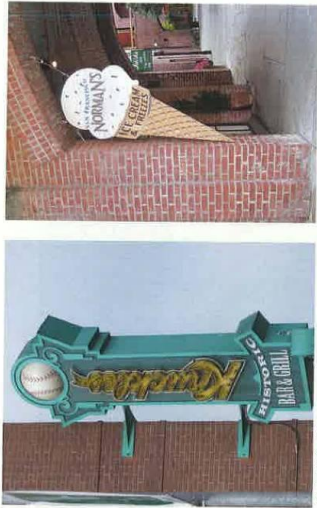
Awnings are limited to 1 per window and may not exceed 80% of the building street frontage. Valance depth is 18" maximum. The copy area or individual letter size of an awning sign cannot exceed 80% of the valance.



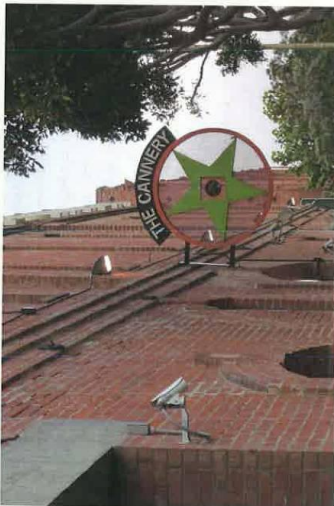
*Graphics may be applied to the primary surface or valance of the awning.*

### 3.0 GENERAL GUIDELINES

**Type 2 – Blade /Projecting Sign** – Blade signs may mount to building facades and will typically include decorative metal brackets. Wood or metal panel signs are allowed. Blade signs may be double-sided. Signs may protrude into a driveway or road right-of-way provided a vertical clearance of fifteen (15) feet is maintained and only with the permission of the DRC. Signs projecting into public street rights-of-way also require VDOT approval.



*Colorfully illuminated signs and specialty signs are best used in animated urban settings such as the Waterfront District*





**Type 3 – Building Directory Sign:** Building Directory signs shall be positioned to be read from eye-level by a pedestrian on-site. Directory signs may be attached to the building frontage, mounted on a site wall, or may be freestanding.



**Type 4 – Cinema/Performing Arts:** Cinema and Performing Arts signs are subject to DRC special approval and will be judged for appropriateness according to context. Generally, this type of signage may consist of a combination of lower signs, building signs and changeable poster boxes and marquee signs. Changeable/movable letters are allowed only if located inside the building.

This category also includes specialty animated signs and electronic (or digital/dynamic) signs, computer driven LED, LCD and plasma displays on which video or multimedia content can be displayed for informational, promotional or advertising purposes.

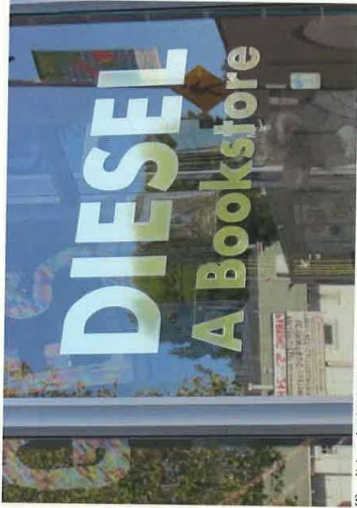


*Cinema Performing Arts sign with computer-driven electronic display*

### 3.0 GENERAL GUIDELINES

**Type 5 – Door/Window Sign:** Besides the boilerplate entry: door graphics and hours of operation, tenants are encouraged to add more interest and identity to their building façade with signs placed directly on or behind the glass of windows and/or doors. Window signs may be painted on, affixed to, or suspended within the window frame to be viewed from outside. Signs may use screen-printing, gilding or machine-cut vinyl; skeletal framed neon graphics; or individual dimensional letters or logos adhered to the second surface of the glass. Like awning signs, these graphics can take advantage of a long length of storefront for their impact.

Door/Window signs are not measured as wall signage and generally shall not be counted against the total allowable signage for a building.



*Traditional gilding (Marketplace District)*



*Screen printing on glass (Lakes, Riverbluff and Waterfront Districts)*



*Neon signs placed directly behind glass windows (Waterfront District)*

**Type 6 Feature/Corner:** Corner signs for corner buildings may either wrap the corner or project out like a blade sign and be visible from two directions. These signs may be cut panels or cut shapes. Building-mounted signs take advantage of strategic locations with multiple view angles. When used as a primary identifier, these signs will be larger and have a deeper profile than a typical “blade sign”.

This sign type is often sculptural and may be colorfully illuminated. It is intended to add vibrancy and should occur only in highly animated urban neighborhoods such as the Waterfront District



*Blade sign with large scale and deep profile*



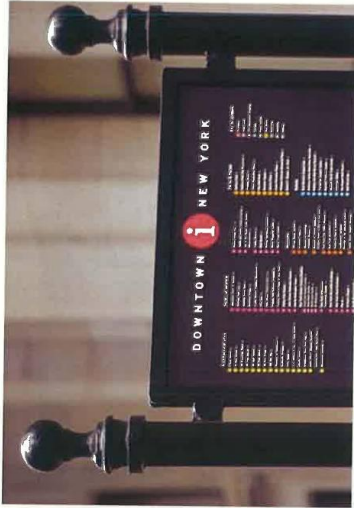
*Feature signs are used at strategic location with multiple view angles*



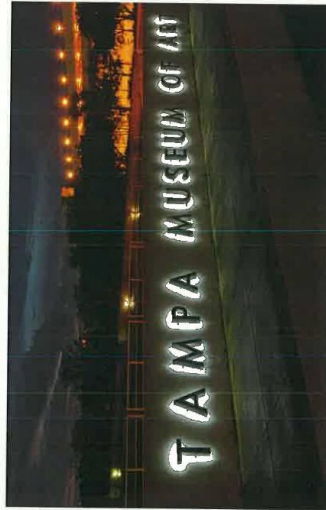
### 3.0 GENERAL GUIDELINES

**Type 7 – Ground/Monument/Freestanding Sign:** Ground signs are wholly separated from any building, though they do need to be located on the same site. They can identify an entry to a building or an entry to a site. They can be intended for vehicular or pedestrian traffic. This sign type may be appropriate for tenant directories, business identification and wayfinding.

This sign type shall be carefully located so as to not obscure sight lines or present a pedestrian hazard.



*Examples of appropriate ground/monument/freestanding signs*

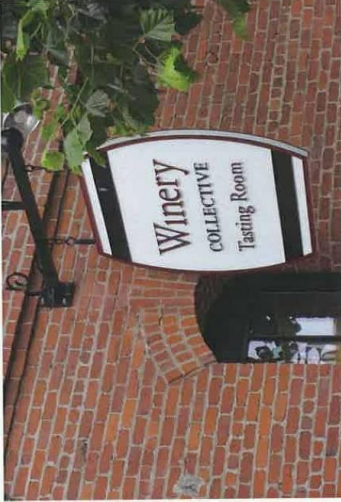


**Attachment E  
Design Guidelines**

**Type 8 – Hanging Sign:** Hanging signs are two-sided panels with various shapes, materials and graphics, hung from overhead arcades and/or mounted to building facades with ornamental brackets. Hanging signs can be contemporary or traditional in character. Hanging signs are particularly suited to pedestrian and retail environments, where they may be used in combination with storefront signage, awnings and arcades.



*Waterfront District signs are more fun/edgy*



*Contemporary graphics to complement architecture and urban environment (Lake District)*

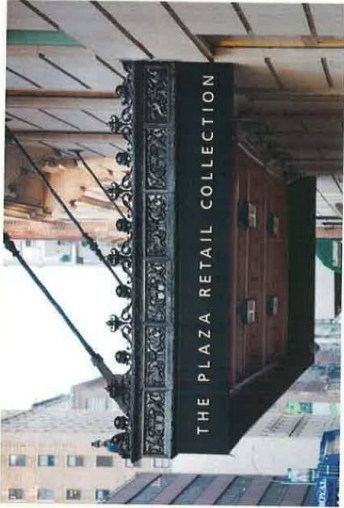


*Traditional hanging signs (Marketplace, Tidewater and Waterfront Districts)*

### 3.0 GENERAL GUIDELINES

**Type 9 – Marquee/Canopy Sign:** These signs take advantage of an architectural feature of a building, typically the entry. The sign types should be closely integrated with the architectural style and expression with appropriate style, form, materials, finishes and lighting.

Signs may use panels and/or letter forms that are attached to or project above the horizontal surface of an architectural marquee or canopy. They provide the main identity for tenants who occupy spaces where an entry canopy is part of the basic building façade. Their location makes them more reverent or sympathetic to the building's street presence.



*Traditional style (Marketplace District)*



*Contemporary architectural style (Lake and Riverbluff Districts)*



### 3.0 GENERAL GUIDELINES

**Type 10 – Overhead Wall Sign:** Wall signs are typically the most common primary sign type.

Wall Signs may be intended for long distance viewing, or may be traditional storefront entry signs intended for pedestrian viewing and everything in-between, as long as the sign remains overhead and relatively flat against the building surface.

Overhead wall signs may be painted directly on the surface of the building or affixed to any side of the building excluding the parapet and roof.



*Building entry sign intended for pedestrian viewing*



*Overhead wall sign intended for long distance viewing*

### 3.0 GENERAL GUIDELINES

**Type 11 – Plaque/Medallion/Wall Mount Sign:** A smaller version of a wall sign, near eye level or lower, even located on the ground plane. This sign type is a humbler or lower-key sign expression, often displayed by small boutique retail or food and beverage tenants or used as address signs. Because of their proximity to the public, these signs must be crafted of the highest quality and materials.

This sign type may use a flat slab, disk or medallion, ornamented or engraved, for mounting on a wall at or near eye level. Plaques may also be used as directional and accessibility signs or as commemorative or historic markers.



*Commemorative plaques (cast bronze)*



*Address sign*



*Accessibility directional sign*

**Type 12 – Pole Sign:** Pole signs are typically designed to be viewed from a distance. They may be double-sided and the preferred form is columnar. One-sided supports are discouraged. Pole signs shall be carefully located within sidewalks so as to not create a visual hazard.

Freestanding parking lot pole signs shall use a standard approved sign at Potomac Shores. The approved standard is a simple columnar monument with overall dimension of 9 feet height x 2 feet width x 8 in. depth. Sign may be elevated an additional 2 feet in height, where need can be demonstrated and subject to DRC approval. Refer to parking lot pole sign approved standard below.



Standard approved sign.



Sign with one-sided support.

**Type 13 – Neighborhood Entry Monuments:** A horizontal or vertical monument typically erected as a marker for the entry into a specific district. These will be evaluated on a case-by-case basis by the DRC and are typically constructed out of masonry and/or metal, may include internal or external lighting, and are part of a larger landscape and streetscape design.



Dunnington Entry



Dunnington Entry - enlarged view



Potomac Shores Parkway Entry

### 3.0 GENERAL GUIDELINES

**Type 14 – Wayfinding/Marketing Sign:** A coordinated program of wayfinding/marketing signs may be used throughout the Potomac Shores community as part of an overall marketing sign program, providing temporary marketing information as well as permanent wayfinding and civic identification.



*Community wayfinding and marketing flags and banners*



**Type 15 – Temporary/Changeable Moveable:** Temporary Signs are signs that are either temporary, promotional or seasonal in nature. This sign type includes menu shadow boxes, poster display cases, event banners/flags, display stands/casels and sidewalk a-frame/sandwich board signs.

**Builder Sign** - A freestanding sign attached to construction fencing that identifies the name of the general contractor and contact information for a project under construction. Typically this sign is posted at the project entry.

**Construction Barricade Signage** - Temporary construction barricade signs installed around the perimeter of construction sites that prevent the general public from entering a construction work zone. At the same time, these signs can serve as a marketing tool to generate excitement about the community and tell people when various districts and amenities are opening.

**Construction/Project Sign** - A freestanding sign or sign attached to construction fencing, that contains information about a future project currently under construction with project description, such as the name of the project, developer and general contractor.

**Lot Marker/Survey Stake** - A temporary wood sign with post and horizontal mast arm, with a hanging sign identifying a lot number for real estate sales.

**Realty Offering** - A freestanding sign or sign attached to the face of a building façade or window, that contains information about the sale or lease of a property, including realty firm name and contact information. Different sign standards apply for residential



Builder sign



Lot marker



Commercial realty



Menu shadow box



Construction/project sign



Residential realty (approved standard)

### 3.0 GENERAL GUIDELINES

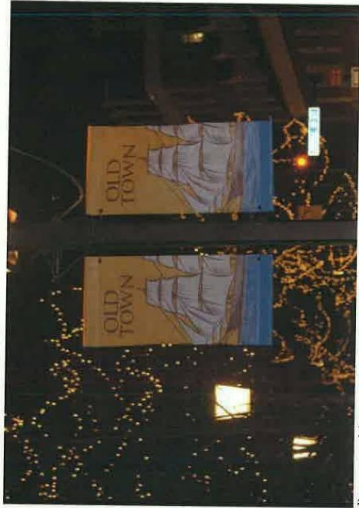
and commercial offerings (Refer to 4.4 Sign Standards By District). For consistency, residential realty offering signs shall be required to use a community standardized design. This shall consist of a white painted wood post with horizontal mast arm. The post and arm are without chamfers. Hanging signs may be one or two-sided and shall be a standard rectangular size (1.5 feet in height x 2 feet in width). Only one sign is allowed per post. A simple white receptacle for fliers may be attached to the sign post.

**A-Frame Sign** - A temporary two sided sign, also called a sandwich board, used to advertise sales and special offerings, particularly in retail environments. These may be double sided. These signs cannot interfere with traffic and shall be removed from the sidewalk when business is closed. Projecting bases which create a pedestrian hazard are not allowed. Inexpensive off-the-shelf plastic sandwich signs are not allowed.

**Banners** - Banners must be made of durable fabric such as nylon, canvas or woven fabric and shall be secured to the structure so it does not deform or move. Banners may be either temporary or permanent.

**Community Events Sign** - A temporary sign used to advertise a civic or community event, typically a free-standing sign or banner.

**Political Campaign Sign** - A temporary sign used to promote a political candidate, typically a free-standing sign with a wood stake or a poster mounted to the inside of a window or attached to a building facade.



Banners celebrate civic events



A-frame signs are intended to augment the retail tenant's storefront character and to provide timely information regarding available products or services.



### 3.4 SIGN MATERIALS AND DESIGN

A strong signage presence is desired at Potomac Shores to add vibrancy and unique character to the districts and community. Individual design solutions should complement the district, building architecture, setting and intended use. Although not limited to these considerations, sign designs should consider the following suggested materials and methods:

#### 3.4.1 Material possibilities:

- o Painted wood – varied shapes, painted graphics/letters, combined with traditional mounting details.
- o Metal – Painted finishes for durability, in classic rich dark colors.
- o Metal – natural finishes, polished, brushed, patterned, weathered and/or patinaed (natural or applied finishes).
- o Cast stone/concrete with textured or painted finish.
- o Glass – combined with black iron or stainless steel, use of tinted glass, etched and frosted finishes, traditional gilding, LED internal lighting.
- o Brick, painted or natural finish (for monuments).
- o Awning and banner materials shall use include quality fabrics such as woven fabric or nylon canvas, but shall exclude paper, vinyl, plastic and PVC.

Materials that are allowed:



*Painted wood*



*Metal-painted finish*

*Brick, cast stone/concrete*



*Metal-natural finish*

*Glass*

### 3.0 GENERAL GUIDELINES

#### 3.4.2 Design Methods:

A variety of creative, individualized sign designs are encouraged. Sign shall utilize durable, high-quality materials and express a high level of craftsmanship. The following suggestions are only a few of the many possibilities.

- Fabricated, crafted and assembled signs, combining several complementary materials
- Layering, using materials in surprising ways
- Using new environmentally responsible materials
- Individual letter forms used as signage
- Dimensional, sculptural or representational forms, cleanly executed and not embellished

- Traditional signage methods such as gold and silver leaf, sand-blasted glass, porcelain enamel, carved stone and wrought iron (and others)

The following are not allowed:

- Vacuum-formed plastic graphics
- Tinted storefront windows
- Internally illuminated cabinet construction letters
- Acrylic sign faces shall be matte finish only; shiny faces are not allowed



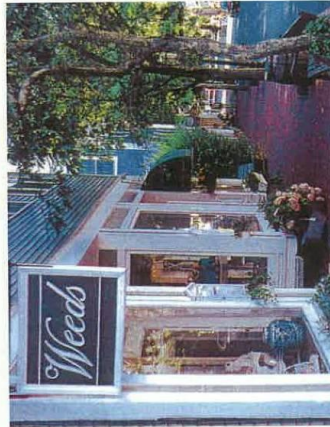
**Attachment E  
Design Guidelines**



*Tidewater, Marketplace and Woods Districts*



*Lake and Riverbluff Districts*

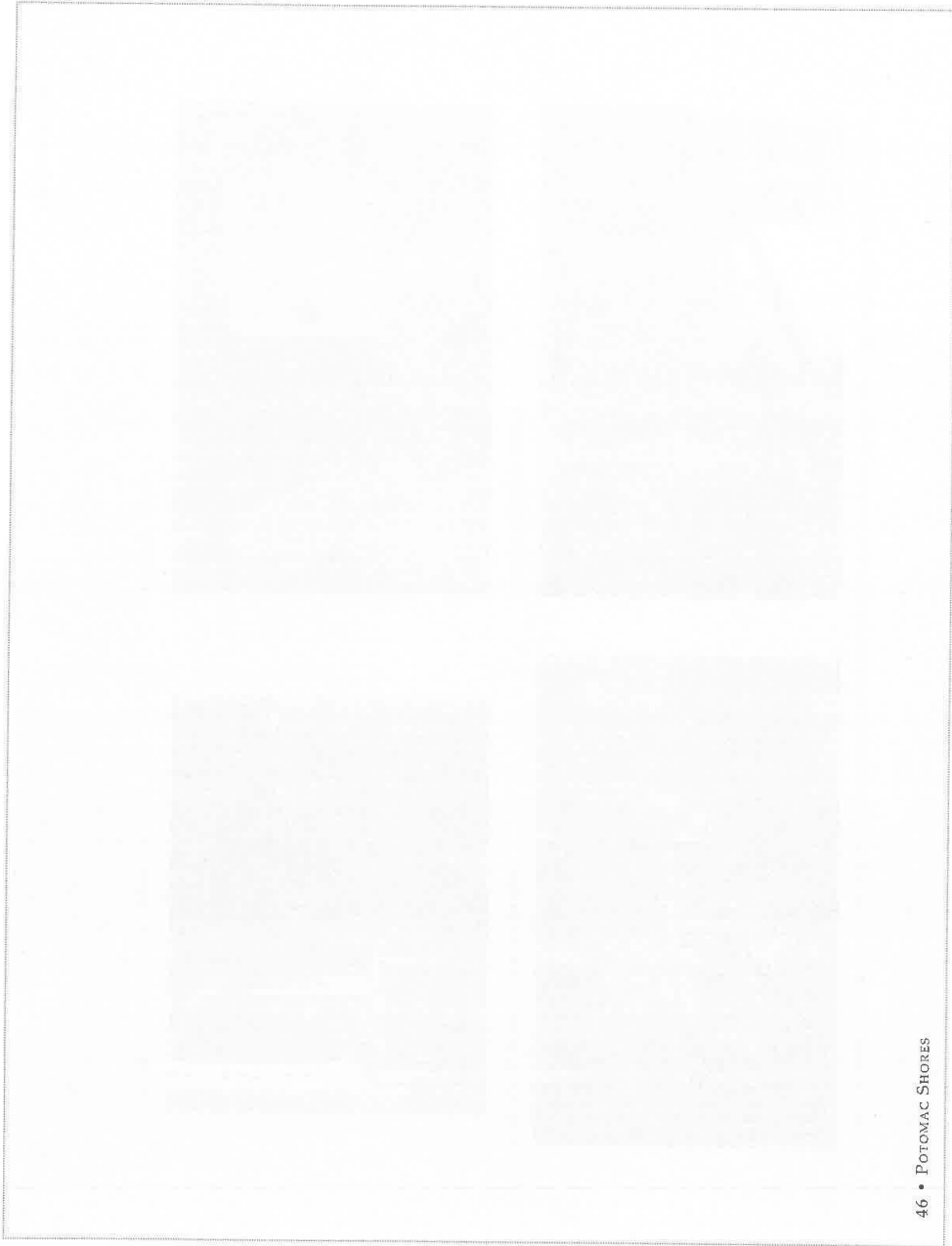


*Tidewater, Marketplace and Woods Districts*



*Waterfront District*

**Attachment E**  
**Design Guidelines**



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## **4.0 REVIEW AND APPROVAL PROCESS**

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#### 4.0 REVIEW AND APPROVAL PROCESS

The Owner/Developer and/or Tenant is responsible for providing and submitting drawings to the DRC which adequately convey compliance with the Design Guidelines for the approval of all signs. Before any construction may commence, the Owner/Developer and/or Tenant must first submit drawings and obtain approval of the DRC and then obtain approvals from local government jurisdiction. The DRC will review signs for aesthetics, suitability of materials and outline dimensions.

No variations or omissions shall be made without specific prior written approval from DRC, which may be granted or denied in its sole discretion. Any modifications to plans are subject to DRC approval. DRC approval of plans and specifications does not imply code compliance or engineering sufficiency. It is the Owner/Developer and/or Tenant's responsibility to ensure that all improvements meet the local Building Code and all applicable codes.

##### Review Process

The review process will include a Preliminary and Final review by the DRC.

##### Preliminary Drawing Submittal

The submittal set should include scaled drawings with complete information regarding proposed signs and their locations in a format that adequately expresses the idea. This may include any or all of the following types of information:

- a) Exterior Building Elevations with critical dimension
- b) Building materials, finishes and colors
- c) Sign details with critical dimensions and showing true location relative to building façade, street/sidewalk and applicable easements, lease lines, property lines and/or street right-of-way.
- d) Sign materials, finishes and colors
- e) Proposed sign lighting (fixtures, attachments and specifications)



**Construction Coordination and Access**

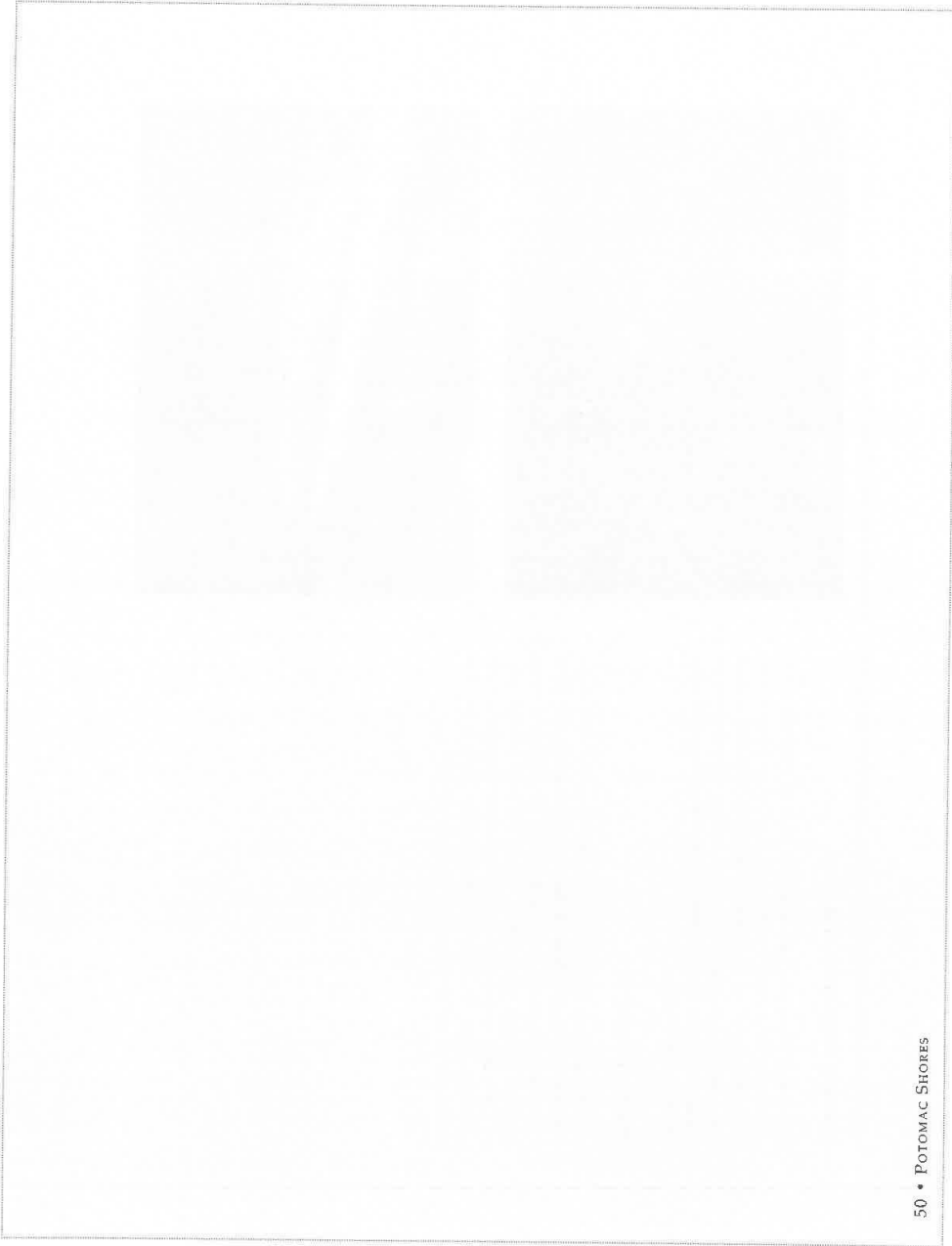
The Owner/Developer and/or Tenant shall be responsible for coordinating the work of its contractor so as not to interfere with the construction of the Site. Further, the Owner/Developer and/or Tenant shall grant to the DRC access to building or tenant space as may be reasonably required for DRC review of work for conformance with the approved design.

**Final Design Approval**

The DRC will issue final design approval prior to Owner/Developer or Tenant's submittal to Prince William County, in writing and within a reasonable period after receipt of drawings. The DRC shall return one set of drawings marked either "approved", "approved as noted" or "revise and resubmit". In the event that final submittals are not approved by DRC, the Owner/Developer and/or Tenant must revise the drawing set and resubmit within 15 business days.



**Attachment E**  
**Design Guidelines**



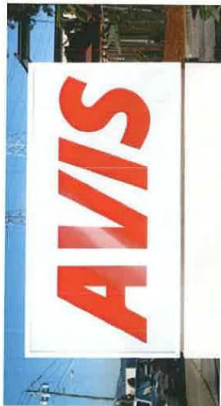
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## 5.0 EXAMPLES & DEFINITIONS

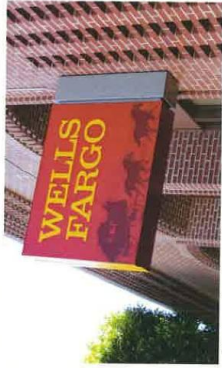
- 5.1 Examples of Bad Signs
- 5.2 Examples of Good Signs
- 5.3 Definitions
- 5.4 Dunnington Rd Entry Monument

## 5.0 EXAMPLES & DEFINITIONS

### 5.1 EXAMPLES OF BAD SIGNS



“Light boxes” (Singular cabinet construction with internal lighting) are not allowed. Shiny, acrylic faces are also not allowed.



Singular cabinet construction with acrylic faces are discouraged. Matte finish acrylic is acceptable.



Exceeding the prescribed maximum of storefront window occlusion is not allowed.



Movable (or permanent) signs with changeable plastic letters. Note: Cinemas may use this sign type inside buildings.



Exposed electrical raceways are not allowed.



Internally illuminated fabric awnings are not allowed.

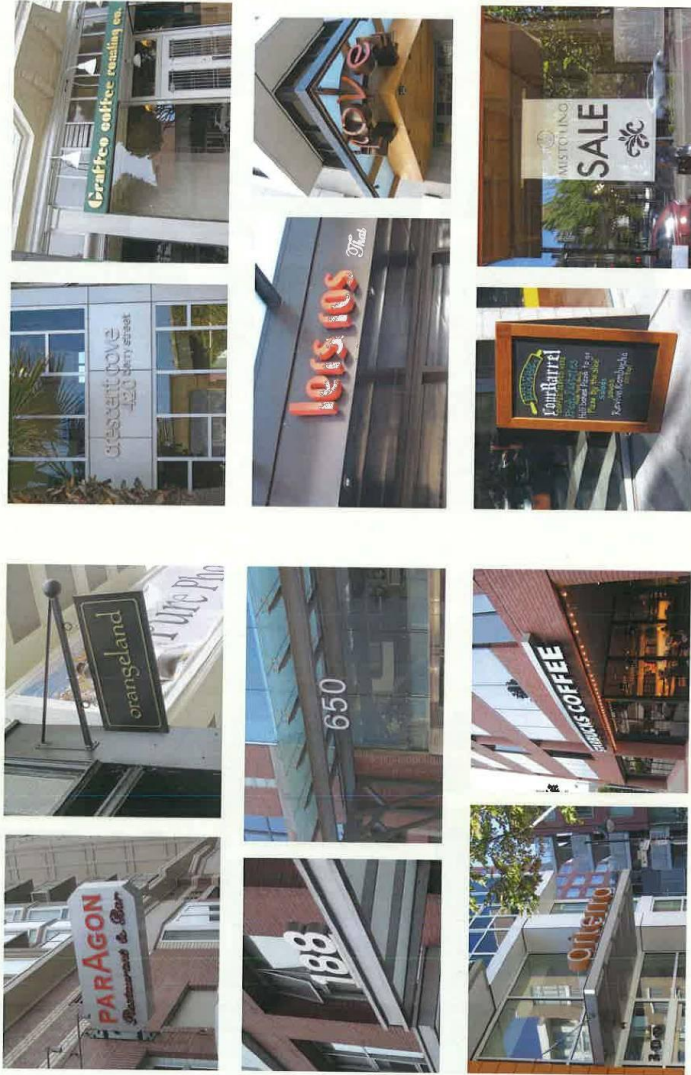


5.2 EXAMPLES OF GOOD SIGNS





5.0 EXAMPLES & DEFINITIONS





### 5.3 DEFINITIONS

For the purpose of these guidelines, the following words have the following meanings, unless the context clearly indicates otherwise. Words used in the present tense include the future tense; words used in the singular include the plural and words used in the plural include the singular. The word "shall" is always mandatory. The word "person" includes a firm, association, organization, partnership, trust, company or corporation as well as an individual. If a word is not specifically defined in this section, the standard dictionary definition(s) will be used.

#### Advertising Display Area (Sign Area)

The total cumulative sign area for all signs which are supported by and part of the same sign structure. For a double-face sign, only one face shall be counted in calculating sign area, provided the sign faces do not have greater than a 45° angle measured from the apex.

#### Animated Sign

A type of specialty sign that is designed to create the illusion of motion.

#### Awning

A sheltering screen extending over or before any building, door or window which provides shelter or protection against the weather. Except as provided for herein, the term shall include any canopy, roller curtain or umbrella.

#### Awning Sign

Any sign, painted, stamped, perforated or stitched on the surface of an awning, canopy, roller curtain or umbrella.

#### Banner Sign

Temporary or permanent signs either with or without frames, possessing characters, letters, illustrations or ornamentations applied to cloth, or fabric of any kind.

#### Building Frontage

The wall of a building that contains the main entrance. If there is more than one wall that contains a main entrance, then "Building Frontage" shall be the shortest of those walls. Where buildings have been divided for more than one use, "Building Frontage" shall be base on that portion of the building delegated to the individual use therein and which contains the main entrance for that use. If a portion of said building is vacant, then that portion of the building reserved for a future use and containing the main entrance for that use shall not be allowed in any calculation of "Building Frontage."

#### Building Sign

A sign fastened to or painted on the wall of a building or attached in such a manner that the wall becomes the supporting structure for and forms the background surface of the sign and which does not project more than 12 inches from such building.

#### Changeable Copy Sign

A sign on which the copy can be changed, rearranged or altered, either manually or through electronic means, without changing the face of the sign. The term shall include electronic message centers and lampbank displays.

## 5.0 EXAMPLES & DEFINITIONS

- Cinema Sign**  
A sign used to advertise a building that houses a movie theater, as well as, advertising movies that are currently playing and upcoming movies.
- Collection Box**  
Drop-off boxes for donations for charitable or non-profit organizations which contain no advertising, other than the name of the charitable or non-profit organization.
- Commemorative Plaque**  
A type of memorial sign, identifying the name of the building or its date of erection, which is cut into any masonry surface, or constructed of permanent or non-combustible material mounted on the face of a building. The commemorative plaque may also contain the names of individuals associated with or commemorated by the building. A commemorative plaque shall not be counted against sign square footage.
- Community Event Sign**  
A temporary sign advertising sales of personal property by not-for-profit organizations and events such as picnics, carnivals, bazaars, game nights, art fairs, craft shows and Christmas tree sales.
- Construction Barricade Sign**  
A temporary marketing sign attached to temporary construction fencing, used to convey community marketing information.
- Construction Sign**  
A temporary sign that is displayed on the site only during the time of actual construction work and indicates the ultimate character of the development or the names of the firms, individuals, or material suppliers involved in the work.
- Copy Area (Sign Face)**  
The area or display surface used for the message on a sign, which does not include the framework or support for the sign.
- Corner Sign**  
A permanent sign attached to a building on a corner lot that may either wrap the corner of the building or project out at the corner to be visible from two directions.
- Directional Sign**  
An on-site giving directions, instructions or facility information and which may contain the name or logo of an establishment, but shall have no advertising. Both temporary directional signs for construction purposes and permanent directional signs are allowed within Potomac Shores, each type of directional sign having different restrictions as noted herein.
- Directory Sign**  
Sign that gives the name, occupation and location of occupants of a building and is primarily designed to be read from on-site.
- District**  
A geographic area designated and utilized by these guidelines for the purpose of regulating the signage.
- Electronic Message Center**  
A sign comprised of independent light bulbs arranged in a lampbank controlled by a programmable computer.

**Attachment E  
Design Guidelines**

**Illuminated Sign**  
A sign that incorporates an artificial light source either internal or external to the sign, for the purpose of illuminating the sign.

**Indirect Lighting**  
Lighting which is not an integral part of the sign, but which is attached to the sign and directed toward the sign to provide illumination of the sign message.

**Inverse Illumination**  
The complete illumination of a lampbank, except for the characters of the message which are not illuminated.

**Lampbank Display**  
Any arrangement of lights affixed to or made a part of a sign, where all or part of the advertising message is created by illuminating the lights, either by electronic or other means, in various patterns to show words, characters, or images of any kind.

**Marketing Sign**  
A temporary sign used to advertise a product.

**Memorial Sign**  
A sign commemorating an event, person, or building, which may contain information of a historical or biographical nature concerning the thing commemorated, but which contains no advertising.

**Monument Sign (Ground Sign/Pylon Sign)**  
A monument sign which may be either a building entry sign or a commercial sign independent of support from any building and which is supported by a structure or supports so that the bottom of

**Entry Sign (Development Identification Sign)**  
Any monument, pole or pier sign with or without a suspension yardarm, or a fence or wall sign, which contains no advertising other than the name of the subdivision or development.

**Flag**  
Usually constructed of fabric, used to display the emblem of a municipality, county, state, or country; or of a business, firm, company, or charitable non-profit organization. The term does not include the pole upon which the flag is mounted. When displayed in the same manner as a flag, the term shall include pennants and banners.

**Government Sign**  
Any sign erected and maintained by Prince William County, the State of Virginia, or the United States Government and which is deemed necessary by that government for the health, safety and welfare of the public. Examples include official traffic signage and regulatory signage.

**Hanging Sign**  
A sign that is suspended from a roof overhang, arcade, awning structure, or a bracket mounted to the building facade.

**Height, Sign**  
The vertical distance measured from the highest point of the sign to the crown of the road or sidewalk it is intended to serve.

**Home Business Sign**  
A sign that identifies a home occupation.

## 5.0 EXAMPLES & DEFINITIONS

the sign rests upon the ground.

### Mosaic Sign

A type of specialty sign that is comprised of small tiles that form an image and are constructed as an integral part of a wall surface.

### Multi-Tenant Building

Any business or group businesses under a common roof.

### Neon Signs

A sign constructed of internally lit glass tubes that contain inert gas that is intended to display names or logos and/or advertise products.

### Non-Conforming Sign

Any sign which was lawfully erected but which does not comply with the land use, setback, size or lighting provisions of this guideline or which later fails to comply due to changed conditions.

### Off-Site Sign

A sign structure advertising an establishment, merchandise, commodities, accommodations, services or entertainment which are not sold, produced, manufactured, or furnished at the site on which the sign is located.

### On-Site Sign

A sign relating, in its subject matter, to commodities, accommodations, services or activities offered at the site on which the sign is located.

### Painted Window or Door Signs

Signs painted on, or affixed to, the glass surfaces of windows or doors and pertaining, in their context, to the lawful business conducted therein.

### Performing Arts/Cinema/Marquee Sign

A sign with changeable copy area intended to advertise performances for cinemas, theaters, civic centers and other such facilities.

### Permanent Sign

A sign that is attached to or painted on a permanent structure, whether it be a building, wall, pier or the ground. A permanent sign is to be constructed in a manner that is in accordance with County regulations unless otherwise specified herein.

### Personal Name Sign

A sign that is used to identify a person or persons dwelling in a residence.

### Pole Sign

A sign structure which is independent of any building and which is supported by a single pole so that the bottom of the sign is raised above the ground and may be suspended from a yardarm.

### Political or Campaign Sign

A temporary sign erected to advocate a political party, issues, or individuals for elective office.

### Posters

Posters announce or advertise specific events, activities, movies or performances. Posters are enclosed in a transparent housing.

### Poster Box Signs

Poster boxes are transparent housing devices used to protect advertisement posters from the weather. The advertisements within poster boxes are changeable.

## Attachment E Design Guidelines

### Sandwich Board

A temporary movable sign located along sidewalks within the property lines (or between the building frontage and right-of-way) advertising the services provided by the adjacent establishment.

### Sign

Any object, device, display, structure, or part thereof, or character, whether animate or inanimate, situated outdoors or indoors, which is used to advertise, identify, display, direct or attract attention to an event or location by any means, including words, letters, figures, design, symbols, fixtures, colors, illumination or projected images. The term shall include, but not be limited to, any permanently installed or situated merchandise, or any emblem, painting, flag, banner, pennant, festoon, placard or lighting, when the intent of said lighting is to act as a sign. The term shall not include murals or artistic designs painted on the wall of a building, unless the design includes logos, or any other element that is so uniquely identified with the business conducted on that parcel, or any other parcel, that its intent is to advertise said business.

### Sign Structure (Sign Support/Sign Framework)

Any single structure which may contain any combination of one or more separate signs, which are all supported by and attached to said structure.

### Professional Name Plate

A sign designating the office of a professional practice and which indicates the names of the person or persons and the nature of the professional practice.

### Project Sign (Future Development Sign)

A temporary sign that provides the name(s) and/or opening date(s) of a planned building, facility or future development.

### Realty Offering Sign

A sign of a temporary nature which is intended to offer, for sale, lease, or rent, the exact property upon which the sign is placed.

### Repairs or Maintenance of Signs

The cleaning, painting, repair or replacement of defective parts of a sign in a manner that does not alter the size, design, or structure of the sign. The term shall include the change in appearance or replacement of the sign face.

### Replacement of Signs

Replacement shall mean the rebuilding, enlarging, or change of the size, design, or structure of the sign, other than repairs of maintenance of signs. The term shall include the change in appearance or replacement of the sign face.

### Roof Sign

A sign that is mounted to the roof in a vertical plane and oriented parallel to the line of the main ridge.

## 5.0 EXAMPLES & DEFINITIONS

### Street Sign

A permanent sign that displays the name(s) of a street(s).

### Structure, Change of

Any activity which includes any alteration to the supporting structure of the sign, fastening anything to the support structure by means whatever, detaching anything from the supporting structure by any means whatever, any change whatever to the braces, frames or attachments of a sign, or any other activity which is determined by the Building Official for Prince William County to be a change of structure.

### Survey Stake

A temporary sign that identifies a real estate lot.

### Temporary Street Sign

A temporary sign that is located at the intersection of two streets to display the names of the streets during construction.

### Trail Sign

A permanent sign located adjacent to trails and sidewalks intended to indicate the location of specific activities/facilities, such as tennis courts and the distance to the activity/facility.

### Vertical Clearance (Sign Clearance)

The smallest vertical distance between the lowest point of any sign, excluding framework and supports and the pedestrian or vehicular surface directly beneath said sign.

### Wall Sign

A sign fastened to or painted on a freestanding wall.

### Water Tower Structure Sign

A permanent sign painted on a water tower.

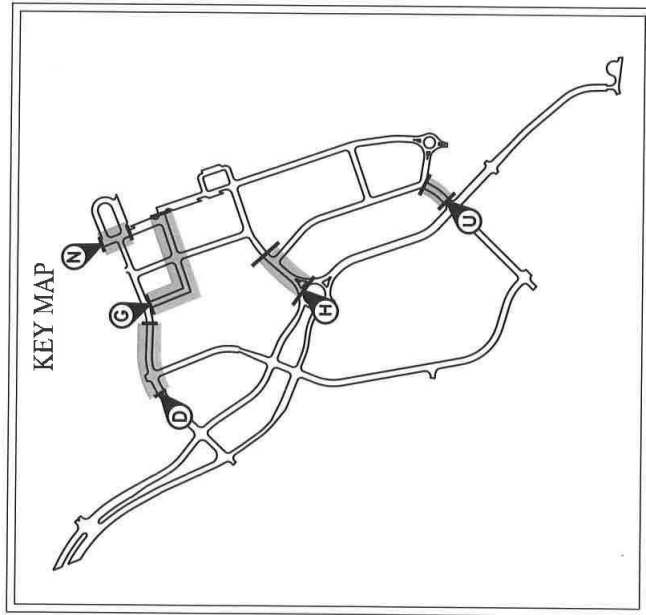
### Window Sign

A sign installed inside of a window and intended to be viewed from the outside. See also Painted Window Signs and Neon Window Signs.

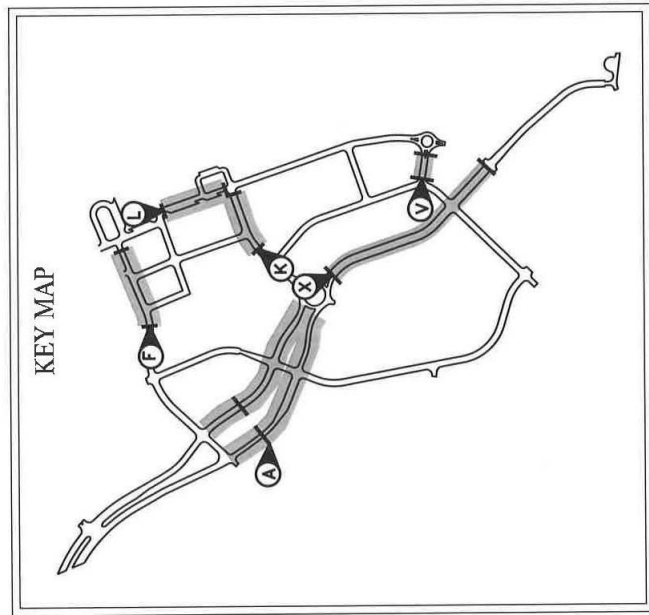


**APPENDIX B: STREET SECTIONS**

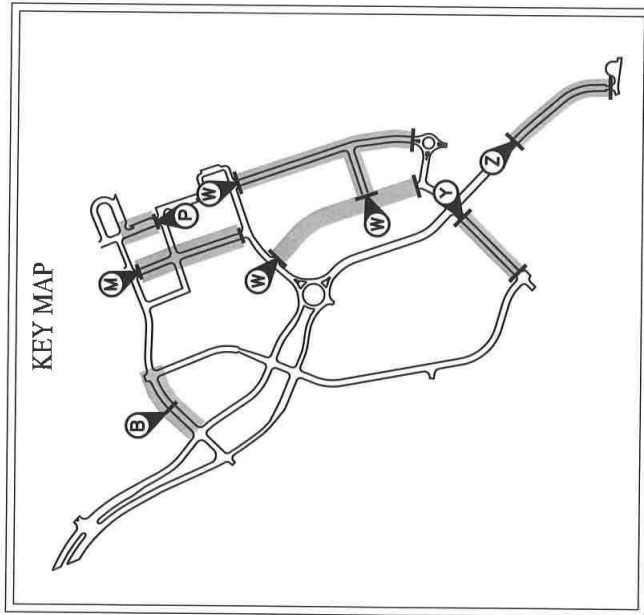
**Attachment E**  
**Design Guidelines**



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**Design Guidelines**

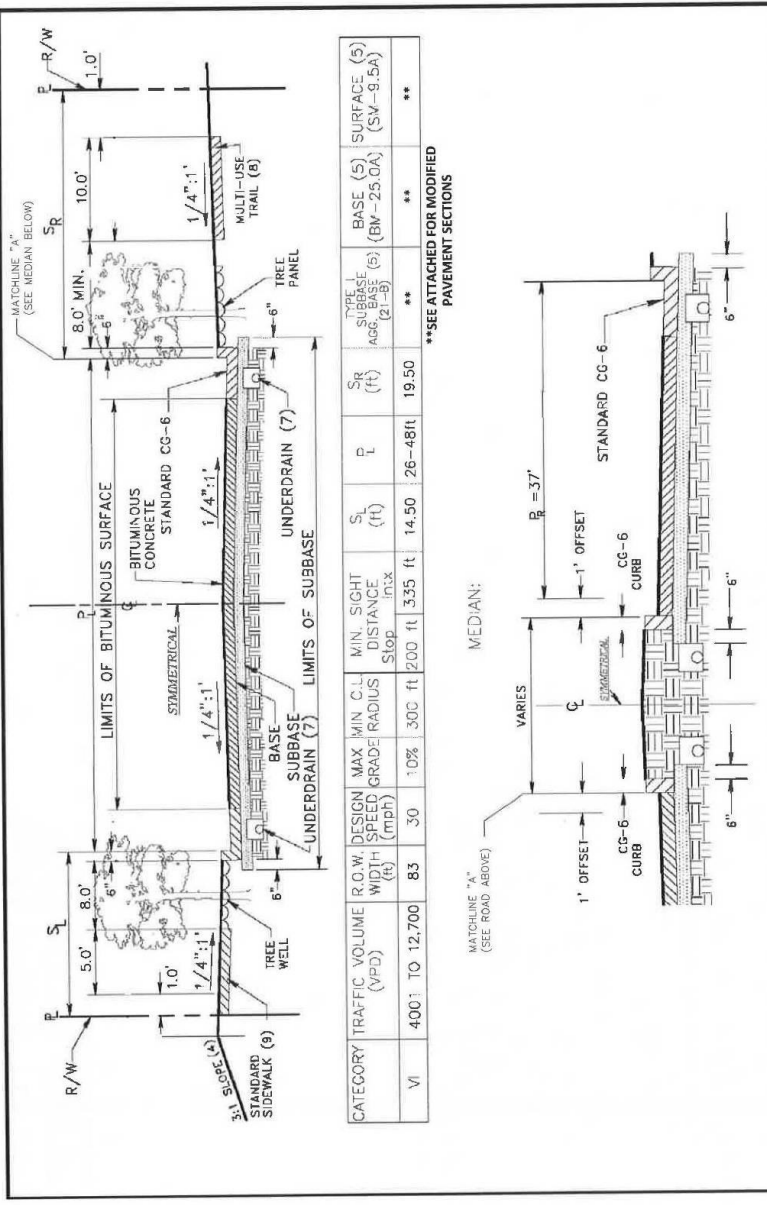


**Attachment E**  
**Design Guidelines**



**Attachment E  
Design Guidelines**

**ROAD "A"**



CATEGORY VI	TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAX. GRADE (%)	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	S <sub>L</sub> (ft)	P <sub>L</sub> (ft)	S <sub>R</sub> (ft)	TYPE I SUBBASE AGG. (S <sub>1</sub> -B)	BASE (S) SURFACE (S)	SURFACE (S)
VI	4001 TO 12,700	83	30	1.0%	300 ft	200 ft	335 ft	14.50	26-48ft	19.50	**	**

\*\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

Detail No. <b>650.XX</b>	<b>MAJOR AVENUE</b>	 <b>COUNTY OF PRINCE WILLIAM VIRGINIA</b>	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

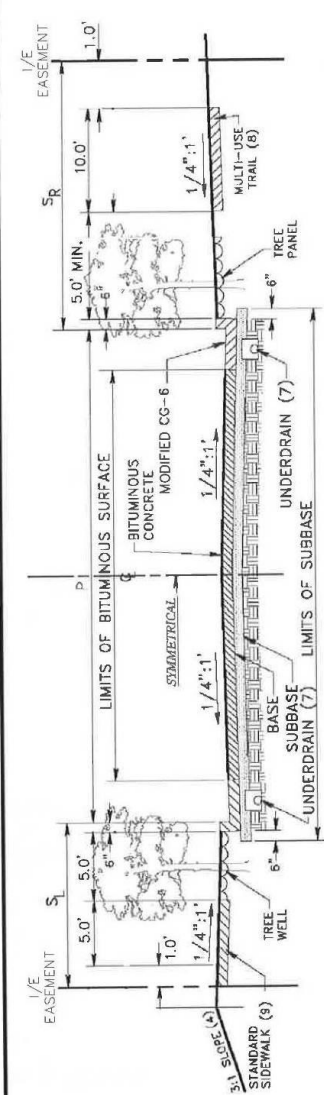
- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 5) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 6) Super-elevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 16' and 36' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 16) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.	<b>650.XX</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	Rev. No.
		STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 2 OF 2		Date 12/17/12
		<b>MAJOR AVENUE</b>		



Attachment E  
Design Guidelines

ROAD "B"

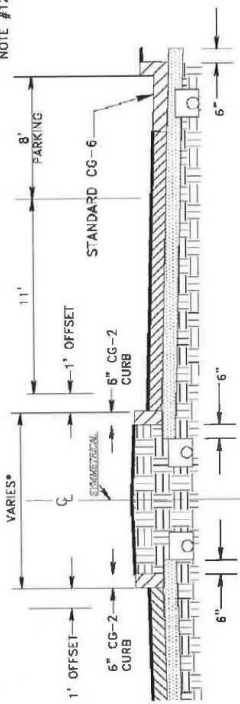


CATEGORY	TRAFFIC VOLUME (V <sub>PD</sub> )	1/2 ESM DESIGN WIDTH (ft)	MAX. MIN. C.L. SPEED (mph)	MIN. SIGHT DISTANCE (ft)	MIN. SLOPE (%)	MIN. SLOPE (in/ft)	S <sub>L</sub> (ft)	P	S <sub>R</sub> (ft)	TYPE SUBBASE (1-6)	BASE (5) (BW-25.0A)	SURFACE (5) (SW-9.5A)
VI	4001 TO 9,000	71	30	10%	300 ft	200 ft	335 ft	43 ft	16.50	**	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN:

\* - RANGES FROM 16'-36' (SEE NOTE #12)



Detail No. <b>650.XX</b>	 COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No.
			Date 12/17/12

## Attachment E Design Guidelines

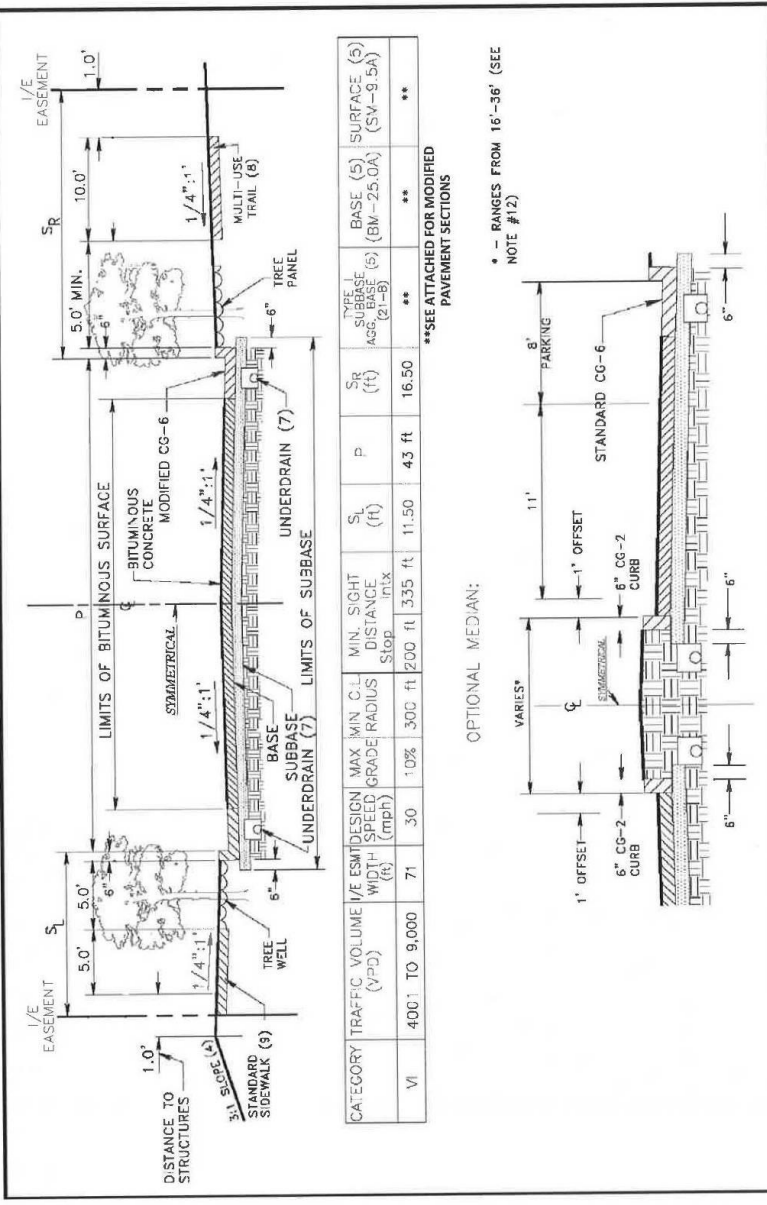
**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil types supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 5) **Foamert sections shall be subject to modification at the time of Site Plan approval process.**
- 6) Superlevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 16' and 36' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 16) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.	<b>650.XX</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	Rev. No.
		STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 2 OF 2		Date 12/17/12
		<b>MAJOR AVENUE</b>		

**Attachment E  
Design Guidelines**

**ROAD "D"**

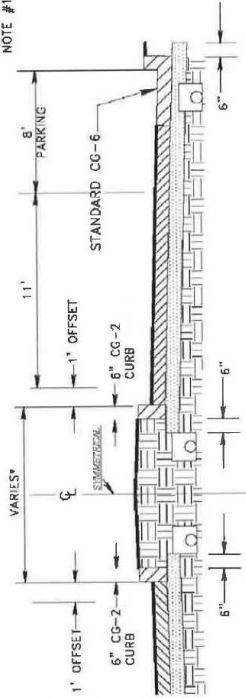



CATEGORY	TRAFFIC VOLUME (V <sub>PD</sub> )	1/4 EASEMENT WIDTH (ft)	MAX. MIN. C.L. SPEED (mph)	GRADE (%)	MIN. SIGHT DISTANCE (ft)	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE SLOP. INCL.	S <sub>L</sub> (ft)	P	S <sub>R</sub> (ft)	TYPE SUBBASE (1-B)	BASE (5) (BM-25.0A)	SURFACE (5) (SV-9.5A)
VI	4001 TO 9,000	71	30	1.0%	200 ft	300 ft	200 ft	335 ft	43 ft	16.50	**	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN:

\* - RANGES FROM 16'-36' (SEE NOTE #12)



Detail No. <b>650.XX</b>	 <p align="center"><b>COUNTY OF PRINCE WILLIAM VIRGINIA</b></p>	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No.
			Date 12/17/12

## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control Ordinance.
- 5) Pavement sections shall be subject to modification at the time of Site Plan approval process.
- 6) Super-elevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 16' and 36' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking line must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 16) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.  
**650.XX**

**MAJOR AVENUE**



**COUNTY OF PRINCE WILLIAM VIRGINIA**

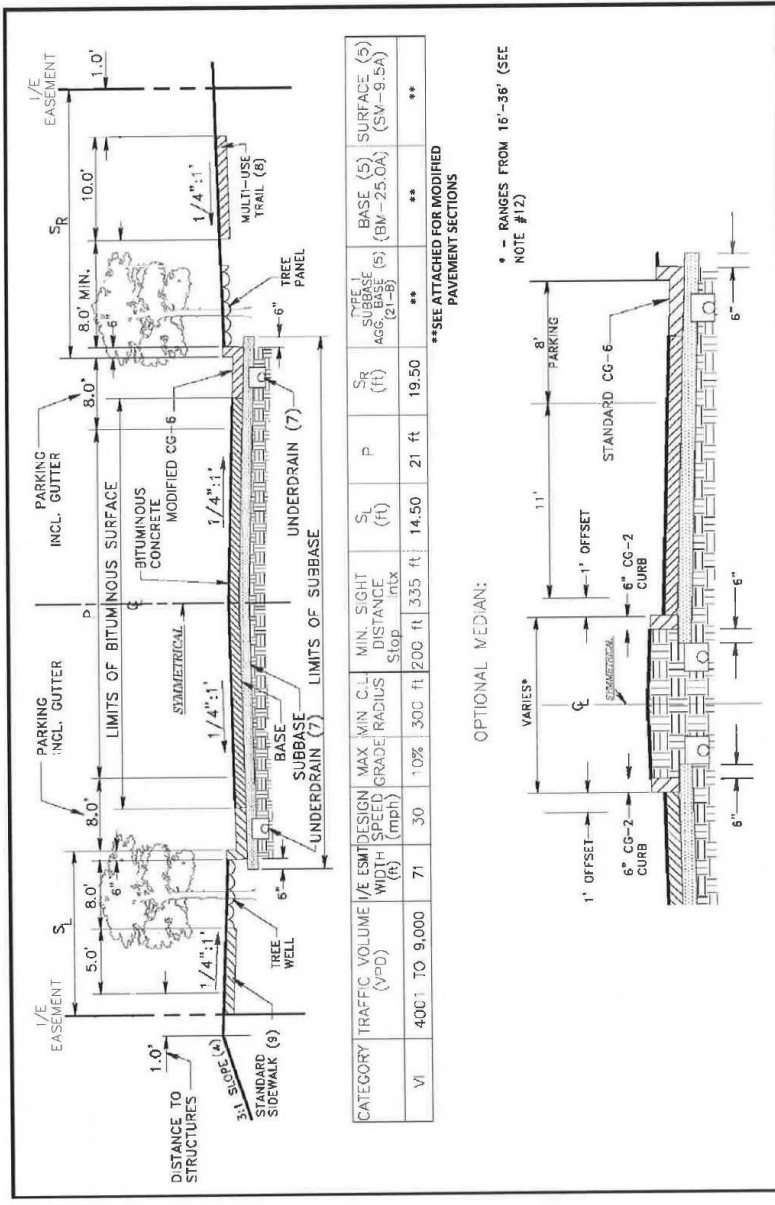
STANDARD TYPICAL SECTION FOR  
URBAN AREAS  
(MAJOR AVENUE)  
SHEET 2 OF 2

Rev. No.

Date  
12/17/12

Attachment E  
Design Guidelines

ROAD "F"

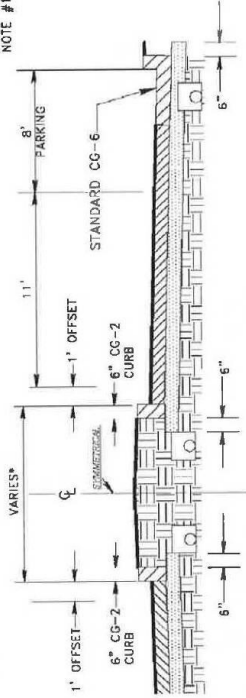


CATEGORY	TRAFFIC VOLUME (VPD)	DESIGN WIDTH (ft)	MAX. MIN. C.L. GRADE RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	SR (ft)	P	S <sub>1</sub> (ft)	S <sub>2</sub> (ft)	TYPE SUBBASE AC <sup>19</sup> (21-8)	BASE (5) (BW-25.0A)	SURFACE (5) (SW-9.5A)
VI	4001 TO 9,000	71	30	10%	300 ft	200 ft	335 ft	14.50	21 ft	19.50	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN:

\* - RANGES FROM 16'-36" (SEE NOTE #12)



Detail No. <b>650.XX</b>	MAJOR AVENUE	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

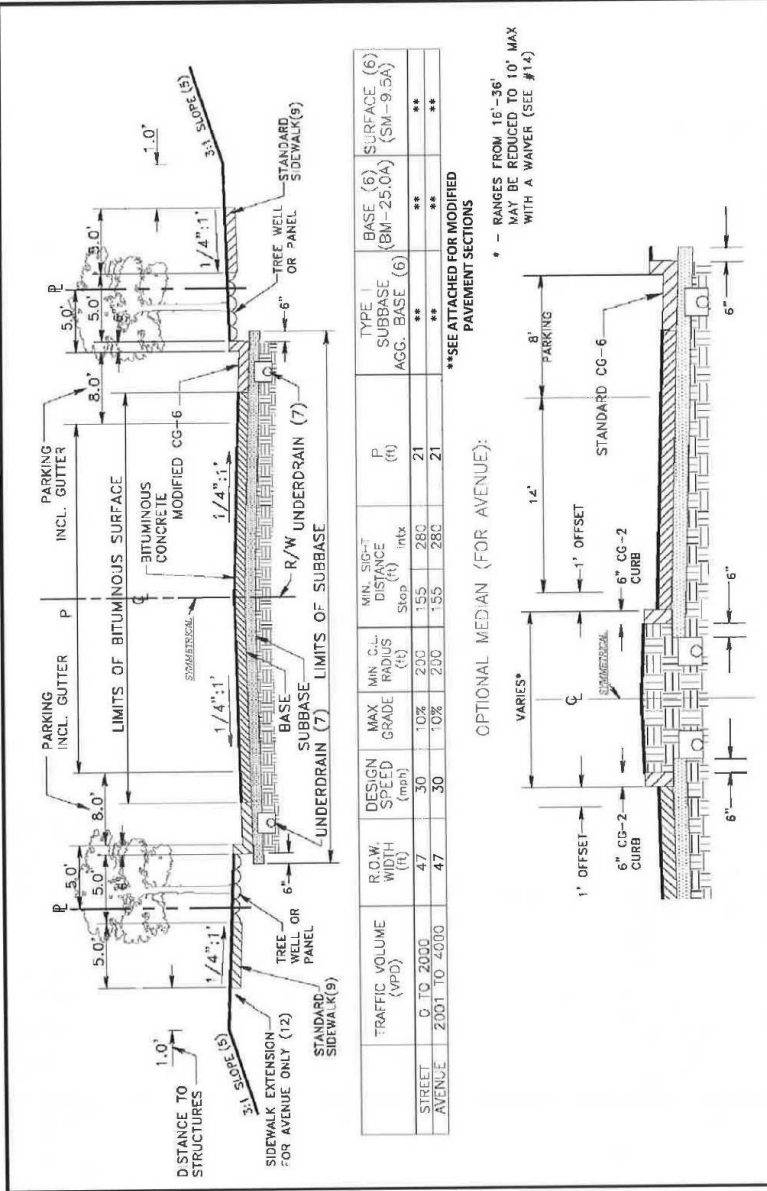
**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 5) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 6) Super-elevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 16' and 36' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 16) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.	<b>650.XX</b>	<b>MAJOR AVENUE</b>		Rev. No.	Date
		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 2 OF 2		12/17/12



ROAD "G"



Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16' and 36' wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No.	650.XX		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No.	Date
						12/17/12



## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- \*4) An optional raised landscape median shall be between 16" and 36" wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

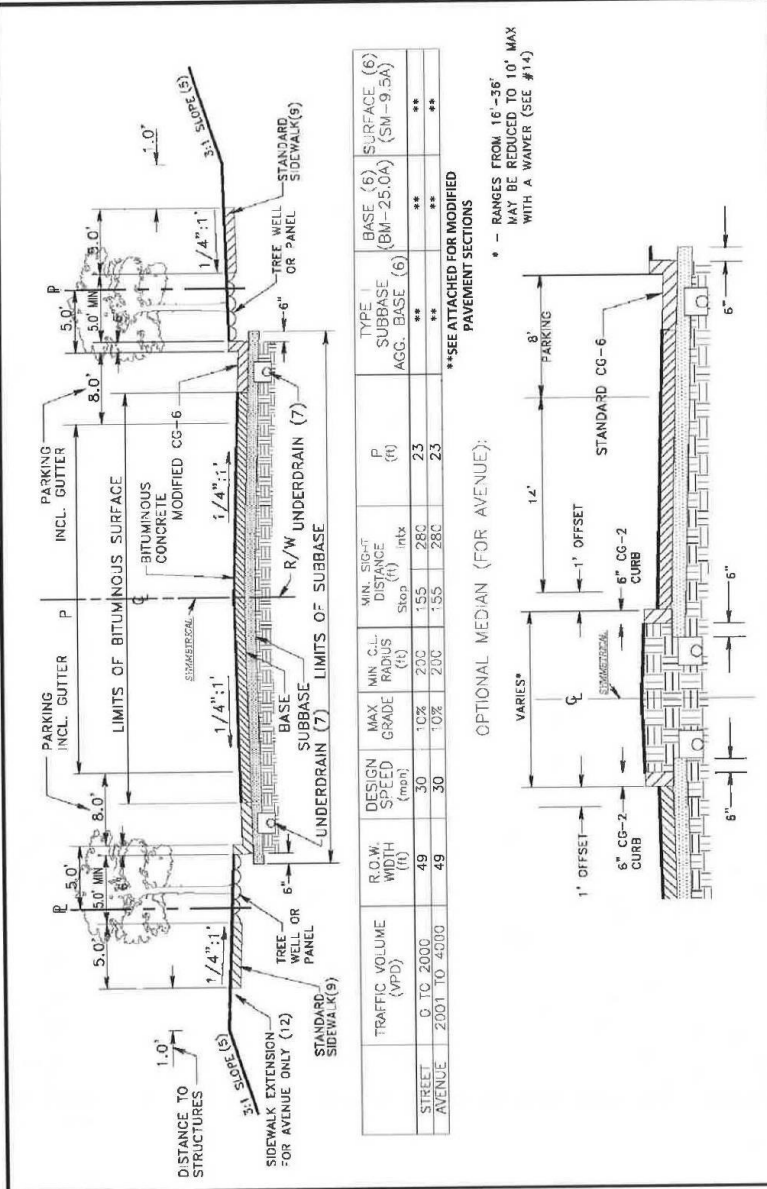
**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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Attachment E  
Design Guidelines

ROAD "K"



TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAY GRADE	MIN. C.L. DISTANCE (ft)	MIN. SIG-H'T DISTANCE (ft)	P	TYPE I SUBBASE AGG. BASE (6)	BASE (6) SURFACE (6)
G. TO 2000	49	30	1.0%	200	155	23	**	(BM-25.04) (SM-9.5A)
2001 TO 4000	49	30	1.0%	200	155	23	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

\* - RANGES FROM 16'-36" MAY BE REDUCED TO 10' MAX WITH A WAIVER (SEE #14)

OPTIONAL MEDIAN (FOR AVENUE):

Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised andscape median shall be between 16' and 36' wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

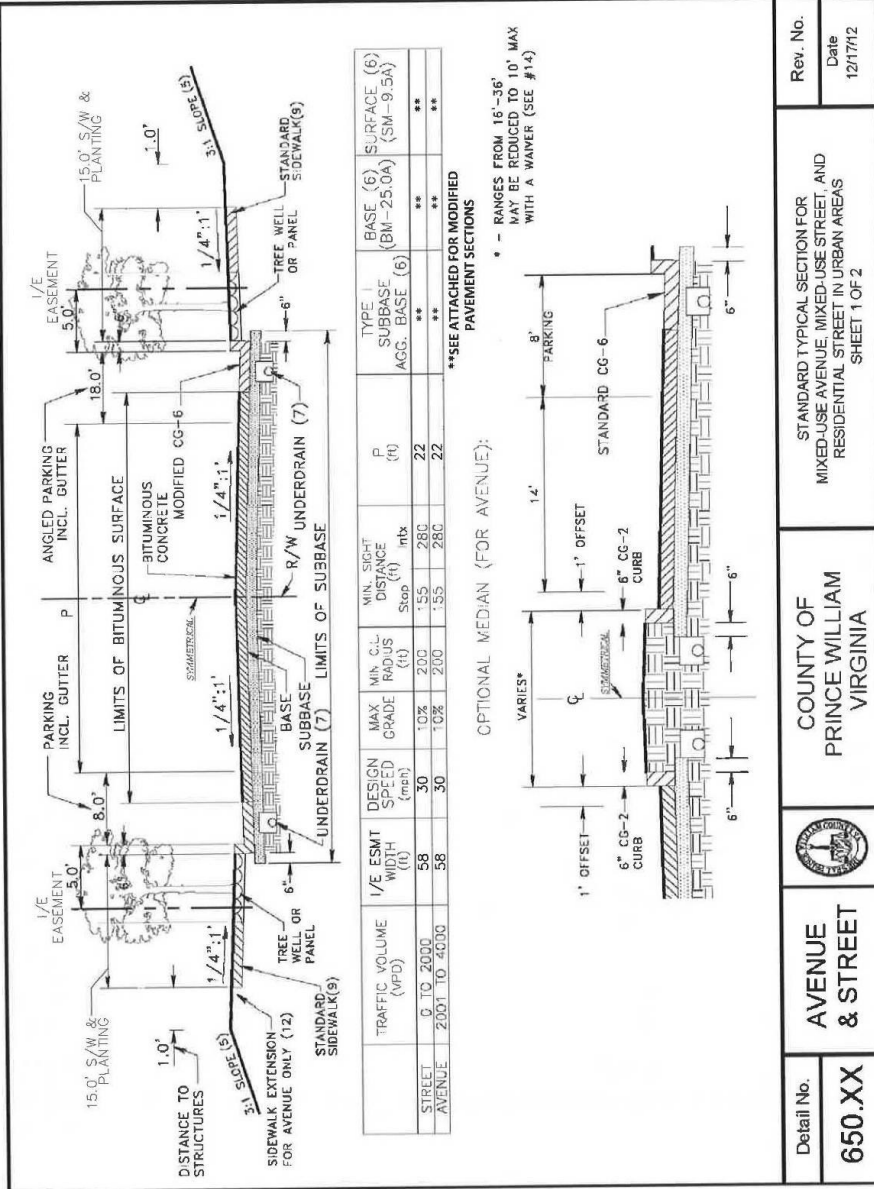
**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		<b>COUNTY OF PRINCE WILLIAM VIRGINIA</b>	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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ROAD "L"



TRAFFIC VOLUME (VPD)	I/E ESMT WIDTH (ft)	DESIGN SPEED (mph)	MAX GRADE (%)	MIN. CL. ROAD (ft)	MIN. SIGHT DISTANCE (ft)	P	TYPE 1 SUBBASE (6) AGG. BASE (6)	BASE (6) SURFACE (6) (BM-25.0A) (SM-9.5A)
0 TO 2000	58	30	1.0%	200	1:55	280	22	**
2001 TO 4000	58	30	1.0%	200	1:55	280	22	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN (FOR AVENUE):

\* - RANGES FROM 16"-36" MAY BE REDUCED TO 10" MAX WITH A WAIVER (SEE #14)

Detail No. <b>650.XX</b>	AVENUE & STREET	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No superelevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-outs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

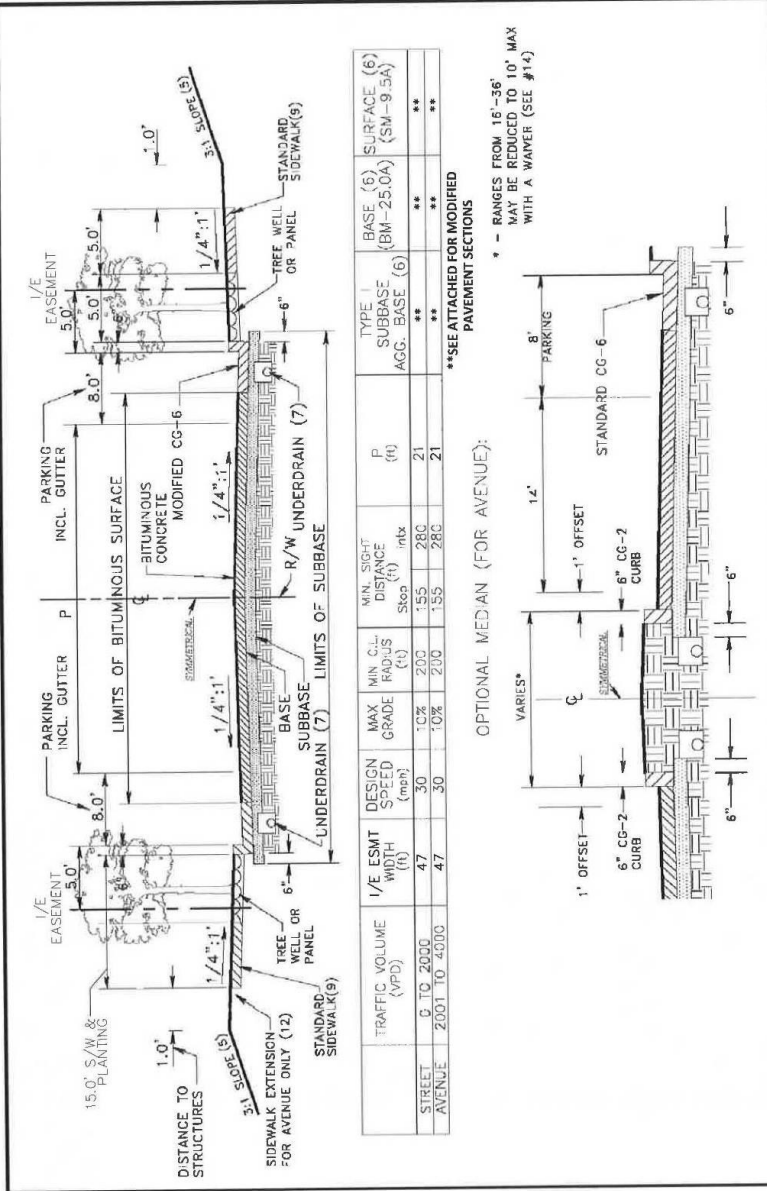
- 14) An optional raised landscape median shall be between 16' and 36" wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No.	Date 12/17/12
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ROAD "M"

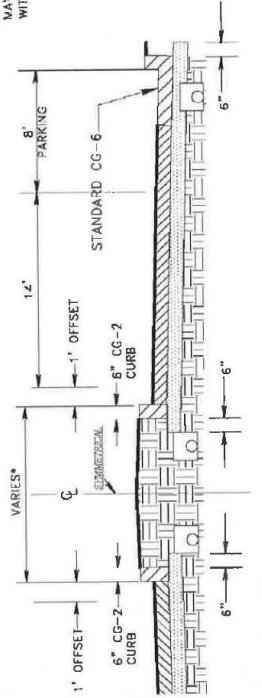


TRAFFIC VOLUME (VPD)	I/E ESMT WITH (ft)	DESIGN SPEED (mph)	MAX GRADE (%)	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	P	TYPE I SUBBASE AGG. BASE (6)	BASE (6) SUBBASE (BM-25.0A)	SURFACE (6)
G TO 2000	47	30	10%	200	155	28C	21	**	**
2001 TO 4000	47	30	10%	200	155	28C	21	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN (FOR AVENUE):

\* - RANGES FROM 16'-36" MAY BE REDUCED TO 10' MAX WITH A WAIVER (SEE #14)



Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control Ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-outs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16" and 36" wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

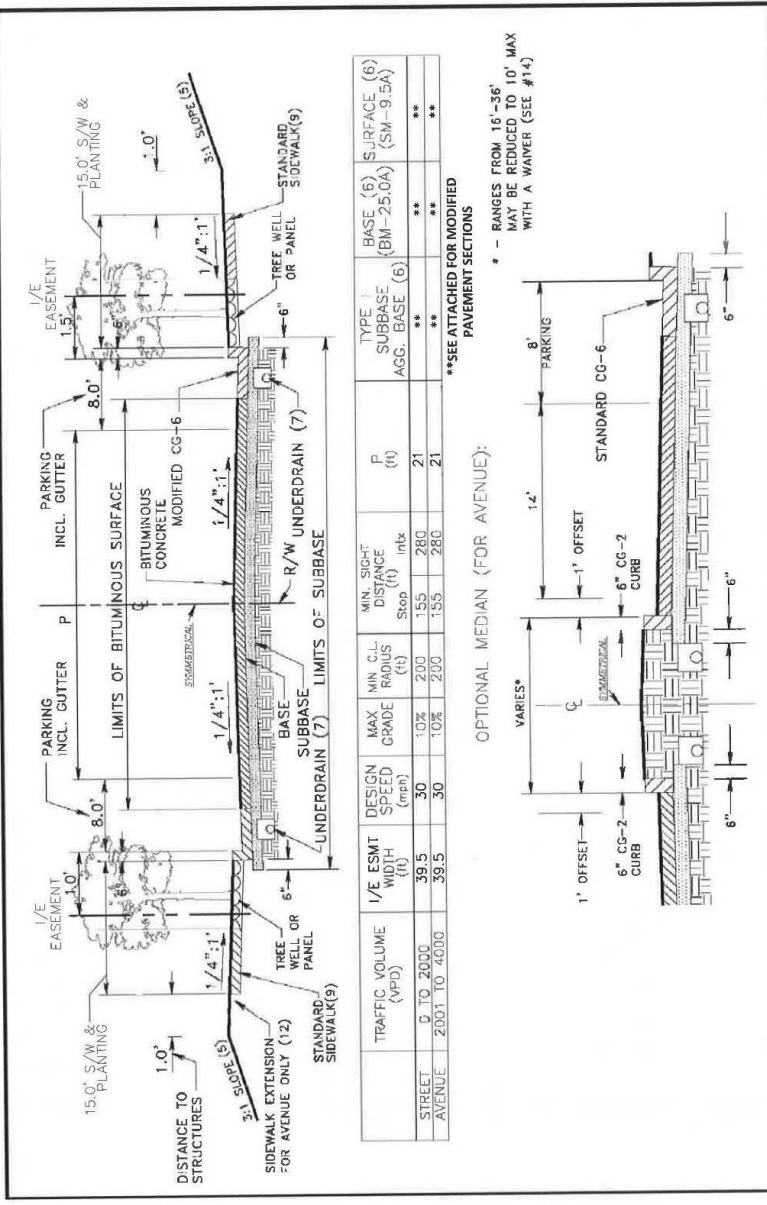
**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		<b>COUNTY OF PRINCE WILLIAM VIRGINIA</b>	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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Attachment E  
Design Guidelines

ROAD "N"

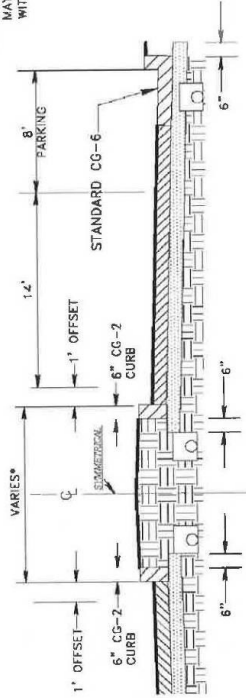


TRAFFIC VOLUME (VPD)	I/E ESMT WIDTH (ft)	DESIGN SPEED (mph)	MAX GRADE	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE STOP (ft)	P	TYPE 1 SUBBASE AGC. BASE (6)	BASE (6) SURFACE (6)
0 TO 2000	39.5	30	10%	200	155	21	**	(BM-25.04) (SM-9.5A)
2001 TO 4000	39.5	30	10%	200	155	21	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN (FOR AVENUE):

\* - RANGES FROM 16'-36" MAY BE REDUCED TO 10' MAX WITH A WAIVER (SEE #14)

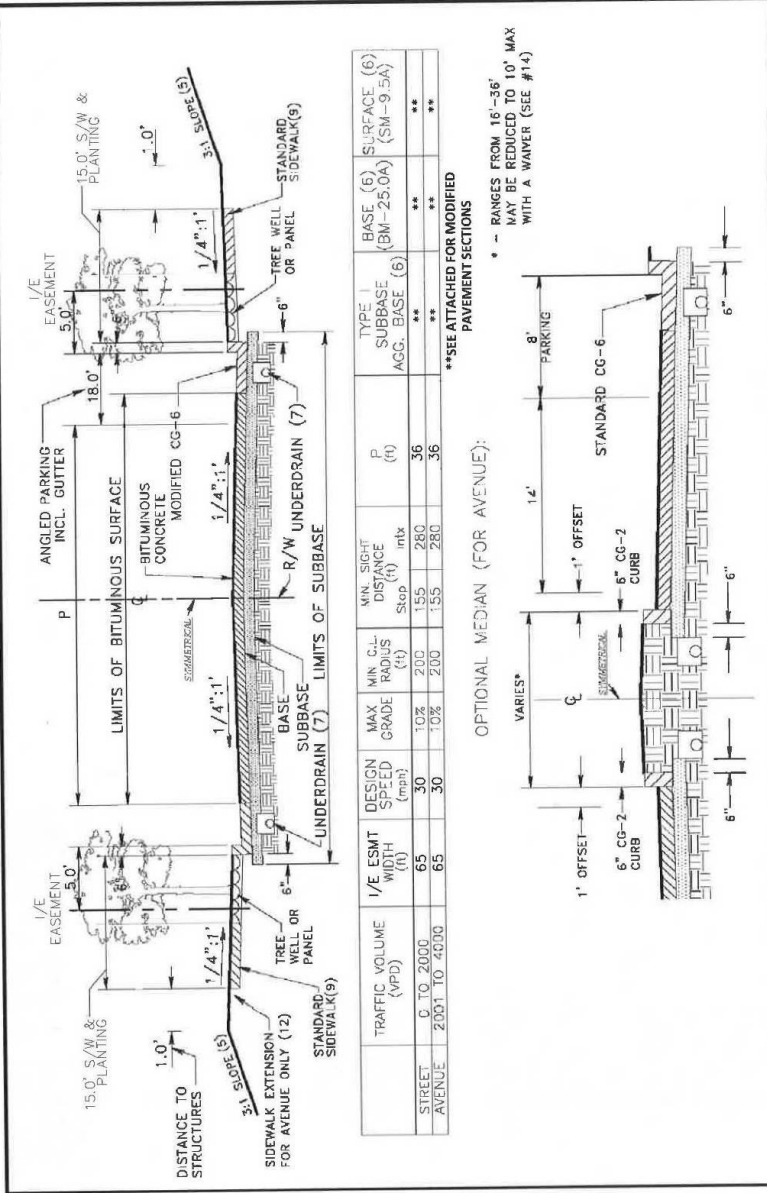


Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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ROAD "P"



TRAFFIC VOLUME (VPD)	I/E ESMT WIDTH (ft)	DESIGN SPEED (mph)	MAX GRADE	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	P	TYPE I SUBBASE AGG. BASE (6)	BASE (6) SURFACE (6)
0 TO 2000	65	30	1.0%	200	155	36	**	(BM-25.0A) (SM-9.5A)
2001 TO 4000	65	30	1.0%	200	155	36	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN (FOR AVENUE):

\* RANGES FROM 16'-36" MAY BE REDUCED TO 10' MAX WITH A WAIVER (SEE #14)

Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-outs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16' and 36' wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 12' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 4' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

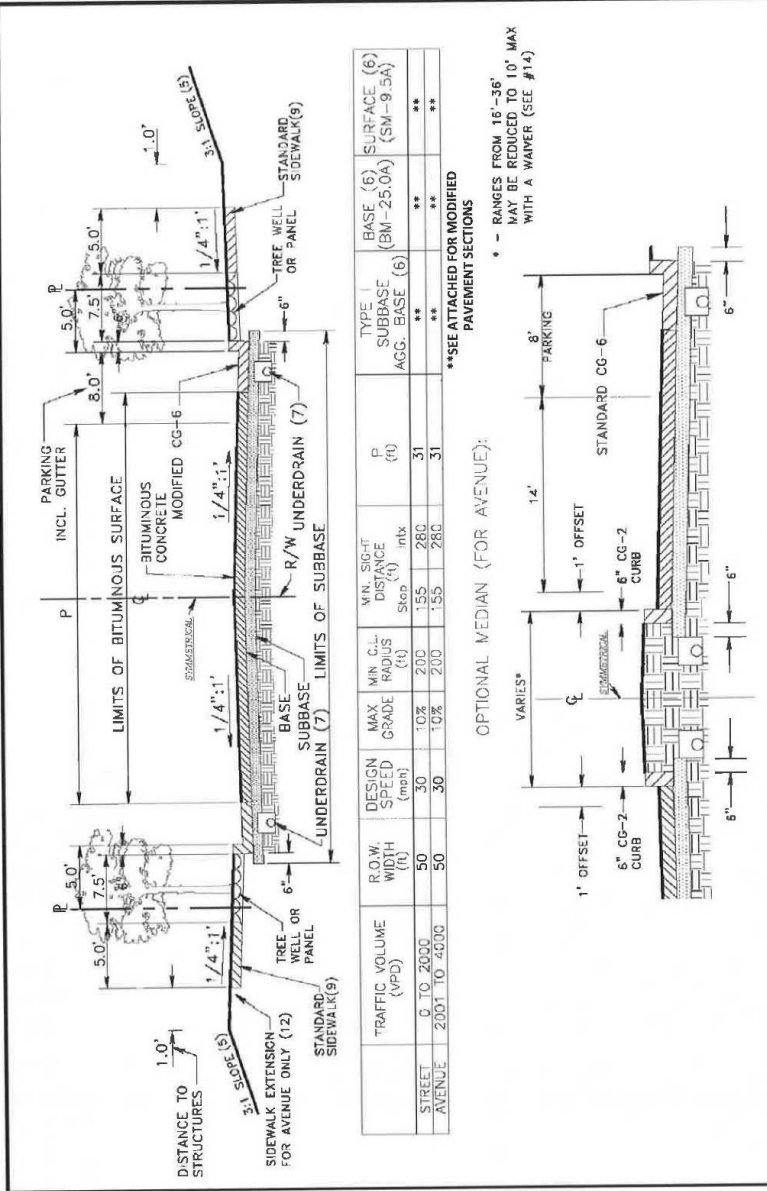
**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>	 <b>COUNTY OF PRINCE WILLIAM VIRGINIA</b>	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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Attachment E  
Design Guidelines

ROAD "U"

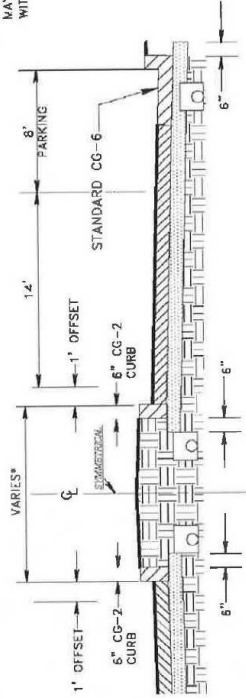


TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAY. GRADE	MIN. C.L. DISTANCE (ft)	MIN. SIG-T DISTANCE (ft)	P	TYPE I SUBBASE AGG. BASE (6)	BASE (6) SURFACE (6)
0 TO 2000	50	30	1.0%	200	155	280	31	**
2001 TO 4000	50	30	1.0%	200	155	280	31	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

\* - RANGES FROM 16'-36" MAY BE REDUCED TO 10" MAX WITH A WAIVER (SEE #14)

OPTIONAL MEDIAN (FOR AVENUE):



Detail No. <b>650.XX</b>	AVENUE & STREET	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control Ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking area must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-outs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16" and 36" wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- \*6) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No.	Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-outs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16" and 36" wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 4' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

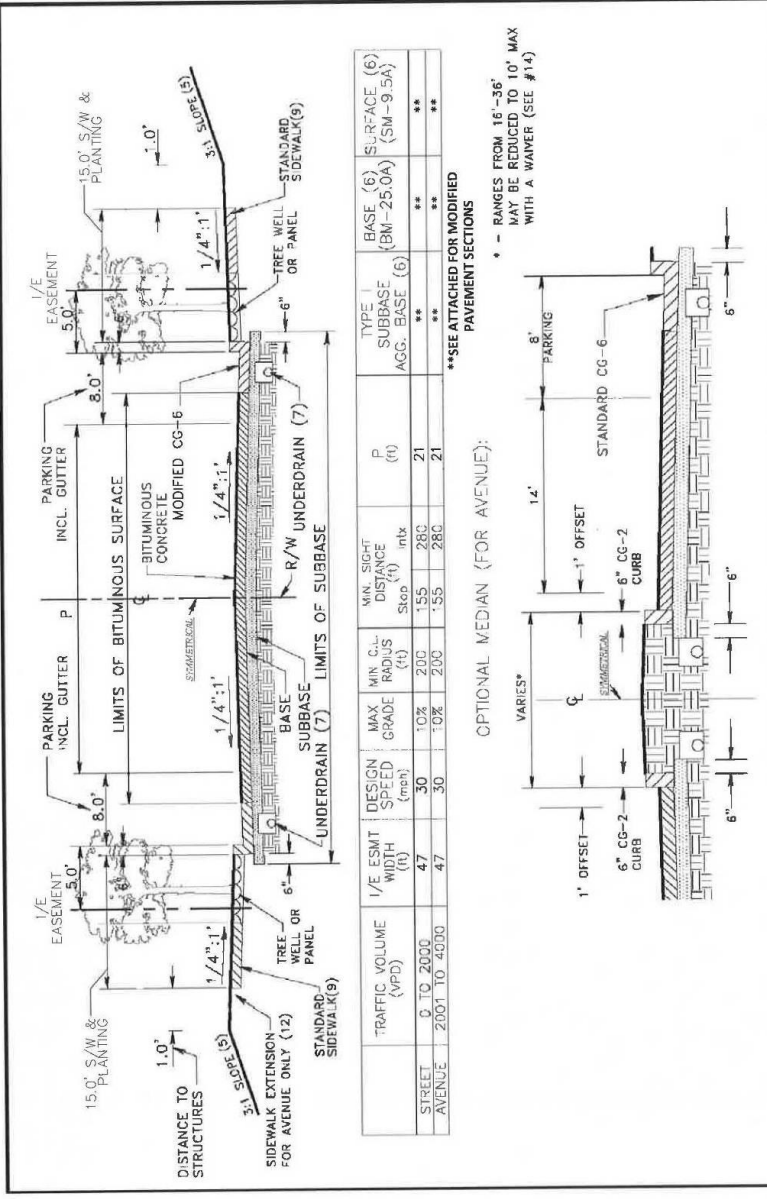
- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No.	650.XX		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No.	Date
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Attachment E  
Design Guidelines

ROAD "W"

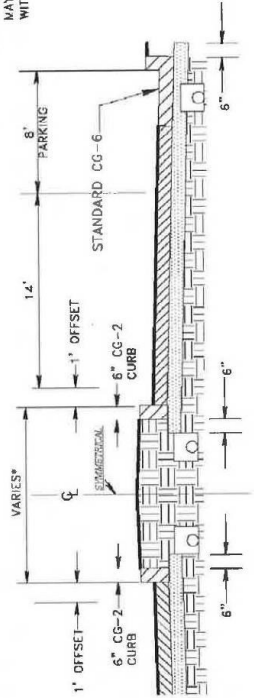


TRAFFIC VOLUME (VPD)	I/E ESMT WIDTH (ft)	DESIGN SPEED (mph)	MAX GRADE	MIN. CURB RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	P	TYPE I SUBBASE AGG. BASE (6)	BASE (6)	SURFACE (6)
0 TO 2000	47	30	10%	200	155	280	21	**	(BM-25.0A) (SM-9.5A)
2001 TO 4000	47	30	10%	200	155	280	21	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

\* - RANGES FROM 16"-36" MAY BE REDUCED TO 10" MAX WITH A WAIVER (SEE #14)

OPTIONAL MEDIAN (FOR AVENUE):



Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16' and 36' wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

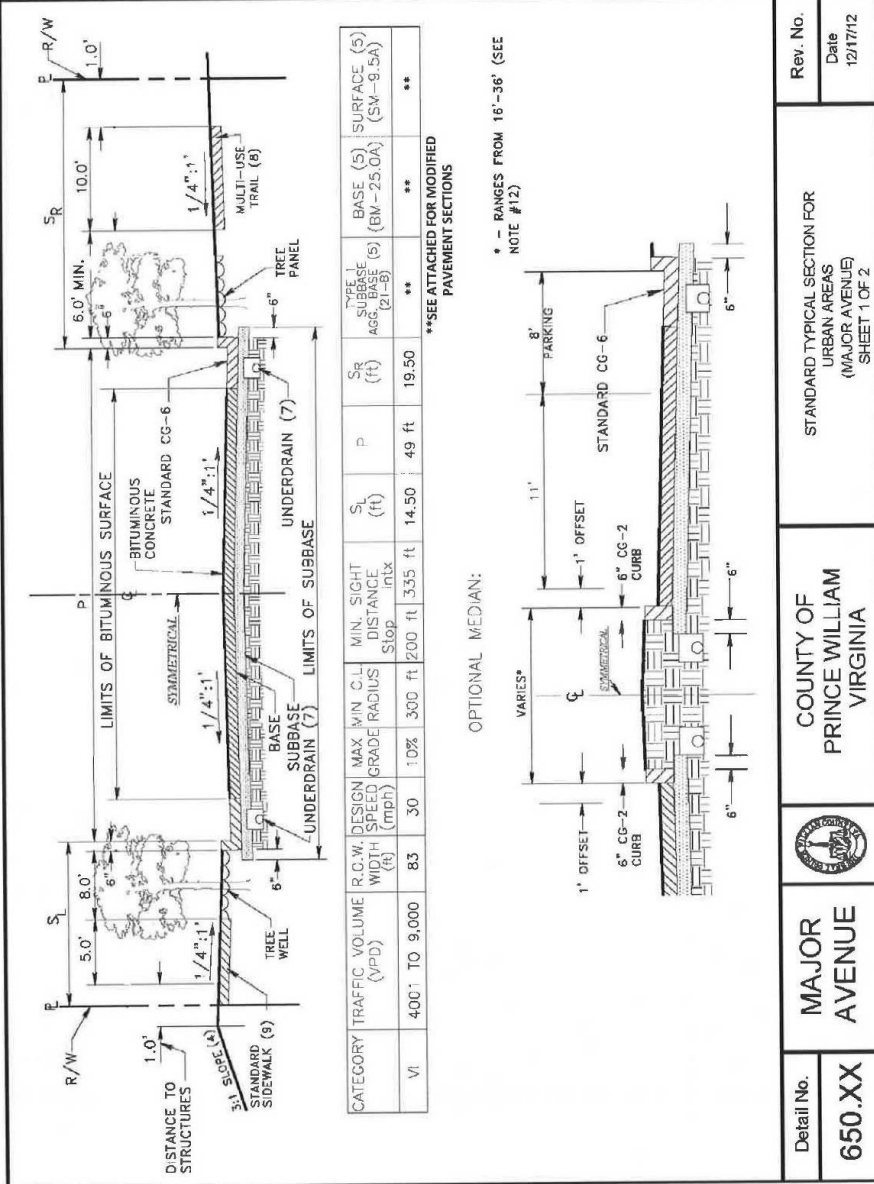
**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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**Attachment E  
Design Guidelines**

**ROAD "X"**

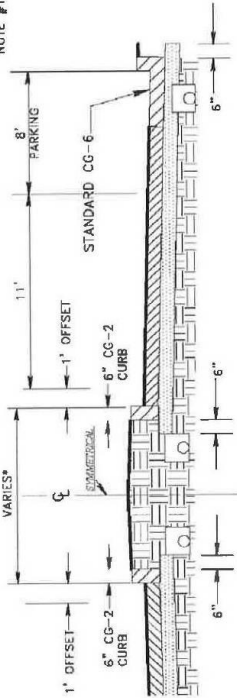


CATEGORY	TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAX. GRADE (%)	MIN. C.L. RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	S_L (ft)	P	S_R (ft)	TYPE SUBBASE AGG. (S)	BASE (S)	SURFACE (S)
VI	4,001 TO 9,000	83	30	1.0%	300 ft	200 ft	335 ft	49 ft	19.50	**	**	**

\*\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN:

\* - RANGES FROM 16'-36' (SEE NOTE #12)



Detail No. <b>650.XX</b>	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

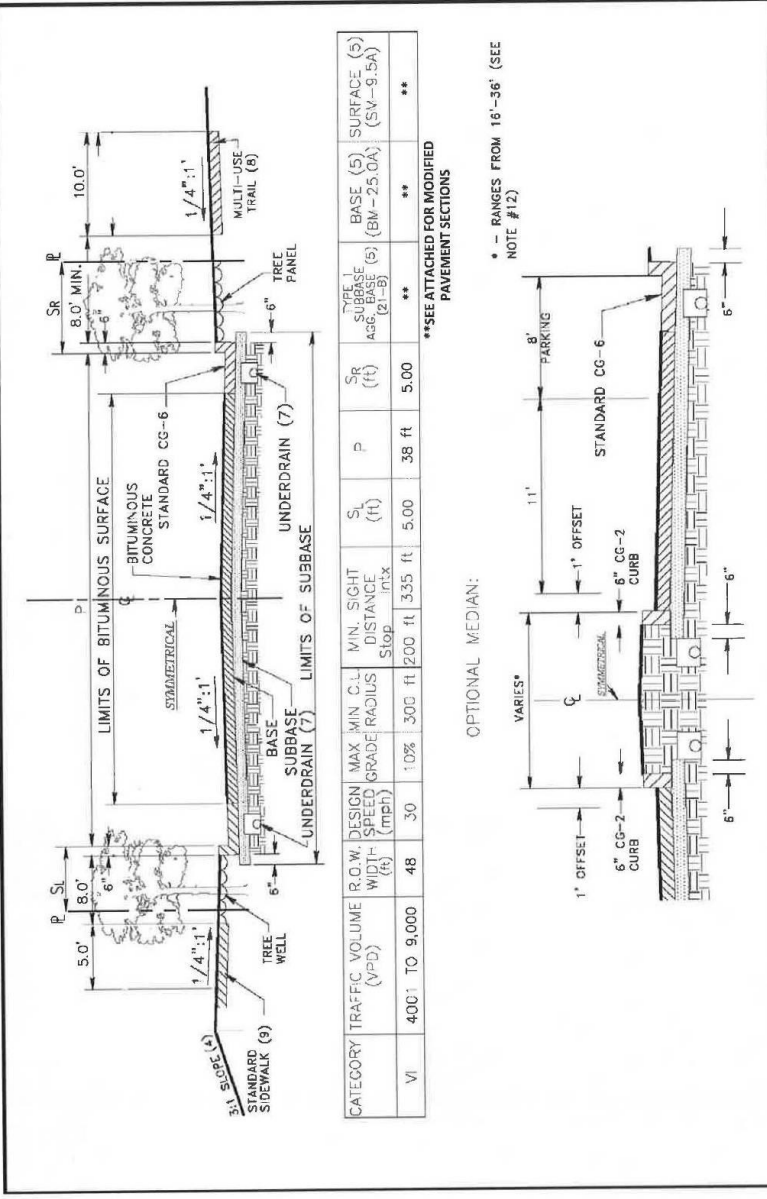
**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control ordinance.
- 5) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 6) Superlevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 16' and 36' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 15) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.	<b>650.XX</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	Rev. No.
		STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 2 OF 2		Date 12/17/12

Attachment E  
Design Guidelines

ROAD "Y"

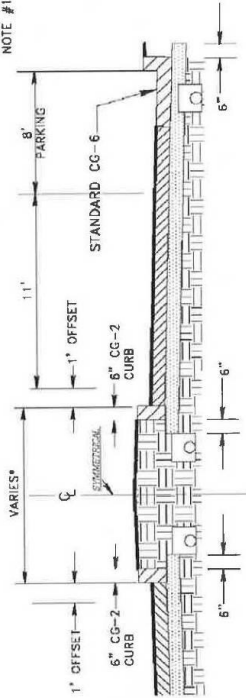


CATEGORY	TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAX. GRADE RADIUS	MIN. SIGHT DISTANCE	MIN. SIGHT DISTANCE STOP	S1	P	SR (ft)	TYPE SUBBASE (2-B)	BASE (5)	SURFACE (5)
VI	4001 TO 9,000	48	30	1.0%	300 ft	200 ft	335 ft	5.00	38 ft	5.00	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN:

\* - RANGES FROM 16'-36' (SEE NOTE #12)



Detail No. <b>650.XX</b>	MAJOR AVENUE	COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 1 OF 2	Rev. No. Date 12/17/12
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## Attachment E Design Guidelines

**GENERAL NOTES:**

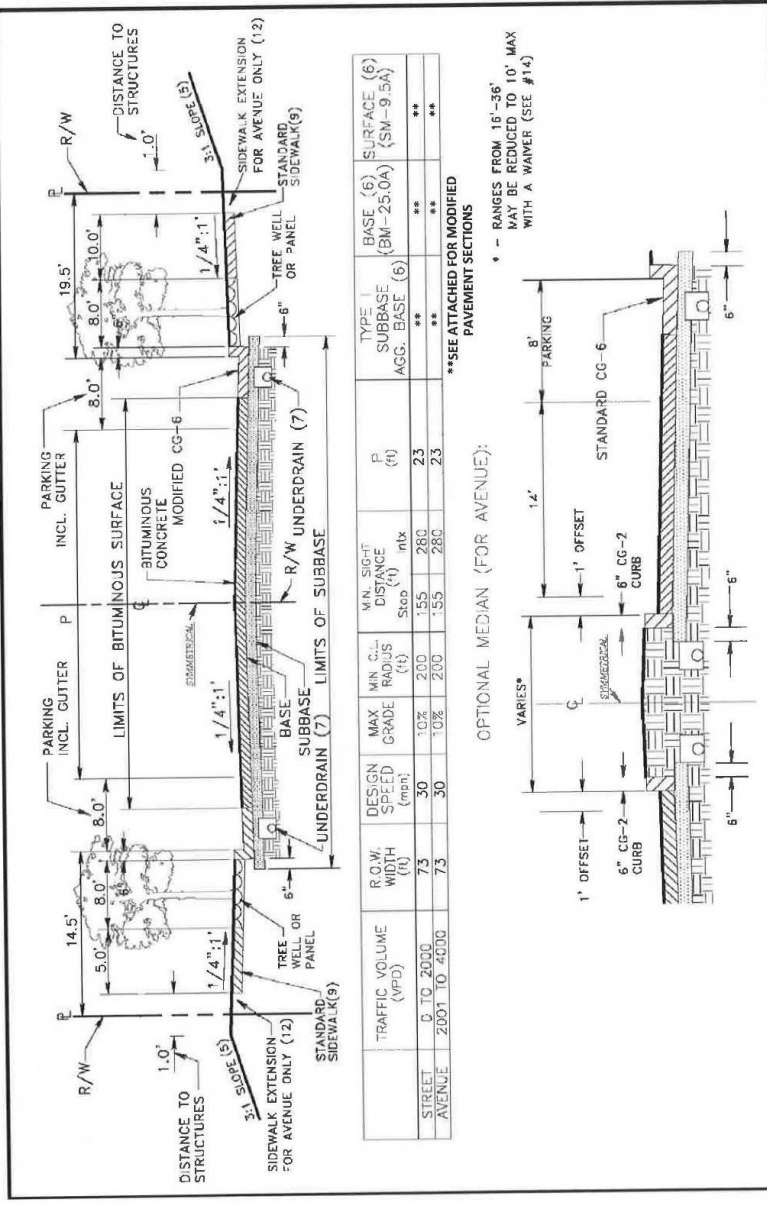
- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control Ordinance.
- 5) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 6) Super-elevation shall be provided in accordance with VDOT Road and Bridge Standards.
- 7) Underdrains shall be required. A modified UD-4 shall be used on each side of the optional median.
- 8) A multi-use trail no less than 10' wide (in addition to a 2' graded shoulder on either side for the bicyclist recovery & clear zone, and a 1' setback from the right-of-way line) must be located on one side of the street.
- 9) A sidewalk no less than 5' wide must be located on one side of street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) An optional raised landscape median shall be designed in accordance with VDOT standards as appropriate and must be between 15' and 35' (with a 2' offset) to accommodate a single left-turn lane at intersections.
- 13) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 14) Pedestrian lights, gardens, and stair-railings should be located behind the public right-of-way within private property.
- 15) A separate plan for landscaping and irrigation inside the right-of-way should be submitted to VDOT for review and approval.
- 16) Additional right-of-way may be required to accommodate channelization (right/left turn lanes) and/or signalization at intersections.

Detail No.	<b>650.XX</b>		COUNTY OF PRINCE WILLIAM VIRGINIA	STANDARD TYPICAL SECTION FOR URBAN AREAS (MAJOR AVENUE) SHEET 2 OF 2	Rev. No.	Date 12/17/12
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Attachment E  
Design Guidelines

ROAD "Z"



TRAFFIC VOLUME (VPD)	R.O.W. WIDTH (ft)	DESIGN SPEED (mph)	MAX GRADE (%)	MIN. CURB RADIUS (ft)	MIN. SIGHT DISTANCE (ft)	P	TYPE SUBBASE	BASE (6) SURFACE (6)
G TO 2000	73	30	10%	200	155	23	AGG. BASE (6)	(BM-25.0A) (SM-9.5A)
2001 TO 4000	73	30	10%	200	155	23	**	**

\*\*SEE ATTACHED FOR MODIFIED PAVEMENT SECTIONS

OPTIONAL MEDIAN (FOR AVENUE):

\* - RANGES FROM 16'-36" MAY BE REDUCED TO 10' MAX WITH A WAIVER (SEE #14)

Detail No.	Rev. No.
650.XX	Date 12/17/12
AVENUE & STREET	
COUNTY OF PRINCE WILLIAM VIRGINIA	
STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET AND RESIDENTIAL STREET IN URBAN AREAS SHEET 1 OF 2	

## Attachment E Design Guidelines

**GENERAL NOTES:**

- 1) This typical cross section shall be used in centers of community or commerce or as otherwise approved by the Director of Transportation.
- 2) Standard landings required at intersections.
- 3) Stone material shall extend under the curb and gutter a minimum of six inches (6 in.) beyond the back of curb. The stone thickness under the curb and gutter shall be that in excess of the depth of the gutter face or a minimum of four inches (4 in.) whichever is greater.
- 4) No super-elevation is required.
- 5) 2:1 slopes will be allowed when soil type supported by soil report is acceptable and where stabilization is provided in accordance with the Erosion Control Ordinance.
- 6) **Pavement sections shall be subject to modification at the time of Site Plan approval process.**
- 7) Underdrains shall be required where traffic volumes exceed 1,000 vpd.
- 8) If transit is provided where on-street parking is allowed, the on-street parking lane must clearly terminate in advance of the nearest intersection to provide adequate space for transit pull-offs.
- 9) Sidewalks no less than 5' wide shall be provided on both sides of the street.
- 10) VDOT will maintain up to the standard sidewalk width or as approved. A longitudinal joint should separate the VDOT sidewalk from the private sidewalk extension.
- 11) A one foot (1') wide sidewalk maintenance easement is required behind the right-of-way where the sidewalk extends past the right-of-way.
- 12) For AVENUE, sidewalk materials must extend to property line.
- 13) Pedestrian lights, gardens, and stair railings should be located behind the public right-of-way within private property.

**NOTES THAT APPLY TO MIXED-USE AVENUE ONLY**

- 14) An optional raised landscape median shall be between 16' and 36' wide (with a 2' offset) and will accommodate single left-turn lanes at intersections. Subject to waiver approval, the optional raised landscape median may be reduced to no less than 10' wide (with a 2' offset) and if provided, will not require single left-turn lanes at intersections.
- 15) Two (2) 14' wide travel lanes, one (1) in each direction are REQUIRED. These shared-use facilities can accommodate transit, trucks, personal vehicles, and on-road bicycle traffic. Lanes can be reduced to 11' wide at the discretion of the Director of Transportation if it can be shown that bicycle lanes can be provided on a parallel facility.

**NOTES THAT APPLY TO MIXED-USE OR RESIDENTIAL STREET ONLY**

- 16) Distinction between mixed-use street and residential street is determined by dooryards. Refer to the Prince William County Comprehensive Plan for dooryard standards.

Detail No. <b>650.XX</b>	<b>AVENUE &amp; STREET</b>	 <b>COUNTY OF PRINCE WILLIAM VIRGINIA</b>	STANDARD TYPICAL SECTION FOR MIXED-USE AVENUE, MIXED-USE STREET, AND RESIDENTIAL STREET IN URBAN AREAS SHEET 2 OF 2	Rev. No. Date 12/17/12
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**Attachment F**  
**Historical Commission Resolution**

HISTORICAL COMMISSION MEETING – MAY 14, 2013

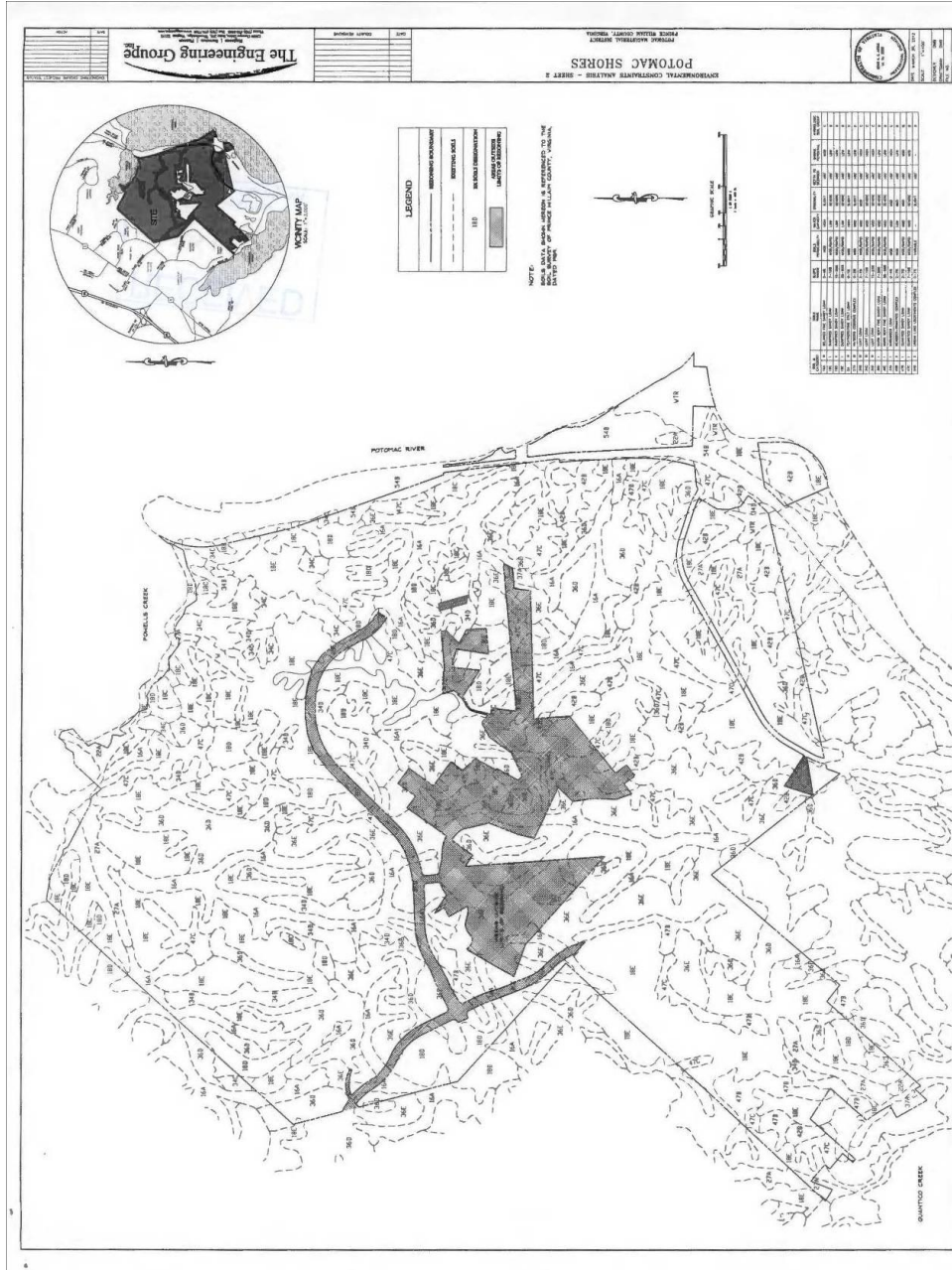
**DRAFT LAND DEVELOPMENT RECOMMENDATIONS**

Resolution to be approved at the June 11<sup>th</sup> meeting of the Historical Commission

<b><u>Case Number</u></b>	<b><u>Name</u></b>	<b><u>Recommendation</u></b>
SUP PLN2012-00434	Potomac Shores Town Center	No Further Work
REZ PLN2013-00256	Barnes Goodman Property	Request popular history of Methodism in the area
REZ PLN2013-00291	The Haven at Haymarket Crossing	Phase II and, if necessary, Phase III on site 44PW1838 and curation of artifacts with the County
SUP PLN2013-00310	CFP, LLC	No Further Work



# Attachment G Environmental Constraints Analysis



# Attachment G Environmental Constraints Analysis

